



City of Cockburn
Ordinary Council Meeting
Minutes

For Thursday, 10 September 2020

These Minutes are confirmed

Presiding Member's signature



Date: 8 October 2020

CITY OF COCKBURN
SUMMARY OF MINUTES OF THE ORDINARY COUNCIL MEETING
HELD THURSDAY, 10 SEPTEMBER 2020

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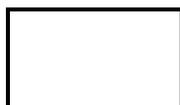
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CITY OF COCKBURN MINUTES OF ORDINARY COUNCIL MEETING HELD ON THURSDAY, 10 SEPTEMBER 2020

PRESENT:**ELECTED MEMBERS**

| | | |
|-----------------|---|---------------------------|
| Mr L Howlett | - | Mayor (Presiding Member) |
| Ms L Kirkwood | - | Deputy Mayor (arr 7.01pm) |
| Mr K Allen | - | Councillor |
| Mr M Separovich | - | Councillor |
| Ms P Corke | - | Councillor |
| Ms L Smith | - | Councillor (via eMeeting) |
| Dr C Terblanche | - | Councillor |
| Mr P Eva | - | Councillor |
| Ms C Stone | - | Councillor |

IN ATTENDANCE

| | | |
|---------------|---|---|
| Mr D Arndt | - | Acting Chief Executive Officer |
| Mr S Downing | - | Director Finance and Corporate Services |
| Mr C Sullivan | - | Director Engineering and Works |
| Mrs G Bowman | - | Acting Director Governance and Community Services |
| Ms R Pleasant | - | Acting Director Planning and Development |
| Mr A Lees | - | Manager Parks and Environment |
| Mr M Emery | - | Ranger and Community Safety Services Manager |
| Mr J Fiori | - | Risk and Governance Advisor |
| Mr S Cecins | - | Media and Communications Officer |
| Mrs B Pinto | - | Moderator |
| Ms S D'Agnone | - | Council Minute Officer |

1. DECLARATION OF MEETING

The Presiding Member declared the meeting open at 7.00pm.

“Kaya, Wanju Wadjuk Budjar” which means “Hello, Welcome to Wadjuk Land”

The Presiding Member acknowledged the Nyungar People who are the traditional custodians of the land on which the meeting is being held and pay respect to the Elders of the Nyungar Nation, both past and present and extend that respect to Indigenous Australians who are with us tonight.

This Council meeting will be electronically recorded and live streamed on the City’s website, except where Council resolves to go behind closed doors.



All recordings are retained in accordance with the General Disposal Authority for Local Government Records, produced by the State Records Office. A copy of the recorded proceedings will be available on the website within two business days of the Council meeting.

Images of the public gallery are not included in the webcast, however the voices of people will be captured and streamed.

Mayor Howlett reminded everybody present to be mindful of their conduct during the recorded meeting. Live streaming of meetings is a Council initiative, aimed at increasing transparency and openness, as well as making Council meetings more accessible to our community and those beyond.

Mayor Howlett advised that Cr Lee-Anne Smith, OAM, would be participating in the meeting via the eMeeting platform, and that he would be notifying Council, administration, and those attending the meeting in the public gallery, of Cr Smith's vote on each occasion.

Mayor Howlett further advised that the City was the recipient of a Parks and Leisure Australia (WA) PlaySpace (<\$0.5M) High Commendation Award in August 2020, for the Perena Rocchi Reserve and pump track in Yangebup.

This award reflects a great design outcome, and of course the use of the pump track by local children, teenagers and adults has met a need identified by the community, making it the fourth such pump track in the City.

Deputy Mayor Kirkwood entered the meeting 7.01pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

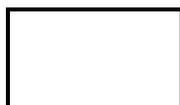
N/A

3. DISCLAIMER (Read aloud by Presiding Member)

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

4. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)

| | | |
|---------------------|---|-----------------------------------|
| Mayor Logan Howlett | - | Impartiality Interest - Item 23.1 |
| Cr Kevin Allen | - | Impartiality Interest - Item 23.1 |
| Cr Lee-Anne Smith | - | Impartiality Interest - Item 23.1 |



5. APOLOGIES AND LEAVE OF ABSENCE

| | | |
|---|---|------------------|
| Cr T Widenbar | - | Leave of Absence |
| Mr D Green Director Governance and Community Services | - | Annual Leave |

6. WRITTEN REQUESTS FOR LEAVE OF ABSENCE

Nil

7. RESPONSE TO PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE

Nil

8. PUBLIC QUESTION TIME

Alison Bolas, Hamilton Hill

Item 17.1 Adoption of the Animal Management and Exercise Plan 2020-2025

Q1. Given the predicted expansion of Cockburn population as outlined by the City's own population modelling, and with these new limitations on off lead exercise areas, will the City be designating additional off lead exercise areas for exercise and socialisation opportunities for the dogs?

If so, does the City guarantee that these areas will be significant for the current and expanded population?

A1. The Acting Manager Governance and Community Services advised that the Animal Management and Exercise Plan 2020-2025 will increase the number of dog off-leash areas in three phases, over the next two years, to meet the projected growth in need for the Cockburn district.

If the phase one and two trials are successful, and all three phases are implemented, all large ovals and reserves will include the ability to have dogs off-leash when there are no sporting or maintenance activities being undertaken. These additional exercise areas are anticipated to meet the projected growth in community need.

Claus Lorenzen, Coogee

Mayor Road

Q1. When will the work to straighten up Mayor Road east of Fawcett Road via a new road through to Cockburn Road commence?

A1. The Director Engineering and Works advised Officers have checked City records, and the proposal for the Beeliar Drive extension is not included in the Budget for the 2020/21 financial year. In addition, the proposal for the Beeliar Drive extension is not included in the current City Regional and Major Roadworks 2018–2031 list.

The road reserve for the Beeliar Drive extension is included in the Metropolitan Region Scheme and classified as Other Regional Road, which is a 'blue' road; in the MRS. The Main Roads WA Guide for Determining and Assigning Responsibility for Roads in Western Australia, states: ultimate planning control for 'red' and 'blue' roads lies with the Western Australian Planning Commission, through the Department of Planning.

Whilst the project is not currently included in the City's Regional and Major Roadworks 2018–2031 list, the proposed realignment is included in the City's draft Integrated Transport Strategy 2020-2030, which provides the vision and high level direction for the management and development of the City's transport network, and is also the guiding document for the City's various transport plans and strategies.

As such when the project transitions to the Regional and Major Roadworks list, a proposed schedule for the design and construction stage will indicate, subject to funding, and further subject to requirements for environmental assessment and survey as per relevant policies required by the Department of Planning. This will also include community consultation and a survey of existing environmental conditions.

Q2. Have there been any studies on how the work on the intersection at Stock Road and Beeliar Drive affects the volume of traffic on Mayor Road?

A2. The Director Engineering and Works advised that works at the intersection of Stock Road and Beeliar Drive are predicted to improve traffic congestion on all approaches to the traffic signals, and at the nearby Rockingham Road and Mayor Road roundabout, with works expected to commence in November 2020.

During the reconstruction of the Stock Road and Beeliar Drive intersection, temporary traffic management of traffic flows and safety will be managed by Main Roads WA, in accordance with the Main Roads WA Code of Practice for Traffic Management for Works on Roads.

To subscribe to the project and receive progress updates, or to request further information, residents can refer to the following Main Roads WA webpage link: <https://www.mainroads.wa.gov.au/projects-initiatives/projects/metropolitan/stock-road-and-beeliar-drive/>.



- Q3. When was the latest information/data on speed and traffic volume collected and reviewed by the City for Mayor Road?
- A3. The Director Engineering and Works advised that Traffic Data has been collected on Mayor Road recently in the following locations:
- Cockburn Road and Peel Road in February 2019 (additional counts to be undertaken this September 2020),
 - Between Peel Road to Hamilton Road in June 2020,
 - Between Hamilton Road to Fawcett Road February 2019, and August 2019,
 - Between Fawcett Road to Rockingham Road in February 2019.
- Q4. Are there plans to reduce traffic volume and speeding issues on Mayor Road in the foreseeable future?
- A4. The Director Engineering and Works advised there are no current plans identified in the City's Regional and Major Roadworks 2018–2031 Plan to redirect traffic from Mayor Road, which is identified as a District Distributor B Road, with current volumes and speed limit consistent with this classification.

In regards to alleged speeding issues, the surveyed 85th percentile speeds on various sections of Mayor Road have not indicated a speed that would meet the benchmark for referral to the WA Police Traffic Intelligence Services, in accordance with the WALGA Local Government Speed Enforcement Program Guidelines for Local Governments.

While existing traffic speeds do not warrant any further action by the City at this time, the following agencies are best placed to assist with incidents of hooning or anti-social driving behaviour:

Residents are encouraged to contact local police directly for speeding issues. The Police have an online reporting facility: <https://www.police.wa.gov.au/Police-Direct/Traffic>.

Judy Fogarty, Spearwood

Manning Park Ridge

- Q1 Can Mr Sullivan please provide evidence to the whereabouts of the mountain bike trails he was advised have been shut down? (Meeting 13 August 2020, Page 8, A4)
- A1. The Director Engineering and Works advised the trail that was closed runs parallel to the Manning Stairs. This trail was closed due to concerns about bicycles coming down the hill at speed, exiting directly out onto Azelia Road and endangering themselves and those using the stairs. A number of illegal structures have also been removed from many of the unsanctioned trails due to safety concerns.



Q2. Why is the City of Cockburn considering mountain bike trails in the significantly smaller 39 hectare Manning Park when the Western Australian Mountain Bike Management Guidelines state that 250 hectares is the recommended area size for mountain bike activity and network trails?

A2. The Director Engineering and Works advised that Manning Park comprises more than 90 hectares and the road reserve approximately 16 hectares. The Western Australian Mountain Bike Management Guidelines are guidelines only, and not standards. The area identified for mountain bike trails in the Concept Plan was 61 hectares. The State Government Perth Peel Mountain Bike Master Plan has identified Manning Park as an area suitable for a local mountain bike trail network.

Examples of other trails that have been constructed on lesser areas include: Margaret River (41 hectares), Dunsborough (66 hectares), and Pemberton (114 hectares).

There are a number of site specific factors influencing the appropriate scale of any trail network including health, intactness of the ecosystem, context (proximity of other trails or networks), and other recreation pursuits on the site.

In an urban context, finding a site of the scale in the guidelines is difficult and as such smaller sites are considered, but with careful consideration of the carrying capacity of the particular site.

As seen in existing trail development throughout the South West, the scale of many sites is well below the stated threshold, yet these developments have proven sustainable from both an environmental and social perspective.

The City believes that activating the two quarries and forming two connecting trails will adequately cater for mountain bikes without further impacting the park. The closure and revegetation of many of the unsanctioned trails will enhance the environmental values of Manning Park.

Q3. Why has the City of Cockburn agreed to the Manning Park Bush Forever Site 247 being used as a mountain bike trail network when the Cities of Perth, Claremont, Swan, Canning, Gosnells and others, which have larger conservation areas, in order to preserve the environment, made the decision not to allow mountain bikers riding in their conservation areas, banned it and have controlled it?

A3. The Director Engineering and Works advised that the Beeliar Regional Park Management Plan identifies the upland area of Manning Park as suitable for cycle trails with the purpose identified as recreation.

The City intends to enhance the environmental values of Bush Forever Site 247 by closing down and rehabilitating the unsanctioned trails once the sanctioned trails have been supported and developed.



Wanda Keightley, Coolbellup*Item 17.1 Adoption of the Animal Management and Exercise Plan 2020-2025*

Q1. Regarding the current advisory board for the animal welfare management, I am currently aware that the people on this board perhaps do not have the knowledge and education of current best practice standards for the management of animals and their welfare and behaviour.

Would Council consider a review of the advisory board with the view to increasing the quality of the engagement and the information that comes out of this group?

A1. The Acting Director Governance and Community Services advised that as she was not aware of which advisory board the question related to, nor that she was aware the City had an Advisory Board, the question would be taken on notice.

Leanne Chaproniere, Jandakot*Glen Iris Golf Course Estate - Firebreaks*

Q1. I am President of Jandakot Resident and Ratepayers Association. We were advised today that DFES and Council have met with the Project Managers of the Glen Iris Estate in relation to firebreaks on the property, and ask how and if Council will monitor any damage or removal of the federally protected banksias within the boundaries of the property that the developer's contractor could make, if not identified as such?

A1. The Acting Director Planning and Development Services advised that when the developer commences the Scheme Amendment Structure Plan process, that will be when the City will require relevant studies, including a Bushfire Management Plan.

Q2. The Council has met with them this week, to talk about the firebreaks that are going in before 1 November 2020. What we are worried about is that because they are new they do not understand where the banksias are now. Because some of the banksias are within three metres from the fence line, we are worried that the contractor will just bulldoze the whole three metres from the fence line, which does include a number of banksias.

A2. The Ranger and Community Safety Services Manager advised that firebreaks issues on the unused area of land is a matter the City is well aware of. Regarding the concern about the clearing of banksia woodland to create firebreaks, this is certainly not the discussions City Officers have had with DFES.

What the City is concerned about, going into summer, is ensuring its fire management has been well planned. There have been no approvals from the City, under the Bushfires Act, to clear any vegetation. The aim is to keep existing trails where they are, to a standard acceptable under our fire control order.



- Q3. At the briefing we gave earlier this year, we raised the issue of asbestos being buried on the site. Has Council taken up this matter in any way, and will they in future ask for an asbestos assessment for the site?
- A3. The Acting Director Planning and Development Services advised that the City has raised this issue and suggested the developer investigate this matter. As part of the assessment process, such matters can be considered.

Dean Maroon, Coolbellup

Manning Park Ridge - Mountain Bike Trail

- Q1. Has the Council set a date for more public consultation in regards to the proposed mountain bike trail?
- A1. The Director Engineering and Works advised that a working group, controlled by an independent facilitator, will be formed.
- Q2. Within the scope of these meetings, will another site be considered?
- A2. The Director Engineering and Works advised that no sites outside Manning Park are being considered, and that the working group will be focusing on the current project, being Manning Park.
- Q3. So the first meeting will be soon, is that right, and will it be a public or private meeting?
- A3. The Director Engineering and Works advised that, following the recent public engagement process, it is a working group that has been recruited, which differs from a public meeting.
- The independent facilitator of the working group will seek to recruit groups representative of the various interest groups associated with the project.
- Q4. Who will those interest groups be?
- A4. The Director Engineering and Works advised the working group will comprise of various user groups of the site, and also the local resident group.
- Q5. Can myself, who likes walking on the ridge, be involved?
- A5. The Director Engineering and Works advised that all residents will have an opportunity to put their name forward for inclusion when the independent facilitator calls for submissions to join the working group.
- Q6. Will that be soon?
- A6. The Director Engineering and Works advised that an exact date is not currently know, however when available, will be provided.

Andrew Joske, Hilton

The Presiding Member advised that as Mr Joske was not present at the meeting, a response to his questions would be provided in writing.



9. CONFIRMATION OF MINUTES

9.1 (2020/MINUTE NO 0183) MINUTES OF THE ORDINARY COUNCIL MEETING - 13/08/2020

RECOMMENDATION

That Council confirms the Minutes of the Ordinary Council Meeting held on Thursday, 13 August 2020 as a true and accurate record.

COUNCIL DECISION

MOVED Cr P Eva SECONDED Cr C Terblanche

That the recommendation be adopted.

CARRIED 9/0

10. DEPUTATIONS

The Presiding Member invited the following deputations:

- **Matthew Hayes, Department of Local Government, Sport and Cultural Industries**, in relation to Item 17.1 Adoption of the Animal Management and Exercise Plan 2020-25.
The Presiding Member thanked the deputation for their presentation.
- **Megan Jaceglav**, in relation to Item 17.1 Adoption of the Animal Management and Exercise Plan 2020-25.
The Presiding Member thanked the deputation for their presentation.

Cr Philip Eva left the meeting at 7.43pm and returned at 7.51pm.

- **Ms Jacqui Rownes**, in relation to Item 16.1 Aubin Grove Skate Park.
The Presiding Member thanked the deputation for their presentation.
- **Derek Olsen**, in relation to Item 16.1 Aubin Grove Skate Park.
The Presiding Member thanked the deputation for their presentation.
- **Brendan Sobczak**, in relation to Item 16.1 Aubin Grove Skate Park.
The Presiding Member thanked the deputation for their presentation.
- **Daniella Mrdja, Urbanista Town Planning**, in relation to Item 14.2 Proposed Structure Plan - Lot 2 (7) Fawcett Road, Lake Coogee.
The Presiding Member thanked the deputation for their presentation.

Cr Kevin Allen left the meeting at 8.19pm and returned at 8.21pm.

11. BUSINESS LEFT OVER FROM PREVIOUS MEETING (IF ADJOURNED)

Nil

12. DECLARATION BY MEMBERS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS CONTAINED IN THE BUSINESS PAPER PRESENTED BEFORE THE MEETING

AT THIS POINT IN THE MEETING, THE TIME BEING 8.33PM THE FOLLOWING ITEMS WERE CARRIED BY 'EN BLOC' RESOLUTION OF COUNCIL

| | | | | | | |
|------|------|------|------|------|------|------|
| 14.1 | 15.1 | 16.2 | 19.1 | 21.1 | 22.1 | 23.2 |
| | 15.3 | | | | 22.2 | |
| | | | | | 22.4 | |



13. COUNCIL MATTERS

13.1 (2020/MINUTE NO 0184) PROPOSED AMENDMENT TO STANDING ORDERS LOCAL LAW 2016

| | |
|--------------------|---|
| Author(s) | D Green |
| Attachments | 1. Standing Orders Amendment No. 1 Local Law 2020 2. Amended Policy 'Council Meetings' |

RECOMMENDATION

That Council adopt the amendment to its *Standing Orders Local Law 2016* as follows:

- (1) in Clause 4.9 'Notices of Motion' subclause (3) and subclauses (5)(a) and (b), delete 4.10 where it appears and insert 4.9;
- (2) in Clause 14.2 'Method of Taking Vote':
 - (a) in subclause 14.2(1) delete 'by a show of hands';
 - (b) in subclause 14.2(2) insert 'or any electronic system installed for this purpose', and
 - (c) delete subclauses 14.2(3)(a) and (b); as shown in the attachments to the Agenda;
- (3) publish the adopted City of Cockburn Standing Orders Amendment Local Law No.1 2020 in the Government Gazette;
- (4) provide a copy of the gazetted City of Cockburn Standing Orders Amendment Local Law No.1 2020 to the Minister for Local Government; and
- (5) upon gazettal give local public notice in a newspaper circulating in the district.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr P Corke

That Council adopt the amendment to its *Standing Orders Local Law 2016* as follows:

- (1) as recommended;
- (2) in Clause 14.2 'Method of Taking Vote':
 - (a) in subclause 14.2 (1) insert after show of hands 'or by any electronic system installed for this purpose';
 - (b) as recommended and
 - (c) as recommended;
- (3) as recommended;
- (4) as recommended; and
- (5) as recommended.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 9/0



Reason for Decision

If Council is required to conduct an electronic meeting as we have had during COVID-19, it still needs to have the opportunity to vote by hand.

Background

At the Annual Electors' Meeting held on 4 February 2020, the following motion was carried:

That the Council Policy 'Council Meetings' be updated with one addition, this being Item (6):

(6) Elected Member Voting

Elected Members are required to vote on each item presented in the agenda document. Voting outcomes will be recorded in the minutes directly below the resolution and will contain the following:

- *Carried summary*
- *Elected Member name*
- *Elected Member vote recorded as Yes/No.*

The motion was carried by the electors present – five (5) votes for and two (2) votes against.

The proposed City of Cockburn Standing Orders Amendment Local Law No.1 2020 was adopted by Council at its meeting in June 2020 for public comment.

Submission

N/A

Report

The purpose of the amended Local Law is to expand the method of voting at Council Meetings to allow for greater flexibility and transparency of Elected Members and to attend to a minor drafting error.

The effect of the amended Local Law is to provide for an electronic voting facility to be used by members when voting at Council Meetings.

It is a requirement of the *Local Government Act 1995* (the Act) that all motions passed at an Electors Meeting are formally considered by Council.

The voting process at Council Meetings is governed by the *Local Government Act 1995* (Section 5.21) and the *City of Cockburn Standing Orders Local Law 2016* (Clause 14.2).



While it is a requirement for all Elected Members in attendance at a Council Meeting to vote in such a way so that their vote is not a secret, it is largely unknown how individual members voted on matters contained in the meeting agenda.

From a perspective of accountability and transparency, there can be no doubt that the inclusion of Elected Members' names against all decisions of Council will provide a more complete record of the meeting proceedings than currently is the case in relation to City of Cockburn Council meetings. While the Act provides the opportunity for any member to request these details to be recorded, it is a discretionary practice which is only initiated at the request of an Elected Member, after a vote is taken.

Accordingly, incorporating such a mechanism into a Council policy would provide an obligation on the Presiding Member, or members, to trigger the statement, as the Act does not stipulate any such requirement to do so. This matter was considered at the Delegated Authority and Policy (DAP) Committee Meeting on 28 May 2020 and was adopted at the 11 June 2020 Council Meeting.

Including the names of members to the voting outcomes at Council meetings would provide an extra layer of accountability to the constituents, in addition to elevating the level of detail available for public view.

Currently, voting at Council meetings is governed by the requirement for all voting to be demonstrated on a "show of hands" basis only. The audio system recently installed in the Council meeting chambers is able to provide for electronic voting to be implemented by those in attendance at a meeting. This would make the record of voting process for minute taking purposes a seamless exercise, however, it requires the Standing Orders provisions which govern the voting method to be altered to facilitate an electronic voting facility. The proposed changes are shown in the attachment.

This process complements the adopted Policy that prompts the Presiding Member at Council meetings to call for the names of all members to be recorded as voting either "for" or "against" each motion considered.

In addition, Clause 4.9 requires some minor administrative attention to correct a numerical error which has been discovered.



Strategic Plans/Policy ImplicationsListening and Leading

A community focused, sustainable, accountable and progressive organisation.

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

Minor associated advertising costs are provided for in the City's Governance budget.

Legal Implications

Sections 3.12 and 5.21 of the *Local Government Act 1995* and Clause 14.2 of the *City of Cockburn Standing Orders Local Law 2016* refer.

Community Consultation

The proposal was advertised in the Cockburn Gazette on 25 June 2020 for a period of six weeks. Advertising closed on 7 August 2020. A notice of the proposal was advertised at all City Libraries, on the City website, and on the Notice Board at the Administration Offices.

No submissions were received during this period.

Risk Management Implications

There is a "Low" level of "Brand/Reputation" and "Compliance" risk associated with this item.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



LOCAL GOVERNMENT ACT 1995*City of Cockburn***STANDING ORDERS AMENDMENT NO.1 LOCAL LAW 2020**

Under the powers conferred by the *Local Government Act 1995* and all other powers enabling it, the Council of the City of Cockburn resolved on to make the following local law.

1. Citation

This local law may be cited as the *City of Cockburn Standing Orders Amendment No1 Local Law 2020*.

2. Commencement

This local law comes into operation 14 days after the day of its publication in the *Government Gazette*.

3. Principal Local Law amended

The local law amends the *City of Cockburn Standing Orders Local Law 2016* as published in the *Government Gazette* on 22 September 2016, and as amended on 21 March 2017 and 1 December 2017.

4. Part 4 amended

- (1) In subclause 4.9 (3) delete "4.10" and insert "4.9".
- (2) In subclause 4.9 (5) (a) delete "4.10 (1)" and insert "4.9 (1)".
- (3) In subclause 4.9 (5) (b) delete "4.10 (1)" and insert "4.9 (1)".

5. Part 14 Amended

- (1) In subclause 14.2 (1) delete "by a show of hands".
 - (2) In subclause 14.2 (2) insert "or any electronic system installed for this purpose" after the word "hands".
 - (3) Delete subclause 14.2 (3) (a) and (b).
-



Dated.....

The Common Seal of the City of Cockburn was affixed under the authority of a resolution of Council in the presence of –

Logan K Howlett, Mayor

Acting Chief Executive Officer



| | |
|--|-------------------------|
| Title | Council Meetings |
| Policy Number (Governance Purpose) | |



Policy Type

Council

Policy Purpose

This policy establishes the requirements and standards for Council Meetings and Standing Committee Meetings which apply in accordance with the Local Government Act (Part 5) and the City of Cockburn Standing Orders Local Law 2016 (as amended).

Policy Statement

This policy applies to Council Meetings and Council Standing Committee Meetings.

All Council Standing Committee Meetings will be held in accordance with the adopted Terms of Reference of the particular Committee, or as otherwise determined by Council resolution.

(1) Meeting Times

1. Ordinary Council Meetings will be conducted on the second Thursday of each month at 7.00pm in the Council Chamber and are open to the public.
2. The Council meeting will consider matters as listed on the Council Agenda paper.
3. An Ordinary Council Meeting is not conducted during January of each year and any required business will be conducted as a Special Council Meeting during this specific period.
4. Any further variation to the date, place and/or time of Council Meetings will be subject to the resolution of Council.

(2) Provision of Committee Business Papers to Elected Members

Council appoints Committees under the Local Government Act, 1995, the Business Papers for which are to be provided to all Elected Members of Council in advance of relevant Committee meetings being conducted.

1. Upon a meeting of a Committee established by Council being called in accordance with Council's Standing Orders Local Law, a notice of the meeting will be forwarded to:
 - a. all Committee members, and
 - b. other Elected Members who are not a member of the Committee for information

[1]

| | |
|--|-------------------------|
| Title | Council Meetings |
| Policy Number (Governance Purpose) | |



2. The Business Papers for the Committee, including the Agenda, related attachments and where applicable, the Minutes of the previous meeting of the Committee, shall be forwarded to all Committee members with the Notice of Meeting, if possible, at least 72 hours prior to the commencement of the Meeting.
3. Notwithstanding anything contained in this Policy, any Elected Member may request a copy of Committee documentation to be provided to them.

(3) Seating Arrangements

Following the election of a new Council from time to time, the seating arrangements in the Council Chamber will be allocated on the following basis:

1. The elected Mayor will be seated at the head of the table, facing the public gallery.
2. The CEO will also be seated at the head of the table, immediately to the left of the Mayor.
3. The elected Deputy Mayor will be seated immediately to the right of the Mayor.
4. The Mayor will be responsible for the allocation of the remaining Councillors (with Ward members grouped together, wherever possible) who are to be seated in the Council Chamber.

(4) Reports

It is critical that reports prepared by staff for Council contain adequate information on which the Council can make an informed decision.

1. Principles

Reports prepared by officers for the Council's consideration are to:

- a. be according to law;
- b. take account of any State or Council Policy;
- c. have regard for the Council's Strategic Community Plan;
- d. be balanced and objective;
- e. be technically correct;
- f. be properly researched using relevant information and data;
- g. ensure procedural fairness;
- h. include options, consequences and associated impacts where appropriate;
and
- i. include expert opinion or advice where necessary

2. Recommendations

Recommendations prepared by officers for the Council's consideration are to:

- a. be clear and unambiguous;
- b. be implementable;
- c. be professional and ethical;

[2]

| | |
|--|-------------------------|
| Title | Council Meetings |
| Policy Number (Governance Purpose) | |



- d. not expose the Council to unreasonable risk or liability;
- e. have regard for the interests of the applicant/submitter as well as the wider community; and
- f. include the administrative actions to enable implementation of the Council's decision

(5) Proposed Amendments by Elected Members to recommendations for Council Meeting

Elected Members are encouraged to provide suggested alternatives to officer recommendations to the Administration in advance of the relevant Council meeting, to enable consistency and clarity in terminology being proposed, as well as ensuring the legality of any proposed amendments.

1. Any proposed amendments to officer recommendations contained in the Council Agenda Paper are to be made personally by the Elected Member either with or without input from the relevant staff member.
2. All proposed amendments are to be forwarded to the Minute Clerk following the distribution of the Agenda Paper to Elected Members, by no later than 10.00am on the day of the Council Meeting. A copy of the proposed amendment will be circulated to all other Elected Members.
3. A reason for the proposed amendment must be included with the information provided in accordance with Clause (5) 2 above.
4. Elected Members proposing amendments of a similar nature on the same item(s) may consult with each other in order to achieve a consensus position on any item and agree to withdraw any amendment, or part thereof, or to agree to provide an alternative of similar meaning and/or wording to one, or another, of the presented amendments.
5. Having received all proposed amendments in accordance with this Policy, a listing will be compiled with all proposed amendments received, together with relevant reasons, for provision to the Council Meeting. Where there are multiple proposed amendments for the same item, they will be listed in the order they have been received from Elected Members.
6. When called upon to do so at the relevant time during the Council Meeting by the Presiding Member, the CEO will inform the Meeting of the items on the Agenda Paper for which notice of a proposed amendment/s has been received.
7. At the relevant point of the Council Meeting, the Presiding Member shall invite the Elected Member proposing the amendment(s) to move the alternative motion accordingly. In the case where more than one proposal has been received, the Presiding Member shall call for the amendment(s) to be put in order of their receipt as per Clause (5) 5 above and thereafter dealt with in accordance with Council's Standing Orders.

(6) **Voting at Council and Standing Committee Meetings**

[3]



| | |
|--|-------------------------|
| Title | Council Meetings |
| Policy Number (Governance Purpose) | |



1. When called upon to do so by the Presiding Member, all Elected Members present at a formal meeting of the Council or Standing Committee are required to vote on each item put to the Meeting, unless required to leave the Meeting after declaring a financial or proximity interest in the item;
2. Voting may be called by the Presiding Member by way of a show of hands, or by any electronic system installed for this purpose; and
3. Once the votes have been cast, the Presiding Member shall ensure that the names of all elected members voting for and against a motion are recorded in the Minutes of the Meeting, unless there is a unanimous decision on a motion, which shall be recorded in the Minutes as such

| | |
|---|------------------------------|
| Strategic Link: | Corporate Governance Charter |
| Category | Elected Members |
| Lead Business Unit: | Governance |
| Public Consultation: (Yes or No) | No |
| Adoption Date: (Governance Purpose Only) | 14 March 2019 |
| Next Review Due: (Governance Purpose Only) | March 2021 |
| ECM Doc Set ID: (Governance Purpose Only) | 4133909 |

13.2 (2020/MINUTE NO 0185) MEMBERSHIP - GRANTS AND DONATIONS COMMITTEE**Author(s)** B Pinto**Attachments** N/A**RECOMMENDATION**

That Council appoints Cr Tom Widenbar as a Member of the Grants and Donations Committee.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

MOVED Cr M Separovich SECONDED Cr P Eva

That the recommendation be adopted.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 9/0

Background

By email received on 12 August 2020, Cr Widenbar has requested that he be appointed as a member of the Grants and Donations Committee.

Submission

N/A

Report

In his nomination, Cr Widenbar states that the reason for his nomination to the Grants and Donations Committee as *'I have had questions on grants and donations issued from the previous two committee meetings, and feel it would be more efficient to raise these questions at the meeting rather than at Council'*.

The power for Council to appoint Committee Members is provided by the *Local Government Act 1995* (Section 5.10). The membership in the Terms of Reference for this Committee states that at least three members be appointed. Therefore, the appointment of additional members can be approved by Council should any other Elected Member wish to nominate.

It is therefore recommended that Council consider the nomination, as received.



Strategic Plans/Policy ImplicationsListening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.
- Deliver value for money through sustainable financial management, planning and asset management.

Budget/Financial Implications

N/A

Legal Implications

Section 5.10 of the *Local Government Act 1995* refers.

Community Consultation

N/A

Risk Management Implications

There is a “Low” level of “Compliance” Risk associated with this item.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

N/A



13.3 (2020/MINUTE NO 0186) MINUTES OF THE DELEGATED AUTHORITIES AND POLICIES COMMITTEE MEETING - 27 AUGUST 2020

Author(s) B Pinto

Attachments 1. Delegated Authorities and Policies Committee Meeting Minutes - 27 August 2020

RECOMMENDATION

That Council receive the Minutes of the Delegated Authorities and Policies Committee Meeting held on 27 August 2020 and adopt the recommendations contained therein.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

(2020/MINUTE NO 0187) EXTENSION OF MEETING

MOVED Mayor L Howlett SECONDED Cr K Allen

That Council, pursuant to Clause 14.3 of City of Cockburn Standing Orders, the time being 8.57pm, extend the meeting for a period of 60 minutes to conclude unfinished business of Council.

CARRIED 9/0

COUNCIL DECISION

MOVED Cr M Separovich SECONDED Cr P Eva

That the recommendation be adopted.

ABSOLUTE MAJORITY REQUIRED – LOST 5/4

COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr M Separovich

That Council receive the Minutes of the Delegated Authorities and Policies Committee Meeting held on 27 August 2020 and adopt the recommendations contained therein, excluding Item 8.1, which requires further consideration at the next Committee Meeting.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 9/0

Reason for Decision

More work is required on the Delegated Authority issue to ensure Elected Members receive the full and comprehensive legal advice that was received by the City, so that we can be certain that what we are voting on is clear and correct, and that we are not shooting ourselves in the foot.



Background

The Delegated Authorities and Policies Committee (DAP) conducted a meeting on 27 August 2020. The Minutes of the meeting are required to be presented.

Submission

N/A

Report

The Committee recommendations are now presented for consideration by Council, and if accepted, are endorsed as the decisions of Council. Any Elected Member may withdraw any item from the Committee meeting for discussion and propose an alternative recommendation for Council's consideration. Any such items will be dealt with separately, as provided for in Council's Standing Orders.

The primary focus of this meeting is to review the Delegated Authorities pursuant to the *Local Government Act 1995*. These delegations were presented to the May 2020 DAP Committee Meeting for consideration and were deferred for further consideration. Also presented are Delegated Authorities extraneous to the *Local Government Act 1995* which have now been reviewed.

There were a number of Policies that referenced Administration and Strategic Delegated Authorities. The Administrative and Strategic Delegated Authorities have been reviewed now that the Policy Project has been completed. The majority of these delegations are no longer required as a number of Policies that relate to these delegations were either consolidated into one Policy or identified to be converted to Administration Policies, Procedures, or were to be deleted.

Also included is Local Planning Policy LPP5.6 'Vehicle Access' which has been amended.

In addition to the above, there are two new delegations namely, 'Debt Write-off, Concession or Waiver' under the Local Government Act and the 'Community Funding to Support Local Economic Development' associated with the same Policy title which is to be before Council for adoption.

Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.



- Deliver value for money through sustainable financial management, planning and asset management.

Budget/Financial Implications

As contained in the minutes.

Legal Implications

As contained in the minutes.

Community Consultation

As contained in the Minutes.

Risk Management Implications

Failure to adopt the Minutes may result in inconsistent processes and lead to non-conformance with the principles of good governance, and non-compliance with the *Local Government Act 1995*.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil





City of Cockburn
Delegated Authorities and Policies
Committee
Minutes

For Thursday, 27 August 2020

These Minutes are subject to confirmation

Presiding Member's signature

Date: 26 November 2020



DAP 27/08/2020

CITY OF COCKBURN

SUMMARY OF MINUTES OF THE DELEGATED AUTHORITIES AND POLICIES COMMITTEE HELD ON THURSDAY, 27 AUGUST 2020

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DAP 27/08/2020

**CITY OF COCKBURN
MINUTES OF DELEGATED AUTHORITIES AND POLICIES
COMMITTEE HELD ON THURSDAY, 27 AUGUST 2020**

PRESENT:**ELECTED MEMBERS**

| | | |
|-----------------|---|-------------------------------|
| Ms P Corke | - | Councillor (Presiding Member) |
| Mr L Howlett | - | Mayor |
| Mr M Separovich | - | Councillor |
| Dr C Terblanche | - | Councillor |
| Mr T Widenbar | - | Councillor |

IN ATTENDANCE

| | | |
|---------------|---|---|
| Mr D Arndt | - | Acting Chief Executive Officer |
| Mr S Downing | - | Director Finance and Corporate Services |
| Mr C Sullivan | - | Director Engineering and Works |
| Mrs G Bowman | - | Acting Director Governance and Community Services |
| Ms R Pleasant | - | Acting Director Planning and Development |
| Mrs B Pinto | - | Governance Officer |

1. DECLARATION OF MEETING

The Acting Chief Executive Officer declared the meeting open at 6.01pm.

2. APPOINTMENT OF PRESIDING MEMBER (If required)

The Acting Chief Executive Officer advised the meeting that due to the absence of the Presiding Member, Cr Stone, there was a need to appoint a Presiding Member for the meeting.

The Acting Chief Executive Officer advised the meeting that he had received two nominations for Presiding Member, one each from Cr Phoebe Corke and Cr Michael Separovich, and called for any further nominations from those present. No further nominations were forthcoming. He then asked the two nominees if either wished to withdraw their nomination. Both nominees declined.

A ballot was conducted and votes counted, after which the Acting Chief Executive Officer declared Cr Corke Presiding Member for the Committee meeting. Cr Corke assumed the role of Presiding Member.

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3. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)

Nil

4. APOLOGIES AND LEAVE OF ABSENCE

Cr Stone - Apology
 Mr D Green,
 Director Governance and Community Services - Apology

5. CONFIRMATION OF MINUTES

5.1 (2020/MINUTE NO 0014) MINUTES OF THE DELEGATED AUTHORITIES AND POLICIES COMMITTEE MEETING - 28/05/2020

RECOMMENDATION
 That Committee confirms the Minutes of the Delegated Authorities and Policies Committee Meeting held on Thursday, 28 May 2020 as a true and accurate record.

COMMITTEE RECOMMENDATION
 MOVED Cr M Separovich SECONDED Cr C Terblanche
 That the recommendation be adopted.
CARRIED 5/0

6. BUSINESS LEFT OVER FROM PREVIOUS MEETING (IF ADJOURNED)

Nil

7. DECLARATION BY MEMBERS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS CONTAINED IN THE BUSINESS PAPER PRESENTED BEFORE THE MEETING

Nil

AT THIS POINT IN THE MEETING, THE TIME BEING 6.07PM, THE FOLLOWING ITEMS WERE CARRIED BY 'EN BLOC' RESOLUTION OF THE COMMITTEE

| | | | | |
|-----|-----|------|------|------|
| 8.1 | 9.1 | 10.1 | 12.1 | 14.1 |
| 8.3 | | | | |



DAP 27/08/2020

Item 8.1

8. COUNCIL MATTERS**8.1 (2020/MINUTE NO 0015) ANNUAL REVIEW OF DELEGATED AUTHORITIES MADE UNDER THE LOCAL GOVERNMENT ACT 1995****Author(s)** B Pinto

- Attachments**
1. Proposed Amendment - DA-LGACS2 'City of Cockburn (*Local Government Act 1995*) Local Laws - Grants'
 2. Proposed Amendment - DA-LGACS5 'City of Cockburn (*Local Government Act 1995*) Local Laws - Use of Reserves'
 3. Proposed Amendment - DA-LGACS11 'City of Cockburn Local Laws – Application to Keep More Than Two Dogs at a Residential Property'
 4. Proposed Amendment - DA-LGACS12 'Usage and Management of Community and Sporting Facilities'
 5. Proposed Amendment - DA-LGACS13 'Community Funding for Sport Clubs and Individuals'
 6. Proposed Deletion - DA-LGAES6 'Local Government Act 1995 - Authority to Call Public Meetings'
 7. Proposed Deletion - DA-LGAES2 'Local Government Act 1995 - Appointment of Authorised Persons'
 8. Proposed Deletion - DA-LGAES5 '*Local Government Act 1995* - Legal Proceedings'
 9. Proposed Deletion - DA-LGACS3 'City of Cockburn (*Local Government Act 1995*) Local Laws - Notices'
 10. Proposed Deletion - DA-LGACS4 'City of Cockburn (*Local Government Act 1995*) Local Laws - Stallholders'
 11. Proposed Deletion - DA-LGAEW1 'City of Cockburn (*Local Government Act 1995*) Local Laws - Parking Controls'
 12. Proposed Deletion - DA-LGAEW3 '*Local Government Act 1995* - Dangerous Trees on Private Land'
 13. Proposed Deletion - DA-LGAPD1 'City of Cockburn (*Local Government Act 1995*) Local Laws 2000 (as amended) - Signs, Hoardings and Bill Postings'
 14. Proposed Deletion - DA-LGAFCS1 '*Local Government Act 1995* - Advertising Proposed Differential Rates'
 15. Proposed Deletion - DA-LGAFCS5 '*Local Government Act 1995* - Recovery of Rates and Service Charges - Leased Properties'

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Item 8.1

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RECOMMENDATION

That Council:

- (1) adopt proposed amendments to the Delegated Authorities made under the *Local Government Act 1995*, as shown in the attachments to the Agenda;
- (2) delete Delegated Authorities made under the *Local Government Act 1995*, that have been identified under Part 9 Division 2 of the *Local Government Act 1995*, as listed in the report;
- (3) adopt Delegated Authorities under the *Local Government Act 1995*, that have no changes as listed in the report; and
- (4) update the Delegated Authority Register accordingly.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**COMMITTEE RECOMMENDATION**

MOVED Mayor L Howlett SECONDED Cr C Terblanche

That the recommendation be adopted.

CARRIED 5/0**Background**

Section 5.46 of the *Local Government Act 1995* requires local governments to keep and maintain a Register of Delegations. In accordance with section 5.46(2), Governance Services has coordinated a full review of the Delegations made under the *Local Government Act*, as required.

The review of the Delegations made under the *Local Government Act* is now complete. The Delegated Authorities and Policies (DAP) Committee is now required to consider and recommend adoption of the proposed amendments to Council.

Submission

N/A

Report

In accordance with s5.46 of the *Local Government Act 1995* the City currently maintains a Register of Delegated Authority.

Section 5.46(2) of the Act requires a complete review of the Register of Delegations to be conducted at least once every financial year. A review was undertaken earlier in 2020 and a report presented to the May 2020 DAP Committee Meeting. The recommended amendments were deferred by a subsequent decision of Council for a more thorough review, which has now been undertaken and is now presented for adoption.



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DAP 27/08/2020

Item 8.1

In consultation with the relevant officers, changes were made to delegations, in line with their objective.

Listed in the tables below are the proposed amended delegations, delegations using the 'acting through' provisions, delegations to be retained in their present form and deleted delegations for consideration by Council.

| Delegation | Proposed Amendment | Comments |
|---|---|---|
| DA – LGACS2 'City of Cockburn (<i>Local Government Act 1995</i>) Minor Funding' | Title change 'Community Funding for Community Organisations and Individuals (Grants, Donations and Sponsorship) Conditions / Guidelines updated to include additional category | Title updated to reflect title of Policy. Now a delegation under the Policy associated with it. (Community Funding for Community Organisations and Individuals (Grants, Donations and Sponsorships)) Additional category will provide financial assistance grants in relation to impacts of COVID-19. |
| DA – LGACS5 'City of Cockburn (<i>Local Government Act 1995</i>) Local Laws – Use of Reserves | Conditions / Guidelines updated Autonomy of Discretion updated Sub-Delegate/s updated to include additional staff. | Amendments have been undertaken in accordance with current operational procedures whereby Recreation Services staff has authority to approve reserve bookings within existing terms and conditions. Fee reductions and extensions are to be approved by management which is consistent with Delegated Authority 'Usage and Management of Community and Sporting Facilities' |
| DA – LGACS11 'City of Cockburn Local Laws – Application to Keep More Than Two Dogs at a Residential Property' | Conditions / Guidelines updated | Section of Local Law included regarding the distances of neighbour feedback. Condition 6 has been removed as it is not supported by the Local Law. |
| DA – LGACS12 'Usage and Management of Community and Sporting Facilities' | Function Delegated updated | The additional text added for clarity as approval for usage is delegated to staff under approved terms and conditions set by Manager Recreation and Community Safety. |
| DA – LGACS13 'Community Funding for Sport Clubs and Individuals' | Conditions / Guidelines updated | Minor amendment to include recently approved Sporting Club COVID-19 Financial Assistance Grants adopted by Council in April 2020. |

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Item 8.1

DAP 27/08/2020

The Instruments of Delegation listed below have been reviewed by staff and no changes are required, as they are considered appropriate for Council to adopt in their present form.

| | |
|---------------|---|
| DA - LGAES3 | Local Government Act 1995 – Calling of Tenders or Expressions of Interest |
| DA - LGAES4 | Local Government Act 1995 – Contract Variation |
| DA – LGAES12 | Acquisition and Disposal of Property (Land) |
| DA – LGAES13 | Legal Representation – Elected Members and Employees |
| DA – LGACS7 | Local Government Act 1995 – Funding Assistance – Community Associations |
| DA – LGACS14 | Community Funding for Community Organisations and Individuals |
| DA – LGAEW2 | Local Government Act 1995 – Temporary Road Closures |
| DA – LGAEW4 | Local Government Act 1995 – Sand Drift' |
| DA – LGAEW5 | Local Government Act 1995 (Uniform Local Provisions) Regulations 1996 – Obstruction of Streets' |
| DA – LGAFCS4 | Local Government Act 1995 – Payment from Municipal and Trust Funds |
| DA – LGAFCS10 | Objections to the Rate Record and Rateable Status of Land' |
| DA – LGAPD4 | Local Government Act 1995 – Preparation of Business Plans for Disposal of Land' |

A briefing of the Delegated Authority Register was presented to Elected Members on 21 July 2020, which outlined the concept of 'acting through' provisions, which is defined below:

'Section 5.45 of the Act states that in relation to delegations, nothing prevents a 'local government from performing any of its functions by acting through a person other than the CEO' or 'a CEO from performing any of his or her functions by acting through another person

Authorised Persons - Part 9 Division 2 (1) and (2) states:

- (1) *The local government may, in writing, appoint persons or classes of persons to be authorised for the purposes of performing particular functions.*
- (2) *The local government is to issue to each person so authorised a certificate stating that the person is so authorised, and the person is to produce the certificate whenever required to do so by a person who has been or is about to be affected by any exercise of authority by the authorised person.*

The City maintains a Schedule of Authorisations which is updated annually and issued to certain officers, appointed by the Chief Executive Officer for the purposes of performing particular functions.

The Instruments of Delegation listed below have been reviewed by staff and are now recommended for deletion.

| Delegation | Proposed Amendment | Comments |
|--|--------------------|--|
| DA - LGAES6 'Local Government Act 1995 - Authority to Call Public Meetings' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAES2 'Local Government Act 1995 - Appointment of Authorised Persons' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAES5 'Local Government Act 1995 - Legal Proceedings' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGACS3 'City of Cockburn (Local Government Act 1995) Local Laws - Notices' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGACS4 'City of Cockburn (Local Government Act 1995) Local Laws - Stallholders' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAEW1 'City of Cockburn (Local Government Act 1995) Local Laws - Parking Controls' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAEW3 'Local Government Act 1995 - Dangerous Trees on Private Land' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAPD1 'City of Cockburn (Local Government Act 1995) Local Laws 2000 (as amended) - Signs, Hoardings and Bill Postings' | To be deleted | Delegation not required; listed under 'Authorised Persons' Part 9, Division 2 of the <i>Local Government Act 1995</i> refers |
| DA - LGAFCS1 'Local Government Act 1995 - Advertising Proposed Differential Rates' | To be deleted | It is an administrative function which does not impact the Council Budget setting process |
| DA - LGAFCS5 'Local Government Act 1995 - Recovery of Rates and Service Charges - Leased Properties' | To be deleted | Delegation no longer required. 'Acting Through' provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |



Item 8.1

DAP 27/08/2020

Strategic Plans/Policy ImplicationsListening and Leading

A community focused, sustainable, accountable and progressive organisation.

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

N/A

Legal Implications

Sec.5.46 (2) of the *Local Government Act 1995* refers.

Community Consultation

N/A

Risk Management Implications

Failure to adopt the recommendation would result in a "Moderate" level of "Compliance" risk with the *Local Government Act 1995* requirement to complete a review of the Register of Delegated Authority at least once every financial year.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



11 of 192



| | | |
|----|--|---------------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT, 1995) – COMMUNITY FUNDING FOR COMMUNITY ORGANISATIONS & INDIVIDUALS MINOR FUNDING (SMALL (GRANTS, DONATIONS & SPONSORSHIPSUBSIDIES) | LGACS2 |
|----|--|---------------|

| | |
|----------------------------------|----------------------------------|
| DELEGATED AUTHORITY CODE: | LGACS2 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Community Development & Services |
| SERVICE UNIT: | Grants & Research |
| RESPONSIBLE OFFICER: | Manager, Community Development |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 11 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|-------------------|
| DAPPS Meeting: | 24 May 2012 | 18 May 2017 |
| | 23 May 2013 | 24 August 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 28 February 2019 |
| | 26 May 2016 | 23 May 2019 |
| | OCM: | 9 June 2011 |
| | 14 June 2012 | 8 June 2017 |
| | 13 June 2013 | 14 September 2017 |
| | 12 June 2014 | 14 June 2018 |
| | 11 June 2015 | |

FUNCTION DELEGATED:

The authority to approve payment of minor grants, sponsorships, donations and subsidies available in the City's Grants and Donation budget

CONDITIONS/GUIDELINES:

- (1) Funding will be considered under the following sub-categories as provided for in Council Policy 'Community Funding for Community Organisations and Individuals (Grants, Donations & Sponsorships), Category G – Major and Minor Funding:
 - 1. Minor Donations
 - 2. Subsidies
 - 3. Minor Grants
 - 4. Youth Reward and Recognition
 - 5. Donations to Schools – Reimbursements
 - 6. Community Welfare Funding
 - 6-7. **COVID-19 Community Funding**

[1]



| | | |
|----|--|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT, 1995) – COMMUNITY FUNDING FOR COMMUNITY ORGANISATIONS & INDIVIDUALS MINOR FUNDING (SMALL GRANTS, DONATIONS & SPONSORSHIP SUBSIDIES) | LGACS2 |
|----|--|--------|

(2) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in the Council Policy 'Community Funding for Community Organisations and Individuals (Grants, Donations & Sponsorships)' and associated Guidelines. The authority to make minor donations of up to \$200 to individuals and groups, but only after consultation with the Mayor.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995, s3.5, s5.42 and s5.44
 Council Policy – Community Funding for Community Organisations and Individuals (Grants, Donations and Sponsorship)

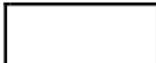
DELEGATE:

Chief Executive Officer
 Note: The Chief Executive Officer will sub-delegate this authority to:-

DELEGATE/S AUTHORISED:

Manager, Community Development
~~Manager, Corporate Communications – (Item 1.4 only)~~
~~Art and Culture Coordinator – (Item 1.4 only)~~
~~Director, Governance & Community Service (Item 1.6 only)~~

[2]



Item 8.1 Attachment 2

DAP 27/08/2020

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – USE OF RESERVES | LGACS5 |
|----|---|--------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGACS5 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Recreation Services |
| RESPONSIBLE OFFICER: | Manager, Recreation & Community Safety services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | | |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |
| | | |

FUNCTION DELEGATED:

The authority to approve applications for Use of Reserves by individuals / organisations

CONDITIONS/GUIDELINES:

- (1) ~~Events must be for not-for-profit activities~~ Activities must be approved within approved guidelines or conditions.
- (2) ~~Minimum bond of \$100.00 to be paid prior to approval being confirmed~~ Charges are applied as per the City's annually approved Fees and Charges Schedule.
- (3) Any reduction of fees may be applied as a customer service measure.
- (4) Any payment extensions for unpaid usage fees that enable continued access.
- (5) Permission to consume alcohol must be approved.
- (6) ~~All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

All Officers in accordance with conditions (1) and (2) above.

[1]



DAP 27/08/2020

Item 8.1 Attachment 2

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – USE OF RESERVES | LGACS5 |
|----|---|--------|

Only Manager Recreation and Community Safety and Coordinator Recreation Services in accordance with conditions (3), (4) and (5) above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

City of Cockburn (Local Government Act) Local Laws (Part IV)
Local Government Act, 1995, s3.5, s5.42 and s5.44

DELEGATE:

Chief Executive Officer
Note: The Chief Executive Officer will sub delegate this authority to:

SUB-DELEGATE/S:

Manager, Recreation & Community Safety
Co-ordinator, Recreation Services
Senior Recreation Facilities and Reserves Officer
Community Facilities Project Officer
Bookings Officer

[2]

| | | |
|----|--|---------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – APPLICATIONS TO KEEP MORE THAN TWO(2) DOGS AT A RESIDENTIAL PROPERTY | LGACS11 |
|----|--|---------|

| | |
|---------------------------|---|
| DELEGATED AUTHORITY CODE: | LGACS11 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Community Services |
| SERVICE UNIT: | Ranger & Community Safety Services |
| RESPONSIBLE OFFICER: | Manager, Ranger & Community Safety Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|-------------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| OCM: | 13 September 2007 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

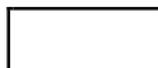
FUNCTION DELEGATED:

The authority to approve / reject applications to keep more than two (2) dogs at a residential property.

CONDITIONS/GUIDELINES:

- (1) The premises complying in all respects with the provisions of the Act and this Local Law.
- (2) The occupier-Applicant of the premises notifying the Council-City of this intent in the prescribed form and pays the prescribed fee.
- (3) The Applicant provides approval for the City's Officers to request community feedback;
 - A. In residential areas – within 50 metres of the applicants premises;
 - and
 - B. In rural areas – within 100 metres of the applicnats premises.~~Evidence that neighbours have been informed of the proposed use in accordance with the Local Law.~~

[1]



| | | |
|----|--|---------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – APPLICATIONS TO KEEP MORE THAN TWO(2) DOGS AT A RESIDENTIAL PROPERTY | LGACS11 |
|----|--|---------|

- (4) The ~~City~~ council has not received any objections to the notification within a period of twenty-one days of the notification having been given.
- (5) In the event that any objections are received, then an applicant may not keep more than two dogs without the specific approval of Council.
- (6) ~~Any approval issued will only entitle an occupier to keep the specified number of dogs up until the end of October next following the date of the notification unless Council determines otherwise.~~
- (7) Any approval issued is not transferable or assignable either to any other occupier of the premises or to any other premises within the District.
- (8) Any approval issued is subject to the relevant dog or dogs being registered.
- (9) The number of dogs is limited to six over the age of three months and the young of those dogs under that age.
- (10) All transactions utilising this delegation are to be recorded ~~in the relevant register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As per Conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Dog Act Section 26
City of Cockburn (Local Government Act) Local Laws - Sec. 2.9
Local Government Act, 1995, s3.5, s5.42 and s5.44

DELEGATE:

Chief Executive Officer

SUB-DELEGATE/S:

Ranger and Community Safety Services Manager

[2]

| | | |
|----|--|----------------|
| DA | USAGE & MANAGEMENT OF COMMUNITY & SPORTING FACILITIES | LGACS12 |
|----|--|----------------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGACS12 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Recreation Services |
| RESPONSIBLE OFFICER: | Manager Recreation and Community Safety Recreation Services Co-ordinator |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| POLICY REF.: | Usage & Management of Community & Sporting Facilities |
| VERSION NO. | 7 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|---|---|
| DAPPS Meeting: | 24 May 2012 28 August 2014 26 May 2016 | 25 August 2016 28 February 2019 23 May 2019 |
| OCM: | 9 April 2009 14 June 2012 11 September 2014 | 9 June 2016 8 September 2016 14 June 2018 |

FUNCTION DELEGATED:

The authority to apply specific conditions for the casual hire use of Council controlled Community and Sporting Facilities, in accordance with the Terms and Conditions of hire, which may be amended from time to time.

CONDITIONS/GUIDELINES:

- (1) The following conditions can be approved under this delegation in conjunction with conditions listed in Council Policy "Usage and Management of Community and Sporting Facilities"
 1. Community Facilities
 2. Community Sporting Facilities
 3. Passive Reserves
- (2) Any reduction of fees may be applied a customer service measure.
- (3) Any payment extensions for unpaid usage fees that enable continued access.
- (4) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

[1]



DAP 27/08/2020

Item 8.1 Attachment 4

| | | |
|----|---|---------|
| DA | USAGE & MANAGEMENT OF COMMUNITY & SPORTING FACILITIES | LGACS12 |
|----|---|---------|

AUTONOMY OF DISCRETION:

As provided in Council Policy
Usage and Management of Community and Sporting Facilities

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995 sec 5.42 and 5.44
Council Policy 'Usage and Management of Community and Sporting Facilities' refers.

DELEGATE:

Chief Executive Officer
Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Manager Recreation and Community Safety
Co-ordinator Recreation Services

[2]

| | | |
|----|---|----------------|
| DA | COMMUNITY FUNDING FOR SPORTING CLUBS AND INDIVIDUALS | LGACS13 |
|----|---|----------------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | LGACS13 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Recreation Services |
| RESPONSIBLE OFFICER: | Co-ordinator Recreation Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 14 March 2019 |
| DATE LAST REVIEWED: | |
| POLICY REF.: | Community Funding for Sporting Clubs & Individuals |
| VERSION NO. | 1 |

| | |
|---------------------------------------|------------------|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 28 February 2019 |
| OCM: | |

FUNCTION DELEGATED:

The authority to evaluate funding submissions in accordance with grant programs listed under policy Community Funding for Sporting Clubs and Individuals and to manage and allocate funds to submissions compliant with this policy and respective guidelines.

CONDITIONS/GUIDELINES:

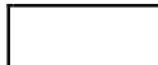
- (1) To approve applications for the following grant programs:
 - 1. Major Capital Works Grant
 - 2. Minor Capital Works Grant
 - 3. Healthy Canteens Incentive
 - 4. Sports Equipment Grant
 - 5. Junior Sports Travel Assistance
 - 6. [Sporting Club COVID-19 Financial Assistance Grants](#)
- (2) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided as in the conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

[1]



DAP 27/08/2020

Item 8.1 Attachment 5

| | | |
|----|---|---------|
| DA | COMMUNITY FUNDING FOR SPORTING CLUBS AND INDIVIDUALS | LGACS13 |
|----|---|---------|

Local Government Act, 1995 sec 5.42 and 5.44
Council Policy "Community Funding for Sporting Clubs and Individual" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Governance & Community Services
Manager, Recreation & Community Safety
Co-ordinator Recreation Services

[2]

| | | |
|----|--|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – AUTHORITY TO CALL PUBLIC MEETINGS | LGAES6 |
|----|--|--------|

| | |
|----------------------------------|------------------------------|
| DELEGATED AUTHORITY CODE: | LGAES6 |
| DIRECTORATE: | Executive Support Department |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

To be deleted

FUNCTION DELEGATED:

The Authority to arrange and conduct Public Meetings on behalf of Council.

CONDITIONS/GUIDELINES:

- (1) The issue, which is the subject of the Meeting, must be of widespread interest or concern within the Cockburn community.
- (2) Details of the Meeting first being referred to and supported by the Mayor.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Legislative Requirements and Conditions (1) to (3) above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995 (Sec. 3)

DELEGATE:

Chief Executive Officer

[1]



DAP 27/08/2020

Item 8.1 Attachment 6

| | | |
|----|---|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – AUTHORITY TO CALL PUBLIC MEETINGS | LGAES6 |
|----|---|--------|

SUB-DELEGATE/S:

Nil.

To be deleted

[2]

| | | |
|----|---|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – APPOINTMENT OF AUTHORISED PERSONS | LGAES2 |
|----|---|--------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGAES2 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Governance |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

FUNCTION DELEGATED:

The authority to appoint authorised persons pursuant to Section 9.10(1) of the Local Government Act, 1995.

CONDITIONS/GUIDELINES:

- (1) The area(s) of responsibility to which the authority applies is/are to be stipulated in the authorisation.
- (2) The required Certificate stating the authority granted for production as required, to be issued to each authorised person.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer

AUTONOMY OF DISCRETION:

As per conditions/guidelines and as contained within the relevant legislation.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995 (Sec.9.10), City of Cockburn (Local Government Act) Local Laws, 2000.

[1]



DAP 27/08/2020

Item 8.1 Attachment 7

| | | |
|----|--|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – APPOINTMENT OF AUTHORISED PERSONS | LGAES2 |
|----|--|--------|

DELEGATE:

CEO.

SUB-DELEGATE/S

Director, Governance & Community Services

To be deleted

[2]

| | | |
|----|--|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – LEGAL PROCEEDINGS | LGAES5 |
|----|--|--------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGAES5 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| | | |
|---------------------------------------|--------------|--------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | 9 June 2011 | 11 June 2015 |
| OCM: | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |
| | | |

To be deleted

FUNCTION DELEGATED:

The Authority to initiate legal proceedings and the signing of prosecution complaint forms in relation to breaches appurtenant to the Local Government Act, 1995, (Part 9 Division 2).

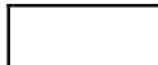
CONDITIONS/GUIDELINES:

- (1) Copy of duly completed Summons of Complaint form to be retained.
- (2) Any delegate has the authority to deal with such matters relevant to this declaration.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

Delegate to be satisfied that:-

- (a) All other avenues to attain compliance that have been exhausted or;
- (b) The alleged offender has been convicted of the same or a similar offence in the past or;
- (c) The alleged offender has been formally warned on another occasion or;

[1]



| | | |
|----|--|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – LEGAL PROCEEDINGS | LGAES5 |
|----|--|--------|

(d) the nature of the offence is such so as to warrant immediate prosecution action

AUTONOMY OF DISCRETION:

As indicated in conditions (1) to (3) and (a) to (d) above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995.

DELEGATE:

Chief Executive Officer

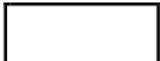
Note: The Chief Executive Officer will further sub-delegate this authority to:-

SUB-DELEGATE/S:

- Director, Governance & Community Services
- Director, Engineering & Works
- Director, Finance & Corporate Services
- Director, Planning & Development
- Manager, Building Services
- Manager, Environmental Health
- Manager, Statutory Planning
- Manager, Financial Services

To be deleted

[2]



| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – NOTICES | LGACS3 |
|----|---|--------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGACS3 |
| DIRECTORATE: | Executive Support Department |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

FUNCTION DELEGATED:

The authority to issue Notices as prescribed by the City of Cockburn (Local Government Act) Local Laws. (Consolidated).

CONDITIONS/GUIDELINES:

- (1) Notices are to be drafted in the appropriate format by the responsible Departmental Officer.
- (2) Due cause must be given to issues which could have the effect of causing harm or injury to persons or property.
- (3) Where no evidence of potential harm or injury is indicated by the initial investigation of issues relevant to this delegation, all reasonable attempts to resolve any issues under this authority must be unsuccessfully pursued prior to the issuing of the prescribed Notice.
- (4) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Local Laws and conditions (1) to (4) above.

[1]



DAP 27/08/2020

Item 8.1 Attachment 9

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – NOTICES | LGACS3 |
|----|---|--------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Laws (consolidated).

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

SUB-DELEGATE/S:

Director, Governance and Community Services
Governance & Risk Management Co-ordinator

To be deleted

[2]

| | | |
|----|--|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – STALLHOLDERS | LGACS4 |
|----|--|--------|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | LGACS4 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Community Services |
| SERVICE UNIT: | Ranger & Community Safety Services |
| RESPONSIBLE OFFICER: | Manager, Ranger & Community Safety Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 10 |

| | | |
|---------------------------------------|--------------|--------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 2 June 2015 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | | 23 May 2019 |
| OCM: | 9 June 2011 | 12 June 2014 |
| | 14 June 2012 | 11 June 2015 |
| | 13 June 2013 | 8 June 2017 |
| | | 14 June 2018 |
| | | |

To be deleted

FUNCTION DELEGATED:

The express authority to issue, renew and transfer Stallholders Licences pursuant to the provisions of the City of Cockburn (Local Government Act) Local Laws. (Part IX).

CONDITIONS/GUIDELINES:

- (1) Consideration must be given to the location of proposed stalls and whether adequate coverage of the goods for sale is available, in any case, within the vicinity.
- (2) Council consideration will be given to any objections received on any proposed or current stallholders locations.
- (3) All transactions utilising this delegation are to be recorded in the Stallholders Licence File by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Local Law and conditions (1) to (3) above

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

[1]



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Item 8.1 Attachment 10

| | | |
|----|--|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – STALLHOLDERS | LGACS4 |
|----|--|--------|

City of Cockburn Consolidated Local Law (Stall Holders)
Local Government Act, 1995, s3.5, s5.42 and s5.44

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub delegate this authority to:-

SUB-DELEGATE/S:

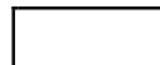
Manager, Ranger and Community Safety Services

CONDITIONS

- * Either delegate has the authority to deal with such matters relevant to this declaration .
- * All decisions taken under this authority which are to refuse to issue, renew or transfer a licence are to be advised to the applicant with an explanation of the objection and appeal rights of the applicant, pursuant to section 9.5 and section 9.7 of the Local Government Act, 1995, and Functions and General Regulations 33(1) and 34(1).

To be deleted

[2]



| | | |
|----|--|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – PARKING CONTROLS | LGAEW1 |
|----|--|--------|

| | |
|----------------------------------|----------------------|
| DELEGATED AUTHORITY CODE: | LGAEW1 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering Services |
| SERVICE UNIT: | Engineering Services |
| RESPONSIBLE OFFICER: | Manager, Engineering |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 10 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|-------------------|
| DAPPS Meeting: | 24 May 2012 | 27 August 2015 |
| | 23 May 2013 | 26 May 2016 |
| | 22 May 2014 | 18 May 2017 |
| | 2 June 2015 | 24 May 2018 |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 10 September 2015 |
| | 13 June 2013 | 9 June 2016 |
| | 12 June 2014 | 8 June 2017 |
| | | 14 June 2018 |

To be deleted

FUNCTION DELEGATED:

The authority to approve the installation or modification of parking controls for the management of vehicle parking on public roads, reserves or any City owned or managed facility pursuant to the Local Laws (Parking and Parking Facilities).

CONDITIONS/GUIDELINES:

- (1) A suitable level of information to support the proposal for the installation/modification of parking controls shall be provided to the Authorising Officer prior to any approval being issued.
- (2) Where the proposal is considered to have a low impact no community consultation is necessary prior to approval of the proposal being granted. However, informing the community about the change(s) to parking controls may be desirable at the time that the changes are implemented.

Low impact proposals are considered to be proposals that either will have no negative impact on adjacent properties or may affect only 1-2 properties.
- (3) Community consultation shall be undertaken with any occupier/landholder whose property has a direct frontage with vehicle access to the section of road(s) associated with the parking controls, where the proposal is considered to have a high impact.

[1]



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| | | |
|----|--|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – PARKING CONTROLS | LGAEW1 |
|----|--|--------|

Examples of high impact proposals would include:

- The introduction of parking controls, where none existed previously, along a complete road section between two side streets;
- Variation of the time/days of existing parking controls along a road section; and
- Any parking controls that will affect the parking practices of a reasonable number of motorists.

- (4) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Legislative requirements and conditions (1) to (3) above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995, s3.5, s5.42 and s5.44
City of Cockburn Parking and Parking Facilities Local Law, 2007, s8

DELEGATE:

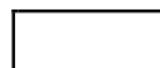
Chief Executive officer (CEO)
Note: The CEO will sub-delegate this authority to:

SUB-DELEGATE/S:

Director, Engineering & Works
Manager, Engineering
Transport Engineer

To be deleted

[2]



| | | |
|----|---|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – DANGEROUS TREES ON PRIVATE LAND | LGAEW3 |
|----|---|--------|

| | |
|----------------------------------|---------------------|
| DELEGATED AUTHORITY CODE: | LGAEW3 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Parks Services |
| RESPONSIBLE OFFICER: | Manager, Parks |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | 9 June 2011 | 11 June 2015 |
| OCM: | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |
| | | |

FUNCTION DELEGATED:

The authority to issue a Notice on an owner or occupier of a property and initiate necessary proceedings to ensure a tree that endangers any person or thing or adjoining land is made safe, pursuant to Sec. 3.27(2) (Schedule 3.2(7)) of the Local Government Act, 1995.

CONDITIONS/GUIDELINES:

- 1) Assessment by person suitably qualified to provide guidance on such matters should be obtained, prior to issue of a Notice under these provisions if appropriate.
- 2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer

AUTONOMY OF DISCRETION:

As provided under Legislative requirements and conditions above

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

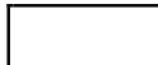
Local Government Act, 1995, s3.27(2) (Schedule 3.2(7)), s5.42 and s5.44.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

[1]



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Item 8.1 Attachment 12

| | | |
|----|---|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – DANGEROUS TREES ON PRIVATE LAND | LGAEW3 |
|----|---|--------|

SUB-DELEGATE/S:

Manager, Parks & Environment
Parks Manager

To be deleted

[2]

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS 2000 (AS AMENDED) – PART VIII SIGNS, HOARDINGS, BILL POSTING | LGAPD1 |
|----|---|--------|

| | |
|----------------------------------|------------------------|
| DELEGATED AUTHORITY CODE: | LGAPD1 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Development Services |
| SERVICE UNIT: | Building Services |
| RESPONSIBLE OFFICER: | Manager, Building |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| | | |
|---------------------------------------|--------------|--------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 14 June 2018 |
| | 12 June 2014 | |

To be deleted

FUNCTION DELEGATED:

The authority to undertake the functions of the Council in respect to the City of Cockburn (Local Government Act) Local Law 2000 relevant to Part VIII of the aforementioned Local Laws.

CONDITIONS/GUIDELINES:

- (1) The delegate shall become acquainted with the subsidiary legislation and Council's Town Planning Scheme No. 3.
- (2) All decisions taken under this authority which are to refuse the issue of Sign Licences are to be advised to the applicant, pursuant to s9.5 and s9.7 of the Local Government Act, 1995, and Local Government (Functions and General) Regulations r33(1) and r34(1).
- (3) All transactions utilising this delegation are to be recorded in the CI Sign Licence System by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under legislation.

[1]



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Item 8.1 Attachment 13

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS 2000 (AS AMENDED) – PART VIII SIGNS, HOARDINGS, BILL POSTING | LGAPD1 |
|----|---|--------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act 1995, s3.5, s5.42 and s5.44
City of Cockburn (Local Government Act) Local Laws 2000 (Part VIII)

DELEGATE:

Chief Executive Officer
Note: The Chief Executive Officer will sub-delegate this authority to:-

SUB-DELEGATE/S:

Manager, Building Services
Co-ordinator, Building Services
Senior Building Surveyors

To be deleted

[2]

| | | |
|----|---|---------|
| DA | LOCAL GOVERNMENT ACT, 1995 – ADVERTISING PROPOSED DIFFERENTIAL RATES | LGAFCS1 |
|----|---|---------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | LGAFCS1 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Rates & Revenue Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 10 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|---------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 2 June 2015 |
| | 26 July 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| OCM: | 9 June 2011 | 23 May 2019 |
| | 14 June 2012 | 12 June 2014 |
| | 9 August 2012 | 11 June 2015 |
| | 13 June 2013 | 9 June 2016 |
| | | 8 June 2017 |
| | | 14 June 2018 |

To be deleted

FUNCTION DELEGATED:

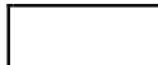
To act as Council in advertising proposed Differential Rates.

CONDITIONS/GUIDELINES:

- (1) Proposed Differential Rates to be advertised immediately in the following media after the presentation of the Budget Concept Forum for Elected Members covering Rates Modelling each year:
 1. Display advert in the West Australian newspaper – Local Government Notices.
 2. Display advert in the Community newspaper – Cockburn Gazette.
 3. Display advert in the Cockburn Herald newspaper.
 4. City's Public Notice Board.
 5. City's Libraries – Spearwood, Coolbellup and Success.
 6. Front page of the City's web site.
 7. City's Social Media outlets.
 8. Copy sent to community and ratepayer groups.
 9. Copy sent to groups and organisations who have registered to receive the City's email newsletters.

- (2) Either delegate has the authority to deal with such matters as relevant to this declaration.

[1]



| | | |
|----|---|---------|
| DA | LOCAL GOVERNMENT ACT, 1995 – ADVERTISING PROPOSED DIFFERENTIAL RATES | LGAFC51 |
|----|---|---------|

- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Legislative requirements and above conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act 1995, Section 6.36

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

SUB-DELEGATE/S:

Director, Finance & Corporate Services
Manager, Financial Services

To be deleted

[2]

| | | |
|----|---|---------|
| DA | LOCAL GOVERNMENT ACT, 1995 – RECOVERY OF RATES AND SERVICE CHARGES – LEASED PROPERTIES | LGAFC55 |
|----|---|---------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | LGAFC55 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Rates & Revenue Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| | | |
|---------------------------------------|--------------|--------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | 9 June 2011 | 11 June 2015 |
| OCM: | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

To be deleted

FUNCTION DELEGATED:

The authority to recoup rates and service charges from lease property by the service of notice requiring the lessee to pay any rent, as it falls due, off the outstanding rates and service charges.

CONDITIONS/GUIDELINES:

- (1) Any delegate has the authority to deal with such matters relevant to this declaration.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer

AUTONOMY OF DISCRETION:

As provided under Legislative requirements and conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act 1995, Sections 6.60 to 6.62

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

[1]



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Item 8.1 Attachment 15

| | | |
|----|--|--------|
| DA | LOCAL GOVERNMENT ACT, 1995 – RECOVERY OF RATES AND SERVICE CHARGES – LEASED PROPERTIES | LGAFC5 |
|----|--|--------|

SUB-DELEGATE/S:

Director, Finance & Corporate Services
Manager, Financial Services

To be deleted

[2]

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Item 8.2

**8.2 (2020/MINUTE NO 0016) PROPOSED AMENDMENTS/
DELETIONS OF ADMINISTRATION AND STRATEGIC DELEGATED
AUTHORITIES, POLICIES AND POSITION STATEMENTS**

Author(s) B Pinto

- Attachments**
1. Proposed Amendment - DA -ACS5 'Completion of Firebreaks'
 2. Proposed Amendment - DA-APD55 'Structure Plans and Activity Centre Plans'
 3. Proposed Amendment - DA-APD60 'Leasing of City of Cockburn Property for Community and/or Recreational Purposes (including Not-for-Profit)'
 4. Proposed Amendment - DA-SES1 'Obtaining Legal or Other Expert Advice'
 5. Proposed Deletion - DA-APD59 'Commercial Leasing of City of Cockburn Owned and Controlled Land'
 6. Proposed Deletion - DA-SFCS1 'Investments'
 7. Proposed Deletion - DA-AEW2 'Kerbside House Numbering'
 8. Proposed Deletion - DA-SEW1 'Maintenance of Verges and Public Open Space (POS) following Residential Subdivision'
 9. Proposed Deletion - DA-SEW2 'Street and Public Area Lighting'
 10. Proposed Deletion - DA-SEW3 'Traffic Management Investigation'
 11. Proposed Deletion - DA-SPD7 'Prevention of Sand Drift from Subdivision and Development Sites'
 12. Proposed Deletion - DA- APD52 'Appointment of Real Estate Agent to Sell Council Owned Property'
 13. Proposed Deletion - DA-APD58 'Large Public Events'
 14. Proposed Deletion - DA-AC2 'Seating Arrangements for Council Meeting'
 15. Proposed Deletion - DA-ACS4 'Rewards for Civic Deeds'
 16. Proposed Deletion - DA-ACS6 'Volunteers Fire Fighters Training'
 17. Proposed Deletion - DA-ACS13 'Emergency Relief Fund'
 18. Proposed Deletion - DA-ACS14 'City of Cockburn Annual Arts Competition'
 19. Proposed Deletion - DA-AES1 'Annual General Meeting of Electors'
 20. Proposed Deletion - DA-AES3 'Industrial Relations'
 21. Proposed Deletion - DA-AES4 'Annual Arts Competition'
 22. Proposed Deletion - DA-AES7 'Approval for Research/Study Visits'

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23. Proposed Deletion - DA-AES8 'Council Owned Vehicle Usage'
24. Proposed Deletion - DA-AES9 'Approval to Participate in Representative Delegations'
25. Proposed Deletion - DA-AES10 'Underground Power'
26. Proposed Deletion - DA-AES11 'Industrial Relations - Employee Redundancy Payment'
27. Proposed Deletion - DA-AEW1 'Street Verge Improvements'
28. Proposed Deletion - DA-AEW3 'Street Lighting'
29. Proposed Deletion - DA-AEW4 'Installation of Playground/Recreation Equipment on Reserves'
30. Proposed Deletion - DA-AEW5 'Landowner Biodiversity Conservation Grant Program'
31. Proposed Deletion - DA-AEW6 'Promotional Street Banners'
32. Proposed Deletion - DA-AEW8 'Submission and Comment on Environmental Approvals and Matters'
33. Proposed Deletion - DA-AEW9 'Internally Illuminated Directional Signs'
34. Proposed Deletion - DA-AEW10 'Installation of Private Memorial Plaques in Public Open Space'
35. Proposed Deletion - DA-AFCS1 'Employee Development'
36. Proposed Deletion - DA-AFCS2 'Leasing of Council Controlled Land'
37. Proposed Deletion - DA-AFCS3 'Disposal of Assets'
38. Proposed Deletion - DA-AFCS4 'Defence Force Reserves - Staff Participation'
39. Proposed Deletion - DA-AFCS6 'Renewal of Leases and Licenses for Council Owned or Controlled Property'
40. Proposed Deletion - DA-AFCS8 'Change of Basis for Valuation of Land for Rating Purposes'
41. Proposed Deletion - DA-SCS1 'Media Activity'
42. Proposed Deletion - DA-SC8 'Conduct of Elections by Postal Ballot'
43. Proposed Deletion - DA-SES2 'Access To Tape Recordings of Council Meetings'
44. Proposed Deletion - Policy-APD74 'Large Public Events - Approval'
45. Proposed Deletion - POS-PSPD11 'Public Buildings'

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Item 8.2

RECOMMENDATION
 That Council:

- (1) adopt proposed amendments to Administrative and Strategic Delegated Authorities, as shown in the attachments to the Agenda;
- (2) adopt Administrative and Strategic Delegated Authorities that have no changes, as listed in the report;
- (3) delete the Administrative and Strategic Delegated Authorities and Policy APD74 'Large Public Events – Approval' and Position Statement PSPD11 'Public Buildings', as listed in the report; and
- (4) update the Delegated Authority Register accordingly

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION
 MOVED Cr C Terblanche SECONDED Cr M Separovich
 That the recommendation be adopted.

CARRIED 4/1

Background

Following on from the Policy Review Project, there were a number of Administrative and Strategic Delegated Authorities that were placed on hold until the entire Project was completed, which was finalised at the November 2019 Delegated Authority and Policies (DAP) Committee meeting and endorsed at the December 2019 Council Meeting.

A review of these Delegated Authorities has now been undertaken and was presented to Committee for consideration and adoption in May 2020. By resolution at the June 2020 Ordinary Council Meeting, the matter was deferred for further consideration.

Submission

N/A

Report

In consultation with the relevant officers, a comprehensive review of these delegations was undertaken. There are some recommended changes made to the delegations, in line with their objectives.

Listed in the table below are the proposed amended delegations, delegations no longer required by using the 'acting through' provisions of Section 5.45 (2) of the *Local Government Act 1995*, delegations to be retained in their present form and deleted delegations for consideration of Council. Also listed for deletion is Council Policy APD74 and Position Statement PSPD11 for consideration of Council.



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Where documents have been presented for amendment or deletion, they are shown in the Attachments. Where there is no change proposed to the current delegation, these documents are not attached.

| Delegation | Proposed Amendment | Reason for Amendment |
|---|---|--|
| DA - ACS5 'Completion of Firebreaks' | Title updated and amended reference to Policy in delegation | Amendment reflects the correct title of delegation in accordance with the Policy |
| DA – APD55 'Structure Plans and Activity Centre Plans' | Legislative Requirements updated | Reference to Legislation |
| DA – APD60 'Leasing of City of Cockburn Property for Community and/or Recreational Purposes (including Not-for-Profit)' | Legislative Requirements updated | Reference to Legislation |
| DA – SES1 'Obtaining Legal or Other Expert Advice' | Title of delegation updated Conditions/Guidelines updated. | Delegation updated to accurately reflect the Policy |

The Instruments of Delegation listed below have been reviewed by staff and no changes are required, as they are considered appropriate for Council to adopt in their present form below have been reviewed by staff and no changes are required, as they are considered appropriate for Council to adopt in their present form:

| | |
|-------------|--|
| DA – APD56 | Building Permits/Strata Plan |
| DA – APD57 | Land Administration Act 1997 – Naming of Streets and Public Open Space |
| DA – AES5 | Payments to Employees in Addition to Contract or Award |
| DA – AES6 | Attendance at Conferences and Seminars |
| DA – LPP5.4 | Location of High Voltage Overhead Power Lines and Microwave Towers |

A briefing of the Delegated Authority Register was presented to Elected Members on 21 July 2020, which outlined the concept of 'acting through' provisions, which is defined below:

'Section 5.45 of the Act states that in relation to delegations, nothing prevents a 'local government from performing any of its functions by acting through a person other than the CEO' or 'a CEO from performing any of his or her functions by acting through another person

Authorised Persons - Part 9 Division 2 (1) and (2) states:

- (1) The local government may, in writing, appoint persons or classes of persons to be authorised for the purposes of performing particular functions.*
- (2) The local government is to issue to each person so authorised a certificate stating that the person is so authorised, and the person is to produce the certificate whenever required to do so by a person who has been or is about to be affected by any exercise of authority by the authorised person.*



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The City maintains a Schedule of Authorisations which is updated annually and issued to certain officers, appointed by the Chief Executive Officer for the purposes of performing particular functions.

The Instruments of Delegation listed below have been reviewed by staff and are now recommended for deletion.

| Delegation | Proposed Amendment | Reason for Amendment |
|---|--------------------|---|
| DA – APD59 ‘Leasing of Council Controlled Land’ | To be deleted | Delegation not required for functions (obtain and apply valuations) to be undertaken. Amended Policy ‘Commercial Leasing of City of Cockburn Owned or Controlled Land’ refers ‘Acting Through’ provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers |
| DA – SFCS1 ‘Investments’ | To be deleted | Delegation not required; Policy ‘Investment of Funds’ applies. Regular Reports provided to Council Meeting. ‘Acting Through’ provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – AEW2 ‘Kerbside House Numbering’ | To be deleted | Delegation not required to consider applications. ‘Acting Through’ provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – SEW1 ‘Maintenance of Verges and Public Open Space (POS) following Residential Subdivision’ | To be deleted | Delegation not required. Requirement for land developers to maintain Public Open Space is provided as a condition of subdivision. Policy ‘Maintenance of Public Open Space and Road Reservations Following Residential Subdivision’ refers. ‘Acting Through’ provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – SEW2 ‘Street and Public Area Lighting’ | To be deleted | Delegation not required for approving street and public area lighting, which is a required as a condition of development approval. Policy ‘Street and Public Open Space Lighting’ refers. ‘Acting Through’ provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – SEW3 ‘Traffic Management Investigation’ | To be deleted | Delegation not required as guidance for traffic management measures is contained in Policy ‘Local Area Traffic Management Investigation’. ‘Acting Through’ provisions under |



Item 8.2

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| Delegation | Proposed Amendment | Reason for Amendment |
|--|--------------------|---|
| | | s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – SPD7 'Prevention of Sand Drift from Subdivision and Development Sites' | To be deleted | Delegation not required to approve Dust Management Plans. Policy "Dust Management From Development Sites" refers. 'Acting Through' provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – APD52 'Appointment of Real Estate Agent to Sell Council Owned Property' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Delegation no longer required. |
| DA – APD58 'Large Public Events – Approval' | To be deleted | Well established procedures have been implemented within the organisation when assessing and approving public events to be held. Delegation no longer required. |
| DA – AC2 'Seating Arrangements for Council Meeting' | To be deleted | Policy deleted in accordance with the Policy Review Project. It is now captured in Policy 'Council Meetings' Delegation not required. |
| DA – ACS4 'Rewards for Civic Deeds' | To be deleted | Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation not required; |
| DA – ACS6 'Volunteer Fire Fighters Training' | To be deleted | Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation not required; |
| DA – ACS13 'Emergency Relief Fund' | To be deleted | Policy has been identified to be converted to a Procedure in accordance with Policy Project Review. Delegation no longer required. |
| DA – ACS14 'City of Cockburn Art Collection' | To be deleted | Delegation not required for updating the Record of the City's Art Collection. Policy "City of Cockburn Artwork Collection" refers. 'Acting Through' provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – AES1 'Annual General Meeting of Electors' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. To be converted to a new delegation (LGAES) pursuant to Section 5.27 of the <i>Local Government Act 1995</i> . |



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| Delegation | Proposed Amendment | Reason for Amendment |
|---|--------------------|---|
| DA – AES3 'Industrial Relations' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Delegation not required. |
| DA – AES4 'Annual Arts Competition' | To be deleted | Policy "City of Cockburn Artwork Collection refers. Delegation not required. |
| DA – AES7 'Approval for Research/Study Visits' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Delegation not required. |
| DA – AES8 'Council Owned Vehicle Usage' | To be deleted | Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation not required. |
| DA – AES9 'Approval to Participate in Representative Delegations' | To be deleted | Delegation not required for approving delegations on behalf of the City of Cockburn. Policy "Approval to Participate in Representative Delegations refers 'Acting Through' provisions under s5.45(2) of the <i>Local Government Act 1995</i> refers. |
| DA – AES10 'Underground Power' | To be deleted | Delegation not required to lodge Expressions of Interest and Applications for the installation of Underground Power Projects within the City. Policy has been deleted in accordance with the Policy Project Review. |
| DA – AES11 'Industrial Relations – Employee Redundancy Payment' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Staff matter administered through Reserve Fund approved by Council. Delegation no longer required. |
| DA – AEW1 'Street Verge Improvements' | To be deleted | The "Street Verge Improvement Policy" provides a framework for the improvement of an adjacent verge by a property owner which is governed by the City of Cockburn Local Laws 2000. The Local Laws provide the power for "Authorised Officers" to enforce any non-compliance thereby rendering the delegated authority for street verge improvements obsolete. |
| DA – AEW3 'Street Lighting' | To be deleted | Delegation no longer required as the Policy has been consolidated with Policy 'Street and Public Open Space Lighting' provides guidance for installing street lighting in the City |



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| Delegation | Proposed Amendment | Reason for Amendment |
|--|--------------------|---|
| DA – AEW4 'Installation of Playground/Recreation Equipment on Reserves' | To be deleted | of Cockburn. "Installation of Playgrounds/Recreation on Reserve" can be deleted as the revised policy "Installation of Recreational Equipment on Public Open Space" aligns with the Public Open Space Strategy adopted by Council 2014-2024 (5 Year Review) and is only enacted following the adoption of Council's annual capital works budget. |
| DA – AEW5 'Landowner Biodiversity Conservation Grant Program' | To be deleted | The Landowners Biodiversity Conservation Grant Program is presented to the Grants and Donations Committee for review and adoption. This decision by Council eliminates the requirement for a delegated authority. |
| DA – AEW6 'Promotional Street Banners' | To be deleted | Incorporated into a Local Planning Policy 'Signs and Advertising'. Delegation not required. |
| DA – AEW8 'Submission and Comment on Environmental Approvals and Matters' | To be deleted | Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation not required. |
| DA – AEW9 'Internally Illuminated Directional Signs' | To be deleted | Policy has been identified to be converted to a Procedure in accordance with Policy Project Review. Delegation no longer required. |
| DA – AEW10 'Installation of Private Memorial Plaques in Public Open Space' | To be deleted | Delegated Authority can be deleted as the revised policy "Installation of Private Memorial Plaques in Public Open Space" aligns with the Public Open Space 2014-2024 Strategy (5 Year Review) and provides the process for considering submissions. |
| DA – AFCS1 'Employee Development' | To be deleted | Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation no longer required. |
| DA – AFCS2 'Leasing of Council Controlled Land' | To be deleted | Policy now deleted and incorporated into new Policy "Commercial Leasing of City of Cockburn Owned or Controlled Land" Delegation no longer required. |
| DA – AFCS3 'Disposal of Assets' | To be deleted | Policy has been identified to be converted to a Procedure in accordance with Policy Project Review. Delegation no longer |

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Item 8.2

| Delegation | Proposed Amendment | Reason for Amendment |
|--|--------------------|---|
| DA – AFCS4 'Defence Force Reserves – Staff Participation' | To be deleted | required. Policy has been identified to be converted to an Administration Policy in accordance with Policy Project Review. Delegation no longer required. |
| DA – AFCS6 'Renewal of Leases and Licenses for Council Owned or Controlled Property' | To be deleted | Accompanying Policy has now been deleted and incorporated into new Policy "Commercial Leasing of City of Cockburn Owned or Controlled Land" Delegation not required. |
| DA – AFCS8 'Change of Basis for Valuation of Land for Rating Purposes' | To be deleted | Policy has been identified to be converted to a Procedure in accordance with Policy Project Review. Delegation not required. |
| DA – SCS1 'Media Activity' | To be deleted | Policy has now been deleted and content incorporated into "Codes of Conduct" and the new Policy "Social Media". Delegation no longer required. |
| DA – SC8 'Conduct of Elections by Postal Ballot' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Matter considered by Council on a biennial basis (each election year). Delegation no longer required. |
| DA – SES2 'Access to Tape Recordings of Council Meetings' | To be deleted | Policy has been deleted in accordance with the Policy Project Review. Provision for recording Council Meetings is now contained in new Policy "Live Streaming at Council Meetings". Delegation no longer required. |
| Policy - APD74 'Large Public Events – Approval' | To be deleted | Accompanying Policy recommended for deletion. Well established procedures have been implemented within the organisation when assessing and approving public events. Delegation no longer necessary. |
| Position Statement – PSPD11 'Public Buildings' | To be deleted | The Position Statement is no longer required as well established procedures have been implemented within the organisation when assessing and approving public buildings. |

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Item 8.2

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Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

There is a "Low" level of "Compliance" risk associated with this item.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) Local Government Act 1995

Nil.



| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – USE OF RESERVES | LGACS5 |
|----|---|--------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | LGACS5 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Recreation Services |
| RESPONSIBLE OFFICER: | Manager, <u>Recreation & Community Safety</u> services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | | |
| OCM: | 9 June 2011 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |
| | | |

FUNCTION DELEGATED:

The authority to approve applications for Use of Reserves by individuals / organisations

CONDITIONS/GUIDELINES:

- (1) ~~Events must be for not-for-profit activities~~Activities must be approved within approved guidelines or conditions.
- (2) ~~Minimum bond of \$100.00 to be paid prior to approval being confirmed~~Charges are applied as per the City's annually approved Fees and Charges Schedule.
- (3) ~~Any reduction of fees may be applied as a customer service measure.~~
- (4) ~~Any payment extensions for unpaid usage fees that enable continued access.~~
- (5) ~~Permission to consume alcohol must be approved.~~
- (6) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

All Officers in accordance with conditions (1) and (2) above.

[1]



DAP 27/08/2020

Item 8.2 Attachment 1

| | | |
|----|---|--------|
| DA | CITY OF COCKBURN (LOCAL GOVERNMENT ACT) LOCAL LAWS – USE OF RESERVES | LGACS5 |
|----|---|--------|

[Only Manager Recreation and Community Safety and Coordinator Recreation Services in accordance with conditions \(3\), \(4\) and \(5\) above.](#)

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

City of Cockburn (Local Government Act) Local Laws (Part IV)
Local Government Act, 1995, s3.5, s5.42 and s5.44

DELEGATE:

Chief Executive Officer
Note: The Chief Executive Officer will sub delegate this authority to:

SUB-DELEGATE/S:

Manager, Recreation & Community Safety
Co-ordinator, Recreation Services
[Senior Recreation Facilities and Reserves Officer](#)
[Community Facilities Project Officer](#)
[Bookings Officer](#)

[2]

| | | |
|----|---|--------------|
| DA | STRUCTURE PLANS, REZONING APPLICATIONS AND METROPOLITAN REGION SCHEME AMENDMENTS | APD55 |
|----|---|--------------|

| | |
|----------------------------------|-----------------------------|
| DELEGATED AUTHORITY CODE: | APD55 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Strategic Planning |
| SERVICE UNIT: | Strategic Planning |
| RESPONSIBLE OFFICER: | Manager, Strategic Planning |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 6 |

| | | |
|---------------------------------------|---|-----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 22 August 2013 26 February 2015 | 26 November 2015 26 May 2016 |
| OCM: | 9 April 2009 14 June 2012 12 September 2013 | 12 March 2015 10 December 2015 |

FUNCTION DELEGATED:

- (1) Structure Plans
 - 1. In accordance with Clause 17(1) of the Deemed Provisions, the authority to determine whether:
 - a. A Proposed Structure Plan complies with Clause 16(1) of the Deemed Provisions; or
 - b. Further information from the applicant is required before a Proposed Structure Plan can be accepted for assessment and advertising.
 - 2. In accordance with Clause 17(1)(b) of the Deemed Provisions, the authority to estimate and provide to the applicant the fee for dealing with a Proposed Structure Plan in accordance with the Planning and Development Regulations 2009.
 - 3. In accordance with Clause 18 of the Deemed Provisions, the authority to advertise the Proposed Structure Plan;
 - 4. In accordance with Clause 19(1) of the Deemed Provisions the authority:
 - a. To request further information from a person who prepared a Proposed Structure Plan and;
 - b. To advertise any modifications proposed to a Proposed Structure Plan to address issues raised in submissions.

[1]



DAP 27/08/2020

Item 8.2 Attachment 2

| | | |
|----|---|--------------|
| DA | STRUCTURE PLANS, REZONING APPLICATIONS AND METROPOLITAN REGION SCHEME AMENDMENTS | APD55 |
|----|---|--------------|

5. In accordance with Clause 29(3) of the Deemed Provisions, the authority to decide not to advertise an amendment to a Structure Plan if, in the opinion of the officer, the amendment is of a minor nature.
 6. In accordance with Clause 20(1) of the Deemed Provisions, the authority to prepare a report on an amendment to a Structure Plan, where the amendment is considered to be minor in nature, and to submit this directly to the Commission.
- (2) Activity Centre Plans
1. In accordance with Clause 33(1) of the Deemed Provisions, the authority to determine whether:
 - a. A Proposed Activity Centre Plan complies with Clause 32(1) of the Deemed Provisions; or
 - b. Further information from the applicant is required before a Proposed Activity Centre Plan can be accepted for assessment and advertising.
 2. In accordance with Clause 33(1)(b) of the Deemed Provisions, the authority to estimate and provide to the applicant the fee for dealing with a Proposed Activity Centre Plan in accordance with the Planning and Development Regulations 2009.
 3. In accordance with Clause 34 of the Deemed Provisions, the authority to advertise the Proposed Activity Centre Plan;
 4. In accordance with Clause 35(1) of the Deemed Provisions the authority:
 - a. To request further information from a person who prepared a Proposed Activity Centre Plan and;
 - b. To advertise any modifications proposed to a Proposed Activity Centre Plan to address issues raised in submissions.
 5. In accordance with Clause 45(3), the authority to decide not to advertise an amendment to an Activity Centre Plan if, in the opinion of the officer, the amendment is of a minor nature.
 6. In accordance with Clause 36(1) of the Deemed Provisions, the authority to prepare a report on an amendment to an Activity Centre Plan, where the amendment is considered to be minor in nature, and to submit this directly to the Commission.

CONDITIONS/GUIDELINES:

- (1) Where an amendment to a Structure Plan or Activity Centre Plan may be considered minor in nature
 1. As per Clause 17 of the Structure Plan Framework, a minor amendment to a Structure Plan or Activity Centre Plan is a change or departure that:
 - a. Does not materially alter the purpose and intent of the structure plan;
 - b. Does not change the intended lot / dwelling yield by more than 10 per cent;

[2]

| | | |
|----|---|--------------|
| DA | STRUCTURE PLANS, REZONING APPLICATIONS AND METROPOLITAN REGION SCHEME AMENDMENTS | APD55 |
|----|---|--------------|

- c. Does not adversely impact upon the amenity of adjoining landowners and occupiers;
- d. Does not restrict the use and development of adjoining land;
- e. Does not significantly impact on infrastructure provision;
- f. Does not impact upon the environment;
- g. Is consistent with Council adopted policies; and
- h. Is deemed to be consistent with orderly and proper planning.

(2) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in (1) of Conditions/Guidelines above

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

City of Cockburn Town Planning Scheme No.3
 Planning and Development Act 2005
[Planning & Development \(Local Planning Schemes\) Regulation 2015](#)

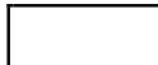
DELEGATE:

Nil.

DELEGATE/S AUTHORISED:

Director, Planning and Development
 Manager, Strategic Planning
 Coordinator, Strategic Planning
 Senior Strategic Planning Officers

[3]



DAP 27/08/2020

Item 8.2 Attachment 3

| | | |
|----|---|-------|
| DA | RENEWAL OF LEASES AND LICENSES FOR COUNCIL OWNED OR CONTROLLED PROPERTY | APD60 |
|----|---|-------|

| | |
|---------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | APD60 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Strategic Planning Services |
| SERVICE UNIT: | Leasing & Land Administration |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2016 24 November 2016 |
| OCM: | 9 April 2009 14 June 2012 9 June 2016 |

FUNCTION DELEGATED:

The authority to renew a lease and licence agreement.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy [APD87: Leasing of City of Cockburn Property for Community and/or Recreational Purposes \(including Not-for-Profit\)](#).
- (2) Local Government Act 1995 section 3.58
Local Government (Functions and General) Regulations 1996 section 30
- (3) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Council Policy [APD87: Leasing of City of Cockburn Property for Community and/or Recreational Purposes \(including Not-for-Profit\)](#).

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

[Planning & Development \(Local Planning Schemes\) Regulations 2015](#)
Council Policy [APD87: Renewal of Leasinges of City of Cockburn Property for Community and/or Recreational Purposes \(including Not-for-Profit\) and Licenses for Council-Owned or Controlled Property](#) refers.

[1]

Item 8.2 Attachment 3

DAP 27/08/2020

| | | |
|----|---|-------|
| DA | RENEWAL OF LEASES AND LICENSES FOR COUNCIL OWNED OR CONTROLLED PROPERTY | APD60 |
|----|---|-------|

DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

Nil.

[2]



DAP 27/08/2020

Item 8.2 Attachment 4

| | | |
|----|--|------|
| DA | OBTAINING LEGAL OR OTHER EXPERT ADVICE & LEGAL PROCEEDINGS BETWEEN CITY OF COCKBURN & OTHER PARTIES | SES1 |
|----|--|------|

| | |
|---------------------------|---|
| DELEGATED AUTHORITY CODE: | SES1 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 3 |

| | |
|---------------------------------------|-----------------------------|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 June 2012 26 May 2016 |
| OCM: | 13 May 2010 14 June 2012 |

FUNCTION DELEGATED:

The authority to obtain legal or other expert advice and respond to legal proceedings on behalf of Council.

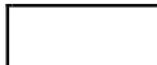
CONDITIONS/GUIDELINES:(1) Obtaining Legal or Other Expert Advice:

1. in the instances where Council has resolved or requested to seek legal or other expert advice, a copy of that advice and Council's letter of instruction be provided to all Elected Members as soon as practicable within seven(7) days of receipt by the City unless otherwise resolved by Council;
2. where copies of legal or other expert advice are made available to Elected Members, the content of the advice is not permitted to be disclosed to third parties, unless by resolution of Council', following the opinion of the Solicitor or specialist who provided advice to the Council about the possible consequences of making that advice available to a third party;
3. Council maintain its retainer arrangement with its Solicitors for the purpose of ascertaining matters of an administrative nature where procedural verbal advice is obtained and that such advice sought be recorded as a file note on the appropriate file;

| | | |
|----|--|------|
| DA | OBTAING LEGAL OR OTHER EXPERT ADVICE & LEGAL PROCEEDINGS BETWEEN CITY OF COCKBURN & OTHER PARTIES | SES1 |
|----|--|------|

- 4. that where a legal or other expert opinion is sought in relation to an item placed before Council, a note that the item is subject to legal or other expert advice (as appropriate) be included in the relevant Agenda or Minutes.
- 5. before the Council considers an item on an agenda that includes or is based on expert advice (eg legal, environmental, financial) the Council shall have been provided with a copy of that advice (or summary if appropriate) prior to the meeting with adequate time to read and understand the advice before making its decision.
- 6. Legal advice sought by Council can only be obtained utilising the services of practitioners who form part of the Panel of Preferred Suppliers, as adopted by Council from time to time, unless, in the CEO's opinion, it is advantageous to utilise the services of a different provider who has specific expertise in a particular case.

- ~~(1) In the instances where Council has resolved or requested to seek legal or other expert advice, a copy of that advice be provided to all Elected Members as soon as practicable within seven (7) days of receipt by the City unless otherwise resolved by Council.~~
 - ~~(2) The intent of any advice received relating to any matter placed before Council for determination being conveyed to Elected Members via "Elected Members Newsletter" within seven days of receipt and Elected Members may obtain a copy or further details of this advice.~~
 - ~~(3) Where copies of legal or other expert advice are made available to Elected Members, the content of the advice is not permitted to be disclosed to third parties, unless by resolution of Council.~~
 - ~~(4) Council maintain its retainer arrangement with its Solicitors for the purpose of ascertaining matters of an administrative nature where procedural verbal advice is obtained.~~
 - ~~(5) Legal advice obtained for:-~~
 - ~~1. initiating specific action by Council or its authorised officers;~~
 - ~~2. the interpretation of statute;~~
 - ~~3. the interpretation of the state of the law in relation to any matter; or~~
 - ~~4. any other advice to be relied upon as legal advice~~
- ~~is only to be obtained from certified practitioners.~~
- (2) Responding to Legal Proceedings:



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Item 8.2 Attachment 4

| | | |
|----|--|------|
| DA | OBTAINING LEGAL OR OTHER EXPERT ADVICE & LEGAL PROCEEDINGS BETWEEN CITY OF COCKBURN & OTHER PARTIES | SES1 |
|----|--|------|

1. The Elected Members must be advised that a legal proceeding has been commenced against the City as soon as practicable after the City has been given notice of the proceeding.

2. A record of the proceedings taken pursuant to this Policy shall be presented to the Audit and Strategic Finance Committee at least annually, or as often as considered appropriate by the CEO, or as requested by the Audit and Strategic Finance Committee.

- (6) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy ~~SES1~~ "Obtaining Legal or Other Expert Advice & Legal Proceedings Between City of Cockburn & Other Parties" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director - Finance and Corporate Services
 Director - Planning & Development
 Director - Governance & Community Services
 Director - Engineering & Works
 Manager, Statutory Planning
 Manager, Strategic Planning
 Manager, Environmental Health
 Manager, Building Services

| | | |
|----|------------------------------------|-------|
| DA | LEASING OF COUNCIL CONTROLLED LAND | APD59 |
|----|------------------------------------|-------|

| | |
|----------------------------------|----------------------------------|
| DELEGATED AUTHORITY CODE: | APD59 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Strategic Planning Services |
| SERVICE UNIT: | Leasing & Land Administration |
| RESPONSIBLE OFFICER: | Director, Planning & Development |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|----------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 22 August 2013 | 24 November 2016 |
| OCM: | 9 June 2011 | 12 September 2013 |
| | 14 June 2012 | 9 June 2016 |

FUNCTION DELEGATED:

To obtain and apply valuations in relation to land to be leased from Council.

CONDITIONS/GUIDELINES:

- (1) When Council owned land or land (vested) in Council is to be leased a sworn Valuer will be requested to value the land and the GRV or Unimproved Value of the land will be used as the basis of determining the annual rental and each case will be considered on its merits.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy APD86.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy APD86 "Leasing of Council Controlled Land" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate authority to:

[1]



DAP 27/08/2020

Item 8.2 Attachment 5

| | | |
|----|------------------------------------|-------|
| DA | LEASING OF COUNCIL CONTROLLED LAND | APD59 |
|----|------------------------------------|-------|

SUB-DELEGATE/S:

Director, Planning & Development
Manager, Strategic Planning
Property & Lands Officer

To be deleted

[2]

| | | |
|----|--|-------|
| DA | LOCAL GOVERNMENT ACT, 1995 – INVESTMENTS | SFCS1 |
|----|--|-------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | SFCS1 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Finance Services |
| SERVICE UNIT: | Accounting Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2012 24 November 2016 |
| OCM: | 9 June 2011 14 June 2012 9 June 2016 |

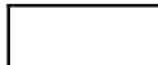
FUNCTION DELEGATED:

The authority to invest monies on behalf of Council.

CONDITIONS/GUIDELINES:

- (1) The Director, Finance & Corporate Services and Manager, Financial Services are to invest monies held in Council Funds as may, from time to time, not be required for use as working funds. The Director, Finance & Corporate Services, or in his absence the Manager, Management Accounting and Budgeting is authorised to sign all cheques/EFTs/other forms of payment prepared for investment of funds. Vouchers for investments made are to be submitted to Council.
- (2) Either delegate has the authority to deal with such matters relevant to this declaration.
- (3) All decisions taken under this authority are to be recorded on Investment Lists presented to Council.
- (4) Any requirements of the Local Government Act, 1995, or (Financial Management) Regulations, 1996, are to be complied with.
- (5) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

[1]



| | | |
|----|--|-------|
| DA | LOCAL GOVERNMENT ACT, 1995 – INVESTMENTS | SFCS1 |
|----|--|-------|

AUTONOMY OF DISCRETION:

Council's investment strategy is:

- (1) Funds required for day to day liquidity requirements shall be invested in either managed Cash Funds or managed investments.
- (2) The balance shall be invested in accordance with Policy SFCS1.
- (3) Choice of investment shall be governed by Council's Investment Policy and continual review of performance.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SFCS1 "Investments" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Finance & Corporate Services
Manager, Financial Services

To be deleted

[2]



| | | |
|----|---|------|
| DA | KERBSIDE HOUSE NUMBERS NUMBERING | AEW2 |
|----|---|------|

| | |
|---------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | AEW2 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Engineering |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | | |
|---------------------------------------|----------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 27 August 2015 | 23 February 2017 |
| OCM: | 9 April 2009 | 10 September 2015 |
| | 14 June 2012 | 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve/refuse applications to apply kerbside property numbering within the district.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy [AEW2'Kerbside House Numbering'](#)
- (2) Any relevant Australian Standard must be complied with in any permissions granted.
- (3) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegated Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy [AEW2'Kerbside House Numbering'](#).

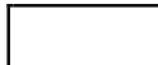
LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy [AEW2](#) "Kerbside House Numbering" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:



DAP 27/08/2020

Item 8.2 Attachment 7

| | | |
|----|--|------|
| DA | KERBSIDE HOUSE NUMBERS <u>NUMBERING</u> | AEW2 |
|----|--|------|

DELEGATE/S AUTHORISED:

~~Director, Engineering & Works
Manager, Engineering
Engineering Works Manager~~



| | | |
|----|--|------|
| DA | MAINTENANCE OF VERGES AND PUBLIC OPEN SPACE (POS) FOLLOWING RESIDENTIAL SUBDIVISION | SEW1 |
|----|--|------|

| | |
|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | SEW1 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Parks |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|-------------------------------|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 27 August 2015 | 26 May 2016 23 February 2017 |
| OCM: | 9 April 2009 14 June 2012 | 10 September 2015 9 June 2016 |

FUNCTION DELEGATED:

The authority to require developers to maintain verge and P.O.S. areas in the district following subdivision.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy SEW1.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy SEW1.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SEW1 'Maintenance of Verges and Public Open Space Following Residential Subdivision' refers.

DELEGATE:

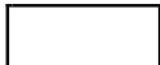
Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Engineering & Works.

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 8

| | | |
|----|---|------|
| DA | MAINTENANCE OF VERGES AND PUBLIC OPEN SPACE (POS) FOLLOWING RESIDENTIAL SUBDIVISION | SEW1 |
|----|---|------|

To be deleted

| | | |
|----|-------------------------------|------|
| DA | STREET & PUBLIC AREA LIGHTING | SEW2 |
|----|-------------------------------|------|

| | |
|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | SEW2 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Engineering |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|----------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 27 August 2015 | 23 February 2017 |
| OCM: | 9 April 2009 | 10 September 2015 |
| | 14 June 2012 | 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve street and public area lighting standards proposed to be erected in association with subdivision, re-development or development within the district.

CONDITIONS/GUIDELINES:

- (1) The requirements specified in Council Policy SEW2.
- (2) To approve the installation of street and public area lighting standards within all existing and proposed streets within the district in accordance with Council Policy SEW2.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided for in Conditions (1) and (2).

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 9

| | | |
|----|-------------------------------|------|
| DA | STREET & PUBLIC AREA LIGHTING | SEW2 |
|----|-------------------------------|------|

DELEGATE/S AUTHORISED:

Director, Engineering and Works
Manager, Engineering Services
Road Design Manager
Project Engineer / Officer - Development
Works Manager

To be deleted

| | | |
|----|----------------------------------|------|
| DA | TRAFFIC MANAGEMENT INVESTIGATION | SEW3 |
|----|----------------------------------|------|

| | |
|----------------------------------|---------------------|
| DELEGATED AUTHORITY CODE: | SEW3 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Road Design |
| RESPONSIBLE OFFICER: | Transport Engineer |
| FILE NO.: | 086/003; 163/006 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|--------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 27 August 2015 |
| | 23 May 2013 | 23 February 2017 |
| OCM: | 9 April 2009 | 13 June 2013 |
| | 14 June 2012 | 10 September 2015 |

FUNCTION DELEGATED:

The authority to investigate and determine requests for traffic management and traffic calming measures to be installed on roads within the district.

CONDITIONS/GUIDELINES:

- (1) The requirements specified in Council Policy SEW3.
- (2) To investigate the installation of traffic management measures using the Warrant Criteria and Weightings incorporated as part of Council Policy SEW3.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken or by another officer under the direction of the initiating officer

AUTONOMY OF DISCRETION:

As provided for in Conditions (1) and (2) above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

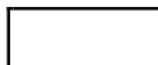
Refer Policy SEW3 'Traffic Management Investigation'
Local Government Act 1995 s5.44 refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

[1]



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Item 8.2 Attachment 10

| | | |
|----|----------------------------------|------|
| DA | TRAFFIC MANAGEMENT INVESTIGATION | SEW3 |
|----|----------------------------------|------|

SUB-DELEGATE/S:

Director Engineering and Works
Manager Engineering Services
Transport Engineer
Road Design Manager
Engineering Technical Officers

To be deleted

[2]

| | | |
|----|--|------|
| DA | PREVENTION OF SAND DRIFT FROM SUBDIVISION AND DEVELOPMENT SITES | SPD7 |
|----|--|------|

| | |
|----------------------------------|----------------------------------|
| DELEGATED AUTHORITY CODE: | SPD7 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Environmental Health |
| SERVICE UNIT: | Environmental Health |
| RESPONSIBLE OFFICER: | Director, Planning & Development |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 6 |

| | | |
|---------------------------------------|---|-----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 22 August 2013 26 February 2015 | 26 November 2015 26 May 2016 |
| OCM: | 9 April 2009 14 June 2012 12 September 2013 | 12 March 2015 10 December 2015 |

To be deleted

FUNCTION DELEGATED:

The authority to approve Dust Management Plans as required and implement Policy SPD7.

CONDITIONS/GUIDELINES:

- (1) Compliance with the provisions and requirements of Policy SPD7.
- (2) The classification of subdivisions and developments as provided for under the Policy.
- (3) Prohibition of bulk earthworks on Class 3 and Class 4 subdivision and development sites between 1st October and 31 March each year.
- (4) The approval of Dust Management Plans.
- (5) Ensuring compliance with any conditions of subdivision and development relating to the requirements of the Policy.
- (6) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Conditions above.

[1]



DAP 27/08/2020

Item 8.2 Attachment 11

| | | |
|----|---|------|
| DA | PREVENTION OF SAND DRIFT FROM SUBDIVISION AND DEVELOPMENT SITES | SPD7 |
|----|---|------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SPD7 "Prevention of Sand Drift from Subdivision and Development Sites" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

SUB-DELEGATE/S:

Director, Planning & Development
Manager, Environmental Health
Co-ordinator, Environmental Health

To be deleted

[2]

| | | |
|----|--|-------|
| DA | APPOINTMENT OF REAL ESTATE AGENT TO SELL COUNCIL OWNED PROPERTY | APD52 |
|----|--|-------|

| | |
|----------------------------------|-----------------------------|
| POLICY CODE: | APD52 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Strategic Planning |
| SERVICE UNIT: | Land & Lease Administration |
| RESPONSIBLE OFFICER: | Property & Lands Officer |
| FILE NO.: | 182/001 |
| DATE FIRST ADOPTED: | 12 October 2006 |
| DATE LAST REVIEWED: | 14 December 2017 |
| ATTACHMENTS: | N/A |
| DELEGATED AUTHORITY REF.: | APD52 |
| VERSION NO. | 6 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|-------------------|------------------|
| DAPPS Meeting: | 27 September 2012 | 26 February 2015 |
| | 31 January 2013 | 26 November 2015 |
| | 22 August 2013 | 23 November 2017 |
| OCM: | 11 October 2012 | 12 March 2015 |
| | 14 February 2013 | 10 December 2015 |
| | 12 September 2013 | |

To be deleted

BACKGROUND:

The City of Cockburn owns the freehold of over 180 properties. Council at its Meeting held on 14 April 2011 resolved to adopt the Land Management Strategy 2011-2016.

The Land Strategy identifies land that can either be sold after being value added (subdivision rezoning) or land that can be sold immediately. Land that has obvious appeal to a range of potential purchasers is best put in the hands of a Real Estate Agent active in the vicinity of the land.

Land that only has value to an adjoining owner does not warrant the services of an agent. In these cases provisions of section 3.58 of the Local Government Act will apply, which necessitates a market valuation by a Licensed Valuer. This valuation would act as a guide in the negotiation to sell the land to an adjoining owner.

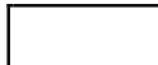
If the land is to be marketed by a Real Estate Agent then Council's Purchasing Policy and the requirements of the Local Government Act 1995 must be complied with.

PURPOSE:

The purpose of this policy is to provide guidance when appointing Real Estate Agents to sell Council freehold land.

POLICY:

[1]



DAP 27/08/2020

Item 8.2 Attachment 12

| | | |
|----|--|-------|
| DA | APPOINTMENT OF REAL ESTATE AGENT TO SELL COUNCIL OWNED PROPERTY | APD52 |
|----|--|-------|

1. The following procedure is to be implemented when appointing a Real Estate Agent to sell on behalf of the City freehold land suitable for residential purposes.
 - (1) Determine whether the land parcel has value to a wide range of potential purchasers or only to the adjoining owner.
 - (2) If the land parcel only has value to an adjoining owner, instructions will be forwarded to a licensed valuer to determine the market value of the land. The market value determined by the licensed valuer will then be used by the CEO under delegated authority to negotiate a purchase price. Provisions of section 3.58 of the Local Government Act apply.
 - (3) If the land parcel has obvious appeal to a wider market and the marketing costs are likely to be in excess of \$100,000, then a public tender will be called. If the fees are estimated to be less than \$100,000, the City will seek quotes from at least three Real Estate Agents. Real Estate Agents with detailed knowledge of the local market is important in this regard.
2. Selection:
 - (1) Selection of a Real Estate Agent will be based on submitted proposal that address the following criteria:

| Measure | Weighting |
|--|-----------|
| Demonstrated successful marketing of similar property with evidence of listings and sales of properties in the vicinity of the subject property | 15 |
| Demonstrated expertise /skill Including years in real estate, local knowledge and membership of professional institutions | 15 |
| Fee structure based on percentage of selling price | 70 |
| | 100 |

3. The appointment of a Real Estate Agent after consideration of the selection criteria will be determined by the CEO pursuant to delegated authority, within the confines prescribed by the Local Government Act, 1995.
4. Appointment of Real Estate Agents to sell properties that are likely to command a selling fee of above \$100,000 will be determined by public tender.
5. Tender documentation and selection criteria will be based on criteria determined for non-public tender selection with additional criteria to suit the particular land parcel.
6. In all cases the asking price will be determined by the CEO after discussion with the successful agent. Acceptance of all offers will be decided by the CEO

[2]

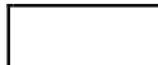
| | | |
|----|--|-------|
| DA | APPOINTMENT OF REAL ESTATE AGENT TO SELL COUNCIL OWNED PROPERTY | APD52 |
|----|--|-------|

pursuant to delegated authority, within the confines prescribed by the Local Government Act 1995.

7. All land sales will have to comply with provisions of section 3.58 of the Local Government Act 1995.
8. The method of selling the property pursuant to the Act either by Public Tender, Public Auction or Private Treaty will be decided by the CEO after discussion with the successful agent pursuant to delegated authority, within the confines prescribed by the Local government Act 1995.
9. The appointment of a Real Estate Agent will be for a minimum three month period (to be determined by the CEO) after which time, if the agent has failed to provide the City with offers in accordance with their submitted proposal, their services will be withdrawn .The City may choose to offer the land sale to the next best candidate based on the original assessment process, or call for new expressions of interest to sell the property.

To be deleted

[3]



DAP 27/08/2020

Item 8.2 Attachment 13

| | | |
|----|--------------------------------|-------|
| DA | LARGE PUBLIC EVENTS – APPROVAL | APD58 |
|----|--------------------------------|-------|

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|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | APD58 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Development Services |
| SERVICE UNIT: | Environmental Health Services |
| RESPONSIBLE OFFICER: | Manager, Environmental Health |
| FILE NO.: | 086/003; 021/005 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 December 2015 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 6 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|----------------|-------------------|
| DAPPS Meeting: | 26 July 2012 | 26 February 2015 |
| | 23 May 2013 | 26 November 2015 |
| | 22 August 2013 | |
| OCM: | 10 April 2008 | 12 September 2013 |
| | 9 August 2012 | 12 March 2015 |
| | 13 June 2013 | |

FUNCTION DELEGATED:

The authority to approve outdoor concerts and large public events.

CONDITIONS/GUIDELINES:

- (1) Only applications made strictly in accordance with Council's conditions will be considered. Timeframes set in the policy will be deemed to commence from the time of receipt of a complying application.
- (2) The Mayor and Ward Councillors from the ward in which the proposed venue is located shall be advised, by e-mail, within 2 working days of the receipt of an application to conduct such an event.
- (3) All other Elected Members shall be advised of the receipt of an application via the next available Elected Members newsletter.
- (4) If requested by any Elected Member the application shall be referred to the next appropriate Council meeting for consideration.
- (5) Where considered to be necessary, or at the request of any Elected Member, residents likely to be affected by the event shall be advised in writing of the nature of the event and be given the opportunity to comment on it. The delegate shall make the determination of which residents are "affected residents" in consultation with the Ward Councillors.
- (6) Where practicable the affected residents' comments shall be addressed through conditions of approval.

[1]

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| DA | LARGE PUBLIC EVENTS – APPROVAL | APD58 |
|----|--------------------------------|-------|

- (7) The City shall ensure that all affected residents are advised as to the outcome of consideration of the application.
- (8) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Large Public Events – Approval DA-APD74
Local Government Act 1995, S 5.44

DELEGATE:

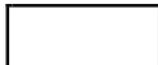
CEO

SUB-DELEGATE/S:

Manager, Environmental Health

To be deleted

[2]



DAP 27/08/2020

Item 8.2 Attachment 14

| | | |
|----|---|-----|
| DA | SEATING ARRANGEMENTS FOR COUNCIL MEETING | AC2 |
|----|---|-----|

| | |
|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AC2 |
| DIRECTORATE: | Executive Support |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 14 September 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|---------------------------------|---------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 28 November 2013 | 26 May 2016 24 August 2017 |
| OCM: | 9 April 2009 14 June 2012 | 12 December 2013 9 June 2016 |

FUNCTION DELEGATED:

The authority to arrange Councilor's seating positions for Council Meetings.

CONDITIONS/GUIDELINES:

- (1) As provided for in Council Policy AC2.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided for Council Policy AC2.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AC2 "SEATING ARRANGEMENTS FOR COUNCIL MEMBERS" refers.

DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

N/A

[1]

81 of 192

| | | |
|----|-------------------------|------|
| DA | REWARDS FOR CIVIC DEEDS | ACS4 |
|----|-------------------------|------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | ACS4 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Ranger & Community Safety Services |
| RESPONSIBLE OFFICER: | Manager, Recreation & Community Safety |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 September 2016 |
| POLICY REF.: | ACS4 |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|-------------------------------|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 28 August 2014 | 26 May 2016 25 August 2016 |
| OCM: | 9 April 2009 14 June 2012 | 11 September 2014 9 June 2016 |

FUNCTION DELEGATED:

The authority to determine the level of reward for the reporting of a crime or good civic deed.

CONDITIONS/GUIDELINES:

The system for rewarding people for both crime and civic good deeds is split into two levels, the higher level being for very serious crime issues of outstanding civic deeds and the lower level being for less serious crimes and good civic deeds.

- (1) To be eligible for an award a candidate must meet one of the following criteria:
 - (a) The perpetrator(s) of a crime are reported to the Police by a person in no way connected to the crime.
 - (b) The Police are satisfied that the person(s) identified has committed the crime and/or has been prosecuted for the crime.
 - (c) Civic Awards will be made to a person or persons who performs a deed well beyond that which would be normally expected.
- (2) All candidates for rewards must be recommended by either senior public figures such as School Principals, Elected Members, JP's, senior Police Officers or Council staff so authorised.
- (3) A cash award of up to \$500 for high level matters;
- (4) All relevant details of recipients will be recorded.
- (5) A Budget Account will be established and maintained for the funding of rewards paid under this delegation.

[1]



| | | |
|----|-------------------------|------|
| DA | REWARDS FOR CIVIC DEEDS | ACS4 |
|----|-------------------------|------|

- (6) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Conditions (3) (4) and (5) above

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy ACS4 "Rewards" refers.

DELEGATE:

Chief Executive Officer

Note: Chief Executive Officer will sub-delegate this authority to:-

DELEGATE/S AUTHORISED:

Manager, Recreation & Community Safety
Ranger & Community Safety Services Manager

To be deleted

[2]



| | | |
|----|----------------------------------|------|
| DA | VOLUNTEER FIRE FIGHTERS TRAINING | ACS6 |
|----|----------------------------------|------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | ACS6 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Ranger & Community Safety Services |
| RESPONSIBLE OFFICER: | Manager, Recreation & Community Safety |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 September 2016 |
| POLICY REF.: | ACS6 |
| VERSION NO. | 4 |

| | |
|---------------------------------------|---|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 28 August 2014 25 August 2016 |
| OCM: | 9 April 2009 14 June 2012 11 September 2014 |

To be deleted

FUNCTION DELEGATED:

The authority to enrol / approve of Volunteer Fire Fighters in relevant training programme.

CONDITIONS/GUIDELINES:

- (1) Training is provided by the State's Lead Fire or Emergency Services Combat Authority.
- (2) Training costs are to be within the Department of Fire and Emergency Services (D.F.E.S.) Grant Allocation.
- (3) Any requirements of the Bush Fires Act, 1954, are to be complied with.

AUTONOMY OF DISCRETION:

As provided in conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Bush Fires Act, 1954

DELEGATE:

Chief Executive Officer

Note: Chief Executive Officer will sub-delegate this authority to:-

[1]



DAP 27/08/2020

Item 8.2 Attachment 16

| | | |
|----|----------------------------------|------|
| DA | VOLUNTEER FIRE FIGHTERS TRAINING | ACS6 |
|----|----------------------------------|------|

DELEGATE/S AUTHORISED:

Chief Bush Fire Control Officer
Deputy Chief Bush Fire Control Officer
Manager, Recreation & Community Safety

To be deleted

[2]

| | | |
|----|-----------------------|-------|
| DA | EMERGENCY RELIEF FUND | ACS13 |
|----|-----------------------|-------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | ACS13 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Recreation & Community Safety |
| SERVICE UNIT: | Grants & Research |
| RESPONSIBLE OFFICER: | Manager, Recreation & Community Safety |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 September 2016 |
| POLICY REF.: | ACS13 |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|--|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 22 September 2011 24 May 2012 28 August 2014 | 26 May 2016 25 August 2016 |
| OCM: | 13 October 2011 14 June 2012 | 11 September 2014 9 June 2016 |

FUNCTION DELEGATED:

The authority to make financial donations towards emergency relief, if and when, an emergency/disaster occurs. To evaluate and determine the level of relief funding to be donated in accordance with the Emergency Relief Fund Policy.

CONDITIONS/GUIDELINES:

Funds for an emergency/disaster which meet the following criteria shall be drawn from the Emergency Relief Fund.

- (1) Situation 1: Where a 'State of Emergency Declaration' is made by either a Hazard Management Agency or relevant Minister of Western Australia.
 - The Delegated Officer in consultation with the Mayor can award a donation of up to \$15,000 to the relevant relief fund.
- (2) Situation 2: Where an emergency occurs within Western Australia and a 'State of Emergency Declaration' is not made but the emergency/disaster is deemed significant by both the Mayor and the Delegated Officer.
 - The Delegated Officer in consultation with the Mayor can award a donation of up to \$10,000 to the relevant relief fund.
- (3) Situation 3: Where an emergency occurs outside the jurisdiction of the State of Western Australia and a 'State of Emergency Declaration' is made.
 - The Delegated Officer in consultation with the Mayor can award a donation of up to \$15,000 to the relevant relief fund.

[1]



| | | |
|----|------------------------------|-------|
| DA | EMERGENCY RELIEF FUND | ACS13 |
|----|------------------------------|-------|

- (4) Situation 4: Where an emergency occurs outside the jurisdiction of the State of Western Australia and a 'State of Emergency Declaration' is not made but the emergency/disaster is deemed significant by both the Mayor and the Delegated Officer.
 - The Delegated Officer in consultation with the Mayor can award a donation of up to \$10,000 to the relevant relief fund.
- (5) Situation 5: When an Emergency or Disaster occurs but does not meet the criteria of situation 1- 4.
 - The Delegated Officer in consultation with the Mayor can award a donation of up to \$5,000 to the relevant relief fund.

The funds are available for donation to the relevant relief fund and be awarded to the maximum stated under each situation. The actual amount is to be determined at the discretion of the Delegated Officer in consultation with the Mayor in consideration of the overall estimated cost of relief and unique circumstances for each specific emergency/disaster.
- (6) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in the conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy "ACS13 'Emergency Relief Fund, refers.

DELEGATE:

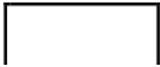
Chief Executive Officer (CEO)
 Note: The CEO will sub-delegate this authority to

DELEGATE/S AUTHORISED:

Manager, Recreation & Community Safety

To be deleted

[2]



| | | |
|----|---------------------------------|-------|
| DA | CITY OF COCKBURN ART COLLECTION | ACS14 |
|----|---------------------------------|-------|

| | |
|----------------------------------|-----------------------------------|
| DELEGATED AUTHORITY CODE: | ACS14 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Corporate Communication |
| SERVICE UNIT: | Events & Culture |
| RESPONSIBLE OFFICER: | Cultural Development Co-ordinator |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 9 June 2016 |
| DATE LAST REVIEWED: | 8 September 2016 |
| ATTACHMENTS: | N/A |
| POLICY REF. | ACS14 |
| VERSION NO. | 2 |

| | |
|---------------------------------------|-------------------------------|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 26 May 2016 25 August 2016 |
| OCM: | 9 June 2016 |

FUNCTION DELEGATED:

The authority to administer the City's Art Collection Policy and ensure the City's Art Collection is suitably recorded and maintained in accordance with the Policy.

CONDITIONS/GUIDELINES:

- (1) Funds being provided on the Annual Budget.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decision Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy ACS14 "Annual Art Collection" refers.

DELEGATE:

Chief Executive Officer.

DELEGATE/S AUTHORISED:

Cultural Development Co-ordinator.

[1]



DAP 27/08/2020

Item 8.2 Attachment 19

| | | |
|----|------------------------------------|------|
| DA | ANNUAL GENERAL MEETING OF ELECTORS | AES1 |
|----|------------------------------------|------|

| | |
|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES1 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|---------------------------------|---------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 27 February 2014 | 25 February 2016 26 May 2016 |
| OCM: | 9 April 2009 11 June 2012 | 13 March 2014 10 March 2016 |

FUNCTION DELEGATED:

To enable the ongoing organisation of the Annual Meeting of Electors.

CONDITIONS/GUIDELINES:

- (1) The meeting to be conducted on the first Tuesday in February each year.
- (2) Receipt of the Auditor's Report on the Annual Financial Statement.
- (3) The provisions of Sec. 5.27 of the Local Government Act, 1995, are to be adhered to.
- (4) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES1 "Annual General Meeting of Electors" refers.

DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

N/A

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|----|-------------------------------|------|
| DA | INDUSTRIAL RELATIONS SERVICES | AES3 |
|----|-------------------------------|------|

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|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES3 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 March 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|---|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 27 February 2014 25 February 2016 |
| OCM: | 9 April 2009 14 June 2012 13 March 2014 |

To be deleted

FUNCTION DELEGATED:

The authority to appoint a provider of industrial relations services on behalf of Council to represent the City in an industrial relations action or matter, if it is likely to result in attendance in a court or tribunal of competent jurisdiction.

CONDITIONS/GUIDELINES:

- (1) Elected Members to be notified of the status of industrial relations action commenced against the City.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decision Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES3 "Industrial Relations" refers.

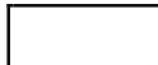
DELEGATE:

Chief Executive Officer

SUB-DELEGATE/S:

N/A

[1]



DAP 27/08/2020

Item 8.2 Attachment 21

| | | |
|----|-------------------------|------|
| DA | ANNUAL ARTS COMPETITION | AES4 |
|----|-------------------------|------|

| | |
|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES4 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 14 June 2012 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 1 |

| | |
|---------------------------------------|--------------|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 |
| OCM: | 9 April 2009 |

FUNCTION DELEGATED:

The authority to choose the winning or other suitable entry(s) in the "Cockburn Local Features" category at the Annual Arts and Crafts exhibition.

CONDITIONS/GUIDELINES:

- (1) Funds being provided on the Annual Budget.
- (2) All transactions utilising this delegation are to be recorded in the Delegated Authority Portal System by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES4 "Annual Arts Competition" refers.

DELEGATE:

Chief Executive Officer.

DELEGATE/S AUTHORISED:

N/A

| | | |
|----|------------------------------------|------|
| DA | APPROVAL FOR RESEARCH/STUDY VISITS | AES7 |
|----|------------------------------------|------|

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|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES7 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 March 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|---|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 27 February 2014 25 February 2016 |
| OCM: | 9 April 2009 14 June 2012 13 March 2014 |

FUNCTION DELEGATED:

Authority to approve the attendance of Council Representatives for Research/Study Visits.

CONDITIONS/GUIDELINES:

- (1) As provided in Council Policy AES7.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AES7.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES7 "APPROVAL OF RESEARCH/STUDY VISITS" refers.

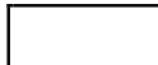
DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

N/A

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 23

| | | |
|----|-----------------------------|------|
| DA | COUNCIL OWNED VEHICLE USAGE | AES8 |
|----|-----------------------------|------|

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|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES8 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 June 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 27 February 2014 25 February 2016 18 May 2017 |
| OCM: | 9 April 2009 14 June 2012 13 March 2014 10 March 2016 |

FUNCTION DELEGATED:

The authority to approve or negotiate private and commuting vehicle usage to staff to whom Council owned or leased vehicles are allocated.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AES8.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AES8.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES8 "Council Owned Vehicle Usage" refers.

DELEGATE:

Chief Executive Officer.

Item 8.2 Attachment 23

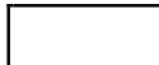
DAP 27/08/2020

| | | |
|----|-----------------------------|------|
| DA | COUNCIL OWNED VEHICLE USAGE | AES8 |
|----|-----------------------------|------|

DELEGATE/S AUTHORISED:

N/A

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 24

| | | |
|----|--|------|
| DA | APPROVAL TO PARTICIPATE IN REPRESENTATIVE DELEGATIONS | AES9 |
|----|--|------|

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|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES9 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|------------------|------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 25 February 2016 |
| | 27 February 2014 | 26 May 2016 |
| OCM: | 9 April 2009 | 13 March 2014 |
| | 14 June 2012 | 10 March 2016 |

FUNCTION DELEGATED:

The authority to approve the attendance of Council representation to delegations arranged to promote the interests of the City of Cookburn and/or the adjacent Region.

CONDITIONS/GUIDELINES:

- (1) The Mayor to be consulted on each occasion a delegation is proposed, pursuant to Council Policy.
- (2) Sufficient funds being available in the relevant Conference Account(s).
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AES9.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES9 "Approval to Participate in Representative Delegations" refers.

DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

N/A

| | | |
|----|-------------------|-------|
| DA | UNDERGROUND POWER | AES10 |
|----|-------------------|-------|

| | |
|----------------------------------|-------------------------|
| DELEGATED AUTHORITY CODE: | AES10 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 March 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|---|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 27 February 2014 25 February 2016 |
| OCM: | 9 April 2009 14 June 2012 13 March 2014 |

FUNCTION DELEGATED:

The authority to lodge Expressions of Interest and subsequent Applications for participation in future stages of the State Government's Underground Power Program.

CONDITIONS/GUIDELINES:

- (1) Compliance with any criteria applied to the Program by the State Government; and
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided for in Conditions and Council Policy.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AES10 "Underground Power" refers.

DELEGATE:

Chief Executive Officer

SUB-DELEGATE/S:

N/A

[1]



DAP 27/08/2020

Item 8.2 Attachment 26

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|----|---|-------|
| DA | INDUSTRIAL RELATIONS – EMPLOYEE REDUNDANCY PAYMENT | AES11 |
|----|---|-------|

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|----------------------------------|------------------------------|
| DELEGATED AUTHORITY CODE: | AES11 |
| DIRECTORATE: | Executive Support Department |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 March 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|--------------|------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 27 February 2014 |
| | 23 May 2013 | 25 February 2016 |
| OCM: | 14 June 2007 | 13 June 2013 |
| | 14 June 2012 | 13 March 2014 |

FUNCTION DELEGATED:

The authority to utilise funds from the "Staff Payments and Entitlements Reserve" Account to fund redundancy payments for staff.

CONDITIONS/GUIDELINES:

- (1) Payments to be made in accordance with Council's Enterprise Agreements (E.A's).
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

In accordance with the provisions outlined in Council's E.A's.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Employee Redundancy Payments POL-AES11
Local Government Act 1995, S 5.44

DELEGATE:

CEO

SUB-DELEGATE/S:

Nil

[1]

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|----|---------------------------|------|
| DA | STREET VERGE IMPROVEMENTS | AEW1 |
|----|---------------------------|------|

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|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | AEW1 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Parks |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|-------------------------------|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 27 August 2015 | 26 May 2016 23 February 2017 |
| OCM: | 9 April 2009 14 June 2012 | 10 September 2015 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve/refuse applications for street verge development and/or improvement.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AEW1.
- (2) Compliance with Part IX, Division 2 of Council's Consolidated Local Laws required.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AEW1.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AEW1 "Street Verges - Improvements" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to

DELEGATE/S AUTHORISED:

Director, Engineering & Works
Manager, Parks & Environment

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 28

| | | |
|----|-----------------|------|
| DA | STREET LIGHTING | AEW3 |
|----|-----------------|------|

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|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | AEW3 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Engineering |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|---------------------------------|---------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 26 November 2015 | 26 May 2016 23 February 2017 |
| OCM: | 9 April 2009 14 June 2012 | 10 December 2015 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve applications for new street lighting within the district.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AEW3
- (2) Any requirements of State Electricity Commission Legislation must be complied with.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AEW3

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AEW3 "Street Lighting" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

DELEGATE/S AUTHORISED:

Director, Engineering & Works

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| DA | INSTALLATION OF PLAYGROUND/RECREATION EQUIPMENT ON RESERVES | AEW4 |
|----|--|------|

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| DELEGATED AUTHORITY CODE: | AEW4 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Parks Services |
| RESPONSIBLE OFFICER: | Manager, Parks & Environment |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|-------------------------------|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 27 August 2015 | 26 May 2016 23 February 2017 |
| OCM: | 9 April 2009 14 June 2012 | 10 September 2015 9 June 2016 |

FUNCTION DELEGATED:

The authority to determine the suitability of installing equipment in parks.

CONDITIONS/GUIDELINES:

- (1) Prior to the installation of basketball areas and playground equipment, a sign be erected for 2 weeks advising of the location.
- (2) A letter be sent to all houses within 50 metres of the proposed site advising them of the intention to install basketball practice areas and playground equipment.
- (3) If there are any objections to the positioning, Ward Councillors are to be advised so that on-site discussions can be held to agree on a location.
- (4) If no objections are received the installation of the basketball practice area and playground equipment is to proceed.
- (5) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

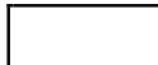
AUTONOMY OF DISCRETION:

As provided in Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AEW4 "Installation of Playground/Recreational Equipment on Reserves" refers.

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 29

| | | |
|----|--|------|
| DA | INSTALLATION OF PLAYGROUND/RECREATION EQUIPMENT ON RESERVES | AEW4 |
|----|--|------|

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Manager, Parks & Environment

To be deleted

| | | |
|----|--|------|
| DA | LANDOWNER BIODIVERSITY CONSERVATION GRANT PROGRAM | AEW5 |
|----|--|------|

| | |
|----------------------------------|---------------------|
| DELEGATED AUTHORITY CODE: | AEW5 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Parks Services |
| RESPONSIBLE OFFICER: | Manager, Parks |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|-------------------------------|----------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 27 August 2015 | 26 May 2016 23 February 2017 |
| OCM: | 9 April 2009 14 June 2012 | 10 September 2015 9 June 2016 |

FUNCTION DELEGATED:

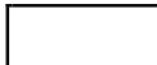
The authority to determine and allocate grants under the Landowner Biodiversity Conservation Grant Program.

CONDITIONS/GUIDELINES:

Landowners may, upon written application to the Chief Executive Officer, be Awarded a grant on condition that:

- (1) The landowner(s) submits a completed application form, signs a 'Conditions of Funding' contract and signs a 'Voluntary Management Agreement' that states that they agree to protect the bushland/wetland area, for which the grant was obtained, from activities likely to adversely impact on it, for a minimum period of 3 years;
- (2) The subject land is zoned rural, rural living or resource zones.
- (3) The project benefits the natural environment on the property and promotes conservation of bushland or wetland
- (4) The subject land has a minimum area of 1,000 square metres of remnant bushland/wetland on the property.
- (5) Landowners contribute to the project at a dollar for dollar ratio; or contribute in-kind labour or other resources to an equal value. The value of proposed in-kind labour or other resources shall be calculated and approved by an officer from the City of Cockburn;

To be deleted



DAP 27/08/2020

Item 8.2 Attachment 30

| | | |
|----|--|------|
| DA | LANDOWNER BIODIVERSITY CONSERVATION GRANT PROGRAM | AEW5 |
|----|--|------|

- (6) The grant is used for projects that feature physical on-ground works such as weed control, erection of fencing to exclude stock, revegetation to link bushland areas and erosion control; and
- (7) Locally provenance plant species only are used for revegetation works.
- (8) Landowners shall be eligible to apply for subsequent grants on condition that they have successfully acquitted previous grants.
- (9) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

Allocation of Grants

Applications for grants will be evaluated and awarded:

- (1) In order of merit until the funding pool approved in the Council's annual operating budget is exhausted; and
- (2) In order of preference to properties with large remnant bushland or wetland areas that are of good biodiversity value. Smaller areas of bushland shall be considered if they are linked to adjoining properties or green corridors or if the conservation value of the area in question is extremely high

Acquittal of Grants

Grant recipients shall be required to complete and submit an acquittal advice to the Chief Executive Officer within twelve months of receiving a grant, confirming that funds have been expended in accordance with the 'Conditions of Funding' contract.

AUTONOMY OF DISCRETION:

As provided in Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AEW5 "Landowner Biodiversity Conservation Grant Program " refers

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Manager, Parks & Environment

| | | |
|----|----------------------------|------|
| DA | PROMOTIONAL STREET BANNERS | AEW6 |
|----|----------------------------|------|

| | |
|----------------------------------|-------------------------------|
| DELEGATED AUTHORITY CODE: | AEW6 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Engineering |
| RESPONSIBLE OFFICER: | Director, Engineering & Works |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|----------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 27 August 2015 | 23 February 2017 |
| OCM: | 9 April 2009 | 10 September 2015 |
| | 14 June 2012 | 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve/refuse applications for promotional street banners on light poles within new and renewal property developments.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AEW6.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AEW6.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AEW6 "Promotional Street Banners" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

DELEGATE/S AUTHORISED:

Director, Engineering & Works
 Manager, Engineering
 Senior Development Engineer

To be deleted



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Item 8.2 Attachment 31

| | | |
|----|----------------------------|------|
| DA | PROMOTIONAL STREET BANNERS | AEW6 |
|----|----------------------------|------|

To be deleted

| | | |
|----|--|------|
| DA | SUBMISSION AND COMMENT ON ENVIRONMENTAL APPROVALS AND MATTERS | AEW8 |
|----|--|------|

| | |
|----------------------------------|--------------------------------|
| DELEGATED AUTHORITY CODE: | AEW8 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Environmental Services |
| RESPONSIBLE OFFICER: | Manager, Parks and Environment |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| POLICY REF.: | AEW10 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 6 |

| | | |
|---------------------------------------|--|------------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 23 May 2013 2 June 2015 | 27 August 2015 23 February 2017 |
| OCM: | 11 December 2008 14 June 2012 13 June 2013 | 11 June 2015 10 September 2015 |

FUNCTION DELEGATED:

Authority is given to the delegate to respond on behalf of Council to matters relevant to the Environmental Protection Act 1986, and the Environmental Protection and Biodiversity Conservation Act 1999 and other matters of an environmental nature.

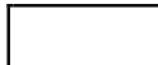
CONDITIONS/GUIDELINES:

- (1) The provision of critical technical comment, submissions and appeals in relation to reports which are generated under Part IV of the Environmental Protection Act 1986;
- (2) Appeals generated under Part VII of the Environmental Protection Act 1986;
- (3) Licences and works approvals relevant to Part V of the Environmental Protection Act 1986;
- (4) The provision of critical technical comment, submissions and appeals in relation to other relevant Environmental legislation including the Environmental Protection and Biodiversity Conservation Act 1999.
- (5) Other reports which are not necessarily derived from legislation, but require comment from Council on a technical basis in relation to environmental matters.

All of the considerations and comment will give regard to the following:-

- Where the time period for comment or appeals allows, the matter is to be referred to Council for consideration.

[1]



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Item 8.2 Attachment 32

| | | |
|----|--|------|
| DA | SUBMISSION AND COMMENT ON ENVIRONMENTAL APPROVALS AND MATTERS | AEW8 |
|----|--|------|

- Where the time period does not allow for the matter to be referred to Council, technical comments and decisions in respect to the submission of appeals are to be made by the delegate taking into account the following:-
 - Council policy and previous decisions
 - views of Ward Councillors and senior staff
 - community views and concerns
 - professional knowledge and opinion of delegate
 - history of issue where relevant
- (6) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer

AUTONOMY OF DISCRETION:

As provided in Conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Policy AEW10 'Submission & Comment on Environmental Approvals & Matters
Local Government Act 1995, s5.44

DELEGATE:

CEO

SUB-DELEGATE/S:

Manager, Parks & Environment
Manager, Health Services
Environment Manager

To be deleted

[2]



| | | |
|----|--|------|
| DA | INTERNALLY ILLUMINATED DIRECTIONAL SIGNS | AEW9 |
|----|--|------|

| | |
|----------------------------------|-----------------------|
| DELEGATED AUTHORITY CODE: | AEW9 |
| DIRECTORATE: | Engineering and Works |
| BUSINESS UNIT: | Engineering |
| SERVICE UNIT: | Engineering |
| RESPONSIBLE OFFICER: | Manager, Engineering |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 March 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 6 |

| | | |
|---------------------------------------|-------------------|-------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 22 August 2013 | 23 February 2017 |
| | 27 August 2015 | |
| OCM: | 9 April 2009 | 10 September 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 12 September 2013 | |

FUNCTION DELEGATED:

- (1) The authority to approve or refuse applications for internally illuminated directional signs within the road reserve of commercial and industrial areas.
- (2) The authority to approve or refuse electronic and other messages proposed for display on any sign approved under point (1) above.
- (3) The authority to endorse agreements between the City of Cockburn and sign companies.

CONDITIONS/GUIDELINES:

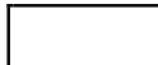
- (1) Applications for signs within residential and other non – commercial / non- industrial areas are to be determined by Council.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

City of Cockburn Town Planning Scheme No. 3 Section 11.3.1 and 11.3.2



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Item 8.2 Attachment 33

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|----|--|------|
| DA | INTERNALLY ILLUMINATED DIRECTIONAL SIGNS | AEW9 |
|----|--|------|

DELEGATE:

CEO.

SUB DELEGATE/S:

Director, Engineering & Works
Manager, Engineering

To be deleted

| | | |
|----|--|-------|
| DA | INSTALLATION OF PRIVATE MEMORIAL PLAQUES IN PUBLIC OPEN SPACE | AEW10 |
|----|--|-------|

| | |
|----------------------------------|------------------------------|
| DELEGATED AUTHORITY CODE: | AEW10 |
| DIRECTORATE: | Engineering & Works |
| BUSINESS UNIT: | Parks & Environment |
| SERVICE UNIT: | Parks Services |
| RESPONSIBLE OFFICER: | Manager, Parks & Environment |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 10 December 2015 |
| DATE LAST REVIEWED: | 9 March 2017 |
| POLICY REF.: | AEW12 |
| VERSION NO. | 2 |

| | | |
|---------------------------------------|------------------|------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 26 November 2015 | 23 February 2017 |
| | 26 May 2016 | |
| OCM: | 9 June 2016 | |

FUNCTION DELEGATED:

To give clear guidance to applicants regarding what is required for approval from the City, what type of memorial plaque is allowed, what sites are available (if any), the specifications for the memorial plaque and who is responsible for the associated costs.

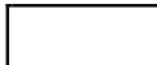
CONDITIONS/GUIDELINES:

- (1) A brief submission, in writing, shall be issued to Parks Services that demonstrates the significance of the individual, nature of the memorial and the POS location.
- (2) Where POS has no or limited park furniture the combination of a plaque on an item of approved park furniture will only be considered.
- (3) Where POS is already well appointed with park furniture containing memorial plaques an alternate location will need to be considered.
- (4) The memorial plaque and park furniture, if approved, shall be funded by the applicant and include the full cost of the procurement, manufacture and installation, including a concrete pad (if required).
- (5) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AEW12.

To be Deleted



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Item 8.2 Attachment 34

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|----|--|-------|
| DA | INSTALLATION OF PRIVATE MEMORIAL PLAQUES IN PUBLIC OPEN SPACE | AEW10 |
|----|--|-------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Policy AEW12 'Installation of Private Memorial Plaques in Public Open Space' refers.

DELEGATE:

Chief Executive Officer (CEO)

Note: The CEO will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Engineering & Works
Manager, Parks & Environment

To be deleted

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| DA | EMPLOYEE DEVELOPMENT | AFCS1 |
|----|----------------------|-------|

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|----------------------------------|------------------------------|
| DELEGATED AUTHORITY CODE: | AFCS1 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Corporate Services |
| SERVICE UNIT: | Human Resources |
| RESPONSIBLE OFFICER: | Manager, Human Resources |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2016 24 November 2016 |
| OCM: | 9 April 2009 14 June 2012 9 June 2016 |

FUNCTION DELEGATED:

The authority to approve / refuse employee applications for Study Assistance from Council.

CONDITIONS/GUIDELINES:

- (1) As provided in Council Policy AFCS1.
- (2) Any requirements of Local Government Employee Awards to be complied with.
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Council Policy AFCS1.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AFCS1 "Employee Development" refers.

DELEGATE:

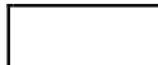
Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

DELEGATE/S AUTHORISED:

Manager, Human Resources

To be deleted



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Item 8.2 Attachment 36

| | | |
|----|------------------------------------|-------|
| DA | LEASING OF COUNCIL CONTROLLED LAND | AFCS2 |
|----|------------------------------------|-------|

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|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | AFCS2 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Financial Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 22 August 2013 26 May 2016 |
| OCM: | 9 June 2011 14 June 2012 12 September 2013 |

FUNCTION DELEGATED:

To obtain and apply valuations in relation to land to be leased from Council.

CONDITIONS/GUIDELINES:

- (1) When Council owned land or land (vested) in Council is to be leased a sworn Valuer will be requested to value the land and the GRV or Unimproved Value of the land will be used as the basis of determining the annual rental and each case will be considered on its merits.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Policy AFCS2.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AFCS2 "Leasing of Council Controlled Land" refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate authority to:

[1]

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| | | |
|----|------------------------------------|-------|
| DA | LEASING OF COUNCIL CONTROLLED LAND | AFCS2 |
|----|------------------------------------|-------|

SUB-DELEGATE/S:

Director, Finance and Corporate Services
Manager, Financial Services
Manager, Strategic Planning
Property & Lands Officer

To be deleted

[2]



| | | |
|----|--------------------|-------|
| DA | DISPOSAL OF ASSETS | AFCS3 |
|----|--------------------|-------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | AFCS3 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Accounting Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2016 24 November 2016 |
| OCM: | 9 June 2011 14 June 2012 9 June 2016 |

FUNCTION DELEGATED:

The authority to dispose of certain assets.

CONDITIONS/GUIDELINES:

- (1) Where assets have been identified as surplus to the requirements of the City the following procedures will apply to the disposal of that asset.

Land:

Land with a value of greater than \$5,000 must be disposed in accordance with the requirements of Section 3.58 of the Local Government Act, 1995 (being by public auction or public tender).

The disposal of land with a value of less than \$5,000 is an exempt disposition under Regulation 30 of the Local Government (Functions and General) Regulations. The City may sell this land by private treaty.

Property (other than Land) – Plant, Machinery, and Furniture and Equipment

Property (other than land) with a value of greater than \$20,000 must be disposed in accordance with the requirements of Section 3.58 of the Local Government Act, 1995 (being by public auction or public tender), unless it is disposed of as part of the consideration (trade-in) for other property than the local government is acquiring where the consideration value received is not worth more than \$50,000.

[1]

To be deleted

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| DA | DISPOSAL OF ASSETS | AFCS3 |
|----|--------------------|-------|

The disposal of property (other than land) with a value of less than \$20,000 is an exempt disposition under Regulation 30 of the Local Government (Functions and General) Regulations. Accordingly, the following methods of disposal may be used by Council:

- Sale of the property by private treaty
- Trade-in for other property
- Sale by public or private auction, including a bidding system and a portal system such as e-Bay
- Donation or gifting of the items to approved not for profit or local community organisations

The sale by private auction method is restricted to Staff. This is intended as a workplace benefit whereby the City does not suffer economic loss (eg. Auction of used motor vehicle at a reserve price equal to that offered to the City as a trade).

Unsaleable Items:

When all efforts fail to dispose of property in accordance with the above guidelines and it is determined to have no commercial value, the City may offer the property to Staff at no cost or authorise its disposal by recycling or scrapping.

- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

To be deleted

AUTONOMY OF DISCRETION:

As provided in Policy AFCS3.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AFCS3 "DISPOSAL OF ASSETS" refers.

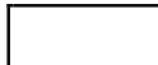
DELEGATE:

Chief Executive Officer
 Note: This authority will be sub-delegated to:

DELEGATE/S AUTHORISED:

- Director, Finance & Corporate Services
- Director, Engineering & Works
- Manager, Infrastructure
- Manager, Financial Services

[2]



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Item 8.2 Attachment 38

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|----|---|-------|
| DA | DEFENCE FORCE RESERVES – STAFF PARTICIPATION | AFCS4 |
|----|---|-------|

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|----------------------------------|------------------------------|
| DELEGATED AUTHORITY CODE: | AFCS4 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Corporate Services |
| SERVICE UNIT: | Human Resources |
| RESPONSIBLE OFFICER: | Manager, Human Resources |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | | |
|---------------------------------------|------------------------------|---------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 19 March 2009 24 May 2012 | 26 May 2016 24 November 2016 |
| OCM: | 9 April 2009 14 June 2012 | 9 June 2016 |

FUNCTION DELEGATED:

To consider and approve if appropriate, requests from City of Cooruburn employees for time off from duties to attend formal Defence Force Reserve activities, as required.

CONDITIONS/GUIDELINES:

- (1) As provided in Council Policy AFCS4.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Council Policy AFCS4.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Defence Force Reserve Service (Protection) Act, 2001, refers.

DELEGATE:

Nil.

DELEGATE/S AUTHORISED:

Manager, Human Resources

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| DA | RENEWAL OF LEASES AND LICENSES FOR COUNCIL OWNED OR CONTROLLED PROPERTY | AFCS6 |
|----|--|-------|

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| DELEGATED AUTHORITY CODE: | AFCS6 |
| DIRECTORATE: | Executive Services |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Support |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 14 June 2012 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 3 |

| | |
|---------------------------------------|------------------------------|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2016 |
| OCM: | 9 April 2009 14 June 2012 |

FUNCTION DELEGATED:

The authority to renew a lease and licence agreement.

CONDITIONS/GUIDELINES:

- (1) As provided in Policy AFCS6.
- (2) Local Government Act 1995 section 3.58
Local Government (Functions and General) Regulations 1996 section 30
- (3) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided in Council Policy AFCS6.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy AFCS6 "Renewal of Leases and Licenses for Council Owned or Controlled Property" refers.

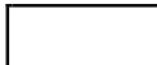
DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

Nil.

[1]



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Item 8.2 Attachment 40

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|----|--|-------|
| DA | CHANGE OF BASIS FOR VALUATION OF LAND FOR RATING PURPOSES | AFCS8 |
|----|--|-------|

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|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | AFCS8 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Rating & Revenue Services |
| RESPONSIBLE OFFICER: | Director, Finance & Corporate Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 26 May 2016 24 November 2016 |
| OCM: | 9 April 2009 14 June 2012 9 June 2016 |

FUNCTION DELEGATED:

The authority to make application to the Director General, Department of Local Government and Regional Development for a change to the basis of land valuation for rating purposes from Unimproved Value to Gross Rental Value as subdivisions occur.

CONDITIONS/GUIDELINES:

- (1) As set out in the policy and in guidelines issued by the Department of Local government and Regional Development.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under legislative requirements.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act 1995 Section 6.28.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:-

Item 8.2 Attachment 40

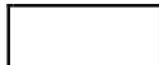
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|----|--|-------|
| DA | CHANGE OF BASIS FOR VALUATION OF LAND FOR RATING PURPOSES | AFCS8 |
|----|--|-------|

DELEGATE/S AUTHORISED:

Director, Finance & Corporate Services

To be deleted



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Item 8.2 Attachment 41

| DA | MEDIA ACTIVITY | SCS1 |
|----|----------------|------|
|----|----------------|------|

| | |
|----------------------------------|-----------------------------------|
| DELEGATED AUTHORITY CODE: | SCS1 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Corporate Communications |
| SERVICE UNIT: | Marketing & Media |
| RESPONSIBLE OFFICER: | Manager, Corporate Communications |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 8 September 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| Dates of Amendments / Reviews: | |
|--------------------------------|--|
| DAPPS Meeting: | 24 May 2012 26 May 2016 25 August 2016 |
| OCM: | 9 April 2009 14 June 2012 9 June 2016 |

FUNCTION DELEGATED:

The authority to liaise with the Media on matters of interest to Council within the Community.

CONDITIONS/GUIDELINES:

- (1) As provided under Policy SCS1 and the Local Government Act, 1995, Sec. 2.8 and Sec. 5.41.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Policy SCS1.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SCS1 'Media Activity' refers.

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

To be deleted

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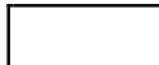
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| | | |
|----|----------------|------|
| DA | MEDIA ACTIVITY | SCS1 |
|----|----------------|------|

DELEGATE/S AUTHORISED:

Manager, Corporate Communications
Media Liaison Officer

To be deleted



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Item 8.2 Attachment 42

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|----|---------------------------------------|-----|
| DA | CONDUCT OF ELECTIONS BY POSTAL BALLOT | SC8 |
|----|---------------------------------------|-----|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | SC8 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Governance |
| SERVICE UNIT: | Governance |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 9 June 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 May 2012 28 November 2013 26 May 2016 |
| OCM: | 9 April 2009 14 June 2012 12 December 2013 |

FUNCTION DELEGATED:

The authority to seek the permission of the Electoral Commissioner to be responsible for the conduct of Council elections

CONDITIONS/GUIDELINES:

- (1) Agreement to be sought in August preceding each ordinary election cycle;
- (2) Once agreement has been obtained, the matter will be presented to Council for formal consideration;
- (3) Any relevant provisions of the Local Government Act, 1995, to be complied with; and
- (4) All transactions utilising this delegation are to be recorded in the Recording of Delegated Decisions Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As per conditions above

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SC8 "Review of Electoral Matters" refers.

[1]

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| | | |
|----|---------------------------------------|-----|
| DA | CONDUCT OF ELECTIONS BY POSTAL BALLOT | SC8 |
|----|---------------------------------------|-----|

DELEGATE:

Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Governance & Community Services

To be deleted

[2]



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Item 8.2 Attachment 43

| | | |
|----|---|------|
| DA | ACCESS TO TAPE RECORDINGS OF COUNCIL MEETINGS | SES2 |
|----|---|------|

| | |
|----------------------------------|---------------------------------|
| DELEGATED AUTHORITY CODE: | SES2 |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Governance |
| SERVICE UNIT: | Governance |
| RESPONSIBLE OFFICER: | Chief Executive Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 10 March 2016 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| | | |
|---------------------------------------|------------------|------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 24 May 2012 | 22 May 2014 |
| | 31 January 2013 | 25 February 2016 |
| | 27 February 2014 | |
| OCM: | 9 April 2009 | 13 March 2014 |
| | 14 June 2012 | |
| | 14 February 2013 | |

FUNCTION DELEGATED:

The authority to approve access to tape recordings of Council meetings.

CONDITIONS/GUIDELINES:

- (1) Any request to access the tape must be given to the Chief Executive Officer on at least 6 office hours notice.
- (2) All transactions utilising this delegation are to be recorded in the Recording of Delegations Decision Register by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As contained in conditions.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Council Policy SES2 'Access to Tape Recordings of Council Meetings' refers.

DELEGATE:

Chief Executive Officer

DELEGATE/S AUTHORISED:

N/A

| | | |
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| POL | LARGE PUBLIC EVENTS – APPROVAL | APD74 |
|-----|--------------------------------|-------|

| | |
|----------------------------------|-------------------------------|
| POLICY CODE: | APD74 |
| DIRECTORATE: | Planning & Development |
| BUSINESS UNIT: | Development Sites |
| SERVICE UNIT: | Environmental Health Services |
| RESPONSIBLE OFFICER: | Manager, Environmental Health |
| FILE NO.: | 182/001 |
| DATE FIRST ADOPTED: | 13 June 2013 |
| DATE LAST REVIEWED: | 10 December 2015 |
| ATTACHMENTS: | N/A |
| DELEGATED AUTHORITY REF.: | APD58 |
| VERSION NO. | 4 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 22 August 2013 26 February 2015 26 November 2015 |
| OCM: | 13 June 2013 12 September 2013 12 March 2015 |

To be deleted

BACKGROUND:

The City of Cockburn is required to consider occasional applications to conduct large outdoor events within the District that may attract a large audience and could impact on the amenity of residents within the proximity of the proposed event.

PURPOSE:

To identify issues which allow the City to consider applications for large outdoor events within the District that have the potential to impact on the amenity of City residents.

POLICY:

- (1) The City of Cockburn will consider applications to conduct large public events within the District that have the potential to impact on the amenity of City residents.
- (2) Only applications made strictly in accordance with Council's conditions will be considered. Timeframes set in this delegation will be deemed to commence from the time of receipt of a complying application.

[1]



| | | |
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| POL | LARGE PUBLIC EVENTS – APPROVAL | APD74 |
|-----|--------------------------------|-------|

- (3) The Mayor and Ward Councillors from the ward in which the proposed venue is located shall be advised, by e-mail, within 2 working days of the receipt of an application to conduct such an event.
- (4) All other Elected Members shall be advised of the receipt of an application via the next available Elected Members newsletter.
- (5) If requested by any Elected Member the application shall be referred to the next appropriate Council meeting for consideration.
- (6) Where the delegate considers it to be necessary or at the request of any Elected Member, residents likely to be affected by the event shall be advised in writing of the nature of the event and be given the opportunity to comment on it. The delegate shall make the determination of which residents are "affected residents" in consultation with the Ward Councillors.
- (7) Where practicable the affected residents' comments shall be addressed through conditions of approval.
- (8) The delegate shall ensure that all affected residents are advised as to the outcome of consideration of the application.

To be deleted

[2]



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| POS | PUBLIC BUILDINGS | PSPD11 |
|-----|------------------|--------|

| | |
|---------------------------------|--------------------------------------|
| POSITION STATEMENT CODE: | PSPD11 |
| DIRECTORATE: | Planning and Development |
| BUSINESS UNIT: | Development Services |
| SERVICE UNIT: | Building & Health Services |
| RESPONSIBLE OFFICER: | Managers, Building & Health Services |
| FILE NO.: | 182/002 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 14 December 2017 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 5 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|-------------------|------------------|
| DAPPS Meeting: | 31 January 2013 | 26 November 2015 |
| | 22 August 2013 | 23 November 2017 |
| | 26 February 2015 | |
| OCM: | 14 February 2013 | 12 March 2015 |
| | 12 September 2013 | 10 December 2015 |

BACKGROUND:

The Public Buildings Regulations (1992) require that application is made and approvals sought from the local authority for public buildings

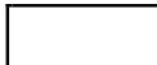
PURPOSE:

To clarify administrative responsibilities and requirements for the submission, assessment and approval of applications for public buildings within the City.

POSITION STATEMENT:

- (1) When an application for a Public Building is received, Building Services take responsibility for:-
- co-ordination and processing of the building application.
 - issue of the building permit.
 - obtaining necessary certifications such as structural engineering, electrical and fire brigades.
 - the issue of the Occupancy Permit.
- Health Services is responsible for:-
- ensuring compliance of plans with Public Building Regulations.
 - measurement of public areas.

[1]



| | | |
|-----|------------------|--------|
| POS | PUBLIC BUILDINGS | PSPD11 |
|-----|------------------|--------|

- assessment of permissible occupancy numbers..
 - issue of final Certificate of Approval.
 - on-going assessment of public building area safety and amenity.
- (2) Health and Building Services are to be advised of any applications received for use of temporary structures such as Circus', large outdoor concert events etc so that compliance with the Public Building Regulations can be achieved, and where a Certificate from a Qualified Structural Engineer certifying that the temporary structure, inclusive of the seating and staging, is safe.

To be deleted

[2]

DAP 27/08/2020

Item 8.3

8.3 (2020/MINUTE NO 0017) ANNUAL REVIEW OF DELEGATED AUTHORITIES MADE UNDER ACTS EXTRANEIOUS TO THE LOCAL GOVERNMENT ACT 1995

- Author(s)** B Pinto
- Attachments**
1. Proposed Amendment - OLCS2 'Bush Fires Act 1954 - Prohibited and Restricted Burning Period'
 2. Proposed Amendment - OLCS3 'Bush Fires Act - Legal Proceedings'
 3. Proposed Amendment - OLEW1 'Graffiti Vandalism Act 2016 - Administration and Enforcement'

RECOMMENDATION
That Council:

- (1) adopt proposed amendments to Delegated Authorities made under Acts extraneous to the *Local Government Act 1995*, as shown in the attachments to the Agenda;
- (2) adopt Delegated Authorities Extraneous to the *Local Government Act 1995*, that have no changes as listed in the report; and
- (3) update the Delegated Authority Register accordingly.

COMMITTEE RECOMMENDATION
MOVED Mayor L Howlett SECONDED Cr C Terblanche
That the recommendation be adopted.

CARRIED 5/0

Background

Section 5.46 of the *Local Government Act 1995* requires local governments to keep and maintain a Register of Delegations. Governance Services has coordinated a full review of the Delegations made under Acts extraneous to the *Local Government Act 1995*.

The review of the Delegations made under Acts extraneous to the *Local Government Act 1995* is now complete. The DAPS Committee is now required to consider and recommend adoption of the proposed amendments to Council.

Submission

N/A



Item 8.3

DAP 27/08/2020

Report

In accordance with s5.46 of the *Local Government Act 1995* the City currently maintains a Register of Delegations.

Section 5.46(2) of the Act requires a complete review of the Register of Delegations to be conducted at least once every financial year. While the mandatory review does not extend to those Delegations made under Acts extraneous to the *Local Government Act 1995*, it has been done to conform to the principle of good governance.

In consultation with the relevant officers, a comprehensive review of Delegations made under other Acts has been undertaken. Very few changes were recommended to these delegations, indicating they are still in line with their original intent and objectives.

Listed in the table below are the proposed amended delegations for the consideration of Council.

| Delegation | Proposed Amendment | Reason for Amendment |
|---|---|---|
| DA - OLCS2 'Bush Fires Act 1954 – Prohibited and Restricted Burning Period' | Conditions / Guidelines updated | To avoid confusion as to which Deputy Fire Control Officer the Policy refers to. |
| DA - OLCS3 'Bush Fires Act 1954 – Legal Proceedings' | List of Delegates updated | New position and structure change. |
| DA – OLEW1 'Graffiti Vandalism Act 2016 – Administration and Enforcement' | Conditions / Guidelines and Sub-Delegates updated | References to Directorate/Business Unit and Service Unit have been updated to accurately reflect the area of responsibility in relation to the delegation. Delegation reference amended accordingly. Sub-delegation provided to use section 18 of the Act to issue notices for graffiti. This will allow for improved removal of graffiti from private land if issued with a notice. |

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Item 8.3

The Instruments of Delegation listed below have been reviewed by staff and no changes are required, as they are considered appropriate for Council to adopt in their present form.

| | |
|--------|---|
| OLPD17 | City of Cockburn - Town Planning Scheme No 3 - Development Contributions |
| OLPD20 | <i>Building Act 2011</i> - Approve or Refuse Building Permit |
| OLPD21 | <i>Building Act 2011</i> - Approve or Refuse a Demolition Permit |
| OLPD22 | <i>Building Act 2011</i> - Approve or Refuse an Extension of Time for Building and Demolition Permits |
| OLPD23 | <i>Building Act 2011</i> - Issue an Occupancy Permit or Building Approval Certificate |
| OLPD24 | <i>Building Act 2011</i> - Make an Order for Building or Demolition Work |
| OLPD25 | <i>Building Act 2011</i> - Revoke Order for Building or Demolition Work |
| OLPD26 | <i>Building Act 2011</i> - Approve or Refuse an Extension of Time for an Occupancy Permit and Building Approval Certificate |
| OLPD27 | <i>Building Act 2011</i> - Appoint Authorised Persons |
| OLPD28 | <i>Building Act 2011</i> - Legal Proceedings |
| OLPD29 | <i>Food Act 2008</i> – Prosecutions |
| OLPD30 | <i>Food Act 2008</i> - Prohibition Orders |
| OLPD31 | <i>Food Act 2008</i> - Registration of Food Business |
| OLPD32 | <i>Food Act 2008</i> - Appointment of Authorised Persons and Designated Officers |
| OLPD33 | Town Planning Scheme No.3 - Development Control |
| OLPD34 | <i>Public Health Act 2016</i> - Appointment of Authorised Officer |
| OLCS1 | <i>Bush Fires Act 1954</i> - Abatement of a Fire Danger |
| OLCS5 | <i>Bush Fires Act 1954</i> - Powers and Duties |
| OLCS14 | <i>Cat Act 2011</i> - Administration and Enforcement |
| OLCS15 | <i>Dog Act 1976</i> - Administration and Enforcement |

Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

N/A

Legal Implications

Sec.5.46 (2) of the *Local Government Act 1995* refers.

Community Consultation

N/A



Item 8.3

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Risk Management Implications

Failure to adopt the recommendation may result in inconsistent Instruments of Delegation that do not reflect current practices and positions, thus not adhering to the principles of good governance.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



| | | |
|----|--|--------------|
| DA | BUSH FIRES ACT 1954 –PROHIBITED AND RESTRICTED BURNING PERIOD | OLCS2 |
|----|--|--------------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | OLCS2 |
| DIRECTORATE: | Executive Support Department Governance & Community Services |
| BUSINESS UNIT: | Executive Services Recreation & Community Safety |
| SERVICE UNIT: | Executive Services Rangers & Community Safety |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 31 May 2007 | 2 June 2015 |
| | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | | 23 May 2019 |
| OCM: | 14 June 2007 | 11 June 2015 |
| | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |
| | | |

FUNCTION DELEGATED:

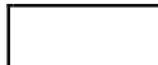
To vary prohibited and restricted burning times, in accordance with s17(7) and (8) and s18(5), regarding:

- shortening, extending, suspending or reimposing a period of prohibited or restricted burning times; or
- imposing a further period of prohibited or restricted burning times.

CONDITIONS/GUIDELINES:

- (1) The Mayor and Chief Bush Fire Control Officer shall jointly sign a memorandum prepared by the Chief Bush Fire Control Officer giving effect to the extension of the Prohibited Burning Period and such memorandum shall not extend the Prohibited Burning Period for a period greater than fourteen(14) days at any one time. A notice giving effect to the extension of the Prohibited Burning Period shall be published in a newspaper circulating in the district and a copy of the notice shall be distributed to:

[1]



DAP 27/08/2020

Item 8.3 Attachment 1

| | | |
|----|---|-------|
| DA | BUSH FIRES ACT 1954 –PROHIBITED AND RESTRICTED BURNING PERIOD | OLCS2 |
|----|---|-------|

| | |
|--|--|
| Mayor | Chief Executive Officer |
| Chief Bush Fire Control Officer | Deputy Chief Bush Fire Control Officer |
| Rangers | Jandakot Volunteer Bush Fire Brigade |
| South Coogee Volunteer Bush Fire Brigade | City of Fremantle |
| City of Armadale | City of Melville |
| City of Gosnells Shire of Serpentine Jarrahdale | City of Joondalup |
| City of Rockingham | City of Kalamunda |
| City of Wanneroo | Shire of Gingin |
| Shire of Chittering | Shire of Swan |
| Shire of Mundaring | Department of Fire & Emergency Services of WA |
| City of Kwinana | Department of Biodiversity, Conservation & Attractions |
| Rottneest Island Authority | City of Canning |

- (2) In the absence of the Mayor the Deputy Mayor becomes the Delegate in relation to signing of declarations as prescribed by the Bush Fires Act 1954, and in the absence of the Chief Bush Fire Control Officer the Deputy Chief Bush Fire Control Officer ([Rangers and Community Safety Services Manager](#)) becomes the Delegate.
- (3) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegations Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

As provided under Legislative requirements and Conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Pursuant to the provisions of Section 17(10) of the Bush Fires Act 1954, the powers and duties of Section 17(7) and (8) of the aforesaid Act

DELEGATE:

Mayor
Chief Bush Fire Control Officer

SUBDELEGATE/S:

Nil
(No statutory power provided to sub-delegate [s48(3)].)

[2]

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| | | |
|----|---|-------|
| DA | BUSH FIRES ACT 1954 – LEGAL PROCEEDINGS | OLCS3 |
|----|---|-------|

| | |
|---------------------------|---|
| DELEGATED AUTHORITY CODE: | OLCS3 |
| DIRECTORATE: | Executive Support Department |
| BUSINESS UNIT: | Executive Services |
| SERVICE UNIT: | Executive Services |
| RESPONSIBLE OFFICER: | Director, Governance & Community Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 1997 |
| DATE LAST REVIEWED: | 13 June 2019 |
| ATTACHMENTS: | N/A |
| VERSION NO. | 9 |

| Dates of Amendments / Reviews: | | |
|--------------------------------|--------------|--------------|
| DAPPS Meeting: | 24 May 2012 | 26 May 2016 |
| | 23 May 2013 | 18 May 2017 |
| | 22 May 2014 | 24 May 2018 |
| | 2 June 2015 | 23 May 2019 |
| | 14 June 2007 | 11 June 2015 |
| OCM: | 14 June 2012 | 9 June 2016 |
| | 13 June 2013 | 8 June 2017 |
| | 12 June 2014 | 14 June 2018 |

FUNCTION DELEGATED:

That by virtue of a written instrument of delegation the delegates herein mentioned have the expressed authority to issue infringement notices and institute legal proceedings on behalf of the City of Cockburn pursuant to section 59(3) of the Bush Fires Act 1954.

CONDITIONS/GUIDELINES:

All delegates have the individual carriage and conduct of matters initiated.

- (1) Consider offences alleged to have been committed against this Act in the district of the local government and, if the delegate thinks fit, to institute and carry on proceedings in the name of the local government against any person alleged to have committed any of those offences in the district.
- (2) All transactions utilising this delegation are to be recorded ~~in the Recording of Delegations Decisions Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

Bush Fires Act 1954,
 s59(3) Prosecution of Offences
 s59A(2) Alternative Procedure - Infringement Notices

[1]



DAP 27/08/2020

Item 8.3 Attachment 2

| | | |
|----|---|-------|
| DA | BUSH FIRES ACT 1954 – LEGAL PROCEEDINGS | OLCS3 |
|----|---|-------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:*Bush Fires Act 1954**s59(3) (Delegation by the local government)***DELEGATE:**

Director, Governance & Community Services – institute legal proceedings only

Manager, Recreation & Community Safety - institute legal proceedings only

Chief Bush Fire Control Officer

Ranger and Community Safety Services Manager - institute legal proceedings only

[Emergency Management Co-ordinator](#)[Fire and Emergency Management Officer](#)

Senior Ranger

Ranger

[CoSafe Team Leader](#)**SUB-DELEGATE/S:**

Nil

(No statutory power provided to sub-delegate [s48(3)].)

[2]

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| | | |
|----|--|--------------|
| DA | GRAFFITI VANDALISM ACT 2016- ADMINISTRATION & ENFORCEMENT | OLEW4 |
|----|--|--------------|

| | |
|----------------------------------|--|
| DELEGATED AUTHORITY CODE: | OLEW4 |
| DIRECTORATE: | Engineering & Works Governance & Community Services |
| BUSINESS UNIT: | Engineering & Works Recreation & Community Safety |
| SERVICE UNIT: | Roads Services Ranger & Community Safety |
| RESPONSIBLE OFFICER: | Manager-Engineering Manager, Recreation & Community Safety |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 8 December 2016 |
| DATE LAST REVIEWED: | 13 June 2019 |
| POLICY REF.: | N/A |
| VERSION NO. | 4 |

| | |
|---------------------------------------|---|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | 24 November 2016 18 May 2017 24 May 2018 23 May 2019 |
| OCM: | 8 December 2016 8 June 2017 14 June 2018 |

FUNCTION DELEGATED:

1. Exercise of any powers or the discharge of any of its duties under the Graffiti Vandalism Act 2016.
2. Appointment of authorised persons.

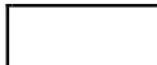
CONDITIONS/GUIDELINES:

1. A power under Section 17 to delegate the exercise of this power or the discharge of this duty ~~is sub-delegated to those positions listed within this authority. is not delegated~~
2. All transactions utilising this delegation are to be recorded ~~in the Recording of Delegations Register~~ by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

AUTONOMY OF DISCRETION:

Section 16 of the Graffiti Vandalism Act 2016 and conditions above.

[1]



DAP 27/08/2020

Item 8.3 Attachment 3

| | | |
|----|--|-------|
| DA | GRAFFITI VANDALISM ACT 2016- ADMINISTRATION & ENFORCEMENT | OLEW4 |
|----|--|-------|

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act 1995.
Local Government (Functions and General) Regulations 1996.
Graffiti Vandalism Act 2016.

DELEGATE:

Chief Executive Officer

SUB DELEGATE/S:

[Nil](#)
[Manager, Recreation and Community Safety](#)
[Rangers and Community Safety Services Manager](#)

[2]

DAP 27/08/2020

Item 9.1

9. PLANNING AND DEVELOPMENT DIVISION ISSUES

9.1 (2020/MINUTE NO 0018) ADOPTION FOR FINAL APPROVAL: PROPOSED AMENDMENTS TO LOCAL PLANNING POLICY 5.6 - VEHICLE ACCESS

- Author(s)** K Knuckey
Attachments 1. Local Planning Policy 5.6 - Vehicle Access (with tracked amendments)
 2. Schedule of Submissions

RECOMMENDATION
 That Council:

- (1) adopts the proposed amendments to Local Planning Policy 5.6 - Vehicle Access, as included at Attachment 1, in accordance with Clause 5 of the Deemed Provisions for Local Planning Schemes pursuant to the *Planning and Development (Local Planning Schemes) Regulations 2015*;
- (2) in accordance with Clause 5 of the Deemed Provisions for Local Planning Schemes pursuant to the *Planning and Development (Local Planning Schemes) Regulations 2015*, publish a notice of the policy in a newspaper circulating in the Scheme area; and
- (3) notify submitters of Council's decision.

COMMITTEE RECOMMENDATION
 MOVED Mayor L Howlett SECONDED Cr C Terblanche
 That the recommendation be adopted.

CARRIED 5/0

Background

Local Planning Policy 5.6 'Vehicle Access' (LPP 5.6) was adopted by Council for the purposes of readvertising in accordance with Clause 4 (1) of the *Planning and Development (Local Planning Schemes) Regulations 2015* at its meeting held on 11 June 2020.

The purpose of LPP5.6 is to provide a framework for the planning and development of safe and efficient movement of motorists, heavy vehicle operators, public transport users, pedestrians and cyclists. It does this by identifying 'Vehicle Access Plans', which identify vehicle access arrangements, including crossover and public access easement details. Two vehicle access plans are currently included within the policy – one for North Lake Road (between the Kwinana Freeway and Semple Court) and the other for Verde Drive (between the Kwinana Freeway and the intersection of Armadale Road and Tapper Road).



Item 9.1

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This amendment proposes changes to both plans currently contained in the Policy at Appendix 1 and 2, as well as some additional changes to the local planning policy text.

Submission

N/A

Report

The purpose of this report is for consideration to be given to adopting amendments to Local Planning Policy 5.6 'Vehicle Access' for final approval, in light of the outcomes of community consultation.

The draft amended policy is included at Attachment 1, and the 28 May 2020 Delegated Authorities and Policies Committee meeting report (Item 9.1) provides a comprehensive overview of the proposed changes and what they are seeking to achieve.

Overall, the amendments proposed are considered to have very little (if any) impact on any existing developments. The proposed amendments have generally resulted from the following:

- Responding to design needs of the duplication of North Lake Road the emerging Cockburn Central West precinct and the ARNLRB project.
- Responding to the realignment of Verde Drive as a result of the ARNLRB project, which now sees the western end of Verde Drive connect with Armadale Road instead of the North Lake Road Bridge.

Based on the feedback received during the community consultation period, two further amendments are proposed to the Verde Drive Vehicle Access Plan:

1. Modifying the Biscayne Way/Verde Drive intersection from the advertised left-in/left-out intersection to a right-in/left-in/left-out (with medium break) intersection.
2. Modifying the plan to provide a crossover north of the Prinsep Road and Verde Drive roundabout, in order to provide a necessary crossover option for the rear portion of lots at 33 and 39 Cutler Road should they be subdivided.

These two modifications have been included in the final proposed LPP5.6, included at Attachment 1.

Community Consultation

The policy was advertised from 25 June 2020 to 24 July 2020, during which time nine submissions were received, three of which noted objection to the proposed amendments to LPP5.6.



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Item 9.1

Submissions raised the following matters:

- The incorrect illustration of the Biscayne Way/Verde Drive intersection on the Verde Drive Vehicle Access Plan.
- The relevance of LPP5.6 now that the ARNLRB project plans have been finalised.
- The potential future extension of Monash Gate as indicated on the Verde Drive Vehicle Access Plan.
- Number of crossovers north of Verde Drive between Prinsep Road and Chifley Brace.
- Various easement in gross access crossover locations indicated on the two existing vehicle access plans.

Biscayne Way/Verde Drive Intersection

The advertised version of the proposed amendments to LPP5.6 and the accompanying Verde Drive Vehicle Access Plan illustrated the Biscayne Way/Verde Drive intersection as a left-in/left-out intersection on the Verde Drive Vehicle Access Plan.

This was an unfortunate administrative error which does not reflect the existing, recently constructed, intersection with the ability to turn right into Biscayne Way.

This matter was raised early during the consultation period and affected landowners/occupiers were notified by letter of the error.

It is recommended the advertised Verde Drive Vehicle Access Plan be modified to reflect the existing recently constructed intersection, a right-in/left-in/left-out (with medium break) intersection.

Relevance of LPP5.6 given ARNLRB Project Plans Finalisation

The Verde Drive Vehicle Access Plan and the North Lake Road Vehicle Access Plan are two separate vehicle access plans that have been established to coordinate access and reduce the number of crossovers in these localities. These vehicle access plans are still relevant, and the finalisations of ARNLRB project plans have not addressed the matters that these vehicle access plans propose.

Potential Future Extension of Monash Gate

One submission raised concern with the 'potential future road' indicated on the Verde Drive Vehicle Access Plan for the potential future extension of Monash Gate. This is simply an indicative layout, utilising the most direct alignment. Further extensive planning and consultation is required to define a precise location for the Monash Gate extension. The indicative Monash Gate extension is not formally binding and simply proposes an opportunity to create a more direct road connection as lots in this precinct develop in the future.

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Figure 3: Excerpt from Verde Drive Vehicle Access Plan (with proposed amendments as advertised) illustrating the Monash Drive 'potential future road opportunity subject to subdivision realignment/amalgamation by landowners'.

Various Easement in Gross Access Crossover Locations

One submission raised concern to a number of access crossover locations and easement in gross locations illustrated on the Verde Drive Vehicle Access Plan; the easement in gross located over Lot 26 Verde Drive, the access crossover located on the common boundary of Lots 25 and 24 Verde Drive, and the access crossovers available north of Verde Drive between Prinsep Road and Chifley Brace.

In regards to the easement in gross location over Lot 26 Verde Drive; this will not result in a traffic hazard as stated in the submission because the Monash Gate extension does not yet exist. If the Monash Gate extension was to be considered in the future, the easement in gross over Lot 26 would be considered in this process, particularly in regards to traffic hazards. The easement in gross over Lot 26 provides this lot with a clear route of access, in keeping with the objectives of LPP5.6.

In regards to relocating the access crossover located on the common boundary of Lots 25 and 24 Verde Drive; this location is only illustrated as 'indicative' on the Verde Drive VAP and will be given further consideration as the lots develop in the future and the VAP is reviewed.

Specifically, Clause 4(a) and Clause 4(d) of LPP5.6 are included to address some of the concern raised by the submitter:

- a) *The precise location of rights of carriageways or crossovers delineated on a Vehicle Access Policy Plan may be varied subject to the agreement of the City of Cockburn, provided that the purpose and the intent of the Vehicle Access Policy Plan is maintained.*
- d) *With specific regard to the Verde Drive Access Plan, consideration will be given to the exact location of crossovers and need for an easement (West of Solomon Road) as development occurs. This recognises the likely need to reconfigure long narrow lots and the likely timing of various developments will likely be staged over time. The City at such time(s) will ensure the staging of development in this area is provided with suitable access arrangements, and will use discretion by having due regard to the objectives of this policy and the intent of the Plan illustrated at Appendix 2.*

Number of Crossovers to Easement in Gross between Prinsep Road and Chifley Brace

In regards to Lot 71 and the matter of points of access to the easement in gross north of Verde Drive, this reflects the Cockburn Central East Structure Plan (CCESP). The CCESP is a due regard document that indicatively showed the easement in gross, potential cross overs, all of which require confirmation and detailed consideration within a subsequent LPP. The further detailed considerations have identified 3 crossovers. Noted is in the advertising of the LPP and the Verde Drive plan the cross over now shown north of the Prinsep Road and Verde Drive roundabout was removed in an administrative error. This has now been added in the final policy version. This will provide a necessary cross over option for the rear portion of lots at 33 and 39 Cutler Road should they be subdivided. There are no anticipated negative impacts on landowners as a result of this inclusion.

It is again noted that the LPP includes a variation clause of which provides the City and future applications with the ability to consider cross over needs as lots evolve. Noting that cross overs may be required in different locations to give access to certain lots that develop ahead of time as anticipated by the LPP. Put simply, if the landowner of Lot 71 developed tomorrow and an alternative easement in gross connection was not yet in place nor was a crossover as per the policy on the northern side of Verde Drive then the City would grant a crossover for Lot 71. The City would then reconsider the subsequent locations as a result – as long as the strategic intent of the LPP was met.

All submissions have been addressed in detail in the Schedule of Submissions (refer Attachment 1).

Given that the proposed amendments are generally minor in nature, align with the finalised Armadale Road North Lake Road Bridge (ARNLRB) Project Plans, and are generally supported by those affected, it is recommended that the amendments to LPP5.6 – Vehicle Access be adopted, subject to the modifications to the Verde Drive Vehicle Access Plan outlined above.

Finally, it is noted the policy document has been updated to the revised local planning policy layout adopted by the City.



Strategic Plans/Policy ImplicationsCity Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Advocate and plan for reduced traffic congestion.
- Continue to complete the coverage of accessible cycle ways, footpaths, parking and end of trip facilities, and trails networks across the City.

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

The policy was advertised from 25 June 2020 to 24 July 2020 in accordance with Clause 4(1) of the *Planning and Development (Local Planning Schemes) Regulations 2015*. During this time, nine submissions were received, three of which noted objection to the proposed amendments to LPP5.6.

Stakeholder consultation included the following:

- Advertisements in the Cockburn Gazette and 'Comment on Cockburn';
- display of the proposed amendments to LPP5.6 in the administration building; and
- Consultation with landowners and occupiers within the localities surrounding the vehicle access plans.

All submissions have been addressed in detail in the Schedule of Submissions (refer Attachment 1).

The policy amendments were originally advertised in 2018. However, the City has held off progressing the amendments to LPP 5.6, until the Armadale Road and North Lake Road Bridge (ARNLRB) project plans have been finalised. This includes understanding the design implications of Verde Drive west of Solomon Road. Furthermore, the City has been working with affected landowners and Main Roads WA (MRWA) to determine appropriate easement outcomes which have now been agreed upon.



Risk Management Implications

Given the policy was advertised in 2018, but that the consultation process was not closed out, there could be a perception with those who previously lodged a submission that their concerns were not taken on board. This is not the case, with some of the modifications reflecting those earlier submissions.

Not supporting the modification will also result in unclear guidance for developers and landowners in the vicinity of North Lake Road and Verde Drive, and could lead to financial and legal implications for the City as a result of providing inaccurate information.

Advice to Proponent(s)/Submitters

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 27 August 2020 Delegated Authorities and Policies Committee.

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



| | |
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| Title | Vehicle Access |
| Policy Number <small>(Governance Purpose)</small> | LPP 5.6 |



Policy Type

Local Planning Policy

Policy Purpose

When land adjacent to major/arterial/distributor/important roads is developed for more intensive uses the resulting additional traffic generated by such uses, can cause conflict, especially where pre-existing traffic -volumes are high. This can create dangerous and unattractive road environments.

In these situations, a coordinated approach to vehicle access is required to ensure that development does not introduce any undesirable impacts on the safe and efficient movement for motorists, heavy vehicles operators, public transport users, pedestrians and cyclists.

The purpose of the Policy provides a framework for the planning and development of safe and efficient movement of motorists, public transport users, pedestrians and cyclists, where such a coordinated approach to vehicle access is required.

The Policy will be used by the City of Cockburn to guide the assessment of applications for development, subdivision and Local Development Plans, where a Vehicle Access Plan has been prepared.

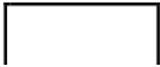
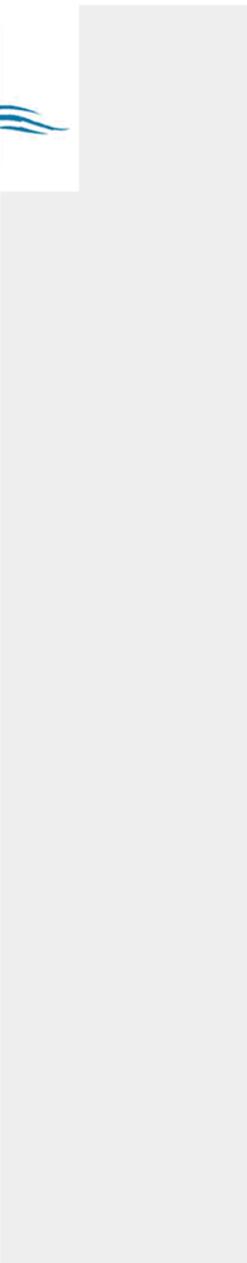
The overall objectives for the policy are to:

- (1) Provide for safe and efficient movement of motorists, public transport users, pedestrians and cyclists;
- (2) Provide for safe and efficient movement of waste management and other service vehicles;
- (3) Minimise the potential for conflict between through and local traffic;
- (4) Provide visually attractive road environments; and
- (5) Provide for reasonable property access that is direct, convenient and safe.

Policy Statement

- (1) Vehicle Access Policy Plans:
 1. Vehicle Access Policy Plans are to be prepared by the City of Cockburn in consultation with relevant stakeholders, which may include Main Roads WA, the Department of Planning and landowners of affected properties.
 2. Vehicle Access Policy Plans are to be adopted by Council for inclusion as an Appendix to the Vehicle Access Policy.

[1]



| | |
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| Title | Vehicle Access |
| Policy Number <small>(Governance Purpose)</small> | LPP 5.6 |



3. The access arrangements in Vehicle Access Policy Plans shall be constructed to public road standards and should consider the following design features to ensure traffic safety and efficiency:
 - a) road reserve widths;
 - b) intersection location and spacing;
 - c) intersection type;
 - d) traffic control type (i.e traffic signals, roundabout, signage);
 - e) median location and breaks;
 - f) the number, location and design of vehicle access points to adjoining private properties;
 - g) the size and length of vehicles that will be using the access;
 - h) the volume of traffic that will be using the access;
 - i) road lighting;
 - j) mechanisms for organising and securing shared use of crossovers;
 - k) servicing of properties by waste management and other commercial vehicles; and
 - l) provision of cyclist and pedestrian facilities and integration with the adjacent path network.

4. Vehicle Access Policy Plans are to consider pedestrian and cyclist movement patterns and key desire lines, particularly where residential development exists or is planned in proximity to the Vehicle Access Policy Plans area.

5. Vehicle Access Policy Plans are to be prepared with reference to the relevant local / state / national policies, guidelines and Australian Standards as determined appropriate by the Local Government.

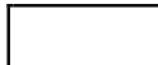
(2) Vehicle Access Controls:

1. Crossovers

- a) Control over the location, design and number of crossovers will be exercised by the responsible authority to reflect the arrangements shown on a Vehicle Access Policy Plan.
- b) There will be a presumption against the creation of new vehicle crossovers across the primary frontage of the lot where alternative access is, or could be made available, to secondary roads and laneways, or via easements in gross arrangements to a nominated crossover as shown on the Vehicle Access Policy Plan.
- c) Where a vehicle crossover is permitted under a Vehicle Access Policy Plan, conditions may be imposed on the width and design of the crossover to ensure adequate visibility and to provide for the safe and convenient movement of vehicles entering and leaving the road.

2. Easement in Gross

[2]



| | |
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| Title | Vehicle Access |
| Policy Number (Governance Purpose) | LPP 5.6 |



- a) Where indicated on a Vehicle Access Policy Plan, the City of Cockburn will require as a condition of development or subdivision, easements in gross in the form of a public access easement on land titles. The easements in gross are granted for the City of Cockburn to maintain public access across the subject land to the side streets and crossover access points as delineated on the Vehicle Access Policy Plan. These easements in gross on land titles are to be provided at the cost of the developer or landowners of the subject land. The treatment of the area covered by the easement in gross shall be designed and treated to the satisfaction of the City to ensure consistency across lots.
 - b) In determining development applications where a Vehicle Access Policy Plan requires the provision of a right-of-carriageway and where such access is not available through adjacent properties to a dedicated road, consideration may be given for retention of a temporary access crossover until such time as alternative access is available via the right-of-carriageway system.
 - c) Provisions for the closure of the temporary crossover shall be provided by way of legal agreement.
3. Parking/Circulation
- a) All applications for development approval on land subject to a Vehicle Access Policy Plan will be required to provide for traffic to enter and leave the site in a forward direction and to comply with the City of Cockburn's car parking and on-site vehicle circulation requirements, as detailed in the City's Town Planning Scheme No. 3 (TPS 3). This requirement is intended to ensure that there is no traffic congestion on-site which would interfere with the safety and free flow of traffic along the primary road.
4. Variation Clause
- a) The precise location of rights of carriageways or crossovers delineated on a Vehicle Access Policy Plan may be varied subject to the agreement of the City of Cockburn provided that the purpose and the intent of the Vehicle Access Policy Plan is maintained.
 - b) A detailed traffic assessment may be required in the form of a traffic management plan, a traffic impact assessment, a road safety audit, and/or traffic modelling may be required where a variation to a Vehicle Access Policy Plan is proposed. The form and scope of the required traffic assessment will be subject to guidance and agreement by the City of Cockburn.
 - c) This policy document may be updated with agreement from the City of Cockburn.

[3]

Item 9.1 Attachment 1

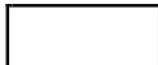
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d) With specific regard to the Verde Drive Access Plan consideration will be given to the exact location of crossovers and need for an easement (West of Solomon Road) as development occurs. This recognises the likely need to reconfigure long narrow lots and the likely timing of various developments will likely be staged over time. The City at such time(s) will ensure the staging of development in this area is provided with suitable access arrangements and will use discretion by having due regard to the objectives of this policy and the intent of the Plan illustrated at Appendix 2.

[4]



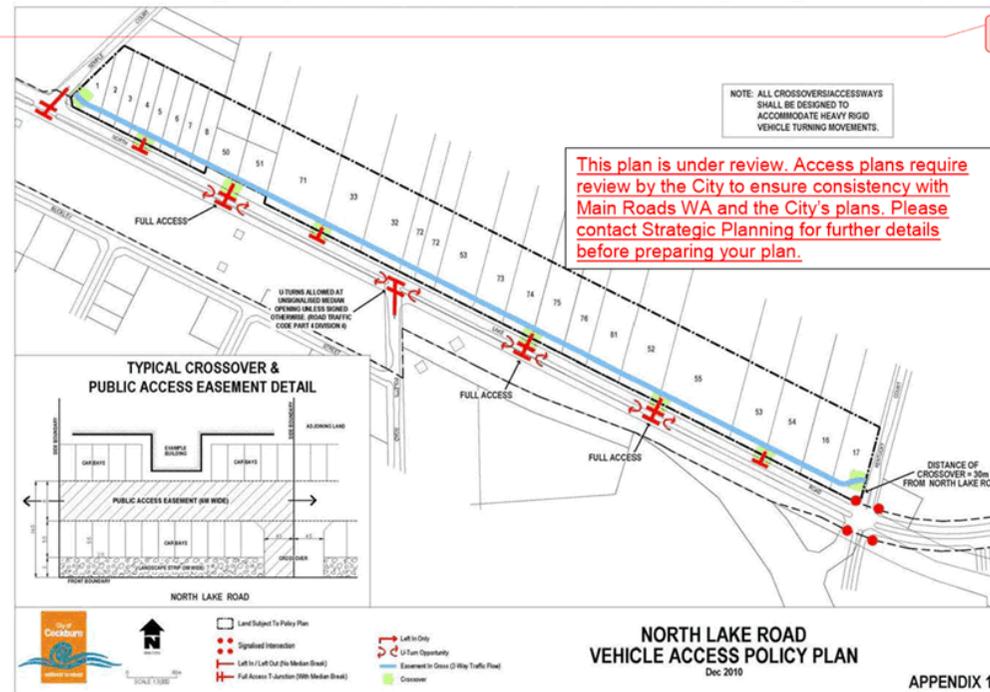
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Item 9.1 Attachment 1

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| Title | Vehicle Access |
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Comment [KK1]: Delete map and replace with map on page 7.

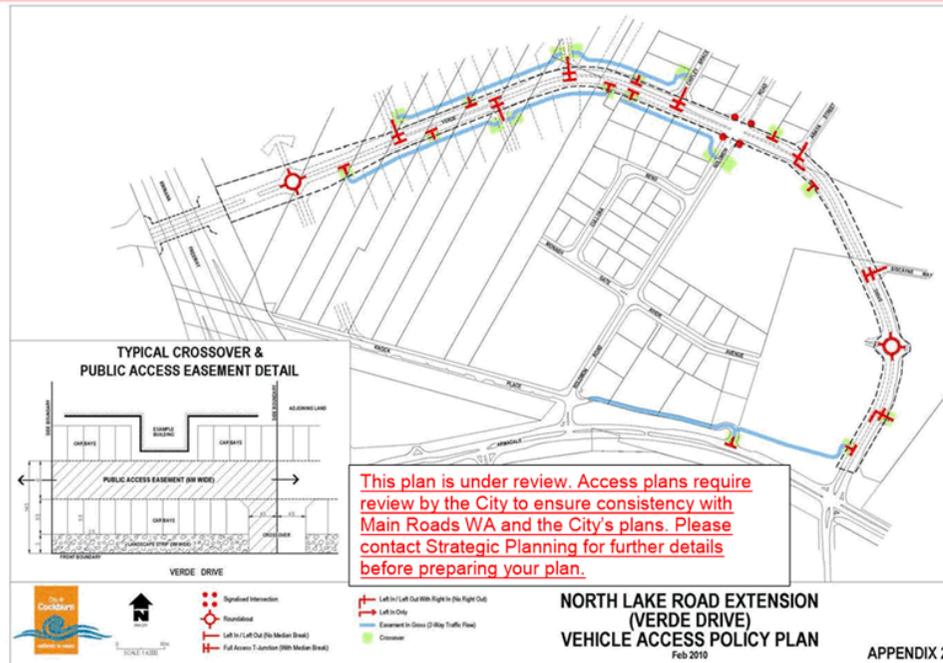


[5]

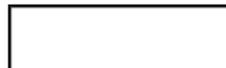
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| Title | Vehicle Access |
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Comment [KK2]: Delete map and replace with map on page 8.



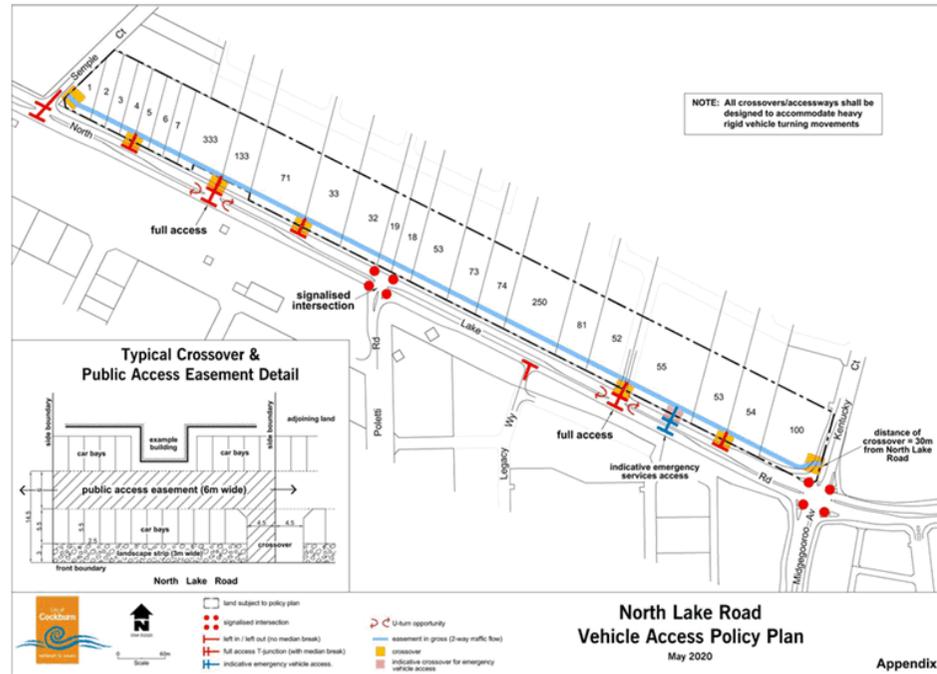
[6]



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Item 9.1 Attachment 1

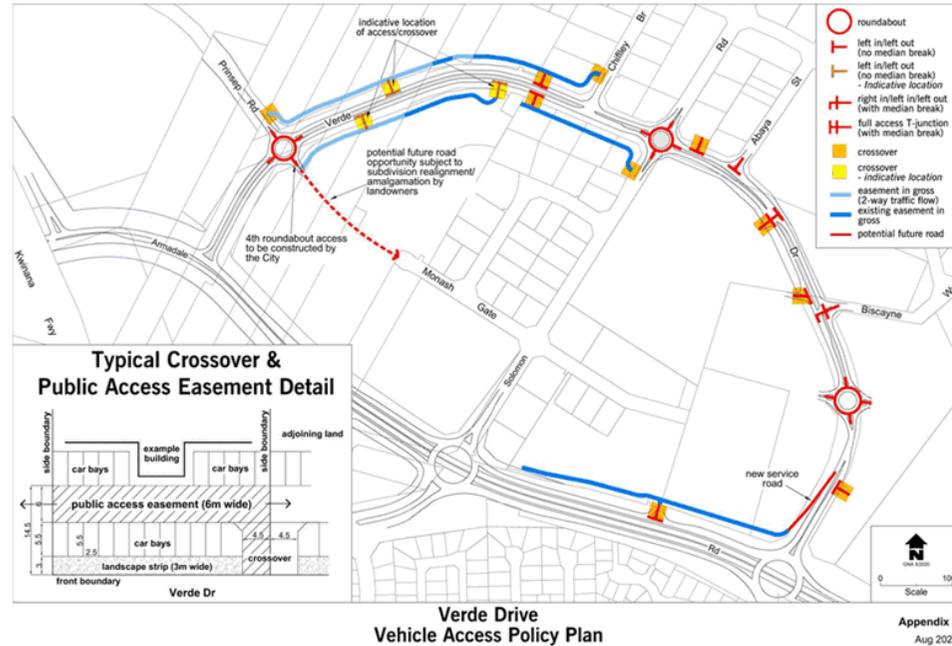
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[7]



| | |
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| Title | Vehicle Access |
| Policy Number (Governance Purpose) | LPP 5.6 |



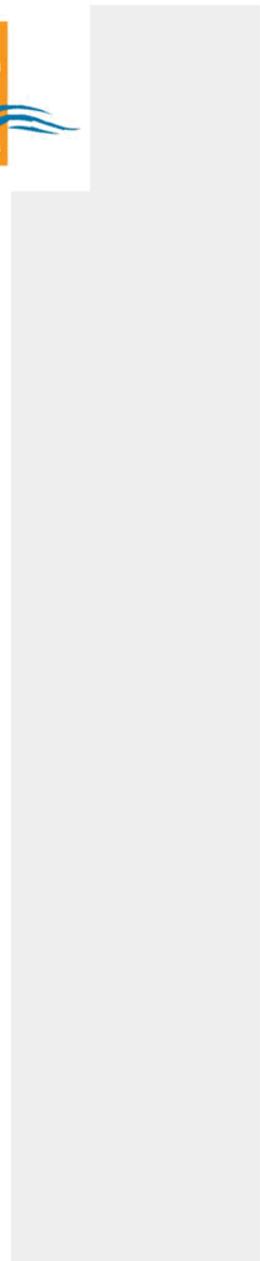
[8]



| | |
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| Title | Vehicle Access |
| Policy Number (Governance Purpose) | LPP 5.6 |



| | |
|---|--|
| Strategic Link: | Town Planning Scheme No. 3 |
| Category | Planning - Town Planning & Development |
| Lead Business Unit: | Statutory Planning |
| Public Consultation: (Yes or No) | Yes |
| Adoption Date: (Governance Purpose Only) | 12 December 2019 |
| Next Review Due: (Governance Purpose Only) | December 2021 |
| ECM Doc Set ID: (Governance Purpose Only) | 4517299 |



[9]



Item 9.1 Attachment 2

DAP 27/08/2020

File No. 104/002

SCHEDULE OF SUBMISSIONS
Amendment to Local Planning Policy 5.6 – Vehicle Access

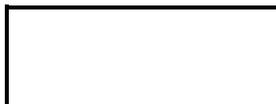
| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|--|--------------------|---|--|
| SERVICE PROVIDERS AND GOVERNMENT AGENCIES | | | |
| 1 | DFES Land Use Team | <p>COMMENTS:</p> <p>Given the proposal seeks to include additional modifications to LPP 5.6 relating to the North Lake Road Bridge, the application of <i>State of Planning Policy 3.7 Planning in Bushfire Prone Areas</i> (SPP 3.7) may not be required.</p> <p>I understand that DFES (operational teams) have previously been consulted during the formulation of the LPP 5.6. In addition, as the DFES Cockburn Central Fire Station is located on Lot 55 North Lake Road, I have copied in the DFES project manager for this facility for information.</p> <p>On this basis, DFES Land Use Planning has no further comment to make for the LPP 5.6.</p> | <p>Noted.</p> <p>Given that the proposed amendments are all relatively minor in nature, it is considered that the application of SPP 3.7 is not be required.</p> <p>DFES was consulted due to the proposed DFES Cockburn Central Fire Station to be located on the affected Lot 55 North Lake Road. The proposed amendments to the North Lake Road Vehicle Access Plan reflect the proposed fire station development.</p> <p>No further comments have been received from DFES.</p> |
| 2 | Water Corporation | <p>COMMENTS:</p> <p>Thank you for your letter dated 16 June 2020. We offer the following comments in regard to this proposal.</p> <p>The proposed changes to the policy do not appear to affect Water Corporation. If our assets are affected, the proponent may be required to fund new works or the upgrading of existing works and protection of all works.</p> | <p>Advice is noted.</p> |



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| | | <p>All existing and future water and sewerage services must be located within the existing and proposed road reserves, on the correct alignment and in accordance with the Utility Providers Code of Practice.</p> <p>Water Corporation easements are currently located in the subject area (Verde Drive) Please contact the Water Corporations Procurement and Property Branch regarding these easements.</p> <p>The information provided above is subject to review and may change. If the proposal has not proceeded within the next 12 months, please contact us to confirm that this information is still valid.</p> | |
| PRIVATE SUBMISSIONS | | | |
| 3 | John Brew – U2, 130 Cutler Road, Jandakot | <p>OBJECTION:</p> <p>We are a business at Unit 2, 130 Cutler Road Jandakot and are opposing the above amendment to close our vehicle access right turn from Verde Drive into Biscayne Way.</p> <p>There are many businesses in our area and we fought hard in 2018/2019 with the Council , Charles Sullivan & Yaz Mubarakai to have our access road (Biscayne Way) re-instated after the City of Cockburn was going to close Biscayne Way access.</p> <p>Our staff have experienced in the past 12-15 months since the roadworks were completed on Verde Drive is that the right hand turn into Biscayne Way is safe.</p> | <p>Objection noted.</p> <p>The specification of the Biscayne Way / Verde Drive intersection as a left-in / left-out intersection on the Verde Drive Vehicle Access Plan was an unfortunate administrative error which does not reflect the existing recently constructed intersection with the ability to turn right into Biscayne Way.</p> <p>This matter was raised early during the consultation period and affected landowners / occupiers were notified by letter of the error.</p> <p>It is recommended the advertised Verde Drive Vehicle Access Plan be modified to reflect the</p> |



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| | | No-one has experienced a right hand turning vehicle into Biscayne Way blocking the flow of traffic along Verde drive towards Armadale Road. | existing recently constructed intersection. |
| 4 | Michelle & Dean McKain Elite Salon Supplies 1/47 Biscayne Way Jandakot | <p>OBJECTION:</p> <p>I am writing to object to the removal of the right turn into Biscayne Way off Verde Drive. We had the Mayor out for a meeting on site and vehemently objected to this change in late 2018. At that time we were advised that the matter was dealt with as there was a huge objection from businesses in the area and lengthy discussions and meetings held in regards to this matter, we have since then been under the impression that the matter was settled. The right hand turn was replaced and we have used this without any traffic flow issues and safely since.</p> <p>We need this right hand turn into Biscayne way to allow easy access for our customers to get to our business. We purchased the premises with this in mind and if the right hand turn was not in place at the time of purchase we would not have proceeded.</p> <p>The right hand turn, as it is, is working perfectly and there is no safety or traffic issues from this access.</p> <p>Thank-you for your time with this matter, I hope to hear that this right hand turn will not be removed and we can continue business as usual.</p> | <p>Objection noted.</p> <p>See comments made addressing submission 3.</p> |
| 5 | Andy Graham Civil Contractors Federation WA | <p>COMMENTS:</p> <p>Access to our building at 70 Verde Dr Jandakot is via an</p> | <p>Comments noted.</p> <p>The Solomon Road access crossover was not</p> |



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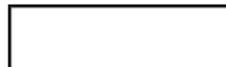
Item 9.1 Attachment 2

| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|-----|----------------------------|--|---|
| | 70 Verde Drive Jandakot | <p>'easement in gross' which has two entrances - on Solomon Rd and Verde Dr.</p> <p>With the new duplication of Verde Dr we will now be restricted to left in/left out access via the Verde Dr entrance. This will result in increased use of the Solomon Rd entry/exit. Entry and exit via Solomon Road is compromised by the sharply angled kerb profile used for that entrance. The contractor has tried to alleviate the issue by placing an asphalt 'ramp', which has helped, but the basic problem is the design of the kerb.</p> <p>Turning right into the easement or onto Solomon Rd is dangerous enough as it is immediately after a roundabout, but is made even worse by the design of the kerb as it has to be traversed very slowly, especially in normal passenger vehicles. I encourage you to try entering and exiting the easement and imagine having to do that every day.</p> <p>Adding to the danger is the presence of 4wds from the adjacent business, which uses the vacant land around the easement to park 10-15 vehicles on most days. The vehicles are generally parked off the actual easement though occasionally they are parked partially on the easement.</p> <p>As the easement is surrounded by sand, this sand is dug up by the 4WDs and thrown onto the easement, so that the easement is generally sandy and it can be hard to tell where the easements ends and the sand begins. The result is a very unprofessional-looking entrance for visitors to our building.</p> | <p>modified by the City as part of the Verde Drive upgrade. It appears that the kerb located at the Solomon Road access crossover is a mountable kerb and in line with the City's specification and requirements. According to the City's engineering team, asphalt was placed as a temporary measure and needs to be removed (see Figure 1 below).</p> <p>In regards to the easement in gross area being covered in sand due to surrounding undeveloped industrial lots; the easement in gross is located within private lots and therefore the burden of maintenance lies with the relevant land owners. It is recommended that businesses affected by this, contact the landowners to address the issue on private land.</p> |

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| | | <p>Can I request the council please:</p> <ul style="list-style-type: none"> • Modify the kerb on the easement entrance on Solomon Rd to make access safer. • Clean the easement regularly so it isn't always covered in sand. |  <p><i>Figure 1: A recent photo of the Solomon Road access crossover in question.</i></p> |
| 6 | <p>John Lee WCSH Co Pty Ltd Lot 24 & 25 Cutler Road Jandakot</p> | <p>COMMENTS:</p> <p>The objective of this framework was to allow for the safe and efficient movement of traffic, motorists and pedestrians along Verde Drive, constructed as the North Lake Road extension. This is not the case anymore, as the extension of Armadale Road has since been the connecting road to the North Lake Road by-pass.</p> <p>As a result of this, the need for the previous vehicle access policy design to continue is redundant. The traffic flow will be significantly less along Verde drive. The costs of</p> | <p>Comments noted and not supported</p> <p>The objective of LPP5.6 is to coordinate access and reduce the number of crossovers on high volume roads to reduce traffic conflict, accomplished by identifying 'Vehicle Access Policy Plans'.</p> <p>Regardless of the change in the road network, Verde Drive is still expected to carry high numbers of vehicles per day and as a result requires a plan to reduce the number of cross</p> |



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| | | <p>constructing such infrastructure may be far greater than any potential benefit that will arise from the access design. Land owners need the option to allow for freedom of design when planning projects, free from past planning framework associated with prior structure plans. New constraints such as the height of Verde drive are now considerations that are questioning the need for the construction of the proposed vehicle access plan; challenging the practicality and costs.</p> <p>The indicative location of access/cross over on Verde Drive and the Monash extension via the round-about are sensible options for landowners to consider when planning developments, encompassing subdivision. Appropriately, an option to follow the framework is sensible, but it should not be mandatory upon development, as design concepts presented for proposal may not marry in well with the access policy.</p> | <p>overs.</p> <p>The Verde Drive Vehicle Access Plan and the North Lake Road Vehicle Access Plans are two separate vehicle access plans that have been established to coordinate access and reduce the number of crossovers in these localities. These vehicle access plans are still relevant as the finalisation of ARNLRB project plans have not addressed the matters that these vehicle access plans propose.</p> <p>Agree the "access/cross over on Verde Drive and the Monash extension via the round-about" is an option and is not mandatory. This will be assessed at time of future subdivision and/or amalgamation of lots south of Verde Drive into the future.</p> |
| 7 | Confidential | <p>SUPPORT:</p> <p>As a cyclist, driver and public transport commuter, I feel this policy is critical in helping to keep our roads safe and community respect each other. At present, despite all my safety gear, I don't feel very safe riding between the PSP along the freeway and Calleya. I am looking forward to North Lake Road/Verde Drive/Solomon Road upgrades being completed. The designs to date look well thought out. The crossovers in Calleya are a good example of what works and what doesn't- the footpaths near the road allows cars in driveways to be off the footpath, while footpaths closer to the house often means cars are parked over the footpath causing safety concerns, in particular for kids and prams. So</p> | <p>Support noted.</p> |

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| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>I fully support the need for Vehicle Access Policy Plans. Supportive of traffic models were required, as they would help the city to better understand traffic movement and make well informed decisions.</p> | |
| 8 | <p>Jeff & Dee Sinton c/- Leapfrog Property Group</p> | <p>OBJECTION:</p> <p>I write to you as the owner of the above-mentioned properties to register my objection to your proposed changes to the Vehicle Access Policy for the precinct. For ease of reference I have detailed our objections for each of our parcels of land separately.</p> <p>In this respect, I submit the following comments for your review and consideration.</p> <p>1. Lot 26 Verde Drive:</p> <p>We note the intention of access to Lot 26 being provided from the Verde Drive / Prinsep Road roundabout and confirm this is consistent with our previous discussions with Daniel Arndt and Rachel Pleasant to provide ingress and egress to our land holding from Verde Drive when it is developed.</p> <p>We do not however agree with the proposed potential future road opportunity for the extension of Monash Gate extending through Lots 26 / 25 & 24 Verde drive (subject to subdivision / realignment / amalgamation by landowners?) - as this adversely severs the sites and diminishes the capacity of the building envelope that could otherwise be constructed on the lands.</p> <p>In this respect, we suggest a clear distinction be made</p> | <p>Objection noted.</p> <p>Firstly, it is noted that access to Lot 26 and Lot 71 Verde Drive have been discussed and agreed upon between the owners of the subject lots and the City in person and then email correspondence dated 10 December 2019. The amendments to the Verde Drive VAP correspond to the access routes that were agreed upon.</p> <p><u>Lot 26 Verde Drive</u></p> <p>In regards to the 'potential future road' indicated on the Verde Drive Vehicle Access Plan (VAP) for the potential future extension of Monash Gate. This is simply an indicative layout, utilising the most direct alignment. However, further extensive planning and consultation is required to define a precise location for the Monash Gate extension. The indicative Monash Gate extension is not formally binding and simply proposes an opportunity to create a more direct road connection as lots in this precinct develop in the future.</p> <p>In regards to the easement in gross location over Lot 26 Verde Drive; this will not result in a</p> |



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| | | <p>between a subdivision and amalgamation outcome. To clarify the Monash Gate extension proposal may have some merit if the 3 sites (Lots 26 / 25 & 24) were collectively subdivided into smaller lots (subject to a suitable subdivision design incorporating the proposed road reserve) - but not if the sites were to be amalgamated to create one larger lot.</p> <p>In this regard, if the amalgamation option eventuated, we suggest a better outcome would be for Monash Gate to end in a cul de sac to the eastern boundary of the amalgamated sites within the adjoining Lot 23 - with ingress and egress to the amalgamated site to be provided to Monash Gate from its eastern boundary as part of any development approval.</p> <p>We also consider that the Easement in Gross depicted on Lot 26 Verde Drive extending south west from the adjoining Lot 25 is unwarranted - as it is too close to the roundabout access to be provided to the site which will result in a traffic hazard for vehicles - particularly those entering the site from the roundabout who then slow to immediately turn left onto the Easement in Gross. This scenario will stifle the traffic flow at the roundabout and create the prospect of stacking to accommodate this left hand turn movement.</p> <p>We also note that the configuration of the north eastern portion of Lot 26 is now of long and narrow dimensions - making it unsuitable for any built form development and effectively limiting its use to associated car parking purposes only. Consequently we request that the Easement in Gross on Lot 26 to the north east of the roundabout be removed.</p> <p>We also note that the indicative location of the access</p> | <p>traffic hazard as stated because the Monash Gate extension does not yet exist. If the Monash Gate extension was to be considered in the future, the easement in gross over Lot 26 would be considered in this process, particularly in regards to traffic hazards. The easement in gross over Lot 26 provides this lot with a clear route of access, in keeping with the objectives of LPP5.6.</p> <p>In regards to relocating the access crossover located on the common boundary of Lots 25 and 24 Verde Drive; this location is only illustrated as 'indicative' on the Verde Drive VAP and will be given further consideration as the lots develop in the future and the VAP is reviewed.</p> <p>The Easement in Gross depicted on Lot 26 Verde Drive extending south west from the adjoining Lot 25 is shown as an option due to the need to connect the easement in gross to the roundabout. However should the Monash connection eventuate then this would need further obvious consideration. This is the intent of the variation clause – to allow a flexible and pragmatic response at a future point in time acknowledging we do not yet have a clear understanding of what landowners will propose.</p> <p>Specifically, Clause 4 (a) and Clause 4 (d) of LPP5.6 are included to address some of the</p> |



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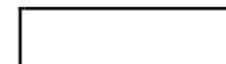
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| | | <p>crossover on the southern side of Verde drive is positioned on the common boundary of Lots 25 & 24. In this respect we would suggest a more practical position to accommodate functional design outcomes on the combined lots would be further east on the common boundary of Lots 24 & Lot 2.</p> <p>2. Lot 71 Verde Drive:</p> <p>We note the CCESP has 4 crossovers depicted on the northern side of Verde Drive to the Easement in the Gross and the proposed Vehicle Access Policy has reduced this number to 2 - with the crossover positioned on the common boundary of Lots 25 / 24 being the sole ingress and egress point for Lot 71.</p> <p>This would require vehicles seeking access to Lot 71 to drive past the site in a north east direction and then backtrack some 50 metres to gain entry to the land holding.</p> <p>Additionally when egressing the site, vehicles will only be able to travel in a north east direction through to the Solomon Road roundabout - before choosing their exit route from the precinct. This is far less desirable from an occupiers / patrons point of view as the convoluted entry and exit linkages create uncertainty and limit the appeal of the premises for commercial purposes.</p> <p>In this respect, the easement in gross and crossovers depicted on the CCESP provide greater flexibility and options for access to Lot 71 - with the Verde Drive crossover positioned on the common boundary of Lot 71 and Lot 25 and the easement in gross extending through to Prinsep</p> | <p>concern raised by the submitter:</p> <p>a) <i>The precise location of rights of carriageways or crossovers delineated on a Vehicle Access Policy Plan may be varied subject to the agreement of the City of Cockburn provided that the purpose and the intent of the Vehicle Access Policy Plan is maintained.</i></p> <p>d) <i>With specific regard to the Verde Drive Access Plan consideration will be given to the exact location of crossovers and need for an easement (West of Solomon Road) as development occurs. This recognises the likely need to reconfigure long narrow lots and the likely timing of various developments will likely be staged over time. The City at such time(s) will ensure the staging of development in this area is provided with suitable access arrangements and will use discretion by having due regard to the objectives of this policy and the intent of the Plan illustrated at Appendix 2.</i></p> <p><u>Lot 71 Verde Drive</u></p> <p>The CCESP is a due regard document that indicatively showed the easement in gross, potential cross overs, all of which require confirmation and detailed consideration within a subsequent LPP. The further detailed</p> |

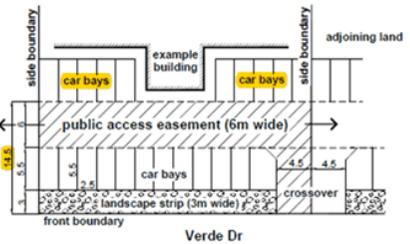


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| | | <p>Road with an additional crossover provided there.</p> <p>This scenario is a far better outcome for Lot 71 as it provides direct access to the site from Verde Drive with the additional option of exiting the property to Prinsep Road - rather than being restricted to traversing through to the Solomon Road roundabout.</p> <p>In view of the above, we would like the easement in gross to remain as currently depicted on the CCESP - as the proposed Access Policy adversely impacts on the ingress and egress options available to Lot 71 and by extension its appeal for commercial occupancy purposes.</p> <p>1. Summary:</p> <p>I trust the above conveys are position and concerns on the proposed Amendment to the Jandakot Vehicle Access Policy and consider that the points raised above are far more likely to enhance the overall functionality of the road hierarchy and traffic circulation in the precinct - than what is currently proposed.</p> <p>The net effect of this should be stronger demand for commercial premises in the locality upon completion of the infrastructure upgrades in 18 months time and a shorter lead time to the lands being developed - rather than lying dormant for extended periods as the restricted access to the properties will limit their appeal for occupation in the commercial market.</p> | <p>considerations have identified 3 crossovers. Noted is in the advertising of the LPP and the Verde Drive plan the cross over now shown north of the Prinsep Road and Verde Drive roundabout was removed in an administrative error. This has now been added in the final policy version. This will provide a necessary cross over option for the rear portion of lots at 33 and 39 Cutler Road should they be subdivided. There are no anticipated negative impacts on landowners as a result of this inclusion.</p> <p>It is again noted that the LPP includes a variation clause of which provides the City and future applications with the ability to consider cross over needs as lots evolve. Noting that cross overs may be required in different locations to give access to certain lots that develop ahead of time as anticipated by the LPP.</p> <p>Put simply if the landowner of Lot 71 developed tomorrow and an alternative easement in gross connection was not yet in place nor was a crossover as per the policy on the northern side of Verde Drive then the City would grant a crossover for Lot 71. The City would then reconsider the subsequent locations as a result - as long as the strategic intent of the LPP was met.</p> |



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| 9 | Lindsay Stephens 1/49 Birdwood Avenue Como 6152 | <p>COMMENTS:</p> <p>I am a joint landholder of a Unit at Lot 4, 800 North Lake Road.</p> <p>It is noted that there is a proposal to change Local Planning Policy 5.6 - Vehicle Access.</p> <p>ISSUES</p> <p>Ability to Construct</p> <p>The proposal raises a number of issues, particularly with respect to access, the plans provided, legality and public liability.</p> <p>The design drawing (Typical Crossover & Public Access Easement Detail is understood to be a concept drawing.</p> <p>However it is not possible to construct such a feature.</p> <p>One Set of Parking Bays only</p> <p>The concept drawing shows a set back of 3 metres for a landscape strip, PLUS 5.5 metres of car bay, PLUS 6 metres of public access easement, another 5.5 metres of car bays. It is noted that there is no provision of a footpath.</p> <p>All of that totals 20 metres with no footpath.</p> <p>I draw your attention to the constructed development which is set back around 14.81 metres. That means it is only possible</p> | <p>Comments clarified and not supported</p> <p><u>Public access easement design detail</u></p> <p>The easement currently provides for the following –</p>  <p>The diagram shows a plan view of a property layout. At the bottom is the 'front boundary' with 'Verde Dr'. A 'landscape strip (3m wide)' is shown with a 'crossover' leading to a 'public access easement (6m wide)'. Above the easement are 'car bays' and an 'example building'. 'side boundaries' are shown on both sides, and 'adjoining land' is to the right. Dimensions are indicated: 14.81m from the front boundary to the start of the easement, 3m for the landscape strip, 5.5m for the car bay, 6m for the easement, and 5.5m for another car bay. A 4.5m setback is also shown for the building.</p> <p>This detail suggests proponents need to make provision for a setback of 14.5m inclusive of a landscape strip from the front property line to the front of a building. Noted is that in this example the building is set back in part and makes provision for more car parking bays. Should car parking requirements be able to be accommodated without having to undertake this option and/or provide parking elsewhere then the partial further setback of the building is not required and therefore this is why 14.5m is referenced.</p> |



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| | | <p>to have one set of parking bays. If one set of parking bays was present, the distance required would be 5.5 metres PLUS 6 metres PLUS 3 metres= 14.5 metres.</p> <p>The only way that could be constructed is to have the parking bays on the northern side of the public access road to enable a vehicle to safely swing into the subject land.</p> <p>The site has a secured access point and there must at all times be sufficient ground available to the landholders to safely access their property.</p> <p>Current Land Use</p> <p>The current land use is shown on the attached aerial photograph. That shows that the footpath is currently on portion of Lot 4.</p> <p>Current Liabilities held by the City of Cockburn</p> <p>As there is portion of the existing footpath located on Lot 4 we assume that the City of Cockburn has indemnified the strata owners of Lot 4 from any Public Liability that could currently arise from an accident or injury by a member of the public when using the City of Cockburn footpath.</p> <p>Future Liabilities held by the City of Cockburn</p> <p>Further, if any public access is provided by the City of Cockburn by way of a feeder road, that could only be constructed and provided if the Landholders of Lot 4 agreed</p> | <p>This is consistent with what has been approved for this site when a DA was previously submitted. It is also what has been constructed as shown below.</p>  <p>Footpath on portion of Lot 4</p> <p>The aerial and cadaster submitted in the submission is not consistent with the City's records and mapping of which show the following –</p> |

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| | | <p>and the City of Cockburn indemnified the landholders for all costs, inconveniences and claims made against the landholders by any person or entity traversing an easement or public access provided by the City. That of course would include any accident or injury.</p> <p>If the City constructed any feature or enabled public access that decreased the safety and access of the landholders then the landholders would reserve the rights for compensation and indemnity from any matter arising from the City's actions.</p> <p>Compensation and Setback</p> <p>It appears that the City can only implement Policy 5.6 when land is subdivided or developed.</p> <p>As the Development is constructed and there is no intention of the landholders to subdivide the land any changes to the land would be totally at the City of Cockburn's costs.</p> <p>The way around the City of Cockburn indemnifying the landholders would be for the City or if Main Roads managed North Lake Road to purchase the land from the landholders.</p> <p>Road Reserve of North Lake Road</p> <p>It is noted that there is additional land on the southern side of North Lake Road, and whilst that might not be the cheapest option it is unclear why private landholders are being forced to give up their land for the feeder road.</p> <p>➤ Please respond to the points relating to the</p> |  <p>The footpath is not located on private land as suggested and therefore the response requested regarding public liability is not required.</p> <p>Therefore the requested meeting and response including addressing various costs expected to be borne by the City is also no longer seen as required.</p> <p><u>Trigger for the easement in gross</u></p> <p>The easement in gross indicated on the North Lake Rad Vehicle Access Plan (VAP) can only be enforced through a subdivision approval or development approval. LPP5.6 and the North Lake Road VAP were adopted in 2011 and have been in place ever since. The proposed modifications are minor in nature and the overall intent has not changed. No change is</p> |



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| | | <p>construction proposed in Local Planning Policy 5.6.</p> <ul style="list-style-type: none"> ➤ Please provide documentation indemnifying the landholders from any claim made by any person or entity for use on the current footpath. ➤ Please provide comment that only one set of car bays would be constructed on the northern side of North Lake Road. ➤ Please provide commitment that all costs will be met by the City of Cockburn for any City of Cockburn activities on the private land of Lot 4. ➤ Please provide a public Forum or site visit for the affected landholders. <p>Plus attachments</p> | <p>proposed to the existing crossover and public access easement design detail.</p> <p>In regards to an easement in gross over the land, this can only be required as a condition of development or subdivision. The easements in gross are granted for the City of Cockburn to maintain public access across the subject land to the side streets and crossover access points as delineated on the Vehicle Access Policy Plan. These easements in gross on land titles are to be provided at the cost of the developer or landowners of the subject land. As a condition of development / subdivision, the City is not bound to compensation of indemnity arising from the lodgement of the easement in gross.</p> <p>In regards to the matter of compensation and implementation of vehicle access plans, 800 North Lake Road has already lodged an easement in gross for shared access in line with LPP5.6. LPP5.6 has always stated that these easements in gross on land titles are to be provided at the cost of the developer or landowners of the subject land.</p> |



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Item 10.1

10. FINANCE AND CORPORATE SERVICES DIVISION ISSUES

10.1 (2020/MINUTE NO 0019) PROPOSED NEW DELEGATED AUTHORITY - DEBT WRITE-OFF, CONCESSION OR WAIVER

Author(s) N Mauricio

Attachments 1. Proposed New Delegated Authority - Debt Write Off, Concession or Waiver

RECOMMENDATION
That Council:

- (1) adopt the proposed new delegated authority "Debt Write Off, Concession or Waiver" made under the Local Government Act 1995, as attached to the Agenda; and
- (2) update the Delegated Authority Register accordingly.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION
MOVED Mayor L Howlett SECONDED Cr C Terblanche
That the recommendation be adopted.

CARRIED 5/0

Background

The review of administrative and strategic Delegated Authorities presented at this meeting includes the deletion of DA – AFCS9 'Debtors Management' (writing off debts), as the associated policy has been identified to be converted to an Administration Policy in accordance with the Policy Project Review.

However, a new delegation made under the *Local Government Act 1995* is warranted to ensure low value debts and monies owing to Council continue being administratively handled, rather than brought to Council for determination.

Submission

N/A

Report

Section 6.12 (1) of the *Local Government Act 1995* provides the power for local governments to waive, grant concessions or write off any amount of money owing to it (other than rates and service charges).

Department of Local Government operational guidelines indicate this exercisable power, though not suitable for 'acting through' provisions of Section 5.45 (2) of the *Local Government Act 1995*, is applicable to be delegated.



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Council has previously delegated this power to the CEO and sub-delegates, limited to a value of \$300 per transaction. This has served the City well in the past, allowing small debt write-offs to be administratively handled without the need for a formal Council decision on what are immaterial items.

Amounts written off under delegation have previously been reported to Council via the Audit and Strategic Finance Committee and this condition is also proposed in the new delegation.

A short review of the sector was undertaken specific to this delegation to assess and determine an appropriate value for the delegation. The following was identified:

- City of Melville - limit of \$10,000 to the CEO, \$5,000 for Directors and \$1,000 for Manager Financial Services;
- City of Fremantle – limit of \$100,000 for general debts and \$10,000 limit for infringements to CEO. Also Directors, Managers and Finance and Revenue Team Leaders have varying limits of sub-delegation up to \$20,000;
- City of Mandurah – limit of \$100,000 to the CEO;
- City of Joondalup – limit of \$20,000 to the CEO and Director Corporate Services, other Managers with varying limits to \$5,000;
- City of Wanneroo – limit of \$10,000 to the CEO and all Directors (write-offs above \$5,000 to be reported to Audit and Risk Committee);
- City of Stirling – limit of \$10,000 per transaction with annual limit of \$150,000 to the CEO with varying sub-delegations to all Directors up to \$7,500 (\$75,000 annually);
- City of Canning – limit of \$10,000 (\$25,000 annual limit) to the CEO, with sub-delegation permitted (but not listed). Quarterly report to Council on exercised delegations;
- City of South Perth – limit of \$2,000 (ex GST) to the CEO.

Based on this review, it is proposed the City's delegated limit be increased from \$300 to \$5,000 per transaction or debtor. This limit is less than that of other comparable Councils, but given the City's risk profile of relatively low debt write-offs, considered sufficient and appropriate for its circumstances.

Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.



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Budget/Financial Implications

N/A

Legal Implications

Power to defer, grant discounts, waive or write off debts under S 6.12 (1) (b), S 6.12 (1) (c) and S 6.12 (2) of the *Local Government Act 1995* refers.

Community Consultation

N/A

Risk Management Implications

There is a "Low" level of "Compliance" risk associated with this item, effectively mitigated through administrative procedures used for managing the exercising of this delegation.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil.

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| DA | DEBTORS MANAGEMENT DEBT WRITE OFF, CONCESSION OR WAIVER | AFCS9 |
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| DELEGATED AUTHORITY CODE: | AFCS9 |
| DIRECTORATE: | Finance & Corporate Services |
| BUSINESS UNIT: | Financial Services |
| SERVICE UNIT: | Rating & Revenue Services |
| RESPONSIBLE OFFICER: | Manager, Financial Services |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | 9 August 2012 |
| DATE LAST REVIEWED: | 8 December 2016 |
| ATTACHMENTS: | AFCS9 |
| VERSION NO. | 4 |

| | | |
|--------------------------------|-----------------------------------|---------------------------------|
| Dates of Amendments / Reviews: | | |
| DAPPS Meeting: | 26 July 2012 27 November 2014 | 26 May 2016 24 November 2016 |
| OCM: | 9 August 2012 11 December 2014 | 9 June 2016 |

FUNCTION DELEGATED:

The authority to write-off any amount of money, or to waive or grant a concession in relation ~~bad debts to any amount of money owing to the City~~ (other than rates & prescribed service charges)

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CONDITIONS/GUIDELINES:

- (1) ~~(1)~~ Authority only applies to amounts of money owing all debts owing to the City up to a maximum value of \$300,5,000 per debtor or transaction.
- (2) Authority does not apply to amounts of money owing for (other than rates or and prescribed service charges.)
- (3) All reasonable endeavours must be used to recover outstanding debts before any write off is made.
- (4) A summary of transactions utilising this delegation is to be reported to the Audit & Strategic Finance Committee on an annual basis.
- (5) All transactions utilising this delegation are to be recorded in the City's record-keeping system (ECM)Recording of Delegated Decisions Register by the delegated officer responsible for initiating the action taken, or by another officer under the direction by of the delegated initiating officer.

AUTONOMY OF DISCRETION:

| | | |
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| DA | DEBTORS MANAGEMENT DEBT WRITE OFF, CONCESSION OR WAIVER | AFCS9 |
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As provided under legislative requirements and the above conditions/guidelines.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

S6.12 (1) ~~(b)~~, S6.12 (1)(c) & 6.12 (2) of the Local Government Act 1995

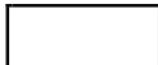
~~Policy AFCS9 "Debtors Management" refers.~~

DELEGATE:

CEO

DELEGATE/S AUTHORISED:

Director, Finance & Corporate Services



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11. ENGINEERING AND WORKS DIVISION ISSUES

Nil



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12. COMMUNITY SERVICES DIVISION ISSUES

12.1 (2020/MINUTE NO 0020) PROPOSED NEW COUNCIL POLICY 'COMMUNITY FUNDING TO SUPPORT LOCAL ECONOMIC DEVELOPMENT (GRANTS)'

Author(s) S Kahle

- Attachments**
1. Proposed New Policy 'Community Funding to Support Local Economic Development'
 2. Proposed New Delegated Authority 'Community Funding to Support Local Economic Development'

RECOMMENDATION
 That Council:

- (1) adopt the proposed new Policy 'Community Funding to Support Local Economic Development and the associated Delegated Authority, as attached to the Agenda; and
- (2) approve use of the existing Grants and Donations budget for Category A business grants with immediate effect.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COMMITTEE RECOMMENDATION
 MOVED Mayor L Howlett SECONDED Cr C Terblanche
 That the recommendation be adopted.

CARRIED 5/0

Background

As a consequence of the COVID-19 pandemic the City has undertaken a number of initiatives to support our local economy and business community. This has accelerated some of the actions which support the objectives within the new Local Economy outcome area of the City's Strategic Community Plan 2020-2030.

The City of Cockburn now has a strategic outcome area focused on local economy, which seeks to support a sustainable and diverse local economy that attracts increased investment and provides local employment. By supporting the business community with grants to support local economic development, the City is able to facilitate opportunities for local business, local activity centres and industry to thrive.



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Submission

N/A

Report

A review of the current Policy SC35 'Grants, Donations and Sponsorships – Community Organisations and Individuals' has identified that Community Funding specifically to support economic development activities within the City of Cockburn are not adequately addressed within this policy.

Given that the Strategic Community Plan now has a Local Economy outcome and objectives, the City has a new focus on supporting our local business community, with Grants to support local economic development, and as such will be able to facilitate opportunities for local business, local activity centres and industry to thrive.

Specifically in response to the COVID-19 pandemic, the City has been well placed to respond to the concerns of businesses and assist the local economy to respond and recover from a period of significant business disruption. As such many businesses are attempting to adapt and pivot their operations, or have identified opportunities to adapt to changing economic conditions and public health guidelines.

In the recent Markyt Community Resilience Scorecard, 'Recovery of the local economy' was listed as the second highest COVID-19 impact of concern, and respondents indicated that the highest Local Government priority should be 'Economic recovery and employment opportunities'.

To enable the City to respond quickly, rather than wait until later in the year, the amount for the grants for this financial year is recommended to be restricted to Category A funding for up to \$5,000 which is proposed to be approved under delegated authority.

However, the draft policy 'Community Funding to support Local Economic Development' provides for two categories of funding.

Category A, Grants up to \$5,000, as detailed above; to be implemented with immediate effect in 2020/2021 using funds from the \$113,543 COVID-19 Community Funding rolled over to the Grants and Donations budget from the 2019/2020 Budget, as recommended at the Grants and Donations Committee meeting 21 July 2020, and as detailed in the Corporate Business Plan 2020-2024, adopted by Council at the Special Council Meeting, 23 July 2020. The recommendation is that these grants are administered under delegated authority to enable the City to respond quickly.



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The recommendation is that the policy includes a second Category B (Business Grants – Minor and Major) that will provide for major funding proposals, minor funding and requests outside of established categories, and that this be initiated in 2021-22 based on guidelines that will be taken to the October 2020 Grants and Donations Committee. These grants will require Council approval, and mirror the Category G funding from the Community Funding for Community Organisations and Individuals (Grants, Donations and Sponsorships) Policy.

Any eligible applications by businesses or eligible organisations that are currently covered by the Community Funding for Community Organisations and Individuals (Grants, Donations and Sponsorships) Policy, (ie. Sustainability Grants and Sponsorships) will continue to be administered via the current process.

This staged approach allows the City to respond quickly to the immediate needs of the COVID-19 impacted economy by providing immediate access to grants of up to \$5,000 under delegated authority, while making provision for next financial year in the policy for larger grants of up to \$15,000. The delay in implementing these larger grants enables the guidelines to be further developed and then taken to the Grants and Donations (GAD) Committee for consideration in October 2020, which requires subsequent approval by Council in November.

In the interim some preliminary research has been undertaken in order to determine the general areas to be included in the guidelines for future consideration by the GAD Committee. The research indicates that local businesses should be supported through the grant program in the following key areas:

- Promotion of innovation
- Technology/Digital enhancements
- Enhancements to local activity centres
- Assisting businesses with COVID-19 recovery

In accordance with the proposed Policy the guidelines will require that grants align with the Strategic Community Plan Local Economy objectives and include the eligibility criteria and evaluation criteria for local businesses to apply.

Strategic Plans/Policy Implications

Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Plan for and facilitate opportunities for local business (including home business and sole traders), local activity centres and industry to thrive.



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- Build local business capacity through partnerships, networks and skill development.

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

In 2020/2021 funds for the Community Funding for Business Grants program are recommended to come from the Grants and Donations budget, as indicated in the Corporate Business Plan.

It is recommended that the Business Grants Program be funded from the COVID-19 grants budget that was allocated for 2020/2021 using funds from of the \$113,543 COVID-19 Community Funding rolled over to the Grants and Donations budget from the 2019/2020 Budget, as recommended at the Grants and Donations Committee meeting 21 July 2020.

In regard to the future funding of the Business Grants, it is recommended that they are funded from the Grants and Donations budget. Over the last five years the Grants and Donations budget has not been fully expended by existing grants programs.

The budget for 2020-2021 is already approved as per the Corporate Business Plan 2020-2024.

Legal Implications

N/A

Community Consultation

In the recent Markyt Community Resilience Scorecard, 'Recovery of the local economy' was listed as the second highest COVID-19 impact of concern, and respondents indicated that the highest Local Government priority should be 'Economic recovery and employment opportunities'.

Risk Management Implications

The Council allocates a significant amount of money to support individuals and groups through a range of funding programs. There are clear guidelines and criteria established to ensure that Council's intent for the allocation of funds are met. To ensure the integrity of the process there is an acquittal process for individuals and groups to ensure funds are used for the purpose they have been allocated.



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Item 12.1

The reputation of the City of Cockburn could be seriously compromised should funds be allocated to businesses, individuals or groups who did not meet the criteria and guidelines and/or did not use the funds for the purposes they were provided. Adherence to these requirements should be essential to mitigate such reputational risk.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil

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Item 12.1 Attachment 1

| | |
|--------------|---|
| Title | Community Funding to support local Economic Development (Grants) |
|--------------|---|



Policy Type

Council

Policy Purpose

The purpose of this policy is to provide a framework for the provision of community funding to support local Economic Development.

Policy Statement

- (1) Council provides up to two per cent of the annual rates income to a budget for grants, donations, sponsorships and subsidies. To ensure that these funds are distributed in a rational way, eligibility, selection and evaluation criteria are required for the assessment and prioritisation of applications to be funded.
- (2) The funds allocated to applicants are to assist in the provision of activities and services that support local Economic Development and facilitate opportunities for local business, local activity centres and industry to thrive, and the objectives of the Local Economy outcome of the Strategic Community Plan 2020-2030. This program is divided into two (2) categories (A-B) to encourage and cater for a range of applications and activities.

| Community Funding Category | Description | Open for applications | Maximum amount available per application | Approval |
|---|--|--------------------------------------|---|---------------------|
| Category A – Economic Development Grants | One-off projects, programs or activities that support local economic development | Twice a year | \$5,000 | Delegated Authority |
| Category B – Economic Development – Minor & Major | Major funding proposals, minor funding and requests outside of established categories. | Upon enquiry and invitation to apply | Dependent on funding type (in alignment with Policy 'Grants, Donations and Sponsorships – Community Organisations and Individuals') | Council decision |

[1]

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Item 12.1 Attachment 1

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| | |
|--------------|---|
| Title | Community Funding to support local Economic Development (Grants) |
|--------------|---|



- (3) The categories are described in further detail in the associated 'Community Funding Guidelines for Economic Development' that outlines the relevant selection and evaluation criteria, and limitations of each funding category.
- (4) Applicants for funding to be distributed from the Grants and Donations budget are required to apply and address the relevant criteria in one of the funding categories. Assessment and approval will be done according to the category and be done under the relevant delegated authority or recommendation by the Grants and Donations Committee to Council, and applicants cannot bypass these processes.
- (5) An applicant may successfully apply for and receive funding from two different Community Funding categories per financial year, provided they meet the criteria for the particular category. This funding can be granted from either of the following categories:
 - two different business categories from this policy, or
 - seven different categories from the Community Funding for Community Organisations & Individuals (Grants, Donations & Sponsorships) policy

This is provided they meet the criteria for the particular category; however, the applications cannot be for the same project. Applicants that have been successful in previous years are eligible to apply provided all previous funding has been satisfactorily acquitted.

| | |
|---|--------------------------------|
| Strategic Link: | Strategic Community Plan |
| Category | Business, Economy & Technology |
| Lead Business Unit: | Corporate Communications |
| Public Consultation: (Yes or No) | No |
| Adoption Date: (Governance Purpose Only) | |
| Next Review Due: (Governance Purpose Only) | |
| ECM Doc Set ID: (Governance Purpose Only) | |

[2]



DAP 27/08/2020

Item 12.1 Attachment 1

| | |
|-------|--|
| Title | Community Funding to support local Economic Development (Grants) |
|-------|--|



[3]

| | | |
|----|--|--|
| DA | COMMUNITY FUNDING TO SUPPORT LOCAL ECONOMIC DEVELOPMENT | |
|----|--|--|

| | |
|----------------------------------|---|
| DELEGATED AUTHORITY CODE: | |
| DIRECTORATE: | Governance & Community Services |
| BUSINESS UNIT: | Corporate Communications |
| SERVICE UNIT: | Communications & Marketing |
| RESPONSIBLE OFFICER: | Business Engagement Officer |
| FILE NO.: | 086/003 |
| DATE FIRST ADOPTED: | |
| DATE LAST REVIEWED: | |
| POLICY REF.: | Community Funding to support Local Economic Development |
| VERSION NO. | 1 |

| | |
|---------------------------------------|--|
| Dates of Amendments / Reviews: | |
| DAPPS Meeting: | |
| OCM: | |

FUNCTION DELEGATED:

The authority to evaluate funding submissions in accordance with grant programs listed under policy 'Community Funding to Support Local Economic Development' and to manage and allocate funds to submissions compliant with this policy and respective guidelines.

CONDITIONS/GUIDELINES:

- (1) To approve applications for the following grant programs:
 - 1. Category A – Economic Development Grants
- (2) All transactions utilising this delegation are to be recorded by the officer responsible for initiating the action taken, or by another officer under the direction of the initiating officer.

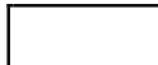
AUTONOMY OF DISCRETION:

As provided as in the conditions above.

LEGISLATIVE REQUIREMENTS/COUNCIL POLICY:

Local Government Act, 1995 sec 5.42 and 5.44
 Council Policy "Community Funding to support local Economic Development (Grants)" refers.

[1]



DAP 27/08/2020

Item 12.1 Attachment 2

| | | |
|----|---|--|
| DA | COMMUNITY FUNDING TO SUPPORT LOCAL ECONOMIC DEVELOPMENT | |
|----|---|--|

DELEGATE:

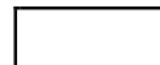
Chief Executive Officer

Note: The Chief Executive Officer will sub-delegate this authority to:

DELEGATE/S AUTHORISED:

Director, Governance & Community Services
Manager, Corporate Communications

[2]



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13. EXECUTIVE DIVISION ISSUES

Nil

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Item 14.1

DAP 27/08/2020

14. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**14.1 (2020/MINUTE NO 0021) PROPOSED AMENDMENT TO 'CITY OF COCKBURN BRANDING AND LOGOS' POLICY****Author(s)** S Seymour-Eyles**Attachments** 1. Branding and Logos Policy**RECOMMENDATION**

That Council:

- (1) receive the report; and
- (2) retain Policy 'City of Cockburn Branding and Logos' in its current form, as attached to the Agenda.

COMMITTEE RECOMMENDATION

MOVED Mayor L Howlett SECONDED Cr C Terblanche

That the recommendation be adopted.

CARRIED 5/0**Background**

Cr C Terblanche submitted the following Notice of Motion via email on 16 June 2020:

That Council amend the City of Cockburn Branding and Logos Policy so that the Gold Crest is for use of all Elected Members and the City of Cockburn Corporate Logo is for staff only

Reason

Elected Members (EMs) are not staff members and should not be expected to wear staff branded clothing or use staff logos on things like their business cards. There have been many instances to date where residents confuse the role of Elected Members in the organisation and by expecting Elected Members to brand themselves as staff members are making this even harder for the public to understand that there is a distinct difference between staff and Elected Members.

Instead, the City's "Gold Crest" should exclusively be for use by all Elected Members as we are all a part of the official Council, as is the practice at many other Councils.

Submission

N/A

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Item 14.1

Report

The current 'City of Cockburn Branding and Logos' policy stipulates that the Crest should only be used in the Office of the Mayor Gold Crest format as follows.

'The Office of the Mayor Gold Crest (the crest) retains the historical and ceremonial significance of the City. The crest is used on all Mayoral stationary, e-Signature, the Mayor's name badge, the Mayoral chain, Mayoral attire, the City flag (used for civic events), decor in and around the Mayor's office and for other Mayoral related activities. It may also be used in combination with the City's logo on invitations to civic events. Otherwise the Crest is not to be used for any other purpose.'

The point made by Cr Terblanche relating to the separation of staff and Elected Members is a valid one, however any 'board', and the organisation it represents, should all be working towards the same goals, which in the City's case is to:

- fulfil our vision, to make Cockburn the best place to be; and
- achieve our Strategic Community Plan objectives.

A single logo unites the City of Cockburn in that goal. No other organisation would have a separate logo for its board. The Federal and State Government Members of Parliament do not use a different logo to that of the civil servants who are employed by the related government departments.

From previous research undertaken, there are indeed Councils who have two logos, but this is usually a competing legacy of conversion to a newer refreshed logo to better reflect modern government with the desire of Elected Members to retain the old Crest. In some cases, such as the City of Melville, the local government has retained the crest and both Elected Members and staff use it.

The City of Cockburn considers itself a progressive, digital, responsive, organisation. When the City measures itself against its Strategic Community Plan objectives, it is performing well and is also one of the highest performing Councils in the Perth Metropolitan area, and the highest performing growth Council of those taking part in the Catalyse Community Scorecard survey. The brand in the community should represent the image of a high performing, modern Council. The brand most identified in the Cockburn community is the City of Cockburn logo and should be proudly associated with.

Any confusion can be addressed through ensuring that badges clearly state Councillor, and by using any confusion as an opportunity to introduce oneself as a Councillor.

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Strategic Plans/Policy ImplicationsListening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

Minimal cost to produce business cards.

Possible uniform updates.

Some time and cost to update corporate style guide.

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

From a community perspective it is better that Elected Members and administration are seen to be working together towards shared common goals under one umbrella, and therefore one logo. The Elected Members set the direction through decisions of Council, and the organisation, through the administration, makes it happen.

A brand reflects what an organisation promises and how it performs. Having two brands is counter-intuitive to this, however, for all Elected Members to use the crest instead of the logo is not a high risk. If this were the chosen path, it would need to be consistent for all Elected Members, not some.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) Local Government Act 1995

Nil



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Item 14.1 Attachment 1

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| | |
|--|-----------------------------------|
| Title | City of Cockburn Branding & Logos |
| Policy Number (Governance Purpose) | |



Policy Type

Council policy

Policy Purpose

To protect and clarify the use of the City's corporate branding and logos and to control the use of the title 'City of Cockburn'.

Policy Statement

- (1) Office of the Mayor Gold Crest

The Office of the Mayor Gold Crest (the crest) retains the historical and ceremonial significance of the City. The crest is used on all Mayoral stationary, e-Signature, the Mayor's name badge, the Mayoral chain, Mayoral attire, the City flag (used for Civic Events), decor in and around the Mayor's office and for other Mayoral related activities. It may also be used in combination with the City's logo on invitations to civic events. Otherwise the Crest is not to be used for any other purpose.

- (2) City of Cockburn corporate logo

The logo represents the City of Cockburn and must be used on all online and hard copy internal and external communications and marketing material used by staff and Councillors.

- (3) Use of the corporate logo by Elected Members

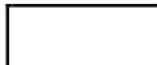
Elected Members may only use City of Cockburn branded material, including but not limited to stationary, badge, e-signatures, headed paper, envelopes, for the purpose of their official duties. The corporate logo may not be used for promoting the political interest of Elected Members or of any State or Federal Political Party or representative

- (4) Community usage of the logo

In cases where the City supports an organisation financially or in kind, or is a project partner, it may be part of the agreement that the organisation must use the City's logo in agreed marketing material for the duration of the agreement.

[1]

Document Set ID: 443333



DAP 27/08/2020

Item 14.1 Attachment 1

| | |
|--|-----------------------------------|
| Title | City of Cockburn Branding & Logos |
| Policy Number (Governance Purpose) | |



| | |
|---|---------------------------------------|
| Strategic Link: | Communications Strategy & Action Plan |
| Category | Governance |
| Lead Business Unit: | Corporate Communications |
| Public Consultation: (Yes or No) | No |
| Adoption Date: (Governance Purpose Only) | 14 March 2019 |
| Next Review Due: (Governance Purpose Only) | March 2021 |
| ECM Doc Set ID: (Governance Purpose Only) | 4133992 |

[2]

Document Set ID: 4133992



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15. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING

Nil

16. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY MEMBERS OR OFFICERS

Nil

17. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

Nil

18. CONFIDENTIAL BUSINESS

Nil

19. CLOSURE OF MEETING

The meeting closed at 6.18pm.



14. PLANNING AND DEVELOPMENT DIVISION ISSUES

14.1 (2020/MINUTE NO 0188) PROPOSED NOMINATION TO THE CITY'S SIGNIFICANT TREE LIST - FIG TREE, REGIONAL ROAD RESERVATION, HAMILTON HILL

| | |
|--------------------|---|
| Author(s) | L Dunstan |
| Attachments | 1. Heritage Assessment 2. Draft Place Record |

RECOMMENDATION

That Council advertise the proposed inclusion of the Fig Tree (near Starling Street and Dixon Reserve) Hamilton Hill, on the City of Cockburn Local Government Inventory as a 'Significant Tree' as shown in the Draft Place Record included at Attachment 2.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

The City has received a heritage nomination from a member of the public to include a fig tree in the suburb of Hamilton Hill onto the 'Significant Tree' list.

The City has considered the nomination and undertaken a heritage assessment, and has determined that further information (by way of advertising) is required prior to determining the appropriateness of including the fig tree on the Heritage Inventory.

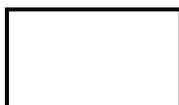
City of Cockburn Local Government Inventory and Heritage List

Local Government Inventories (or Local Heritage Surveys) are required to be prepared by local governments under the *Heritage Act 2018*. The inventories are a survey of places in the district that are, or may become, of cultural heritage significance.

The City of Cockburn Local Government Inventory (LGI) was revised and adopted by Council on 14 June 2018.

The LGI also includes a 'Significant Tree' list, which are trees listed for their cultural heritage significance. Trees are assessed against their historic value, horticultural value, whether they are rare or localised, their locational context, whether they are of exceptional size, age and form, their Aboriginal association, and their social, cultural or spiritual value.

Submission



A member of the public has submitted the following in nomination of the proposed fig tree:

The tree is likely to have been part of Sunnyside Farm. Sunnyside Farm was owned and farmed by a number of non-English immigrants, including the Stelis, Gerovich and the Ricci families. The farm house and outbuildings at different times housed many new immigrants, working as farm labourers, while seeking Australian residency (Dom Ricci). The farm was noted as a significant wine-producing property, but also grew and marketed other produce.

Horticultural Value

The fig (ficus carica) tree has miraculously survived annual spraying of surrounding castor oil plants and other weeds. It is remarkably healthy and produces an abundant crop of figs during the summer months.

Location or Context

The tree contributes to the area and is a gathering place for locals who forage from the tree. On most summer mornings, you can see at least two or three people (often accompanied by their children), picking and eating the fruit. In fact, one gentleman comes on the bus each year from Nollamara to pick figs for his family.

Exceptional Size, Age and Form

The tree is exceptionally large (approximately 6 metres in diameter and 4.5 metres tall). It has multiple trunks and an extensive canopy. It is estimated to be 60-80 years old.

Social Culture or Spiritual Value

The tree is well known to the local community and is a place where people meet and chat while picking fruit together.

Report

The purpose of this report is for Council to consider the nomination and to determine whether the fig tree should be advertised for proposed inclusion on the LGI. The City undertook a heritage assessment and site visit which is provided within Attachment 1, and briefly summarised below.

Historical Significance

The nominator asserts that the fig tree was planted by the Stelis, Gerovich and Ricci families operating Sunnyside Farm. A recent Heritage Assessment by *Terra Rosa Consulting* was undertaken for the area and the location of Sunnyside Farm is noted on aerial photography, as well as a description of the homestead activities. It is considered that the advertising of this listing will help to provide further information on the historical significance of the fig tree, and perhaps shed further light on its connection to these families.



Horticultural Value

The prevalence of fig trees within Perth, being abundant and easy to source, suggest that horticultural values are low and therefore the fig tree does not demonstrate compliance with this criterion.

Rare or Localised

Fig trees are not Australian Natives, and therefore are not identified as potentially threatened under the *Biodiversity and Conservation Act 2016*. Fig trees are not considered 'rare' or 'localised' and therefore the subject fig tree does not demonstrate compliance with this criterion.

Location or Context

In order to comply with this criterion the fig tree would be expected to provide a *major* contribution to the landscape or local character. Given its minimal presence on the landscape, it is not considered that the fig tree demonstrates compliance with this criterion.

Exceptional Size, Age and Form

The fig tree is significant in size (8.5m wide) and has, over time, grown wildly in an unkempt fashion, suggesting old age and relatively undisturbed unique growth.

Assessment of the City's aerial mapping suggests that the tree was potentially planted in the 1950s (5m wide) making the tree at least 70 years old, with the earliest high quality aerial (1953) clearly depicting the tree (as shown circled below):



It is considered that the fig tree represents compliance with this criteria, in that it has existed onsite for a number of years, has an irregular or abnormal growth pattern, which lends interest to visitors, and may be of value given its unusual size and relatively healthy appearance.



Officer Recommendation

The fig tree complies with at least one assessment criterion and this warrants further investigation into the potential for it to comply with other (more difficult to establish) assessment criteria; particularly *social value* and *historical significance*. It is therefore recommended that the fig tree be advertised for public comment, to determine whether there are indeed social and historical values, and to ascertain what these exact values may be.

Given that the tree is currently dormant, there will be a need to take additional photos once the tree takes on new leaf growth for the year. The current photos though assist in presenting the structural form of the tree.

Consultation Requirements

There are no statutory consultation requirements for inclusion of a place on the LGI pursuant to the Scheme, the *Heritage Act 2018* or *Heritage Regulations 2019*. However, for places proposed to be included on the Heritage List, the *Planning and Development (Local Planning Scheme) Regulations 2015* require a minimum advertising period of 21 days.

It is therefore considered that a 21 day advertising period is reasonable, and should allow sufficient time for the community to review the proposal, including the draft place record, and provide comments.

Conclusion

It is recommended that Council resolve to advertise the proposed inclusion of the fig tree, located on the regional road reservation adjacent to Starling Street, Hamilton Hill, on the City of Cockburn Local Government Inventory as shown in the draft place record included at Attachment 2.

Strategic Plans/Policy Implications

Environmental Responsibility

Improve our urban forest and streetscapes across the City.

Budget/Financial Implications

Nil

Legal Implications

Nil

Community Consultation

If adopted by Council, the proposed inclusion of the fig tree on the LGI will be advertised for a period of 21 days.



This will include letters to the landowner of the subject land, surrounding landowners, and the Hamilton Hill Community Group, and a sign onsite. An advertisement will also be put in the Cockburn Gazette, and a notice in Comment on Cockburn inviting comments on the proposal.

Risk Management Implications

In ordinary circumstances, pursuant to Clause 4.18 of the Town Planning Scheme No. 3, planning approval is required in the event that a 'Significant Tree' is proposed to be removed.

It should be noted, however, that the subject tree is located within a Main Roads WA Regional Road Reservation. Clause 16(1) of the Metropolitan Region Scheme Clause 16(1) states that development approval under the MRS is not required if the reserved land is owned by or vested in a public authority, and that public authority is proposing to use the land for the purpose for which it is reserved. Notwithstanding, it is likely that any works would trigger consultation with the local government and that comments could be provided on the details of any tree of heritage significance.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



Assessment Criteria - Significant Trees

Subject Tree: Fig Tree, Regional Road Reserve (near Starling Street), Hamilton Hill

Definition:

“Tree” includes shrubs and other perennial plants, and should be read in the singular or plural to include a group of trees.

The criteria for assessing whether a tree is sufficiently ‘significant’ to include in the Local Government Inventory are based upon similar principles which are used to determine the suitability of heritage sites and buildings, being:

- Aesthetic value
- Historic Value
- Research Value
- Social Value
- Rarity
- Condition, Integrity and Authenticity

Significant Tree/s may be in the form of:

- individual specimens, avenues or stands of trees or native vegetation
- a landscape design, memorial arrangement or celebratory alignment
- immature specimens, mature, post mature or notably old

Significant Tree/s may occur in public parks and reserves, streets, car parks, private and public gardens etc.

The following criteria are the basis for the analysis of trees nominated as ‘significant’ to the City, either by a resident, community group or the City of Cockburn. The nomination and assessment of a significant tree/s is required to be supported by a qualified arborist or similar professional related to the field of flora and fauna, and shall address either a single criteria from those listed below, or multiple criteria.

In order to be deemed ‘significant’ and thus warrant inclusion within the City of Cockburn Local Government Inventory, a nominated tree or trees must adequately demonstrate compliance with **one or more of the following criteria**:



| Criteria | Description | Supporting Information/Examples | Strategic Planning Comments |
|--------------------------------|---|---|---|
| Historical Significance | Tree/s commemorating a particular occasion, including plantings by notable people, or having associations with an important event in local, state or national history. Tree/s that possess a history specifically related to the City or its surrounding areas. | <ul style="list-style-type: none"> • Plantings by well-known public figure or group • Relates to a historical occasion or event | The fig tree may have been part of Sunnyside Farm. Sunnyside Farm was owned and farmed by a number of non-English immigrants, including the Stelis, Gerovich and the Ricci families. The farm house and outbuildings at different times housed many new immigrants, working as farm labourers, while seeking Australian residency (Dom Ricci). The farm was noted as a significant wine-producing property, but also grew and marketed other produce. The fig tree may meet this criterion, however further research is required. |
| Horticultural Value | Tree/s of outstanding horticultural or genetic value and that which could be an important source of propagating stock, including specimens particularly resistant to disease or exposure. | <ul style="list-style-type: none"> • Tolerance selection (pest and disease) • Propagating potential • Scientific value | The prevalence of fig trees within Perth, being abundant and easy to source, suggest that horticultural values are low and therefore the fig tree does not demonstrate compliance with this criterion. |



| | | | |
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| <p>Rare or Localised</p> | <p>Tree/s species or variety rare or very localised in distribution, enhancing the diversification of the local urban forest.</p> | <ul style="list-style-type: none"> • Only known species in area • Rare species (2 - 50 known specimens) • End of natural range • One of few examples of the family / genus / species in precinct | <p>Fig trees are not Australian Natives, and are therefore not identified as potentially threatened under the <i>Biodiversity and Conservation Act 2016</i>. Fig trees are not considered 'rare' or 'localised' and therefore the subject fig tree does not demonstrate compliance with this criterion.</p> |
| <p>Location or Context</p> | <p>Tree/s that occur in a unique location or context so as to provide a major contribution to landscape and/or local place character. Includes outstanding aesthetic value which frame or screen views, or act as a landmark.</p> | <ul style="list-style-type: none"> • Important landmark • Contribution to landscape • High visibility of tree • Presence on a ridgeline • Screening function • Historic planting style | <p>The fig tree is located central to the reserve lot, in a low-lying strand of castor oil plants. The tree is not significant in contributing to landscape character, as it is arguably not aesthetically beautiful, however is impressive in size. In order to comply with this criterion the fig tree would be expected to provide a <i>major</i> contribution to the landscape or local character. It is arguably dwarfed by the castor oil growth and is not distinctive by way of its visibility from the streetscape. Given its minimal presence on the landscape or character, it is not considered that the fig tree demonstrates compliance with this criterion.</p> |
| <p>Exceptional Size, Age and Form</p> | <p>Tree/s noted for particular age, size or irregular form relative to other normal mature tree species that currently reside within the City. Also includes</p> | <ul style="list-style-type: none"> • Height • Circumference • Canopy spread • Fusion of branches • Unusually damaged (but healthy) • Form and vigour reflecting an outstanding specimen of its species | <p>The fig tree is significant in size and has, over time, grown wildly in an unkempt dystopian fashion, suggesting old age and relatively undisturbed unique growth. Assessment of the City's aerial mapping suggests that the tree was potentially planted in the 1950s, with the earliest high quality aerial (1953) clearly depicting the tree (as shown circled below):</p> |



| | | | |
|--|--|--|--|
| | <p>curious forms, particularly abnormal outgrowths, fused branches or unusual root structures.</p> | |  <p>The aerial suggests that the tree was planted along the original fence-line of the now subdivided Lot 54 Rockingham Road (currently containing commercial strata units). An approximate assessment of the circumference of the tree (from aerial photography) shows growth from 5m² in 1953 to 14m² in 1965 and 51m² in 2000, with the current circumference being around 59m². (These calculations are approximate only). The tree is therefore at least seventy years old.</p> <p>It is considered that the fig tree represents compliance with this criteria, in that it has existed onsite for a number of years, has an irregular or abnormal growth pattern which lends interest to visitors and may be of value given its unusual size and relatively healthy appearance. It is noted that the fig tree is still producing fruit (as evidenced by a recent local newspaper article written in February 2020 – Fremantle Herald).</p> |
|--|--|--|--|



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|-------------------------------|---|---|--|
| | | | Due to this, it is considered that the fig tree presents some values in terms of size and features, and therefore complies with this criterion. |
| Indigenous Association | Tree/s that has a recognised association with Indigenous people, or that is valued for continuing and developing cultural traditions. | <ul style="list-style-type: none"> • Scarred tree • Corroboree tree • Canoe tree | <p>The Hamilton Hill Swamp Precinct, of which the fig tree is located, is situated within the traditional lands of the Whadjuk Nyoongar Traditional Owners, located within the suburb of Hamilton Hill and bordering the City of Fremantle, in close proximity to Clontarf Hill. A recent Heritage Study of the area (Terra Rosa 2020) identifies Indigenous Significance, particularly in relation to the nearby swamp (buried under Dixon Reserve) and identifies in close proximity to the fig tree a current inundation area 'current swamp' (edge of the original swamp). Water sources hold significance to Traditional Owners, given intangible values relating to the Waugal and the wider cultural landscapes. The Study acknowledges several accounts of 'first punishments' with the belief that one of these was undertaken in Hamilton Hill, resulting in the deaths of several women and children. These painful accounts confirm that Indigenous association to the area is of high importance and undeniable. In relation to the subject fig tree, it was likely planted as part of the mid-twentieth century market garden land uses, as aerial photography suggests. It possesses no known Indigenous specific values (such as scarring etc.) as it was likely planted at a time when Traditional Owners had little to no access to the subject land. Therefore, the fig tree does not demonstrate compliance with this criterion.</p> |



| | | | |
|---|---|---|---|
| <p>Social, Cultural or Spiritual Value</p> | <p>Tree/s that has an important link to the community or a specific group located within the City</p> | <ul style="list-style-type: none"> • Community engagement focussed around the tree for positive social or cultural reasons • Spiritual importance of a tree to a specific group in the community. | <p>A recent article (February 2020 – Fremantle Herald) suggests that the fig tree contains some social values, or provides community focussed engagement given it is still producing fruit. It is not considered to provide cultural or spiritual values, however, as the fig tree has little association with an historical land use, or significant cultural attributes, such as connecting it to a known Hamilton Hill family or similar connection. A recent Heritage Study of the area (Terra Rosa 2020) does not mention the fig tree, however this may not accurately represent its social values as perhaps the Study was more broadly focussed upon early European and Indigenous Heritage.</p> <p>It is considered that further information is required, in the form of community advertising, to confirm the fig tree’s compliance with this criterion.</p> <p>Photo courtesy of Fremantle Herald – local community in front of subject fig tree (Feb 2020)</p>  |
|---|---|---|---|



Officer Recommendation:

The fig tree complies with one (1) of the above criterion: **Exceptional Size, Age and Form**, given it is likely to be at least seventy years old, and has grown an impressively sized canopy which is unusual for its local context. It has likely been protected from encroaching development given its location within the Roe 9 Regional Road Reservation and has been permitted to grow with little to no visible maintenance regime. It is still producing fruit, although recent site visits cannot confirm this due to figs being out of season.

The fig tree may comply with two additional criterions above: **Social, Cultural or Spiritual Value**, given a recent local newspaper article (Fremantle Herald) depicts various local community members together calling for the tree to be nominated as a Significant Tree on the City's Heritage Inventory. *"It brings the community together every year when its sweet fruits ripen"*. Further, the tree may provide fruit to a broader community, demonstrating potential social values: *"I have met people who come from other suburbs just to pick its fruit," said HHCG member Christine Duckham.* Further, **Historical Significance**, given it may have been planted by Sunnyside Farms, which was a prominent homestead in the area in the early part of the twentieth century.

The fig tree complies with one assessment criterion and this warrants further investigation into the potential for it to comply with other (more difficult to establish) assessment criteria; particularly social value and historical significance. It is therefore recommended that the fig tree be advertised for public comment, to determine whether there are indeed social and historical values, and to ascertain what these exact values may be, as currently there is insufficient information to either include or reject the nomination of the tree onto the City's Heritage Inventory.

PHOTOS





LOCAL GOVERNMENT INVENTORY

FIG TREE, ROE HIGHWAY RESERVE

T Heritage Tree(s)



| | |
|-----------------------------|---|
| LGI # | TBA |
| PIN No. | |
| LOT/PLAN DIAGRAM | |
| LOCATION | Roe Highway Reserve, Hamilton Hill, north-west of Starling Street, south-west Dixon Park (Figure 1) |
| OTHER NAME(S) | ---- |
| CONSTRUCTION DATE(S) | ---- |

STATEMENT OF SIGNIFICANCE

The fig tree (*ficus carica*) is significant for its importance to the Hamilton Hill community given its age, unusual growth pattern and its healthy condition, which bears fruit on a yearly basis that is enjoyed by





LOCAL GOVERNMENT INVENTORY

the locals and beyond.

The fig tree is significant in that its wild growth forms a relatively untouched canopy, which has been permitted to grow in a unique sprawling way that lends interest to visitors.

PHYSICAL DESCRIPTION

The fig tree (*ficus carica*) is significant in size and has, over time, grown wildly in an unkempt dystopian fashion, suggesting old age and relatively undisturbed unique growth. Research of the City's aerial mapping suggests that the tree was potentially planted in the 1950s. The fig tree has several trunks, growing in an outward direction and a height of approximately 4 metres. Its growth circumference is approximately 59m² and appears to be generally in good condition.

The fig tree currently sits on the edge of a strand of castor oil plants. Locals claim that the nearby and broader community visit the tree annually to pick fruit¹, which is abundant during summer.

HISTORY

The Roe Highway reservation was first gazetted in 1959 as part of the Stephenson & Hepburn plan for the Perth Metropolitan area. The length of reserve has been historically been used for market gardens, farming and other rural uses. The fig tree is likely to have been planted on the original property, which has since been subdivided by Main Roads WA, who is the current landowner. Aerial photography depicts the likely age of the fig tree as early 1950s (Figure 2) and it appears to have been planted along the fence line of the original property (now 54 Rockingham Road, Hamilton Hill).

The fig tree may have been part of Sunnyside Farm. Sunnyside Farm was owned and farmed by a number of non-English immigrants, including the Stelis, Gerovich and the Ricci families. The farm house and outbuildings at different times housed many new immigrants, working as farm labourers, while seeking Australian residency (Dom Ricci). The farm was noted as a significant wine-producing property, but also grew and marketed other produce.

| | |
|--------------------------------|--|
| CONDITION | Good |
| ASSOCIATED PEOPLE | Nil known. |
| OTHER LISTINGS | <i>HCWA No.</i> ----- <i>Register National Estate</i> ----- <i>National Trust WA</i> ----- |
| SUPPORTING INFORMATION | Fremantle Herald, 'Locals Give a Fig', February 21, 2020 |
| ASSESSOR(S) NAME | City of Cockburn |
| ADDITIONAL NOTES | |
| DATE OF LAST ASSESSMENT | |

¹ Fremantle Herald, 'Locals Give a Fig', February 21, 2020



LOCAL GOVERNMENT INVENTORY

LISTING HISTORY



Figure 1: 2020 Aerial Photo Location Plan



Figure 2: 1953 Aerial Photo



14.2 (2020/MINUTE NO 0189) PROPOSED STRUCTURE PLAN - LOT 2 (7) FAWCETT ROAD, LAKE COOGEE

| | |
|------------------------------|--|
| Author(s) | K Knuckey |
| Attachments | 1. Schedule of Submissions 2. Location Plan 3. Proposed Structure Plan Map |
| Location | Lot 2 (7) Fawcett Road, Lake Coogee |
| Owner | Vem Contracting Pty Ltd |
| Applicant | Urbanista Town Planning |
| Application Reference | 110/204 |

RECOMMENDATION

That Council:

- (1) adopts the Schedule of Submissions prepared in respect to the Proposed Structure Plan;
- (2) endorse the Bushfire Management Plan prepared by Green Start Consulting in respect of the proposed structure plan and dated 9 October 2019 (version 1.0);
- (3) pursuant to Schedule 2, Part 4, clause 20 of the deemed provisions of the *Planning and Development (Local Planning Scheme) Regulations 2015*, recommend to the Commission the approval of the proposed Structure Plan at Lot 2 (No. 7) Fawcett Road, Lake Coogee, subject to the following modifications:
 1. Modify the Structure Plan Map as follows:
 - a. Land proposed to be zoned 'R40/60' to be modified to 'R30/60';
 - b. Land proposed to be zoned 'R40' to be modified to 'R30';
 - c. Land proposed to be zoned 'R30' which is located over the existing Water Corporation sewer easement, be reserved as 'Road Reserve';
 2. Revise and update the entirety of the structure plan report and all relevant appendices to reflect the modified zoning and reserves;
 3. Update Part 1 Section 4.7 Treatment of Road Reserve to include the following provision:

Land that is reserved as road reserve and located over the Water Corporation easement is required to be embellished by the responsible developer to the satisfaction of the City of Cockburn.;
 4. All references to Local Structure Plan or 'LSP' replaced with 'Structure Plan'; and
- (4) Advise the landowners within the structure plan area and those



who made a submission of Council's recommendation.

COUNCIL DECISION

MOVED Cr K Allen SECONDED Cr C Stone

That Council defer this item to the 8 October 2020 Ordinary Council Meeting.

CARRIED 9/0

Reason for Decision

This item requires more discussion. The sewer issues needs to be clarified, worked through, and an outcome negotiated.

Background

This Structure Plan has been prepared to guide the subdivision and development of Lot 2 (No. 7) Fawcett Road, Lake Coogee in a coordinated manner. The structure plan has been advertised in accordance with the requirements of City of Cockburn Town Planning Scheme No. 3, and the purpose of this report is to consider the assessment of the structure plan and the submissions that have been received. It is recommended that Council adopts the structure plan, subject to modifications, in order to address the issues noted in the assessment below.

Submission

N/A

Report**Planning Background**

The proposed structure plan was lodged with the City on 26 May 2020. Following subsequent discussions with the applicant, the proposed structure plan was advertised from 18 June to 16 July 2020. During this time, 14 submissions were received; four submissions from government agencies/utility providers, eight private submissions of objection and two private submissions of support.

The submissions are identified in the Schedule of Submissions (refer Attachment 1). Each of these submissions is responded to in detail in the schedule and the key planning issues and considerations are discussed within this report.

The subject site is zoned 'Urban' under the Metropolitan Region Scheme (MRS) and 'Development' under City of Cockburn Town Planning Scheme No. 3 (Scheme). The subject site is also located within Development Area No 5 (DA 5), Development Contribution Area

No. 6 (DCA 6) and Development Contribution Area No. 13 (DCA 13) under the Scheme.

Development Area 5 as identified in TPS3 requires a structure plan to be prepared and approved in accordance with clause 27(1) of the *Planning and Development (Local Planning Scheme) Regulations 2015*, prior to subdivision and/or development.

Pursuant to the above Scheme provisions, the applicant has submitted a Structure Plan for assessment. This report aims to summarise the outcome of that assessment pursuant to the planning framework, the legislative requirements of the Regulations and that of the State and local Schemes.

Locality - Context

The site is located in the middle of Lake Coogee, nearby Mayor Road and Stock Road as shown on the Location Plan at Attachment 2.

The subject Lot 2 (outlined in red below) and Lot 1 (immediately adjacent to the north) are some of the final lots left to be structure planned for the northern part of this cell as illustrated below:

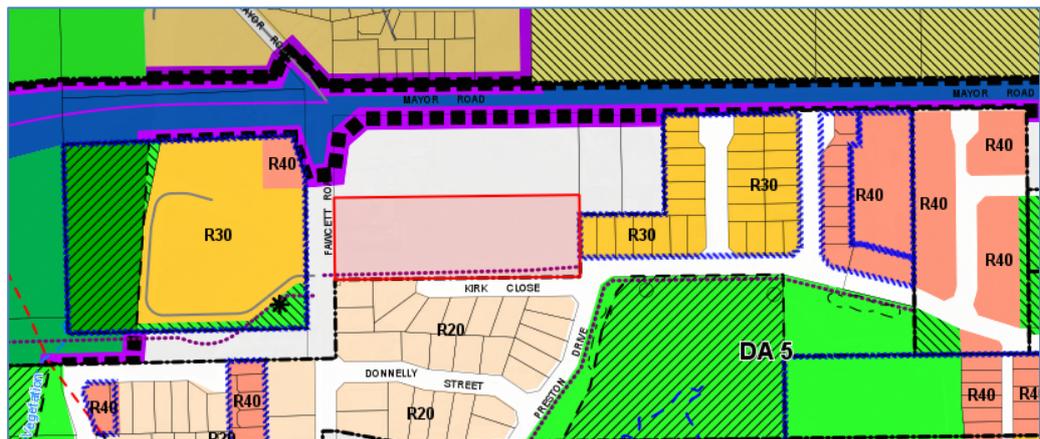


Figure 1: Approved structure plans surrounding the subject Lot 2 (shown in red).

Structure plans surrounding the subject lot are generally coded R20 to R30 with a pocket of R40 to the west and a large pocket to the east. The R20, R30 and R40 coded lots largely provide for single detached dwellings as illustrated below:

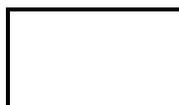




Figure 2: Single detached dwellings characterise the surrounding area in proximity to the Bindjar Reserve wetland.

The Proposal

The proposed Structure Plan Map is included at Attachment 3 showing multiple density codes – R30, R40 and a split code of R40-60. Along the southern boundary of the site is a Water Corporation Sewerage easement. This is the proposed location of public open space. This would facilitate the subdivision of approximately 22 residential lots (however this could be higher depending on the outcome of the split-coded land).



Planning Assessment - Structure Plan Considerations

Public Open Space and Water Corporation Sewer Easement

The surrounding Lake Coogee green spaces are significantly characterised by wetlands and vegetated areas. As the surrounding structure planned areas have evolved, for several reasons, there is a limited amount of usable POS for the purposes of supporting residential development including spaces for families to gather and recreate in.

As a result the City has encouraged the provision of POS and has discouraged a cash in lieu alternative as structure plans are submitted for assessment. Also encouraged is consideration of how individual lots can work together to provide consolidated, functional spaces rather than small, disconnected unusable spaces.

In this context the City did advise in pre lodgement correspondence early on with the land owner the preference for the POS to be located along the northern boundary of Lot 2, so as to enable the consolidation of POS with Lot 1 to the north as it will result in a larger, more useable and active POS (as shown at Figure 4).

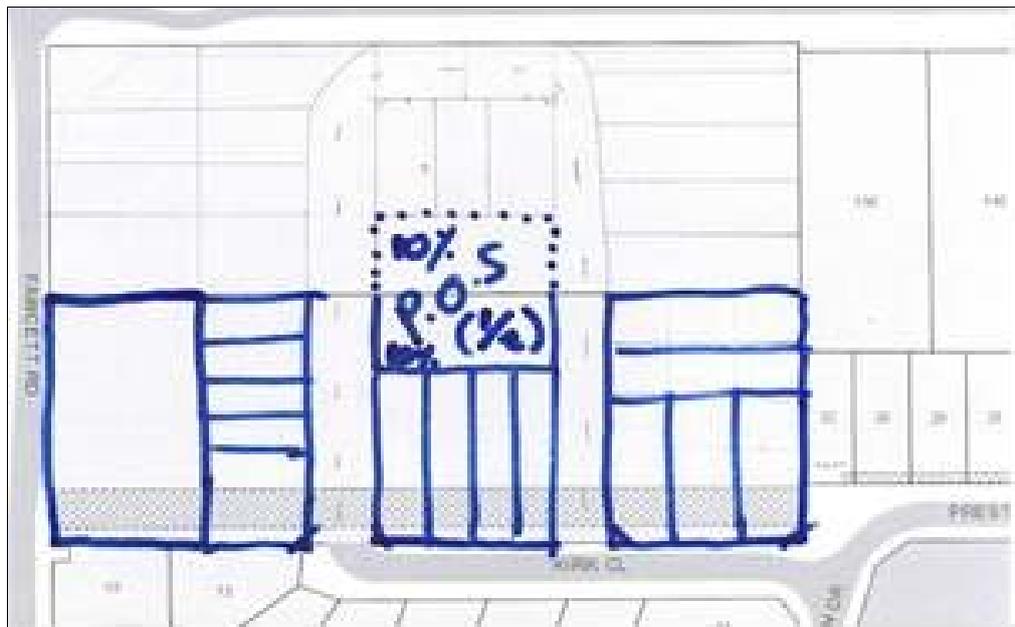


Figure 4: A concept drawing of the combined POS outcome that was requested by City staff as the preferred outcome.

The proponent in this instance however seeks to utilise the large diameter gravity sewer (2,250mm diameter Bibra Lake Main Sewer) alignment that traverses through Lake Coogee and runs through the subject land, contained within a 9m wide easement in favour of the Water Corporation which might otherwise be considered unproductive land from a development perspective.

Given the challenges associated with structures and trees co-located alongside sewer infrastructure City staff have worked with the applicant to locate the POS within the Water Corporation easement area, if Water



Corporation are supportive of the proposal and a good active design can be achieved.

Water Corporation has provided advice, stating that they do not have any objection to the POS location over the mains easement, subject to the POS being embellished in accordance with Water Corporation advice and guidelines. They have also provided further information on how the POS area could be embellished, as follows:

- Seating, paving, garden walls, landscaping and play and exercise equipment (with small/shallow footings) are acceptable over the sewer easement. They can be dismantled and removed in order to access the area for maintenance, if required.
- Trees could be located in the space either side of the Building Restriction Zone (there are a few metres either side of the BRZ that could accommodate certain tree types).

While the POS in this location removes any ability to link in with the future development of Lot 1 to the north, the structure plan includes a POS concept plan that is acceptable to the Water Corporation and the City, noting also the further discussions and proposed changes (relevant to supporting this approach) regarding proposed changes to densities further into this report.

Proposed Lots and Water Corporation Sewer Easement

In addition to the POS alignment, the front setback of lots fronting Kirk Close includes a 10m deep easement as illustrated in Figure 5.

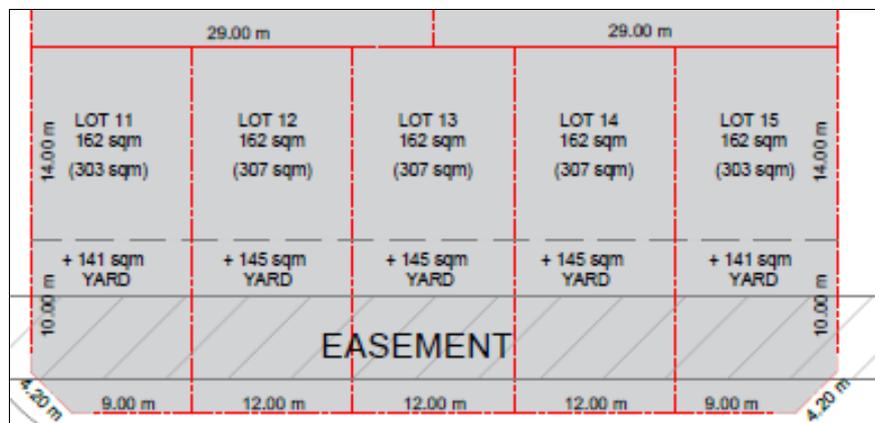


Figure 5: Indicative subdivision plan illustrates a 10m front setback of lots proposed to be an easement to provide for the sewer alignment.

This is not recommended for the following reasons:

1. Water Corporation find this unacceptable as it constrains access for the agency should future maintenance works be required.
2. It is relatively easy for City staff to relocate small structures and landscaping from the POS for easement maintenance, however it will be far more complicated to organise this with a number of landowners on private land, who for example would have their driveway located over the easement.

3. The 12.5m setback impacts rather negatively the urban design outcome which will impact the street and be an obvious maintenance issue.
4. Water Corporation provided advice for what structures cannot be located within the Building Restriction Zone (BRZ) on the private lots, which included the following:

'The purchasers of the affected lots will not be able to plant trees and shrubs, sink garden bores, construct building piling, sheds, walls, improvements or footings within the Building Exclusion zone, or place anything in the area that would prevent the Corporation from readily accessing the sewer, or which would cause damage to the sewer. The existing easement conditions will need to be reinstated on each lot at the subdivision stage.'

It is considered that the easement will have higher amenity and a coordinated approach if it is managed by the City of Cockburn as road reserve. There is the opportunity to make this easement area, in essence, an extension of the POS area, with landscaping, pedestrian walk ways and street furniture, which will have a far greater value to the local community.

Furthermore, given that the City has worked with the applicant on a POS outcome that is not preferable in size or location, the addition of the road reserve located over the sewer easement will provide extra open space for the community and an important wide boulevard style landscaped street design between the wetland located to the east and connecting in with development to the west.

There is a precedent example within the City of Cockburn, where a structure plan has included a 12 metre wide Water Corporation easement; Lot 9002 Prizmic Street, Beeliar, considered within the 'Lots 1001 and 83 Watson Road Structure Plan'. The approved structure plan resulted in the land located over the easement being reserved for the purposes of 'Road Reserve', to be embellished with landscaping by the developer.

It is recommended that the same principle be applied in the proposed Structure Plan, with the proposed R30 designation removed from the land affected by the Water Corporation easement and replaced with 'Road Reserve'.

Proposed R40/60 split code fronting Fawcett Road

The proponent advises the intention for the split-coded land fronting Fawcett Road is to create a mixed-use multiple dwelling development as illustrated in Figure 6. The R60 coding can provide for this outcome however the City commonly finds such a development is difficult to achieve in this and similar locations with similar characteristics when it comes to market conditions and therefore holds concern at the development and design outcome should this option not be pursued.

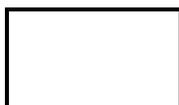




Figure 6: Mixed use option that can be achieved through the R60 slit coding option.

As a result the City is focused on ensuring the alternative split coding is appropriate should a single detached dwelling subdivision be pursued. The proposal in this scenario identifies an R40 coding of which there are some examples scattered around the locality to the east and west.

The built form outcomes under this coding have resulted in no on lot tree provision and very limited outdoor spaces as illustrated in Figure 7.

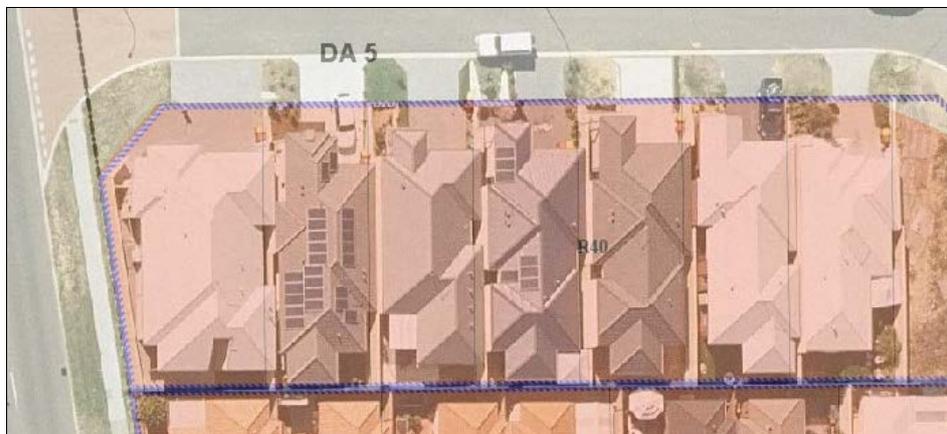


Figure 7: Example of R40 coded development in the local area.

As a result of these common built form and urban outcomes under the R40 coding and in addition to the reduced POS provision (both in terms of reduced size and embellishment options) as described above the City recommends an R30 coding of which can provide an increased lot size to accommodate at least 1 tree, a reasonable outdoor area, reduce the number of cross overs and make an improved contribution to the street by for example providing more opportunities for trees.

This recommendation is consistent with lots on the opposite side of Kirk Close and responds to what the City is trying to achieve in new

development areas; quality urban design that is responsive to the existing urban area, design that will create open community spaces and landscaped areas with tree canopy.

It is recommended that the proposed R60/40 and R40 zoning be modified to R60/30 and R30, which will result in a development that is in keeping with the surrounding neighbourhood and addresses the above mentioned matters.

Bushfire Management Plan

The subject land is located within a Bushfire Prone Area and accordingly a Bushfire Management Plan (BMP) has been prepared as part of the Proposed Structure Plan.

The Proposed Structure Plan was referred to the Department of Fire and Emergency Services (DFES) during the advertising period. No advice has been received from DFES. The BMP demonstrates that the entirety of the site achieves BAL-29 or lower. The recommended strategies within the BMP have been included within the structure plan.

Public and Agency Consultation

The proposed structure plan was advertised from 18 June to 16 July 2020. During this time, 14 submissions were received; four submissions from government agencies/utility providers, eight private submissions of objection and two private submissions of support.

The key issues that were raised during the advertising period were:

- Location of private residential lots located over the Water Corporation sewer easement.
- The unavailability of ground water allocation to irrigate the POS area.
- Increased vehicle traffic and whether the existing road network is able to cater for this.
- Height of the proposed R60 development and impacts on noise and overlooking.
- Proposed density – some submissions suggesting that it is too high and some suggesting that it is too low.
- A lack of open green space.
- The proposed structure plan not including or accounting for the surrounding lots that are also zoned 'Development Area'.
- Damage to surrounding property during the construction period.

The full Schedule of Submissions can be found at Attachment 3.

The issues relating to the Water Corporation sewer easement, POS and density have been addressed above and some other key issues raised in the consultation period are addressed below.



Unavailability of Ground Water Allocation

The subject site is located within the Cockburn Groundwater area, Kogalup Sub Area. DWER have stated that this aquifer is fully allocated and would not be available for use as irrigation of the POS. Discussions between DWER and City staff are ongoing on this matter, as this will likely continue to be an issue raised for any new development areas within the City of Cockburn. A solution raised by the City with DWER in this instance, is for a portion of the water allocation identified for the Port Coogee development be transferred to Lake Coogee.

Increased Vehicle Traffic

A Traffic Impact Assessment (TIA) prepared by KCTT informs and supports the proposed structure plan based on the zoning proposed by the proponent. KCTT concluded that the surrounding network would successfully absorb this 'worst-case scenario' additional traffic.

With the officer recommendation to lower the proposed densities from R60/40 and R40 to R60/30 and R30, the resultant increase in traffic will be reduced again. Furthermore, the future upgrade of Beeliar Drive (Mayor Road) will see significant road modifications and improvements made in this area. It is considered that the small development proposed is unlikely to have significant effects on the overall traffic of this locality.

Height of proposed R60 development and impacts of overlooking

State Planning Policy 7.3 Volume 2 provides for R60 multiple dwellings up to 3 storeys. As a result this relatively lower scale height is not expected to negatively impact the street and surrounding developments.

Furthermore in terms of overlooking, any proposed development at the development assessment stage is required to meet the requirements of the Residential Design Codes, addressed in future development applications and this includes impacts such as overshadowing, amenity impacts and privacy considerations.

Structure Plan responding and providing opportunities to link with surrounding 'Development Areas' yet to be structure planned.

A coordinated and strategic approach to structure planning, which incorporates all Development Areas within the locality is preferred, however the City does not have the ability to require proponents to include surrounding lots within their structure plan. Though the City does actively encourage this and when unable it is requested an indicative structure plan layout be demonstrated to ensure at least 1 suitable option can be provided.



Given that the advertised structure plan did not account for how development could be achieved at 138 and 140 Mayor Road, it was requested that the proponent provide a concept plan of how development could be achieved to the surrounding lots that are zoned Development Area, of which was provided.

Conclusion

Noting the recommendations, it is recommended the proposed Structure Plan be forward to the WAPC for approval.

Strategic Plans/Policy Implications

Local Economy

A sustainable and diverse local economy that attracts increased investment and provides local employment.

- Plan for and facilitate opportunities for local business (including home business and sole traders), local activity centres and industry to thrive.
- Advocate and attract investment, economic growth and local employment.

Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Provide accessible high-quality open spaces and parks for community benefit.

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Plan to provide residents with great places to live, activated social connections and high quality open spaces.
- Plan and facilitate diverse and affordable housing choices for residents and vulnerable communities.

Budget/Financial Implications

N/A

Legal Implications

N/A



Community Consultation

The proposal was advertised for a period of 28 days from 18 June to 16 July 2020. Advertising consisted of a newspaper advertisement in the Cockburn Gazette, Comment on Cockburn and letters to surrounding landowners and affected government agencies.

A total of 14 submissions were received, of which included:

- Eight objections and two support from private submitters; and
- Four government agencies/service providers, with advice.

All submissions are outlined and addressed in Attachment 3.

Risk Management Implications

The Officer Recommendation takes into consideration all relevant planning factors associated with this proposal. It is considered that the Officer Recommendation is appropriate in recognition of making the most appropriate planning decision.

Advice to Proponent(s)/Submitters

The Proponent and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 10 September 2020 Ordinary Council Meeting.

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



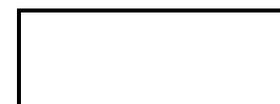
File No. 110/204

SCHEDULE OF SUBMISSIONS
PROPOSED STRUCTURE PLAN : Lot 2 (7) Fawcett Road, Lake Coogee

| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|---|-------------------------|--|-----------------|
| Service Providers and Government Agencies | | | |
| 1 | Telstra | <p>COMMENTS:</p> <p>With regard to your proposed works, Telstra has no objections but may have network in the area that may be impacted by the proposal and may require relocation. Please ensure that you locate all services and protect during your works.</p> <p>I urge you to contact Dial Before You Dig and request some plans showing the assets in the area, once received utilising an Accredited Plant Locator, locate all services and survey to determine if conflicts exist.</p> <p>If you are able to complete your works to avoid the Telstra network and adhere to the standards then please do so otherwise costs will be incurred to relocate the Telstra network accordingly to meet the required standards and practices.</p> | Comments noted. |
| 2 | Department of Transport | <p>SUPPORT:</p> <p>The Department of Transport (DoT) has reviewed the submitted documents and support the shared-path connection through the proposed POS.</p> <p>The Department has no further comment to provide.</p> | Support noted. |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|-----|-------------------|--|---|
| 3 | Water Corporation | <p>COMMENTS:</p> <p>The site is located within the Water Corporation’s planning areas for water and sewerage. Services can be extended from the surrounding networks to the land by the developer undertaking mains extensions at the subdivision stage. The Water Corporation has a large diameter gravity sewer (2,250mm diameter Bibra Lake Main Sewer) that traverses the Munster area generally from north-east to southwest. The main sewer runs through the subject land and is contained within a 9m wide easement in favour of the Water Corporation.</p> <p>This section of the Bibra Lake main sewer was installed in 2002. At the time of planning and designing the sewer, the Water Corporation based the route for the sewer along existing and proposed roads and public open space in accordance with then adopted and draft structure plans for the area. The main sewer route is reflected in the City’s ‘Munster – Phase 3’ overall Local Structure Plan, as well as other more detailed structure plans and subdivision layouts that have since been approved through the Munster area. The sewer is mostly contained within public road reserves and POS. Where it traverses currently undeveloped land (including Lot 2), the sewer has been aligned generally along proposed future roads and open space based on information obtained from the City and landowners at that time.</p> <p>The sewer is 5-6m deep through the subject land. While unlikely, the Water Corporation may at some point in the long term need to excavate down to the sewer for maintenance purposes. The draft subdivision layout depicted on the</p> | <p>Comments are noted, specifically:</p> <ol style="list-style-type: none"> 1. That the main sewer was installed in 2002 along existing and proposed roads and public open space in accordance with existing structure plans and development. 2. That the Water Corporation may at some point in the long term need to excavate down to the sewer for maintenance purposes. 3. That private residential lots located over the easement is not the Water Corporation’s preferred solution, as this will constrain access to the sewer if required in the future. 4. That the Corporation’s preference is that the layout should be redesigned to accommodate the sewer in POS or a road reserve. 5. Permanent mass structures that cannot easily be removed must not be located over the mains easement. Subject to this, Water Corporation does not object to the POS location. 6. Seating, paving and garden walls are acceptable over the sewer easement. 7. Play and exercise equipment will also be acceptable in the area. The footings for these structures would typically be relatively small and shallow. 8. The minimum Building Restriction zone |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|-----|--------------|--|---|
| | | <p>structure plan places the main sewer through the front yards of several residential lots ranging from 260m2 to 300m2. This arrangement is not the Water Corporation's preferred solution, as this will constrain access to the sewer if required in the future.</p> <p>The Corporation's preference is that the layout should be redesigned to accommodate the sewer in POS or a road reserve. However, it is understood from discussions with the proponents that this has been considered and that additional POS is not required in the immediate locality and that duplication or relocation of the road reserve for Kirk Close is not viable. It is understood that the loss of additional land/lots to properly accommodate the sewer in a reserve is not financially feasible for the developer.</p> <p>In the event that the City and the DPLH adopt the LSP in its current form, the developer has advised that a more detailed Development Plan/design guidelines will be put in place over the affected lots in order to ensure that the building envelopes for these lots are located outside the easement, to support the use restrictions contained in the Water Corporation's easement, and to ensure future land owners are aware of the use limitations over the easement area.</p> <p>The easement registered on the title of the Lot 2(7) Fawcett Rd is only 9m wide straddling the sewer. In accordance with the minimum requirements in the Water Corporation's <i>Technical Guidelines for Safely Working Near Water Corporation Assets</i> the width of the 'Building Restriction Zone' is the minimum width required to excavate the pipeline using trench supports to safely undertake emergency</p> | <p>around this sewer is 6.25m either side of the pipe centreline.</p> <p>The officer recommendation and proposed modifications to the structure plan respond to the issues raised by Water Corporation and incorporate their recommendations.</p> |



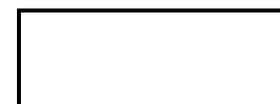
| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
|-----|--------------|--|----------------|
| | | <p>maintenance and repairs, which depends on the size and depth of the pipeline. Under the Technical Guidelines the Building Restriction Zone measured from the centreline of this sewer is at least 4m plus the diameter of the pipe i.e. at least 6.25m either side of the pipe centerline, which is in excess of the easement width (which is only 4.5m either side). The Structure Plan and subdivision layout will therefore need to achieve the minimum Building Restriction Zone. A detailed scaled plan should be included in the LSP report to demonstrate how this can be achieved on the proposed subdivision layout.</p> <p>The purchasers of the affected lots will not be able to plant trees and shrubs, sink garden bores, construct building piling, sheds, walls, improvements or footings within the Building Exclusion zone, or place anything in the area that would prevent the Corporation from readily accessing the sewer, or which would cause damage to the sewer. The existing easement conditions will need to be reinstated on each lot at the subdivision stage.</p> <p>Included Attachments</p> <p>Further comments provided as a response to officer queries:</p> <p>Our Building Services team has previously provided the following advice to Urbanista regarding the proposed POS over the sewer easement:</p> <p>As long as there are no permanent mass structures that cannot easily be removed, the Water Corporation has no</p> | |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>objections to the proposed location of the POS. Seating, paving and garden walls are considered as structures exempt from WC building approval and are acceptable over the sewer easement. They can be dismantled and removed in order to access the area for maintenance and repair to the sewer main, if required.</p> <p>Play and exercise equipment will also be acceptable in the area. The footings for these structures would typically be relatively small and shallow.</p> <p>The minimum Building Restriction zone around this sewer is 6.25m either side of the pipe centreline.</p> | |
| 4 | Department of Water and Environmental Regulations | <p>6 August 2020:</p> <p>We are currently reviewing the document, however in the interest of time I have noted some initial aspects of the plan that would require some clarification following the Departments previous advice.</p> <p><i>Commercial area drainage</i> – there does not appear to be reference to drainage assumptions of the commercial area and how this impacts public open space (POS). There is mentions of 2 soakwells however connectivity to (and capacity to manage) the commercial precinct, and how this impacts POS in critical events have not been discussed. The role of POS in managing stormwater is important to confirm at the structure planning stage to ensure adequate land is set aside for proposed configurations and usability to be achieved. It is also noted no water quality treatment is</p> | <p>A requirement of State Planning Policy 2.9: Water Resources and Better Urban Water Management, is that structure plans are accompanied by a Local Water Management Strategy (LWMS). In some instances, a Stormwater Management Plan (SMP) may be accepted instead, subject to approval by DWER and the City. Given this site is small in nature with clearance to groundwater and no water dependent ecosystems, DWER and City officers have accepted the SMP prepared by the proponent. Modifications were requested by both DWER and the City, and these modifications have been incorporated into the final report.</p> <p>DWER also raised the issue of POS irrigation.</p> |



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| | | <p>proposed at all.</p> <p><i>Non-potable resource</i> – The Departments previous comments requested a source be confirmed for irrigation and dust suppression requirements. This has not been done. This is also important at this stage to ensure areas proposed as green space can in fact be kept green.</p> <p>7 August 2020:</p> <p>Yes upon learning the commercial area is not considered in the Local Structure Plan area, we would consider this resolved.</p> <p>With regard to the POS irrigation issue I can advise that the site is in the Cockburn Groundwater area, Kogalup Sub Area as proclaimed under the Rights in Water Irrigation Act 1914. Unfortunately, this aquifer is fully allocated, and as such no local groundwater resources are available to irrigate this POS.</p> <p>As such, the following needs to be undertaken.</p> <ul style="list-style-type: none"> • Confirm the amount of water required for irrigation of this are per year (generally POS on average is irrigated at 6,750 kL/ha/year); • Seek to resolve the issue though one of the following approaches: • Secure a groundwater entitlement through a water | <p>The subject site is located within the Cockburn Groundwater area, Kogalup Sub Area. DWER have stated that this aquifer is fully allocated and would not be available for use as irrigation of the POS.</p> <p>Discussions between DWER and City staff are ongoing on this matter, as this will likely continue to be an issue raised for any development areas within the City of Cockburn. A solution raised by the City with DWER in this instance, is for excess water allocation from Port Coogee to be reallocated to Lake Coogee.</p> <p>Discussions between DWER and City staff are ongoing on this matter, as this will likely continue to be an issue raised for any new development areas within the City of Cockburn. A solution raised by the City with DWER in this instance, is for a portion of the water allocation identified for the Port Coogee development be transferred to Lake Coogee.</p> <p>In regards to the subject structure plan, a dry park design is not a preferable option given the restrictions already placed upon the POS and that it is the City's preference for this POS is for it to be embellished specifically for 0-7 year old children, thereby requiring grass (and irrigation).</p> |



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| | | <p>trade agreement with an existing groundwater licence holder in the Kogalup Sub Area;</p> <ul style="list-style-type: none"> • Confirm with City of Cockburn that irrigation will be possible utilising a proportion of any existing licence they may have in the area (this may also be conditional upon the developer installing the bore and infrastructure); • Confirm with City of Cockburn that a dry park design is an amenable approach; • Confirm with City of Cockburn that irrigation via scheme water is an amenable approach, noting the City will eventually have to take on this cost upon handover of POS. <p>Please note, the above communication with the City is to be undertaken by, or on behalf of, the proponent...not by the Department.</p> <p>I hope this information is useful. Please feel free to contact me with any queries.</p> <p>13 August 2020:</p> <p>As you may be aware, the requirement to operate the groundwater interception drain (GID) is in accordance with the relevant Waterways Environmental Management Plan and is tied to nitrogen concentrations as per the ANZECC</p> | |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>(2000) low reliability trigger for marine ecosystems. However, with the submitted Port Coogee Annual Groundwater Monitoring Summary 2018-2019, it appears that the City of Cockburn is not currently operating the system within its primary intended purpose, as there was zero re-injection despite nitrogen levels in the intercepted groundwater exceeding ANZECC (2000) trigger levels.</p> <p>The department is prepared to review the City of Cockburn groundwater licences for the Kogalup subarea and subsequently to free up some of the water from the Port Coogee licence for other locations within the same resource. For this we request information on the future of the GID, including any current commitments and obligations for the City of Cockburn for its operation, and on the current and future water use requirements for the non-potable water supply in the Port Coogee area. Furthermore, please provide updated information on the City's additional water requirements arising from current and future developments within the wider Kogalup subarea.</p> <p>19 August 2020:</p> <p>The stormwater plan was accepted as a manner to satisfy requirements of State Planning Policy 2.9: Water Resources and Better Urban Water Management (WAPC, 2008) as a substitute for a Local Water Management Strategy (which would be more comprehensive report) given this site is small in nature with clearance to groundwater and no water dependent ecosystems.</p> <p>The issue of non-potable supply was raised in initial comments, and should be resolved within either the</p> | |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>Stormwater Plan or the Engineering Servicing Report that will support the Local Structure Plan.</p> <p>Please note the City of Cockburn will also be required to confirm satisfaction with the plan, given they will be the managing authority of resultant infrastructure.</p> | |
| Community Submissions | | | |
| 5 | Doris Dengel 34 Preston Drive LAKE COOGEE | <p>OBJECT:</p> <p>As a local resident right next to the proposed area I don't support the proposal as it would make the area too busy and increase the local traffic.</p> | <p>Objection noted.</p> <p>Should the site be developed there will always be an impact on traffic, as with any proposed structure plan that proposes an increased number of residents or commercial uses. Therefore, a standard requirement is that structure plans be accompanied by a Traffic/Transport Impact Statement (TIS).</p> <p>A TIS was prepared for the proposed structure plan based on the zoning proposed by the proponent. The outcomes of this were the following:</p> <p><i>It is expected that the proposed development will generate up to 723 VPD; 37 VPH in the AM peak and 65 VPH in the PM peak. This is considered as a worst-case scenario, given KCTT have assumed that the entire commercial part of the development will be retail with significant food component.</i></p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | | <p><i>According to WAPC Guidelines 65 VPH is considered as a moderate impact to the surrounding network. KCTT believe that the surrounding network would successfully absorb this 'worst-case scenario' additional traffic.</i></p> <p>Given that officer recommendations are to lower the proposed densities from R60/40 and R40 to R60/30 and R30, this proposed traffic increase will be reduced again. Furthermore, the future upgrade of Beeliar Drive (Mayor Road) will see significant road modifications and improvements made in this area. It is hoped that the future road upgrades will include improvements to public transport also. Regardless, the small development proposed is unlikely to have significant effects on the overall traffic of this locality.</p> |
| 6 | Confidential | <p>SUPPORT:</p> <p>I have a few questions regarding the development as my property shares a boundary with lot 1</p> <ul style="list-style-type: none"> - what is the intended finished height for each of the blocks on the eastern edge of lot 1? - there is currently a retaining wall on the boundary of my property and lot 1. Will this be replaced by a limestone block wall or remain as is? - would the developer be willing to construct a fence on our shared boundary as this is something we were | <p>Support noted.</p> <p>In regards to the specific details of finished development levels, this level of detail is generally not required during the structure plan process. At this stage of structure planning, the process is to assign zoning (e.g. residential or commercial) and reserves (such as public open space and roads). Finished height levels of the development site will be identified, assessed and approved closer to the subdivision application stage. However the following</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>considering doing before the end of 2020, but will hold off for now as we assume this may have to come down for earthworks.</p> <ul style="list-style-type: none"> - our house sits at 12m. Will the eastern end of lot 1 be excavated deeper than this? If so, what provisions are in place to protect our house and property from damage? - what general provisions are in place to protect our property from noise, dust and debris/damage? - Will excavation of our council crossover/verge be required for connecting utilities - we currently have an easement partially across the front of our property. Will this be extended in order to provide provisions to the new development? | <p>additional comments have been provided by the planning consultant in relation to this matter:</p> <p><i>'The client has confirmed that the site is relatively flat so that there will be not much change in levels, although he cannot provide FFL's at this point in time as engineering has not been finalised. However, a feature survey has now been completed which illustrates that the height of the retaining wall to the property on the south is 10.85 and the height of the retaining wall adjacent to the adjoining eastern property is 9.53 (these are marked on the survey in yellow). As you will see from the green spot levels, the adjoining southern and eastern properties, currently sit at a higher level than the subject property.'</i></p> <p>The survey plan has been provided to the submitter.</p> <p>Details such as retaining and fencing are not determined at the subdivision stage. The submitter has been recommended to contact the proponent, should they wish to discuss the matter of retaining and fencing treatments.</p> <p>In regards to provisions to protect house and property damage, this is a civil matter addressed between the developer and</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | | <p>residents. Damage to surroundings properties should be avoided and where damage does occur, it is at the developer's expense to fix.</p> <p>This matter is given consideration at the subdivision and development application stages – and there are guidelines in place to protect surrounding properties from noise and dust.</p> <p>The Water Corporation sewer easement already extends over the subject lot, with sewer infrastructure located beneath the easement.</p> |
| 7 | Sam Cook 15 Fawcett Road, LAKE COOGEE | <p>OBJECT:</p> <p>Hi. We live at 15 Fawcett Road next door to the proposal. We reject the proposal based on the below.</p> <ol style="list-style-type: none"> 1. Footpath is proposed in our front yard, We have spent a lot of time and effort to maintain our verge, Can this be moved the street as plenty of other options on it. Please see attached photo of front of the property. We already have an ugly transformer that was installed this would devalue our property even further with this footpath. 2. The proposed footpath will go along the POS and our fence line, Can this be relocated. 3. All the developments behind our property only have 1 exit which is Fawcett road, too many cul-de-sac's with no exits. These need to exit on Mayor Road. <p>Happy to discuss further our concerns.</p> | <p>Objection noted.</p> <p>In regards to the comments about increased traffic congestion, please refer to officer comments at Submission 5.</p> <p>Specifically in regards to requiring access onto Mayor Road, this is not a preference of the City's, as Mayor Road is identified as a 'Distributor B' road by Main Roads and is reserved for a future realignment and upgrade of Beeliar Drive, to facilitate a key east-west link for the City of Cockburn. Additional road intersections onto Mayor Road are not preferable. It also cannot form part of this structure plan, as the lot to the north (Lot 1, No. 5 Mayor Road) does not form part of this structure plan area.</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>Additional comments/questions provided by submitter at a later date:</p> <p>I have some questions on the Proposed Structure Plan:</p> <ol style="list-style-type: none"> 1. The Planning team are putting in a lot of Cul De Sac's in the area and in turn are causing all the traffic to turn onto Fawcett Road with no other exit. The structure plan/plans behind us need to incorporate roads exiting onto Mayor Road. The road is busy enough with 1,000 cars a day(Please refer to the latest volume of car numbers the city recently completed) this was cause more vehicles onto the road exiting onto Fawcett + the recently approved development across the road will bring in another 30-45 cars. 2. Myself and neighbour agree we do not want a footpath butted up against our fence line. Can the footpath please be pushed further away from our fence lines. Reason behind this: People jumping the fences/hitting the fences/graffiti etc. 3. The proposed café is a great idea, but how high will the units be? Will they be able to see into our backyard (15 Fawcett road) because currently we can see the single story house without units. Have we thought about noise for residents? 4. Proposed footpath outside our property shows (2) different locations in the plan, our preference would be to move this footpath to align with Page 27 of the proposed plan (Photo below). Can this please be confirmed? The location proposed outside our property does not make sense knowing the lay of the | <p>Concern regarding the location of the footpath is noted, and it is recognised that a consistent location has not been applied throughout the structure plan document, rather different locations and footpath crossovers are presented throughout the document. However, the footpath location shown on the structure plan map is only 'indicative' and will be given further consideration in the subdivision, local development plan and landscaping plan processes. The comments are noted and will be considered at a more detailed planning stage.</p> <p>That level of detail (building height) is not assigned at the Structure Plan stage, however the R-Coding is (density as per the Residential Design Codes) and would guide what building height might be possible on the site. The applicant has proposed a density of R40/R60 at this lot, with the higher density subject to conditions (such as, the development must be multiple dwellings, not single houses or grouped dwellings). It is the position of City officers that this density should be revised to R30/60 to create better urban design outcomes. It is likely that if the density of R60 is developed (subject to the specific design provisions), then a two to three-storey mixed use development will be the outcome.</p> <p>In terms of overlooking, any proposed development would have to meet the</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | land and the approval of the new development and where it should be located so people use it. | <p>requirements of the Residential Design Codes in regards to this matter. This would be addressed in future development applications.</p> <p>Noise will be a consideration if a commercial component to the development is proposed.</p> |
| 8 | Tobias Busch 29 Dodd Street HAMILTON HILL | <p>OBJECT:</p> <p>R30 density is too low in this area to achieve urban infill densities. Consider R40. The indicative development layout showing typical proposed building envelopes is undesirable. It exceeds site cover does not allow space for planting, does not facilitate passive solar design, access to private outdoor space. The lot layout has been tested against a built form outcome that demonstrates poor design and will most likely not meet the new R codes currently developed by Design WA. While this does not mean that the subdivision won't 'work' for better design outcomes than those indicated in this proposal, the overall structure plan should be reviewed with this in mind.</p> | <p>Objection noted.</p> <p>It is considered that R30 is in fact a far more appropriate density than R40, throughout the structure plan area. R30 is more compatible with the surrounding residential areas and will result in wider frontages and more landscaped areas, conducive to a good neighbourhood design. R40 density as a single house or grouped dwelling typology, often results in undesirable outcomes of</p> <p>All residential development will be required to meet the requirements of the Residential Design Codes (R-Codes).</p> |
| 9 | Confidential | <p>OBJECT:</p> <p>I am very concerned with the extra traffic that will be exiting Fawcett road into Major Road once the future houses are built. It is often difficult turning into Major Road from Apium Mews especially weekends and Christmas holidays with the extra traffic with attached boats. Also, at present there is no pedestrian crossing across Major Road from Apium Mews</p> | <p>Objection noted.</p> <p>In regards to traffic concerns, please refer to officer comments under Submission 5.</p> <p>In regards to the matter of a pedestrian crossing from Apium Mews to cross Mayor Road. This is not a matter that can be addressed within the</p> |



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| | | and the road must be crossed to gain access to the footpath and it is a 60 kilometres road. | proposed structure plan, as it is not located close to the subject site. There is some footpaths either side of Mayor Road and should this road be upgraded in the future, pedestrian access will be a consideration. |
| 10 | Confidential | <p>OBJECT:</p> <p>There is significant lack of open green space, community areas/ shared Areas and too much land for road without active pursuit of green transport areas.</p> | <p>Objection noted.</p> <p>The proposed structure plan provides the minimum public open space requirement of 10 percent. The proposed road network is consistent with the requirements of the Residential Design Codes and Liveable Neighbourhoods.</p> |
| 11 | Confidential | <p>OBJECT:</p> <p>R40/60 is too high density for Fawcett Rd given the increasing traffic usage of the road as the surrounding area develops and is putting an increased demand on the lack of curb parking and an increased risk of injury due to the high pedestrian use in the area to access Lake Coogee.</p> | <p>Objection noted.</p> <p>It is the officer's recommendation to require the proposed densities of R40/60 and R40, be modified to R30/60 and R30 respectively.</p> <p>The higher coding of R60 is conditioned in Section 1 of the proposed structure plan report to only be permitted under specific provisions. These are designed to achieve a mixed-use development with high amenity. The provisions that have been included are:</p> <ul style="list-style-type: none"> • That the split-coded land is subdivided to no more than one lot, prior to a development application being approved over the site; |



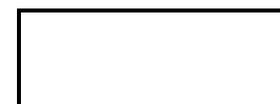
| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | | <ul style="list-style-type: none"> • The development site consists of a total land area of no less than 1,400m2. • It can only be achieved in a mixed-use development with multiple dwellings and a ground floor commercial component. • The proposed development does not consist of any single house of grouped dwelling development. • the proposed development consists of two-storey (or greater) height development (subject to development standards). • A local development plan is prepared. |
| 12 | Confidential | <p>OBJECT:</p> <p>I am submitting this comment with authority on behalf of both adjoining lots at #138 & #140 Mayor Rd Lake Coogee. Regarding the proposed SP and indicative future plans to develop the adjoining lots of 7 & 5 Fawcett Rd the owners Object to the current SP for the following reasons:</p> <ul style="list-style-type: none"> - The proposed SP does not include or consider 138 or 140 Mayor Rd. Both owners would like to see their lots included in this plan due to its direct impact on their lots and the future development of this area as the only source of access currently supported by the City. The SP in its current form affects their property values negatively and is not in keeping with the City's own comments on the matter (Being that the City would like to see a SP including all lots at 5 & 7 Fawcett and 138 & 140 Mayor Rd). The owners are willing to | <p>Objection noted.</p> <p>A coordinated and strategic approach to structure planning, which incorporates all development areas within the locality is preferred, however the City does not have the ability to require proponents to include surrounding lots within their structure plan.</p> <p>Given that the advertised structure plan document did not account for how development could be achieved at 138 and 140 Mayor Road, it was requested that the proponent provide a concept plan of how development (in particular access) could be achieved to the surrounding lots that are zoned Development Area. A concept plan has now been included within the report at page 55, which indicates how access</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>discuss with the applicant the prospect of including their properties in this SP overview for future submission.</p> | <p>could be achieved to 5 Mayor Road, 138 Mayor Road and 140 Mayor Road, without access via Mayor Road. This concept plan is not binding and other outcomes could be achieved, however it provides the land owners to the north with some conceptual ideas of how their land could be developed in the future.</p> |
| 13 | Confidential | <p>SUPPORT:</p> <p>I am really excited for the proposed development. My family and I love the ideas of the public open space that connects Kirk Close and Fawcett so there is direct access from lake to lake, we are hoping for lots of greenery, playground or gym equipment. We also are really excited for the potential of a cafe/shop area as we do not have anything of reasonable walking distance in this area, having a cafe overlooking the lake and in walking distance will be a really nice feature there is nothing like this in the Cockburn area. We are hoping that figure 20 alternative lot layout of the housing might be considered... the other proposed layout is too squashed in, it will make parking on Kirk very difficult, its already very hard with parking on Kirk now especially at the top end. We look forward to seeing what happens in this area.</p> | <p>Support noted.</p> <p>In regards to the concept lot layouts presented in the proposed structure plan report at Figure 20, officers are in agreement. A lot layout design that runs parallel to the easement will facilitate a better outcome in regards to a more open design and less pressure on the vehicle entry points to Kirk Close. Officers have recommended that the easement area be reserved as 'road reserve', which will likely facilitate a development outcome similar to the concept plan at Figure 20.</p> |
| 14 | Kevin & Rosemary Clark 140 Mayor Road, Lake Coogee | <p>OBJECT:</p> <p>Our house at 140 Mayor Road Lake Coogee sustained significant damage due to the construction of a previous subdivision to the east and south of our property. As such we engaged the services of David Wills & Associates Consulting Engineers (DWA) for their professional opinion, to review the</p> | <p>Objection noted.</p> <p>The proposed structure plan does not permit any development, rather it simply assigns zoning and reserves.</p> <p>Issues in regards to construction management</p> |



| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <p>Proposed structure plan - Lot 2 (No. 7) Fawcett Road, Lake Coogee to assess the impact that construction may have on our house.</p> <p>DWA's assessment and recommendations are in the letter attached.</p> <p>Should council support the Proposed structure plan - Lot 2 (No. 7) Fawcett Road, Lake Coogee, for the protection of our house at 140 Mayor Road Lake Coogee, being subject to further damages, we seek that council would request that the support for the Proposed structure plan - Lot 2 (No. 7) Fawcett Road, Lake Coogee would be subject to the inclusion of DWA's recommendations as follows:-</p> <ul style="list-style-type: none"> • That vibration monitoring equipment be installed at 140 Mayor Road on the subdivision side of the lot before the contractor mobilises to site. • That vibrations at 140 Mayor Road be limited to 2.5mm/sec, and that construction equipment be selected to achieve this limit. • On the day compaction commences on lot 2 vibration readings be submitted to the owners of 140 Mayor Road by 8:00 the following morning. Thereafter, weekly readings are to be taken and submitted by noon of the day after the readings are due. If this is a weekend day, the reading period shall be adjusted so the results are received no later than noon of every Wednesday. | <p>will be addressed in the subdivision application and engineering works approvals.</p> <p>Should property damage result, this is a civil matter addressed between the developer and residents. Damage to surroundings properties should be avoided and where damage does occur, it is at the developer's expense to fix.</p> |

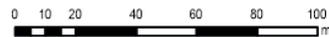


| NO. | NAME/ADDRESS | SUBMISSION | RECOMMENDATION |
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| | | <ul style="list-style-type: none"> That where vibration readings exceed 2.5mm/sec at 140 Mayor Road, compaction equipment be changed to bring the vibrations down to at or below 2.5mm/sec, and daily readings from the vibration monitoring equipment shall be submitted until the subdivision is completed. Failure to do so shall result in a stop work order being placed on the work site at lot 2. <p>If these recommendations are declined to be included into the plan then we would object to the plan.</p> <p>Included Attachments</p> | |





FIGURE 2 — SITE PLAN



SCALE 1:2,000 @ A4



LOT 2 FAWCETT ROAD LAKE COOGEE

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14.3 (2020/MINUTE NO 0190) ACQUISITION AND DEDICATION OF LAND AS ROAD - PORTION LOT 316 (NO 287) JANDAKOT ROAD, JANDAKOT

Author(s) L Gatt

Attachments 1. Lot 316 Jandakot Road - Sketch

RECOMMENDATION

That Council:

- (1) authorise the Chief Executive Officer to request the Minister for Lands to dedicate the portion of Lot 316 and using s168 of the *Planning and Development Act 2005* to acquire the land and s56 of the *Land Administration Act 1997*;
- (2) agrees to indemnify the State of Western Australia and the Minister for Lands; and
- (3) in order to secure early payment to landowners and early access to land for the public works, the basis of an agreement for compensation will be subject to those arrangements being secured through the subdivision process.

COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr P Corke

That the recommendation be adopted.

CARRIED 8/1

Cr Smith requested that her vote against the motion be recorded in the minutes.

Background

Council at its meeting of 18 July 2018 (Minute No 0103 refers) resolved to request the Minister endorse Option 1 as the preferred design option for the Jandakot Road/Solomon Road Public Works Project to facilitate the upgrade of Jandakot Road. The Option 1 design did not include land that is now needed to be acquired from Lot 316 Jandakot Road.

Stage 1 of the construction works are complete and stage 2 of the works are unable to progress until the land acquisition process has been finalised. This includes finalising the further comment period currently being undertaken for the Notice of Intention to Take (NOIT) of which closes on 1 September 2020.

The City anticipates the land acquisition to be finalised shortly and construction to commence in in the coming months.



Submission

N/A

Report

The initial design for the upgrading of Jandakot Road prior to the adoption of Option 1 included a 3m² corner of Lot 316. As with all road widenings and land acquisitions, the City makes every effort to avoid any land acquisitions wherever possible, especially for any relatively minor areas if possible. Based on this, the 3m² corner was initially excluded from Option 1.

Stage 1 of the Jandakot Road Widening works have now been completed and site measurements taken indicate that to maintain the required sight distance at the intersection of Solomon Road and Jandakot Road, this 3m² piece of land is required (see Attachment 1). A decision of Council is required for any land acquisition.

A valuation was undertaken at the time of the initial design to assess the applicable compensation and the landowners of Lot 316 agreed to accept the compensation amount offered. The landowners have agreed to accept the same compensation amount previously agreed of \$33,150, this amount includes injurious affection.

The City at its cost has to date constructed a new driveway and installed colorbond fencing at Lot 316, and will ensure that reinstatement works will be carried out following the acquisition and new construction works. No costs for the acquisition of the land, including legal costs, will be borne by the land owners.

It is recommended that Council consent to the acquisition of the 3m² of land from the owners of Lot 316 to maintain the required sight distance at the intersection of Solomon Road and Jandakot Road.

Strategic Plans/Policy ImplicationsCommunity, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

- Facilitate and advocate for increased community safety.

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Advocate and plan for reduced traffic congestion.
- Plan, develop and advocate for safe, sustainable, integrated local transport networks, public transport and regional transport networks.



Budget/Financial Implications

The money is with McLeods pending settlement. Therefore there are no implications to the municipal budget.

Legal Implications

The *Land Administration Act 1997* and the *Planning and Development Act 2005* will be utilised to acquire and dedicate the road land.

Community Consultation

N/A

Risk Management Implications

The risks to the City if the recommendation is not followed or deferred is that the Jandakot Road Solomon Road round-a-about intersection maybe unsafe, and place the community at risk.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



15. FINANCE AND CORPORATE SERVICES DIVISION ISSUES

15.1 (2020/MINUTE NO 0191) PAYMENTS MADE FROM MUNICIPAL AND TRUST FUND - JULY 2020

| | |
|--------------------|---|
| Author(s) | N Mauricio |
| Attachments | 1. Payments Listing July 2020 2. Credit (Purchase) Cards Summary June 2020 |

RECOMMENDATION

That Council receive the list of payments made from the Municipal and Trust Funds for July 2020, as attached to the Agenda.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

Council has delegated its power to make payments from the Municipal or Trust fund to the CEO and other sub-delegates under LGAFCS4. Regulation 13(1) of the *Local Government (Financial Management) Regulations 1996* requires a list of accounts paid under this delegation to be prepared and presented to Council each month.

Submission

N/A

Report

A listing of payments made during July 2020 with a net total of \$16,973,581 is attached to the agenda for review. This comprises:

- EFT payments list (trade suppliers and others) - \$14,062,913;
- Payroll payments summary - \$2,817,454;
- Corporate credit card expenditure - \$78,746; and
- Bank and merchant fees - \$14,466.

Also attached is a separate listing of credit card spending during the month of June (settled at bank in July), grouped by each card holder. This includes the transaction details for the Acting CEO spend total of \$741.23. This is being reported in line with an Office of the Auditor General "better practice" recommendation, given the CEO role reports directly to Council.

Payments made for monies held in Trust are initially made from the Municipal Fund and then reimbursed by the Trust Fund.



Strategic Plans/Policy ImplicationsListening and Leading

Deliver sustainable governance through transparent and robust policy and processes.

Ensure sound long term financial management and deliver value for money.

Budget/Financial Implications

All payments made have been provided for within the City's annual budget as adopted and amended by Council.

Legal Implications

This item ensures compliance with S6.10(d) of the *Local Government Act 1995* and Regulations 12 and 13 of the *Local Government (Financial Management) Regulations 1996*.

Community Consultation

N/A

Risk Management Implications

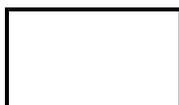
Council is receiving the list of payments already made by the City under delegation in meeting its contractual obligations. This is a statutory requirement and allows Council to review and question any payment that has been made.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



JULY 20 PAYMENT LISTING

MUNICIPAL & TRUST FUND

| PAYMENT No. | ACCOUNT No. | PAYEE | PAYMENT DESCRIPTION | DATE | VALUE \$ |
|-------------|-------------|---|---|------------|------------|
| EF131408 | 99997 | FAMILY DAY CARE | FDC PAYMENT WE 28/06/2020 | 2/07/2020 | 51,007.00 |
| EF131409 | 99997 | IN HOME CARE PAYMENTS | IHC PAYMENTS WE 28/06/20 | 2/07/2020 | 13,150.20 |
| EF131410 | 26987 | CTI RISK MANAGEMENT | SECURITY - CASH COLLECTION | 7/07/2020 | 4,553.55 |
| EF131411 | 10152 | AUST SERVICES UNION | PAYROLL DEDUCTIONS | 13/07/2020 | 1,091.70 |
| EF131412 | 10154 | AUSTRALIAN TAXATION OFFICE | PAYROLL DEDUCTIONS | 13/07/2020 | 442,178.00 |
| EF131413 | 10305 | CHILD SUPPORT AGENCY | PAYROLL DEDUCTIONS | 13/07/2020 | 1,418.91 |
| EF131414 | 11001 | LOCAL GOVERNMENT RACING & CEMETERIES EMPLOYEES UNION LGRCEU | PAYROLL DEDUCTIONS | 13/07/2020 | 82.00 |
| EF131415 | 11857 | CHAMPAGNE SOCIAL CLUB | PAYROLL DEDUCTIONS | 13/07/2020 | 412.00 |
| EF131416 | 11860 | 45S CLUB | PAYROLL DEDUCTIONS | 13/07/2020 | 14.00 |
| EF131417 | 19726 | HEALTH INSURANCE FUND OF WA | PAYROLL DEDUCTIONS | 13/07/2020 | 1,454.97 |
| EF131418 | 25987 | TOYOTA FLEET MANAGEMENT | PAYROLL DEDUCTIONS - NOVATED LEASE | 13/07/2020 | 608.14 |
| EF131419 | 27874 | SMARTSALARY | SALARY PACKAGING/LEASING ADMINISTRATION | 13/07/2020 | 12,340.90 |
| EF131420 | 99997 | DESMOND O'BRIAN | SENIOR SECURITY SUBSIDY SCHEME | 13/07/2020 | 300.00 |
| EF131421 | 99997 | CURTIN UNIVERSITY | SPONSORSHIP - CURTIN IGNITION 2020 | 13/07/2020 | 7,150.00 |
| EF131422 | 99997 | DEPARTMENT OF JUSTICE | WEST COAST SKIN & HIDE 305 & 306 CULTER COMPENSATIO | 13/07/2020 | 985,783.78 |
| EF131423 | 99997 | CHARLES IAN SULLIVAN | REIMBURSEMENT OF MEMBERSHIP FEES | 13/07/2020 | 666.50 |
| EF131424 | 27492 | SUPERCHOICE SERVICES PTY LIMITED | PAYROLL DEDUCTIONS | 16/07/2020 | 565,712.01 |
| EF131425 | 26987 | CTI RISK MANAGEMENT | SECURITY - CASH COLLECTION | 14/07/2020 | 2,112.10 |
| EF131426 | 99996 | IVANA LUKIC | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 576.00 |
| EF131427 | 99996 | ROBERT WILLIAM SHEEHAN | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 30.00 |
| EF131428 | 99996 | SAMUEL EDGE | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 150.00 |
| EF131429 | 99996 | SHANA WONG | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 150.00 |
| EF131430 | 99996 | YELENA FIRINSHTIYN | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 150.00 |
| EF131431 | 99996 | RYAN SOARES | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 150.00 |
| EF131432 | 99996 | ELIZA BRANCH | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 100.00 |
| EF131433 | 99996 | ANDANTINO PTY LTD | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 56.65 |
| EF131434 | 99996 | KEITH LAMPLUGH | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 56.65 |
| EF131435 | 99996 | SCRIBE DESIGN GROUP | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 118.30 |
| EF131436 | 99996 | DEBORAH BATTY | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 167.35 |
| EF131437 | 99996 | VANESSA BARNARD | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 30.00 |
| EF131438 | 99996 | DEBORAH DICKSON | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 21.25 |
| EF131439 | 99996 | CHRISTOPHER WOODWARD | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 150.00 |
| EF131440 | 99996 | TOMAHAWK PROPERTIES PTY LTD | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 995.00 |
| EF131441 | 99996 | DEPARTMENT OF PLANNING, LANDS AND HERITA | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 217.76 |
| EF131442 | 99996 | DEPARTMENT OF PLANNING LANDS & HERITAGE | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 1,625.47 |
| EF131443 | 99996 | LISA BURCHELL | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 5,230.34 |
| EF131444 | 99996 | RONALD TAYLOR | RATES AND PROPERTY RELATED REFUNDS | 17/07/2020 | 800.00 |
| EF131445 | 88888 | MICHAEL ROBERT HOOD | REFUND | 17/07/2020 | 500.00 |
| EF131446 | 88888 | NICHOLAS CUNNINGHAM | REFUND | 17/07/2020 | 500.00 |
| EF131447 | 88888 | LANE FARMING | REFUND | 17/07/2020 | 500.00 |
| EF131448 | 88888 | BUILDING DEVELOPMENT GROUP | REFUND | 17/07/2020 | 14,298.82 |
| EF131449 | 88888 | SYMBOLISE HOLDINGS | REFUND | 17/07/2020 | 3,278.00 |
| EF131450 | 99997 | GRAEME WELLS | MEDICAL CHECK - F EXTENSION | 17/07/2020 | 110.00 |
| EF131451 | 99997 | ADRIAN PALJETAK | CROSSOVER CONTRIBUTION - A PALJETAK | 17/07/2020 | 300.00 |
| EF131452 | 99997 | BARBARA ABBOTT T/A BEHAVIOUR MATTERS | INVOICE 602 - GRIEVANCE CONCILIATION | 17/07/2020 | 1,330.00 |



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| EF131453 | 99997 | GLYNIS DIMOPOULOS | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131454 | 99997 | STELLA COLLICA | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131455 | 99997 | GARRY CUMMING | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131456 | 99997 | MARIA BROADFOOT | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131457 | 99997 | SHARON SCARCELLI DE JONG | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 60.00 |
| EF131458 | 99997 | WENG FONG | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 40.00 |
| EF131459 | 99997 | HER LIAN HEE | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131460 | 99997 | PENG LIM | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131461 | 99997 | NORMAN ANDREW | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131462 | 99997 | BARBARA SOLTOGGIO | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131463 | 99997 | FREDERICK MOOIBROEK | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131464 | 99997 | KERRY DUNN | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131465 | 99997 | VITOR FAUSTINO | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 140.00 |
| EF131466 | 99997 | FLORENCE VELLA | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131467 | 99997 | JANAYA MENEGHINI | REIMBURSEMENT - CO SAFE OPEN DAY FOOD | 17/07/2020 | 197.24 |
| EF131468 | 99997 | N & I STOJANOV | COMPOST BIN - IVANA STOJANOV | 17/07/2020 | 50.00 |
| EF131469 | 99997 | WILLIAM ABBOTT & ISABELLA MCGAVOCK | COMPOST BIN - ISABELLA MCGAVOCK | 17/07/2020 | 50.00 |
| EF131470 | 99997 | CARL SIMONETTI | COMPOST BIN - CARL SIMONETTI | 17/07/2020 | 45.00 |
| EF131471 | 99997 | JANAYA MENEGHINI | PETTY CASH - COLES FOOD TEAM MEETING | 17/07/2020 | 41.21 |
| EF131472 | 99997 | CHETAN POUTULA | INK FOR DVD LABEL PRINTER | 17/07/2020 | 52.20 |
| EF131473 | 99997 | ALETHEA AND RONALD WHITSUN | COMPOSIT BIN REBATE - A WHITSUN | 17/07/2020 | 50.00 |
| EF131474 | 99997 | ELIZABETH DUNCAN | COMPOST BIN REBATE - E DUNCAN | 17/07/2020 | 50.00 |
| EF131475 | 99997 | JOHN RUTHERFORD AND KAREN LOWES | COMPOST BIN REBATE - KAREN LOWES | 17/07/2020 | 45.00 |
| EF131476 | 99997 | BENJAMIN SEAH | COMPOST BIN REBATE - BEN SEAH | 17/07/2020 | 50.00 |
| EF131477 | 99997 | GEOFFREY A MCCANN | COMPOST BIN REBATE - G MCCANN | 17/07/2020 | 50.00 |
| EF131478 | 99997 | T A KILIAN & R H SHEPPARD | COMPOST BIN REBATE - T KILIAN | 17/07/2020 | 45.00 |
| EF131479 | 99997 | JOSHUA HARPER | COMPOST BIN REBATE - JOSHUA HARPER | 17/07/2020 | 50.00 |
| EF131480 | 99997 | JAMES ANDREWS | REIMBURSEMENT - JAMES ANDREWS | 17/07/2020 | 91.00 |
| EF131481 | 99997 | ABHIJIT SINGHA | SUSTAINABILITY GRANT | 17/07/2020 | 1,000.00 |
| EF131482 | 99997 | BRANDON'S SHREDDING BOXES | SUSTAINABILITY GRANT | 17/07/2020 | 2,000.00 |
| EF131483 | 99997 | COASTAL MOTORCYCLE CLUB | SUSTAINABILITY GRANT | 17/07/2020 | 4,274.97 |
| EF131484 | 99997 | COCKBURN COMMUNITY WILDLIFE CORRIDOR | SUSTAINABILITY GRANT | 17/07/2020 | 650.00 |
| EF131485 | 99997 | COOBY CARES INC. | SUSTAINABILITY GRANT | 17/07/2020 | 2,197.80 |
| EF131486 | 99997 | COOLBELLUP LEARNING CENTRE (EDUCATION SU | SUSTAINABILITY GRANT | 17/07/2020 | 1,745.00 |
| EF131487 | 99997 | GROUND WIND TURBINES | SUSTAINABILITY GRANT | 17/07/2020 | 4,400.00 |
| EF131488 | 99997 | HARVEST LAKES RESDIENTS ASSOCIATION | SUSTAINABILITY GRANT | 17/07/2020 | 3,999.49 |
| EF131489 | 99997 | IDEALINGS ARCHITECTURE | SUSTAINABILITY GRANT | 17/07/2020 | 2,644.40 |
| EF131490 | 99997 | RANDWICK STABLES | SUSTAINABILITY GRANT | 17/07/2020 | 3,982.00 |
| EF131491 | 99997 | SMYL COMMUNITY COLLEGE COCKBURN CAMPUS | SUSTAINABILITY GRANT | 17/07/2020 | 3,026.10 |
| EF131492 | 99997 | THE ORGANIC COLLECTIVE | SUSTAINABILITY GRANT | 17/07/2020 | 3,300.00 |
| EF131493 | 99997 | YANGEBUP FAMILY CENTRE INC | SUSTAINABILITY GRANT | 17/07/2020 | 4,400.00 |
| EF131494 | 99997 | N TAYLOR | BIRD BATH REBATE - NIC TAYLOR | 17/07/2020 | 19.95 |
| EF131495 | 99997 | JAMES WILLIAMSON | ARC REFUND - J WILLIAMSON | 17/07/2020 | 34.90 |
| EF131496 | 99997 | LUKE F COFFEY | COMPOST BIN REBATE - LUKE COFFEY | 17/07/2020 | 45.00 |
| EF131497 | 99997 | KADEN SARGEANT | ARC REFUND | 17/07/2020 | 119.50 |
| EF131498 | 99997 | BEELIAR COMMUNITY VOICE | DONATION | 17/07/2020 | 174.00 |
| EF131499 | 99997 | PORTUGUESE AUSTRALIAN WOMEN'S ASSOCIATIO | DONATION TO GO TOWARDS DOOR PRIZE AT CHR | 17/07/2020 | 200.00 |
| EF131500 | 99997 | SOUTH BEACH COMMUNITY GROUP | DONATION | 17/07/2020 | 135.83 |
| EF131501 | 99997 | JUNE PEARCE | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131502 | 99997 | HELEN HUTCHENS | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131503 | 99997 | CARMEL FEATHERSTONE | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |

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| EF131504 | 99997 | KATHERINE SKOOG | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131505 | 99997 | MICHAEL DURACK | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131506 | 99997 | HELMAN KOCH | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131507 | 99997 | ANNA DEGENNARO | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131508 | 99997 | ANNA MARTIN | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131509 | 99997 | PATRICIA BLYSMA | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 200.00 |
| EF131510 | 99997 | NORMAN ANDREW | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131511 | 99997 | DONI TREZISE | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 45.00 |
| EF131512 | 99997 | TERESA CARABETTA | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131513 | 99997 | NADA GVOZDIN | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131514 | 99997 | MERYL JEWELL | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131515 | 99997 | NORMAN COCKERTON | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 100.00 |
| EF131516 | 99997 | LYNETTE RILETTI | SENIOR SECURITY SUBSIDY SCHEME | 17/07/2020 | 300.00 |
| EF131517 | 99997 | HEALTHCARE AUSTRALIA | UNSPENT FUNDS ON HCP PACKAGE | 17/07/2020 | 17,693.34 |
| EF131518 | 99997 | CHRISTINE WYSE | REFUND REQUEST ARC - CHRISTINE WYSE | 17/07/2020 | 169.07 |
| EF131519 | 99997 | MICHELLE COSTER | REFUND FOR UNUSED RYDE CREDITS | 17/07/2020 | 105.00 |
| EF131520 | 99997 | GOODSTART EARLY LEARNING COCKBURN CENTRA | SUSTAINABILITY GRANT | 17/07/2020 | 2,497.00 |
| EF131521 | 99997 | LAKE MARTIN FILMS | SUSTAINABILITY GRANT | 17/07/2020 | 2,200.00 |
| EF131522 | 99997 | SOUTH COOGEE PRIMARY SCHOOL | SUSTAINABILITY GRANT | 17/07/2020 | 1,045.00 |
| EF131523 | 99997 | ERNEST AND MONA FOX | PAYMENT FOR OVER PAID FEES - ERNEST FOX | 17/07/2020 | 44.00 |
| EF131524 | 99997 | MARIE LA FRENAS | UNIVERSITY REIMBURSEMENT | 17/07/2020 | 3,450.00 |
| EF131525 | 99997 | PENG LIM | CROSSOVER REBATE | 17/07/2020 | 300.00 |
| EF131526 | 99997 | MATTHEW JONES | CROSSOVER REBATE | 17/07/2020 | 300.00 |
| EF131527 | 99997 | AMIR LADONI | CROSSOVER REBATE | 17/07/2020 | 300.00 |
| EF131528 | 99997 | KATHERINE MATILIONIS | CROSSOVER REBATE | 17/07/2020 | 300.00 |
| EF131529 | 99997 | COOGEE BEACH PROGRESS ASSOCIATION | DELEGATED AUTHORITY LGACS7 | 17/07/2020 | 487.00 |
| EF131530 | 10047 | ALINTA ENERGY | NATURAL GAS & ELECTRCITY SUPPLY | 17/07/2020 | 8,044.70 |
| EF131531 | 11794 | SYNERGY | ELECTRICITY USAGE/SUPPLIES | 17/07/2020 | 348,996.05 |
| EF131532 | 10007 | MJ & AR BAMFORD CONSULTING ECOLOGISTS | CONSULTANCY SERVICES - ECOLOGY | 17/07/2020 | 12,045.00 |
| EF131533 | 10058 | ALSCO PTY LTD | HYGIENE SERVICES/SUPPLIES | 17/07/2020 | 188.09 |
| EF131534 | 10097 | BLACKWOODS ATKINS | ENGINEERING SUPPLIES | 17/07/2020 | 3,819.20 |
| EF131535 | 10118 | AUSTRALIA POST | POSTAGE CHARGES | 17/07/2020 | 16,367.00 |
| EF131536 | 10184 | BENARA NURSERIES | PLANTS | 17/07/2020 | 1,314.76 |
| EF131537 | 10201 | BIG W DISCOUNT STORES | VARIOUS SUPPLIES | 17/07/2020 | 104.50 |
| EF131538 | 10207 | BOC GASES | GAS SUPPLIES | 17/07/2020 | 343.59 |
| EF131539 | 10221 | BP AUSTRALIA PTY LTD | DIESEL/PETROL SUPPLIES | 17/07/2020 | 18,740.88 |
| EF131540 | 10226 | BRIDGESTONE AUSTRALIA LTD | TYRE SERVICES | 17/07/2020 | 12,746.17 |
| EF131541 | 10244 | BUILDING & CONST INDUSTRY TRAINING FUND | LEVY PAYMENT | 17/07/2020 | 8,402.35 |
| EF131542 | 10246 | BUNNINGS BUILDING SUPPLIES PTY LTD | HARDWARE SUPPLIES | 17/07/2020 | 4,351.81 |
| EF131543 | 10279 | CASTROL AUSTRALIA PTY LTD | GREASE/LUBRICANTS | 17/07/2020 | 2,462.60 |
| EF131544 | 10287 | CENTRELINE MARKINGS | LINEMARKING SERVICES | 17/07/2020 | 1,045.00 |
| EF131545 | 10333 | CJD EQUIPMENT PTY LTD | HARDWARE SUPPLIES | 17/07/2020 | 22,767.58 |
| EF131546 | 10338 | CLEANAWAY PTY LTD | WASTE DISPOSAL SERVICES | 17/07/2020 | 1,186.02 |
| EF131547 | 10353 | COCKBURN CEMENT LTD | CEMENT AND LIME | 17/07/2020 | 1,481.04 |
| EF131548 | 10359 | COCKBURN PAINTING SERVICE | PAINTING SUPPLIES/SERVICES | 17/07/2020 | 11,456.50 |
| EF131549 | 10368 | COCKBURN WETLANDS EDUCATION CENTRE | COMMUNITY GRANT | 17/07/2020 | 7,756.50 |
| EF131550 | 10375 | VEOLIA ENVIRONMENTAL SERVICES | WASTE SERVICES | 17/07/2020 | 9,569.14 |
| EF131551 | 10483 | LANDGATE | MAPPING/LAND TITLE SEARCHES | 17/07/2020 | 3,689.12 |
| EF131552 | 10484 | DEPARTMENT OF MINES, INDUSTRY REGULATION AND SAFETY | BUILDING SERVICES LEVY | 17/07/2020 | 32,117.74 |
| EF131553 | 10526 | E & MJ ROSHER PTY LTD | MOWER EQUIPMENT | 17/07/2020 | 6,739.00 |
| EF131554 | 10528 | EASIFLEET | VEHICLE LEASE | 17/07/2020 | 368.56 |



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| EF131555 | 10590 | DEPARTMENT OF FIRE AND EMERGENCY SERVICES | ESL LEVY & RELATED COSTS | 17/07/2020 | 16,925.73 |
| EF131556 | 10597 | FLEXI STAFF PTY LTD | EMPLOYMENT SERVICES | 17/07/2020 | 5,918.89 |
| EF131557 | 10609 | FORESTVALE TREES P/L | PLANTS - TREES/SHRUBS | 17/07/2020 | 4,092.00 |
| EF131558 | 10611 | FORPARK AUSTRALIA | PLAYGROUND EQUIPMENT | 17/07/2020 | 9,051.46 |
| EF131559 | 10655 | GHD PTY LTD | CONSULTANCY SERVICES | 17/07/2020 | 2,176.08 |
| EF131560 | 10726 | HOLTON CONNOR ARCHITECTS & PLANNERS | ARCHITECTURAL SERVICES | 17/07/2020 | 9,906.60 |
| EF131561 | 10778 | IWF FENCING | FENCING REPAIRS/MAINTENANCE | 17/07/2020 | 23,818.30 |
| EF131562 | 10787 | JANDAKOT ACCIDENT REPAIR CENTRE | PANEL BEATING SERVICES | 17/07/2020 | 853.53 |
| EF131563 | 10794 | JASON SIGNMAKERS | SIGNS | 17/07/2020 | 4,554.00 |
| EF131564 | 10879 | LES MILLS AEROBICS | INSTRUCTION/TRAINING SERVICES | 17/07/2020 | 1,256.27 |
| EF131565 | 10888 | LJ CATERERS | CATERING SERVICES | 17/07/2020 | 2,401.30 |
| EF131566 | 10912 | M2 TECHNOLOGY GROUP | MESSAGING SERVICES | 17/07/2020 | 396.00 |
| EF131567 | 10913 | BUCHER MUNICIPAL PTY LTD | PURCHASE OF NEW PLANT / REPAIR SERVICES | 17/07/2020 | 2,670.78 |
| EF131568 | 10923 | MAJOR MOTORS PTY LTD | REPAIRS/MAINTENANCE SERVICES | 17/07/2020 | 56,026.54 |
| EF131569 | 10938 | MAXWELL ROBINSON & PHELPS | PEST & WEED MANAGEMENT | 17/07/2020 | 208.15 |
| EF131570 | 10942 | MCGEES PROPERTY | PROPERTY CONSULTANCY SERVICES | 17/07/2020 | 1,188.00 |
| EF131571 | 10944 | MCLEODS | LEGAL SERVICES | 17/07/2020 | 56,438.74 |
| EF131572 | 10951 | MELVILLE MOTORS PTY LTD | MOTOR CARS | 17/07/2020 | 27,775.32 |
| EF131573 | 10982 | MODERN TEACHING AIDS PTY LTD | TEACHING AIDS | 17/07/2020 | 539.82 |
| EF131574 | 10991 | BEACON EQUIPMENT | MOWING EQUIPMENT | 17/07/2020 | 23,956.65 |
| EF131575 | 11004 | MURDOCH UNIVERSITY OFFICE OF FINANCE, PLANNING & REPORTING | ANALYSING SERVICES | 17/07/2020 | 764.50 |
| EF131576 | 11022 | NATIVE ARC | GRANTS & DONATIONS | 17/07/2020 | 500.00 |
| EF131577 | 11028 | NEVERFAIL SPRINGWATER LTD | BOTTLED WATER SUPPLIES | 17/07/2020 | 393.00 |
| EF131578 | 11036 | NORTHLAKE ELECTRICAL | ELECTRICAL SERVICES | 17/07/2020 | 48,547.66 |
| EF131579 | 11076 | P & D AMARANTI ENGINEERING | ENGINEERING SERVICES | 17/07/2020 | 830.50 |
| EF131580 | 11077 | P & G BODY BUILDERS PTY LTD | PLANT BODY BUILDING SERVICES | 17/07/2020 | 1,419.00 |
| EF131581 | 11182 | PREMIUM BRAKE & CLUTCH SERVICE | BRAKE SERVICES | 17/07/2020 | 2,480.50 |
| EF131582 | 11208 | QUICK CORPORATE AUSTRALIA PTY LTD | STATIONERY/CONSUMABLES | 17/07/2020 | 5,964.05 |
| EF131583 | 11235 | REINFORCED CONCRETE PIPES PTY LTD | CONCRETE PIPE SUPPLIES | 17/07/2020 | 268.40 |
| EF131584 | 11248 | RICOH AUSTRALIA | OFFICE EQUIPMENT | 17/07/2020 | 235.70 |
| EF131585 | 11284 | THE ROYAL LIFE SAVNG SOCIETY WA INC PTY LTD | TRAINING SERVICES | 17/07/2020 | 159.00 |
| EF131586 | 11304 | SANAX MEDICAL & FIRST AID SUPPLIES | MEDICAL SUPPLIES | 17/07/2020 | 321.35 |
| EF131587 | 11307 | SATELLITE SECURITY SERVICES PTY LTD | SECURITY SERVICES | 17/07/2020 | 5,904.18 |
| EF131588 | 11308 | BOSS INDUSTRIAL FORMALLY SBA SUPPLIES | HARDWARE SUPPLIES | 17/07/2020 | 1,185.50 |
| EF131589 | 11331 | SHAWMAC PTY LTD | CONSULTANCY SERVICES - CIVIL | 17/07/2020 | 16,236.00 |
| EF131590 | 11334 | SHENTON ENTERPRISES PTY LTD | POOL EQUIPMENT/SERVICES | 17/07/2020 | 352.00 |
| EF131591 | 11361 | SIGMA CHEMICALS PTY LTD | CHEMICAL SUPPLIES | 17/07/2020 | 589.60 |
| EF131592 | 11387 | BIBRA LAKE SOILS | SOIL & LIMESTONE SUPPLIES | 17/07/2020 | 176.00 |
| EF131593 | 11425 | SOUTHERN METROPOLITAN REGIONAL COUNCIL | WASTE DISPOSAL GATE FEES | 17/07/2020 | 2,430.00 |
| EF131594 | 11502 | STATE LAW PUBLISHER | ADVERTISING SERVICES | 17/07/2020 | 972.10 |
| EF131595 | 11557 | TECHNOLOGY ONE LTD | IT CONSULTANCY SERVICES | 17/07/2020 | 2,599.30 |
| EF131596 | 11625 | TOTAL EDEN PTY LTD | RETICULATION SUPPLIES | 17/07/2020 | 10,408.45 |
| EF131597 | 11667 | TURFMASTER FACILITY MANAGEMENT | TURF & MOWING SERVICES | 17/07/2020 | 6,182.00 |
| EF131598 | 11701 | VIBRA INDUSTRIAL FILTRATION AUSTRALASIA | FILTER SUPPLIES | 17/07/2020 | 219.78 |
| EF131599 | 11715 | WA BLUEMETAL | ROADBASE SUPPLIES | 17/07/2020 | 1,199.17 |
| EF131600 | 11722 | WA HINO SALES & SERVICE | PURCHASE OF NEW TRUCKS / MAINTENANCE | 17/07/2020 | 626.88 |
| EF131601 | 11749 | WARREN'S EARTHMOVING CONTRACTORS | EARTHMOVING SERVICES | 17/07/2020 | 2,915.00 |
| EF131602 | 11795 | WESTERN POWER | STREET LIGHTING INSTALLATION & SERVICE | 17/07/2020 | 81,913.00 |
| EF131603 | 11806 | WESTRAC PTY LTD | REPAIRS/MTNCE - EARTHMOVING EQUIPMENT | 17/07/2020 | 890.82 |
| EF131604 | 11828 | WORLDWIDE ONLINE PRINTING - O'CONNOR | PRINTING SERVICES | 17/07/2020 | 2,118.24 |
| EF131605 | 11835 | WURTH AUSTRALIA PTY LTD | HARDWARE SUPPLIES | 17/07/2020 | 2,237.66 |

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| EF131606 | 12153 | HAYS PERSONNEL SERVICES PTY LTD | EMPLOYMENT SERVICES | 17/07/2020 | 4,785.26 |
| EF131607 | 12207 | CIVICA PTY LTD | SOFTWARE SUPPORT/LICENCE FEES | 17/07/2020 | 11,809.26 |
| EF131608 | 12219 | PARKS & LEISURE AUSTRALIA | SUBSCRIPTION RENEWAL | 17/07/2020 | 2,751.00 |
| EF131609 | 12320 | MUNDARING GARDEN CENTRE | PLANT SUPPLIES | 17/07/2020 | 1,557.25 |
| EF131610 | 12388 | ELITE POOL COVERS | POOL COVERS | 17/07/2020 | 1,474.00 |
| EF131611 | 12394 | MP ROGERS & ASSOCIATES PTY LTD | CONSULTANCY SERVICES - MARINE | 17/07/2020 | 4,967.67 |
| EF131612 | 12500 | ELLENBY TREE FARM | PLANT SUPPLIES | 17/07/2020 | 4,180.00 |
| EF131613 | 12507 | TECHNOLOGY FOR AGEING AND DISABILITY WA | MEDICAL SUPPLIES | 17/07/2020 | 603.75 |
| EF131614 | 12565 | SOUTHERN METRO REGIONAL COUNCIL - LOANS | LOAN REPAYMENT | 17/07/2020 | 17,867.34 |
| EF131615 | 12796 | ISENTIA PTY LTD | MEDIA MONITORING SERVICES | 17/07/2020 | 1,496.00 |
| EF131616 | 13563 | GREEN SKILLS INC | EMPLOYMENT SERVICES | 17/07/2020 | 46,667.47 |
| EF131617 | 13779 | PORTER CONSULTING ENGINEERS | ENGINEERING CONSULTANCY SERVICES | 17/07/2020 | 550.00 |
| EF131618 | 13825 | JACKSON MCDONALD | LEGAL SERVICES | 17/07/2020 | 9,996.09 |
| EF131619 | 13860 | KRS CONTRACTING | WASTE COLLECTION SERVICES | 17/07/2020 | 21,293.25 |
| EF131620 | 14530 | DONALD VEAL CONSULTANTS PTY LTD | CONSULTANCY SERVICES | 17/07/2020 | 10,433.50 |
| EF131621 | 14593 | AUSTREND INTERNATIONAL PTY LTD | ALUMINIUM SUPPLIES | 17/07/2020 | 8,275.92 |
| EF131622 | 14598 | ALF REBOLA THE GOOD GUYS | ELECTRICAL GOODS | 17/07/2020 | 169.00 |
| EF131623 | 15393 | STRATAGREEN | HARDWARE SUPPLIES | 17/07/2020 | 3,173.34 |
| EF131624 | 15550 | APACE AID INC | PLANTS & LANDSCAPING SERVICES | 17/07/2020 | 16,917.45 |
| EF131625 | 15609 | CATALYSE PTY LTD | CONSULTANCY SERVICES | 17/07/2020 | 5,500.00 |
| EF131626 | 15746 | WESTERN AUSTRALIA POLICE SERVICE | POLICE CLEARANCES | 17/07/2020 | 164.00 |
| EF131627 | 15850 | ECOSCAPE | ENVIRONMENTAL CONSULTANCY | 17/07/2020 | 7,260.00 |
| EF131628 | 15868 | CARDNO (WA) PTY LTD | CONSULTANCY SERVICES - ENGINEERING | 17/07/2020 | 12,181.40 |
| EF131629 | 16064 | CMS ENGINEERING PTY LTD | AIRCONDITIONING SERVICES | 17/07/2020 | 7,745.08 |
| EF131630 | 16107 | WREN OIL | WASTE DISPOSAL SERVICES | 17/07/2020 | 313.50 |
| EF131631 | 16396 | MAYDAY EARTHMOVING | ROAD CONSTRUCTION MACHINE HIRE | 17/07/2020 | 35,542.10 |
| EF131632 | 16653 | COMPLETE PORTABLES PTY LTD | SUPPLY & HIRE OF MODULAR BUILDINGS | 17/07/2020 | 117.85 |
| EF131633 | 16706 | COUNCIL ON THE AGEING (WA) INC | EDUCATION SERVICES | 17/07/2020 | 300.00 |
| EF131634 | 16846 | ACTION GLASS & ALUMINIUM | GLAZING SERVICES | 17/07/2020 | 4,507.25 |
| EF131635 | 16985 | WA PREMIX | CONCRETE SUPPLIES | 17/07/2020 | 13,889.92 |
| EF131636 | 17471 | PIRTEK (FREMANTLE) PTY LTD | HOSES & FITTINGS | 17/07/2020 | 695.27 |
| EF131637 | 17600 | LIGHTFORCE ASSET PTY LTD (ERECTIONS!) | GUARD RAILS | 17/07/2020 | 728.20 |
| EF131638 | 17827 | NIL SEN (WA) PTY LTD | ELECTRICAL SERVICES | 17/07/2020 | 2,219.14 |
| EF131639 | 18126 | DELL AUSTRALIA PTY LTD | COMPUTER HARDWARE | 17/07/2020 | 3,311.00 |
| EF131640 | 18203 | NATSYNC ENVIRONMENTAL | PEST CONTROL | 17/07/2020 | 800.60 |
| EF131641 | 18272 | AUSTRACLEAR LIMITED | INVESTMENT SERVICES | 17/07/2020 | 41.28 |
| EF131642 | 18286 | IW PROJECTS PTY LTD | CONSULTANCY SERVICES - CIVIL ENGINEERING | 17/07/2020 | 12,100.00 |
| EF131643 | 18688 | CAVE DESIGN | GRAPHIC DESIGN | 17/07/2020 | 583.00 |
| EF131644 | 18962 | SEALANES (1985) P/L | CATERING SUPPLIES | 17/07/2020 | 323.13 |
| EF131645 | 19107 | FOREVER SHINING | MONUMENT | 17/07/2020 | 24,299.00 |
| EF131646 | 19533 | WOOLWORTHS LTD | GROCERIES | 17/07/2020 | 1,314.88 |
| EF131647 | 19541 | TURF CARE WA PTY LTD | TURF SERVICES | 17/07/2020 | 1,672.00 |
| EF131648 | 19673 | WA INTERPRETERS PTY LTD | TRANSLATION/INTERPRETING | 17/07/2020 | 165.00 |
| EF131649 | 19776 | JOSH BYRNE & ASSOCIATES | ENVIRONMENTAL CONSULTANT | 17/07/2020 | 1,800.00 |
| EF131650 | 19856 | WESTERN TREE RECYCLERS | SHREDDING SERVICES | 17/07/2020 | 45,843.80 |
| EF131651 | 20000 | AUST WEST AUTO ELECTRICAL PTY LTD | AUTO ELECTRICAL SERVICES | 17/07/2020 | 22,048.76 |
| EF131652 | 20146 | DATA#3 LIMITED | CONTRACT IT PERSONNEL & SOFTWARE | 17/07/2020 | 5,700.48 |
| EF131653 | 20247 | CHRISTIE PARKSAFE | PARKS & RECREATIONAL PRODUCTS | 17/07/2020 | 12,285.90 |
| EF131654 | 20321 | RIVERJET PTY LTD | EDUCTING-CLEANING SERVICES | 17/07/2020 | 21,994.50 |
| EF131655 | 20857 | DOCKSIDE SIGNS | SIGN MAKERS | 17/07/2020 | 691.90 |
| EF131656 | 20864 | PARAGON CORPORATE TRAINING | TRAINING/ INSTRUCTION | 17/07/2020 | 21,900.00 |



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| EF131657 | 21287 | T.J.DEPIAZZI & SONS | SOIL & MULCH SUPPLIES | 17/07/2020 | 16,299.25 |
| EF131658 | 21291 | CHITTERING VALLEY WORM FARM | ENVIRONMENTAL EDUCATION | 17/07/2020 | 3,530.00 |
| EF131659 | 21294 | CAT HAVEN | ANIMAL SERVICES | 17/07/2020 | 1,369.00 |
| EF131660 | 21371 | LD TOTAL SANPOINT PTY LTD | LANDSCAPING WORKS/SERVICES | 17/07/2020 | 24,480.93 |
| EF131661 | 21397 | THE PLAY ROOM O'CONNOR | TOYS AND GAMES | 17/07/2020 | 499.92 |
| EF131662 | 21627 | MANHEIM PTY LTD | IMPOUNDED VEHICLES | 17/07/2020 | 2,640.00 |
| EF131663 | 21665 | MMJ REAL ESTATE (WA) PTY LTD | PROPERTY MANAGEMENT SERVICES | 17/07/2020 | 20,962.68 |
| EF131664 | 21744 | JB HI FI - COMMERCIAL | ELECTRONIC EQUIPMENT | 17/07/2020 | 10,827.00 |
| EF131665 | 21747 | UNICARE HEALTH | WHEELCHAIR HIRE | 17/07/2020 | 272.80 |
| EF131666 | 21946 | RYAN'S QUALITY MEATS | MEAT SUPPLIES | 17/07/2020 | 597.08 |
| EF131667 | 22106 | INTELFIE GROUP | SERVICES - DAIP | 17/07/2020 | 5,717.87 |
| EF131668 | 22337 | SEGAFREDO ZANETTI AUST PTY LTD | COFFEE & COFFEE MACHINES | 17/07/2020 | 1,006.30 |
| EF131669 | 22553 | BROWNES FOOD OPERATIONS | CATERING SUPPLIES | 17/07/2020 | 449.66 |
| EF131670 | 22569 | SONIC HEALTH PLUS PTY LTD | MEDICAL SERVICES | 17/07/2020 | 1,377.20 |
| EF131671 | 22623 | LANDMARK PRODUCTS LTD | LANDSCAPE INFRASTRUCTURE | 17/07/2020 | 10,296.00 |
| EF131672 | 22658 | SOUTH EAST REGIONAL CENTRE FOR URBAN LANDCARE INC (SERCUL) | URBAN LANDCARE SERVICES | 17/07/2020 | 2,970.00 |
| EF131673 | 22682 | BEAVER TREE SERVICES PTY LTD | TREE PRUNING SERVICES | 17/07/2020 | 23,148.17 |
| EF131674 | 22806 | PUMA ENERGY (AUSTRALIA) FUELS PTY LTD | FUEL SUPPLIES | 17/07/2020 | 32,887.90 |
| EF131675 | 22874 | ECONOMIC DEVELOPMENT AUSTRALIA LTD | CONFERENCE/EVENTS | 17/07/2020 | 2,200.00 |
| EF131676 | 22913 | AUSTRALIAN OFFICE LEADING BRANDS.COM.AU | ENVELOPES | 17/07/2020 | 231.46 |
| EF131677 | 23253 | KOTT GUNNING | LEGAL SERVICES | 17/07/2020 | 2,284.48 |
| EF131678 | 23450 | CLEVER DESIGNS | UNIFORMS | 17/07/2020 | 390.15 |
| EF131679 | 23457 | TOTALLY WORKWEAR FREMANTLE | CLOTHING - UNIFORMS | 17/07/2020 | 3,531.71 |
| EF131680 | 23570 | A PROUD LANDMARK PTY LTD | LANDSCAPE CONSTRUCTION SERVICES | 17/07/2020 | 4,510.00 |
| EF131681 | 23849 | JCB CONSTRUCTION EQUIPMENT AUSTRALIA | PLANT/MACHINERY PURCHASE & MAINTENANCE | 17/07/2020 | 588.79 |
| EF131682 | 23971 | FIND WISE LOCATION SERVICES | LOCATING SERVICES - UNDERGROUND | 17/07/2020 | 916.30 |
| EF131683 | 24142 | WATMARINE ENGINEERING SERVICES | FABRICATION SERVICES | 17/07/2020 | 54,120.00 |
| EF131684 | 24643 | BIBLIOTHECA RFID LIBRARY SYSTEMS AUSTRALIA PTY LTD | PURCHASE OF LIBRARY TAGS | 17/07/2020 | 1,038.95 |
| EF131685 | 24655 | AUTOMASTERS SPEARWOOD | VEHICLE SERVICING | 17/07/2020 | 715.00 |
| EF131686 | 24718 | SOLAR LIGHTING DESIGNS | SOLAR DESIGN | 17/07/2020 | 825.00 |
| EF131687 | 24724 | QUALITY MARINE COATING SYSTEMS P/L | CLEANING SERVICES - ROAD SURFACES | 17/07/2020 | 1,430.00 |
| EF131688 | 24725 | FERAL INVASIVE SPECIES ERADICATION MANAGEMENT | ERADICATION MANAGEMENT SERVICES | 17/07/2020 | 1,950.00 |
| EF131689 | 24734 | MYRIAD IMAGES | PHOTOGRAPHY SERVICES | 17/07/2020 | 9,295.00 |
| EF131690 | 24736 | ZENIEN | CCTV CAMERA LICENCES | 17/07/2020 | 9,472.32 |
| EF131691 | 24748 | PEARMANS ELECTRICAL & MECHANICAL SERVICES P/L | ELECTRICAL SERVICES | 17/07/2020 | 6,896.19 |
| EF131692 | 24945 | NS PROJECTS PTY LTD | PROJECT MANAGEMENT SERVICES | 17/07/2020 | 30,662.50 |
| EF131693 | 24949 | BITUMEN SURFACING THE TRUSTEE FOR COMPLETE ROAD SERVICES TRUST | BITUMEN SUPPLIES | 17/07/2020 | 1,074.15 |
| EF131694 | 24978 | AMBIUS | PLANTS SUPPLIES | 17/07/2020 | 690.29 |
| EF131695 | 25063 | SUPERIOR PAK PTY LTD | VEHICLE MAINTENANCE | 17/07/2020 | 978.17 |
| EF131696 | 25115 | FIIG | INVESTMENT MANAGEMENT SERVICES | 17/07/2020 | 2,750.00 |
| EF131697 | 25121 | IMAGESOURCE DIGITAL SOLUTIONS | BILLBOARDS | 17/07/2020 | 4,429.70 |
| EF131698 | 25127 | MILMAR DISTRIBUTORS | PRINTING SERVICES - ID CARDS | 17/07/2020 | 80.00 |
| EF131699 | 25201 | JTAGZ PTY LTD | WRISTSTRAPS | 17/07/2020 | 3,243.90 |
| EF131700 | 25325 | NATSALES ADVERTISING PTY LTD | PRINTING SERVICES | 17/07/2020 | 1,320.00 |
| EF131701 | 25415 | JANDAKOT STOCK & PET SUPPLIES | PET SUPPLIES | 17/07/2020 | 53.80 |
| EF131702 | 25418 | CS LEGAL | LEGAL SERVICES | 17/07/2020 | 1,185.30 |
| EF131703 | 25586 | ENVIROVAP PTY LTD | HIRE OF LEACHATE UNITS | 17/07/2020 | 9,597.50 |
| EF131704 | 25713 | DISCUS ON DEMAND THE TRUSTEE FOR DISCUS ON DEMAND UNIT TRUST | PRINTING SERVICES | 17/07/2020 | 2,391.44 |
| EF131705 | 25733 | MIRACLE RECREATION EQUIPMENT | PLAYGROUND INSTALLATION / REPAIRS | 17/07/2020 | 4,477.00 |
| EF131706 | 25737 | THE TRUSTEE FOR SARB ENTERPRISES HYBRID TRUST T/A DATABASE CONSULT | CONSULTANCY SERVICES | 17/07/2020 | 7,232.50 |
| EF131707 | 25813 | LG CONNECT PTY LTD | ERP SYSTEMS DEVELOPMENT | 17/07/2020 | 11,441.59 |

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| EF131708 | 25819 | BRAJKOVICH DEMOLITION & SALVAGE (WA) PTY LTD | DEMOLITION SERVICES | 17/07/2020 | 56,980.00 |
| EF131709 | 25940 | LEAF BEAN MACHINE | COFFEE BEAN SUPPLY | 17/07/2020 | 400.00 |
| EF131710 | 26029 | AUTOSWEEP WA | SWEEPING SERVICES | 17/07/2020 | 4,763.00 |
| EF131711 | 26067 | SPRAYKING WA PTY LTD | CHEMICAL WEED CONTROL SERVICES | 17/07/2020 | 66,136.17 |
| EF131712 | 26114 | GRACE RECORDS MANAGEMENT | RECORDS MANAGEMENT SERVICES | 17/07/2020 | 1,375.41 |
| EF131713 | 26211 | AMCOM PTY LTD | INTERNET/DATA SERVICES | 17/07/2020 | 27,669.50 |
| EF131714 | 26257 | PAPERBARK TECHNOLOGIES | ARBORICULTURAL CONSULTANCY SERVICES | 17/07/2020 | 24,302.85 |
| EF131715 | 26314 | CPE GROUP | TEMPORARY EMPLOYMENT SERVICES | 17/07/2020 | 3,132.86 |
| EF131716 | 26359 | WILSON SECURITY | SECURITY SERVICES | 17/07/2020 | 204,984.33 |
| EF131717 | 26399 | PAPERSCOOT THE TRUSTEE FOR PETERS MORRISON FAMILY TRUST | GRAPHIC DESIGN SERVICES | 17/07/2020 | 3,289.00 |
| EF131718 | 26442 | BULLANT SECURITY PTY LTD KEY WEST LOCK SERVICE & SALES | LOCKSMITH & SECURITY SERVICES | 17/07/2020 | 6,444.97 |
| EF131719 | 26470 | SCP CONSERVATION | FENCING SERVICES | 17/07/2020 | 36,762.00 |
| EF131720 | 26499 | ELITE TRAVEL SOLUTIONS PTY LTD | TRAVEL AGENT SERVICES | 17/07/2020 | 3,055.00 |
| EF131721 | 26549 | SHARON GREGORY (KOORT-KADAK CONSULTANCY) | CONSULTANCY SERVICES | 17/07/2020 | 50.00 |
| EF131722 | 26588 | SOURCE SEPARATION SYSTEMS P/L | PROVIDING WASTE AND RECYCLING BINS | 17/07/2020 | 227.82 |
| EF131723 | 26606 | ENVIRO INFRASTRUCTURE PTY LTD | CONSTRUCTION& FABRICATION | 17/07/2020 | 66,298.80 |
| EF131724 | 26610 | TRACC CIVIL PTY LTD | CIVIL CONSTRUCTION - CONTRACT C100661 | 17/07/2020 | 623,438.98 |
| EF131725 | 26614 | MARKETFORCE PTY LTD | ADVERTISING | 17/07/2020 | 11,202.06 |
| EF131726 | 26645 | PROFESSIONAL TRAPPING SUPPLIES | ANIMAL TRAPPING PRODUCTS/WILDLIFE/SECURI | 17/07/2020 | 4,649.00 |
| EF131727 | 26667 | TANGELO CREATIVE | GRAPHIC DESIGN | 17/07/2020 | 16,500.00 |
| EF131728 | 26686 | WORK METRICS PTY LTD | ONLINE SOFTWARE | 17/07/2020 | 1,320.00 |
| EF131729 | 26709 | TALIS CONSULTANTS PTY LTD | WASTE CONSULTANCY | 17/07/2020 | 4,950.00 |
| EF131730 | 26721 | QUAD SERVICES PTY LTD | CLEANING SERVICES | 17/07/2020 | 18,437.03 |
| EF131731 | 26735 | SHANE MCMASTER SURVEYS | SURVEY SERVICES | 17/07/2020 | 1,650.00 |
| EF131732 | 26739 | KERB DOCTOR | KERB MAINTENANCE | 17/07/2020 | 5,865.20 |
| EF131733 | 26756 | TRENCHBUSTERS PTY LTD | EARTHMOVING | 17/07/2020 | 360.00 |
| EF131734 | 26782 | SOFT LANDING | RECYCLING SERVICES | 17/07/2020 | 6,883.80 |
| EF131735 | 26791 | MONSTERBALL AMUSEMENT & HIRE | AMUSEMENT HIRE | 17/07/2020 | 890.00 |
| EF131736 | 26800 | THE GOODS | RETAIL | 17/07/2020 | 102.43 |
| EF131737 | 26883 | GTA CONSULTANTS | TRANSPORT PLANNING | 17/07/2020 | 6,019.75 |
| EF131738 | 26884 | PEOPLE ON BICYCLES | TRAINING - BICYCLE CLASSES | 17/07/2020 | 180.00 |
| EF131739 | 26888 | MEDIA ENGINE | GRAPHIC DESIGN, MARKETING, VIDEO PRODUCT | 17/07/2020 | 13,730.00 |
| EF131740 | 26898 | SPANDEX ASIA PACIFIC PTY LTD | SIGNAGE SUPPLIER | 17/07/2020 | 5,473.52 |
| EF131741 | 26900 | BG & E PTY LTD | CONSULTING ENGINEERING | 17/07/2020 | 4,781.50 |
| EF131742 | 26901 | ALYKA PTY LTD | DIGITAL CONSULTANCY AND WEB DEVELOPMENT | 17/07/2020 | 5,390.00 |
| EF131743 | 26904 | GREEN SERVICES | SUSTAINABILITY EDUCATION FOR HOUSEHOLDS | 17/07/2020 | 4,750.00 |
| EF131744 | 26917 | CIRRUS NETWORKS PTY LTD | IT NETWORK & TELEPHONY SERVICES | 17/07/2020 | 26,746.51 |
| EF131745 | 26923 | WOODLANDS DISTRIBUTORS PTY LTD | RUBBISH COLLECTION EQUIPMENT | 17/07/2020 | 3,498.00 |
| EF131746 | 26929 | ELAN ENERGY MATRIX PTY LTD | RECYCLING SERVICES | 17/07/2020 | 610.46 |
| EF131747 | 26938 | MAJESTIC PLUMBING | PLUMBING SERVICES | 17/07/2020 | 8,417.16 |
| EF131748 | 26946 | AV TRUCK SERVICES PTY LTD | TRUCK DEALERSHIP | 17/07/2020 | 278.62 |
| EF131749 | 26982 | PLANTRITE | PLANTS | 17/07/2020 | 18,574.27 |
| EF131750 | 26985 | ACCESS ICON PTY LTD | DRAINAGE PRODUCTS | 17/07/2020 | 2,871.00 |
| EF131751 | 26987 | CTI RISK MANAGEMENT | SECURITY - CASH COLLECTION | 17/07/2020 | 683.10 |
| EF131752 | 26988 | BLADON WA PTY LTD | PROMOTIONAL PRODUCTS | 17/07/2020 | 2,396.90 |
| EF131753 | 27006 | BIBRA LAKE IGA XPRESS | LIQUOR SUPPLIES | 17/07/2020 | 399.90 |
| EF131754 | 27010 | QUANTUM BUILDING SERVICES PTY LTD | BUILDING MAINTENANCE | 17/07/2020 | 3,806.00 |
| EF131755 | 27011 | BAILEYS MARINE FUEL AUSTRALIA | FUEL | 17/07/2020 | 11.78 |
| EF131756 | 27027 | FRIG TECH WA | REFRIDGERATION SERVICES | 17/07/2020 | 1,572.36 |
| EF131757 | 27031 | DOWNER EDI WORKS PTY LTD | ASPHALT SERVICES | 17/07/2020 | 2,596.00 |
| EF131758 | 27034 | ADELBY PTY LTD | FIREBREAK CONSTRUCTION | 17/07/2020 | 43,450.00 |



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| EF131759 | 27044 | GRAFFITI SYSTEMS AUSTRALIA | GRAFFITI REMOVAL & ANTI-GRAFFITI COATING | 17/07/2020 | 1,134.71 |
| EF131760 | 27045 | GANTNER ELECTRONICS PTY LTD | ELECTRICAL | 17/07/2020 | 14,253.80 |
| EF131761 | 27054 | VOCUS PTY LTD | TELECOMMUNICATIONS | 17/07/2020 | 4,646.40 |
| EF131762 | 27059 | FRONTLINE FIRE AND RESCUE EQUIPMENT | MANUFACTURE-FIRE VEHICLES/EQUIPMENT | 17/07/2020 | 2,906.86 |
| EF131763 | 27065 | WESTBOOKS | BOOKS | 17/07/2020 | 492.04 |
| EF131764 | 27072 | NORDIC FITNESS EQUIPMENT | FITNESS EQUIPMENT | 17/07/2020 | 1,060.00 |
| EF131765 | 27077 | CARBON NEUTRAL | CARBON SOLUTIONS PROVIDER | 17/07/2020 | 2,411.75 |
| EF131766 | 27078 | INFOCOUNCIL PTY LTD | SOFTWARE | 17/07/2020 | 29,095.00 |
| EF131767 | 27082 | KULBARDI PTY LTD | STATIONERY SUPPLIES | 17/07/2020 | 542.85 |
| EF131768 | 27093 | MAGNETIC AUTOMATION PTY LTD | GATES/BARRIERS | 17/07/2020 | 371.80 |
| EF131769 | 27097 | GLOBAL TRADE SALES | MARINE EQUIPMENT | 17/07/2020 | 1,485.00 |
| EF131770 | 27161 | NEXT POWER | SOLAR PANEL | 17/07/2020 | 22,986.34 |
| EF131771 | 27187 | NEVE CONTRACTING | DESIGN SERVICES | 17/07/2020 | 9,252.00 |
| EF131772 | 27189 | HEALTHSTRONG PTY LTD | HOME CARE | 17/07/2020 | 220.00 |
| EF131773 | 27198 | GREEN PROMOTIONS PTY LTD | PROMOTIONAL SUPPLIES | 17/07/2020 | 558.25 |
| EF131774 | 27211 | CHRIS MELSOM | URBAN PLANNING AND DESIGN | 17/07/2020 | 720.00 |
| EF131775 | 27241 | LANDSCAPE ELEMENTS PTY LTD | LANDSCAPING SERVICES | 17/07/2020 | 52,000.48 |
| EF131776 | 27246 | VEALE AUTO PARTS | SPARE PARTS MECHANICAL | 17/07/2020 | 7,372.40 |
| EF131777 | 27268 | FOCUS ENVIRO | PLANT & MACHINERY | 17/07/2020 | 5,048.58 |
| EF131778 | 27269 | INTEGRAPAY PTY LTD | PAYMENT PROCESSING | 17/07/2020 | 6,554.47 |
| EF131779 | 27285 | XTREME FIRE DETECTION | FIRE SAFETY | 17/07/2020 | 979.00 |
| EF131780 | 27334 | WESTCARE PRINT | PRINTING SERVICES | 17/07/2020 | 198.00 |
| EF131781 | 27346 | OFFICE LINE | FURNITURE OFFICE | 17/07/2020 | 633.60 |
| EF131782 | 27348 | MESSAGE MEDIA | TELECOMMUNICATIONS | 17/07/2020 | 1,395.99 |
| EF131783 | 27374 | SOUTHERN CROSS CLEANING | COMMERCIAL CLEANING | 17/07/2020 | 7,397.25 |
| EF131784 | 27377 | ACCIDENTAL HEALTH AND SAFETY - PERTH | FIRST AID SUPPLIES | 17/07/2020 | 4,867.36 |
| EF131785 | 27379 | ESRI AUSTRALIA PTY LTD | GIS SOFTWARE | 17/07/2020 | 21,125.72 |
| EF131786 | 27385 | PROGRAMMED ELECTRICAL TECHNOLOGIES | ELECTRICAL SERVICES | 17/07/2020 | 7,971.35 |
| EF131787 | 27396 | ANKEET MEHTA SPEARWOOD NEWSPAPER ROUND DELIVERY | NEWSPAPER DELIVERY | 17/07/2020 | 292.63 |
| EF131788 | 27401 | EMPRISE MOBILITY PTY LTD | MOBILITY EQUIPMENT | 17/07/2020 | 703.00 |
| EF131789 | 27404 | K2 AUDIOVISUAL | AUDIO VISUAL EQUIPMENT | 17/07/2020 | 28,802.14 |
| EF131790 | 27417 | NATIVE PLANTS WA | PLANTS | 17/07/2020 | 30,072.00 |
| EF131791 | 27423 | MECHANICAL PROJECT SERVICES PTY LTD | AIRCONDITIONING SERVICES | 17/07/2020 | 3,838.78 |
| EF131792 | 27427 | HOME CHEF | COOKING/FOOD SERVICES | 17/07/2020 | 384.52 |
| EF131793 | 27437 | PB RETICULATION & MAINTENANCE SERVICES PTY LTD | IRRIGATION SERVICES | 17/07/2020 | 503.80 |
| EF131794 | 27448 | SELECTRO SERVICES PTY LTD | ELECTRICAL | 17/07/2020 | 209.00 |
| EF131795 | 27455 | SITE PROTECTIVE SERVICES | CCTV PARTS | 17/07/2020 | 64,608.03 |
| EF131796 | 27456 | SECUREPAY PTY LTD | PAYMENT SOLUTIONS | 17/07/2020 | 1,229.25 |
| EF131797 | 27482 | BILLI AUSTRALIA PTY LTD | WATER FILTER TAPS | 17/07/2020 | 630.30 |
| EF131798 | 27485 | ASHLEY GROUP | AUTO ELECTRICAL | 17/07/2020 | 10,392.58 |
| EF131799 | 27524 | DAVID WILLS AND ASSOCIATES | ENGINEERING SERVICES | 17/07/2020 | 15,331.25 |
| EF131800 | 27539 | JASMIN CARPENTRY & MAINTENANCE | CARPENTRY | 17/07/2020 | 10,965.20 |
| EF131801 | 27560 | ARTEM DESIGN STUDIO PTY LTD | ARCHITECTURAL SERVICES | 17/07/2020 | 5,912.50 |
| EF131802 | 27567 | CHORUS AUSTRALIA LIMITED | HEALTH CARE SERVICES | 17/07/2020 | 2,940.30 |
| EF131803 | 27575 | SHRED X SECURE DESTRUCTION | DOCUMENT DESTRUCTION | 17/07/2020 | 30.36 |
| EF131804 | 27579 | TRAVIS HAYTO PHOTOGRAPHY | PHOTOGRAPHY SERVICES | 17/07/2020 | 5,500.00 |
| EF131805 | 27599 | DOCKPRO PTY LTD | SALES, SERVICING & INSTALLATION OF DRY D | 17/07/2020 | 9,530.00 |
| EF131806 | 27602 | RAWLINSONS (WA) | SURVEYING SERVICES | 17/07/2020 | 4,840.00 |
| EF131807 | 27610 | ROCKWATER PTY LTD | HYDROGEOLOGICAL CONSULTANCY | 17/07/2020 | 554.40 |
| EF131808 | 27617 | GALAXY 42 PTY LTD | CONSULTANCY - IT | 17/07/2020 | 9,856.00 |
| EF131809 | 27622 | TRUGRADE MEDICAL SUPPLIES | MEDICAL SUPPLIES | 17/07/2020 | 856.95 |

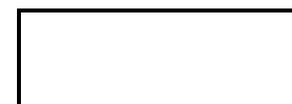
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| EF131810 | 27631 | AQUATIC SERVICES WA PTY LTD | POOL EQUIPMENT & MAINTENANCE | 17/07/2020 | 29,378.20 |
| EF131811 | 27644 | CMAKTECH | ICT ENGINEERING & CONSULTING | 17/07/2020 | 128,283.95 |
| EF131812 | 27646 | THE TRUSTEE FOR SAS UNIT TRUST (SITE ARCHITECTURE STUDIO) | ARCHITECTURAL SERVICES | 17/07/2020 | 7,271.00 |
| EF131813 | 27648 | HFM ASSET MANAGEMENT PTY LTD | ENGINEERING CONSULTANCY | 17/07/2020 | 7,018.00 |
| EF131814 | 27657 | POSITIVE BALANCE MASSAGE | MASSAGE THERAPY | 17/07/2020 | 300.00 |
| EF131815 | 27667 | THE KP COLLECTIVE | MARKETING | 17/07/2020 | 6,215.00 |
| EF131816 | 27676 | BLUE FORCE PTY LTD | SECURITY SERVICES | 17/07/2020 | 220.00 |
| EF131817 | 27677 | DODGY BROS DODGEBALL CO. | DODGEBALL GAME | 17/07/2020 | 1,500.00 |
| EF131818 | 27695 | QTM PTY LTD | TRAFFIC MANAGEMENT | 17/07/2020 | 7,541.62 |
| EF131819 | 27701 | PERTH BETTER HOMES | SHADE SAILS | 17/07/2020 | 21,142.00 |
| EF131820 | 27702 | ARCHAE-AUS PTY LTD | CONSULTANCY - CULTURAL | 17/07/2020 | 14,833.47 |
| EF131821 | 27708 | FUNNELBACK PTY LTD | IT SERVICES | 17/07/2020 | 13,200.00 |
| EF131822 | 27712 | PERTH PLAYGROUND AND RUBBER PTY LTD | PLAYGROUND SOFTFALL/EQUIPMENT | 17/07/2020 | 12,606.00 |
| EF131823 | 27714 | JSH ENGINEERING | MECHINICAL SERVICES | 17/07/2020 | 5,243.98 |
| EF131824 | 27722 | METRA AUSTRALIA | SOFTWARE | 17/07/2020 | 330.00 |
| EF131825 | 27776 | URBAN RESOURCES PTY LTD | HIRE PALNT & EQUIPMENT | 17/07/2020 | 8,800.00 |
| EF131826 | 27779 | SPORTS CIRCUIT LINEMARKING | LINEMARKING | 17/07/2020 | 10,190.50 |
| EF131827 | 27794 | DOMUS NURSERY | PLANT NURSERY | 17/07/2020 | 736.52 |
| EF131828 | 27798 | X-SEED EDUCATION AND DEVELOPMENT | EDUCATION SERVICES | 17/07/2020 | 1,630.00 |
| EF131829 | 27801 | TRANSPONDER TECHNOLOGIES PTY LTD | FUEL MANAGEMENT SYSTEMS | 17/07/2020 | 990.00 |
| EF131830 | 27805 | SKYWARD ROOFING SERVICES PTY LTD | ROOFING | 17/07/2020 | 300.00 |
| EF131831 | 27806 | CREATIVE CANARY | WEB HOSTING | 17/07/2020 | 330.00 |
| EF131832 | 27808 | CAMMS | SOFTWARE | 17/07/2020 | 17,600.00 |
| EF131833 | 27816 | ASTERISK INFORMATION SECURITY | IT CONSULTANCY | 17/07/2020 | 7,260.00 |
| EF131834 | 27827 | ABC CONTAINERS | SEA CONTAINERS | 17/07/2020 | 275.00 |
| EF131835 | 27829 | SMEC AUSTRALIA PTY. LTD. | CONSULTANCY - ENGINEERING | 17/07/2020 | 4,237.20 |
| EF131836 | 27842 | LIGHT HOUSE LAUNDRY | LAUNDERING | 17/07/2020 | 105.27 |
| EF131837 | 27855 | TOTAL LANDSCAPE REDEVELOPMENT SERVICE PTY LTD | TREE WATERING | 17/07/2020 | 34,342.00 |
| EF131838 | 27863 | CARERS PLUS | NURSING SERVICES | 17/07/2020 | 6,372.67 |
| EF131839 | 27869 | SELECT FRESH PTY LTD | FOOD SUPPLIE,FRUIT & VEG | 17/07/2020 | 438.75 |
| EF131840 | 27875 | TOWN TEAM MOVEMENT | PUBLIC ENGAGEMENT | 17/07/2020 | 1,848.00 |
| EF131841 | 27894 | LIFECARE HOMECARE | HEALTHCARE | 17/07/2020 | 1,593.63 |
| EF131842 | 27917 | GO DOORS PTY LTD | DOOR MAINTENANCE & REPAIR | 17/07/2020 | 15,786.10 |
| EF131843 | 27919 | BARK ENVIRONMENTAL | DIEBACK TREATMENT | 17/07/2020 | 6,187.50 |
| EF131844 | 27931 | BIG ASS FANS AUSTRALIA PTY LTD | CEILING FANS | 17/07/2020 | 46,244.64 |
| EF131845 | 27932 | EPIC ASPHALT | CIVIL CONSTRUCTION | 17/07/2020 | 13,763.20 |
| EF131846 | 27941 | NATURE BASED PLAY PTY LTD | DESIGNING PLAYGROUNDS | 17/07/2020 | 20,000.00 |
| EF131847 | 27946 | KAMBARANG SERVICES PTY LTD | TRAINING INDIGENOUS CULTUTE | 17/07/2020 | 2,500.00 |
| EF131848 | 27950 | GREENLITE ELECTRICAL CONTRACTOR PTY LTD | ELECTRICAL SERVICES | 17/07/2020 | 621.06 |
| EF131849 | 27953 | TRUCKLINE | SPARE PARTS, TRUCK/TRAILER | 17/07/2020 | 1,572.18 |
| EF131850 | 27963 | BUFFALO SOLUTIONS | TRAINING | 17/07/2020 | 5,100.59 |
| EF131851 | 27964 | EAST INFLATABLES MANUFACTURING CO LTD | INFLATEABLE GAMES | 17/07/2020 | 16,380.00 |
| EF131852 | 27965 | STANTEC AUSTRALIA PTY LTD | ENGINEERING SERVICES | 17/07/2020 | 32,285.00 |
| EF131853 | 27978 | FRONTLINE SAFETY AUSTRALIA PTY LTD | CLOTHING - UNIFORMS | 17/07/2020 | 11,933.03 |
| EF131854 | 27981 | BROADWAY MARINE CONSTRUCTIONS | MARINE SERVICES | 17/07/2020 | 17,244.70 |
| EF131855 | 27982 | PEP TRANSPORT | TRANSPORT | 17/07/2020 | 1,632.60 |
| EF131856 | 27984 | SABRINA FENWICK | EXCERCISE CLASSES | 17/07/2020 | 400.00 |
| EF131857 | 27989 | IMCO AUSTRALASIA | CONCRETE PRODUCTS | 17/07/2020 | 7,101.60 |
| EF131858 | 27993 | BUSINESS BASE | OFFICE FURNITURE | 17/07/2020 | 1,500.00 |
| EF131859 | 20751 | DEPARTMENT OF TRANSPORT - BULK BILL | VEHICLE LICENCING REGISTRATIONS | 14/07/2020 | 69,896.00 |
| EF131860 | 10747 | IINET LIMITED | INTERNET SERVICES | 17/07/2020 | 110.98 |



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| EF131861 | 11758 | WATER CORP UTILITY ACCOUNT ONLY - PLEASE REFER TO 11760 WHEN RAISING | WATER USAGE / SUNDRY CHARGES | 17/07/2020 | 10,170.38 |
| EF131862 | 11760 | WATER CORPORATION | SEWER EASEMENT | 17/07/2020 | 77,908.93 |
| EF131863 | 99997 | FAMILY DAY CARE | FDC PAYMENT WE 12/07/20 | 16/07/2020 | 49,508.81 |
| EF131864 | 99997 | IN HOME CARE PAYMENTS | IHC PAYMENTS WE 12/07/2020 | 16/07/2020 | 11,659.04 |
| EF131865 | 99997 | SYLVIA BEUKES | JOB KEEPER PAYMENT - SYLVIA BEUKES | 17/07/2020 | 1,200.00 |
| EF131866 | 99997 | ALANA FAHEY | BRP PAYMENT - ALANA FAHEY | 17/07/2020 | 386.40 |
| EF131867 | 26987 | CTI RISK MANAGEMENT | SECURITY - CASH COLLECTION | 21/07/2020 | 577.20 |
| EF131868 | 10152 | AUST SERVICES UNION | PAYROLL DEDUCTIONS | 28/07/2020 | 1,091.70 |
| EF131869 | 10154 | AUSTRALIAN TAXATION OFFICE | PAYROLL DEDUCTIONS | 28/07/2020 | 460,087.00 |
| EF131870 | 10305 | CHILD SUPPORT AGENCY | PAYROLL DEDUCTIONS | 28/07/2020 | 1,479.55 |
| EF131871 | 11001 | LOCAL GOVERNMENT RACING & CEMETERIES EMPLOYEES UNION LGRCEU | PAYROLL DEDUCTIONS | 28/07/2020 | 82.00 |
| EF131872 | 11857 | CHAMPAGNE SOCIAL CLUB | PAYROLL DEDUCTIONS | 28/07/2020 | 412.00 |
| EF131873 | 11860 | 45S CLUB | PAYROLL DEDUCTIONS | 28/07/2020 | 14.00 |
| EF131874 | 19726 | HEALTH INSURANCE FUND OF WA | PAYROLL DEDUCTIONS | 28/07/2020 | 1,454.90 |
| EF131875 | 25987 | TOYOTA FLEET MANAGEMENT | PAYROLL DEDUCTIONS - NOVATED LEASE | 28/07/2020 | 608.14 |
| EF131876 | 26987 | CTI RISK MANAGEMENT | SECURITY - CASH COLLECTION | 28/07/2020 | 1,223.90 |
| EF131877 | 27277 | DEPARTMENT OF WATER AND ENVIRONMENTAL REGULATION | QUARTERLY LAND FILL LEVY | 28/07/2020 | 1,438,657.87 |
| EF131878 | 27874 | SMARTSALARY | SALARY PACKAGING/LEASING ADMINISTRATION | 28/07/2020 | 12,493.55 |
| EF131879 | 11867 | KEVIN JOHN ALLEN | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131880 | 12740 | LOGAN HOWLETT | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 11,439.09 |
| EF131881 | 20634 | LEE-ANNE SMITH | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,139.83 |
| EF131882 | 25353 | PHILIP EVA | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131883 | 26696 | CHAMONIX TERBLANCHE | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131884 | 27326 | MICHAEL SEPAROVICH | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131885 | 27327 | CHONTELLE STONE | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131886 | 27475 | LARA KIRKWOOD | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 4,509.66 |
| EF131887 | 27871 | TOM WIDENBAR | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131888 | 27872 | PHOEBE CORKE | MONTHLY ELECTED MEMBER ALLOWANCE | 29/07/2020 | 2,639.83 |
| EF131889 | 88888 | IGLESIA NI CHRISTO | REFUND | 31/07/2020 | 150.00 |
| EF131890 | 88888 | ROBERT KAGI | REFUND | 31/07/2020 | 500.00 |
| EF131891 | 88888 | BRUNO E RIKLI | REFUND | 31/07/2020 | 200.00 |
| EF131892 | 99997 | MARIA VILLAMAGNA | SENIOR SECURITY SUBSIDY SCHEME | 31/07/2020 | 200.00 |
| EF131893 | 99997 | KADEN C SARGEANT | REFUND OF DUPLICATE CHARGE | 31/07/2020 | 119.50 |
| EF131894 | 99997 | KELVIN AND JODIE GRAY | CROSSOVER CONTRIBUTION - JODIE GRAY | 31/07/2020 | 300.00 |
| EF131895 | 99997 | J T HORNE | CROSSOVER CONTRIBUTION - JACINTER HORNE | 31/07/2020 | 300.00 |
| EF131896 | 99997 | CATHERINE PATERSON | CROSSOVER CONTRIBUTION - C PATERSON | 31/07/2020 | 300.00 |
| EF131897 | 99997 | NICHOLAS TARQUINIO | CROSSOVER CONTRIBUTION - N TARQUINIO | 31/07/2020 | 300.00 |
| EF131898 | 99997 | CLARA REYES | COMPOST BIN REBATE | 31/07/2020 | 45.00 |
| EF131899 | 99997 | WESTERN AUSTRALIAN PLANNING COMMISSION | SALE OF LAND - INVOICE IN600748 | 31/07/2020 | 280,000.00 |
| EF131900 | 99997 | EMMA POLAIN | SEASON CANCELLATION REFUND | 31/07/2020 | 165.00 |
| EF131901 | 99997 | KERRY STREET COMMUNITY SCHOOL | INVOICE 00004681 | 31/07/2020 | 311.58 |
| EF131902 | 99997 | DAEHEE KIM | ARC MEMBERSHIP REFUND | 31/07/2020 | 82.00 |
| EF131903 | 99997 | ALEX J KINGSBURY | REFUND FOR SEASON CANCELLATION | 31/07/2020 | 126.00 |
| EF131904 | 99997 | AKD & MA O'GRADY | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131905 | 99997 | RICHARD HOLMES | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131906 | 99997 | SRIDHAR CHOWBINA R | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131907 | 99997 | MONIQUE CRABBE | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131908 | 99997 | MARIE LA FRENAIS | REIMBURSEMENT OF BOOKS | 31/07/2020 | 77.48 |
| EF131909 | 99997 | AM & TG HOSCHKE | ELECTRICITY BILL CHARGED IN ERROR - D123 | 31/07/2020 | 32.63 |
| EF131910 | 99997 | PRAYATNA BHATTARAI | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131911 | 99997 | KAREN ANGELICA FOLEY, ANDREW IAN FOLEY | CROSSOVER CLAIM | 31/07/2020 | 300.00 |



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| EF131912 | 99997 | LUKE ABATANIA | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131913 | 99997 | KIERAN BYRNE & VANESSA BENINO | CROSSOVER CLAIM | 31/07/2020 | 300.00 |
| EF131914 | 99997 | COOLBELLUP COMMUNITY ASSOCIATION INC | NEWSLETTER SUBSIDY LGACS7 | 31/07/2020 | 820.60 |
| EF131915 | 99997 | ZUZANA ALBRECHT | REFUND REQUEST ARC - ZUZANA ALBRECHT | 31/07/2020 | 34.00 |
| EF131916 | 99997 | VOGT GRAHAM LAWYERS | INVOICE NUMBER 012240 - KEVIN ALLEN | 31/07/2020 | 732.60 |
| EF131917 | 99997 | GWEN SMITH | UNSPENT FUNDS ON HCP PACKAGE | 31/07/2020 | 589.70 |
| EF131918 | 99997 | SERVAU OFFCL DEPARTMENTAL RECPTS & PYMTS | DOCUMENT NO: 180111628 CENTREPAY | 31/07/2020 | 233.64 |
| EF131919 | 99997 | CLAY HADWIGER | COMPOST BIN REABTE - C GADWIGER | 31/07/2020 | 50.00 |
| EF131920 | 99997 | EVERSWELL PTY LTD | INVOICE INV0002 SYNERGY REIMBURSEMENT | 31/07/2020 | 583.06 |
| EF131921 | 99997 | CA & HL SCOTT | COMPOST BIN REBATE - CHRIS SCOTT | 31/07/2020 | 50.00 |
| EF131922 | 99997 | BRAIN TEASERS OZ PTY LTD | PAYMENT FOR GAME PACK PURCHASE | 31/07/2020 | 77.00 |
| EF131923 | 99997 | BRAIN TEASERS OZ PTY LTD | PAYMENT FOR GAME PACK PURCHASE | 31/07/2020 | 77.00 |
| EF131924 | 99997 | MALGORZATA JONCZYK | PEN FEE REFUND F204 | 31/07/2020 | 37.00 |
| EF131925 | 99997 | BEV MCDONALD | SENIOR SECURITY SUBSIDY SCHEME | 31/07/2020 | 200.00 |
| EF131926 | 99997 | PHYLLIS STANDLEY | SENIOR SECURITY SUBSIDY SCHEME | 31/07/2020 | 90.00 |
| EF131927 | 99997 | GRAHAM HOLDAWAY | SENIOR SECURITY SUBSIDY SCHEME | 31/07/2020 | 140.00 |
| EF131928 | 99997 | BRAD AND MAREE DAY | CHANGE OF DOG STATUS - CINDY 41075 | 31/07/2020 | 120.00 |
| EF131929 | 99997 | LEAH SMITH | CAT STERILISATION SUBSIDY - L SMITH | 31/07/2020 | 50.00 |
| EF131930 | 99997 | NADINE BARNES | CROSSOVER CLAIM - N BARNES | 31/07/2020 | 300.00 |
| EF131931 | 99997 | MA & WR KNIBBS | WR, GR & MA KNIBBS - STORMWATER DRAINAGE | 31/07/2020 | 1,510.49 |
| EF131932 | 99997 | JOASH ANG | CROSSOVER CLAIM - J ANG | 31/07/2020 | 300.00 |
| EF131933 | 99997 | SENMEI BENNET | CROSSOVER CLAIM - S BENNET | 31/07/2020 | 300.00 |
| EF131934 | 99997 | ANDREW HOSKING | CROSSOVER CLAIM - A HOSKING | 31/07/2020 | 300.00 |
| EF131935 | 99997 | MISS CARIA LAUREN SMITH | CROSSOVER CLAIM - C SMITH | 31/07/2020 | 300.00 |
| EF131936 | 99997 | VOGT GRAHAM LAWYERS | INVOICE NUMBER 012083 | 31/07/2020 | 6,464.04 |
| EF131937 | 99996 | C DE BARI | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 238.91 |
| EF131938 | 99996 | DANIELLA & ADAM CAPUTI | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 147.00 |
| EF131939 | 99996 | TANGENT NOMINESS T/AS SUMMIT PROJECTS | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 1,042.08 |
| EF131940 | 99996 | JOHNS LYNG INSURANCE BUILDING | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 1,491.67 |
| EF131941 | 99996 | SCOTT WILLMER | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 406.60 |
| EF131942 | 99996 | MICHELLE ANNE DOSWELL | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 100.00 |
| EF131943 | 99996 | JASMYN ANN BURNS | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 25.00 |
| EF131944 | 99996 | CAMILLA BEATE HILL | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 150.00 |
| EF131945 | 99996 | NAOMI SMYTH | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 150.00 |
| EF131946 | 99996 | RHYS ANDREW MACKAY | RATES AND PROPERTY RELATED REFUNDS | 31/07/2020 | 150.00 |
| EF131947 | 10047 | ALINTA ENERGY | NATURAL GAS & ELECTRCITY SUPPLY | 31/07/2020 | 88.30 |
| EF131948 | 11794 | SYNERGY | ELECTRICITY USAGE/SUPPLIES | 31/07/2020 | 11,465.34 |
| EF131949 | 12025 | TELSTRA CORPORATION | COMMUNICATIONS SERVICES | 31/07/2020 | 23,559.92 |
| EF131950 | 99997 | FAMILY DAY CARE | FDC PAYMENT WE 26/07/20 | 30/07/2020 | 54,481.73 |
| EF131951 | 99997 | IN HOME CARE PAYMENTS | IHC PAYMENTS WE 26/07/20 | 30/07/2020 | 17,403.15 |
| EF131952 | 10097 | BLACKWOODS ATKINS | ENGINEERING SUPPLIES | 31/07/2020 | 463.12 |
| EF131953 | 10184 | BENARA NURSERIES | PLANTS | 31/07/2020 | 1,635.72 |
| EF131954 | 10201 | BIG W DISCOUNT STORES | VARIOUS SUPPLIES | 31/07/2020 | 80.00 |
| EF131955 | 10207 | BOC GASES | GAS SUPPLIES | 31/07/2020 | 304.90 |
| EF131956 | 10226 | BRIDGESTONE AUSTRALIA LTD | TYRE SERVICES | 31/07/2020 | 20,994.45 |
| EF131957 | 10239 | BUDGET RENT A CAR - PERTH | MOTOR VEHICLE HIRE | 31/07/2020 | 1,072.50 |
| EF131958 | 10246 | BUNNINGS BUILDING SUPPLIES PTY LTD | HARDWARE SUPPLIES | 31/07/2020 | 3,879.28 |
| EF131959 | 10255 | CABCHARGE AUSTRALIA PTY LTD | CABCHARGES | 31/07/2020 | 1,256.15 |
| EF131960 | 10333 | CJD EQUIPMENT PTY LTD | HARDWARE SUPPLIES | 31/07/2020 | 46.48 |
| EF131961 | 10368 | COCKBURN WETLANDS EDUCATION CENTRE | COMMUNITY GRANT | 31/07/2020 | 250.00 |
| EF131962 | 10483 | LANDGATE | MAPPING/LAND TITLE SEARCHES | 31/07/2020 | 2,911.24 |



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| EF131963 | 10526 | E & MJ ROSHER PTY LTD | MOWER EQUIPMENT | 31/07/2020 | 3,005.17 |
| EF131964 | 10528 | EASIFLEET | VEHICLE LEASE | 31/07/2020 | 1,074.83 |
| EF131965 | 10535 | WORKPOWER INCORPORATED | EMPLOYMENT SERVICES - PLANTING | 31/07/2020 | 162,099.21 |
| EF131966 | 10597 | FLEXI STAFF PTY LTD | EMPLOYMENT SERVICES | 31/07/2020 | 9,596.47 |
| EF131967 | 10787 | JANDAKOT ACCIDENT REPAIR CENTRE | PANEL BEATING SERVICES | 31/07/2020 | 1,414.60 |
| EF131968 | 10814 | JR & A HERSEY PTY LTD | SAFETY CLOTHING SUPPLIES | 31/07/2020 | 1,254.00 |
| EF131969 | 10888 | LJ CATERERS | CATERING SERVICES | 31/07/2020 | 3,542.77 |
| EF131970 | 10896 | LOCAL HEALTH AUTHORITIES ANALYTICAL COMMITTEE | ANALYTICAL SERVICES | 31/07/2020 | 14,338.91 |
| EF131971 | 10913 | BUCHER MUNICIPAL PTY LTD | PURCHASE OF NEW PLANT / REPAIR SERVICES | 31/07/2020 | 2,451.08 |
| EF131972 | 10923 | MAJOR MOTORS PTY LTD | REPAIRS/MAINTENANCE SERVICES | 31/07/2020 | 1,910.42 |
| EF131973 | 10938 | MAXWELL ROBINSON & PHELPS | PEST & WEED MANAGEMENT | 31/07/2020 | 975.35 |
| EF131974 | 10942 | MCGEES PROPERTY | PROPERTY CONSULTANCY SERVICES | 31/07/2020 | 550.00 |
| EF131975 | 10953 | MELVILLE-COCKBURN CHAMBER OF COMMERCE | SPONSORSHIP | 31/07/2020 | 150.00 |
| EF131976 | 10991 | BEACON EQUIPMENT | MOWING EQUIPMENT | 31/07/2020 | 12,369.40 |
| EF131977 | 11004 | MURDOCH UNIVERSITY OFFICE OF FINANCE, PLANNING & REPORTING | ANALYSING SERVICES | 31/07/2020 | 764.50 |
| EF131978 | 11028 | NEVERFAIL SPRINGWATER LTD | BOTTLED WATER SUPPLIES | 31/07/2020 | 121.98 |
| EF131979 | 11036 | NORTHLAKE ELECTRICAL | ELECTRICAL SERVICES | 31/07/2020 | 328,312.43 |
| EF131980 | 11182 | PREMIUM BRAKE & CLUTCH SERVICE | BRAKE SERVICES | 31/07/2020 | 3,750.56 |
| EF131981 | 11208 | QUICK CORPORATE AUSTRALIA PTY LTD | STATIONERY/CONSUMABLES | 31/07/2020 | 3,001.18 |
| EF131982 | 11244 | RESEARCH SOLUTIONS PTY LTD | RESEARCH SERVICES | 31/07/2020 | 20,961.60 |
| EF131983 | 11284 | THE ROYAL LIFE SAVNG SOCIETY WA INC PTY LTD | TRAINING SERVICES | 31/07/2020 | 226.50 |
| EF131984 | 11307 | SATELLITE SECURITY SERVICES PTY LTD | SECURITY SERVICES | 31/07/2020 | 15,532.91 |
| EF131985 | 11308 | BOSS INDUSTRIAL FORMALLY SBA SUPPLIES | HARDWARE SUPPLIES | 31/07/2020 | 3,882.00 |
| EF131986 | 11337 | SHERIDANS FOR BADGES | NAME BADGES & ENGRAVING | 31/07/2020 | 426.25 |
| EF131987 | 11459 | SPEARWOOD VETERINARY HOSPITAL | VETERINARY SERVICES | 31/07/2020 | 150.00 |
| EF131988 | 11469 | SPORTS TURF TECHNOLOGY PTY LTD | TURF CONSULTANCY SERVICES | 31/07/2020 | 3,151.50 |
| EF131989 | 11483 | ST JOHN AMBULANCE AUST WA OPERATIONS | FIRST AID COURSES | 31/07/2020 | 50.00 |
| EF131990 | 11625 | TOTAL EDEN PTY LTD | RETICULATION SUPPLIES | 31/07/2020 | 8,961.97 |
| EF131991 | 11701 | VIBRA INDUSTRIAL FILTRATION AUSTRALASIA | FILTER SUPPLIES | 31/07/2020 | 796.84 |
| EF131992 | 11789 | WALGA | ADVERTISING/TRAINING SERVICES | 31/07/2020 | 75,078.44 |
| EF131993 | 11793 | WESTERN IRRIGATION PTY LTD | IRRIGATION SERVICES/SUPPLIES | 31/07/2020 | 210,826.00 |
| EF131994 | 11795 | WESTERN POWER | STREET LIGHTING INSTALLATION & SERVICE | 31/07/2020 | 218,209.92 |
| EF131995 | 11806 | WESTRAC PTY LTD | REPAIRS/MTNCE - EARTHMOVING EQUIPMENT | 31/07/2020 | 3,325.54 |
| EF131996 | 12018 | O'CONNOR LAWNMOWER & CHAINSAW CENTRE | MOWING EQUIPMENT/PARTS/SERVICES | 31/07/2020 | 126.00 |
| EF131997 | 12153 | HAYS PERSONNEL SERVICES PTY LTD | EMPLOYMENT SERVICES | 31/07/2020 | 8,953.77 |
| EF131998 | 12249 | FAMILY DAY CARE WA | MEMBERSHIP RENEWAL | 31/07/2020 | 440.00 |
| EF131999 | 12500 | ELLENBY TREE FARM | PLANT SUPPLIES | 31/07/2020 | 1,210.00 |
| EF132000 | 12507 | TECHNOLOGY FOR AGEING AND DISABILITY WA | MEDICAL SUPPLIES | 31/07/2020 | 1,333.25 |
| EF132001 | 13068 | STANDARDS AUSTRALIA | COPYRIGHT LICENSING | 31/07/2020 | 2,249.01 |
| EF132002 | 13102 | MICHAEL PAGE INTERNATIONAL (AUSTRALIA) PTY LTD | EMPLOYMENT SERVICES | 31/07/2020 | 16,732.45 |
| EF132003 | 13558 | ENGINEERING TECHNOLOGY CONSULTANTS | CONSULTANTS SERVICES | 31/07/2020 | 3,729.00 |
| EF132004 | 13563 | GREEN SKILLS INC | EMPLOYMENT SERVICES | 31/07/2020 | 3,173.87 |
| EF132005 | 13825 | JACKSON MCDONALD | LEGAL SERVICES | 31/07/2020 | 4,624.40 |
| EF132006 | 13860 | KRS CONTRACTING | WASTE COLLECTION SERVICES | 31/07/2020 | 10,106.25 |
| EF132007 | 14350 | BAILEYS FERTILISERS | FERTILISER SUPPLIES | 31/07/2020 | 1,504.80 |
| EF132008 | 14598 | ALF REBOLA THE GOOD GUYS | ELECTRICAL GOODS | 31/07/2020 | 349.00 |
| EF132009 | 14777 | LGIS JARDINE LLOYD THOMPSON PTY LTD | INSURANCE PREMIUMS | 31/07/2020 | 13,979.90 |
| EF132010 | 15393 | STRATAGREEN | HARDWARE SUPPLIES | 31/07/2020 | 1,320.27 |
| EF132011 | 15588 | NATURAL AREA HOLDINGS PTY LTD | WEED SPRAYING | 31/07/2020 | 228,080.48 |
| EF132012 | 15746 | WESTERN AUSTRALIA POLICE SERVICE | POLICE CLEARANCES | 31/07/2020 | 32.80 |
| EF132013 | 15850 | ECOSCAPE | ENVIRONMENTAL CONSULTANCY | 31/07/2020 | 5,157.90 |

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| EF132014 | 15868 | CARDNO (WA) PTY LTD | CONSULTANCY SERVICES - ENGINEERING | 31/07/2020 | 5,988.40 |
| EF132015 | 16064 | CMS ENGINEERING PTY LTD | AIRCONDITIONING SERVICES | 31/07/2020 | 19,344.85 |
| EF132016 | 16107 | WREN OIL | WASTE DISPOSAL SERVICES | 31/07/2020 | 341.00 |
| EF132017 | 16363 | ATCO GAS AUSTRALIA | GAS SUPPLIES/SERVICES | 31/07/2020 | 622.58 |
| EF132018 | 16396 | MAYDAY EARTHMOVING | ROAD CONSTRUCTION MACHINE HIRE | 31/07/2020 | 57,750.00 |
| EF132019 | 17600 | LIGHTFORCE ASSET PTY LTD (ERLECTIONS!) | GUARD RAILS | 31/07/2020 | 4,452.80 |
| EF132020 | 17827 | NIL SEN (WA) PTY LTD | ELECTRICAL SERVICES | 31/07/2020 | 1,077.44 |
| EF132021 | 17927 | SHARYN EGAN | ARTISTIC SERVICES | 31/07/2020 | 16,500.00 |
| EF132022 | 18122 | SIGNMAN | SIGNAGE | 31/07/2020 | 16,126.00 |
| EF132023 | 18126 | DELL AUSTRALIA PTY LTD | COMPUTER HARDWARE | 31/07/2020 | 14,799.40 |
| EF132024 | 18313 | CITY OF WHITTLESEA | SEMINAR | 31/07/2020 | 22,000.00 |
| EF132025 | 18763 | LOCAL COMMUNITY INSURANCE SERVICES (PART OF JLT GROUP) | COMMUNITY INSURANCE POLICIES | 31/07/2020 | 96.97 |
| EF132026 | 18801 | FREMANTLE BIN HIRE | BIN HIRE - SKIP BINS | 31/07/2020 | 420.00 |
| EF132027 | 18941 | ALLSTAMPS | STATIONERY | 31/07/2020 | 275.10 |
| EF132028 | 18962 | SEALANES (1985) P/L | CATERING SUPPLIES | 31/07/2020 | 1,632.89 |
| EF132029 | 18997 | GISSA INTERNATIONAL PTY LTD | TRAINING SERVICES | 31/07/2020 | 3,637.70 |
| EF132030 | 19058 | FREMANTLE CAMERAHOUSE | PHOTOGRAPHIC EQUIPMENT | 31/07/2020 | 799.00 |
| EF132031 | 19533 | WOOLWORTHS LTD | GROCERIES | 31/07/2020 | 1,494.59 |
| EF132032 | 20000 | AUST WEST AUTO ELECTRICAL PTY LTD | AUTO ELECTRICAL SERVICES | 31/07/2020 | 4,287.42 |
| EF132033 | 21127 | JOANNA AYCKBOURN (VOICES IN SINC) | INSTRUCTION - SINGING | 31/07/2020 | 500.00 |
| EF132034 | 21287 | T.J.DEPIAZZI & SONS | SOIL & MULCH SUPPLIES | 31/07/2020 | 3,259.85 |
| EF132035 | 21471 | WA MACHINERY GLASS | GLAZING SERVICES | 31/07/2020 | 1,078.00 |
| EF132036 | 21665 | MMJ REAL ESTATE (WA) PTY LTD | PROPERTY MANAGEMENT SERVICES | 31/07/2020 | 13,991.80 |
| EF132037 | 21697 | ICT EXPRESS PTY LTD | CONSULTANCY SERVICES - IT | 31/07/2020 | 1,707.00 |
| EF132038 | 21744 | JB HI FI - COMMERCIAL | ELECTRONIC EQUIPMENT | 31/07/2020 | 891.00 |
| EF132039 | 21747 | UNICARE HEALTH | WHEELCHAIR HIRE | 31/07/2020 | 530.00 |
| EF132040 | 21946 | RYAN'S QUALITY MEATS | MEAT SUPPLIES | 31/07/2020 | 1,142.34 |
| EF132041 | 22553 | BROWNES FOOD OPERATIONS | CATERING SUPPLIES | 31/07/2020 | 499.56 |
| EF132042 | 22569 | SONIC HEALTH PLUS PTY LTD | MEDICAL SERVICES | 31/07/2020 | 1,240.80 |
| EF132043 | 22613 | VICKI ROYANS | ARTISTIC SERVICES | 31/07/2020 | 600.00 |
| EF132044 | 22624 | AUSSIE EARTHWORKS PTY LTD | EARTHWORKS | 31/07/2020 | 29,896.90 |
| EF132045 | 22639 | SHATISH CHAUHAN | TRAINING SERVICES - YOGA | 31/07/2020 | 1,260.00 |
| EF132046 | 22681 | ABBAY BLINDS & CURTAINS | BLINDS | 31/07/2020 | 264.00 |
| EF132047 | 22682 | BEAVER TREE SERVICES PTY LTD | TREE PRUNING SERVICES | 31/07/2020 | 77,903.66 |
| EF132048 | 22752 | ELGAS LIMITED | GAS SUPPLIES | 31/07/2020 | 415.70 |
| EF132049 | 22806 | PUMA ENERGY (AUSTRALIA) FUELS PTY LTD | FUEL SUPPLIES | 31/07/2020 | 34,949.73 |
| EF132050 | 22854 | LGISWA | INSURANCE PREMIUMS | 31/07/2020 | 1,123,136.67 |
| EF132051 | 22913 | AUSTRALIAN OFFICE LEADING BRANDS.COM.AU | ENVELOPES | 31/07/2020 | 1,567.50 |
| EF132052 | 23351 | COCKBURN GP SUPER CLINIC LIMITED T/A COCKBURN INTEGRATED HEALTH | LEASING FEES | 31/07/2020 | 991.01 |
| EF132053 | 23457 | TOTALLY WORKWEAR FREMANTLE | CLOTHING - UNIFORMS | 31/07/2020 | 2,639.45 |
| EF132054 | 23570 | A PROUD LANDMARK PTY LTD | LANDSCAPE CONSTRUCTION SERVICES | 31/07/2020 | 42,829.60 |
| EF132055 | 23579 | DAIMLER TRUCKS PERTH | PURCHASE OF NEW TRUCK | 31/07/2020 | 120,933.51 |
| EF132056 | 23671 | URBSOL | TRAFFIC DESIGN | 31/07/2020 | 4,224.00 |
| EF132057 | 23849 | JCB CONSTRUCTION EQUIPMENT AUSTRALIA | PLANT/MACHINERY PURCHASE & MAINTENANCE | 31/07/2020 | 387.95 |
| EF132058 | 24156 | MASTEC AUSTRALIA PTY LTD | PURCHASE OF NEW BINS | 31/07/2020 | 110.00 |
| EF132059 | 24198 | RICOCHE CIRCUS | ENTERTAINMENT SERVICES | 31/07/2020 | 2,000.00 |
| EF132060 | 24275 | TRUCK CENTRE WA PTY LTD | PURCHASE OF NEW TRUCK | 31/07/2020 | 412.50 |
| EF132061 | 24506 | AMARANTI'S PERSONAL TRAINING | PERSONAL TRAINING SERVICES | 31/07/2020 | 375.00 |
| EF132062 | 24610 | ALL FLAGS SIGNS & BANNERS | SIGNS, FLAGS, BANNERS | 31/07/2020 | 586.30 |
| EF132063 | 24655 | AUTOMASTERS SPEARWOOD | VEHICLE SERVICING | 31/07/2020 | 1,245.00 |
| EF132064 | 24748 | PEARMANS ELECTRICAL & MECHANICAL SERVICES P/L | ELECTRICAL SERVICES | 31/07/2020 | 7,047.39 |



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| EF132065 | 24864 | FREMANTLE FOOTBALL CLUB | MERCHANDISE STOCK FOR RETAIL SALE | 31/07/2020 | 3,953.68 |
| EF132066 | 24974 | SCOTT PRINT | PRINTING SERVICES | 31/07/2020 | 652.30 |
| EF132067 | 24978 | AMBIUS | PLANTS SUPPLIES | 31/07/2020 | 690.29 |
| EF132068 | 25063 | SUPERIOR PAK PTY LTD | VEHICLE MAINTENANCE | 31/07/2020 | 620.35 |
| EF132069 | 25128 | HORIZON WEST LANDSCAPE & IRRIGATION P/L | LANDSCAPING SERVICES | 31/07/2020 | 132,812.92 |
| EF132070 | 25264 | ACURIX NETWORKS PTY LTD | WIFI ACCESS SERVICE | 31/07/2020 | 6,366.80 |
| EF132071 | 25415 | JANDAKOT STOCK & PET SUPPLIES | PET SUPPLIES | 31/07/2020 | 74.75 |
| EF132072 | 25586 | ENVIROVAP PTY LTD | HIRE OF LEACHATE UNITS | 31/07/2020 | 7,425.00 |
| EF132073 | 25713 | DISCUS ON DEMAND THE TRUSTEE FOR DISCUS ON DEMAND UNIT TRUST | PRINTING SERVICES | 31/07/2020 | 93.50 |
| EF132074 | 25733 | MIRACLE RECREATION EQUIPMENT | PLAYGROUND INSTALLATION / REPAIRS | 31/07/2020 | 2,904.00 |
| EF132075 | 25736 | BLUE TANG (WA) PTY LTD T/AS EMERGE ASSOCIATES (THE TRUSTEE FOR THE R | CONSULTANCY SERVICES | 31/07/2020 | 12,543.30 |
| EF132076 | 25822 | FIT2WORK.COM.AU MERCURY SEARCH AND SELECTION PTY LTD | EMPLOYEE CHECK | 31/07/2020 | 230.34 |
| EF132077 | 25897 | COMMON GROUND TRAILS PTY LTD | CONSULTANCY SERVICES | 31/07/2020 | 1,980.00 |
| EF132078 | 25940 | LEAF BEAN MACHINE | COFFEE BEAN SUPPLY | 31/07/2020 | 440.00 |
| EF132079 | 25962 | ALL LINES | LINEMARKING SERVICES | 31/07/2020 | 1,540.00 |
| EF132080 | 26119 | BCJ PLASTIC PRODUCTS | ACRYLIC DOOR BARRIERS | 31/07/2020 | 426.80 |
| EF132081 | 26195 | PLAY CHECK | CONSULTING SERVICES | 31/07/2020 | 330.00 |
| EF132082 | 26257 | PAPERBARK TECHNOLOGIES | ARBORICULTURAL CONSULTANCY SERVICES | 31/07/2020 | 2,595.00 |
| EF132083 | 26303 | GECKO CONTRACTING TURF & LANDSCAPE MAINTENANCE | TURF & LANDSCAPE MAINTENANCE | 31/07/2020 | 274,895.71 |
| EF132084 | 26354 | ELECTROFEN | REPAIR SERVICES - SECURITY FENCES | 31/07/2020 | 418.00 |
| EF132085 | 26403 | CHES POWER GROUP | ENGINEERING SOLUTIONS / BACK UP GENERATO | 31/07/2020 | 673.75 |
| EF132086 | 26419 | EQUIFAX AUSTRALASIA CREDIT RATINGS PTY LTD | CREDIT REFERENCE CHECKS | 31/07/2020 | 460.90 |
| EF132087 | 26442 | BULLANT SECURITY PTY LTD KEY WEST LOCK SERVICE & SALES | LOCKSMITH & SECURITY SERVICES | 31/07/2020 | 1,230.02 |
| EF132088 | 26470 | SCP CONSERVATION | FENCING SERVICES | 31/07/2020 | 23,490.50 |
| EF132089 | 26476 | E-STATION PTY LTD | ELECTRICAL SERVICES | 31/07/2020 | 1,980.00 |
| EF132090 | 26574 | EVA BELLYDANCE | ENTERTAINMENT - BELLY DANCING | 31/07/2020 | 375.00 |
| EF132091 | 26600 | TIMMOTHY KELLY | ABORIGINAL CULTURAL DANCING | 31/07/2020 | 900.00 |
| EF132092 | 26606 | ENVIRO INFRASTRUCTURE PTY LTD | CONSTRUCTION& FABRICATION | 31/07/2020 | 26,802.95 |
| EF132093 | 26610 | TRACC CIVIL PTY LTD | CIVIL CONSTRUCTION | 31/07/2020 | 53,351.84 |
| EF132094 | 26625 | ANDOVER DETAILERS | CAR DETAILING SERVICES | 31/07/2020 | 1,308.00 |
| EF132095 | 26709 | TALIS CONSULTANTS PTY LTD | WASTE CONSULTANCY | 31/07/2020 | 4,845.50 |
| EF132096 | 26735 | SHANE MCMASTER SURVEYS | SURVEY SERVICES | 31/07/2020 | 6,820.00 |
| EF132097 | 26773 | LASER CORPS COMBAT ADVENTURES | ENTRY FEES | 31/07/2020 | 990.00 |
| EF132098 | 26781 | THE ARCHERY CENTRE & LASER RANGER | ENTRY FEES | 31/07/2020 | 770.00 |
| EF132099 | 26791 | MONSTERBALL AMUSEMENT & HIRE | AMUSEMENT HIRE | 31/07/2020 | 400.00 |
| EF132100 | 26811 | ROMERI MOTOR TRIMMERS | UPHOLSTERY REPAIR | 31/07/2020 | 715.00 |
| EF132101 | 26829 | PARAQAD INDUSTRIES | GENERAL PACKAGING, INDUSTRIAL CLOTH CUTT | 31/07/2020 | 12,852.83 |
| EF132102 | 26843 | ERGOLINK | ERGONOMIC OFFICE FURNITURE | 31/07/2020 | 650.75 |
| EF132103 | 26854 | IFAP | TRAINING | 31/07/2020 | 199.00 |
| EF132104 | 26888 | MEDIA ENGINE | GRAPHIC DESIGN, MARKETING, VIDEO PRODUCT | 31/07/2020 | 880.00 |
| EF132105 | 26898 | SPANDEX ASIA PACIFIC PTY LTD | SIGNAGE SUPPLIER | 31/07/2020 | 3,529.64 |
| EF132106 | 26901 | ALYKA PTY LTD | DIGITAL CONSULTANCY AND WEB DEVELOPMENT | 31/07/2020 | 577.50 |
| EF132107 | 26917 | CIRRUS NETWORKS PTY LTD | IT NETWORK & TELEPHONY SERVICES | 31/07/2020 | 6,670.89 |
| EF132108 | 26923 | WOODLANDS DISTRIBUTORS PTY LTD | RUBBISH COLLECTION EQUIPMENT | 31/07/2020 | 9,308.18 |
| EF132109 | 26938 | MAJESTIC PLUMBING | PLUMBING SERVICES | 31/07/2020 | 21,608.99 |
| EF132110 | 26940 | FLOORWEST | FLOOR COVERINGS | 31/07/2020 | 550.00 |
| EF132111 | 26946 | AV TRUCK SERVICES PTY LTD | TRUCK DEALERSHIP | 31/07/2020 | 1,944.01 |
| EF132112 | 26964 | SOUTH METROPOLITAN TAFE | EDUCATION | 31/07/2020 | 1,214.75 |
| EF132113 | 26982 | PLANTRITE | PLANTS | 31/07/2020 | 2,265.77 |
| EF132114 | 26985 | ACCESS ICON PTY LTD | DRAINAGE PRODUCTS | 31/07/2020 | 1,727.00 |
| EF132115 | 27010 | QUANTUM BUILDING SERVICES PTY LTD | BUILDING MAINTENANCE | 31/07/2020 | 16,887.17 |

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| EF132116 | 27031 | DOWNER EDI WORKS PTY LTD | ASPHALT SERVICES | 31/07/2020 | 2,667.58 |
| EF132117 | 27065 | WESTBOOKS | BOOKS | 31/07/2020 | 117.24 |
| EF132118 | 27072 | NORDIC FITNESS EQUIPMENT | FITNESS EQUIPMENT | 31/07/2020 | 1,440.00 |
| EF132119 | 27082 | KULBARDI PTY LTD | STATIONERY SUPPLIES | 31/07/2020 | 25.85 |
| EF132120 | 27092 | SPRAYLINE SPRAYING EQUIPMENT | SPRAYING EQUIPMENT | 31/07/2020 | 628.10 |
| EF132121 | 27098 | Q2 (Q-SQUARED) | DIGITAL DATA SERVICE | 31/07/2020 | 2,200.00 |
| EF132122 | 27132 | WILMA SCENINI | TRAINING & INSTRUCTOR | 31/07/2020 | 540.00 |
| EF132123 | 27154 | SUEZ RECYCLING & RECOVERY PTY LTD | WASTE SERVICES | 31/07/2020 | 162,262.10 |
| EF132124 | 27161 | NEXT POWER | SOLAR PANEL | 31/07/2020 | 19,575.00 |
| EF132125 | 27168 | NIGHTLIFE MUSIC PTY LTD | MUSIC MANAGEMENT | 31/07/2020 | 232.76 |
| EF132126 | 27169 | NATURAL POWER SOLUTIONS PTY LTD | POWER SUPPLY PROTECTION, PRODUCTS & SERV | 31/07/2020 | 9,823.00 |
| EF132127 | 27177 | INITIAL HYGIENE | HYGIENE | 31/07/2020 | 811.80 |
| EF132128 | 27189 | HEALTHSTRONG PTY LTD | HOME CARE | 31/07/2020 | 110.00 |
| EF132129 | 27246 | VEALE AUTO PARTS | SPARE PARTS MECHANICAL | 31/07/2020 | 738.00 |
| EF132130 | 27275 | HOSPITALITY TOTAL SERVICES | HOSPITALITY CONSULTANTS | 31/07/2020 | 8,558.00 |
| EF132131 | 27304 | LUSH DIGITAL MEDIA PTY LTD | MEDIA TRAINING | 31/07/2020 | 4,658.50 |
| EF132132 | 27308 | JATU CLOTHING & PPE PTY LTD | CLOTHING PPE | 31/07/2020 | 16,741.61 |
| EF132133 | 27334 | WESTCARE PRINT | PRINTING SERVICES | 31/07/2020 | 1,444.30 |
| EF132134 | 27351 | PROGRAMMED PROPERTY SERVICES | PROPERTY MAINTENANCE | 31/07/2020 | 2,294.78 |
| EF132135 | 27381 | FIT FOR LIFE EXERCISE PHYSIOLOGY | EXERCISE CLASSES | 31/07/2020 | 1,540.00 |
| EF132136 | 27392 | AXIS MAINTENANCE SERVICES PTY LTD | MAINTENANCE | 31/07/2020 | 1,342.70 |
| EF132137 | 27396 | ANKEET MEHTA SPEARWOOD NEWSPAPER ROUND DELIVERY | NEWSPAPER DELIVERY | 31/07/2020 | 10.99 |
| EF132138 | 27405 | COMBAT CLOTHING AUSTRALIA P/L | CLOTHING - PROTECTIVE | 31/07/2020 | 1,522.40 |
| EF132139 | 27423 | MECHANICAL PROJECT SERVICES PTY LTD | AIRCONDITIONING SERVICES | 31/07/2020 | 7,044.93 |
| EF132140 | 27431 | UNITED DIAMOND TOOLS | TOOLS | 31/07/2020 | 3,300.00 |
| EF132141 | 27448 | SELECTRO SERVICES PTY LTD | ELECTRICAL | 31/07/2020 | 836.00 |
| EF132142 | 27455 | SITE PROTECTIVE SERVICES | CCTV PARTS | 31/07/2020 | 64,014.16 |
| EF132143 | 27499 | HODGE COLLARD PRESTON ARCHITECTS | ARCHITECTS | 31/07/2020 | 17,087.40 |
| EF132144 | 27507 | FACILITIES FIRST AUSTRALIA | CLEANING SERVICES | 31/07/2020 | 128,995.83 |
| EF132145 | 27518 | KYOCERA DOCUMENT SOLUTIONS AUSTRALIA PTY LTD | PHOTOCOPYING MACHINES | 31/07/2020 | 3,721.91 |
| EF132146 | 27528 | MONSIDO PTY LTD | COMPUTER SOFTWARE | 31/07/2020 | 6,545.00 |
| EF132147 | 27539 | JASMIN CARPENTRY & MAINTENANCE | CARPENTRY | 31/07/2020 | 5,548.40 |
| EF132148 | 27587 | NEW GROUND WATER SERVICES PTY LTD | IRRIGATION/RETICULATION | 31/07/2020 | 77,876.04 |
| EF132149 | 27599 | DOCKPRO PTY LTD | SALES, SERVICING & INSTALLATION OF DRY D | 31/07/2020 | 2,400.00 |
| EF132150 | 27610 | ROCKWATER PTY LTD | HYDROGEOLOGICAL CONSULTANCY | 31/07/2020 | 4,250.40 |
| EF132151 | 27622 | TRUGRADE MEDICAL SUPPLIES | MEDICAL SUPPLIES | 31/07/2020 | 423.36 |
| EF132152 | 27631 | AQUATIC SERVICES WA PTY LTD | POOL EQUIPMENT & MAINTENANCE | 31/07/2020 | 7,045.50 |
| EF132153 | 27635 | THREAT PROTECT | SECURITY | 31/07/2020 | 198.00 |
| EF132154 | 27657 | POSITIVE BALANCE MASSAGE | MASSAGE THERAPY | 31/07/2020 | 400.00 |
| EF132155 | 27676 | BLUE FORCE PTY LTD | SECURITY SERVICES | 31/07/2020 | 933.00 |
| EF132156 | 27695 | QTM PTY LTD | TRAFFIC MANAGEMENT | 31/07/2020 | 1,104.40 |
| EF132157 | 27701 | PERTH BETTER HOMES | SHADE SAILS | 31/07/2020 | 3,572.80 |
| EF132158 | 27722 | METRA AUSTRALIA | SOFTWARE | 31/07/2020 | 2,639.70 |
| EF132159 | 27749 | ADVISIAN PTY LTD | CONSULTING - ENGINNERING | 31/07/2020 | 3,701.50 |
| EF132160 | 27770 | PLANET HONDA MOTORCYCLES | MOTOBIKE RIDING EQUIPMENT | 31/07/2020 | 889.01 |
| EF132161 | 27809 | RA-ONE PTY LTD | SOFTWARE | 31/07/2020 | 23,595.00 |
| EF132162 | 27829 | SMEC AUSTRALIA PTY. LTD. | CONSULTANCY - ENGINEERING | 31/07/2020 | 31,790.00 |
| EF132163 | 27842 | LIGHT HOUSE LAUNDRY | LAUNDERING | 31/07/2020 | 65.45 |
| EF132164 | 27850 | DOWSING GROUP PTY LTD | CONCRETING SERVICES | 31/07/2020 | 1,648.08 |
| EF132165 | 27858 | DIRTZ TRACK | DESIGN BMX & PUMPTRACK FACILITIES | 31/07/2020 | 8,550.00 |
| EF132166 | 27863 | CARERS PLUS | NURSING SERVICES | 31/07/2020 | 1,798.26 |



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| EF132167 | 27869 | SELECT FRESH PTY LTD | FOOD SUPPLIE,FRUIT & VEG | 31/07/2020 | 98.90 |
| EF132168 | 27881 | BUSHFIRE SAFETY CONSULTING PTY LTD | BUSHFIRE RISK CONSULTING | 31/07/2020 | 6,820.00 |
| EF132169 | 27894 | LIFECARE HOMECARE | HEALTHCARE | 31/07/2020 | 1,050.50 |
| EF132170 | 27898 | AMANA LIVING TRAINING INSTITUTE | TRAINING & EDUCATION | 31/07/2020 | 945.96 |
| EF132171 | 27908 | RAUBEX CONSTRUCTION | ENGINEERING CIVIL - CONTRACT C100684 | 31/07/2020 | 350,848.95 |
| EF132172 | 27917 | GO DOORS PTY LTD | DOOR MAINTENANCE & REPAIR | 31/07/2020 | 1,965.98 |
| EF132173 | 27930 | BE PROJECTS (WA) PTY LTD | CONSTRUCTION SERVICES - CONTRACT C100683 | 31/07/2020 | 355,498.99 |
| EF132174 | 27935 | ART DINOUVEAU PTY LTD | PLAYGROUND EQUIPMENT | 31/07/2020 | 36,212.00 |
| EF132175 | 27953 | TRUCKLINE | SPARE PARTS, TRUCK/TRAILER | 31/07/2020 | 66.04 |
| EF132176 | 27962 | BEINGTHERE SOLUTIONS PTY LTD | COMMUNICATIONS | 31/07/2020 | 8,250.00 |
| EF132177 | 27965 | STANTEC AUSTRALIA PTY LTD | ENGINEERING SERVICES | 31/07/2020 | 4,681.60 |
| EF132178 | 27978 | FRONTLINE SAFETY AUSTRALIA PTY LTD | CLOTHING - UNIFORMS | 31/07/2020 | 1,527.97 |
| EF132179 | 27984 | SABRINA FENWICK | EXCERCISE CLASSES | 31/07/2020 | 320.00 |
| EF132180 | 27986 | DAILY LIVING PRODUCTS | MOBILITY EQUIP | 31/07/2020 | 215.00 |
| EF132181 | 27993 | BUSINESS BASE | OFFICE FURNITURE | 31/07/2020 | 738.00 |
| EF132182 | 27998 | STYLECRAFT AUSTRALIA | FURNITURE - OFFICE | 31/07/2020 | 2,187.80 |
| EF132183 | 27999 | EVENTS INDUSTRY ASSOCIATION (WA) | MEMBERSHIP | 31/07/2020 | 175.00 |
| EF132184 | 10747 | IINET LIMITED | INTERNET SERVICES | 31/07/2020 | 1,319.81 |
| EF132185 | 11758 | WATER CORP UTILITY ACCOUNT ONLY | WATER USAGE / SUNDRY CHARGES | 31/07/2020 | 20,341.95 |
| | | TOTAL OF 784 EFT PAYMENTS | | | 14,071,497.59 |
| | | LESS: CANCELLED EFT PAYMENTS: | | | |
| EF131033 | | CURTIN UNIVERSITY | | 7/07/2020 | -7,150.00 |
| EF130947 | | IVANA LUKIC | | 8/07/2020 | -576.00 |
| EF131016 | | DESOMD OBRIAN | | 9/07/2020 | -300.00 |
| EF130963 | | BIRMAN & RIDE | | 16/07/2020 | -238.91 |
| EF130993 | | MARIA VILLAMAGNA | | 20/07/2020 | -200.00 |
| EF131497 | | KADEN C SARGEANT | | 20/07/2020 | -119.50 |
| | | | | | - 8,584.41 |
| | | TOTAL EFT PAYMENTS (NET OF CANCELLED PAYMENTS) | | | 14,062,913.18 |
| | | ADD: BANK FEES AND CREDIT CARD PAYMENTS | | | |
| | | BANK FEES | | | 15.54 |
| | | MERCHANT FEES COC | | | 6,034.60 |
| | | MERCHANT FEES MARINA | | | 777.60 |
| | | MERCHANT FEES ARC | | | 3617.41 |
| | | MERCHANT FEES VARIOUS OUT CENTRES | | | 2,040.12 |
| | | NATIONAL BPAY CHARGE | | | 1,623.68 |
| | | RTGS/ACLR FEE | | | |
| | | NAB TRANSACT FEE | | | 258.54 |
| | | MERCHANDISE / OTHER FEES | | | 99.00 |
| | | CBA CREDIT CARD PAYMENT | | | 78,746.92 |
| | | | | | 93,213.41 |
| | | ADD: PAYROLL PAYMENTS | | | |
| | | COC05/07/20 Pmt 000163112428 City of Cockburn | | 8/07/2020 | 1,387,969.60 |
| | | COC09/07/20 Pmt 000163202223 City of Cockburn | | 9/07/2020 | 849.00 |
| | | COC12/07/20 Pmt 000163414066 City of Cockburn | | 14/07/2020 | 6,148.00 |
| | | COC19/07/20 Pmt 000163916743 City of Cockburn | | 22/07/2020 | 1,420,395.48 |
| | | COC23/07/20 Pmt 000164011178 City of Cockburn | | 23/07/2020 | 2,092.71 |



| | | | | |
|--|--|-----------------------------------|--|---------------|
| | | | | 2,817,454.79 |
| | | TOTAL PAYMENTS MADE FOR THE MONTH | | 16,973,581.38 |



| Credit Card Transactions June 2020 | |
|------------------------------------|------------------|
| Card Holder Name | \$ |
| ALEXANDRA K MORTON | 3,079.50 |
| ALISON WATERS | 1,441.00 |
| ANTON LEES | 3,144.20 |
| ASANKA VIDANAGE | 423.98 |
| CHRISTOPHER BEATON | 1,889.28 |
| CLIFF MCKINLEY | 3,847.76 |
| COLLEEN MILLER | 174.60 |
| COURTNEE THOMSON | 454.42 |
| KAREN O'REILLY | 694.91 |
| LINDA SEYMOUR | 3,955.23 |
| LINDA WALKER | 842.50 |
| LORENZO SANTORIELLO | 470.81 |
| MARIE LA FRENNAIS | 79.00 |
| MICHAEL EMERY | 1,888.19 |
| MIJALCE DANILOV | 81.15 |
| MISS JESSICA DONALD | 238.79 |
| MR ANTONIO NATALE | 10,646.49 |
| MR BRETT FELLOWS | 2,966.58 |
| MR BRETT MCEWIN | 3,658.32 |
| MR C MACMILLAN | 1,991.68 |
| MR CHARLES SULLIVAN | 147.25 |
| MR CLIFFORD RYAN | 1,678.28 |
| MR CLIVE J CROCKER | 1,476.94 |
| MR DANIEL ARNDT | 741.23 |
| MR DONALD M GREEN | 1,202.85 |
| MR GLEN WILLIAMSON | 279.90 |
| MR GLENN PETHICK | 1,881.49 |
| MR JOHN WEST | 498.11 |
| MR LAWLEY MARIN YUKICH | 593.77 |
| MR LYALL DAVIESON | 1,559.84 |
| MR MICHAEL HAYNES | 362.62 |
| MR NICHOLAS JONES | 3,154.28 |
| MR PAUL HOGAN | 180.61 |
| MR PAUL J DE BRUIN | 271.11 |
| MR S ATHERTON | 83.00 |
| MR S PALMER | 1,617.17 |
| MR TRAVIS MOORE | 449.74 |
| MRS GLORIA ASKANDER | 374.23 |
| MRS J KIURSKI | 2,489.60 |
| MRS JULIE MCDONALD | 1,797.55 |
| MRS KIM HUNTER | 1,895.42 |
| MRS S SEYMOUR-EYLES | 3,033.62 |
| MRS SANDRA TAYLOR | 1,416.60 |
| MRS SARAH KAHLE | 1,061.89 |
| MRS SHARON STILL | 316.43 |
| MS BARBARA FREEMAN | 659.80 |
| MS CAROLINE LINDSAY | 475.44 |
| MS DONNA JORDAN | 381.40 |
| MS GAIL M BOWMAN | 1,425.67 |
| MS JILL ZUMMACH | 8.00 |
| MS MICHELLE CHAMPION | 6.45 |
| MS SAMANTHA BAARD | 1,372.63 |
| MS SAMANTHA STANDISH | 493.58 |
| MS SIMONE SIEBER | 2,974.32 |
| PAUL DANIEL NORLIN | 657.80 |
| RACHEL JANE PLEASANT | 632.00 |
| STEVEN JOHN ELLIOT | 1,381.89 |
| STUART DOWNING | 102.09 |
| Grand Total | 78,746.92 |

| Daniel Arndt Credit Card Transactions June 2020 | | | | |
|---|-----------------------|-----------------------------------|-----------------|---------------|
| Date | Narrative 1 | Narrative 2 | Budget Number | Amount |
| 4/06/2020 | LGPA | LGPA membership | GL116-6303 | 100.00 |
| 5/06/2020 | SECURE PARKING-164-1 | Atten WALGA breakfast meeting | OP9849-6110-853 | 9.23 |
| 29/06/2020 | PLANNING INSTITUTE OF | PIA Planning Institute membership | GL116-6303 | 632.00 |
| | | | | 741.23 |

15.2 (2020/MINUTE NO 0192) STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - JULY 2020

Author(s) N Mauricio

Attachments 1. Monthly Financial Report July 2020

RECOMMENDATION

That Council:

- (1) adopt the Monthly Financial Report for July 2020, as attached to the Agenda;
- (2) amend the 2020/21 Municipal Budget as detailed in the Monthly Financial Report for July 2020 and summarised as follows:

| | | |
|--|--------|----------|
| Operating Revenue | 19,523 | Increase |
| Operating Expenses | 19,523 | Increase |
| Capital Revenue | 97,156 | Increase |
| Capital Expenses | 97,156 | Increase |
| Net impact on closing Municipal budget surplus | Nil | |

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr M Separovich

That the recommendation be adopted.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 9/0

Background

Local Government (Financial Management) Regulations 1996 prescribe that a Local Government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:

1. Details of the composition of the closing net current assets (less restricted and committed assets);
2. Explanation for each material variance identified between YTD budgets and actuals; and
3. Any other supporting information considered relevant by the Local Government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within two months after the end of the month to which the statement relates.



The regulations require the information reported in the statement to be shown either by nature or type, statutory program or business unit. The City has chosen to report the information according to nature or type and its organisational business structure.

Local Government (Financial Management) Regulations 1996 - Regulation 34 (5) states “Each financial year, a Local Government is to adopt a percentage or value, calculated in accordance with the AAS, to be used in statements of financial activity for reporting material variances.”

This regulation requires Council to annually set a materiality threshold for the purpose of disclosing budget variances within monthly financial reporting. Council adopted a materiality threshold of \$300,000 for the 2020/21 financial year (FY) at the August 2020 ordinary Council meeting.

Detailed analysis of budget variances is an ongoing exercise, with necessary budget amendments either submitted to Council each month via this standing agenda item or included in the City’s mid-year budget review, as required by legislation.

Submission

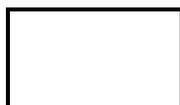
N/A

Report

The City has adopted an updated the format of the monthly financial report for the 2020/21 financial year, based on the Moore Australia (previously Moore Stephens) model financial report template. The Moore Australia model budget template was also used in preparing the 2020/21 statutory budget and the City is planning to utilise the model template for the 2019/20 annual financial report

Moore Australia has delivered financial reporting and budgeting workshops to the sector for some 20 years which include the model financial templates. They are the only accounting firm in WA that provides this service to the sector and were also responsible for developing the WA Local Government Accounting Manual on behalf of the Department of Local Government. This was last updated in 2012 and is now outdated in some parts.

This initiative is largely driven by recent changes to the Australian Accounting Standards, including the introduction of several new standards that apply to local government reporting. Adopting the Moore Australia model templates for financial reporting requirements is aiming for greater consistency across the City’s financial reporting and reduced non-compliance and audit risk. Ultimately, this serves to improve readability and the quality of local government financial reports for end users. An increasing number of large metro Councils are adopting the Moore Australia model templates including the Cities of Wanneroo,



Fremantle, Kwinana and Gosnells. The majority of WA's regional Councils rely on these models for their financial reporting.

While the new report format includes all the information required by the legislation and other useful information, some previously included information is currently not reported in the new report:

- The original Annual Budget figures are not included, as budget performance is always measured against the latest amended Council budget.
- Capital budget commitments (showing amount of budget committed by way of a supply contract/purchase order).
- Main sources of revenue under the Fees and Charges type (eg: waste, ARC, marina).
- Directorate level detail for the Salaries and Materials and Contracts expense types.

There is nothing preventing the City modifying the model template to include any other information deemed valuable. It is envisaged that the model report will be progressively improved by the City, as well as receive an update by Moore Australia each year.

Budget Amendments

There are a number of budget amendments proposed within the financial report as follows:

1. Additional grant funding for the Youth Outreach Program (\$7023)
2. A Local Healthy Food grant received (\$12,500)
3. Forfeited development bond to partly fund parks construction at 213 Frankland Ave Hammond Park (\$97,156)
4. Budget Contingency funding reallocation to various activities (\$95,000):
 - i. Community Engagement –contract staff cost (\$35,000)
 - ii. Community Development consultant (\$50,000)
 - iii. Increase Senior program funding (\$10,000).

Opening Surplus

The opening surplus from FY 2019/20 was budgeted at \$2.0 million, with another \$9.88 million required to also fund carry forward projects. The unaudited actual opening surplus is currently shown at \$10.18 million, but is still subject to EOFY processing for 2019/20. A separate report on the carry forward projects and closing surplus for 2019/20 is being presented to this Council meeting.

Closing Surplus

The City's actual closing surplus to the end of July of \$5.45 million was \$0.17 million under the YTD budget target. This budget variance is a culmination of all variances across the operating and capital programs, as well as any variance in the opening budget surplus.



Operating Revenue

Operating revenue of \$4.24 million was over the YTD budget by \$0.81 million. The following table summarises the operating revenue budget performance by nature and type:

| | Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | Var. % (b)-(a)/(a) | Var. |
|---|--------------------|----------------------|----------------------|--------------------|-----------------------|------|
| | \$ | \$ | \$ | \$ | % | |
| Revenue from operating activities | | | | | | |
| Rates | 107,770,659 | 0 | 0 | 0 | 0.00% | |
| Specified area rates | 550,600 | 0 | 0 | 0 | 0.00% | |
| Operating grants, subsidies and contributions | 13,271,203 | 1,137,142 | 1,173,538 | 36,396 | 3.20% | |
| Fees and charges | 28,021,920 | 2,054,822 | 2,773,837 | 719,015 | 34.99% | ▲ |
| Service charges | 0 | 0 | 0 | 0 | 0.00% | |
| Interest earnings | 2,930,000 | 241,667 | 221,413 | (20,254) | (8.38%) | |
| Other revenue | 0 | 0 | 0 | 0 | 0.00% | |
| Profit on disposal of assets | 5,439,536 | 0 | 73,988 | 73,988 | 0.00% | |
| | 157,983,918 | 3,433,631 | 4,242,776 | 809,145 | | |

The material variances identified within business units for the month included:

- Fees and Charges (\$0.72 million under budget)
 - No material variances although Port Coogee marina pen fees were \$0.25 million ahead of budget due to a timing issue (two year lease payments brought forward into July).

Operating Expenditure

Operating expenditure for the month of \$9.45 million was under the YTD budget by \$2.16 million. The following table shows the operating expenditure budget variance at the nature and type level:

| | Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | Var. % (b)-(a)/(a) | Var. |
|--|----------------------|----------------------|----------------------|--------------------|-----------------------|------|
| | \$ | \$ | \$ | \$ | % | |
| Expenditure from operating activities | | | | | | |
| Employee costs | (61,158,584) | (3,167,219) | (3,062,519) | 104,700 | 3.31% | |
| Materials and contracts | (39,662,955) | (3,206,411) | (1,293,842) | 1,912,569 | 59.65% | ▲ |
| Utility charges | (5,749,538) | (456,580) | (415,534) | 41,046 | 8.99% | |
| Depreciation on non-current assets | (35,641,134) | (2,980,754) | (2,992,042) | (11,288) | (0.38%) | |
| Interest expenses | (696,000) | 0 | 0 | 0 | 0.00% | |
| Insurance expenses | (1,723,200) | (1,128,200) | (1,033,742) | 94,458 | 8.37% | |
| Other expenditure | (9,565,264) | (671,111) | (649,104) | 22,007 | 3.28% | |
| Loss on disposal of assets | (49,487) | 0 | 0 | 0 | 0.00% | |
| | (154,246,162) | (11,610,275) | (9,446,783) | 2,163,492 | | |



The material variances identified within business units for the month included:

- Material and Contracts (\$1.91 million under budget)
 - The Parks and Environment Services business unit was \$0.54 million under YTD budget comprising Parks Maintenance (\$0.34m under) and Environmental Services (\$0.20m under), but these are considered timing variances.
 - The Infrastructure Services business unit was \$0.33 million under YTD budget, mostly comprising Facilities Maintenance under by \$0.26 million (timing variance).

Capital Expenditure

The City's adopted capital budget of \$51.22 million increases to \$88.77 million with the inclusion of carried forward projects from 2019/20.

The City's capital expenditure for the month was \$3.19 million against the YTD budget of \$2.70 million (+\$0.49m timing variance).

The following table details this budget variance by asset class:

| Capital acquisitions | Amended | | YTD Actual | YTD Actual Variance |
|---|-------------------|------------------|------------------|---------------------|
| | Budget | YTD Budget | | |
| | \$ | \$ | \$ | \$ |
| Land | 2,800,000 | | 280,000 | 280,000 |
| Land - vested in and under the control of Council | | | 1,052,207 | 1,052,207 |
| Buildings | 31,157,846 | 1,068,131 | 517,552 | (550,579) |
| Furniture and equipment | 4,472 | | | 0 |
| Plant and equipment | 7,202,853 | 193,000 | 194,070 | 1,070 |
| Information technology | 1,048,091 | 157,085 | 14,960 | (142,125) |
| Infrastructure - roads | 19,877,974 | 175,652 | 421,716 | 246,064 |
| Infrastructure - drainage | 1,955,892 | 0 | 74,399 | 74,399 |
| Infrastructure - footpath | 2,587,217 | 8,333 | 1,486 | (6,847) |
| Infrastructure - parks hard | 7,716,826 | 507,868 | 114,862 | (393,006) |
| Infrastructure - parks landscaping | 3,144,493 | 39,117 | 50,631 | 11,514 |
| Infrastructure - landfill site | 5,092,043 | 6,667 | 468,810 | 462,143 |
| Infrastructure - marina | 5,806,029 | 513,518 | 1,350 | (512,168) |
| Infrastructure - coastal | 372,473 | 30,356 | 0 | (30,356) |
| Payments for Capital Acquisitions | 88,766,209 | 2,699,727 | 3,192,043 | 492,316 |
| Total Capital Acquisitions | 88,766,209 | 2,699,727 | 3,192,043 | 492,316 |

Any significant YTD individual project budget variances are not determined, given the carried forward projects are yet to be phased for budgeting purposes. Note that the \$1.05 million budget variance reported above for vested land will be resolved through a reclassification of existing budget for Verde Drive land acquisition (currently classified under materials and contracts).



Non-Operating Grants, Subsidies and Contributions

A total of \$1.63 million has been received against the YTD budget of \$0.42 million. This comprised the following:

- Non-Operating Grants and Subsidies and Contributions – none received versus YTD budget of \$0.1 million
- Developer Contribution Plans - \$1.13 million received against the YTD budget of \$0.29 million, with \$0.92 million coming in for community infrastructure (DCP13) due to a rise in development activity.
- Development Contribution - \$0.50 million invoiced to Main Roads for their part contribution towards Karel Ave over five years (ahead of YTD budget and is a timing variance).

Reserve Transfers

A detailed schedule of the City's financial reserves is attached to the financial report, showing total reserves of \$152.90 million at reporting date.

There were transfers into reserves of \$1.06 million to the end of the month, all relating to developer contribution plans. There were no transfers made out of reserve processed during the month. Interest earnings allocated (to restricted and developer contribution reserves) were \$17k for the month, reflecting the low rates of interest being earned on term deposits (below 1%).

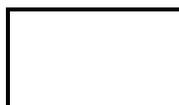
Cash and Investments

The closing cash and financial investment holding at month's end totalled \$167.52 million (down from \$175.48 million last month). The City's financial reserves comprised \$152.90 million of the total, another \$4.02 million covered the City's bonds and deposits liability, with the remaining \$10.6 million representing unrestricted funds for the City's operating activities.

Investment Performance, Ratings and Maturity

The City's investment portfolio yielded a weighted annualised return of 1.47 percent for the month (down from 1.52% last month and 1.60% the month before). The City's longer dated deposits continue to buffer the overall yield, with new investment placements now attracting rates between 0.70% to 0.90%. The yield for July outperformed the City's target rate of 1.05 percent (RBA cash rate of 0.25 percent plus 0.80 percent performance margin) by 0.42 percent. Interest from term deposit investments was \$0.22 million, slightly below the YTD budget of \$0.24 million.

Current investments held are compliant with Council's Investment Policy, other than those made under previous policy and statutory provisions. This includes Australian reverse mortgage funds with a face



value of \$2.526 million and book value of \$0.951 million (net of a \$1.575 million impairment provision), which continue paying interest and returning capital (\$0.47 million returned to date of the original \$3.0 million). Term deposits previously placed with foreign owned banks currently totalling \$6.5 million also now sit outside Council policy. These are redeemed and reinvested with Australian banks as and when they fall due (last one due in September 2020).

After cutting the cash rate to a historic low of 0.25% on 20th March 2020, the RBA has left the rate steady since then and is not expected to move any lower. The RBA has stated there will not be an increase in the cash rate until there is sustainable progress made towards their goals for full employment and inflation. The City is expecting an economic environment of low interest rates for at least the next two years, limiting investment returns from its substantial cash holdings. Whilst the City is allowed to invest in bank term deposits and Government issued bonds for up to three year terms, the relatively flat yield curve is not offering enough incentive to invest longer term.

The City’s investments fall within the following Standard and Poor’s short term risk rating categories:

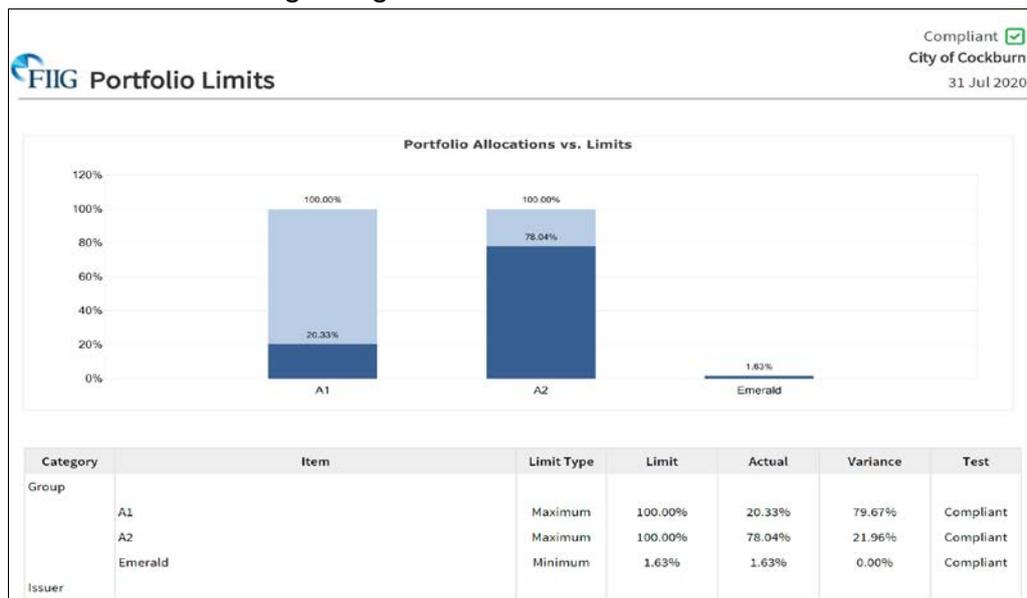


Figure 1: Portfolio allocations compared to Investment Policy limits

The City’s investment portfolio duration as at the end of the month was 141 days (slight increase on 140 days last month). The maturity profile of the City’s investments is graphically depicted below, showing adequate maturities across the next couple of months to satisfy liquidity requirements, noting that rates payments will substantially increase cash holdings in September.



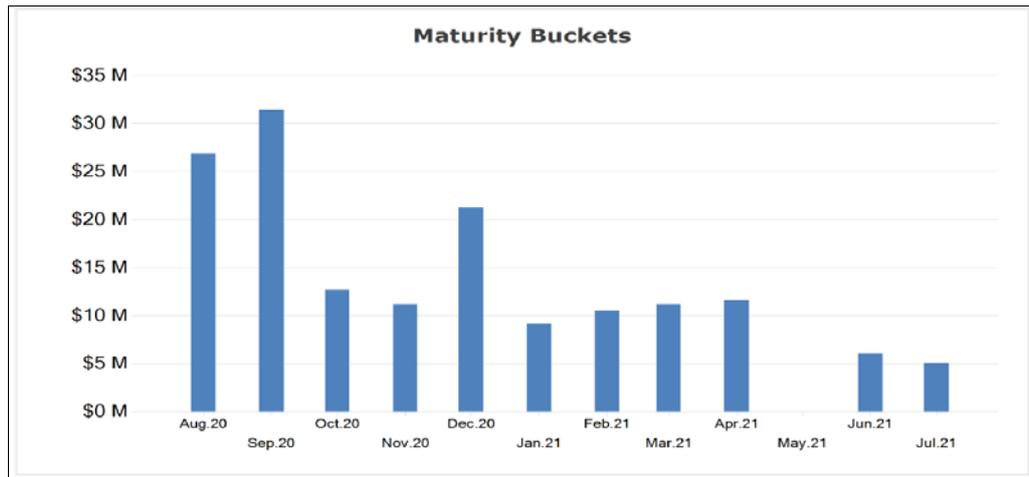


Figure 2: Council Investment Maturity Profile

Investment in Fossil Fuel Free Banks

At month end, the City held 58% or \$90.0 million of its investment portfolio with banks considered non-funders of fossil fuel related industries (slightly down from 59% previous month). The amount invested with fossil fuel free banks fluctuates month to month, due to the attractiveness of deposit rates being offered and the capacity of fossil fuel free banks to accept funds.

Rates Debt Recovery

At the end of July, the City had \$4.06 million in outstanding rates and property charges for the 2019/20 year (reduced from \$4.3 million the previous month). This amount excludes \$2.26 million in prepaid rates that will be applied to the 2020/21 rates charges levied in August). This represented 3.0 percent in uncollected charges against the \$133.1 million total rates levied to month’s end (inclusive of prior year outstanding balances and part year rating). This demonstrates that the collection of rates has not been overly impacted by the COVID-19 pandemic to this point and formal debt recovery processes have now recommenced for outstanding rates (subject to the City’s financial hardship policy).

In terms of overdue rates accounts, the City had 111 properties owing \$0.44 million under legal debt recovery processes (131 properties owing \$0.49 million previous month).

Trust Fund

At month’s end, the City held \$5.69 million within its trust fund (down from \$6.37 million last month), fully comprising POS cash in lieu contributions held for future recreation requirements across specific suburbs within the City.



Strategic Plans/Policy ImplicationsListening and Leading

Deliver sustainable governance through transparent and robust policy and processes.

Listen to and engage with our residents, business community and ratepayers with greater use of social media.

Budget/Financial Implications

The budget amendments included in the monthly financial report for adoption are self-funding and do not change the City's closing budget surplus of \$38,911.

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

Council's adopted budget for revenue, expenditure and the closing financial position could factually misrepresent actual financial outcomes if the recommended budget amendments are not adopted. Further, some services and projects could be disrupted if budgetary requirements are not appropriately addressed.

Advice to Proponent(s)/Submissioners

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



CITY OF COCKBURN**MONTHLY FINANCIAL REPORT
(Containing the Statement of Financial Activity)
For the period ending 31 July 2020****LOCAL GOVERNMENT ACT 1995
LOCAL GOVERNMENT (FINANCIAL MANAGEMENT) REGULATIONS 1996****TABLE OF CONTENTS**

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MONTHLY FINANCIAL REPORT
FOR THE PERIOD ENDED 31 JULY 2020

SUMMARY INFORMATION

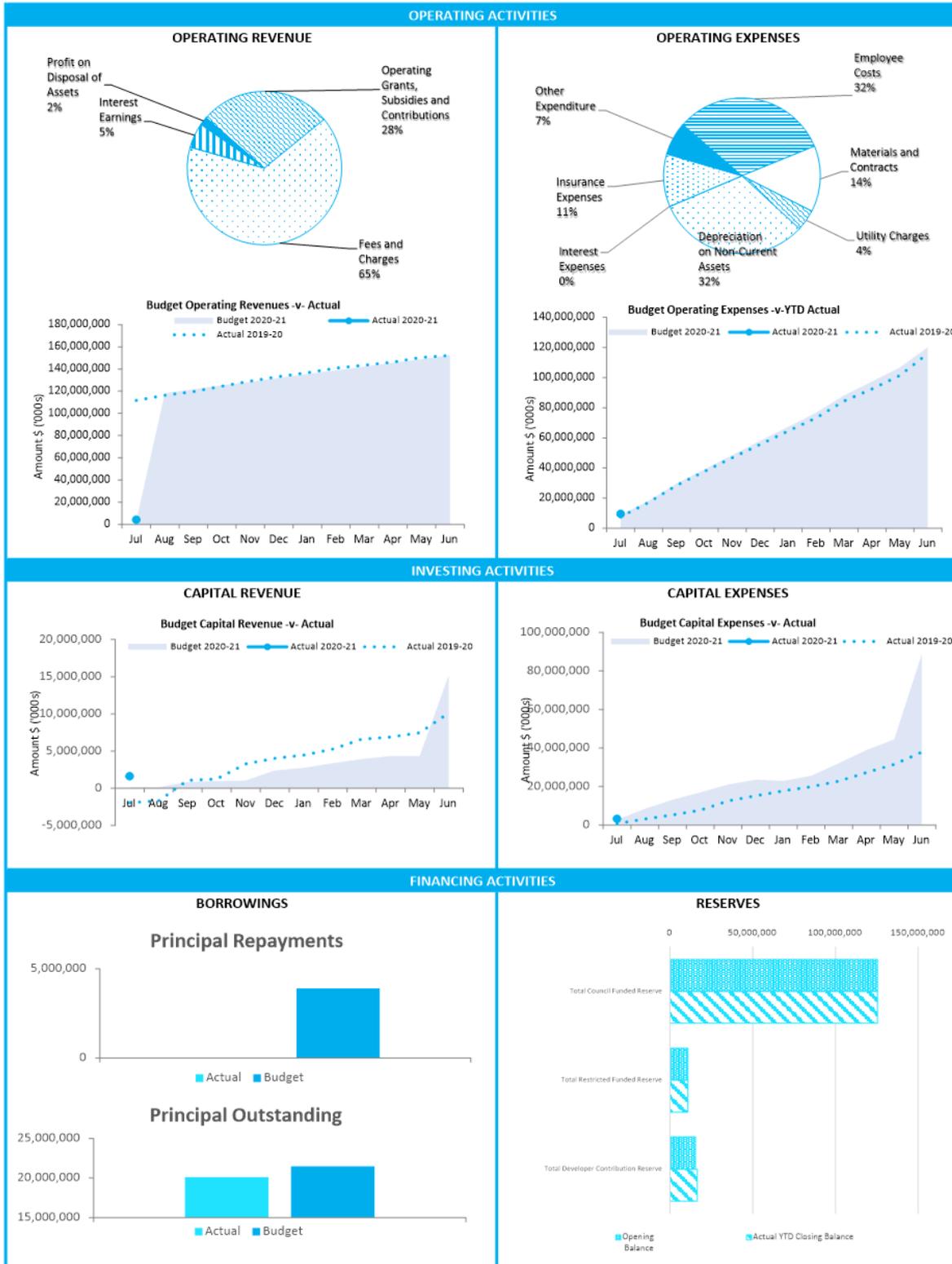
| Funding surplus / (deficit) Components | | | | | |
|---|----------------|--|-----------------|--|------------|
| Funding surplus / (deficit) | | | | | |
| | Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | |
| Opening | \$11.88 M | \$11.88 M | \$10.18 M | (\$1.70 M) | |
| Closing | \$0.04 M | \$5.62 M | \$5.45 M | (\$0.17 M) | |
| Refer to Statement of Financial Activity | | | | | |
| Cash and cash equivalents | | | | | |
| | \$167.52 M | % of total | | | |
| Unrestricted Cash | \$14.17 M | 8.5% | | | |
| Restricted Cash | \$153.35 M | 91.5% | | | |
| Refer to Note 2 - Cash and Financial Assets | | | | | |
| Key Operating Activities | | | | | |
| Amount attributable to operating activities | | | | Employee Cost | |
| Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | YTD Actual | |
| \$33.99 M | (\$5.20 M) | (\$2.16 M) | \$3.03 M | (\$3.06 M) | |
| Refer to Statement of Financial Activity | | | | % Variance | |
| | | | | YTD Budget | |
| | | | | (\$3.17 M) | |
| | | | | (3.3%) | |
| | | | | Refer to Statement of Financial Activity | |
| Rates Revenue | | Fees and Charges | | Materials & Contracts | |
| YTD Actual | \$0.00 M | % Variance | YTD Actual | \$2.77 M | % Variance |
| YTD Budget | \$0.00 M | 0.0% | YTD Budget | \$2.05 M | 35.0% |
| Refer to Statement of Financial Activity | | Refer to Statement of Financial Activity | | Refer to Statement of Financial Activity | |
| Key Investing Activities | | | | | |
| Amount attributable to investing activities | | | | | |
| Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | | |
| (\$63.10 M) | (\$2.25 M) | (\$1.49 M) | \$0.76 M | | |
| Refer to Statement of Financial Activity | | | | | |
| Proceeds on sale | | Asset Acquisition | | Capital Grants | |
| YTD Actual | \$0.07 M | % | YTD Actual | \$3.19 M | % Spent |
| Amended Budget | \$6.39 M | (98.8%) | Amended Budget | \$88.77 M | (96.4%) |
| Refer to Note 3 - Disposal of Assets | | Refer to Note 4 - Capital Acquisition | | Refer to Note 4 - Capital Acquisition | |
| Key Financing Activities | | | | | |
| Amount attributable to financing activities | | | | | |
| Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | | |
| \$17.27 M | \$1.19 M | (\$1.08 M) | (\$2.27 M) | | |
| Refer to Statement of Financial Activity | | | | | |
| Borrowings | | Reserves | | | |
| Principal repayments | \$0.00 M | Reserves balance | \$152.90 M | | |
| Interest expense | \$0.00 M | Interest earned | \$0.02 M | | |
| Principal due | \$20.10 M | Refer to Note 6 - Cash Reserves | | | |
| Refer to Note 5 - Borrowings | | | | | |

This information is to be read in conjunction with the accompanying Financial Statements and notes.



MONTHLY FINANCIAL REPORT
FOR THE PERIOD ENDED 31 JULY 2020

SUMMARY INFORMATION - GRAPHS



This information is to be read in conjunction with the accompanying Financial Statements and Notes.

STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

BY NATURE OR TYPE

| | Ref Note | Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | Var. % (b)-(a)/(a) | Var. |
|---|-------------|----------------------|----------------------|----------------------|--------------------|-----------------------|------|
| | | \$ | \$ | \$ | \$ | % | |
| Opening funding surplus / (deficit) | 1(c) | 11,878,427 | 11,878,427 | 10,182,381 | (1,696,046) | (14.28%) | ▼ |
| Revenue from operating activities | | | | | | | |
| Rates | | 107,770,659 | 0 | 0 | 0 | 0.00% | |
| Specified area rates | | 550,600 | 0 | 0 | 0 | 0.00% | |
| Operating grants, subsidies and contributions | | 13,271,203 | 1,137,142 | 1,173,538 | 36,396 | 3.20% | |
| Fees and charges | | 28,021,920 | 2,054,822 | 2,773,837 | 719,015 | 34.99% | ▲ |
| Interest earnings | | 2,930,000 | 241,667 | 221,413 | (20,254) | (8.38%) | |
| Profit on disposal of assets | | 5,439,536 | 0 | 73,988 | 73,988 | 0.00% | |
| | | 157,983,918 | 3,433,631 | 4,242,776 | 809,145 | | |
| Expenditure from operating activities | | | | | | | |
| Employee costs | | (61,158,584) | (3,167,219) | (3,062,519) | 104,700 | 3.31% | |
| Materials and contracts | | (39,662,955) | (3,206,411) | (1,293,842) | 1,912,569 | 59.65% | ▲ |
| Utility charges | | (5,749,538) | (456,580) | (415,534) | 41,046 | 8.99% | |
| Depreciation on non-current assets | | (35,641,134) | (2,980,754) | (2,992,042) | (11,288) | (0.38%) | |
| Interest expenses | | (696,000) | 0 | 0 | 0 | 0.00% | |
| Insurance expenses | | (1,723,200) | (1,128,200) | (1,033,742) | 94,458 | 8.37% | |
| Other expenditure | | (9,565,264) | (671,111) | (649,104) | 22,007 | 3.28% | |
| Loss on disposal of assets | | (49,487) | 0 | 0 | 0 | 0.00% | |
| | | (154,246,162) | (11,610,275) | (9,446,783) | 2,163,492 | | |
| Non-cash amounts excluded from operating activities | 1(a) | 30,251,085 | 2,980,754 | 3,039,630 | 58,876 | 1.98% | |
| Amount attributable to operating activities | | 33,988,841 | (5,195,890) | (2,164,377) | 3,031,513 | | |
| Investing activities | | | | | | | |
| Proceeds from non-operating grants, subsidies and contributions | | 19,272,146 | 416,731 | 1,628,423 | 1,211,692 | 290.76% | ▲ |
| Proceeds from disposal of assets | 3 | 6,393,380 | 30,000 | 73,988 | 43,988 | 146.63% | |
| Payments for property, plant and equipment and infrastructure | 4 | (88,766,209) | (2,699,727) | (3,192,043) | (492,316) | (18.24%) | ▼ |
| Amount attributable to investing activities | | (63,100,683) | (2,252,996) | (1,489,632) | 763,364 | | |
| Financing Activities | | | | | | | |
| Proceeds from new debentures | 5 | 5,277,400 | 0 | 0 | 0 | 0.00% | |
| Transfer from reserves | 6 | 60,203,371 | 1,698,538 | 0 | (1,698,538) | (100.00%) | ▼ |
| Repayment of debentures | 5 | (3,900,000) | 0 | 0 | 0 | 0.00% | |
| Transfer to reserves | 6 | (44,308,445) | (506,956) | (1,078,950) | (571,994) | (112.83%) | ▼ |
| Amount attributable to financing activities | | 17,272,326 | 1,191,582 | (1,078,950) | (2,270,532) | | |
| Closing funding surplus / (deficit) | 1(c) | 38,911 | 5,621,123 | 5,449,422 | (171,701) | | |

KEY INFORMATION

▲ ▼ Indicates a variance between Year to Date (YTD) Actual and YTD Actual data as per the adopted materiality threshold.

Refer to Note 10 for an explanation of the reasons for the variance.

This statement is to be read in conjunction with the accompanying Financial Statements and Notes.

KEY TERMS AND DESCRIPTIONS FOR THE PERIOD ENDED 31 JULY 2020

REVENUE

RATES

All rates levied under the *Local Government Act 1995*. Includes general, differential, specified area rates, minimum rates, interim rates, back rates, ex-gratia rates, less discounts and concessions offered. Exclude administration fees, interest on instalments, interest on arrears, service charges and sewerage rates.

OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS

Refers to all amounts received as grants, subsidies and contributions that are not non-operating grants.

NON-OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS

Amounts received specifically for the acquisition, construction of new or the upgrading of identifiable non financial assets paid to a local government, irrespective of whether these amounts are received as capital grants, subsidies, contributions or donations.

REVENUE FROM CONTRACTS WITH CUSTOMERS

Revenue from contracts with customers is recognised when the local government satisfies its performance obligations under the contract.

FEES AND CHARGES

Revenues (other than service charges) from the use of facilities and charges made for local government services, sewerage rates, rentals, hire charges, fee for service, photocopying charges, licences, sale of goods or information, fines, penalties and administration fees. Local governments may wish to disclose more detail such as rubbish collection fees, rental of property, fines and penalties, other fees and charges.

SERVICE CHARGES

Service charges imposed under *Division 6 of Part 6 of the Local Government Act 1995*. *Regulation 54 of the Local Government (Financial Management) Regulations 1996* identifies these as television and radio broadcasting, underground electricity and neighbourhood surveillance services. Exclude rubbish removal charges. Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

INTEREST EARNINGS

Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

OTHER REVENUE / INCOME

Other revenue, which can not be classified under the above headings, includes dividends, discounts, rebates etc.

PROFIT ON ASSET DISPOSAL

Excess of assets received over the net book value for assets on their disposal.

NATURE OR TYPE DESCRIPTIONS

EXPENSES

EMPLOYEE COSTS

All costs associate with the employment of person such as salaries, wages, allowances, benefits such as vehicle and housing, superannuation, employment expenses, removal expenses, relocation expenses, worker's compensation insurance, training costs, conferences, safety expenses, medical examinations, fringe benefit tax, etc.

MATERIALS AND CONTRACTS

All expenditures on materials, supplies and contracts not classified under other headings. These include supply of goods and materials, legal expenses, consultancy, maintenance agreements, communication expenses, advertising expenses, membership, periodicals, publications, hire expenses, rental, leases, postage and freight etc. Local governments may wish to disclose more detail such as contract services, consultancy, information technology, rental or lease expenditures.

UTILITIES (GAS, ELECTRICITY, WATER, ETC.)

Expenditures made to the respective agencies for the provision of power, gas or water. Exclude expenditures incurred for the reinstatement of roadwork on behalf of these agencies.

INSURANCE

All insurance other than worker's compensation and health benefit insurance included as a cost of employment.

LOSS ON ASSET DISPOSAL

Shortfall between the value of assets received over the net book value for assets on their disposal.

DEPRECIATION ON NON-CURRENT ASSETS

Depreciation expense raised on all classes of assets.

INTEREST EXPENSES

Interest and other costs of finance paid, including costs of finance for loan debentures, overdraft accommodation and refinancing expenses.

OTHER EXPENDITURE

Statutory fees, taxes, allowance for impairment of assets, member's fees or State taxes. Donations and subsidies made to community groups.

STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

STATUTORY REPORTING BY BUSINESS UNIT

| | Ref Note | Amended Budget | YTD Budget (a) | YTD Actual (b) | Var. \$ (b)-(a) | Var. % (b)-(a)/(a) | Var. |
|---|----------|----------------|----------------|----------------|-----------------|--------------------|------|
| | | \$ | \$ | \$ | \$ | % | |
| Opening funding surplus / (deficit) | 1(c) | 11,878,427 | 11,878,427 | 10,182,381 | (1,696,046) | (14.28%) | ▼ |
| Revenue from operating activities | | | | | | | |
| Executive Services | | 0 | 0 | 500 | 500 | 0.00% | |
| Financial Services | | 113,574,559 | 248,753 | 287,633 | 38,880 | 15.63% | |
| Information Services | | 1,500 | 125 | 91 | (34) | (27.20%) | |
| Human Resource Management | | 297,000 | 24,749 | 4,961 | (19,788) | (79.95%) | |
| Library Services | | 48,550 | 4,046 | 3,642 | (404) | (9.99%) | |
| Recreation and Community Safety | | 12,774,404 | 884,110 | 1,034,863 | 150,753 | 17.05% | |
| Community Development and Services | | 9,551,501 | 991,456 | 1,158,266 | 166,810 | 16.82% | |
| Corporate Communications | | 104,600 | 50 | 0 | (50) | (100.00%) | |
| Governance and Risk Management | | 800 | 67 | 215 | 148 | 220.90% | |
| Statutory Planning Services | | 1,002,000 | 81,192 | 102,372 | 21,180 | 26.09% | |
| Strategic Planning Services | | 8,048,845 | 218,122 | 119,782 | (98,340) | (45.08%) | |
| Building Services | | 1,280,547 | 66,454 | 74,639 | 8,185 | 12.32% | |
| Environmental Health Services | | 238,934 | 127,411 | 261,998 | 134,587 | 105.63% | |
| Waste Services | | 7,862,147 | 464,428 | 656,729 | 192,301 | 41.41% | |
| Parks and Environmental Services | | 1,562,965 | 130,154 | 4,382 | (125,772) | (96.63%) | |
| Engineering Services | | 283,472 | 16,909 | 9,481 | (7,428) | (43.93%) | |
| Infrastructure Services | | 1,352,094 | 175,605 | 523,222 | 347,617 | 197.95% | ▲ |
| | | 157,983,918 | 3,433,631 | 4,242,776 | 809,145 | | |
| Expenditure from operating activities | | | | | | | |
| Executive Services | | (4,632,603) | (333,939) | (315,363) | 18,576 | 5.56% | |
| Executive Support Services | | (70,198) | 3,912 | 15,985 | 12,073 | (308.61%) | |
| Strategy and Civic Support | | (377,704) | (6,124) | 15,084 | 21,208 | 346.31% | |
| Financial Services | | (27,333) | (833,202) | (772,597) | 60,605 | 7.27% | |
| Information Services | | (483,976) | (17,234) | 70,686 | 87,920 | 510.15% | |
| Human Resource Management | | (228,520) | 69,214 | 110,870 | 41,656 | (60.18%) | |
| Library Services | | (5,827,807) | (390,653) | (344,987) | 45,666 | 11.69% | |
| Recreation and Community Safety | | (26,769,723) | (1,799,176) | (1,380,393) | 418,783 | 23.28% | ▲ |
| Community Development and Services | | (16,512,733) | (1,042,345) | (848,746) | 193,599 | 18.57% | |
| Corporate Communications | | (2,494,654) | (102,949) | (29,798) | 73,151 | 71.06% | |
| Governance and Risk Management | | (5,951,692) | (520,485) | (500,091) | 20,394 | 3.92% | |
| Statutory Planning Services | | (2,082,167) | (122,025) | (132,071) | (10,046) | (8.23%) | |
| Strategic Planning Services | | (5,394,850) | (384,234) | (231,885) | 152,349 | 39.65% | |
| Building Services | | (2,372,304) | (139,849) | (143,223) | (3,374) | (2.41%) | |
| Environmental Health Services | | (2,586,696) | (159,937) | (141,892) | 18,045 | 11.28% | |
| Waste Services | | (21,581,971) | (1,564,885) | (1,341,589) | 223,296 | 14.27% | |
| Parks and Environmental Services | | (24,206,172) | (1,781,034) | (1,235,415) | 545,619 | 30.63% | ▲ |
| Engineering Services | | (27,068,133) | (2,118,166) | (2,045,713) | 72,453 | 3.42% | |
| Infrastructure Services | | (5,576,926) | (367,164) | (195,645) | 171,519 | 48.71% | |
| | | (154,246,162) | (11,610,275) | (9,446,783) | 2,163,492 | | |
| Non-cash amounts excluded from operating activities | 1(a) | 30,251,085 | 2,980,754 | 3,039,630 | 58,876 | 1.98% | |
| Amount attributable to operating activities | | 33,988,841 | (5,195,890) | (2,164,377) | 3,031,513 | | |
| Investing Activities | | | | | | | |
| Proceeds from non-operating grants, subsidies and contributions | | 19,272,146 | 416,731 | 1,628,423 | 1,211,692 | 290.76% | ▲ |
| Proceeds from disposal of assets | 3 | 6,393,380 | 30,000 | 73,988 | 43,988 | 146.63% | |
| Payments for property, plant and equipment and infrastructure | 4 | (88,766,209) | (2,699,727) | (3,192,043) | (492,316) | (18.24%) | ▼ |
| Amount attributable to investing activities | | (63,100,683) | (2,252,996) | (1,489,632) | 763,364 | | |
| Financing Activities | | | | | | | |
| Proceeds from new debentures | 5 | 5,277,400 | 0 | 0 | 0 | 0.00% | |
| Transfer from reserves | 6 | 60,203,371 | 1,698,538 | 0 | (1,698,538) | (100.00%) | ▼ |
| Repayment of debentures | 5 | (3,900,000) | 0 | 0 | 0 | 0.00% | |
| Transfer to reserves | 6 | (44,308,445) | (506,956) | (1,078,950) | (571,994) | (112.83%) | ▼ |
| Amount attributable to financing activities | | 17,272,326 | 1,191,582 | (1,078,950) | (2,270,532) | | |
| Closing funding surplus / (deficit) | 1(c) | 38,911 | 5,621,123 | 5,449,422 | | | |

KEY INFORMATION

▲ ▼ Indicates a variance between Year to Date (YTD) Actual and YTD Actual data as per the adopted materiality threshold. Refer to threshold. Refer to Note 10 for an explanation of the reasons for the variance.

The material variance adopted by Council for the 2020-21 year is \$300,000 or 0.00% whichever is the greater.

This statement is to be read in conjunction with the accompanying Financial Statements and notes.



**MONTHLY FINANCIAL REPORT
FOR THE PERIOD ENDED 31 JULY 2020**

BASIS OF PREPARATION

BASIS OF PREPARATION

REPORT PURPOSE

This report is prepared to meet the requirements of *Local Government (Financial Management) Regulations 1996, Regulation 34*. Note: The statements and accompanying notes are prepared based on all transactions recorded at the time of preparation and may vary due to transactions being processed for the reporting period after the date of preparation.

BASIS OF ACCOUNTING

This statement comprises a special purpose financial report which has been prepared in accordance with Australian Accounting Standards (as they apply to local governments and not-for-profit entities) and Interpretations of the Australian Accounting Standards Board, and the *Local Government Act 1995* and accompanying regulations.

The *Local Government (Financial Management) Regulations 1996* take precedence over Australian Accounting Standards. Regulation 16 prohibits a local government from recognising as assets Crown land that is a public thoroughfare, such as land under roads, and land not owned by but under the control or management of the local government, unless it is a golf course, showground, racecourse or recreational facility of State or regional significance. Consequently, some assets, including land under roads acquired on or after 1 July 2008, have not been recognised in this financial report. This is not in accordance with the requirements of *AASB 1051 Land Under Roads paragraph 15* and *AASB 116 Property, Plant and Equipment paragraph 7*.

Accounting policies which have been adopted in the preparation of this financial report have been consistently applied unless stated otherwise. Except for cash flow and rate setting information, the report has been prepared on the accrual basis and is based on historical costs, modified, where applicable, by the measurement at fair value of selected non-current assets, financial assets and liabilities.

PREPARATION TIMING AND REVIEW

Date prepared: All known transactions up to 31 July 2020

SIGNIFICANT ACCOUNTING POLICES

CRITICAL ACCOUNTING ESTIMATES

The preparation of a financial report in conformity with Australian Accounting Standards requires management to make judgements, estimates and assumptions that effect the application of policies and reported amounts of assets and liabilities, income and expenses. The estimates and associated assumptions are based on historical experience and various other factors that are believed to be reasonable under the circumstances; the results of which form the basis of making the judgements about carrying values of assets and liabilities that are not readily apparent from other sources. Actual results may differ from these estimates.

THE LOCAL GOVERNMENT REPORTING ENTITY

All funds through which the City controls resources to carry on its functions have been included in the financial statements forming part of this financial report.

In the process of reporting on the local government as a single unit, all transactions and balances between those funds (for example, loans and transfers between funds) have been eliminated.

All monies held in the Trust Fund are excluded from the financial statements. A separate statement of those monies appears at Note 8 to these financial statements.

GOODS AND SERVICES TAX

Revenues, expenses and assets are recognised net of the amount of GST, except where the amount of GST incurred is not recoverable from the Australian Taxation Office (ATO). Receivables and payables are stated inclusive of GST receivable or payable. The net amount of GST recoverable from, or payable to, the ATO is included with receivables or payables in the statement of financial position. Cash flows are presented on a gross basis. The GST components of cash flows arising from investing or financing activities which are recoverable from, or payable to, the ATO are presented as operating cash flows.

ROUNDING OFF FIGURES

All figures shown in this statement are rounded to the nearest dollar.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

NOTE 1
STATEMENT OF FINANCIAL ACTIVITY INFORMATION

(a) Non-cash items excluded from operating activities

The following non-cash revenue and expenditure has been excluded from operating activities within the Statement of Financial Activity in accordance with Financial Management Regulation 32.

| | Notes | Amended Budget | YTD Budget (a) | YTD Actual (b) |
|--|-------|-------------------|------------------|------------------|
| Non-cash items excluded from operating activities | | | | |
| | | \$ | \$ | \$ |
| Adjustments to operating activities | | | | |
| Less: Profit on asset disposals | 3 | (5,439,536) | 0 | (73,988) |
| Movement in pensioner deferred rates (non-current) | | | | 13,132 |
| Movement in accrued debtors (non-current) | | | | 1,148 |
| Movement in employee benefit provisions (non-current) | | | | 107,296 |
| Add: Loss on asset disposals | 3 | 49,487 | 0 | 0 |
| Add: Depreciation on assets | | 35,641,134 | 2,980,754 | 2,992,042 |
| Total non-cash items excluded from operating activities | | 30,251,085 | 2,980,754 | 3,039,630 |

(b) Adjustments to net current assets in the Statement of Financial Activity

The following current assets and liabilities have been excluded from the net current assets used in the Statement of Financial Activity in accordance with *Financial Management Regulation* 32 to agree to the surplus/(deficit) after imposition of general rates.

| | | Last Year Closing 30 June 2020 | This Time Last Year 31 July 2019 | Year to Date 31 July 2020 |
|---|------|-----------------------------------|-------------------------------------|------------------------------|
| Adjustments to net current assets | | | | |
| Less: Reserves - restricted cash | 6 | (151,824,157) | (139,843,179) | (152,903,107) |
| Less: Bonds & Deposits | | (4,017,650) | (5,892,879) | (4,024,785) |
| Add: Borrowings | 5 | 865,017 | 0 | 865,017 |
| Add: Financial assets at amortised cost - non-current | 2 | 951,228 | 1,000,894 | 951,228 |
| Total adjustments to net current assets | | (154,025,562) | (144,735,163) | (155,111,647) |
| Cash and cash equivalents | 2 | 5,133,910 | 11,345,114 | 14,170,342 |
| Financial assets at amortised cost | 2 | 169,400,000 | 156,900,000 | 152,400,000 |
| Rates receivables | | 2,942,696 | 102,059,810 | 2,711,929 |
| Receivables | | 8,776,391 | 20,454,131 | 6,237,743 |
| Other current assets | | 531,810 | 101,121 | 299,679 |
| Payables | | (12,198,721) | (22,217,011) | (4,814,307) |
| Borrowings | 5 | (865,017) | 0 | (865,017) |
| Contract liabilities | 7 | (1,559,424) | 0 | (1,559,424) |
| Provisions | 7 | (7,953,702) | (7,087,876) | (8,019,876) |
| Less: Total adjustments to net current assets | 1(b) | (154,025,562) | (144,735,163) | (155,111,647) |
| Closing funding surplus / (deficit) | | 10,182,381 | 116,820,126 | 5,449,422 |

CURRENT AND NON-CURRENT CLASSIFICATION

In the determination of whether an asset or liability is current or non-current, consideration is given to the time when each asset or liability is expected to be settled. Unless otherwise stated assets or liabilities are classified as current if expected to be settled within the next 12 months, being the Council's operational cycle.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

OPERATING ACTIVITIES
NOTE 2
CASH AND FINANCIAL ASSETS

| Description | Classification | Unrestricted | Restricted | Total Cash | Trust | Institution |
|--------------------------------------|------------------------------------|-------------------|--------------------|--------------------|------------------|-------------------------|
| | | \$ | \$ | \$ | \$ | |
| Cash on hand | | | | | | |
| Cash at bank | Cash and cash equivalents | 14,142,094 | 0 | 14,142,094 | | NATIONAL AUSTRALIA BANK |
| Cash on hand | Cash and cash equivalents | 28,248 | 0 | 28,248 | | |
| Term deposits - current | Financial assets at amortised cost | 0 | 30,400,000 | 30,400,000 | | BANK OF QUEENSLAND |
| Term deposits - current | Financial assets at amortised cost | 0 | 11,000,000 | 11,000,000 | | MACQUARIE BANK |
| Term deposits - current | Financial assets at amortised cost | 0 | 60,000,000 | 60,000,000 | | MEMBERS EQUITY BANK |
| Term deposits - current | Financial assets at amortised cost | 0 | 12,000,000 | 12,000,000 | | NATIONAL AUSTRALIA BANK |
| Term deposits - current | Financial assets at amortised cost | 0 | 2,000,000 | 2,000,000 | | SUNCORP |
| Term deposits - current | Financial assets at amortised cost | 0 | 25,500,000 | 25,500,000 | | RURAL BANK |
| Term deposits - current | Financial assets at amortised cost | 0 | 5,000,000 | 5,000,000 | | AMP |
| Term deposits - current | Financial assets at amortised cost | 0 | 6,500,000 | 6,500,000 | | ICBC |
| Other investment - non current | Financial assets at amortised cost | 0 | 951,228 | 951,228 | | BARCLAYS BANK |
| Trust fund - cash at bank | Cash and cash equivalents | 0 | | | 192,782 | NATIONAL AUSTRALIA BANK |
| Trust fund - term deposits - current | Financial assets at amortised cost | 0 | | | 5,500,000 | NATIONAL AUSTRALIA BANK |
| Total | | 14,170,342 | 153,351,228 | 167,521,570 | 5,692,782 | |
| Comprising | | | | | | |
| Cash and cash equivalents | | 14,170,342 | 0 | 14,170,342 | 192,782 | |
| Financial assets at amortised cost | | 0 | 153,351,228 | 153,351,228 | 5,500,000 | |
| | | 14,170,342 | 153,351,228 | 167,521,570 | 5,692,782 | |

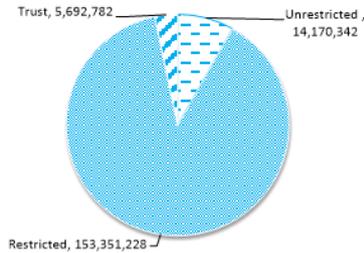
KEY INFORMATION

Cash and cash equivalents include cash on hand, cash at bank, deposits available on demand with banks and other short term highly liquid investments highly liquid investments with original maturities of three months or less that are readily convertible to known amounts of cash and which are subject to an insignificant risk of changes in value and bank overdrafts. Bank overdrafts are reported as short term borrowings in current liabilities in the statement of net current assets.

The local government classifies financial assets at amortised cost if both of the following criteria are met:

- the asset is held within a business model whose objective is to collect the contractual cashflows, and
- the contractual terms give rise to cash flows that are solely payments of principal and interest.

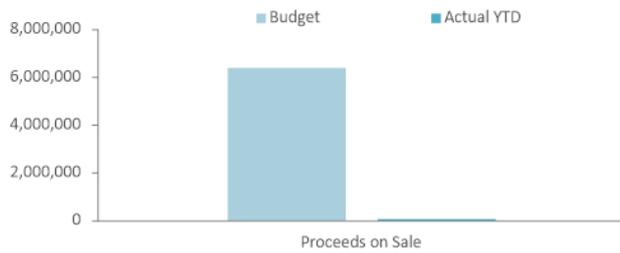
Financial assets at amortised cost held with registered financial institutions are listed in this note other financial assets at amortised cost are provided in Note 4 - Other assets.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

OPERATING ACTIVITIES
NOTE 3
DISPOSAL OF ASSETS

| Asset Ref. | Asset description | Budget | | | | YTD Actual | | | |
|------------|---------------------|----------------|------------------|------------------|------------------|----------------|---------------|---------------|----------|
| | | Net Book Value | Proceeds | Profit | (Loss) | Net Book Value | Proceeds | Profit | (Loss) |
| | | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ |
| | Plant and equipment | 953,844 | 1,093,380 | 183,894 | (119,388) | 0 | 73,988 | 73,988 | 0 |
| | Freehold Land | | 5,300,000 | 5,300,000 | (182,250) | 0 | 0 | 0 | 0 |
| | | 953,844 | 6,393,380 | 5,483,894 | (301,638) | 0 | 73,988 | 73,988 | 0 |



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

INVESTING ACTIVITIES
NOTE 4
CAPITAL ACQUISITIONS

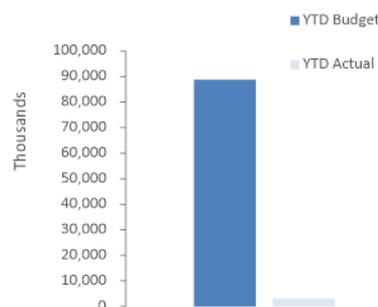
| Capital acquisitions | Amended | | YTD Actual | YTD Actual Variance |
|---|-------------------|------------------|------------------|------------------------|
| | Budget | YTD Budget | | |
| | \$ | \$ | \$ | \$ |
| Land | 2,800,000 | | 280,000 | 280,000 |
| Land - vested in and under the control of Council | | | 1,052,207 | 1,052,207 |
| Buildings | 31,157,846 | 1,068,131 | 517,552 | (550,579) |
| Furniture and equipment | 4,472 | | | 0 |
| Plant and equipment | 7,202,853 | 193,000 | 194,070 | 1,070 |
| Information technology | 1,048,091 | 157,085 | 14,960 | (142,125) |
| Infrastructure - roads | 19,877,974 | 175,652 | 421,716 | 246,064 |
| Infrastructure - drainage | 1,955,892 | 0 | 74,399 | 74,399 |
| Infrastructure - footpath | 2,587,217 | 8,333 | 1,486 | (6,847) |
| Infrastructure - parks hard | 7,716,826 | 507,868 | 114,862 | (393,006) |
| Infrastructure - parks landscaping | 3,144,493 | 39,117 | 50,631 | 11,514 |
| Infrastructure - landfill site | 5,092,043 | 6,667 | 468,810 | 462,143 |
| Infrastructure - marina | 5,806,029 | 513,518 | 1,350 | (512,168) |
| Infrastructure - coastal | 372,473 | 30,356 | 0 | (30,356) |
| Payments for Capital Acquisitions | 88,766,209 | 2,699,727 | 3,192,043 | 492,316 |
| Total Capital Acquisitions | 88,766,209 | 2,699,727 | 3,192,043 | 492,316 |

Capital Acquisitions Funded By:

| | \$ | \$ | \$ | \$ |
|---|-------------------|------------------|------------------|----------------|
| Capital grants and contributions | 19,272,146 | 416,731 | 1,628,423 | 1,211,692 |
| Borrowings | 5,277,400 | 0 | 0 | 0 |
| Other (disposals & C/Fwd) | 6,393,380 | 30,000 | 73,988 | 43,988 |
| Cash backed reserves | | | | |
| Plant & Vehicle Replacement | 5,706,373 | 39,000 | 0 | (39,000) |
| Major Building Refurbishment | 3,170,032 | 183,125 | 0 | (183,125) |
| Waste & Recycling | 5,346,843 | 66,667 | 0 | (66,667) |
| Land Development and Investment Fund | 3,843,994 | 0 | 0 | 0 |
| Roads & Drainage Infrastructure | 5,307,024 | 0 | 0 | 0 |
| Community Infrastructure | 13,103,129 | 490,388 | 0 | (490,388) |
| Greenhouse Action Fund | 155,300 | 0 | 0 | 0 |
| HWRP Post Closure Management & Contaminated Sites | 137,000 | 0 | 0 | 0 |
| Port Coogee Special Maintenance - SAR | 24,500 | 833 | 0 | (833) |
| Community Surveillance | 15,050 | 0 | 0 | 0 |
| Waste Collection | 125,000 | 0 | 0 | 0 |
| Bibra Lake Management Plan | 59,252 | 0 | 0 | 0 |
| Restricted Grants & Contributions | 520,000 | 0 | 0 | 0 |
| CIHCF Building Maintenance | 2,158,416 | 0 | 0 | 0 |
| Port Coogee Marina Assets Replacement | 12,281,679 | 532,126 | 0 | (532,126) |
| Port Coogee Waterways - WEMP | 60,000 | 0 | 0 | 0 |
| Contribution - operations | 5,809,691 | 940,857 | 1,489,632 | 548,775 |
| Capital funding total | 88,766,209 | 2,699,727 | 3,192,043 | 492,316 |

SIGNIFICANT ACCOUNTING POLICIES

All assets are initially recognised at cost. Cost is determined as the fair value of the assets given as consideration plus costs incidental to the acquisition. For assets acquired at no cost or for nominal consideration, cost is determined as fair value at the date of acquisition. The cost of non-current assets constructed by the local government includes the cost of all materials used in the construction, direct labour on the project and an appropriate proportion of variable and fixed overhead. Certain asset classes may be revalued on a regular basis such that the carrying values are not materially different from fair value. Assets carried at fair value are to be revalued with sufficient regularity to ensure the carrying amount does not differ materially from that determined using fair value at reporting date.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

FINANCING ACTIVITIES
NOTE 5
BORROWINGS

Repayments - borrowings

| Information on borrowings Particulars | Loan No. | 1 July 2020 | New Loans | | Principal Repayments | | Principal Outstanding | | Interest Repayments | |
|---|----------|-------------|-----------|-----------|----------------------|-----------|-----------------------|------------|---------------------|--------|
| | | | Actual | Budget | Actual | Budget | Actual | Budget | Actual | Budget |
| | | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ | |
| Governance | | | | | | | | | | |
| To assist fund the stage 2 of Marina infrastructure e | 9 | | | 5,277,400 | | | 0 | 5,277,400 | | |
| Community amenities | | | | | | | | | | |
| SMRC | | 3,848,499 | | | | 1,400,000 | 3,848,499 | 2,448,499 | 170,000 | |
| Recreation and culture | | | | | | | | | | |
| To assist fund the Cockburn Central West developm | 8 | 16,250,000 | | | | 2,500,000 | 16,250,000 | 13,750,000 | 526,000 | |
| C/Fwd Balance | | 20,098,499 | 0 | 5,277,400 | 0 | 3,900,000 | 20,098,499 | 21,475,899 | 696,000 | |
| Total | | 20,098,499 | 0 | 5,277,400 | 0 | 3,900,000 | 20,098,499 | 21,475,899 | 696,000 | |
| Current borrowings | | 3,900,000 | | | | | 865,017 | | | |
| Non-current borrowings | | 16,198,499 | | | | | 19,233,482 | | | |
| | | 20,098,499 | | | | | 20,098,499 | | | |

All debenture repayments were financed by general purpose revenue.

KEY INFORMATION

All loans and borrowings are initially recognised at the fair value of the consideration received less directly attributable transaction costs. After initial recognition, interest-bearing loans and borrowings are subsequently measured at amortised cost using the effective interest method. Fees paid on the establishment of loan facilities that are yield related are included as part of the carrying amount of the loans and borrowings.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

OPERATING ACTIVITIES
NOTE 6
CASH RESERVES

| Reserve name | Opening Balance | Budget Interest Earned | Actual Interest Earned | Budget Transfers In (+) | Actual Transfers In (+) | Budget Transfers Out (-) | Actual Transfers Out (-) | Budget Closing Balance | Actual YTD Closing Balance |
|--|--------------------|------------------------|------------------------|-------------------------|-------------------------|--------------------------|--------------------------|------------------------|----------------------------|
| | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ |
| Council Funded | | | | | | | | | |
| Staff Payments & Entitlements | 1,633,128 | | | | | (40,000) | | 1,593,128 | 1,633,128 |
| Plant & Vehicle Replacement | 11,400,754 | | | 3,000,000 | | (5,706,373) | | 8,694,381 | 11,400,754 |
| Information Technology | 501,249 | | | 1,200,000 | | | | 1,701,249 | 501,249 |
| Major Building Refurbishment | 16,677,163 | | | 3,000,000 | | (3,170,032) | | 16,507,131 | 16,677,163 |
| Waste & Recycling | 13,000,267 | | | 962,268 | | (5,346,843) | | 8,615,692 | 13,000,267 |
| Land Development and Investment Fund | 11,002,645 | | | 5,568,114 | | (3,848,994) | | 12,721,765 | 11,002,645 |
| Roads & Drainage Infrastructure | 10,428,351 | | | 4,500,000 | | (6,899,471) | | 8,028,880 | 10,428,351 |
| Naval Base Shacks | 1,161,639 | | | 18,287 | | | | 1,179,926 | 1,161,639 |
| Community Infrastructure | 27,940,292 | | | 3,500,000 | | (13,103,129) | | 18,337,163 | 27,940,292 |
| Insurance | 2,235,907 | | | 500,000 | | | | 2,735,907 | 2,235,907 |
| Greenhouse Action Fund | 741,641 | | | 200,000 | | (178,330) | | 763,311 | 741,641 |
| HWRP Post Closure Management & Contan | 3,501,513 | | | 250,000 | | (272,000) | | 3,479,513 | 3,501,513 |
| Municipal Elections | 1,420 | | | 150,000 | | | | 151,420 | 1,420 |
| Community Surveillance | 864,697 | | | 200,000 | | (120,050) | | 944,647 | 864,697 |
| Waste Collection | 4,199,528 | | | 2,339,328 | | (151,000) | | 6,387,856 | 4,199,528 |
| Environmental Offset | 308,011 | | | | | (59,252) | | 248,759 | 308,011 |
| Bibra Lake Management Plan | 521,086 | | | | | (520,000) | | 1,086 | 521,086 |
| CIHCF Building Maintenance | 9,327,472 | | | 1,458,228 | | | | 10,785,700 | 9,327,472 |
| Cockburn ARC Building Maintenance | 3,718,365 | | | 1,500,000 | | | | 5,218,365 | 3,718,365 |
| Carry Forward Projects | 4,796,511 | | | 9,878,427 | | (12,729,277) | | 1,945,661 | 4,796,511 |
| Port Coogee Marina Assets Replacement | 1,484,887 | | | 300,000 | | (60,000) | | 1,724,887 | 1,484,887 |
| Total Council Funded Reserve | 125,446,526 | 0 | 0 | 38,524,652 | 0 | (52,204,751) | 0 | 111,766,427 | 125,446,526 |
| Restricted Funded | | | | | | | | | |
| Aged and Disabled Asset Replacement | 391,623 | 4,257 | 326 | 37,716 | | | | 433,596 | 391,949 |
| Welfare Projects Employee Entitlements | 1,611,878 | 18,465 | 436 | 900,000 | | | | 2,530,343 | 1,612,314 |



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

OPERATING ACTIVITIES
NOTE 6
CASH RESERVES

| Reserve name | Opening Balance | Budget Interest Earned | Actual Interest Earned | Budget Transfers In (+) | Actual Transfers In (+) | Budget Transfers Out (-) | Actual Transfers Out (-) | Budget Closing Balance | Actual YTD Closing Balance |
|---|--------------------|------------------------|------------------------|-------------------------|-------------------------|--------------------------|--------------------------|------------------------|----------------------------|
| | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ | \$ |
| Port Coogee Special Maintenance - SAR | 1,820,480 | 22,245 | 1,515 | 400,000 | | (219,815) | | 2,022,910 | 1,821,995 |
| Port Coogee Waterways - SAR | 102,267 | 1,291 | 85 | 55,600 | | (50,000) | | 109,158 | 102,352 |
| Family Day Care Accumulation Fund | 11,474 | | 10 | | | | | 11,474 | 11,484 |
| Naval Base Shack Removal | 652,448 | 7,998 | 544 | 30,477 | | | | 690,923 | 652,992 |
| Restricted Grants & Contributions | 4,949,028 | | | | | (4,493,951) | | 455,077 | 4,949,028 |
| Port Coogee Waterways - WEMP | 1,302,071 | 15,831 | 1,088 | | | (50,000) | | 1,267,902 | 1,303,159 |
| Cockburn Coast SAR | 32,881 | 465 | 27 | 30,000 | | (11,330) | | 52,016 | 32,908 |
| Total Restricted Funded Reserve | 10,874,150 | 70,551 | 4,030 | 1,453,793 | 0 | (4,825,096) | 0 | 7,573,398 | 10,878,180 |
| Developer Contribution Plans | | | | | | | | | |
| Community Infrastructure DCP 13 | 4,650,121 | 17,282 | 3,875 | 3,000,000 | 853,851 | (3,065,564) | | 4,601,839 | 5,507,847 |
| Developer Contribution Plans - Various | 10,853,360 | 162,167 | 9,044 | 1,080,000 | 208,149 | (107,960) | | 11,987,567 | 11,070,553 |
| Total Developer Contribution Reserve | 15,503,481 | 179,449 | 12,920 | 4,080,000 | 1,062,000 | (3,173,524) | 0 | 16,589,406 | 16,578,400 |
| Total Cash Reserve | 151,824,157 | 250,000 | 16,950 | 44,058,445 | 1,062,000 | (60,203,371) | 0 | 135,929,231 | 152,903,107 |



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020

OPERATING ACTIVITIES
NOTE 7
OTHER CURRENT LIABILITIES

| Other current liabilities | Note | Opening Balance 1 July 2020 | Liability Increase | Liability Reduction | Closing Balance 31 July 2020 |
|---|------|-----------------------------------|-----------------------|------------------------|------------------------------------|
| | | \$ | \$ | \$ | \$ |
| Contract liabilities | | | | | |
| Unspent grants, contributions and reimbursements - non-operating | | 1,559,424 | 0 | 0 | 1,559,424 |
| Total unspent grants, contributions and reimbursements | | 1,559,424 | 0 | 0 | 1,559,424 |
| Provisions | | | | | |
| Annual leave | | 4,809,588 | 66,174 | | 4,875,762 |
| Long service leave | | 3,144,114 | | 0 | 3,144,114 |
| Total Provisions | | 7,953,702 | 66,174 | 0 | 8,019,876 |
| Total other current assets | | 9,513,126 | 66,174 | 0 | 9,579,300 |
| Amounts shown above include GST (where applicable) | | | | | |

KEY INFORMATION

Provisions

Provisions are recognised when the City has a present legal or constructive obligation, as a result of past events, for which it is probable that an outflow of economic benefits will result and that outflow can be reliably measured.

Provisions are measured using the best estimate of the amounts required to settle the obligation at the end of the reporting period.

Employee benefits

Short-term employee benefits

Provision is made for the City's obligations for short-term employee benefits. Short-term employee benefits are benefits (other than termination benefits) that are expected to be settled wholly before 12 months after the end of the annual reporting period in which the employees render the related service, including wages, salaries and sick leave. Short-term employee benefits are measured at the (undiscounted) amounts expected to be paid when the obligation is settled.

The City's obligations for short-term employee benefits such as wages, salaries and sick leave are recognised as a part of current trade and other payables in the calculation of net current assets.

Other long-term employee benefits

The City's obligations for employees' annual leave and long service leave entitlements are recognised as provisions in the statement of financial position.

Long-term employee benefits are measured at the present value of the expected future payments to be made to employees. Expected future payments incorporate anticipated future wage and salary levels, durations of service and employee departures and are discounted at rates determined by reference to market yields at the end of the reporting period on government bonds that have maturity dates that approximate the terms of the obligations. Any remeasurements for changes in assumptions of obligations for other long-term employee benefits are recognised in profit or loss in the periods in which the changes occur. The City's obligations for long-term employee benefits are presented as non-current provisions in its statement of financial position, except where the City does not have an unconditional right to defer settlement for at least 12 months after the end of the reporting period, in which case the obligations are presented as current provisions.

Contract liabilities

An entity's obligation to transfer goods or services to a customer for which the entity has received consideration (or the amount is due) from the customer. Grants to acquire or construct recognisable non-financial assets to identified specifications be constructed to be controlled by the City are recognised as a liability until such time as the City satisfies its obligations under the agreement.

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020**

**NOTE 8
TRUST FUND**

Funds held at balance date over which the Shire has no control and which are not included in this statement are as follows:

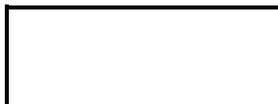
| Description | Opening Balance | Amount | Amount | Closing Balance |
|----------------------------------|--------------------|---------------|------------------|------------------|
| | 1 July 2020 | Received | Paid | 31 Jul 2020 |
| | \$ | \$ | \$ | \$ |
| POS Payments - Bibra Lake (East) | 133,721 | 339 | 0 | 134,060 |
| POS Payments - Aubin Grove | 816,634 | 1,625 | (174,880) | 643,379 |
| POS Payments - Atwell | 108,197 | 274 | 0 | 108,471 |
| POS Payments - Beeliar | 1,958,333 | 4,958 | 0 | 1,963,292 |
| POS Payments - Coogee | 328,680 | 127 | (278,571) | 50,236 |
| POS Payments - Cockburn Central | 164,995 | 361 | (22,300) | 143,056 |
| POS Payments - Hamilton Hill | 912,554 | 2,311 | 0 | 914,864 |
| POS Payments - Jandakot | 250,295 | 410 | (88,464) | 162,241 |
| POS Payments - Munster | 697,767 | 1,433 | (159,981) | 539,219 |
| POS Payments - South Lake | 5,400 | 14 | 0 | 5,414 |
| POS Payments - Yangebup | 547,116 | 1,385 | 0 | 548,501 |
| POS Payments - Hammond Park | 270,960 | 686 | 0 | 271,646 |
| POS Payments - Coolbellup | 179,948 | 456 | 0 | 180,404 |
| POS Payments - Lake Coogee | 0 | 84,000 | (56,000) | 28,000 |
| | 6,374,601 | 98,378 | (780,196) | 5,692,783 |

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020**

**NOTE 9
BUDGET AMENDMENTS**

Amendments to original budget since budget adoption. Surplus/(Deficit)

| Project/ Activity | Description | Council Resolution | Classification | Non Cash Adjustment | Increase in Available Cash | Decrease in Available Cash | Amended Budget Running Balance |
|----------------------|---|--------------------|--------------------|------------------------|-------------------------------|-------------------------------|--------------------------------------|
| | | | | \$ | \$ | \$ | \$ |
| | Budget adoption | | Opening surplus | | | | 38,911 |
| GL 400 | Youth Outreach - increased external funding | to 10 Sept OCM | Operating Revenue | | 7,023 | | 45,934 |
| GL 400 | Youth Outreach - increased service delivery 213 Frankland Ave - Parks construction funded by forfeited bond | to 10 Sept OCM | Operating Expenses | | | (7,023) | 38,911 |
| CW6124 | 213 Frankland Ave - Parks construction funded | to 10 Sept OCM | Capital Revenue | | 97,156 | | 136,067 |
| CW6124 | 213 Frankland Ave - Parks construction funded | to 10 Sept OCM | Capital Expenses | | | (97,156) | 38,911 |
| OP9945 | Local Healthy Food funded by external fund | to 10 Sept OCM | Operating Revenue | | 12,500 | | 51,411 |
| OP9945 | Local Healthy Food activity Community Engagement - Increased staff cost funded by | to 10 Sept OCM | Operating Expenses | | | (12,500) | 38,911 |
| OP8839 | Contingency Fund Community Development consultant funded by Contingency | to 10 Sept OCM | Operating Expenses | | 35,000 | | 73,911 |
| OP9525 | Fund | to 10 Sept OCM | Operating Expenses | | 50,000 | | 123,911 |
| OP8935 | Seniors program funded by Contingency Fund | to 10 Sept OCM | Operating Expenses | | 10,000 | | 133,911 |
| OP8272 | Contingency Fund - funding various projects | to 10 Sept OCM | Operating Expenses | | | (95,000) | 38,911 |
| | | | | 0 | 211,679 | (211,679) | |



**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 31 JULY 2020**

**NOTE 10
EXPLANATION OF MATERIAL VARIANCES**

The material variance thresholds are adopted annually by Council as an indicator of whether the actual expenditure or revenue varies from the year to date Actual materially.

The material variance adopted by Council for the 2020-21 year is \$300,000 or 0.00% whichever is the greater.

| Reporting Program | Var. \$ | Var. % | Timing/ Permanent | Explanation of Variance |
|---|-------------|-----------|-------------------|-----------------------------|
| | \$ | % | | |
| Revenue from operating activities | | | | |
| Infrastructure Services | 347,617 | 197.95% | ▲ Timing | Revenue brought forward |
| Expenditure from operating activities | | | | |
| Recreation and Community Safety | 418,783 | 23.28% | ▲ Timing | Expenditure delayed |
| Parks and Environmental Services | 545,619 | 30.63% | ▲ Timing | Expenditure delayed |
| Investing activities | | | | |
| Proceeds from non-operating grants, subsidies and contributions | 1,211,692 | 290.76% | ▲ Timing | Additional revenue |
| Payments for property, plant and equipment and infrastructure | (492,316) | (18.24%) | ▼ Timing | Expenditure brought forward |
| Financing activities | | | | |
| Transfer from reserves | (1,698,538) | (100.00%) | ▼ Timing | Expenditure delayed |
| Transfer to reserves | (571,994) | (112.83%) | ▼ Timing | Additional revenue |



15.3 (2020/MINUTE NO 0193) CHANGE OF METHOD OF VALUATION USED FOR RATING PURPOSES

| | |
|--------------------|---|
| Author(s) | E Yu |
| Attachments | <ol style="list-style-type: none"> 1. Changes to Method of Valuation - Properties 2. Changes to Method of Valuation Used for Rating Purposes - Rating Information (CONFIDENTIAL) 3. Aerial Images of Properties |

RECOMMENDATION

That Council apply to the Minister for Local Government, Sports and Cultural Industries (MLGSCI) for a determination pursuant to Section 6.28(1) of the *Local Government Act 1995*, to change the basis of rates for properties detailed in Attachment 1 from unimproved value (UV) to gross rental value (GRV) as the predominant use of the land of these properties have been determined to be non-rural.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

Under section 6.28 of the *Local Government Act 1995*, the City of Cockburn is required to review and make recommendations to the Minister for Local Government, Sports and Cultural Industries, as to the method of valuation to be used for rating purposes.

This report seeks Council endorsement to seek Ministerial approval for the basis of rating for various rural properties (refer Attachment 1) to be changed from unimproved value (UV) to gross rental value (GRV).

Submission

N/A

Report

The City of Cockburn has made a commitment to review the method of valuation applied to improved residential and commercial/industrial properties with unimproved valuations.

With growth and change in land uses within the City's rural areas, equitable rating has been an ongoing focus, particularly with properties that are rated using the property's unimproved value (UV), but are essentially residential or commercial/industrial properties in a rural area.

Rural for the purposes of rating is defined - A 'rural purpose' means a purpose pertaining to agriculture, and agriculture is defined to mean the



use or cultivation of land for any purpose of husbandry or horticulture, including the raising of livestock and the growing of crops. This is similar to the permitted uses under TPS3 Part 3 which includes for rural uses – agriculture extensive and intensive, agroforestry, animal husbandry intensive, farm supply centre, hobby farm, rural - industry or pursuit.

Operational guidelines developed by the Department of Local Government identify a number of guiding principles for local governments to consider when developing or assessing their rating structures, these include:

- Objectivity;
- Fairness and equity;
- Consistency;
- Transparency; and
- Administrative efficiency.

As part of this review, improved residential or commercial / industrial properties that are currently valued using the unimproved value method were assessed and are proposed to change to Gross Rental Value (GRV).

In determining which properties to apply these changes to the methods outlined in the Department of Local Government, Sport and Cultural Industries Process Guide – Valuation of Land (S6.28) was used.

A complete listing of affected properties is included in Attachment 1. A detailed list providing the financial impact for each property has been provided as a confidential attachment (Attachment 3) to Councillors.

The City reviewed 3 properties with a recommendation to the Minister that have their valuation methodology changed from UV to GRV.

To manage the increase for properties transitioning from UV to GRV for commercial / industrial properties, it is prudent to phase in increased over three years.

Phased rates will be changed at a rate of:

- 2020/2021 Financial Year – 2/3 UV and 1/3 GRV
- 2021/2022 Financial Year – 1/3 UV and 2/3 GRV
- 2022/2023 Financial Year – 100% GRV

Example: 2020/21 Financial Year Unimproved Value - \$900,000 divided 3 = \$300,000. Gross Rental Value - \$30,000 divided 3. Multiplied by 2 = \$20,000. Total Rates Payable = \$2,354.80 (\$789 UV + \$1,565.80 GRV - using 2018/19 rate in the dollars).



Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

Based on GRV valuation being phased-in for commercial/industrial properties, the change in basis of rating to gross rental value may increase revenue by approximately the following per year (based on 2020/2021 data):

| Year | Current Rates | Proposed Rates | Difference |
|---------|---------------|----------------|-------------|
| 2020/21 | \$4,828.80 | \$10,048.23 | \$5,219.43 |
| 2021/22 | \$4,828.80 | \$15,267.66 | \$10,438.86 |
| 2022/23 | \$4,828.80 | \$20,487.08 | \$15,658.28 |

It is worth reiterating that the purpose of this review is not about increasing rates revenue. Rather, it's about applying objectivity and consistency to the City's differential rating model to ensure a fair and equitable outcome for all ratepayers of the City. The review also aims to uphold the general principle for rating land based on its predominant use, as contained in the governing legislation.

There are not costs associated with the request to the Minister, however, costs will be incurred to advertise in the Government Gazette and may also be incurred when valuation are received from Landgate.

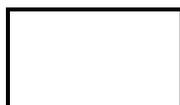
Legal Implications

Local government rating is regulated through Sections 6.28 to 6.82 of the *Local Government Act 1995*. All land within a local government district is rateable land with the exception specified in Section 6.26 of this Act.

Section 6.28 of the *Local Government Act 1995* requires the Minister to determine the method of valuation of land to be used by a local government as the basis for a rate and publish a notice of the determination in the Government Gazette.

In determining the method of valuation to be used by a local government, the Minister is to have regard to the principle that the basis for a rate on any land is to be either:

- Where the land is used predominantly for rural purposes, the unimproved value (UV) of the land; and
- Where the land is used predominantly for non-rural purposes, the gross rental value (GRV) of the land.



Each local government has a role in ensuring that the rating principles of the *Local Government Act 1995* are correctly applied to rateable land within their district.

Community Consultation

Land Use Declaration forms were sent to all affected landowners with a covering letter explaining the process along with a copy of *Section 6.28 of the Local Government Act 1995* and a three page Frequently Asked Questions sheet in February 2020.

Property owners were given 21 days to return the form.

Responses were recorded on a spreadsheet, detailing whether respondents considered their property usage to be Non-Rural, Rural, Predominantly Rural Usage and whether or not they derived their livelihood from the property.

Based on the response to the questions on the Land Use Declaration Forms and officer assessment of the property, a second letter was sent to all property owners informing them whether or not a recommendation was being made to change their valuation method to GRV. Property owners were given at least 21 days to object to this recommendation. Letters were sent on 11 March 2020, with submissions closing 3 April 2020.

Ratepayers were also provided with an indication of the overall likely financial impact of the changes.

Risk Management Implications

No risk implications have been identified as a result of this report or recommendation.

Advice to Proponent(s)/Submitters

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 10 September 2020 Ordinary Council Meeting.

Implications of Section 3.18(3) *Local Government Act 1995*

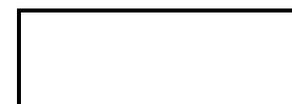
Nil



Attachment 1 - Changes to Method of Valuation

| Plan Type | Plan / Diagram# | Lot # | Street Address |
|------------------|------------------------|--------------|--|
| Deposited Plan | 415678 | 415 | 23 Snowden Street Hammond Park WA 6164 |
| Plan | 4829 | 14 | 55 Phillips Road Wattleup WA 6166 |
| Plan | 3455 | 17 | 109 Lorimer Road Munster WA 6166 |





| | |
|--|---|
| Parcel | |
| PIN | 12345231 |
| Piparcel | P415678 102 |
| Deposited Plans | DP415678 |
| Property | |
| Property No | 6032584 |
| Association | PROPERTY |
| Prop Type | Rural Commercial - Single |
| Address | |
| 23 Snowden Street HAMMOND PARK WA 6164 | |
| Lot | 102 |
| Plan No | DP 415678 |
| Vol Fol | 2961 / 423 |
| Area | 1805 m2 |
| Ward | East |
| TPS3 Zoning | DA 26 DCA 13 DCA 9 Development Zone |
| Structure Plan Zoning | Local Centre R80 |
| R-Code | R80 |
| Rcode-Source | Structure Plan |
| Local Dev Plan | Hammond Park-- Snowden St- Marquis St-Lots1-54 |
| Proposed Scheme Amendment | none |
| Constraints | Bush Fire Prone Area Fire Management Plan |
| Land | none |

| | | | |
|--|--|---------------|--|
| | PRINTED ON: 24/08/2020 | SCALE = 1:511 | |
| | <small>DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.</small> | | |



| | | | |
|---|---|---------------------------|----------------|
|  | Parcel PIN 287179 Piparcel P004829 14 Deposited Plans Property Property No 4411897 Association PROPERTY Prop Type Rural Industrial - Single Address 55 Phillips Road WATTLEUP WA 6166 Lot 14 Plan No P 4829 Vol Fol 1327 / 39 Area 1.8287 Ha Ward Central TPS3 Zoning Subject to HopeValleyWattleup p ReDev Act 2000 Structure Plan Zoning none R-Code none Rcode-Source none Local Dev Plan none Proposed Scheme none Amendment Constraints Bush Fire Prone Area Land Land Easement Encumbrances Additional Yard with dwelling Description Pool No | | |
| |  | PRINTED ON: 24/08/2020 | SCALE = 1:1494 |
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15.4 (2020/MINUTE NO 0194) CARRY FORWARD WORKS/PROJECTS AND BUDGET SURPLUS FOR 2019/20 CARRIED INTO 2020/21 BUDGET

Author(s) N Mauricio

Attachments 1. Carry Forward Works and Projects 2020/21

RECOMMENDATION

That Council:

- (1) amend the 2020/21 budget by including the Carried Forward Works and Projects as set out in the schedule attached to the Agenda and summarised in the following table:

| | |
|---|---------------------|
| Capital Expenses | \$37,540,346 |
| Operating Expenses | \$2,688,053 |
| Total Expenditure | \$40,228,399 |
| Funded By: | |
| Sale of Assets | \$5,557,280 |
| Less: Transfers to Reserves (from land sales) | (\$5,300,000) |
| Grants and Contributions to be received | \$10,470,413 |
| Grants and Contributions received prior to 1/7/2019 (Restricted Grants Reserve) | \$2,245,357 |
| Transfers from Reserves | \$14,526,072 |
| Municipal Funds (Carry Forwards Reserve) | \$2,850,850 |
| Municipal Funds | \$9,878,427 |
| Total Funding | \$40,228,399 |

- (2) amend the 2020/21 budget by adjusting the opening municipal surplus brought forward as follows:

| | |
|--|---------------|
| Increase opening Municipal surplus from \$2,000,000 to \$11,878,427 | (\$9,878,427) |
| Transfer to the Carried Forwards Projects Reserve (Municipal component of carried forward works and projects from 2019/20) | \$9,878,427 |

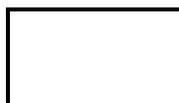
TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

MOVED Cr P Eva SECONDED Cr C Stone

That the recommendation be adopted.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 9/0



Background

When Council adopted its Budget for the 2020/21 financial year (FY) at the special meeting held on 23 July 2020, carry forward works and projects were not included as these were not accurately ascertainable at the time.

An estimated municipal closing surplus position of \$2.0M was adopted for the 2020/21 annual budget, representing an uncommitted surplus (i.e. not allowing for any carry forwards). Now that the carry forwards have been determined, these need to be adopted by Council to enable ongoing delivery of projects.

Submission

N/A

Report

In the 2020/21 adopted budget, the City forecast an opening municipal position of \$2,000,000, being an uncommitted surplus available for budget allocation.

The City's carry forward works and projects from 2019/20 have now been finalised, with a net funding requirement of \$9,878,427 required from Municipal, in addition to other funding sources for a total carry forward of \$40,228,399.

Also carried forward is the sale of land totalling \$5,300,000 which once realised, the net proceeds will be transferred into the Land Development and Investment Fund Reserve as per Council's Land Development Strategy.

There are 211 projects to be carried forward this year (compared to 237 last year). Of these, 175 are capital projects totalling \$39,132,793 with the other 36 being operational projects for \$1,095,606.

There were a total of 465 capital projects included in the 2019/20 budget, meaning that 291 projects (63%) were completed during the year. This represented 48% of the 2019/20 capital budget of \$78.41m.

Of the 175 capital projects, 40 had not commenced in 2019/20 (\$6.279m) compared to 56 projects last year (\$7.0m). The other 134 projects were underway (\$31.261m).



Notably, nine capital projects carried forward for a total of \$27.66m make up 71% of the carried forward value for capital projects. These include:

- Jandakot Road (Berrigan to Solomon stage 1) \$10.70m
- Wetlands Education Centre \$5.23m
- Purchase Lot 1001 Tindal Ave \$2.80m
- Frankland Park Recreation Centre and Ovals \$1.85m
- Capping Cell 6 HWRP \$1.80m
- Purchase Crown Land for Verde Drive/Prinsep Road \$1.59m
- Coogee Beach Master Plan \$1.51m
- Malabar Park BMX Facility \$1.13m
- Operations Centre Stage 2 \$1.04m

The following three projects make up 53% of the carried forward amount for operational projects:

- Roe 8 land rehabilitation \$0.34m
- Leisure management software \$0.13m
- COVID-19 community funding \$0.11m

Note that the purchase of Crown land for Verde Drive/Prinsep Road, whilst classified as operational from an accounting perspective, is really capital in nature.

Strategic Plans/Policy Implications

Listening and Leading

Deliver sustainable governance through transparent and robust policy and processes

Ensure sound long term financial management and deliver value for money.

Budget/Financial Implications

The 2020/21 Budget will be amended to reflect \$11,878,427 of opening funds brought forward from the 2019/20 financial year, an increase of \$9,878,427 compared to the adopted budget. This amount represents the municipal funding required to fund carried forward works and projects totalling \$40,228,399. The actual closing surplus for 2019/20 is still being finalised and will be brought to Council in future for any necessary adjustment.

Legal Implications

N/A



Community Consultation

N/A

Risk Management Implications

Council's budget for revenue, expenditure and the closing financial position will be misrepresented if the recommendation amending the budget is not adopted. The uncompleted carried forward works and projects from 2019/20 (and their associated funding sources) need to be formally adopted by Council to ensure compliance with legislative requirements.

Advice to Proponent(s)/Submissioners

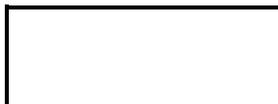
N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|--|--|--|---------------|----------|------------------|-----------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 122 - Executive Support | OP9658 - Staff Recognition Incentives | 19,743 | | | | (19,743) | Recurrent project. Approved to be cfwd by A/CEO due to COVID-19 |
| 221 - Information Communication and Technology | CW1274 - Service Desk Software replacement | 13,097 | | | (13,097) | 0 | Final payment to be made once in production, due early 20/21 FY |
| 221 - Information Communication and Technology | CW1480 - Replace DR Blade Servers | 100,000 | (100,000) | | | 0 | DR facility still to be built, no room to install in current location. Rolled to next FY when room will be completed. |
| 221 - Information Communication and Technology | CW4098 - Admin Server Room Fitout | 4,472 | | | (4,472) | 0 | Server room almost complete, require additional cabling and data works by external provider. Due by late July |
| 221 - Information Communication and Technology | CW4832 - Admin Building North Lake Training Room AV Fitout | 14,331 | | | | (14,331) | Room is being fit out with Accessible doors, frames etc that have held up purchasing and installation of hardware. Based on floor space left after renovations as to size and layout of equipment that can be ordered |
| 221 - Information Communication and Technology | CW4836 - Replace Admin Projectors | 30,000 | | | | (30,000) | Works ordered, due to be completed late July |
| 311 - Management Libraries | OP8828 - Local History Website | 45,320 | | | (20,320) | (25,000) | Due to the resignation of the officer responsible for the continued development of local history and the development of the Covid-19 restrictions, research and development was put on hold. |
| 311 - Management Libraries | OP8917 - Promotions and Marketing Plan | 2,768 | | | (2,768) | 0 | Allocated to signage for new branding at Coolbellup Library. |
| 311 - Management Libraries | OP8918 - Rebrand of Library Signage | 7,645 | | | (7,645) | 0 | Allocated to signage for new branding at Coolbellup Library. |
| 311 - Management Libraries | OP9995 - Library Grants Minor | 276 | | | (276) | 0 | To be used to support digital learning for community as training did not continue due to Covid-19 restrictions. |
| 312 - Spearwood Library | CW4825 - Installation of RFID after hours return chute Spearwood Lib | 10,871 | | | | (10,871) | Project near completion and funds will be expended early in 2020-21 financial year. |
| 314 - Success Library | CW4571 - Success Library Reglazing of upper floor | 3,596 | | | (3,596) | 0 | Further work on window decal removal and replacement to be completed early in the financial year. Delay due to Covid-19 disruptions. |
| 314 - Success Library | CW4826 - Installation of RFID after hours return chute Success Lib | 15,663 | | | | (15,663) | Project near completion and funds will be expended early in the 2020-21 financial year. |
| 323 - Recreation Services | OP7983 - Community Facility Internal Signage | 4,000 | | | | (4,000) | Project delayed due to Covid-19 |
| 323 - Recreation Services | CW4544 - Beale Park Redevelopment | 4,352 | | | (4,352) | 0 | budget of 2175K is located in CW4743. Staff member was unaware of this budget code so have raised all PO's against CW4544 for the Beale Park Redevelopment project, which has also been linked to WAM and PPM in TechOne. Request to finance to carry forward all unspent funds from CW4743 to 2020/21 and then consolidate into CW4544. Please see email from Aaron Thomas. |
| 323 - Recreation Services | CW4829 - Cockburn Coast Oval | 199,899 | | | | (199,899) | Delays in delivery due to staff focus on Covid-19 management and recovery |
| 323 - Recreation Services | CW4830 - Dixon Park Business Case | 70,000 | | | | (70,000) | |
| 323 - Recreation Services | CW5904 - Calleya(Treeby) Floodlights | 227,329 | | | (227,329) | 0 | Project delayed due to aboriginal and european heritage study. Delays in installation due to poles purchased prior to Covid from China. Have just arrived. Project expected to be completed in first 3 months of 2020/21 |
| 323 - Recreation Services | CW5981 - Botany Park Floodlighting | 999 | | (999) | | 0 | Final invoice outstanding for defects inspection |
| 544 - Plant Maintenance | CW7211 - Light Fleet Rates Manager PL211 | 34,000 | (21,000) | (13,000) | | 0 | Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7250 - PL250 Facilities Projects Officer - E Bjelland | 35,000 | (22,000) | (13,000) | | 0 | Carried forward due to low utilisation |
| 323 - Recreation Services | CW6059 - Bakers Square Lighting | 6,715 | | | | (6,715) | \$6,715 to be carried forward to 2020/21 to cover outstanding PO's relating to the project. |
| 323 - Recreation Services | OP9420 - Sports Hall of Fame Event | 4,708 | | | | (4,708) | Project delayed due to Covid-19 |
| 328 - Ranger and Community Safety | CW1387 - Electronic Infringment System and Software | 10,373 | | | (10,373) | 0 | Upgrade to infringement software pending and be considered as a larger project for 2020-21 |
| 544 - Plant Maintenance | CW7523 - Cage to suit new Ranger Vehicle (New) | 9,264 | (9,264) | | | 0 | Committed PO |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|---|--|--|---------------|----------|------------------|-----------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 328 - Ranger and Community Safety | CW1499 - CCTV AI Project | 123,311 | | | | (123,311) | Awaiting RFT to be finalised |
| 328 - Ranger and Community Safety | OP7978 - Elder abuse and defence awareness workshops | 3,000 | | | | (3,000) | Due to restrcuture program was put on hold |
| 328 - Ranger and Community Safety | OP7980 - CoSafe Community Meet & Greets | 3,931 | | | | (3,931) | Due to restrcuture program was put on hold |
| 328 - Ranger and Community Safety | OP8300 - Co-Safe eLearning and Training Course | 19,519 | | | | (19,519) | Further online training packages still to be developed for the new CoSafe service |
| 328 - Ranger and Community Safety | OP8923 - Feral Cat Control | 5,000 | | | | (5,000) | Awaiting outcome of Animal Management Plan |
| 329 - Cockburn ARC | OP5999 - Cockburn ARC - Power Generation System | 8,560 | | | | (8,560) | Project yet to be complete, due to COVID-19. Carry forward all remaining funds for consultancy for 21 FY |
| 329 - Cockburn ARC | OP6999 - Leisure Management Software Conversion | 135,000 | | | | (135,000) | Project has commenced, no financial commitments in 20 FY due to COVID-19 implications, carry forward all remaining funds to 21 FY |
| 329 - Cockburn ARC | OP7972 - External branding screen - Entrance Cockburn ARC | 40,000 | | | | (40,000) | Project has commenced, no financial commitments in 20 FY due to COVID-19 implications, carry forward all remaining funds to 21 FY |
| 329 - Cockburn ARC | CW4823 - Installation of Commercial Ceiling Fans ARC | 4,236 | | | | (4,236) | Commercial fans installation complete, final invoices due in period 1 and 2 of 21 FY, carry forward all remaining funds |
| 329 - Cockburn ARC | CW4824 - Internal cladding sports stadium ARC | 200,000 | | | | (200,000) | Project yet to commence to tender stage, due to implications associated with COVID-19. Carry forward all remaining funds for 21 FY |
| 329 - Cockburn ARC | OP8924 - Health club extension | 19,905 | | | (19,905) | 0 | Project not complete, carry forward all remaining funds to 21 FY |
| 330 - Community Dev and Services Unit Mgt | OP8892 - Youth Grant Funding Costs | 79,121 | | | | (79,121) | 0 Required for Outcomes measurement project |
| 331 - Family and Community Development | OP9729 - MY Time Group | 8,783 | | (8,783) | | 0 | 0 Grant funded money, needed to continue the program |
| 331 - Family and Community Development | OP8932 - Auslan video Comment on Cockburn | 2,156 | | | | (2,156) | 0 This project will continue in the next Fin Year |
| 331 - Family and Community Development | OP9182 - Reconciliation Action Plan review and update | 255 | | | | (255) | 0 The RAP will be reviewed in the coming Fin Year |
| 331 - Family and Community Development | OP9652 - Community Projects - Fremantle Ports Sponsorship | 1,305 | | | | (1,305) | 0 |
| 335 - Seniors Services | OP8247 - Fundraising - Senior Centre | 11,602 | | (5,618) | | (5,984) | 0 Centres fundraising revenue |
| 335 - Seniors Services | OP8936 - Produce Seniors Directory in a range of languages | 5,000 | | | | (5,000) | 0 |
| 337 - Grants and Research | OP9732 - COVID-19 community funding | 113,543 | | | | (113,543) | 0 Due to COVID-19 unable to complete project |
| 544 - Plant Maintenance | CW7546 - PL546 OSH Advisor - Sam Baron | 35,000 | (22,000) | (13,000) | | 0 | 0 Remainder of COVID-19 community funding pool to be used in a second round of funding in 2020-21 focussed on recovery |
| 544 - Plant Maintenance | CW7556 - PL556 CARC Manager - Brett McEwin | 34,000 | (21,000) | (13,000) | | 0 | 0 Funds are committed, awaiting delivery. |
| 431 - Building Services | CW4363 - Microfiche reader digitiser | 11,500 | | | | (11,500) | 0 Custodian may be entering private vehicle lease. Funds c/fwd to ensure potential purchase can be undertaken. |
| 544 - Plant Maintenance | CW7571 - CoSafe Vehicle 1 | 4,608 | (4,608) | | | 0 | 0 The budget has been slightly exceeded due to value of the Australian Dollar dropping against the US dollar, from when the budgeted was prepared. The machine is an american machine. |
| 544 - Plant Maintenance | CW7572 - CoSafe Vehicle 2 | 4,608 | (4,608) | | | 0 | 0 Committed PO |
| 544 - Plant Maintenance | CW7573 - CoSafe Vehicle 3 | 4,608 | (4,608) | | | 0 | 0 Committed PO |
| 544 - Plant Maintenance | CW7574 - CoSafe Vehicle 4 | 4,608 | (4,608) | | | 0 | 0 Committed PO |
| 544 - Plant Maintenance | CW7575 - CoSafe Vehicle 5 | 4,608 | (4,608) | | | 0 | 0 Committed PO |
| 544 - Plant Maintenance | CW7576 - CoSafe Vehicle 6 | 9,840 | (9,840) | | | 0 | 0 Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7708 - Parks Light Truck | 81,000 | (54,840) | (26,360) | | 0 | 0 Funds are committed, awaiting delivery. |
| 521 - Parks Construction and Maintenance | CW1642 - 100 Birchley Ave - Landscaping | 21,136 | | (21,136) | | 0 | 0 Developer contribution, works programmed for late 2020. |
| 521 - Parks Construction and Maintenance | CW5407 - Park Development - Coogee Beach Master Plan | 1,513,101 | | | (1,013,101) | (500,000) | 0 This project is on hold, pending decisions on leasing arrangements with the Coogee Cafe, Caravan Park owners and Main Roads approval for improved access to Coogee Beach car-parks. |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|--|---|--|---------------|-----------|------------------|-----------|---|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 521 - Parks Construction and Maintenance | CW5681 - City Street Tree Planting Requests | 84,607 | | | | (84,607) | Funds required to complete winter planting program. |
| 521 - Parks Construction and Maintenance | CW5731 - Park Signs (NEW) | 38,678 | | | | (38,678) | Surplus funds to be rolled over to 20/21 Parks Sign replacement program. |
| 521 - Parks Construction and Maintenance | CW5762 - Citywide Irrigation Central Control | 27,406 | | | | (27,406) | Works are underway, invoices due July 2020. |
| 521 - Parks Construction and Maintenance | CW5791 - Citywide Irrigation Cabinet Renewal | 19,316 | | | | (19,316) | Works are underway, invoices due July 2020. |
| 521 - Parks Construction and Maintenance | CW5808 - Plumosa Park Developments | 78,872 | | | (78,872) | 0 | Project on hold, waiting on Planning to provide approval for project initiation. |
| 521 - Parks Construction and Maintenance | CW5893 - Radiata Park 1/2 Basketball Court | 12,000 | | | (12,000) | 0 | Project on hold waiting on community consultation outcomes. |
| 521 - Parks Construction and Maintenance | CW5908 - Bibra Lake Master Plan | 54,922 | | | (54,922) | 0 | Works are underway, invoices due July 2020. |
| 521 - Parks Construction and Maintenance | CW5911 - Phoenix Revitalisation Strategy Street Tree Planting Program | 45,251 | | | (45,251) | 0 | Funds required to complete winter planting program and watering requirements for 20/21. |
| 521 - Parks Construction and Maintenance | CW5912 - Len Packham Reserve, Coolbellup Nature Play | 17,899 | | | (17,899) | 0 | Delays to the manufacture of a play element, completion due August/September 2020. |
| 521 - Parks Construction and Maintenance | CW5913 - Bassett Park, North Lake Park Upgrade | 1,208 | | | (1,208) | 0 | Committed PO |
| 521 - Parks Construction and Maintenance | CW5917 - Faiway Park, Jandakot Park Upgrade | 69,037 | | (69,037) | | 0 | Project 80% complete, funds required to pay invoiced due August 2020. |
| 521 - Parks Construction and Maintenance | CW5918 - Yarra Vista Park, Jandakot Park Upgrade | 71,019 | | (71,019) | | 0 | Funds required as works are due for completion September 2020. |
| 521 - Parks Construction and Maintenance | CW5922 - Mervyn Bond Park, Munster Park Upgrade | 92,527 | | (92,527) | | 0 | Funds required as works are due for completion September 2020. |
| 521 - Parks Construction and Maintenance | CW5929 - Lakeridge Park, Cockburn Central Park Upgrade | 129,812 | | (129,812) | | 0 | Project delayed pending investigation on stormwater mangement onsite, works due for completion early 2021. |
| 521 - Parks Construction and Maintenance | CW5956 - Port Coogee Water Play Refurbishment. | 50,000 | | | (50,000) | 0 | Waiting on an engineering feasibility report on modification to the existing system. |
| 521 - Parks Construction and Maintenance | CW5999 - Baler Court Offleash dog exercise area | 45,299 | | | (45,299) | 0 | Funding required to complete new PVC fence install due to safety concerns with the existing metal fence, works due for completion, August 2020. |
| 521 - Parks Construction and Maintenance | CW6011 - CY O'Connor Reserve, Deck replacement. | 136,467 | | | (136,467) | 0 | The South Beach Community Group have identified additional landscape works are required, due for completion August 2020. |
| 521 - Parks Construction and Maintenance | CW6020 - Hamilton Hill Urban forest improvements | 19,200 | | | (19,200) | 0 | Project delayed, waiting on engineering assessment of the cars-park layout, works due for completion late 2020. |
| 521 - Parks Construction and Maintenance | CW6026 - Lucretia Park, playground renewal | 150,000 | | | (150,000) | 0 | Delays in the community consultation process, feedback from Port Coogee Community Association has resulted in a number of concept design revisions, works due for completion late 2020. |
| 521 - Parks Construction and Maintenance | CW6027 - Market Garden Park, clay pump-track renewal. | 6,623 | | | (6,623) | 0 | Works 90% complete, Invoice due July 2020. |
| 521 - Parks Construction and Maintenance | CW6028 - Market Garden Park, playground renewal #5 | 35,000 | | | (35,000) | 0 | Project delayed, works due for completion late 2020. |
| 521 - Parks Construction and Maintenance | CW6030 - Master Plan for BP Pipeline Easement | 100,000 | | | (100,000) | 0 | Project on hold, waiting for land transfer between BP Oil and the City. |
| 521 - Parks Construction and Maintenance | CW6034 - Poole Reserve Irrigation renewal | 81,963 | | | (81,963) | 0 | Works 80% complete, invoice due August 2020. |
| 521 - Parks Construction and Maintenance | CW6039 - Aubin Grove Skate Facility | 49,308 | | (49,308) | | 0 | Community consultation process is in progress, waiting on feedback to determine support for the project. |
| 521 - Parks Construction and Maintenance | CW6044 - Sports Lighting renewal program. | 160 | | | (160) | 0 | Committed PO |
| 521 - Parks Construction and Maintenance | CW6048 - Upgrades to Shelters | 10,000 | | | (10,000) | 0 | Delays in materials works due for completion late 2020. |
| 521 - Parks Construction and Maintenance | CW6052 - Windmill Park, park upgrade | 45,552 | | | (45,552) | 0 | Community consultation process is in progress, waiting on feedback to determine support for the project. |
| 521 - Parks Construction and Maintenance | CW6054 - Duggan Park, Hammond Park – Shelter, tables/chairs | 5,416 | | | (5,416) | 0 | Works are underway, invoices due July 2020. |
| 521 - Parks Construction and Maintenance | CW6060 - Albion Park Basketball | 19,730 | | | (19,730) | 0 | Works are underway, invoices due July 2020. |
| 522 - Environmental Management | CW5825 - Banksia Eucalypt Woodland Signage | 1,819 | | | (1,819) | 0 | Committed PO |
| 522 - Environmental Management | CW5907 - 20 King St Coogee (Ocean Crest Development) | 3,812 | | | (3,812) | 0 | Committed PO |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|---|---|--|---------------|-----------|------------------|-----------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 522 - Environmental Management | CW5971 - Port Coogee Dive Trail Stage 3 | 52,784 | | | | (52,784) | Works still underway: Shower installation still progressing as is drinking fountain and signage update. |
| 522 - Environmental Management | CW5986 - C.Y. O'Connor Dune Stabilisation | 27,680 | | | | (27,680) | Awaiting to determine the impacts of the winter storms |
| 522 - Environmental Management | CW5990 - Manning Park Mountain Bike Trails Design | 95,089 | | | | (95,089) | Awaiting the results of the community consultation on concept plan. Consultation extended until 19th July. |
| 522 - Environmental Management | CW5993 - Sustainable Sump Development Doherty Reserve | 13,117 | | | | (13,117) | Community consultation being undertaken prior to works proceeding |
| 522 - Environmental Management | CW5994 - Yangebup Lake Bridge Upgrade | 36,062 | | | | (36,062) | Structural design finalised. Construction to commence funds permitting. |
| 531 - Road Construction and Maintenance | CW2363 - Street Lighting System - Various Enhancements | 21,237 | | | | (21,237) | Existing PO's raised awaiting invoices increased lighting requests |
| 531 - Road Construction and Maintenance | CW2364 - Minor Roadworks | 12,200 | | | | (12,200) | Committed orders |
| 531 - Road Construction and Maintenance | CW2381 - Minor Drainage Improvements | 4,415 | | | | (4,415) | Existing PO's |
| 531 - Road Construction and Maintenance | CW2970 - Childrens Crossings | 4,086 | | | | (4,086) | Committed Orders |
| 531 - Road Construction and Maintenance | CW3461 - Bus Stop Facilities | 49,938 | | | (49,938) | 0 | Committed orders |
| 531 - Road Construction and Maintenance | CW3836 - Lombe Gardens Dunnage to Waters Ave | 236 | | | | (236) | Unpaid invoices |
| 531 - Road Construction and Maintenance | CW3845 - AW Marvell Ave - Stevenson Way | 6,005 | | | | (6,005) | Unpaid invoices |
| 531 - Road Construction and Maintenance | CW3858 - Barrington Street Wellard to Stock- design and development | 19,176 | | | | (19,176) | Consultancy and design costs in progress |
| 531 - Road Construction and Maintenance | CW3859 - BP Oil line Bike path Carrington to Blackwood | 298,000 | | (148,000) | | (149,000) | Construction Contract pending |
| 531 - Road Construction and Maintenance | CW3860 - Coleville Cr - Bike boulevard | 40,000 | | (20,000) | | (20,000) | Application expired new application required |
| 544 - Plant Maintenance | CW7716 - Truck Under ST - Roads Services | 90,000 | (80,000) | (30,000) | | 0 | Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7728 - Parks Light Truck | 81,000 | (54,840) | (28,360) | | 0 | Funds are committed, awaiting delivery. |
| 531 - Road Construction and Maintenance | CW3944 - Hammond Road Frankland to Rowley | 1,665 | | | (1,665) | 0 | Committed Orders |
| 531 - Road Construction and Maintenance | CW3946 - Jandakot Road Louisiana Glen Proposed Right Turn | 385,703 | | | (385,703) | 0 | Western Power works Scheduled July 2020 |
| 531 - Road Construction and Maintenance | CW3957 - Rowley Road Lyon Road roundabout | 183,955 | | | (183,955) | 0 | Works in Progress, agreement with MRWA and Kwinana in progress |
| 531 - Road Construction and Maintenance | CW3962 - Tolley Court Sump | 323,849 | | | | (323,849) | Contract awarded works to to commence June 2020 |
| 531 - Road Construction and Maintenance | CW3963 - Hartley Sump | 5,559 | | (5,559) | | 0 | Committed PO |
| 531 - Road Construction and Maintenance | CW3982 - King Store Storage | 47,720 | | | (47,720) | 0 | Works in Progress |
| 544 - Plant Maintenance | CW7732 - Parks Light Truck | 81,000 | (54,840) | (28,360) | | 0 | Funds are committed, awaiting delivery. |
| 531 - Road Construction and Maintenance | CW3993 - Coolbellup Ave Traffic Calming | 83,285 | | (83,285) | | 0 | Works in Progress |
| 531 - Road Construction and Maintenance | CW4778 - Murdoch Chase Lighting | 65,690 | | | | (65,690) | Works in progress expected completion July 2020 |
| 531 - Road Construction and Maintenance | CW4800 - Rockingham Road Revitalisation | 152,172 | | | | (152,172) | existing contract awarded |
| 531 - Road Construction and Maintenance | CW4802 - Verde Drive extension | 118,989 | | | | (118,989) | Contract in Progress Project split Journal transfer approved. [Reduced cfwd amount as per NM email 13/8. SN] |
| 531 - Road Construction and Maintenance | CW4803 - Bartram Road parking and traffic management | 95,000 | | | | (95,000) | |
| 531 - Road Construction and Maintenance | CW4804 - Coolbellup wayfinding signage | 20,000 | | | | (20,000) | |
| 531 - Road Construction and Maintenance | CW4806 - Interim Road slow point improvement | 25,000 | | | | (25,000) | Comitted Orders |
| 531 - Road Construction and Maintenance | CW4807 - LED speed display signs | 24,900 | | | | (24,900) | Comitted Orders |
| 531 - Road Construction and Maintenance | CW4808 - Phoenix Revitalisation path refurbishment program | 18,797 | | | | (18,797) | |
| 531 - Road Construction and Maintenance | CW4809 - Phoenix Revitalisation traffic assessment | 25,320 | | | | (25,320) | |
| 531 - Road Construction and Maintenance | CW4810 - South Lake Primary School parking and traffic | 68,537 | | | | (68,537) | |
| 531 - Road Construction and Maintenance | CW4811 - Bartram and Brenchley | 72,738 | | | | (72,738) | Works in Progress awaiting invoices |
| 531 - Road Construction and Maintenance | CW4813 - Jandakot Road Drainage | 300,000 | | | | (300,000) | Work still to be done |
| 531 - Road Construction and Maintenance | CW4814 - Spearwood Avenue Hamilton to Cockburn | 146,800 | | | | (146,800) | Works in Progress awaiting invoices |
| 531 - Road Construction and Maintenance | CW4815 - Armadale to North Lake drainage connection under Freeway | 197,919 | | | | (197,919) | |
| 531 - Road Construction and Maintenance | CW4816 - Berrigan Dr WB (Kwinana Fwy to South lake Dr) | 28,100 | | (28,100) | | 0 | Works in Progress awaiting invoices |
| 531 - Road Construction and Maintenance | CW4818 - North Lake Road Berrigan to Osprey | 15,700 | | (15,700) | | 0 | MRWA Invoice |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|--------------------------------------|---|--|---------------|-----------|------------------|-----------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 542 - Project & Asset Services | CW1474 - Asset Data Collection Initiatives | 4,500 | | | (4,500) | 0 | Committed PO |
| 542 - Project & Asset Services | CW1475 - Works & Assets Systems Development | 19,395 | | | | (19,395) | Future Budget needed for minor configuration and consultancy |
| 542 - Project & Asset Services | OP8962 - Technolgy One: Strategic Asset Management | 7,800 | | | (7,800) | 0 | Existing PO cancelled to reflect revised internal Implementation Strategy. Only 57800 Carry Forward is required allowing for 4 days for review and final checks. New PO (106709) raised for 2 days consultancy for SAM Review of current config and setup, budget for 2 further days thereafter. |
| 543 - Facilities Mtce and Management | CW4010 - Civic & Community Buildings - Height Safety System Treatment | 14,949 | | | (14,949) | 0 | Committed PO |
| 543 - Facilities Mtce and Management | CW4086 - Coogee Beach SLSC Building - Repairs | 34,783 | | | (34,783) | 0 | Late start on works due to pindan covering the repair to the tiling |
| 543 - Facilities Mtce and Management | CW4109 - Civic & Community Buildings - Signage Replacement/Upgrade | 37,123 | | | | (37,123) | works put on hold due to covid |
| 544 - Plant Maintenance | CW7734 - Ride on Sweeper Dulevo 120DK | 190,909 | (190,909) | | | 0 | Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7743 - Waste Articulated Loader | 120,000 | (85,000) | (35,000) | | 0 | Carried forward as still serviceable. |
| 543 - Facilities Mtce and Management | CW4379 - Disability Access Facility Improvements | 35,787 | | | (5,787) | (30,000) | Audit completed and works ongoing |
| 544 - Plant Maintenance | CW7748 - Truck Rubbish Waste 29m3 | 637,000 | (637,000) | | | 0 | Funds are committed, awaiting delivery. |
| 543 - Facilities Mtce and Management | CW4639 - Civic and Community Buildings - Asbestos Removal | 4,231 | | | | (4,231) | There is a number of large asbestos removals required that were indentified out of the LGIS audit. Funds to be added to next FY budget |
| 543 - Facilities Mtce and Management | CW4647 - Civic and Community Buildings - Exterior and Interior Paints | 44,116 | | | | (44,116) | Works held up due to covid. Painting schedule being developed |
| 543 - Facilities Mtce and Management | CW4648 - Civic and Community Buildings - Car Park Linemarking | 1,400 | | | (1,400) | 0 | Committed PO |
| 543 - Facilities Mtce and Management | CW4676 - Frankland Park Recreation Centre & Ovals - Design | 1,853,906 | | (922,926) | (74,597) | (856,383) | Currently at DA, Detailed design commenced, construction tender programmed for September 2020 |
| 544 - Plant Maintenance | CW7774 - Roads Small Loader | 150,000 | (97,500) | (52,500) | | 0 | Delayed due to uncertainty around Howson Way development |
| 543 - Facilities Mtce and Management | CW4680 - Wetland Education Centre - Design | 68,518 | | (68,518) | | 0 | To be Carried Forward - Please journal to CW4156 |
| 543 - Facilities Mtce and Management | CW4683 - Administration Building - IT Server Room Refurbishment | 43,003 | | | (43,003) | 0 | last of the works underway now |
| 543 - Facilities Mtce and Management | CW4686 - Civic and Community Buildings - Furniture Replacement | 15,564 | | | | (15,564) | Other Buildings being assessed for furniture replacement |
| 543 - Facilities Mtce and Management | CW4698 - Jandakot V8FB Constructions | 10,472 | | (10,472) | | 0 | Closing out Project & projects Staff hours (potential Legal Fees) |
| 543 - Facilities Mtce and Management | CW4705 - Success Recreational Facility | 9,500 | | | (6,500) | 0 | Works delayed due to CV19, Business Case to be developed, Minor items identified |
| 543 - Facilities Mtce and Management | CW4631 - Depot DR Facility Construction | 172,532 | (172,532) | | | 0 | To be Carried Forward - Project currently at tender commencing late 2020 estimated completion late 2021 |
| 543 - Facilities Mtce and Management | CW4715 - Calleya Estate 'Treeby' Community Centre | 677,018 | | | (246,018) | (428,000) | Project elements experienced delays at commencement and in COVID |
| 543 - Facilities Mtce and Management | CW4716 - Aboriginal Cultural Centre | 23,233 | | | | (23,233) | Currently out to market for Tender for External PM Services, |
| 543 - Facilities Mtce and Management | CW4717 - Omeo Park Toilet Block, Port Coogee | 69,570 | | | (69,570) | 0 | Delay in community consultation due to COVID-19 resulted in delay on project progress. Project to resume and complete late 2020 / early 2021. |
| 512 - Waste Disposal Services | CW1498 - Capping Cell 6 HWRP (Preliminary design costs) | 1,800,246 | (1,800,246) | | | 0 | Project delayed due to DWER approval delay |
| 543 - Facilities Mtce and Management | CW4719 - Howson Way Site | 11,913 | | | | (11,913) | The project is dependent of spoil (road surface waste) from road rehabilitation and therefore availability of adequate amount of road waste material is anticipated in September 2020. |
| 544 - Plant Maintenance | CW7570 - CoSafe Supervisor Utility | 0 | | | | 0 | Funds are committed, awaiting delivery. |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|---|---|--|---------------|-------------|------------------|-----------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 543 - Facilities Mtce and Management | CW4720 - Shoreline Pedestrian Bridge, North Coogee - Design | 120,000 | | (120,000) | | 0 | Expenditure forms part of City's contribution to detailed design carried out by third party (Development WA) who is yet to completed the detailed design and claim the contribution. |
| 543 - Facilities Mtce and Management | CW4722 - Success Regional Sports Facility - Refurbishments | 52,348 | | | (52,348) | 0 | Works delayed due to CV19, Business Case to be developed, Minor items identified |
| 543 - Facilities Mtce and Management | CW4743 - Beale Park Sporting Facilities - Design | 175,950 | | | | (175,950) | Carry forward budget to be moved to CW4544 - As per discussion / agreement with Finance |
| 512 - Waste Disposal Services | CW1920 - Waste Transfer Station Stage 1 | 38,801 | (38,801) | | | 0 | Resurfacing works were held off due to COVID 19 |
| 543 - Facilities Mtce and Management | CW4746 - Administration Building - Staff workstation area alterations | 18,005 | | | | (18,005) | Committed PO |
| 543 - Facilities Mtce and Management | CW4749 - Banjup Community Hall - Storeroom Conversion | 1,050 | | | | (1,050) | Minor Items picked up during construction |
| 543 - Facilities Mtce and Management | CW4750 - Bibra Lake Community Centre - Storeroom Addition | 51,487 | | | | (51,487) | Builder finishing up final items |
| 543 - Facilities Mtce and Management | CW4751 - Cockburn Tennis Club - Minor Refurbishments & Repairs | 19,100 | | | | (19,100) | late decision to demolish rather than renovate. Awaiting scope and quotes |
| 543 - Facilities Mtce and Management | CW4752 - Coleville Crescent - Secure Pool Vehicle Parking Structure | 28,020 | | | | (28,020) | purchase orders issued for the builder. Permits about to be issued |
| 543 - Facilities Mtce and Management | CW4754 - Coogee Community Centre - Minor Refurbishments & Repairs | 48,299 | | | | (48,299) | engaging community support for rendering and painting the building |
| 543 - Facilities Mtce and Management | CW4757 - East Beeliam Community Centre | 21,211 | | | | (21,211) | Fund originally planned for shade sail. Funds to be used for misc work around the building. |
| 543 - Facilities Mtce and Management | CW4759 - Jean Willis Centre - Bus Parking | 13,293 | | | | (13,293) | Additional paving to ensure bus parking required |
| 543 - Facilities Mtce and Management | CW4760 - Len Packham Club Rooms - Minor Refurbishments & Repairs | 29,145 | | | | (29,145) | moneys to be rolled over and fencing project run along side bin compound in new FY |
| 543 - Facilities Mtce and Management | CW4764 - Operations Centre - Animal Care Facility Access | 45,103 | | | | (45,103) | Project to be carry forward |
| 512 - Waste Disposal Services | CW1923 - Leachate Pump Replacement | 16,155 | (16,155) | | | 0 | Pump 4 is expected to be installed by late June 2020 however C/F if work is not complete |
| 543 - Facilities Mtce and Management | CW4767 - Seniors Centre - Spearwood - Outside Area improvements | 16,401 | | | | (16,401) | Scope to be revisited |
| 543 - Facilities Mtce and Management | CW4769 - Wetlands Education Centre - Stormwater Drainage Treatment | 30,000 | | | | (30,000) | To be Carried Forwards - Please journal to CW4156 |
| 543 - Facilities Mtce and Management | CW4844 - Cockburn Integrated Health Solar Panel | 50,500 | | (50,500) | | 0 | Project awaiting feasibility report from Solargain |
| 512 - Waste Disposal Services | CW1929 - Materials Recovery Facility - Design and Documentation | 38,834 | (38,834) | | | 0 | Talis and the City have reached agreement on the next design phase of the Henderson Redevelopment |
| 512 - Waste Disposal Services | CW1935 - Remediation & landfill leachate management | 41,171 | (41,171) | | | 0 | Management and consultant assistance continue to be required at HWRP until leachate is under control. |
| 512 - Waste Disposal Services | CW1992 - Design of Changes to HWRP | 16,836 | (16,836) | | | 0 | Talis and the City have reached agreement on the next design phase of the Henderson Redevelopment |
| 544 - Plant Maintenance | CW7251 - Light Fleet Commercial PL251 FESA | 35,000 | | (8,700) | | (26,300) | Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7800 - High Lift Loader Bucket | 35,000 | (35,000) | | | 0 | Funds are committed, awaiting delivery. |
| 544 - Plant Maintenance | CW7801 - Green Waste Decontamination Plant | 109,800 | (109,800) | | | 0 | Funds are committed, awaiting delivery. |
| 423 - Leasing and Land Administration | CW1639 - Land Purchase - Sale Lot 1001 Tindal Ave | 2,800,000 | (2,800,000) | | | 0 | Land purchase date extended. |
| 543 - Facilities Mtce and Management | CW4149 - Operations Centre Stage 2 | 1,043,994 | (1,043,994) | | | 0 | To be Carried Forward - Project currently at tender commencing late 2020 estimated completion late 2021 |
| 546 - Coastal Engineering Services | OP7854 - Coogee Beach Foreshore Management Plan | 5,000 | (5,000) | | | 0 | FMP final draft complete but minor extra costs likely to be incurred for consultant to complete additional revision and edit of document |
| 423 - Leasing and Land Administration | OP7851 - Purchase Verde Drive and Prinsep Rd - Crown Land | 1,592,447 | (1,592,447) | | | 0 | Land acquisition agreements ongoing |
| 531 - Road Construction and Maintenance | CW3917 - Jandakot Road (Berrigan to Solomon stage 1) | 10,696,065 | (3,800,333) | (4,820,000) | (2,075,732) | 0 | Awaiting Taking orders and tender process |



| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | | |
|--|---|--|---------------------|---------------------|--------------------|--------------------|--|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | | Justification |
| | | | Reserves | External | Restricted Funds | Municipal | |
| 531 - Road Construction and Maintenance | CW3942 - Karel Ave Berrigan to Farrington | 6,691 | (6,691) | | | 0 | Project in Progress additional funding Jandakot Airport |
| 323 - Recreation Services | CW6056 - Meller Park Floodlighting | 282,831 | (282,831) | | | 0 | Carried forward to 2020/21 to align with program. |
| 323 - Recreation Services | CW6057 - Goodchild Park Floodlighting | 556,610 | (556,610) | | | 0 | |
| 543 - Facilities Mtce and Management | CW4156 - Wetlands Education Ctr | 5,228,325 | (228,325) | (2,930,000) | | (2,070,000) | Budget increased to \$645k at MYR to support Goodchild Park upgrades. All unspent funds to be carried forward to 2020/21 to align with program for floodlighting installation. |
| 543 - Facilities Mtce and Management | CW4677 - Lakelands Reserve Hockey Facility & Clubrooms - CSRRF Funded | 92,548 | (92,548) | | | 0 | Construction underway, PC expected May 2021 |
| 543 - Facilities Mtce and Management | CW4712 - Malabar Park BMX Facility - Design | 144,463 | (133,313) | | | (11,150) | To be Carried Forward - Project about to finish DLP period |
| 543 - Facilities Mtce and Management | CW4718 - Hamilton Hill Community Centre | 33,500 | (33,500) | | | 0 | Design Complete, tender for construction estimated for August 2020 |
| 543 - Facilities Mtce and Management | CW4765 - Operations Centre - PV and Battery System Design | 35,300 | (35,300) | | | 0 | Awaiting confirmation from Governance & Community as to whether this project is to proceed |
| 543 - Facilities Mtce and Management | OP9731 - Feasibility Study - Henderson Energy Park' | 23,030 | (23,030) | | | 0 | Awaiting trial of EV Waste Truck, due for delivery October / November. |
| 328 - Ranger and Community Safety | CW1489 - CCTV Project - Bibra Lake Skate Park | 15,050 | (15,050) | | | 0 | Agreement between City and State Govt just signed in June. |
| 333 - Aged and Disabled Services | OP8324 - Private Fee Income | 4,942 | | (4,942) | | 0 | Awaiting wireless suvery results to determine the pole height required for additiona equipment. |
| 512 - Waste Disposal Services | OP7852 - Beach Bin Litter Collection | 26,000 | (26,000) | | | 0 | As required. Unknown at this stage. [dealt with grant surplus/deficit] |
| 543 - Facilities Mtce and Management | CW4744 - Malabar Park BMX Facility | 1,128,102 | (56,252) | | | (1,068,850) | Only carry forward if the waste education budget is reduced so that the 20-21 beach litter program is not possible |
| 545 - Port Coogee Marina | CW4726 - Marina Stage 2 - Planning and Preliminary Works | 83,729 | | | | (83,728) | See above - CW4712 |
| 545 - Port Coogee Marina | CW4819 - Jetty Section Replacement | 60,000 | (60,000) | | | 0 | Delayed award of tender for design and specification of Stage 3 expansion. This is now due to be awarded in July 2020. Financial Analysis study of MSB still due to commence, funding dependent. |
| 531 - Road Construction and Maintenance | CW3991 - Farrington Rd and Bibra Dr Intersection Modification | 242,856 | | (242,856) | | 0 | Works delayed and due to commence with Marina Expansion works in 2021. |
| 546 - Coastal Engineering Services | CW4732 - C Y O'Connor Protection Modelling & Design | 88,273 | | | (79,929) | (8,344) | Works in Progress Western Power works commence 23 July 2020 |
| 546 - Coastal Engineering Services | CW4820 - Beach Access Ramp - Coogee Beach Jetty | 144,200 | | | | (144,200) | Design study still underway, expect completion around mid FY |
| 522 - Environmental Management | CW6125 - Manning Park Area Assistance Grant | 68,181 | | (68,181) | | 0 | Works to go to tender following extra budget allocation in 2020/21 budget |
| 522 - Environmental Management | OP7849 - Lot 131 Jandakot Road, Treeby Revegation | 50,187 | | (50,187) | | 0 | Unspent grant & contribution |
| 521 - Parks Construction and Maintenance | OP7831 - Minigwal Loop | 19,670 | | (19,670) | | 0 | Unspent grant & contribution |
| 521 - Parks Construction and Maintenance | OP7855 - Cockburn Coast Oval | 4,545 | | (4,545) | | 0 | Unspent grant & contribution |
| 441 - Environmental Health | OP8159 - Bulk Hydromulching of Private Land | 5,434 | | (5,434) | | 0 | Unspent grant & contribution |
| 522 - Environmental Management | OP8906 - Roe 8 - Rehabilitation | 340,428 | | (340,428) | | 0 | Unspent grant & contribution |
| 333 - Aged and Disabled Services | OP8969 - HACC program Services | 20,015 | | (20,015) | | 0 | Unspent grant & contribution |
| 511 - Waste Collection Services | OP9730 - Commercial Food Waste Trial | 17,420 | | (17,420) | | 0 | Unspent grant & contribution |
| 521 - Parks Construction and Maintenance | OP7706 - Cockburn Central West Streetscapes | 29,995 | | (29,995) | | 0 | Unspent grant & contribution |
| | | 40,228,399 | (14,526,072) | (10,727,693) | (5,096,207) | (9,878,427) | |
| Land transactions - transfer to reserve | | | | | | | |
| 423 - Leasing and Land Administration | CW1639 - Land Purchase - Sale Lot 1001 Tindal Ave | 3,100,000 | | (3,100,000) | | | |
| 423 - Leasing and Land Administration | CW1640 - Sale of Lot 1003 Bundegi Grove | 2,200,000 | | (2,200,000) | | | |

| | | 2020/21 Budget Carried Forward Works & Projects from 2019/20 | | | | |
|--------------|---------------------|--|---------------|-------------|------------------|---------------|
| Service Unit | Project Description | Expenditure Amount | Income Source | | | Justification |
| | | | Reserves | External | Restricted Funds | |
| | | 5,300,000 | | (5,300,000) | | |



16. ENGINEERING AND WORKS DIVISION ISSUES

16.1 (2020/MINUTE NO 0195) AUBIN GROVE SKATE PARK

Author(s) A Lees

Attachments

1. Aubin Grove Skate Park Engagement Summary
2. Petition - Aubin Grove Skate Park

RECOMMENDATION

That Council:

- (1) note the results of the community engagement;
- (2) inform the community of the engagement results;
- (3) note the preferred location of a skate park at Radiata Park;
- (4) undertake a visioning workshop with selected community members to explore potential skate park designs along with other types of youth facilities that meet the needs of the community; and
- (5) report the results to a future Council Meeting.

COUNCIL DECISION

MOVED Deputy Mayor L Kirkwood SECONDED Cr P Corke

That Council:

- (1) note the results of the community engagement;
- (2) inform the community of the engagement results;
- (3) undertake a visioning workshop:
 - a. to be run by an independent facilitator,
 - b. with selected Aubin Grove community members, including but not limited to residents surrounding Radiata Park, local youths and representative(s) from the Aubin Grove Community Association,
 - c. to explore designs for potential youth facilities in Radiata Park that meet the desires of the community; and
- (4) report the results to a future council meeting.

CARRIED 8/1

Reason for Decision

Council resolved to set aside \$50,000 for the community consultation and design on this project. The results of the community engagement regarding the skate park show lack of support by the residents in close proximity to Radiata Park. In fact it has created a further divide in the community between the two locations suggested. The Aubin Grove Community Association, along with the residents, asked for a workshop, however due to COVID this was not able to occur. While the community survey results showed 120 people wanted a skate park in Radiata Park, 106 people also said they would prefer something else, this is without including 82 unique signatures on the petition against a skate park



anywhere in Aubin Grove.

The visioning workshop will allow the local residents to explore designs for youth facilities and provide feedback in more depth to the council in order to make a more informed decision for the area.

Background

The Community, Sport and Recreation Facilities Plan 2018-2033 identified the provision of a further four (4) district level skate parks to accommodate the City's projected population forecast of 144,310 by 2026. The four sites are Yangebup, Aubin Grove, Hamilton Hill and Hammond Park, with further detailed investigation required to ascertain the most advantageous location.

In parallel to this plan being prepared, the City is progressing the development, consultation and endorsement of the Public Open Space Cash-In-Lieu Expenditure Plan 2018-2020. The plan listed Radiata Park as the most advantageous location to support a Skate Park, due to a number of factors including; neighbourhood classification, size, existing infrastructure, perimeter car parking, and it is located centrally within the Aubin Grove suburb.

With these two key documents forming the basis of a skate park proposal in Aubin Grove, a Consultation Engagement Plan was prepared and executed. Consultation commenced on 13 May 2020 with a three week response window. This report provides an overview of the consultation outcomes and identifies a series of options in order to progress the delivery of a skate park for the community.

Submission

N/A

Report

Consultation Analysis

Consultation on the proposed skate park was conducted between 13 May and 3 June 2020. Due to the Covid-19 pandemic, feedback was invited through an online survey, in writing (email and written), or over the phone. A copy of the results of the public consultation is included as Attachment 1.

The consultation was promoted through Comment on Cockburn which has 1,000 subscribers in Aubin Grove and immediately surrounding suburbs, newsletters, media release, Aubin Grove Primary School, and 700 letters to residents within approximately 100m-200m of Radiata Park and Aubin Grove Oval.



The community were asked whether they supported a skate park in Aubin Grove, with the following options:

- No, I would prefer something else
- Yes, at Radiata Park (Location A*)
- Yes, at Aubin Grove Oval (Location B*)
- Yes, at Aubin Grove Oval (Location C*).

An overarching map, Image 1 below, along with individual maps of each location was provided to enable respondents to comprehend its position in relation to other park and community facility infrastructure, along with a series of opportunities and challenges for each location.



Image 1: Overarching Map

A review of potential sites was undertaken prior to consultation to reaffirm Radiata Park was the most viable location for a skate park based on previous analysis. Aubin Grove Oval was identified as a potential location with no other open space being suitable. Although the oval had a series of challenges it was included in the consultation as a possible alternative for the community to consider.

The consultation findings revealed the project page was visited 1,290 times, which clearly demonstrates a high level of local interest. There were 321 responses received through the online survey and submissions.

The engagement process identified that more people support a skate park in Aubin Grove than not having one at all, along with respondents listing Radiata Park as the preferred location compared to any other.

The survey revealed 184 out of the 290 respondents want a skate park in Aubin Grove, with 120 respondents supporting Radiata Park as the preferred location. The remaining respondents that did not support a skate park identified Radiata Park as the most popular location to invest future improvements should the funding be reallocated. Image 2 below provides a graphical representation of the survey results.



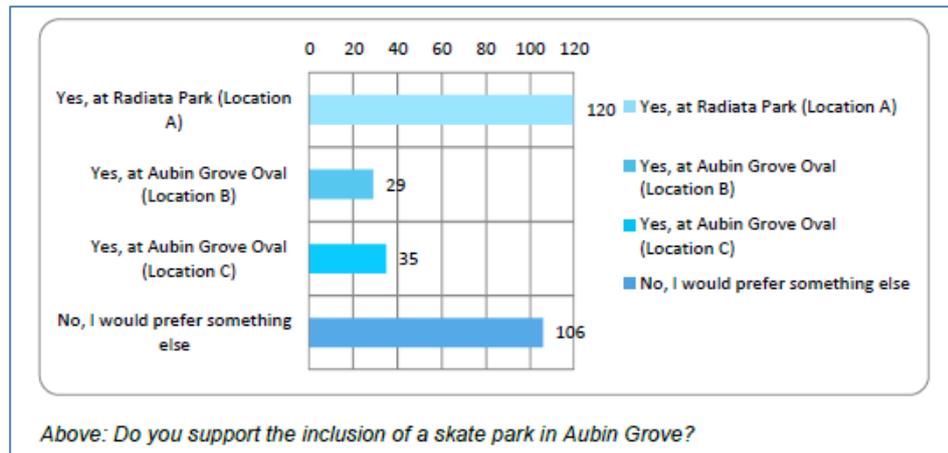


Image 2: Extract from community engagement survey

In addition to the survey, a petition was received on 5 June 2020 (after the engagement closing date) requesting Council to abandon plans to build a skate park at any location in Aubin Grove. The petition was conducted by a local resident and contained 105 names with 102 signatures (three people did not provide their signature). One petition signee is from outside of Aubin Grove.

In reviewing the list of petitioner's, names it was established at least 20 had participated in the City's engagement process. Without a full comprehension of the petitioners details it would be reasonable to interpret a larger number of the petitioners participated in the survey in the negative. A copy of the petition is included as Attachment 2.

Community, Sport and Recreation Facilities Plan 2018-2033

The Community, Sport and Recreation Facilities Plan provides an analysis on the provision of skate parks to support activities for young people ranging in ages from 12-24. The demand for skate parks is increasing with the community indicating it to be one of the top 10 important community facilities. The City currently has three regional level and one district level skate parks. The plan proposes an additional four new district level skate parks to support population growth out to 2026 at the following locations:

1. Aubin Grove – Radiata Park
2. Yangebup – Nicholson Reserve
3. Hammond Park – Frankland Reserve
4. Hamilton Hill – Dixon Reserve.

The skate parks have been scheduled for delivery across the 15 year implementation plan, subject to annual funding considerations, with targeted consultation, to ensure community acceptance and assistance with the designs.



Public Open Space Cash-In-Lieu Expenditure Plan 2018 -2020

The Public Open Space Cash-In-Lieu Expenditure Plan 2018-2020 provides a coordinated approach to the expenditure of funds received through the cash-in-lieu contributions, in accordance with the *Planning and Development Act 2005*. The expenditure of funds can only be allocated within the Public Open Space Reserves on specific infrastructure that ensures the functionality and useability of the park is increased for the betterment of the community.

The plan provides a program of expenditure across 14 suburbs which align with the development opportunities outlined in the Public Open Space Strategy 2014-2024, Shade Sail Strategy 2013-2023, Community Sports and Recreation and Facilities Plan 2018-2033, Public Health Plan and other corporate strategies.

Consultation on the plan was conducted via Comment on Cockburn with responses assessed and included subject to funding and meeting compliance criteria. Due to the impact across 14 suburbs, it was acknowledged further targeted consultation would be undertaken on the larger capital investment works prior to delivery. This would ensure specific feedback was received and the community were cognisant of the works.

The proposed works for the Aubin Grove suburb are outlined in Table 1 below with all being completed, except Radiata Park.

| Location | Reserve No | Description | Amount |
|------------------|------------|--|------------------|
| Radiata Park | 48534 | 1. Skate Park 2. Seating 3. Toilet Facility | \$610,000 |
| Princeton Park | 49085 | 1. Playground Shade Sail 2. Playground Equipment for Young Children 3. Park Sign 4. BBQ 5. Shelter with Seating and Connecting Paths | \$105,000 |
| Bologna Park | 49770 | Playground Shade Sail | \$25,000 |
| Colorado Park | 50600 | 1. Playground Shade Sail 2. BBQ | \$45,000 |
| Observatory Park | 49020 | Playground Shade Sail | \$25,000 |
| Tangle Park | 48473 | Exercise Equipment | \$35,000 |
| Total | | | \$845,000 |

Table 1: Extract from CIL Expenditure plan of Aubin Grove Suburb



The available funds within the Aubin Grove POS Reserve account when the plan was created totalled \$845,929.64, with proposed expenditure of \$845,000. An allocation of \$610,000 was set aside for Radiata Park to support a skate park, toilet facility and appropriate seating.

The expenditure of funds received through the cash-in-lieu provision is governed by the *Planning and Development Act 2005* with any amendments to the current plan requiring review by the Western Australian Planning Commission (WAPC), with subsequent approval by the Minister for Lands. Initial advice from WAPC on potentially not progressing the Radiata Park skate park and redirecting the funds, subject to compliance criteria, to other open spaces in Aubin Grove suburb is that the proposal would be supported.

If the Radiata Park works cease and the funds are not redirected, the remaining balance of funds, less consultation expenses, will be returned to the Aubin Grove Reserve account until a future program of park improvements in the Aubin Grove suburb is prepared with community support.

Current Operation Impacts of Limited Youth Facilities in Aubin Grove

The provision of recreational equipment to support the youth within Aubin Grove is extremely limited which has resulted in vandalism of Banksia Eucalypt Woodland Park. Over a number of years the City has experienced damage to the bushland with a heightened increase over the last six months caused by the construction of BMX tracks.

Concentrated efforts across a number of service units are being implemented to cease these unapproved activities; however it is acknowledged without appropriate infrastructure for the youth of Aubin Grove to participate, they are likely to transition to other bushland areas or public open space to achieve their recreational pursuits.

Recommendation

The survey results reaffirm the skate park delivery plan outlined in the Community, Sport and Recreation Facilities Plan to meet the growing needs of the community. A skate park at Radiata Park will provide the youth in Aubin Grove an active space for recreation and go a long way to reducing damage to the local bushland environments.

Through a series of visioning workshops a concept design can be developed which responds to the underlying features of the park and be incorporated to minimise the potential impact to surrounding property owners. The workshop can also consider the integration of other youth facilities to maximise the range of activities for the 12 -24 age range.

The workshop can also explore socially responsible mechanisms to mitigate anti-social behaviour along with developing a set of self-governing rules. CCTV could also be a key component of the build to further reduce the potential for inappropriate behaviour.



The proposed workshop will be hosted by an independent facilitator and made up of a representative from the Aubin Grove Community Association, students from the Aubin Grove Primary School, residents surrounding Radiata Park and a landscape architect. An invitation to register and appointment schedule will be developed to ensure a broad ranging demographic profile is represented.

Strategic Plans/Policy Implications

Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Provide accessible high-quality open spaces and parks for community benefit.

Community, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

- Provide community, sport, recreational, and cultural facilities and infrastructure to meet our community needs.

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Listen to, communicate, consult and engage with our residents, businesses and community in a timely, open and collaborative manner.

Budget/Financial Implications

In December 2017, Council approved the Public Open Space Cash-In-Lieu Expenditure Plan which included a Skate Park, Toilet Facility and Seating at Radiata Park of \$610,000.

The 2019/2020 Parks Service Units capital works had an allocation of \$50,000 to undertake community consultation and develop a concept design for the skate park. All unspent funds were to be carried forward and incorporated with the remaining balance in the 2020/2021 budget.

The 2020/21 Parks Service Units capital works budget included a line item of \$560,000, remaining balance, for the Aubin Grove Skate Facility.

Legal Implications

Nil

Community Consultation

Refer to the analysis above and Attachments 1 and 2.



Risk Management Implications

The risks associated with not moving forward with the project will have a compounding effect on project delivery, implementation of strategic documents and failing to listen to the community. Controversial projects of this nature are on the increase due to diverse communities and the resistance of adjacent property owners to the location of these projects with Council needing to accept the risks to ensure infrastructure is available for the broader community.

Advice to Proponent(s)/Submitters

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered by Council at the 10 September 2020 Ordinary Council Meeting.

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



Aubin Grove Skate Facility

May – June 2020

Community Engagement Summary



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Background

In 2016, the City of Cockburn commenced development of its Community, Sport and Recreation Facilities Plan 2018 – 2033 (CSRFP). This involved research, gap analysis and consultation with key stakeholders and community.

The CSRFP notes Cockburn's significant population growth and how this is expected to continue over the next 10-20 years. Continued growth presents a number of challenges to ensure community needs are met. The CSRFP provides a strategic framework for amenity provision over the next 15 years.

Analysis of the five year age groups of Aubin Grove in 2016 compared to City of Cockburn shows that there was a higher proportion of people in the younger age groups (under 15) and a lower proportion of people in the older age groups (65+).

Overall, 29.9% of the population was aged between 0 and 15, and 5.0% were aged 65 years and over, compared with 20.0% and 11.6% respectively for City of Cockburn.

It is recognised that whilst children are well catered for in public open spaces across Aubin Grove (many open spaces include a playground), there is little to accommodate the youth demographic. Some have taken to creating their own bike tracks and jumps in nearby bushland.

Following completion of the draft CSRFP, a public advertising period took place. The CSRFP was then finalised and presented to Council for endorsement in 2018. A copy can be accessed on the City's website: <https://www.cockburn.wa.gov.au/Recreation-and-Attractions/Sport-Fitness-and-Aquatic/Sport-and-Recreation-Plans-and-Strategies>.

The CSRFP identifies that by 2026, an additional regional level skate park and another four district level skate parks would be required in the City.

The CSRFP suggests that these new/upgraded skateparks be evenly distributed across the City and in the following suburbs:

- Yangebup
- Aubin Grove
- Hamilton Hill
- Hammond Park

Skate Parks are spaces which largely cater for young people aged 12-24 and include ramps, bowls, basins and rails which provide opportunities for skaters, scooters and BMX riders to utilise.

The design and function of skate parks have significantly changed over the past 10 years, this has seen skate parks placed in more prominent/visual locations and incorporated the opportunity for other wheeled sports to utilise the space i.e. scooter and BMX.

In consideration of the site challenges and constraints posed by the construction of a skate park, Radiata Park was identified as the most appropriate location for the facility.

Radiata is considered appropriate as it is a neighbourhood public open space, has passive surveillance, sufficient parking, a site layout favourable to construction conditions, and the

skate park would be around 50m from properties (when positioned towards the centre of the park).

Funding for the Radiata Park skate park was identified through the City's Public Open Space Cash-In-Lieu Expenditure Plan (June 2018 – December 2020). The Plan identified \$610,000 to be allocated for the provision of a skate park, seating and toilet facility at Radiata Park. Approximately \$500,000 was to be allocated to the skate park component.

The Cash-In-Lieu Expenditure Plan was endorsed by Council at the 10 August 2017 Ordinary Council Meeting. An excerpt from the Council Report is provided below.

17.2 (MINUTE NO 6144) (OCM 10/08/2017) - EXPENDITURE OF PUBLIC OPEN SPACE CASH-IN-LIEU FUNDS (188/001) (A LEES) (ATTACH)

RECOMMENDATION

That Council

- (1) endorse the proposed expenditure of Public Open Space Cash-In-Lieu Expenditure Plan as listed in the attachment to the agenda;
- (2) refer the proposals to the Western Australian Planning Commission for consideration and recommendation to the Minister of Transport; Planning; Lands;
- (3) inform the community of the proposed expenditure of Public Open Space Cash-In-Lieu Funds; and
- (4) upon receipt of advice from the Minister of Transport; Planning; Lands on the proposed expenditure of public open space cash-in-lieu funds, receive a final report on the approved expenditure and delivery timeframes.

COUNCIL DECISION

MOVED Cllr C Terblanche SECONDED Cllr K Allen that the recommendation be adopted.

CARRIED 9/0

Above: Snapshot from Ordinary Council Meeting 10 August 2017 minutes - Council resolution to endorse Cash-In-Lieu Expenditure Plan.

The Plan was subsequently submitted to the Western Australian Planning Commission (WAPC) for consideration. It received Ministerial approval in 14 November 2017.

The City of Cockburn's commitment to proceeding with a skate park at Radiata Park in Aubin Grove was formalised in 2018, through Council's decision to adopt the Community Sport and Recreation Facilities Plan and the Cash In Lieu program. The Radiata Park skate facility has since been listed on the City of Cockburn's website as an upcoming project.

As part of the progress of the development the City undertook further community consultation. The objective was to understand the level of support from interested or impacted community and stakeholders on a skate park at the viable locations, or what the preferred alternative uses are.

The City reviewed potential site options and identified a possible alternative for the facility for the community to consider (two spots within Aubin Grove Oval). Aside from Aubin Grove Oval and Radiata Park, there are no other open spaces in Aubin Grove that are suitable for a skate facility.

It was envisaged that once sentiment had been clarified, further engagement could take place to co-design the space.

This document summarises the process used to invite feedback and the feedback that was received.

Summary

This project coincided with the Covid-19 pandemic, which required the process to be undertaken remotely.

Feedback was invited in the following ways:

- Online survey (Comment on Cockburn)
- In writing (via email submission)
- In writing (via written submission)
- Over the phone

The City also proposed co-design opportunity with the local school, which involved developing a tailored engagement activity to seek the feedback of young people in the area. Unfortunately the school was unable to participate at the time.

The feedback period was open between 13 May and 3 June 2020 and was promoted in the following ways:

- Comment on Cockburn newsletter to over 1000 subscribers in Aubin Grove and immediately surrounding suburbs
- Letter to 700 residents within approximately 100m-200m hundred metres of Radiata Park and Aubin Grove Oval
- Direct phone call, SMS or email to registered site hirers (sporting clubs and community groups)
- Newsletter to Aubin Grove Community Association and Banjup Residents Group

- Local newspaper advertisement in Cockburn Gazette
- Media release (ran 28 May, Cockburn Gazette)
- Shares to social media affiliate pages (e.g. Cockburn Chat)
- Newsletter provided to Aubin Grove Primary School for sharing
- Direct contact with Aubin Grove Primary School

The community were asked whether they support a skate park in Aubin Grove with answer options as follows:

- No, I would prefer something else
- Yes, at Radiata Park (Location A*)
- Yes, at Aubin Grove Oval (Location B*)
- Yes, at Aubin Grove Oval (Location C*)

A map was provided showing the locations and opportunities and challenges of each site.

Those that answered “No, I would prefer something else” were provided a list of public open spaces in Aubin Grove to select from and a list of the potential alternative expenditures (which would require an approval from the WAPC), in keeping with the requirements under the *Planning and Development Act 2005*.

An opportunity to provide further comments and reasoning (open ended responses) was also included.

At the end of the survey, respondents were asked to provide their street name and suburb. Suburb data is also collected from Comment on Cockburn registered accounts. The suburb data used in this report has been obtained from registered accounts, therefore in the unlikely event a respondent has provided a false suburb in the survey, that data has not been used.

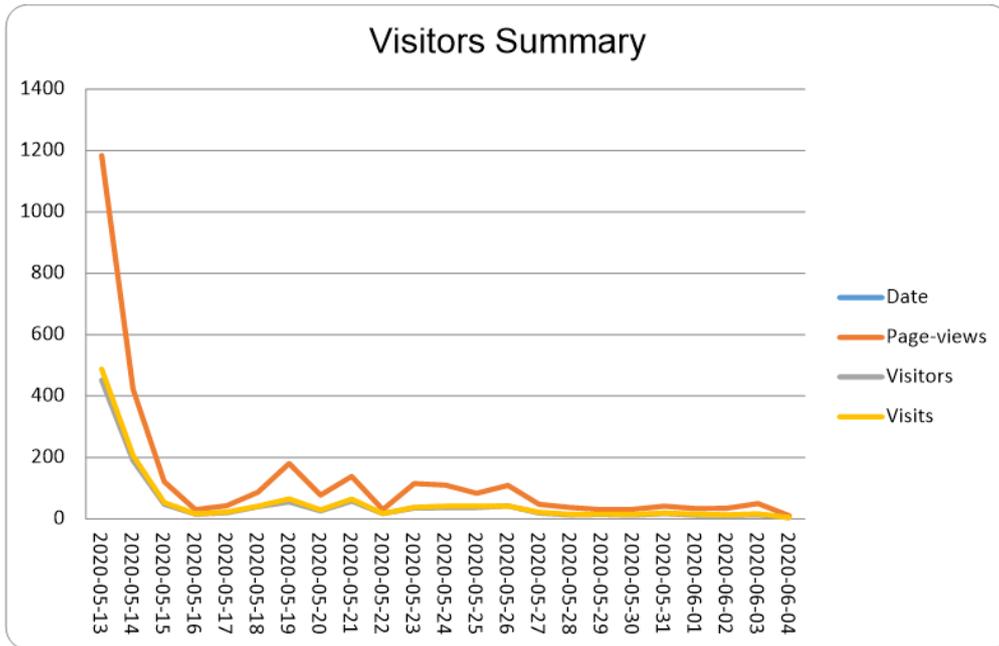
The project history and funding constraints are complex and therefore a detailed Frequently Asked Questions was provided on the Comment on Cockburn page, along with a Question and Answer Board. This tool enables registered participants to post a question and receive a response (no questions were asked using this tool).

Key Findings

Overall, the project page was visited 1290 times. This indicates a high level of local interest in the proposal and the value of the website as an informative tool.

321 responses were received during the comment period through the survey and submissions.





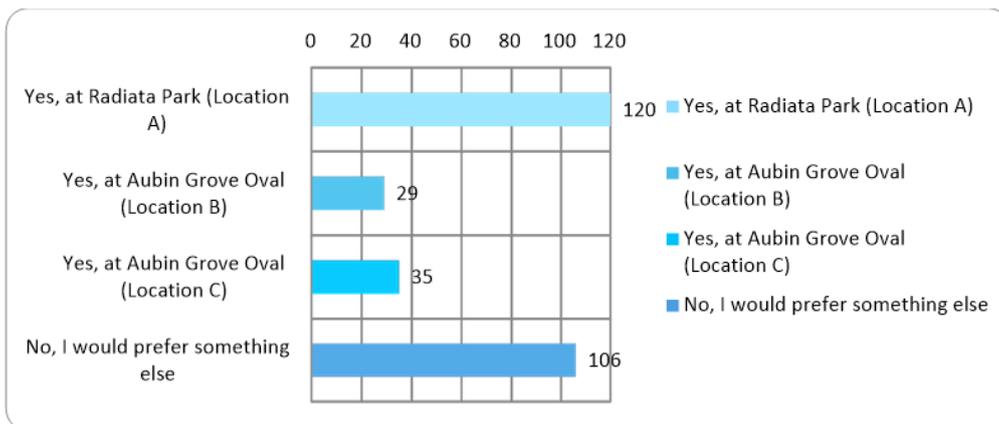
Above: Graph showing the number and date of visitations to the Comment on Cockburn project page.

The feedback gathered through the City’s formal engagement process identified that a skate park at Radiata Park was the most popular response amongst those that participated.

More respondents want a skate park at Radiata Park compared to any other location and compared to no skate park at all.

Overall, more people support a skate park in Aubin Grove than not.

Of those that support a skate park, the most preferred site is Radiata Park.



Above: Do you support the inclusion of a skate park in Aubin Grove?

1. Survey (290 responses)

Most respondents support the inclusion of a skate park in Aubin Grove. The most popular location amongst this group of respondents is Radiata Park.

When looking at Aubin Grove respondents only, a relatively similar sentiment is reflected in that most Aubin Grove residents that responded to the survey support a skate park and the most popular location is Radiata Park.

However, the preference for a skate park at Radiata Park is almost equal to the preference for 'something else', with 'something else' being slightly more preferred.

| |
|---|
| <p>Survey (290 respondents)</p> <ul style="list-style-type: none"> • Support a skate park in Aubin Grove: 63% <i>(Made up of the following)</i> <ul style="list-style-type: none"> ○ Radiata Park - Location A: 41% ○ Aubin Grove Oval - Location B: 10% ○ Aubin Grove Oval - Location C: 12% • Prefer something else: 37% |
| <p>Survey (220 Aubin Grove respondents only):</p> <ul style="list-style-type: none"> • Support a skate park in Aubin Grove: 60% <i>(Made up of the following)</i> <ul style="list-style-type: none"> ○ Radiata Park - Location A: 38% ○ Aubin Grove Oval - Location B: 10% ○ Aubin Grove Oval - Location C: 12% • Prefer something else: 40% |

2. Submissions (31 submissions either by email or phone)

A submission is a written response received by the City.

Consensus on the support of a skate park is almost evenly split amongst those that made a submission (48% in support compared to 52% not in support; or 10 compared to 11). Amongst those that do support the skate park, Radiata Park is the most preferred location.

Please note ten people who made a written submission completed the online survey as well. To ensure equality of every participant, the sentiment of these 10 respondents will be counted once. Therefore, so as to not replicate their response in the data twice (it is already captured in the survey data), they are not included in the submission statistics.

Not all respondents who made a submission provided their street name (approximately 50%). The number of people who did not provide their street name was prevalent in both groups (in support vs. not in support) almost evenly. Despite this, most of the respondents that did not supply their address provided information that insinuated that they reside in Aubin Grove, or are impacted by the proposal (e.g use the site).

| |
|---|
| <p>Submissions (21 respondents)</p> <p><i>*Data originally contained 31 responses, however the feedback of 10 respondents is already captured in the above survey data. For equality, these will not be counted twice and therefore have been omitted from the following statistics.</i></p> |
|---|



- Support a skate park in Aubin Grove: 48%
(Made up of the following)
 - Radiata Park - Location A: 23.5%
 - Aubin Grove (B or C) – 9.5%
 - Aubin Grove Oval - Location B: 0%
 - Aubin Grove Oval - Location C: 4.5%
 - Any location - 9.5%
- Do not support a skate park: 52%

Of those that do not support the skate park, their reasons included:

- Loss of property value and amenity for surrounding residents
- Anti-social behavior / crime
- Will attract undesirable people
- Graffiti / vandalism
- Burden of passive surveillance will fall unfairly on adjacent residents
- Disruption to current uses and user groups
- Loss of green and recreational space
- Increased traffic issues
- Proximity to homes
- Not needed
- Funds could be better utilised

Some respondents who do not support a skate park identified that whilst they like the idea and recognize there is a need to provide things for the youth age group, they would not like the facility in their local park.

Of those that do not support the skate park, the most preferred location for the funding to be invested (with alternative use) was Radiata Park (36%), followed by Aubin Grove (14%). The remaining votes were spread relatively evenly across 13 other parks in the suburb.

There was no overwhelming consensus amongst this group of respondents on how the funding should be reallocated. Respondents who did not support a skate park could choose which alternative uses they would support and the top five preferences were:

- Basketball half courts (46% support from those who do not support a skate park)
- Play equipment (33% support from those who do not support a skate park)
- Seating/shelter/spectator cover (26% support from those who do not support a skate park)
- Toilets and change rooms (20% support from those who do not support a skate park)
- Lighting (15% support from those who do not support a skate park)

3. Summary

In summary, the feedback reaffirms the outcomes of the City's adopted Community Sport and Recreation Facilities Plan and prior Council decision to allocate cash in lieu funding to the construction of a skate facility at Radiata Park in Aubin Grove.

A skate park is generally supported by respondents. When it comes to a location, the most popular preference of the two is Radiata Park.

Of those that do not support a skate park, Radiata Park is the most popular location to spend the reallocated funding. Aubin Grove is second but less popular by almost half.

Of those that do not support a skate park, none of the alternative funding options were supported by a majority in this group. Whilst not widely supported, a half basketball court, play equipment and toilets were the most frequent responses from this group.

This group of respondents are concerned about anti-social behavior and various types of amenity loss.

The Covid-19 pandemic presented a number of challenges in targeting and obtaining feedback from the youth demographic. Whilst a lot of input received overall, it is considered that this age group is underrepresented in the feedback.

This is important to note given this demographic make up a high percentage of the Aubin Grove community (compared to City of Cockburn) and are one of the most impacted by the provision of this amenity.

Results

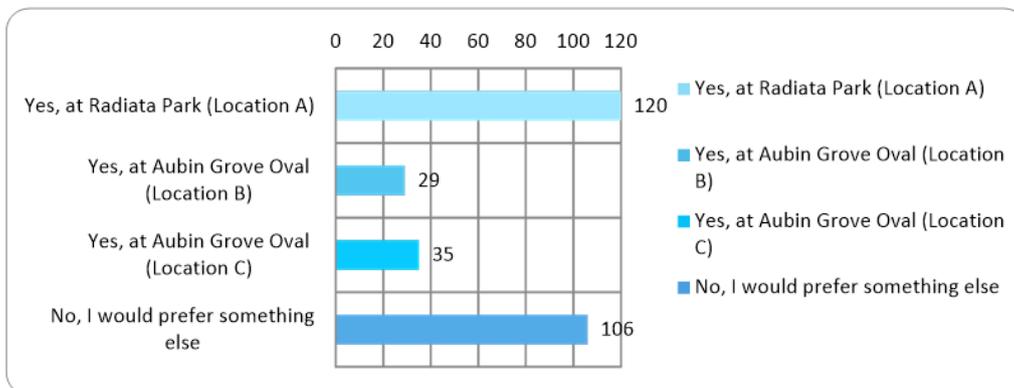
1.1.1 Survey

The survey was available on the City’s Comment on Cockburn website. In addition to the 1290 website visits, 750 people visited the survey. 290 survey responses were received.

A copy of the survey questions is provided at the end of this document.

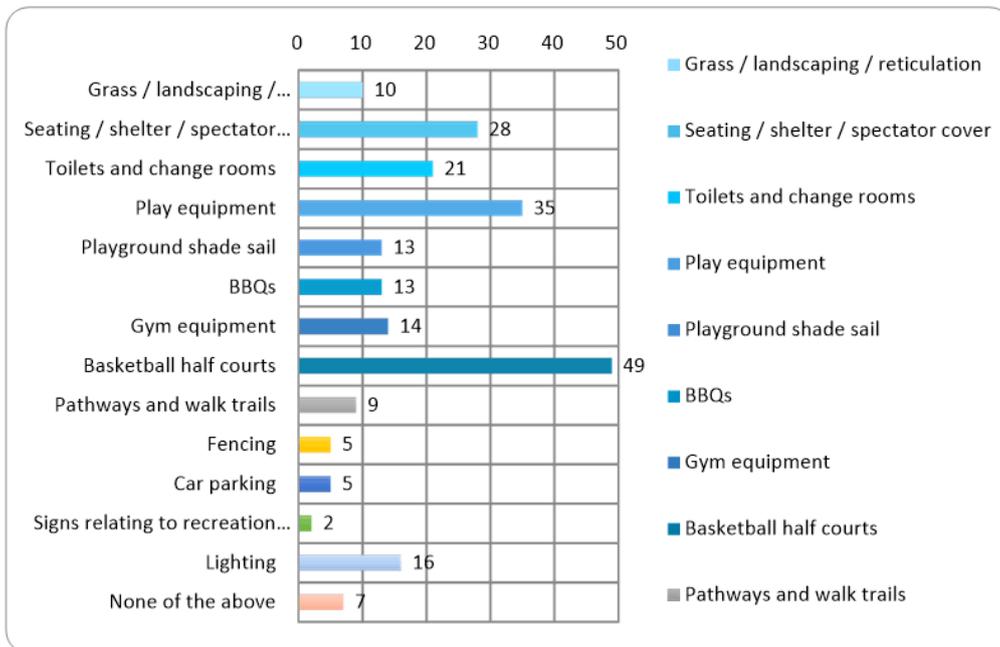
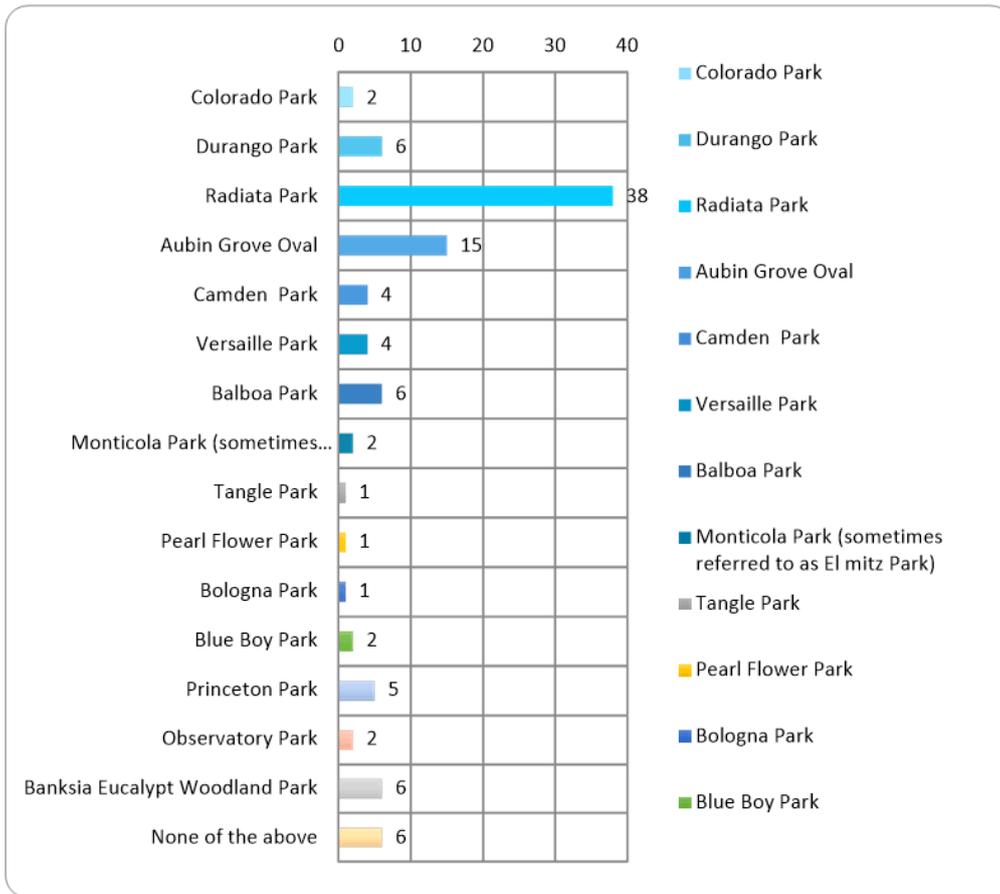
The survey responses are as follows:

1. Do you support a skate park in Aubin Grove?



2. Should you not support a skate park at Radiata Park or Aubin Grove Oval, please state a park in Aubin Grove that requires improvements and what could be allocated from the Cash-In-Lieu expenditure program:





Please outline your reasons for Question 1:

| Respondents who selected "Yes, at Radiata Park- Location A" |
|--|
| <ul style="list-style-type: none"> • Good passive surveillance • A quality plaza style skate park could be placed at the Radiata Park position. There are more family amenities and a toilet could be installed. Minimal parking encourages people to walk/skate to the park as the suburb is small enough to walk. • Such a great spot and under utilised currently! • Better opportunity's at radiata park. • I think Radiata is a better location. • Rather a bigger park so more kids can use it • Bigger style skate park that could accommodate different ages/abilities • Radiata Park is good location, not too close to houses. Are able to have a bigger, better skate park at radiata. • The facility is used extensively now by kids and this would be a great way to attract additional kids and a community hub for play. Whilst you mention Radiata park has limited parking the area on Tapper Road in Atwell had next to nil parking for years - this one could be accessed by kids from safe streets with little traffic • Location A is already set for recreational (play) use and provides ample design opportunity with undulating grounds. Existing amenities provides a good foundation. However you should make use of the wood chipped area and existing decking instead of placing the skate park in the marked A area. The wood chipped area is wasted space and its undulating surface means you could design a great skate park. The decking area could be a view point/sitting area for by-standers/guardians. A sea container cafe open on the weekends would be great for the community as the only cafe we have in the local area is the Aubin Grove Bakery which does not compare. Please use this opportunity and don't limit yourselves and our community. I don't see parking as an issue. If this is for the local community, then people should be in the vicinity where they can walk, scoot, ride to the park. There should only be small numbers of driving traffic (take the skate park on Tapper Road, Atwell as example - there are never more than a few cars parked there). • I would like the skate park to be as big as you can physically make it and to cater for all ages and abilities, Aubin Grove Oval is already to busy with the school and sports clubs • It's larger and central for everyone. Area B or C would be a nightmare during after school time considering how congested it is in the afternoons. • Wasted space at Radiata park not being utilised. Aubin Grove oval does not have enough parking for all the junior sport plus a skate park. Camden boulevard has too many cars parked on it. • Could be bigger at radiata park • I currently drive my children to the nearest skate park to us. I would love them to be able to walk their and having it at Radiator park would do that. My children also do a variety of sport at Aubin Grove oval and it gets packed. It would be absolutely horrid during various sports season if there was also the skate park there. Hence the Radiator Park preference. • Enables uninterrupted use on days where the oval would see large crowds from sports. Suitable for better skate facility. Away from main road and busy streets. More visible to homes which will discourage vandalism. • It is within walking distance to the local community. There are bbq facilities and Play equipment already there and it's a larger space for a better quality skate park. • Nicer family environment Maybe could include a toilet in the funding? • The population of Atwell and Aubin Grove is growing up with more youth in the area. An additional skate park to Atwell would be fantastic for the area. The limited parking might be an issue, but I think a little further away from the school and already heavily |



- utilised oval is a good option and most people could walk to the site.
- Oval is already used by many sporting clubs. Radiata Park has a large space able to accommodate a skate park without losing existing play space
 - I think the oval B option would interfere with Aubin Grove Primary students recess/lunch play. A just seems more logical. But I also don't mind Option C
 - Definitely not at school oval. There is insufficient room on the oval as it is especially with a large school and soccer/tee ball/school cross country events. Skateparks typically have lots broken glass which is not good next to a school. Support either radiata Park or what about Colorado Park which is not used that much by families. Also think about a pump track rather than a skate park.
 - Our kids play sport at aubin grove oval and it will affect the amount of teams that can play. It's also next to the community centre where many private events are held and that could cause disturbances
 - I think it's a great location and a great idea for the families of aubin grove.
 - Wasted space at Radiata park not being utilised. Aubin Grove oval does not have enough parking for all the junior sport plus a skate park. Camden boulevard has too many cars parked on it.
 - I'd actually prefer one in Hammond Park near high school but glad there's talk of building one regardless
 - Sporting clubs rely on all of the space at Aubin Grove oval.
 - I like the idea that it can be bigger. Aubin Grove oval is already busy during soccer season. Radiata Park is accessible via bus and it's nice.
 - Location C and B is used as sporting grounds with a duluting surface, so the skate park would only be single layer. Our children take part in sporting at Aubin Grove Oval and the clubs need all the space they can get. It also would mean even busier on the weekends if it were to have a skate park; making it a difficult facility to share with ease - there would be more risk of injury also due to more people. "
 - The park appears much more people friendly and already accommodates a playground and amenities which a family of all ages can enjoy. This could help make going to the park a fun day out for all members.
 - There is already existing facilities for families with younger kids and wide open space that can be more effectively utilised at option A. Older children/teenagers do not have things to do around AG and we have a growing population of teenagers whose needs are not currently being met. There is already a lot of playgrounds for younger kids so I would like to see activities for older kids. The school oval doesn't meet the distances from houses and it's used to a higher degree by the school, before & after school care, sporting associations, families, dog walkers etc etc. A smaller skate park may not be used as much as it probably won't be that fun for the kids. There isn't natural passive surveillance on the school oval as houses are not overlooking the area. Option b & c are located near a very busy road whereas option a is located on a street with significantly less traffic. Parking shouldn't be an issue as I imagine most people would walk, otherwise they could drive to the skateboard in Atwell!
 - "Existing congregation point for scooters and kids.
 - Better passive surveillance (especially over option C).
 - Seems like the logical place in Aubin Grove."
 - Like the idea of it not being scaled down => however, will add the positives of Aubin Grove Oval that it can be off limits during school hours and monitored, and possibly available to AG students at lunchtime?
 - I live in Hammond Park and am never likely to use the skate park (maybe for grandchild in the future), however I would not want the sporting oval to be impacted as I believe the community sports side of things is important
 - Not too close to the school and good amenities nearby for families.
 - A great location and plenty of room
 - More visibility, closer to homes for kids in the community, existing amenities

- Won't distract the kids Who are at school and there is playground equipment at Radiata for siblings
- As a larger skate park can be built at site A.
- The oval location is best kept as a sport only facility.
- The oval is used for other sports
- Better amenities already in situ.
- Slightly further away for us, but a bigger skate park is always better
- Good location, passive surveillance, not too close to houses
- The positives outweigh the negatives for Radiata Park. The facility will provide a positive focus for younger people in the area, and be readily accessible in what is a very walkable neighbourhood.
- "We have v limited facilities for kids aged 6 and above in this area. All play areas are for lil kids. Desperately need skate park and basketball court or ring to keep our kids amused.
- BMX track like Calleya would be fantastic. Please prove our area with more outdoor activities for older children. "
- With all of the other sports activities on the oval, I feel there would be no room for a skate park. The weekends are especially busy (once normal life resumes) so I think it would cause too many issues. Plus, not having adequate surveillance is not a good idea.
- More amenities for the area, more play and recreation options for children and youth in the area.
- As a father of 3 young kids I welcome the idea of a skate park in our suburb. I believe Radiata Park has a large under-utilised grass area suitable for a skate park. In addition I have been a coach at Aubin Grove United Football Club for the past 3 years and we have 300+ kids using the oval for training each week utilising every last blade of grass on the oval to the point we have to co-ordinate a training timetable between all coaches in order for our kids to have space to train. Our club is growing and I don't see this changing anytime soon. It begs the question why was AGUFC not contacted re this proposed development?
- Hello. This seems like a good spot for parents to also watch the skating and sports.
- I'll be growing my family in this area and I would love to be able to do fun activities and teaching them all sorts of fun that's free and easy access.
- Has extra amenities like seating and drink fountain and far enough from houses plus doesn't take away any of the sporting playing areas.
- Bigger skatepark the better
- If you're going to spend the money on a skate park don't disappoint the locals by putting in something that has to be restricted in size. Go the full-size skate park.
- Skate park requires adequate surveillance to prevent antisocial behaviour. Radiata is visible from all 4 sides of park.
- "Better for sports clubs in the area.
- Also a skate park is such an awesome thing to have in the area. Gives the youth something to do and also us older guys who like to wind back the clock and have a skate. I really hope this goes ahead. "
- "Parking won't be a huge issue as it will encourage famlies to walk or ride to park.
- Can you please make theses facilities available for bike riders as well. Even include a pump track."
- for reasons outlined above and children need lots of areas to play and keep entertained.
- Park has good parking, already has amenities and would be well utilised without affecting other areas.
- Plenty of space, great idea
- As an avid skate park user, I would far prefer a plaza style over toilets.
- "You could build a better skate park here and there are more facilities for a family.

- Option c should not be considered as it is too close to homes"
- Not at Aubin Grove as there is too much sport there already. PARKING AND TOO MANY CARS IS THE PROBLEM. YOU SAY LARGE CAR PARK. GET OUT OF YOUR OFFICE AND COME DOWN WHEN ALL THE SPORT IS ON.
- Camden Blvd already has too much going on. Between the sports on the oval and the school, most days it's always reduced to a single lane with a lot of people choosing not to use the "large car park" and park along the road anyway. Also besides I am an avid supporter of skate parks as I have grown up with them but live directly across the road from proposed location C and don't recall ever going to a skate park in the Perth metro area that close to houses.
- Aubin Grove oval is always busy with sporting activities and there is already a lot of parking on Camden Boulevard. I think Radiata Park is a much better option and would prefer good passive surveillance so there is no antisocial behaviour.
- Good for kids as also playground
- Better location due to lots of opportunities and lesser challenges
- Even though it is listed as an opportunity I feel that the location in such a close proximity to a primary school is unsuitable as it raises the risk of vandalism, groups or older children forming which may intimidate younger children and families. This I have already witnessed in the park area. There is already an increase in traffic at times due to the school and sporting facilities and it also detracts from the purpose of the area being used for sporting groups. If a skate park is going to be built it should be in an area where it can have a full scale park and all the necessary amenities in the one location not a scaled down size, so that all age groups and families can utilise the facilities as like the one at Bibra Lake. Both locations at Aubin Grove Oval are unsuitable "C" being too close to the main road and residents, as well as not being able to accommodate a decent size skate park and "B" too close to the school. Isn't this area utilised by the school for their sporting activities.
- A skate park offers social inclusivity, and social connection. A young, ever developing and ever expanding family orientated suburb like Aubin Grove should cater for the youth now and into the future.
- A larger skate park caters for a more children and Radiata Park has appropriate amenities.
- Compliments existing park amenities
- More spacious for the new facility in option A and less noisy.
- For the reasons already stated- I would be more supportive of this location
- More amenities - toilets can be added to improve this site, venue is more centrally accessible.
- "The oval already causes traffic, noise and antisocial issues. Further visitors to the park will only increase these issues.
- Living opposite the Camden Boulevard option I feel the provision of a skate park will further devalue the house prices and cause unnecessary disruption to this already overused street. Movement to a less busy park may reduce these issues."
- The oval is already too busy with cars double parked at weekends and high noise from the softball sessions. Another activity venue would cause more disruption for local people. We have already been disadvantaged by not being informed of the building of the community centre when we bought the land and there are often parties and loud noise and lots of cars. House devalued already. A skate park is not attractive to potential buyers. Nothing happens on Radiata Park now so would increase the community use of this venue
- Sporting clubs will be impacted by options B and C
- Radiata Park is not being used as anything more than a playground
- Better integration with comm9
- There is so much area at Radiata Park that is not currently being used whereas the other option is currently being used by sporting organisations and this would effect

kids playing these sports

- The oval is highly utilised by sporting clubs, the school and as a venue for community gatherings (eg food truck evening, fair), I feel having the skate park there would have a negative impact on these activities. It would reduce the useable space which is already at a premium when the sporting clubs are using it. Radiata Park already has a great playground for the younger kids but it would be good to have something there for the older kids to enjoy, many already take scooters etc to the paths there - a skate park would just be an extension of this.
- The impact on sporting use at Aubin Grove Oval to great. Radiata is not sporting it is recreational so a better location.
- I believe the sporting community will be impacted by the skate park. There is many clubs that use the venue.
- My children play Tee Ball and soccer at the Aubin Grove oval. Having a skatepark here will impact how many teams can train at one time and how many games can be played.
- No impact on sporting clubs which utilize Aubin Grove Oval
- Leave Aubin Grove Oval for softball and soccer
- A longterm full scale skate park in the central location. People always walk to local parks and it encourages exercise rather than driving small distances.
- This is because, as some who has lived in Aubin Grove their whole life, the Aubin Grove Oval is not a good option. This is because on the oval there are younger kids that play there during school and it would be a concern that they might sneak out there. Also it is impractical as there are clubs that play sport over there which could result in an accident or distractions. Radiata park is an open and friendly place where a larger skate park can be built which would be great in the long term.
- "Skate parks are fantastic for the community however there are established sporting clubs
- situated at Aubin Grove Reserve that would be heavily impacted by the inclusion of a skate park at either of the proposed locations there. The proposed location at Radiata Park would also allow for a larger skate park design."
- Skate parks are excellent infrastructure to provide social and exercise opportunities for residents of City of Cockburn. Radiata park will allow for a larger multi use park installation. There are established sporting associations that utilise Aubin Grove Oval. A skate park in that location will potentially have a negative impact on their ability to conduct their activities/training/game days. Whilst having it located at Radiata Park allows for greater outcomes for a larger number of residents.
- It cannot be put into Aubin grove oval. The Tball and soccer clubs already struggle to have enough space to accommodate the kids without more space being taken away. Radiata park has plenty of space, popular already and far enough away from houses to not cause issues.
- The Aubin Grove Oval is already fully utilised by various sports. If you build this park at the oval you are taking away necessary space that is needed for clubs such as teeball, softball, soccer, baseball etc.
- The sporting facilities at Aubin Grove Oval are an important part of my childs life. Having a skate park at location B or C would affect these facilities. Looking at the pro's and con's for all options it looks like Option A would be the best.
- No disruptions to existing community sporting T-Ball and AGP School sporting events.
- "The oval is already heavily used by the school and sporting venues so a skate park wouldn't be viable there.
- I don't think Radiata Park is a great option either.
- I think Princeton Park would be a better option as it isn't heavily used and is close to public transport for ease of access. "
- Existing amenities and can accomodate a plaza style skate park



- Great location for this in my families opinion. Already has the park, exercise equipment, what a neat addition to add the skate park to this already great space.
- Don't want it at Aubin Grove oval!!!
- Because of the opportunity for a plaza style skate park and because families can visit radiata park with older and young children and have facilities that suit both.
- The other options impact on a softball /soccer playing ground
- Please do not reduce the oval space on Aubin Grove Oval. The sports clubs require lots of space to play games. A skate park will limit how many clubs and teams we can include
- To be able to accomodate a decent sized skate park to suit all ages and the current amenities allow for a family day out BBQ etc.
- I think a skate park would be great for the older children who don't have much to do in the local area. I would be happy with any of the locations suggested.
- Site B and C have a car park but when any sports are on the car park is full and cars are parked on the streets.
- There are BBQs in this area so it could be more of a family setting, The other location (Camden Boulevard) I don't like as the oval should be kept for sporting activities.
- Having the skate park and the playground together would be handy for different aged kids
- full sized park
- so it doesn't impact on the sporting clubs that use Aubin Grove Oval
- "Aubin Grove oval already has a large number of people attending this site for sports activities. It would only further congest this site more.
- Radiata Park is easy access , easy parking."
- Close to home
- The opportunities outweigh those of options B and C - minimal parking doesn't impact me, it's closer to the bus stop (200m vs 300m). I don't mind that the facility doesn't have toilets.
- Bigger skate park
- Other options too close to school play area and sporting facilities plus bigger would be better
- Aubin Grove Oval is home to Junior and senior sporting clubs and associations and I feel that the skate park would conflict with these and be better used at Radiata Park

Respondents who selected "Yes, at Aubin Grove - Location B"

- skate park will look safer and more appealing near a community centre
- "So many little kids go to Radiata Park that I don't think a skate park is suitable at all.
- Oval has plenty more space and proposed spot B is away from the residents"
- "At the Aubin Grove oval as it is a larger space and away from a family orientated park where young children and parents go.
- My girls love skateboarding & should have one close enough that we don't have to drive to it.
- Adjoining the playground is great location
- The other sites would not be suitable for residents living close to the road, especially on Radiata and Caribbean Turn. Lack of parking would be also a nuisance. A self cleaning toilet would be ideal at Aubin Grove Oval as it would also service the other park users.
- Radiata park is used by families for picnics, birthday parties etc. it's small and surrounded by houses with small children . Not as much safe parking either
- It's close by and off the road so it won't change the aesthetics Of the area as much
- Lots of parking close to other facilities. Adding a basketball 3x3 court would be great too
- Bigger space around so for participants who aren't skating can set up without being so crowded. Also abilities to do other activities with smaller children who may not want to skate such as kicking a ball ect

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| <ul style="list-style-type: none"> • Radiata Park is too small and many houses are around this park. The last thing we need are teenage hooligans around residential areas. Noise would be appalling and traffic would be an issue with parking blocking the area. • Aubin Grove Oval has more space to accommodate a skate park. Plenty of parking too. Location C is appears easier for passive surveillance. • Radiata Park is enjoyed by lots of families with young children often under the age of 10 and lots of families with dogs. The skate park I believe may put these young children at risk of accidents if placed too close to the play equipment. • Close to the school. Better amenities. CoSafe base themselves out of the community centre so will have better security at night. Further away and better protection for the houses from noise • Its better choice • Radiata Park is always used by small kids and families. The Skate Park will be very disturbing for the peacefulness of the surrounding and all the families enjoying the park. But, Oval is a big area and a very good venue to accommodate a Skate park because it does not occupy by the small kids and families with young kids. • "Proximity to toilets. • If skate park is built at Radiata there is bbq and drink fountain but no toilets so peoples bladder and stomach would fill up then they would need to go to the toilet while at the oval, there are no bbq and drink fountain but toilets. Plus they can go for a picnic and bring their own food and drink" • It's good for kids to have something else to do and be active while a safe place to skate rather than on streets or abandoned places. • "Enough Parking and away from road at location B • Radiata park too close to homes and not enough parking... neighbourhood is quiet and do not want this too close to our homes" • "Community park with toilets etc. • Option A is way to close to houses and with an influx of teens will drive out the out younger children. It will also put the onus on local house holders to act as Quazi security guards and interfere with their quality of piece and quiet. It will also reduce property values." • "A is far to close to houses, and would wreck one of the best parks in Aubin Grove by putting it smack bang in the middle of a beautiful grassed area. • B is also close to houses. • C is further from houses and the road and kids could possibly skate in their lunch time if possible. Also large car park nearby and bus stop not too far away" • I think it would change the dynamic of Radiata Park. I think it's much better away from housing as this is a place where youth will tend hang out at night time. This could cause disturbances to the neighbourhood • I live near radiata park and I believe that with the playground exercise facilities bbqs and play space that there would not be adequate room in that park for a skate park. Option B puts the skate park away from the open areas where t ball and softball are played and into an area which is a bit of dead space. • I live near Radiata Park and believe that this park is too small with all the facilities that are there now. Location C is where lots of kids play T ball and soft ball so Location B makes more sense if a skate park has to be installed in Aubin Grove • I live near radiata park and I believe that with all the facilities at that park now a skate park would not have enough room. Location B would be a better fit. |
| <p>Respondents who selected "Yes, at Aubin Grove - Location C"</p> <ul style="list-style-type: none"> • "Radiator park is used by many young children. • Ideally I don't want a skate park at all but if we have to, location C" • "We have many kids in the sunburb who would benefit from this. • "Radiata park is beautiful and nice for a casual stroll. The skate park would ruin that park. There are also houses that back on to Radiata so it would ruin the view they |



- purchased their blocks for.
- Advantages of Aubin Grove oval is that families already go there to play sport and so when one child in a family is occupied playing sport, the others can be in the skate park"
 - Better access for cars and parking.
 - Parking available. BBQ and Fountains can be added and would be well used by sporting groups.
 - "I live across the road from Radiata park and it's heavily used already. Also depends on what sort of skate park ie Atwell older skate park or younger kid skate park like in Calleya Junior track the council is thinking?
 - I vote the oval, it's a huge area"
 - A toilet is a necessity, there is already water there so a drink fountain can be easily added too.
 - Parking is excellent in this area and by building it here it won't disturb the use of the oval
 - An area that will have little impact on existing park areas already in use.
 - More of a central hub to the Aubin Grove community. Also has existing infrastructure around.
 - Aubin Grove oval is heavily used with multiple sports during the year. Radiata park has a large grassed area not used and kids already spend a lot of time on bikes, roller skating and scooting on paths at radiator park
 - Radiata Park is always full of families playing in the grasses areas with young children. Putting a skate park there will allow riff raff to take over. Having it at Aubin Grove oval in location C, there is a larger grassed area where users won't be able to annoy younger children more space is better
 - Keeping it at the oval makes sense. Radiata Park isn't a good option as it's already a well used park and too close to many houses.
 - "Anywhere but radiata
 - Too close to houses and not enough parking "
 - I believe we need facilities like these, locally. Although there would be better options than Radiata and Aubin Grove Oval, option C is the better option presented.
 - Radiata is too small and focused for young children.
 - Close to road. Less risk of abuse of facility after hours as in a higher flow visible area to passing traffic
 - "Keep the open space at radiata.
 - A heap of concrete would be an eyesore in a residential area.
 - Lots of room at the oval. "
 - I would love a skate park in aubin grove. The kids are all growing up and they really need an area fir the teenagers and for them to gave something constructive to do. I choose site c near the school as there is parking away from houses but you can see it now hidden behind school
 - The community centre is there and skateparks form part of the community.... Radiata Park is it's own sanctuary.
 - Aubin Grove needs more activities for older kids & teenagers. Lots of kids live in Aubin Grove so we need to plan ahead
 - I don't like any of those locations but no "other" button to tick. Yes to skate park but at one of the other parks. Perhaps Princeton, that's a useless space that is barely used
 - More space compared to A, better surveillance from Camden Blvd (compared to B where view appears to be obstructed), close enough to public transport (kids have legs, they can walk 300m), less noise intrusive as compared to A, BBQ not required at a skate park, there should be a drink fountain at Camden Oval anyway (I'm amazed there isn't one already giving it's a sporting Oval), so installing one should not be a barrier.
 - It's close to the school and camden blvd making activity visible.

- "Parking is hard enough as it is at radiata park, where as there's ample parking at sites B and C. There's enough room for a plaza style as well.
- Families with very young children frequent radiata park with the incredible playground available. A skate park would not be the safest area for kids of that age."
- Parking and closest to street for better security. Also Radiata Park has Magpie's swooping during mating season, which is 2-3months
- If it's on a corner it will have more visual aspects and it will leave more of the oval open for other activities
- The Aubin Grove oval is a better choice as Radiata park is not suited to cater for groups of youth. The proximity to residential houses will certainly be a problem when it comes to noise, littering, vandalism and parking. There will also be a concern about devaluation of house prices around Radiata Park. The oval has the space and the layout to much better accommodate a skate park.
- Something for kids to do hopefully doing the right thing
- The Radiata park is only a small amount of green and needs to be kept that way. It is also further away from resident houses.
- Radiata is to close to houses and heavily used already.
- "Both option B & C are more suitable as it is already a central community area so the set up would be seamless to address more traffic with suitable parking & toilets. A skate park, playground & wide open space next to the community rooms would increase attractiveness & open up the area for more use. A water foundation should be installed.
- Option A is already a busy area for local residents which is used for a wide variety of activities attracting many pedestrians so increased traffic, narrow roads, limited parking with already a high pedestrian footfall would not be ideal in a more confined area."

Respondents who selected "No. I would prefer something else."

- Skate parks are not the best use of these funds
- Why are we accepting payment in leiu by the developer? Surely they should be upholding their end of the agreement and fulfilling the contract. They can pay for the skate Park. Simple.
- I think skate parks may attract unwanted people to the area. Radiata park has enough already and the oval is used so much with different sport group a that think it would be a shame to have a skate park in either of these locations.
- "Radiator park is very popular with kids and families under 10yrs it will attached unwanted older kids.
- There's no access to basketball or tennis courts locally and this would be a better option"
- "We live close to Radiata Park & having lived close to a skate park previously (and part of the reason for moving was to get away from the trouble & noise it caused) we are strongly opposed to a skate park being built at this park. Additionally, as we have young children, we do not believe that a skate park would be ideal and would likely prevent us from enjoying the park as we do now!
- Why should a skate park be built in Aubin grove when there are already plenty of parks of children to play in? Whilst there are zero dog parks yet you take registration money for dogs and allow people to have dogs in Aubin grove yet provide no off leash areas for dogs to exercise and socialise.
- Plenty of other things the money could be spent on that will give a much wider group within the community the opportunity to enjoy.
- The playground needs more toddler friendly play equipment like a slide. Some water play features would be great for the kids too.
- "I work at a local government that has a skate park near housing and I would never want to live anywhere near one. Like it or not, it will attract anti social behaviour, the elderly will not feel comfortable going for a stroll at any time, rubbish will constantly



be left behind, the 'click clack' sound of the skate boards will radiate to houses and upset the peace and tranquility of the neighbourhood and skaters will then want lighting to be left on during the night.

- We already have an increase in anti-social behaviour and this will only add to it.
- I am strongly opposed to a skate park in Aubin Grove."
- As a resident at Radiata Park and the amount of use the park has for families with children old and young and it get used a lot for get togethers A skate park will eliminate that. I lived previously next to skatepark and I'm NOT against one but you seriously need the right spot and really out in the open and this park is not one for that. Kill the family get togethers completely. You need to check out the movements at this park with families and you will realize not right.
- I live right opposite the community centre where the skate park is planned and I strongly object to having kids hanging around all hours of the day and night causing problems outside my house. Skate parks attract older kids and often problems associated (eg drinking) there has enough of this happening already in the community centre carpark with youths often walking up and down the neighbouring streets at night, I don't feel that this should be encouraged or additional places for them to congregate.
- Skateparks are single use - parkland or even better a nature playground with flying fox would appeal to and be used by children of all ages, would be attractive to look on by the homes that surround Radiata and would not have the noise or sight pollution a skate park has. If people want their kids to skate there are other skate parks already in existence in Cockburn but there are very few nature style playgrounds and none in Aubin Grove. I feel this option would be much more accepted in Aubin Grove. I know I would be very unhappy if a skatepark was built in my front yard, like the homes on Radiata would have to put up with. No thanks.
- Every other suburb has multiple basketball courts. I'd like to be able to walk to a local court.
- It will bring all of the dodgy kids to the area and i don't really want to be near that but i won't have a choice since i live close to both of them
- Water it enough and landscape so it is nice to play on
- The oval would not leave enough space for sport groups. A skate park is not suitable for all ages. There is already one close by in Atwell. Radiata could do with a nature play area with a flying fox, toilets, and maybe a basketball half court as some ideas. Thanks for allowing input :)
- I don't want a skate park as I believe it will attract trouble to the neighbour hood
- Aubin Grove Oval parking is already a nightmare with people overflowing on Camden Boulevard when sports are on. I'm also concerned with the potential for increased in crime to nearby houses
- Having a skate park at Aubin Grove Oval is absolutely ridiculous with the amount of sports that are held there. During soccer season there are hundreds of people on games days and numerous games going. The skate park will intringe on space
- There is already a great skate park at Atwell. On my daily walks I see many basketball hoops I'm driveways. Funds would be better spent with a couple of courts at Aubin Grove Oval or grand stand seating if Aubin Grove Oval hosted cricket, football, soccer etc. Be good if Aubin Grove had teams like this in more semi professional leagues.
- Skate parks bring unwanted youths to the area and can increase vandalism. Aubin Grove has the highest density of children in the metro area evidence by the school numbers. Maybe extend radiata park and make it into an all ages park
- It would be great to have a play area that caters for younger children in our community. There is copious amounts of research on the benefits of nature play and there are not any close by. It would be great to see nature play incorporated into radiata park, or a local park in general.

- School oval will get increased break ins at school and area will get trashed like Atwell with rubbish everywhere. Radiata is for young families and not suitable for high school kids. If the park location changed more people would be happy.
- All options are too close to houses, and there is already a skate park in Atwell.
- I don't think another skate park is appropriate. I would like to see something that all the community can enjoy.
- Too many problems associated with skate parks and teenagers.
- Definitely a no to skate park. A lot of kids play basketball on their verges so I think a half court would be great
- skate park doesn't appeal but Something like a half court for basketball would be better at either of the suggested skate park locations (preferably the oval). And maybe with workout equipment and climbing structures. Still great for older kids and appeals to wider range of interest and kids. I know personally neither of my children or their friends, would use a skate park...
- I don't believe there is enough room at Radiata Park and the Aubin Grove Oval is already heavily utilised for sports and other recreational activities. Why concrete a beautiful grassed area when it is the only large grassed area we have in Aubin Grove. And I'm also concerned about the people it would bring to the community. There is a skate park already in Atwell. I don't believe we need one in Aubin Grove.
- Don't want groups of teenagers congregating around Aubin Grove.
- I am happy with a skate park but the two locations selected I don't feel are appropriate. There are a few parks in the first estate that would be perfect for a skate park as it is already hilly.
- It would be closer to the train station, which seems to have brought more people to the area. More convenient, and prevent people having to travel from the train station down Lyon Road, which is already very busy for residence.
- Increase large groups causing trouble in 1 spot.
- "Skate parks are mostly used by older kids and should be located closer to high schools than primary schools. I am also concerned that a skate park will draw a lot of activity at later hours to a quiet neighbourhood.
- There are other skate parks in surrounding suburbs. Skateparks are fit for one use, and they tend to have poor aesthetic appeal. Aubin grove is a beautiful green leafy suburb, and to place a concrete skatepark I do not think will fit in well. Basketball courts can be used by a greater range of ages (adults too!) and integrate well into the existing landscape.
- "There are so many concerns in the community about a skate park in the proposed locations but I feel the biggest problem is taking away what the spaces are already used for - I see children having so much fun playing cricket and soccer in friendly neighbourhood games often orchestrated by parents everyday at Radiata park, the open grasses space is well used for These activities as well as children's parties with the capability to bring in pony rides thanks to the great space and supporting shade, bbq, playground etc.
- It's also horrifying to imagine the AG primary school losing some of the valuable space the community oval provides, not to mention the fairs and markets held here, local sports associations who train and play their games there also would suffer from this change. Surely there is another park with less valuable use of space which could be changed to a skate park.. e.g. Camden park. The overall lack of seating, shade, landscaping, bbq facilities etc makes it a under used space, and the playground is pretty sub-par compared to those surrounding. Couldn't this be easily overhauled to become a skate park - almost perfect proximity to bus location, can share parking with community centre and school, and isn't really a well used park given the other superior spaces surrounding, so I doubt very much the community would feel any negative impacts."
- "Too many teens lingering and already have significant increases of issues around



- the suburb. Also too close to neighbourhood that will create noise and damages.
- Suggest to create skate park near Hammond Park where High school is located for the teens after school play that would make it more convenient for them."
 - The park is so busy already with young children bringing in scooters bikes & skateboards I would think would not be safe
 - Even tho it was just done and the basketball court cancelled, They only added swings for play equipment. I think we can do much better! Nature play for one, so many kids use this park and the climbing structure is useless. No one uses it. Add a slide at least....please
 - "There is a Skate Park in Atwell.
 - A dog park would be better or if a skate park, all options shown are too close to residential properties, especially option A
 - I have seen the way atwell skate park is. I live off radiata . I do NOT want to have a skate park at the end of my street. Lets have some nice water walk ways , dog park . there is enough for children . Definitely a no from us and I have 3 teenage children !
 - Skate parks will attract undesirable youths after hours
 - A skate park will have a detrimental effect on the residents close to the park due to increased noise, lack of parking, decrease to open free play space, increases in anti-social behaviour, driving young families away from area, creates an eyesore - no one wants to live on the same street as a skate park.
 - "We live very close to suggested site at Radiata Park, we feel the skatepark will attract unwanted behavior, and will be extremely noisy. We feel less safe for our young children to play on the play equipment, and could be less inclined to use the BBQ facilities for family lunches etc,...
 - There is also very LIMITED CAR SPACES as it is, so this would also be a major concern, since so many parents drive children to the skatepark and wait in their cars."
 - A golf course In jandakot would be best.
 - Afraid it will bring in troubled youth from surrounding areas want our parks to be more family orientated for younger kids in area
 - While I would support a skate park in Aubin Grove, I don't think Radiata park or Aubin Grove oval are the right places for it. Radiata park is already well used by kids playing cricket and soccer and a skate park would use the space currently being used for these activities. In addition, I don't believe there is enough parking that would be needed if a skate park is built there. Finally, the park is a very popular meeting place for local families.....it is not often empty. A skate park would attract out of area people and thus I believe would make the park too busy and would spoil the atmosphere. The oval maybe a slightly better option, however, this can be very busy already with softball and soccer. A better option would be basketball facilities at either venue. That would provide some facilities for the older kids.
 - There are enough skate parks in Cockburn. We don't need another one. We need more things that everyone can use not just limited to kids.
 - my house is less than 50m to Option C
 - I have lived on Aleppo Street since the 2007 (when the development began) and watched the use of the park grow over this time. My own daughter using it from aged 5 and all through her teenage years. A skate park is not a welcome addition to the beautiful green space. Over the last few years, more and more family groups and community members have come together to play cricket, soccer, football and all the adults/ children get involved - walking from streets surrounding the park. Adding a skate park will take away valuable space for this to occur, potentially increase non-local traffic around an already busy park - with no parking. The roads either side of Radiata already have traffic that speed through AND the Camden Boulevard corner is so tight that more traffic will be a safety concern. I acknowledge that you want to improve the amenities for a certain age group, however, in this area they are already

participating with family groups. As a long term resident, I do not endorse a skate park in this location.

- "We would love a skate park, just not at the locations in question 1.
- In my opinion a skate park would be better suited at Princeton Park."
- "Radiata Park is not big enough for a skate park. Lots of children play & kick the ball. Also the path ways are used for walking, running & kids riding bikes. To close to houses around. Aubin Grove Oval has the space & the parking & not on top of homes. Thank you
- "Not keen on skate park, other expenditures would be more useful for the community in Aubin Grove. Plus option C is too close to my house.
- Noise impacts are too close to residential area. Free grass space is already often wholly utilised
- I live opposite and I believe there is enough large infrastructure at our park already.
- Centrally located . larger population of children
- These are common necessities for the neighbouring community.
- There is already a skate park in Atwell. There is enough traffic, noise, late nights, rowdy unruly people who frequent the Oval in Locations B and C. We don't need any more. Toilets at the playground at Radiata Park would be helpful so it would be better to re-apply the funds.
- Best is to open school sports ground like basket, tennis to public and why not more tennis ground where cockburn city can have a tennis club which is a real sport for youngsters and seniors. Skate park will attract only a certain age group
- Nature playground, flying fox and scooter track around it.
- "Radiata is the main park for entertaining in the suburb. Blue Boy Park is such a great space with nothing to do.... the pigeons and ducks have ruined the grass area... but surely there could be more done to develop that area... with more undercover areas... or something to make that side of the suburb more user friendly.
- Aubin Grove oval is busy all year, with school functions and activities plus numerous sporting clubs, with the school already having 1100 students parking is cramped as it is. Skate parks encourage loitering and antisocial behaviour which we don't need near our school and sporting clubs
- All the options are within dense residential area. This will be too much chaotic. Especially Radiata Park supports quite nicely for small kids including toddlers. Skate park will pose a question to their play area and overall their safety.
- It is quite dense residential area. Skate park will be a safety problem for kids (especially toddlers) who enjoy Radiata Park and Aubin Grove Oval.
- I think a pump track would be a better addition then a skate park. Princeton park has a lot of spare space for 1 to be built.
- Skate parks are unsightly and have a tendency to attract problems. It would be better to provide other sporting facilities within the area.
- "Industry studies show skateboarding involvement is down for nearly every age group, including the youth.
- Participation in skateboarding has declined. In the past ten years, parks are less crowded than they have ever been. "
- Aubin Grove does NOT need a Skate Park especially at Radiata Park. There is already a skate park in Atwell and another for Hammond Park. We have limited large green open space areas especially in such close proximity to housing as Radiata Park. My family, neighbours and fellow Aubin Grove residents vehemently oppose a skate park in Radiata Park or anywhete in Aubin Grove. The funds should be better used maintaining, repairing and/or updating current facilities in Radiata Park and across the suburb.
- "Aubin Grove Oval does not have sufficient lighting to deter loitering, and it's a very busy sporting oval. A skate park would intrude in the sports areas
- Radiata Park does not have sufficient parking at all! And seems to attract bad

behaviour at the moment. Skate parks tend to encourage people who loiter, anytime of the day. There is a big skate park in Atwell, which is hardly used at times."

- The park is well addressed in most of the listed points, but a idea might be having a nature/action type play area
- If a skate park is constructed at Aubin Grove Oval it will negatively impact on tee-ball facilities at Aubin Grove Oval for the Yangebup Knights i.e. affect playing diamonds. There is a skate park in Atwell which is sufficient
- Our children play team sports at the oval and in the playgrounds at both locations. A skate park would create a teenage / older crowd, increase in rubbish, increase in antisocial behaviour, decrease parking availability and impact on the t ball diamond for training, games and carnivals.
- I feel we need to create a 'community' vibe and skate parks create negative vibes. There are no toilets at Radiata and this will pose a huge problem. A nature playground would be better suited. Things to promote safe risk taking. There aren't a lot of parks in Aubin Grove that cater for the younger kids.
- "I don't think there's an appropriate location for skate park in Aubin Grove. Would prefer basketball facilities/nature playground with flying fox/equipment that could be used by wider audience.
- There are several parks/locations that would suit basketball court but as could only choose one above I selected Radiata. "
- Closer to Public Transport, do not need more teenagers travelling down a busy Lyon Road from Train station. Which is a concern, as this could increase troublemakers or crime down the suburb.
- Worried about teenagers if not supervised and no toilets. Also space at Aubin Grove Oval is a huge concern
- It's used for sport so this would take out it's needed area.
- Skate park in Atwell is under utilized
- "Antisocial behaviour
- And poor locations "
- Radiates park is old and tired. New equipment and maybe nature type park would far more beneficial to local children than a skate park. No public toilets at any park in AG. As a resident living within a few metres of the proposed skate park I strongly object to it. There is insufficient parking, will be increased noise, skate parks are ugly and not appealing. Not needed in AG due to skate parks being in Atwell, Bibra Lake and new one in Hammond Park.
- Close to home. Would be nice to have some play equipment
- The oval NEEDS the above points. We do not need a skate park!!!
- We don't really need one in this suburb. There is already one in Atwell.
- A skate park is not a suitable option as it tends to bring together large groups of youths
- There is a skate park in Atwell only a few minutes away and we don't need another one
- 'Neither location is appropriate. Existing skate parks in The City of Cockburn are located well over 100 metres to the nearest residential property e.g. Atwell and Bibra Lake. Radiata Park is within 30-50 metres to many houses. Young people and adults alike are already enjoying this unique and rare green space the way it is and regular activities include:
 - BBQ's and picnics
 - Birthday parties,
 - Community sport and play (mainly cricket, football and soccer), which requires large areas of green space
 - Small children activities, such as scooting and walking with family and friends.
 - Family bike riding,

- Community fitness groups and dog walking
- Parent groups (mainly associated with Aubin Grove primary) meeting before and after school.

The current users should not have to yield to a facility which will attract a more socially contrasting group to those already using the park.

- Graffiti, vandalism, burglary, drug use and littering go hand in hand with a facility of this nature and the council cannot ignore the fact that the burden of “passive surveillance” will fall unwittingly on those residents living closest to the skate park whatever the proposed location.
- The local residents and sporting clubs directly affected by either of the proposed locations do not wish to be forced to vote against each other by preferencing one location over the other as neither is appropriate.
- There are other facilities which could be considered for either location which have not been included in question 3 which should also be considered."
- The idea of a skate park in Aubin Grove was not presented in consultation with the local community. The presence of a skate park so close to residential properties, in both the two locations put forward by the council, is not suitable to local residents. The two parks in this proposal are well utilised with other activities all year round by local residents, activities of which will be greatly impacted should a skate park be built.
- "Radiata park is a popular park for young families like us living adjacent to the park. We would like to see toilets installed there. Also nature play area would be nice and it gives more options to a lot of families who use for weekend picnics and after school. I do not think we would benefit from a skate park.Thanks Sahi"
- There are many young families surrounding Durango park who have to walk all the way to Radiata Park dor proper play and gym equipment. Toilets will be a great addition.
- I prefer for basketball half courts to be built
- "Dear Parks Manager,

I am writing in regards to the proposed Skate Park for Aubin Grove and the 2 options being considered for its construction – Aubin Grove Oval and Radiata Park. I have 2 thoughts on this proposal and will elaborate on each in turn.

In the first instance, I am opposed to the idea of having a skate park all together at Aubin Grove. This decision is partly due to personal experience living near one previously (in NSW) where the escalation of anti-social behavior was evident. There is also enough documented evidence of this occurring, in some instances.

Radiata Park, is a peaceful setting where adults and children engage in bike rides and field activities such as cricket, football and soccer. Also it is conducive for peaceful walks and conducting meditative activities such as Yoga and Tai chi, which is carried out by various people at the park. The addition of a skate park, in this location is dissonant with the overall feeling/vibe of the place, and will most likely only appeal to a minority of people.

The addition of the skate park, will no doubt be greeted with excitement initially but, my concern is that interest will wane for the majority, and with less people interested, the potential of that leading to anti-social behavior with a small group becomes increasingly likely especially “after hours” where this becomes more of a problem. I have seen this first hand.

You also noted as one of the positives of having the skate park at Radiata Park, is “good passive surveillance”. This worries me in that, there is an implicit suggestion that the residence close to the park, will need to take greater responsibility for looking out for the safety of the children, and the potential ensuing anti-social behaviour. I do not agree to this added responsibility, as it does not improve the living conditions of those close to the park.

I would prefer that the substantial funds available be put to better use to enhance the



peaceful environment of Radiata Park, make it a haven for peace and tranquility. The Field area in front of the "viewing Deck next to the BBQ area" for example could do with some sprucing up – a proper native garden, for example would be fantastic. Giving the children an opportunity to engage and learn about our environment, attract more birds species etc.

I am also concerned about Monticola Park. It has been very well landscaped, however the "wild Bush" is looking very sad and Die Back is evident. There is so much potential to make this a little oasis with careful landscaping. There is already an unsealed track around it (mind you there is already graffiti on the walls! – (a digression – but do we want more flat concrete surfaces for more of this around the skate park??)). Funds can be used to improve the environment here.

I am sure other Residence will have other great suggestions. I feel we need to invest in projects and ideas that are enduring – to improve our collective state of mind, body and soul, and not just for the immediate gratification.

If a skate park is something seriously being considered, then my question is has this been thought through thoroughly? I have seen some amazing skills on display at skate parks, where it has been structured in a way that it is viewed as serious sport, with proper structures and governance in place for safe use. It is a dangerous sport if not conducted appropriately, resulting in serious injuries in some cases. Has this been considered?

If it is going to be considered a sport, then it is much better suited to be at the Aubin Grove Oval, where all manner of sport is carried out and the Skate Park and skating in general is viewed as a legitimate sport, with the appropriate mind set and appropriate rules put in place for its use, with a strong focus on zero tolerance of anti-social behavior. Can this be organized and guaranteed by the Council and the Parks Manager? I would suggest that it would be difficult, but needs to be seriously considered.

So in conclusion, I am opposed to a skate park in Aubin Grove based on the reasons given above. If however a skate park is to be constructed, then it is more suited at the Aubin Grove Oval (Location C), with strong focus, deliberation and consideration given to zero tolerance of anti-social behavior, appropriate measures put in place to ensure the physical safety of the users of the Skate Park, appropriate security and surveillance provided by the council.

I look forward to a positive outcome.

- A do not support a skate park being built in Aubin Grove. This will be an attraction for undesirable people and their behaviours. We do not want our quiet and peaceful suburb to be subject to any anti social behaviour. The location of Radiata Park is currently utilised and enjoyed by many different locals for picnics, walks, bbqs, soccer games, kite flying, dog walking, children playing, running, riding, scooting... The construction of a skate park will see these locals and activities decrease and put people off enjoying their beautiful park!
- Radiata Park is 15 years old. It requires maintenance and a refresh of new native species around the deck area. New playground equipment and a half court basketball would be good.
- Do not believe either of these two locations are suitable for a skate park
- Not necessary and woul negatively impact the residence
- We need toilets, nature play or basket ball half courts. These will be more useful to young families living around the park.

1.1.2 Submissions

31 submissions were received via phone or email. Please note that the 31 submissions does not equate to 31 new contributors. 10 people contributed their feedback via both submission and survey. (Those not supportive of the skate park were more likely to contribute a submission and a survey).

The feedback of these 10 respondents is only considered once in the overall data analysis so that every respondents' point of view is considered equally. All 31 submissions are provided below.

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| <p>We would love to see a skate park in our suburb. Our 1st option is Radiata Park but just as happy to see it at the oval. Much needed for the primary school children and the older teenagers. Thank you</p> |
| <p>I have already voted no to a skate park anywhere in Aubin Grove. The suburb would much better benefit from something like the attached image. A multipurpose sports court.</p>  |
| <p>I am a resident at [X], Aubin Grove. In reference to letter received about 2 possible locations for a future skate park in Aubin Grove, neither are appropriate in my opinion and a new survey on a more suitable location should be conducted. The most inappropriate site is the Aubin Grove oval for reasons are as follows.</p> <ul style="list-style-type: none"> •Primary school in immediate vicinity •Youth spaces will attract older children, and typically come with anti-social behaviour. Any advantages of constructing a skate park will be reduced by the new problems you've created because the site is too close to a primary school. Do your research. Local Governments do this (Construct a skate park near a primary school) and time and time again they are having to deploy additional resources (like rangers, police, monitoring, additional cleaning, removing assets like drink fountains or closing toilets because they keep getting vandalised). DO NOT put a skate park in the immediate vicinity of a primary school. •Aubin Grove primary school is an attraction for the suburb – it's been voted the number 1 primary school in the State 2016. Do not destroy any future allocates it may achieve by constructing a youth space in the immediate vicinity. The immediate surroundings should be carefully curated and encourage the right activation. •There are twice as many challenges to the site (Aubin Grove oval) for a reason. Don't over complicate a construction project for the sake of completing one. Review suburb survey for alternative locations that have sufficient site amenities, drainage, transport, passive surveillance etc. If it can't be in Aubin Grove, consider demolishing the Atwell skate park and constructing new OR adding on to it. City of Rockingham has taken this approach on three |



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| <p>sites – demolish and construct – and it was very well received.</p> <p>If you need any further information, feel free to contact me.</p> |
| <p>Good Afternoon,</p> <p>I would like to express my strong support of the proposed skate park in Aubin Grove- at either site. With the growing number of children and young teenagers in the area, it is imperative that they have local access to recreation facilities where they can learn new skills, socialize and be active.</p> <p>I currently drive my son to the Atwell skate park as it's too far to commute by foot- it would be wonderful to simply walk down the road and involve the whole family.</p> <p>Our family would happily be involved if additional community input is required.</p> <p>Warm Regards,</p> |
| <p>As a resident who lives on Camden Blvd and who lives directly across from option "C" of the placement of the Aubin Grove Skate Park i would like to express how much i feel putting the skate park at this oval is a bad idea.</p> <p>I am very much against the skate park being built on the oval especially option C as this is directly in front of my bedroom window and is within steps of my front door.</p> <p>This is a very busy oval already and not only do you get the cars parked along the road for school drop off and pick up but you also have this issue for sports and events held at this oval, there is no need to bring the skate park to such a busy oval.</p> <p>The school kids use this oval for recess and lunch and i am sure if there is a skate park on the oval they will end up using it which could lead to injuries during school hours. Also how will the teacher's be able to police who is at the skate park with who belongs and doesnt belong at the school while the school kids are using the oval.</p> <p>I believe the skate park is a better fit for Radiata Park as you could build a better skate park and the park i already fitted with facilities such as a good playground, bbq facilities, seating and drink fountains.</p> <p>I voted for the skate park for it to be placed at Radiata park as i believe a skate park in the community is a great idea, if i knew there was a chance of it being moved to such a bad spot i would have voted against it.</p> <p>If the Radiata park residents do not want a skate park then why not spend the money on a stand out playground or a community basketball court. There are so many parks in Aubin Grove but none have any stand out playgrounds.</p> |
| <p>Sir/Madam</p> <p>With reference to the above application, I would like submit my objection to such a proposal, especially for the proposed park to be located in Radiata Park.</p> <p>Both proposed location of option A and option C are in very close proximity to residential properties.</p> <p>Location A will have a direct impact on approximately 8 properties and their residents, while Location C having an impact on approximately 4 properties, this is not taking into consideration the indirect impact on all properties surrounding option A</p> <p>A skate park only encourages gatherings of youths, unfortunately older youths in the late</p> |

afternoon/evening, whom are regrettably still at that 'peer pressure' phase with encouraged underage drinking and unsociable behaviour (unfortunately including drug deals, which currently occur at skate parks)

Radiata Park has always been and will always be a very popular park amongst young families with many sitting on the grass and picnicking, as well as playing ball games, particularly around the proposed A location.

As it is, there is a huge strategical issue with school parking. There is very limited parking around the park. Why exacerbate the parking issues?

I'm sorry, but with the very strong possibility of the proposed skate park attracting further unsociable behaviour, removing a current picnic/ball game area, adding to parking issues, direct impact on ALL perimeter properties and course not forgetting to mention the noise factor from the skates/scooters themselves, I need to STRONGLY OBJECT to any such proposal.

However, if the council is insistent on going ahead with this irresponsible idea, why has the far south end of the oval not been considered?

Perhaps consider spending the funds on park upgrades in Aubin Grove, e.g. Camden Park or Bologna Park. Even better yet, how about a secure/fenced in dog park? Or perhaps improving the 'entrance statement' into Aubin Grove, that we were promised approx. 8-9 years ago, rather than being greeted by 'chicken wire' fencing as you enter Aubin Grove from Gibbs Road. We have a beautiful reserve, let's have a nice entrance statement that surrounds the reserve.

Thank you

Dear Parks Manager,

I am writing in regard to the proposed Skate Park for Aubin Grove and the 2 options being considered for its construction – Aubin Grove Oval and Radiata Park. I have 2 thoughts on this proposal and will elaborate on each in turn.

1) In the first instance, I am opposed to the idea of having a skate park all together at Aubin Grove. This decision is partly due to personal experience living near one previously (in NSW) where the escalation of anti-social behavior was evident. There is also enough documented evidence of this occurring, in some instances.

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I would prefer that the substantial funds available be put to better use to enhance the peaceful environment of Radiata Park, make it a haven for peace and tranquility. The Field area in front of the "viewing Deck next to the BBQ area" for example could do with some sprucing up – a proper native garden, for example would be fantastic. Giving the children an opportunity to engage and learn about our environment, attract more birds species etc.

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2) If a skate park is something seriously being considered, then my question is has this been thought through thoroughly? I have seen some amazing skills on display at skate parks, where it has been structured in a way that it is viewed as serious sport, with proper structures and governance in place for safe use. It is a dangerous sport if not conducted appropriately, resulting in serious injuries in some cases. Has this been considered?

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So in conclusion, I am opposed to a skate park in Aubin Grove based on the reasons given above. If however a skate park is to be constructed, then it is more suited at the Aubin Grove Oval (Location C), with strong focus, deliberation and consideration given to zero tolerance of anti-social behavior, appropriate measures put in place to ensure the physical safety of the users of the Skate Park, appropriate security and surveillance provided by the council.

I look forward to a positive outcome.

Best Regards from a concerned long time resident of Aubin Grove.

Cockburn Council

My wife and I have lived at our address in Caribbean Turn for 10 years now and we have watched the park develop and grow to what it is today. The park is used extensively by young and old and is a pleasure to take time out in the park to listen to nature and children playing together. By proposing to build a so called skate park will ruin the tranquillity of the park. We are in total opposition to this skate park for reasons listed below:

Because the houses completely surround the park and are close to the parks perimeter and as we all know sound emits outwards not upwards the impact the noise of the skate park will generate on the surrounding houses will be of great concern.

Because we have lighting around the play areas already in the park this will give free licence to the children to use the skate park after dark and will again cause great concern to the residents around the parks perimeter.

Because of the vast area of open space at the Camden Boulevard Park this area would be a better option if a skate park is indeed required for Aubin Grove.

Regards

Good morning,
 I have already voted for my preferred choice, however, as a resident since Radiata Park was built in 2006, I am opposed to this change of use. The green space you have ear marked is extremely popular with the local community, who play cricket & soccer there most days. Families & community members walk here and join together to play sport. I appreciate you are trying to add more activities for teenagers, however, having had my child grow up a street back from this park (aged 5 to now 20), a skate park has never been a consideration or necessity before. Children love to run and play on the open space, learning to ride their bikes on the grass and playing games. I know that there was a discussion about a half-court basketball court, however, many homes around this area already have basketball hoops and children are playing together on their driveways. I would prefer to see you spend the additional funds you seem to have on maintaining (or supporting residents to maintain) their streetscapes/verges!
 Many community members walk to this park, rather than drive. A new skate park, may increase traffic into the area and we are already having difficulty with drivers speeding down Radiata or Caribbean. The bend at the top of Caribbean is particularly challenging for road users and there is limited parking, should this facility attract members from outside the community. If you insist that we need a Skate Park in Aubin Grove, the green space directly opposite the school or on the oval, has plenty of parking, wider roads and more space to accomodate the increase in activity.
 I do not endorse the skate park at Radiata Park, Aubin Grove. Please could you make sure my opinion is passed to the relevant person responsible for this consultation process.
 Kind regards,

To Whom it may concern,

Firstly I would like to express my frustration on receiving no information regarding the up and coming proposal for a stake park at Radiata park, I only found out from a neighbour who also is aghasd at the idea.

Reasons for not having a stake park on our doorstep at Radiata Park.

- Anti Social behaviours.
- Graffiti.
- Removal of recreational area where people have lots of picnics and play sport.
- Making an area for teenagers to congregate late into the night.
- Bringing lots of noise and disruption to a very quite area, especially for those right opposite the park.
- Lots of littering by people who just come in the area and have no common decency.
- No toilet facilitate causing potential hygiene issues and our area has lots of young children who use our beautiful park, for which a Skate Park will be no benefit.
- Not enough parking facilitates to accommodate an influx of cars and people into our area.
- Upsetting our beautiful wildlife that habitat in our park.

We have lived in this area for five years with little or no issues what so ever with young children currently enjoying the park facilitates as they CURRENTLY are. In my opinion this would not be a suitable location for a skate park and I am vehemently against the proposal.

As a council tax paying home owner I would appreciate regular updates from now on regarding the proposal of this skate park at Radiata park and its proposed location, as I feel it would EXTREMELY detrimental to our beautiful area and quite suburb.

Warm Regards,

With regards to the proposed skate park, particularly at Radiata Park, i would like to express my ABSOLUTE REJECTION to such a ludicrously proposed idea, right in the middle of residential properties.



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| <p>Please re-think your proposal on how to spend the funds rather than on an unsightly, noisy and unsociable behaviour attracting amenity. Use the funds to upgrade parks, install a nature playground, a dog park, install a basketball court or improve the entrance statement into Aubin Grove rather than the unsightly wire fencing along the reserve . Thanks you</p> |
| <p>> Wonderful ! > I vote > Aubin grove oval 🇳🇿</p> |
| <p>Hi, Our family of 4 are residents of Aubin Grove. We support the development of a skate park in Aubin Grove. All locations seem suitable, but it appears that Location C would allow for a larger and better facility - so that would be preferable. There are a lot of young children in Aubin Grove and a facility such as this is greatly needed. Please consider installing CCTV to limit anti-social behaviour and strengthen law and order protections. This would be a great development - our 12 year old son couldn't be more excited by the possibility of this happening. Well done CoC.</p> |
| <p>I believe that the community of Aubin Grove rejected the idea of a Skate Park in the suburb a year or 2 ago. The idea was floated, and the Aubin Grove Community Association reported that it was NOT supported by the local community.</p> <p>I would like to submit my opinion that a skate park is NOT welcome in this suburb. Residential sites are FAR TOO CLOSE to both proposed areas. Yes, we need something for the older kids... but we DO NOT NEED any more areas to invite unsolicited activity.. I believe that is what will happen with a skate park.</p> <p>Rather, an activity zone for families with flying foxes or public facilities more targeted at young teenagers would be beneficial in the suburb as there currently are not good facilities for them...</p> <p>I believe it would be wiser and more beneficial to the community to expand the current sport and community facilities - even add an extension to the current building... to accommodate after school activities and clubs... currently it only holds a small capacity. We are fortunate to be close enough to Atwell to utilise those skate park facilities if need be.</p> <p>We were promised a half basketball court in Radiata Park, and that hasn't come to fruition yet... maybe deliver on some of the things already promised... then see what the suburb can benefit from.</p> |
| <p>Hi, I would just like to express that my family and I who live on a neighbouring street of Radiata Park (Monticola Gardens) are strongly against the proposed skate park.</p> <p>We feel that the funds would be put to much greater use, given there is a skatepark in Atwell that is close by. We would love to see a nature playground or something similar considered as it is something that our community is lacking - and creates a family friendly environment.</p> <p>Some other concerns with the skatepark, particularly in this location (however we would prefer to not see one at all) are:</p> <ul style="list-style-type: none"> • There are lots of residents in close proximity on all sides and it attracts undesirable behaviour |

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| <ul style="list-style-type: none"> • There are limited open spaces of that size this size of Lyon road for kids to run around / play games • Very limited parking around the outside of the park <p>We were hoping as Lara had mentioned to us previously, that some more brainstorming of the funds could be done to provide options to the community.</p> <p>Many thanks for your time and consideration</p> |
| <p>Hi,</p> <p>My husband [X] and I live at no [X] Radiata Promenade in Aubin Grove and absolutely love it here. The thought of a skate park on our beautiful park is abhorrent and so unfair for so many reasons - as listed below:</p> <p>Toilets. A huge issue. Problems always arise around public toilets. They attract the not so nice element of the community and this will not just be in the daytime. To date we have always felt safe here but this will change if public toilets are installed. The school oval already has toilets there. If the skate park does go ahead on Radiata can you categorically say that there will NEVER be any toilets there?</p> <p>Parking. Another huge issue. There are very limited bays around the park. These are used by families with very young children who wish to play on the park. What would happen, for instance, on a skate competition day? We will be surrounded by cars parking all over the place. I envisage many arguments occurring.</p> <p>Will food and drink vans be part of the equation?</p> <p>Graffiti - just look at Atwell.</p> <p>Hooners - we have enough of these already. I requested speed bumps on our roundabout but was told by the council that they weren't necessary. This will be more of a problem should the skate park here go ahead.</p> <p>This is a FAMILY park. Families with small children play here. They run, scoot, cycle around safely. The skate park will take away a lot of their space as well as making it dangerous for many of the little children.</p> <p>Floodlights. Will these be installed? Again - these will attract an unsavoury element.</p> <p>Will there be a curfew? If the skate park is allowed to be used at any time how fair is that on the residents? Will there be a Ranger monitoring this?</p> <p>The school oval, with its space, its parking and its toilets, among other things, is clearly the place for the Aubin Grove Skate Park if it should go ahead (although the cynical side of me tells me that you have already made the decision and you are just paying us lip service?).</p> <p>Will there be a reduction in our rates for the inconvenience that is obviously going to occur should Radiata Park become the designated site?</p> <p>Should a skate park project receive support we fervently hope you do the right thing by us and place it on the school oval - it is a no brainer for many reasons.</p> |
| <ul style="list-style-type: none"> • Do not support skate park • Prefer to see funding invested in alternative use for the "tween/teen" audience: <p>Playground (but designed for older kids, North Coogee for example, table tennis tables and four square courts)</p> <p>Half Basketball Court</p> <p>Toilets at Radiata Park</p> <p>Lighting could be an option.</p> <p>Comments:</p> <p>Has worked as a teacher at a school, where there was a skate park built adjacent. The skate park did not keep the kids busy and they caused damage to school instead. Feel this could happen here too.</p> |
| <p>To whom it may concern,</p> <p>I am writing to provide my input into the proposed skatepark for Radiata Park in Aubin Grove. I strongly oppose the proposed budget allocation towards a skate park.</p> |



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| <p>Radiata Park is already one of the busiest parks in the area full of young children from the primary school and local area. If the budget needs to be allocated to this particular park I believe that upgrades to the playground or more nature based play equipment would be more suitable for the local community.</p> <p>There is already a skate park in Atwell that can be utilised if desired. There however are no nature based playgrounds local to Aubin Grove that are much preferred by primary school children.</p> <p>To cater for older local children the addition of basketball hoops or climbing nets would be preferred as these would not have a negative impact on residents surrounding the park.</p> |
| <p>I am writing in response to letter received about a proposed Skate Park being built in Aubin Grove Oval. I hereby oppose this move and do not wish to see this go through. Thank you</p> |
| <p>[X] are very much against a skate park being built in the oval. We already don't have enough space for training and barley enough pitches to host games.</p> |
| <p>A majority of the community absolutely do NOT want a skate park in Aubin Grove! There is enough crime and graffiti in the area as it is! I am also concerned about noise pollution (not just at the park but in the general area with skate boards going past) Im pretty sure I have replied to a survey which resulted in an overwhelming NO for this idea do you lot not know how to take no for an answer. Also if the council is swimming in money at the moment it might be an idea to reduce the ridiculous amount we for rates because i can tell you that not everyone has money to burn like you lot. Everyone else is taking a hair cut at the moment, its about time you lot do as well. Yours faithfully</p> |
| <p>TO WHOM IT MAY CONCERN Thank you for your letter dated 14th May, 2020 in regards to supporting a new skate park in Aubin Grove. We personally feel that it is inappropriate to establish a Skate Park in Radiata Park, therefore our vote is No. We suggest Aubin Grove Oval is much more appropriate considering it has more space & it is near the Primary School. We thank you for notifying us on this upcoming project & appreciate it if you would keep us informed on the latest developments on this. Kindest Regards,</p> |
| <p>Hi there, I would like to vote for the skate park to be at Radiator Park. As many sports programs happen at aubin grove reserve it would impact on those games and training days significantly. Radiator Park has a large grassed area and playground too it would make it a better play and outdoor area...</p> |
| <p>No skate park at Aubin Grove local park. Thank you.</p> |
| <p>Hi I would like to have comment on the proposed skate park Firstly I am excited about the prospect of a skate park my son (16yrs) has complained about this for a long time now we get to see it come to fruition. I would really love to see it built in Radiata Park, the fact that there are lesser site constraints existing amenities in place and that it is further away from the primary school is really good. Older kids such as my son prefer to be in the company of their own age and friend groups. Aubin grove oval is regularly used by numerous local sports groups/ the school/ and has a child care and smaller kids playground near by. Building a skate park at Aubin grove oval will significantly reduce availability of parking which at many times now is extremely limited. Also having the prospect of more kids in this vicinity is particularly scary and very concerning given that when school finishes this area and the surrounding streets are filled</p> |

| |
|--|
| <p>with people picking up their kids. The cars are parked along Camden boulevard making it sometimes difficult to drive past. To draw more people and more kids to this area may be a particular issue in the case of option C. Radiata Park in my families opinion is safer to park, a bit closer to bus services, has existing infrastructure such as drinking water BBQ and seating, it may facilitate a skate park full size or less risk of being scaled down and is a more family oriented environment a bit quieter. Local kids can use it making it their own. Please build it in Radiata Park. (Plaza style) Thank you very much</p> |
| <p>Yes a fantastic idea! Radiata Park would be the best location and seems to have the best benefits. Thanks</p> |
| <p>Definitely against seeing a skate park at Radiata Park. To close to houses. Park not big enough as many younger children play football ect on the open space. Also lots of toddler, preschoolers run around. Feel it would impact on the people who daily frequent the park. Feel it can also attract undesirables which would be a shame. Our suburb is a beautiful, safe place for young and old. Would not like to see this change.</p> |
| <p>Hello, Providing a skate park is a good idea for the young of this area however, the choice of it's location is the concern? Radiata Park is too small and built in, the parking and youthful noise when busy would upset this quiet setting-more complaints from residents. There is more available suitable land next to the School and/or behind Belmore Bend where another road from Gaebler road could relieve traffic from already busy roads on and around Lyon Road, this road could also connect the skate park to Kinley or Triandra Court from the South. Regards,</p> |
| <p>Hi, i vote for location in Radiata Park. thank you</p> |
| <p>Dear Sir or Madam, I strongly Believe that a skatepark in Aubin Grove is a amazing idea. A skatepark is so much fun and I'm sure that it will be joy to many people across Aubin Grove and other places. The closest Skatepark is all the way in Atwell witch is hard to get to by foot. Having a skatepark will boost the economy and attract lots of Teens and people of all ages. Kind Regards</p> |
| <p>Good Afternoon My Name is [X]. I am a resident of Aubin Grove for over twelve years at [X] Radiata Promenade. I live here with my wife [X] and our three 3 year old son [X]. I am writing you to express our vehement opposition and disappointment to a proposed Skate Park and Toilet Facility at Radiata Park. We are still disappointed that since 2018's Comment on Cockburn draft for the next 15 years, that there has been zero engagement of those residents directly surrounding Radiata Park. What happened to the \$50k budgeted last year to engage residents? We have simply been told to choose between an updated location option that now impacts new residents and sporting club members of Aubin Grove Oval. Naturally people will be voting for a location that least impacts their interests, you've effectively managed to pit residents against one another to choose a location that bothers them least. I have door knocked approximately 150 residents over the long weekend. There were still many residents completely unaware of the proposal. They are now. You can thank me for doing your job later. Our opposition, including my neighbours is the destruction of our views across the park and its family friendly BBQ's, picnics and current sport usage by children both after school and on weekends. We are concerned about the effect this change will have on property values,</p> |



increased traffic, noise, theft and antisocial behaviours.

My family purchased the first block in "The Walk" estate 15 years ago. We were seeking a large block opposite green, open space, such as Radiata Park. At that time, there were not even roads in this estate, it was still bush from the old semi-rural lots of the area. We purchased our block from developer LWP as per their prospectus. We never anticipated our vista to the park would become a concrete monolith skatepark accompanied by a toilet block. It is enjoyable to see families and children daily using the park, the kids playing cricket using the huge Radiata Pines opposite our house as wickets, the laughter and children's birthdays on weekends in the nice weather by the BBQ's. Radiata Park is surrounded by houses that enjoy a view over the green and open space, we paid a premium for blocks opposite the park to enjoy this. We are happy the park is enjoyed by all from Aubin Grove but feel the Skate Park proposal has not considered those living opposite.

Already, I have researched anecdotal experiences of residents in proximity of skate parks across Australia that had raised concerns similar to ours above, that were ultimately assessed as being manageable by their local councils and in the broader communities interest to build a skate park, thereafter having created numerous complaints once the skatepark was completed and utilised. None of these skateparks were as close as the Radiata Park proposed skate park is to Radiata Promenade residents. The two closest Skate Parks to Aubin Grove exist on large reserves more than 100 metres away from houses, namely Atwell and Bibra Lake. Almost every other skate park in metropolitan Perth is appropriately built more than 50 metres from houses. Radiata Park is 30 metres from the front of our house.

The council's own guiding principles around community engagement state "The community are to be consulted with and engaged to ensure that the provision of community and sports facilities meet the needs of the community." I don't ever recall anyone asking for a skatepark opposite housing in my 12 years paying City of Cockburn rates.

DO NOT build a skate park at Radiata Park Aubin Grove.

Kind regards

1.1.3 Demographics

Participant demographics are captured through the Comment on Cockburn platform and therefore the following statistics are representative of the survey participants.

Demographic data highlighted significantly more female respondents than male (64% compared to 32%, four per cent unverified). Whilst females were more represented, there were no significant differences in the sentiments of each group.

All age groups were represented in the respondent data, however the 35-44 bracket more so than others, representing 48% of survey respondents. The 35-44 age group were largely supportive of a skate park (82%).

18-24 made up 2.5% of the respondents and had a similar level of sentiment to the 35-44 age group.

Removing outliers, the skate park was most supported amongst the 17 or younger demographic.

All but one member of this group support a skate park. The one respondent from the 17 and younger demographic who does not support a skate park is three years of age however and it is therefore questionable if this outlier is an accurate representative.

The 17 and under age group is less represented in the overall data, making up approximately two per cent of survey respondents.

The youth demographic are underrepresented in the feedback. The limitations associated with Covid-19 made it particularly challenging to engage and seek feedback from this group. More opportunities for this demographic to have their voice heard would be desirable.

The remaining participants mostly fell in the 25-34 and 45-54 age ranges, with 19% and 16% of respondents respectively. 55 and over represented around 7%.

The sentiment of these remaining age groups (25-34, 45-54, 55-64 and 65-74) was similar and relatively consistent with between 55 – 62% of participants in support.

The vast majority of survey respondents (76%) live in Aubin Grove. Considering only the Aubin Grove data, approximately 60% of respondents support the skate park, the most popular location of all the potential sites is Radiata Park (38%).

Of those that do not live in Aubin Grove, 76% are supportive of a skate park in Aubin Grove, with just over half of those respondents preferring Radiata Park and the remaining half split almost evenly between the two Aubin Grove positions.

The suburb data collected via Comment on Cockburn is provided below.





Appendix A – Survey

| | | |
|--|--|---|
| | <p>Opportunities</p> <ul style="list-style-type: none"> Good passive surveillance Centrally located/accessible Existing amenities (e.g. BBQ, drink fountain, seating) Perimeter car bays 50+m from houses Suitable for plaza style skate park | <p>Challenges</p> <ul style="list-style-type: none"> No toilet 200m to bus stop Minimal parking |
| | <p>Opportunities</p> <ul style="list-style-type: none"> Next to school and community building Large car park 50+m from houses (Option B) Suitable for single layer style skate park | <p>Challenges</p> <ul style="list-style-type: none"> Risk of scaled down skate park (site constraints) Proximity to sporting pitches Limited passive surveillance 300m to bus stop Lack of amenities (e.g. BBQ and drink fountain) Less than 50+m from houses (Option C) |

- Do you support the inclusion of a skate park in Aubin Grove?
- Yes, at Radiata Park (Location A)
 - Yes, at Aubin Grove Oval (Location B)
 - Yes, at Aubin Grove Oval (Location C)
 - No, I would prefer something else

(Conditional – Answered “No” above) If a skate park is not supported by the community, the City will need to seek approval from the Western Australian Planning Commission to reallocate the funds. The funds can only be allocated to Parks within the suburb of Aubin Grove, in accordance with the expenditure conditions outlined in the Planning and Development Act 2005.

Should you not support a skate park at Radiata Park or Aubin Grove Oval, **please state a park in Aubin Grove that requires improvements and what could be allocated from the Cash-In-Lieu expenditure program:**

- Colorado Park
- Durango Park
- Edulis Park
- Radiata Park
- Aubin Grove Oval
- Camden Park
- Versaille Park



Balboa Park
Monticola Park (sometimes referred to as El Mitz Park)
Tange Park
Pearl Flower Park
Bologna Park
Princeton Park
Observatory Park
Banksia Eucalypt Woodland Park
None of the Above

Grass / landscaping / reticulation
Seating / shelter / spectator cover
Toilets and change rooms
Play equipment
Playground shade sail
BBQs
Gym equipment
Basketball half courts
Pathways and walk trails
Fencing
Car parking
Signs relating to recreation pursuits
Lighting
None of the above

Please Tell Us About You

If the community support a skate park, we'll need your help to design it. Please enter your email address below so we can get in touch with you.

Age Group:
Primary School
High School / Graduate
20s
30s
40s
50s
60s
70s +

Gender:
Male
Female
Other

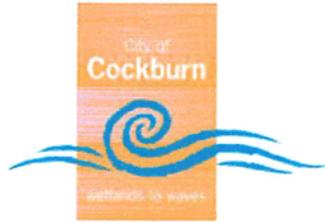
Suburb:
Street Name:



City of Cockburn

9 Coleville Crescent, Spearwood WA 6193
PO Box 1215, Bibra Lake DC Western Australia 6965
T: 08 9411 3444 F: 08 9411 3333
E: comment@cockburn.wa.gov.au
cockburn.wa.gov.au





PETITION FORM

To: Chief Executive Officer
City of Cockburn
9 Coleville Crescent
SPEARWOOD WA 6163

PO Box 1215
BIBRA LAKE WA DC 6965

We, the undersigned electors of the City of Cockburn, request the Council abandons its agenda to build a Skate Park at ANY location in Aubin Grove, including those formally nominated (Radiata Park Aubin Grove or Aubin Grove Oval)

To date, there has been no direct engagement or consultation by the City of Cockburn with Aubin Grove residents with respect to their intention to build. We have simply been told that the City of Cockburn has identified a need for a skate park in Aubin Grove, and for us to "pick a location". We are very disappointed with the way in which the council has attempted to conduct its "surveys" given the heavy impact this proposed development would have on residents, especially those surrounding Radiata Park. There are still many locals who are simply unaware of this proposal.

The residents and sporting clubs directly affected by either of the proposed locations do not wish to be forced to vote against each other by preferencing one location over the other. Collectively we hereby request that **NO skate park be built in ANY location in Aubin Grove.**

We request the existing funding be redirected to alternative initiatives or to supplement existing repair and maintenance budgets to improve the existing parks and their infrastructure in Aubin Grove. Radiata Park is now approximately 15 years old and much of the current infrastructure is worn needing replacing/upgrading.

For the following reasons we oppose the skate park proposal at Radiata Park:

Radiata Park is a family friendly park and is heavily utilised for many community activities, including:

- BBQ's and picnics
 - Birthday parties,
 - Community sport and play (mainly cricket, football and soccer), which requires large areas of green space
 - Small children activities, such as scooting and walking with family and friends.
 - Family bike riding,
 - Community fitness groups and meditative activities and dog walking
 - Parent groups (mainly associated with Aubin Grove primary) meeting before and after school.
1. Christmas time and holidays are a particularly busy time for the park with local community organisations (including church groups) using the space to engage with the community and providing the important social connectivity essential to healthy neighbourhoods. This proposal will radically change both the ambience and dynamic of the area, and will exclude and deter the current users as they are forced to seek more family friendly locations.
 2. The negative effect on property values, negative visual impact, noise and antisocial behaviours, along with the loss of sense of security and peace in one's own home are typical of this type of facility.

Graffiti, vandalism, burglary, theft and littering also go hand in hand with a facility of this nature and the council cannot ignore the fact that the burden of "passive surveillance" will fall unfairly on those residents living closest to the skate park.

3. Existing skate parks in The City of Cockburn are located well over 100 metres to the nearest residential property e.g. Atwell and Bibra Lake. Radiata Park is within 30 metres to many houses.
4. The potential increase in traffic and the failure to date by the council to provide vehicle speed calming devices for Radiata Promenade (despite the many requests to have them installed), will only add to our current concerns around pedestrian and child safety.

We believe the proposed destruction of what is already limited green open space adjacent to houses in Aubin Grove is counter to the council's priority to provide such environments for the health and wellbeing of our citizens, and as such we STRONGLY reject the council proposal for a skate park in Aubin Grove.

As the initiator of this petition, my name is Brendan Sobczak.
For further information I can be contacted via email, brendansobczak@yahoo.com.au or by telephone 0413144232.





| Name | Address | Signature |
|-----------------|----------------------------|-------------|
| J. SPRINGER | 20 CARIBBEAN TURN | [Signature] |
| M. DUNCOMBE | 14 CARIBBEAN TURN | [Signature] |
| S HARDING | 1 Radiata Prom | [Signature] |
| D. Olsen | 6 Caribbean Turn | [Signature] |
| M. MARRIOTT | 13 RADIATA PROM | [Signature] |
| J. WILSON | 2 RADIATA PROM | [Signature] |
| A. Wilson | 2 Radiata Prom | [Signature] |
| J. Rowles | 3 Aleppo street | [Signature] |
| Donna Pean | 8 Weymouth Rd | [Signature] |
| Gillian Wallace | 19 Pinella Parkway | [Signature] |
| S Piton | 11 Cape le Grand Ave | [Signature] |
| R. Parkson | 6 Radiata prom | [Signature] |
| V. Pearsors | 6 RADIATA PROM | [Signature] |
| G. BROWNIE | 4 RADIATA PROM | [Signature] |
| A. MACNATH | 286 LYON ROAD | [Signature] |
| D. Staudam | 286 Lyon Road | [Signature] |
| Amey Key | 4 Chile Way Aubin Grove | [Signature] |
| J. Allegretta | 4 Chile Way, Aubin Grove | [Signature] |
| A. Donchy | 7 WEYMOUTH RD | [Signature] |
| L. MINGER | 3 Weymouth rd, Aubin Grove | [Signature] |
| M Hungerf | 3 Weymouth rd Aubin Grove | [Signature] |
| M Bell | 6 WEYMOUTH RD, Aubin Grove | [Signature] |
| Justin Belle | 6 Weymouth rd Aubin Grove | [Signature] |
| Tina Van | 10 Caribbean turn | [Signature] |
| AMBLER | 4 Aleppo St Aubin Grove | [Signature] |

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| Name | Address | Signature |
|----------------------|---------------------------------|-------------|
| Sally Piotrowski | 294 Lyon Road, Aubin Grove | [Signature] |
| Martin Jones | 7 RADIATA PROM AUBIN GROVE | [Signature] |
| WALT PATTYNAMA | 4 EDULIS AV. AUBIN GROVE | [Signature] |
| THERESA WILSON | 2 RADIATA PROM AUBIN GROVE | [Signature] |
| Douglas Walters | 5 Monticola Gardens | [Signature] |
| MATT LONGDEN | 15 LONGWOOD BEND AUBIN GROVE | [Signature] |
| PAT JONES | 7 RADIATA PROM, AUBIN GROVE | [Signature] |
| Joan Springer | 20 CARIBBEAN COURT, Aubin Grove | [Signature] |
| Michelle Spaczak | 9 RADIATA PROMENADE AUBIN GROVE | [Signature] |
| BRENAN SZCZAK | 9 RADIATA PROM. AUBIN GROVE | [Signature] |
| STEVE BARNES | 6 ALEPPO STREET AUBIN G | [Signature] |
| EMMA BARNFORD | 6 ALEPPO STREET AUBIN G | [Signature] |
| Kirsty Lindsay | 5 Hidcote Turn Aubin Grove | [Signature] |
| Chad. Penahene | 5 Hidcote Turn Aubin Grove | [Signature] |
| Carl Fletcher | 12 Magnolia way, warrick | [Signature] |
| Andrew Rowson | 3 ALEPPO STREET AG | [Signature] |
| IAN OWEN | 18 CARIBBEAN TURN AUBIN GROVE | [Signature] |
| EMMA PARR | 22 CARIBBEAN TURN, Aubin Grove | [Signature] |
| Nicola Connolly | 22 Caribbean Turn | [Signature] |
| Brandon Lipp | 24 Cornelian Turn | [Signature] |
| Dr. Ruben Raja | 1 Monticola Gardens | [Signature] |
| Dr. Shy. Vishnubalan | 1 Monticola Gardens. | [Signature] |
| Giacomo Bevacqua | 1 Mugo lane | [Signature] |
| Samantha Bevacqua | 1 Mugo lane | [Signature] |
| Joan Kongy | 32 Cornell Way, Aubin Grove. | [Signature] |

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| Name | Address | Signature |
|---------------------------------------|---------------------------------------|-------------|
| Limmer | Carol & George Limmer 13 MYGO LANE | [Signature] |
| Margaret Hoogeweg Richard Hoogeweg | 21 RADIATA PROMENADE AR ✓ ✓ ✓ ✓ | [Signature] |
| Andy Puzic Sandra Puzic | 18 Cabroochi Pde | [Signature] |
| Ben Hobbs | 11 RADIATA PROM | [Signature] |
| Kira Harris | 11 Radiata Prom | [Signature] |
| Renee Carr | 3 Armand Drive Aubin Grove | [Signature] |
| Glenn Carr | 3 Armand Drv Aubin Grove | [Signature] |
| HOLLY KELD | 4 Armand Dr. Aubin Grove | [Signature] |
| Michael Keld | 4 ARMAND DR " " | [Signature] |
| Goichi Oya | 6 Armand Drive Aubin Grove | [Signature] |
| David Shortt | 8 Armand Drive Aubin Grove | [Signature] |
| Kyriel Donaldson | 12 ARMAND DRIVE AUBIN GROVE | [Signature] |
| Tavis Donaldson | 12 ARMAND DRIVE AUBIN GROVE | [Signature] |
| Clint Robson | 14 ARMAND DRIVE AUBIN GROVE | [Signature] |
| JUSTIN ABEYED | 17 ARMAND DRIVE AUBIN GROVE | [Signature] |
| NEIL SMITH | 22 ARMAND DRIVE AUBIN GROVE | [Signature] |
| Shibing Cao | 23 Armand Drive Aubin Grove | [Signature] |
| EWAN ROBERTS | 26 ARMAND DRIVE | [Signature] |
| KETIA KENS | 27 ARMAND DRIVE | [Signature] |
| MARK BAUFEX | 29 ARMAND DRIVE AUBIN GROVE | [Signature] |
| DARYL PUYALES | 32 ARMAND DR AUBIN GROVE | [Signature] |
| Sohn Foti | 10 Aristata Promade Aubin Grove | [Signature] |

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City of Cockburn - Peninsula Parks

Document Set ID: 7281645
 Version: 1, Version Date: 27/02/2018
 Document Set ID: 9415367
 Version: 1, Version Date: 05/06/2020



| Name | Address | Signature |
|-----------------|---|-------------|
| Gail Tomich | 6 Aristata pde | [Signature] |
| DAVID TOMICH | 6 ARISTATA PDE | [Signature] |
| Haixing Huang | 5 Spruce Terrace, # | [Signature] |
| Saptak Kulkarni | 5 Radiata Prom | [Signature] |
| NAVDEEP SINGH | 64 DURANGO TURN AUBINGROVE | [Signature] |
| Maren | 314 LYON Rd Aubin Grove | [Signature] |
| Colin MacLure | 316 LYON Rd Aubin Grove | [Signature] |
| Dio | 318 LYON RD Aubin Grove | [Signature] |
| Gregory Tay | 320 LYON RD AUBIN GROVE | [Signature] |
| Paul Cooke | 322 LYON RD " " | [Signature] |
| Jannell | 304 LYON ROAD AUBIN GROVE | [Signature] |
| CONNOR | 320 LYON RD, AUBIN GROVE | [Signature] |
| DEEPAK | 2 ARISTATA PDE | [Signature] |
| VILDARD | 4 ARISTATA PARADES | [Signature] |
| Elizabeth Sia | 26 Edulis Avenue | [Signature] |
| Damian Weary | 23 Edulis Ave A Grove | [Signature] |
| Susan Weary | 23 Edulis AVENUE ^{Aubin Grove} | [Signature] |
| SCOTT HAIGH | 19 EDULIS AVENUE AUBIN GROVE | [Signature] |
| Trish Cardoso | 19 Edulis Ave A.Grove | [Signature] |
| Lawren Hollings | 17 EDULIS AVE A' GROVE | [Signature] |
| SUKH CHANAL | 13 EDULIS AVE A U A GROVE | [Signature] |
| Raman Gill | 11 Ponderosa Road A'Grove | [Signature] |
| Kendrick Chan | 9 Edulis Ave, Aubin Grove | [Signature] |
| Aimee Hills | 2 Pitch way Aubin Grove | [Signature] |
| Kylie Elwin | 6 Pitch Way Aubin Grove | [Signature] |

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City of Cockburn - Planning Form

Document Set ID: 7281645
 Version: 1, Version Date: 27/02/2018
 Document Set ID: 9415367
 Version: 1, Version Date: 05/06/2020



16.2 (2020/MINUTE NO 0196) CLOSURE OF KNOCK PLACE AND HUTT LINK**Author(s)** J Kiurski**Attachments**

1. Scope of Proposed Works A and B
2. MRWA Advertisement

RECOMMENDATION

That Council, pursuant to Section 3.50 of the *Local Government Act 1995*, closes Knock Place between Solomon Road and Kwinana Freeway, and Hutt Link, subject to Main Roads WA and Armadale Access Alliance:

- (1) engaging an appropriately accredited traffic management contractor to monitor and control traffic movement due to the closure;
- (2) ensuring all works on existing City infrastructure (roads, footpaths, drainage, parks or verges) is completed and reinstated in accordance with the Public Utilities Code of Practice 2000, Restoration and Reinstatement Specification for Local Government 2002, and the City of Cockburn Excavation Reinstatement Standards 2002, as a minimum; and
- (3) being fully responsible for all legal costs, the cost of the valuation, public liability, and damages arising from the works.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0**Background**

The subject site of this report is Knock Place between Solomon Road and Kwinana Freeway and Hutt Link as shown in Image 1, which is an extract from the traffic management report by Armadale Access Alliance. Note: all affected roads are within the City of Cockburn boundary.



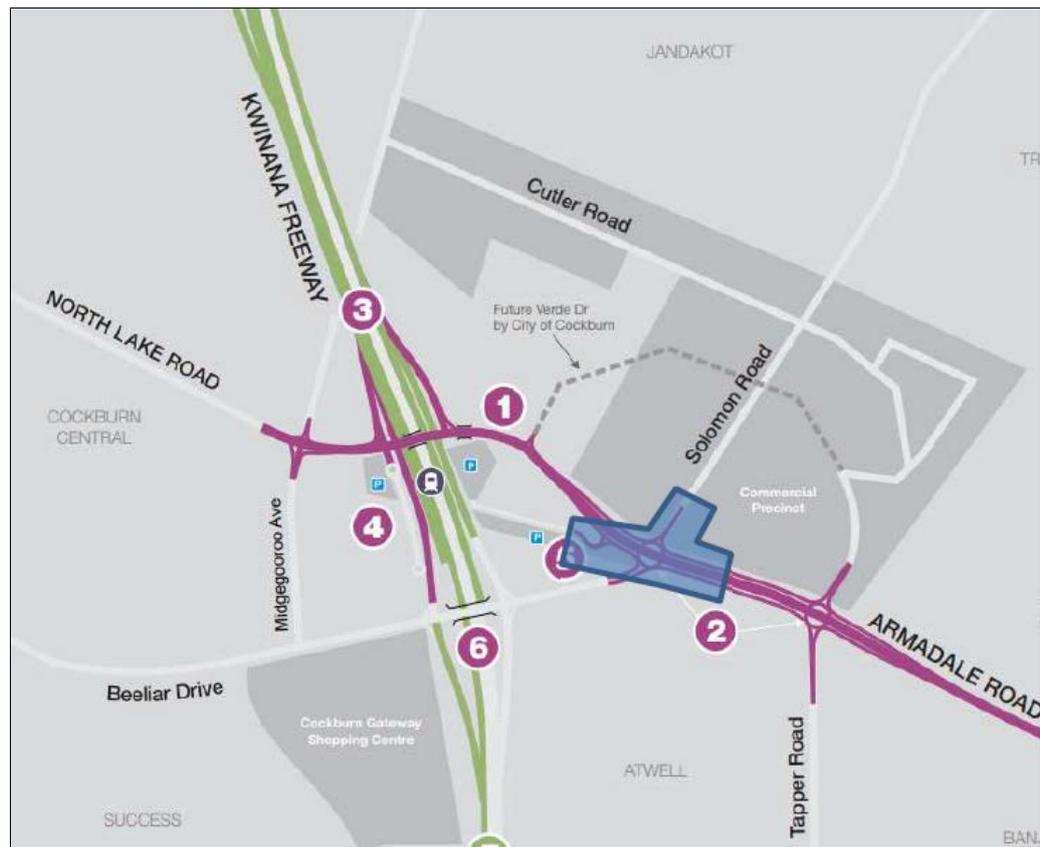


Image 1: Map of Works Area

Submission

MRWA and the Armadale Access Alliance (AAA), construction contractor of the Armadale Road to North Lake Road Bridge project, have requested Council to implement procedures for the closure of Knock Place between Solomon Road and Kwinana Freeway, and Hutt Link. A number of traffic management arrangements will be in place on the local road network to accommodate this closure.

Report

The Armadale Access Alliance submitted the scope of work which includes closure of Knock Place between Solomon Road and Kwinana Freeway, and Hutt Link to facilitate construction of the new Armadale Road / Solomon Road / Beeliar Drive grade separated intersection, as well as the opening of Verde Drive and upgrade of the PTA parking facilities. The scope of the proposed work is provided in (refer Attachment 1) for reference, which is an extract from the complete traffic management report by AAA.

There are significant works associated with closure of Knock Place and will be completed in two stages, concurrently. The staging includes:

- Closure of Knock Place between Solomon Road and Kwinana Freeway and subsequent closure of Hutt Link
- Solomon Road and Armadale Road realignment.



The Stage 1 works are shown in Image 2, which is an extract from the traffic management report by AAA. Knock Place closure is marked in red and proposed to commence 12 September 2020. Hutt Link and the remainder of Knock Place will be closed in October 2020 following construction of the replacement roads. Upon closure of Knock Place, access to the PTA carpark will be via Verde Drive shown in the dark blue lines below.

The construction work area is marked in orange.

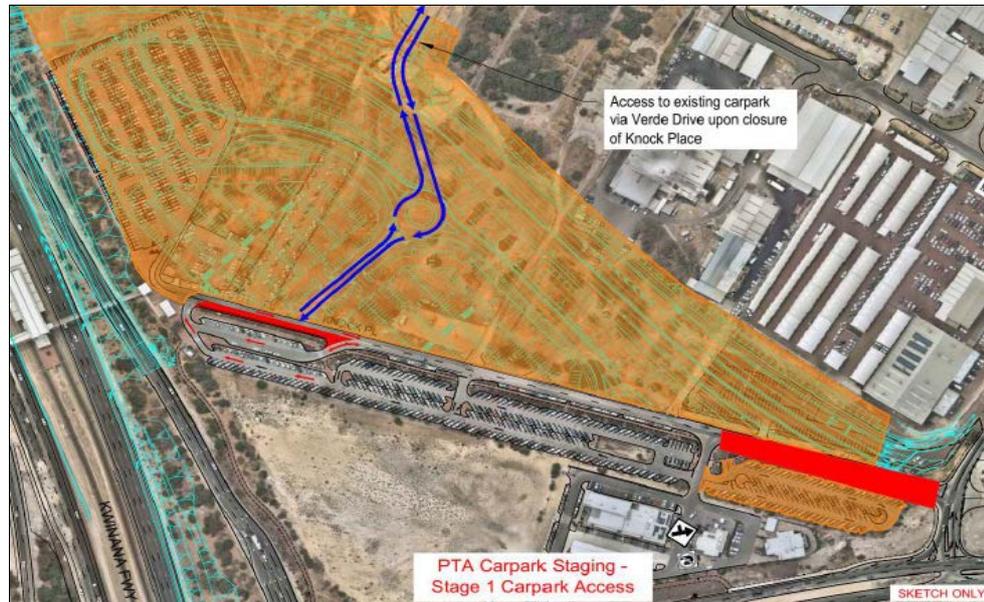


Image 2: Knock Place closure

The Stage 2 works are shown in Image 3, which involves the following long-term temporary road alignment:

- Solomon Road will be diverted around the north-east quadrant of the new grade separated intersection.
- Armadale/Solomon Road intersection will become a left-in/left-out arrangement. Road users wishing to make a right turn onto Armadale Road will follow the detours to make a U-turn at the Armadale/Tapper/Verde temporary roundabout. Road users wishing to make a right turn onto Solomon will follow the detours onto Verde Drive.
- Armadale Road will be realigned towards the southern noise wall between Freshwater Drive and the new temporary Armadale/Solomon intersection.
- A temporary pedestrian path will be provided to maintain continuity of the pedestrian and cycling network along the northern verge of Armadale Road, connecting the existing pedestrian facilities at Armadale/Freshwater to the temporary path provided in the Armadale Road Stage 2 switch (ARNLRP-AAA-MPL-0000-TR-0015).

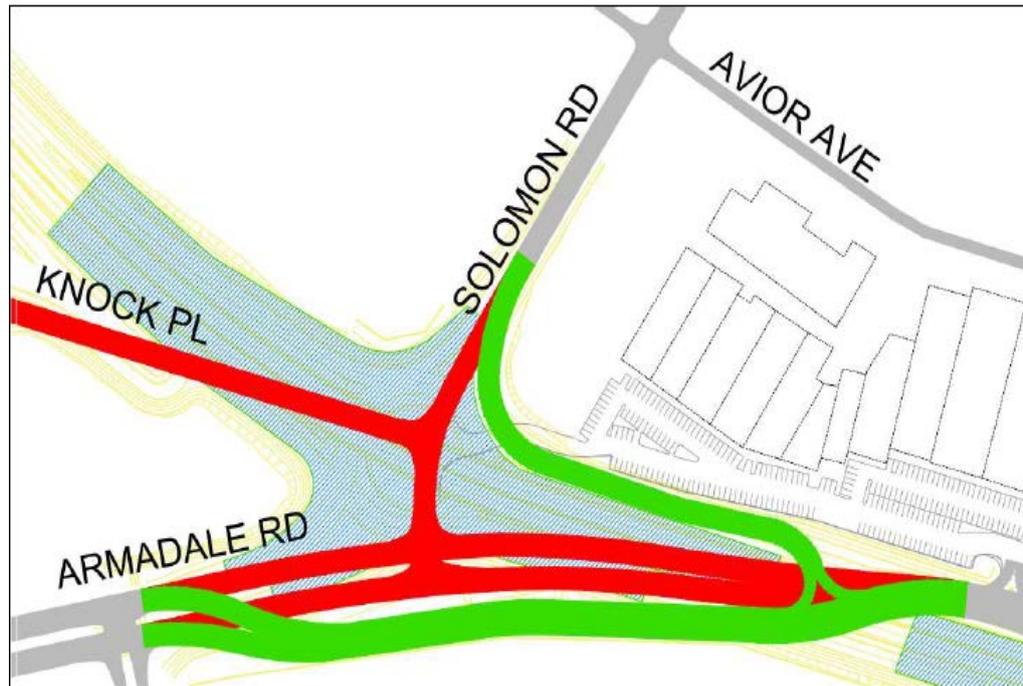


Image 3: Armadale Road / Solomon Road realignment

The map in Image 4 shows the traffic detour during implementation of Stages 1 and 2. Proposed timeframes for affected roads are as follows:

- Knock Place closure from September 2020
- Solomon Road traffic change September 2020 to mid-2021
 - No right turn from Armadale Road
 - Left turn only from Solomon Road onto Armadale Road
- Hutt Link closure from October 2020
- Commercial precinct
 - Access unchanged from Armadale Road
 - October 2020 replacement road for the Hutt Link closure accessible from the north, see purple arrows in detour map.



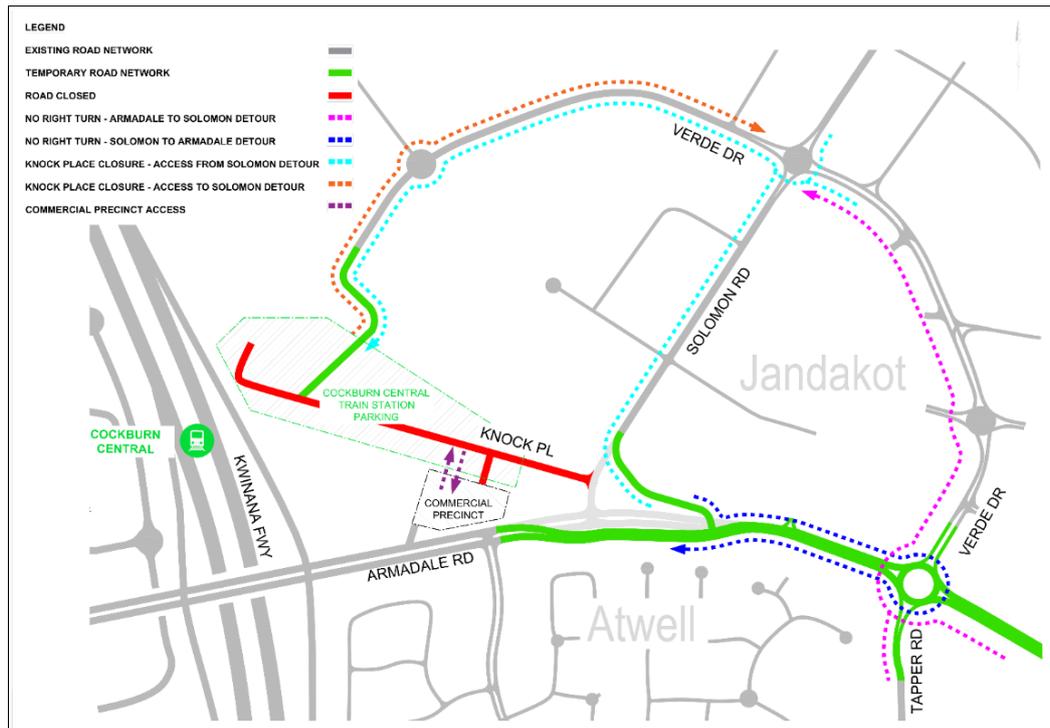


Image 4: Stage 1 and 2 - Traffic Detours

The closure of Knock Place and Hutt Link, and realignment of Armadale Road and Solomon Road is required to facilitate construction of the new Armadale Road / Solomon Road / Beeliar Drive grade separated intersection and can be supported for the following reasons:

1. The proposed closure of Knock Place and Hutt Link will allow workers safe access to the construction site and direct traffic flow away from the construction area.
2. Appropriate road signage will be installed to inform drivers of the site closure and works;
3. The impact on the immediate and surrounding road network has been assessed to operate at a suitable level of service;
4. Armadale Access Alliance has appointed a certified traffic management contractor to monitor the impact of the proposed road closures and access arrangements for the site and adjacent properties;
5. Armadale Access Alliance has submitted a traffic management plan, which complies with Australian Standards and the Main Roads guidelines;
6. Appropriate signage and barrier fencing will maintain continuity of the pedestrian and cycle network, and vehicle safety during construction activities.



Strategic Plans/Policy Implications

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Sustainably revitalise urban areas to deliver high levels of amenity and to cater for population growth.
- Develop Cockburn Central as our City centre and strengthen local area localities through planning and activation.

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.
- Listen to, communicate, consult and engage with our residents, businesses and community in a timely, open and collaborative manner.

Budget/Financial Implications

All the costs of the proposed road closure and traffic management will be covered by the Main Roads Western Australia and Armadale Access Alliance.

Legal Implications

Section 3.50 of the *Local Government Act 1995* refers.

Community Consultation

The Armadale Access Alliance carried out the public consultation and discussions with key stakeholders in the area, including:

- City of Cockburn Traffic and Transport;
- Public Transport Authority;
- Main Roads WA
- AAA project subscribers
- Cockburn Central PTA patrons
- Facebook post to City of Cockburn
- Notices displayed at City of Cockburn, Spearwood Library, Coolbellup Library and Success Library.

The proposed road closure was advertised on 30 July 2020 on page 26 in the Cockburn Gazette. A copy of the advertisement is shown in Attachment 2 for reference. The consultation period extended to 27 August 2020 to comply with the minimum 28-day public consultation period required under section 3.50 of the *Local Government Act 1995*.

As a result of the consultation, no response was received.



Risk Management Implications

The proposed closure of Knock Place and realignment of Solomon Road and Armadale Road is a complex project. If the Council does not support this proposal, it will critically increase the safety risk to construction workers, road users and pedestrians travelling through the area; significantly impact the project schedule; and ultimately affect the AAA team's ability to deliver an essential intersection upgrade for this project.

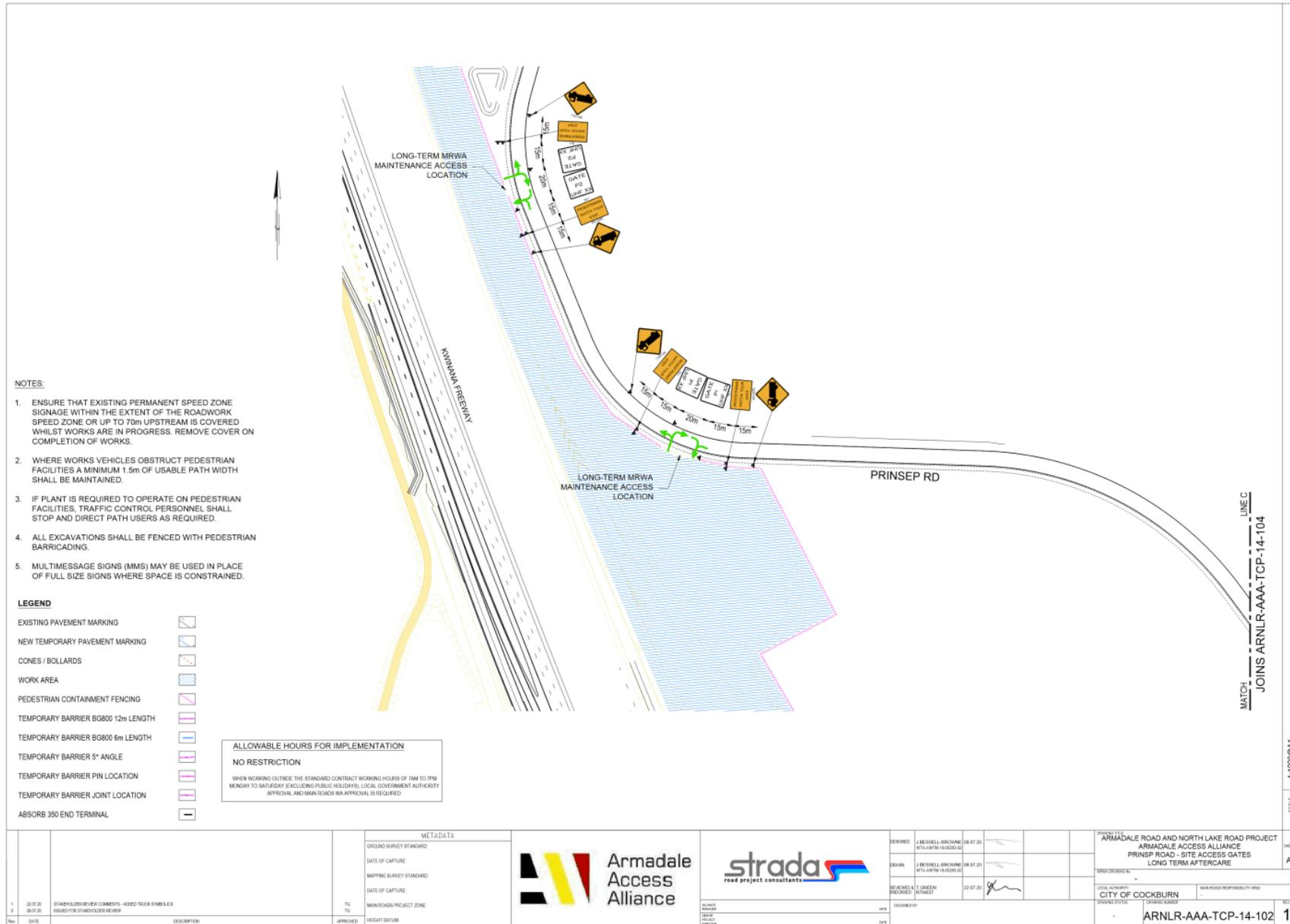
Advice to Proponent(s)/Submitters

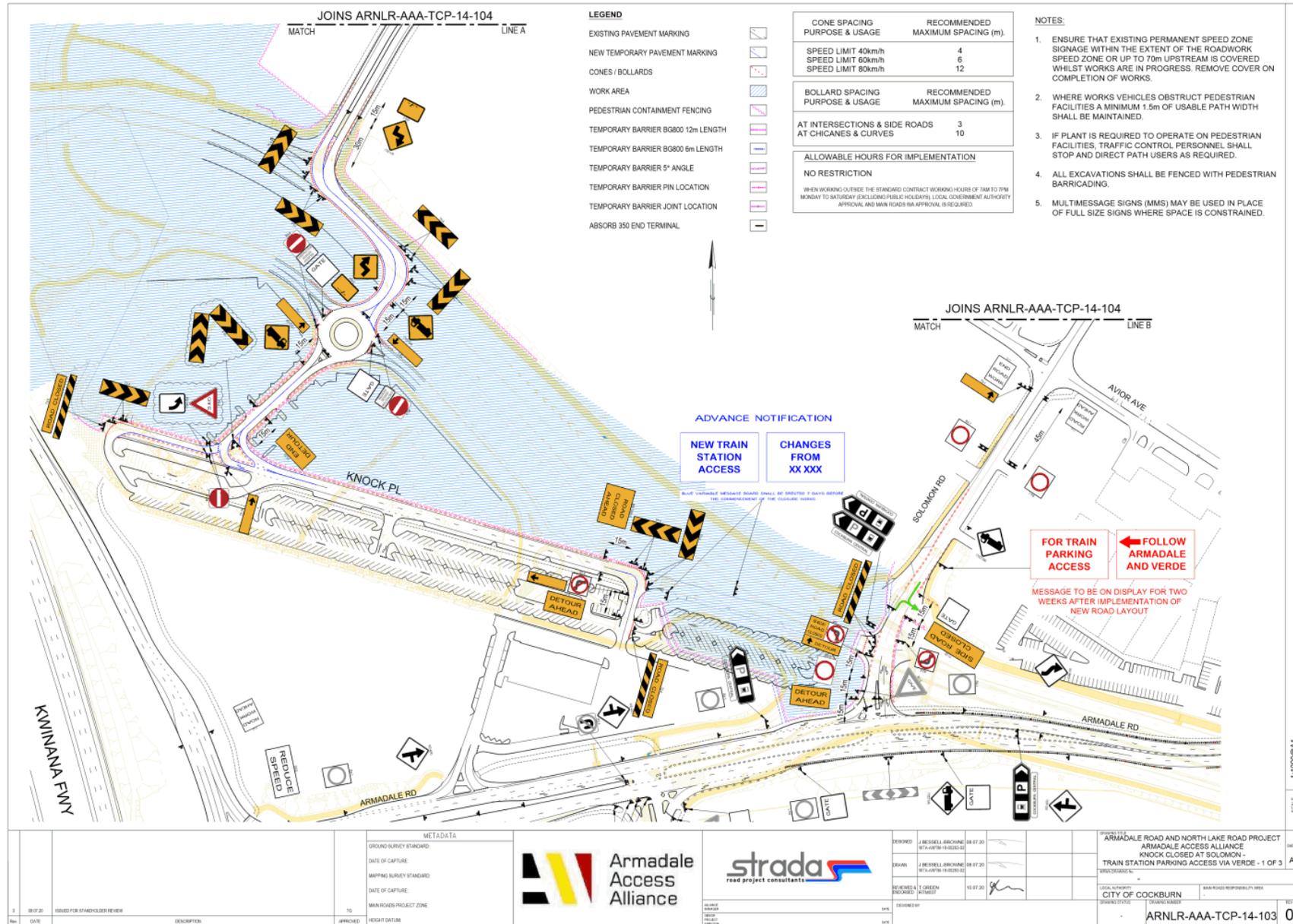
The Proponents have been advised that this matter is to be considered at the 10 September 2020 Ordinary Council Meeting.

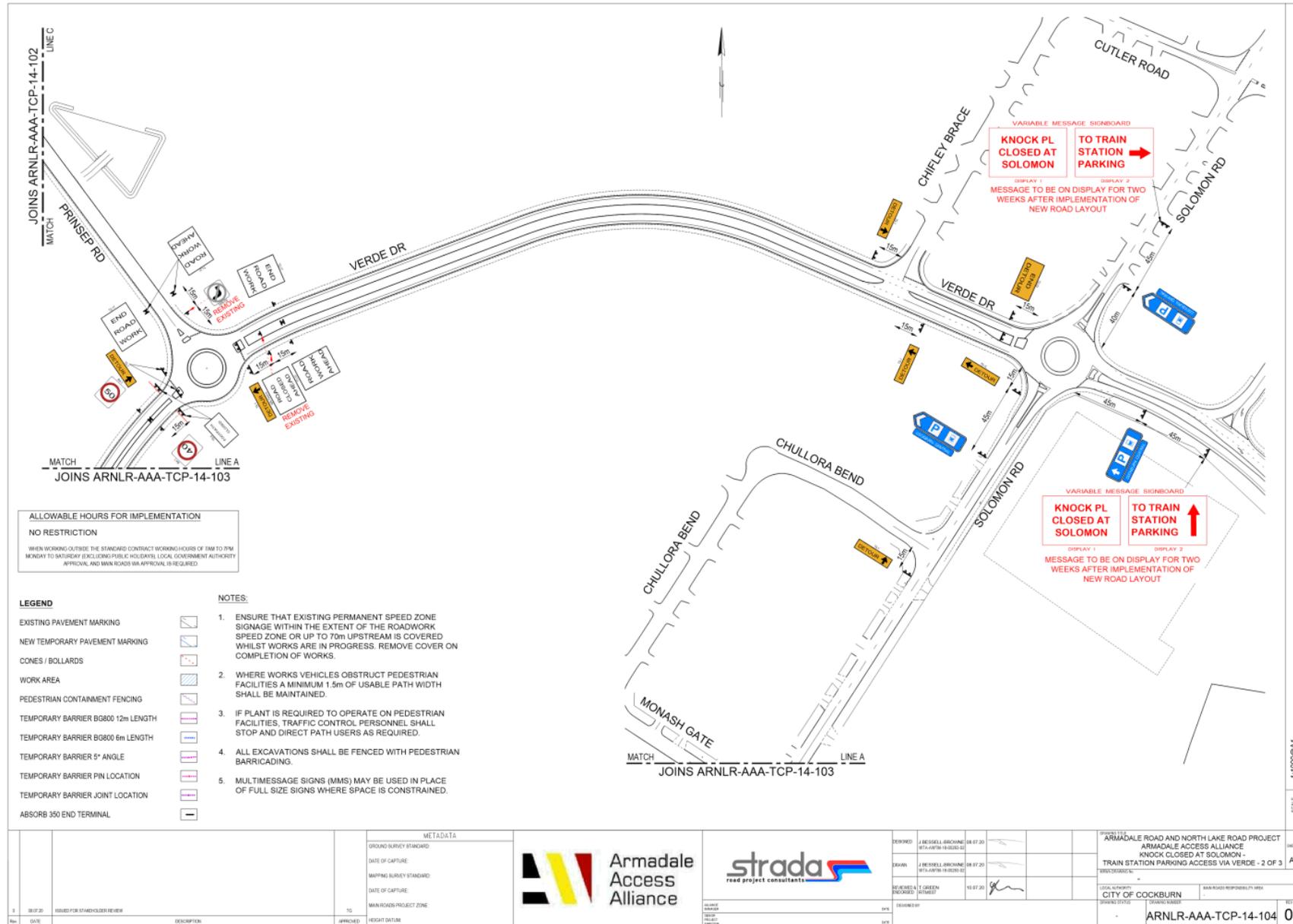
Implications of Section 3.18(3) *Local Government Act 1995*

Nil







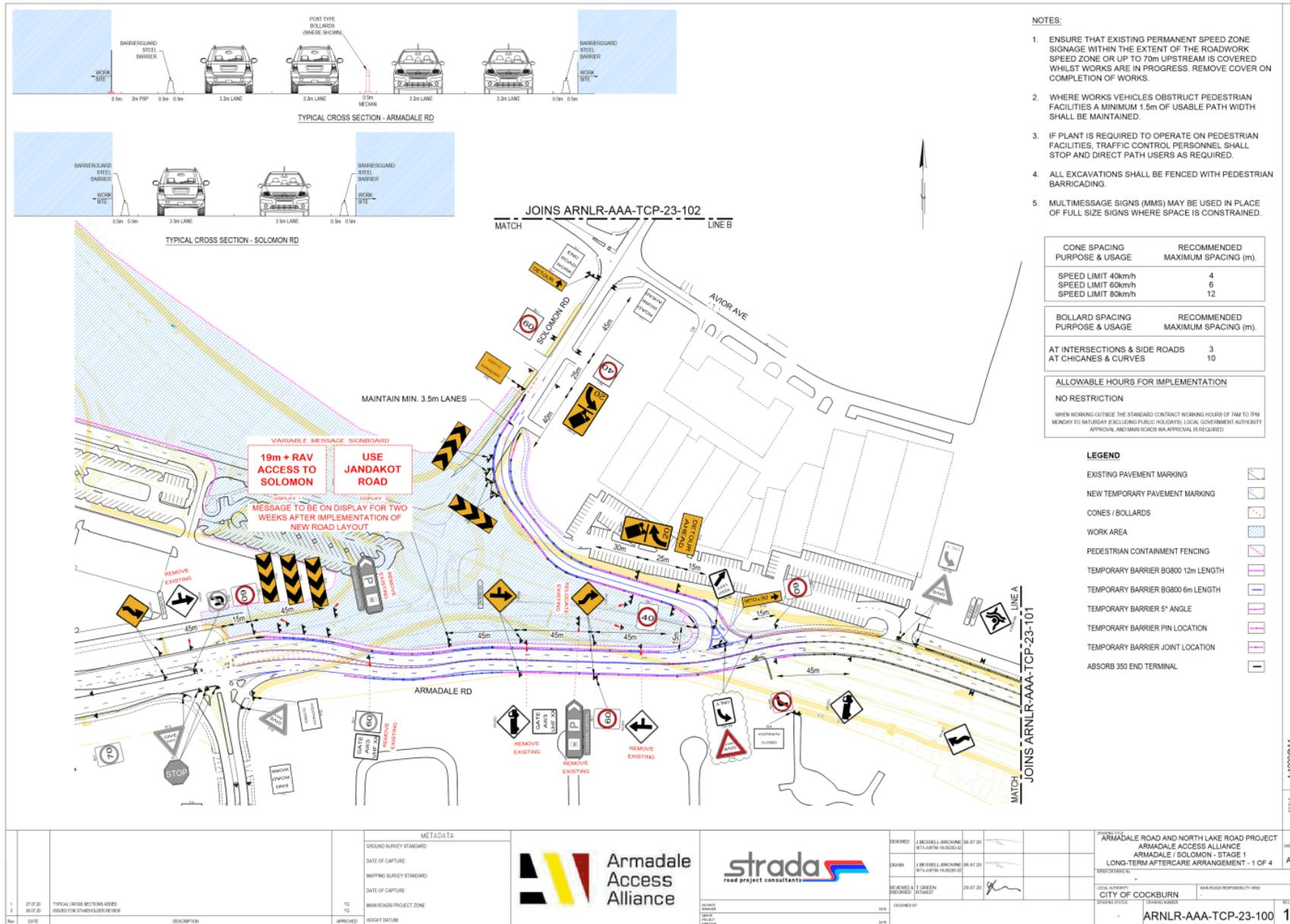


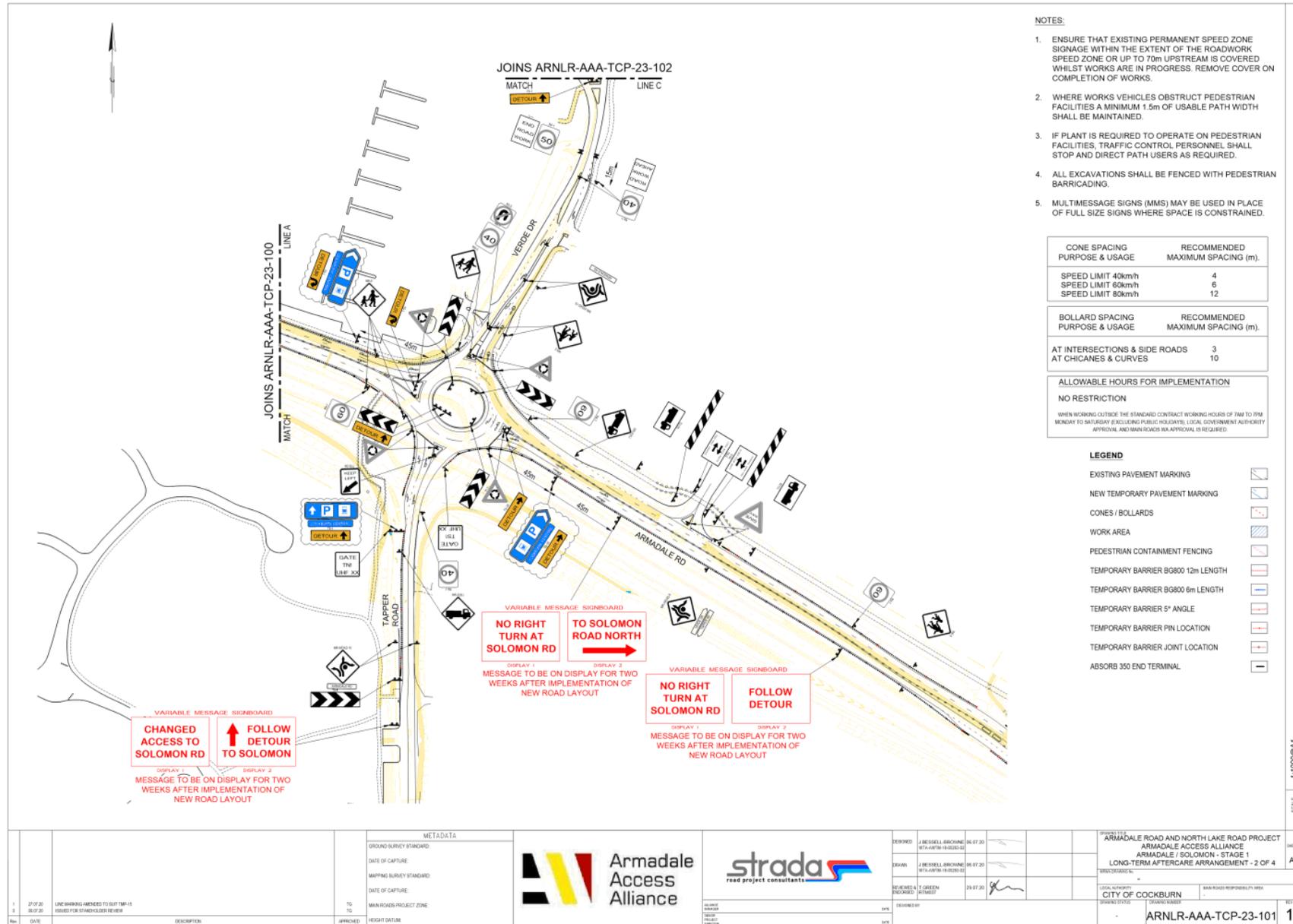
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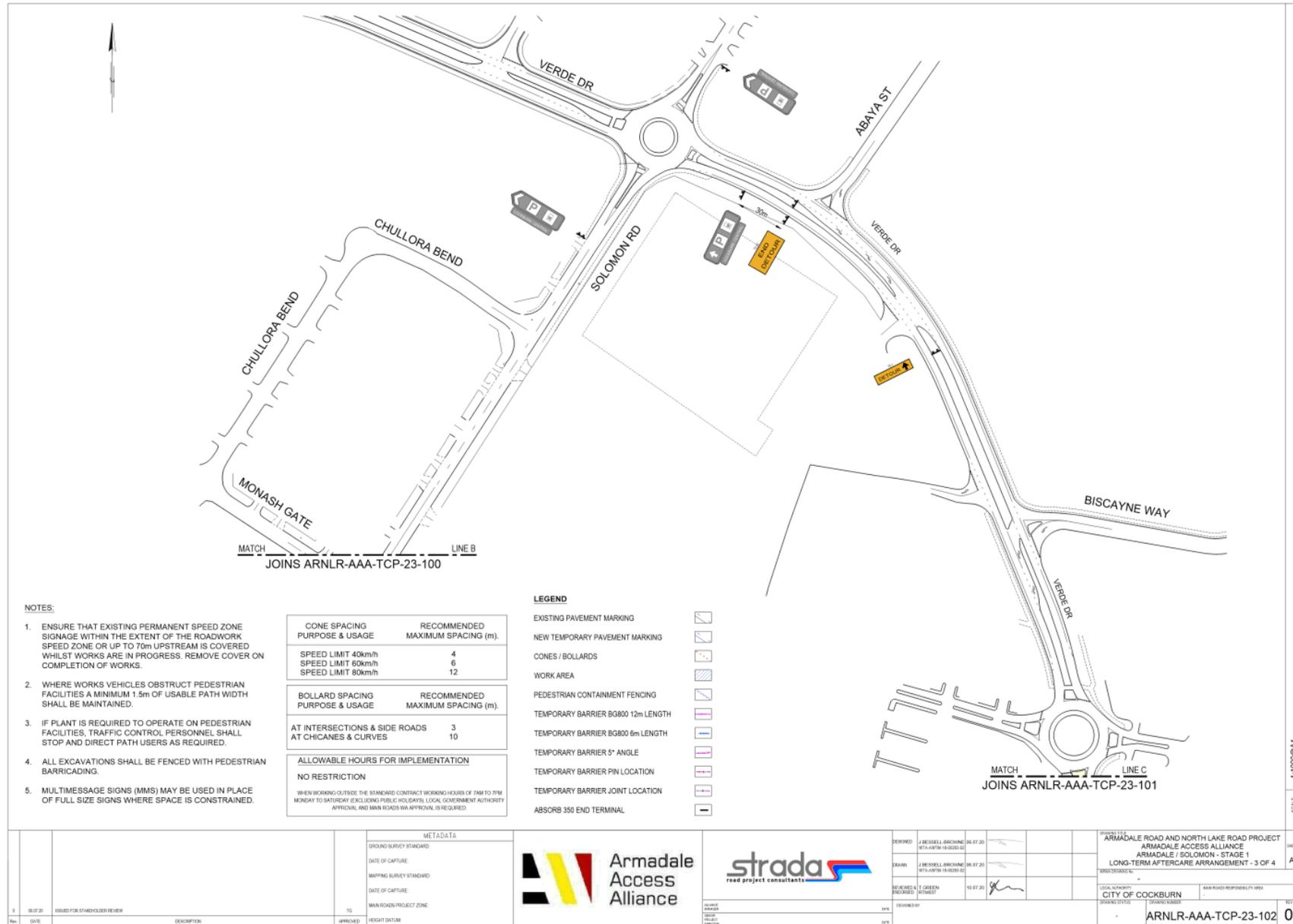
| | | |
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| DESIGNED | J. BESSHELL, ARCADIAN | 08.07.20 |
| DRAWN | J. BESSHELL, ARCADIAN | 08.07.20 |
| CHECKED | T. GREEN, ARCADIAN | 15.07.20 |

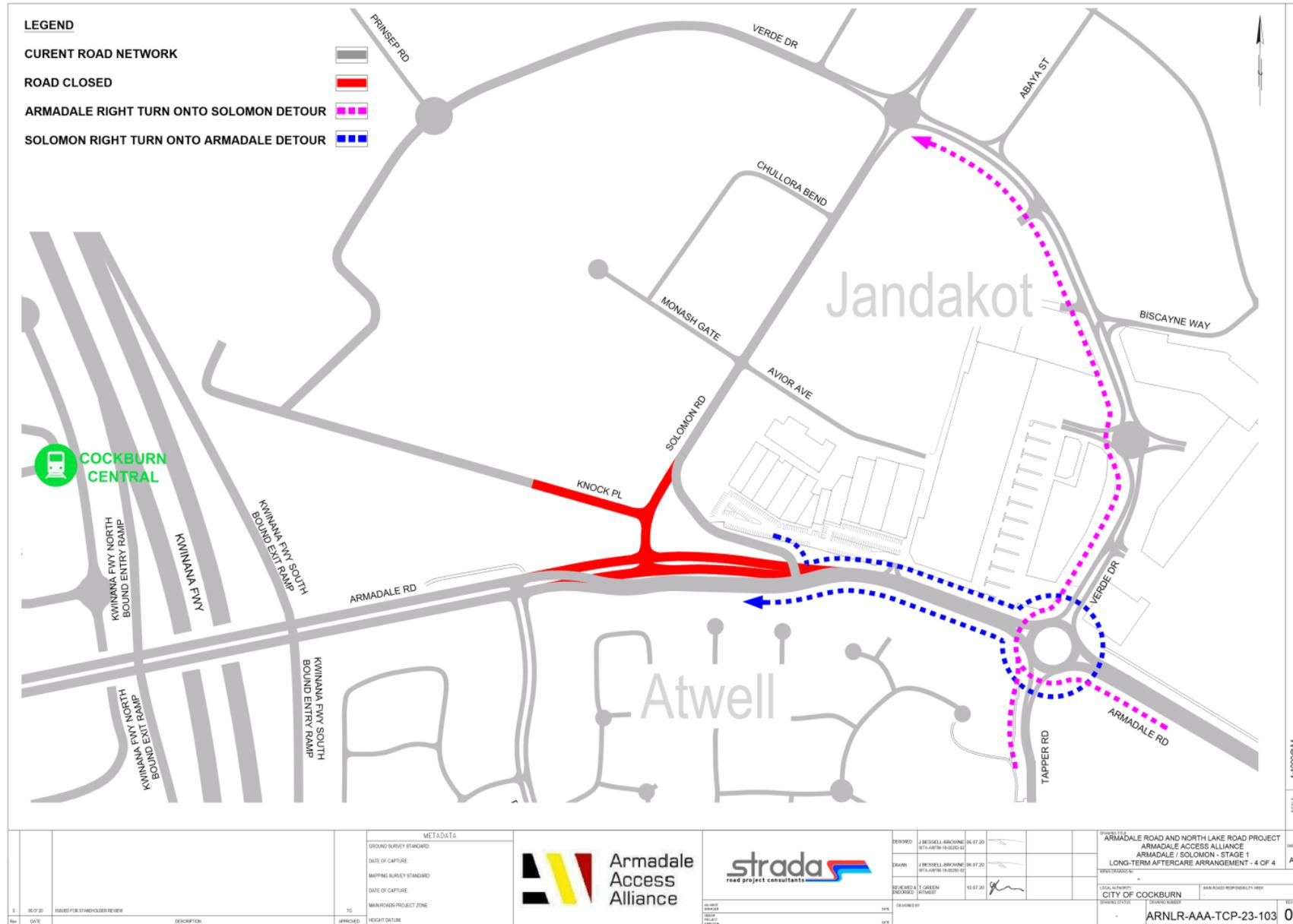
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|---------------------|----------------------|---|
| PROJECT TITLE | | ARMADALE ROAD AND NORTH LAKE ROAD PROJECT |
| CLIENT | | ARMADALE ACCESS ALLIANCE |
| PROJECT DESCRIPTION | | TRAIN STATION PARKING ACCESS VIA VERDE - 2 OF 3 |
| CITY | CITY OF COCKBURN | |
| PROJECT NUMBER | ARNLR-AAA-TCP-14-104 | |
| DATE | 19/10/2020 | |
| SCALE | 1:1000@A1 | |
| SHEET | 2 OF 3 | |





SCALE 1:1000@A1





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| <i>Initiative</i> | | p +61 8 6282 4738 | | CLIENT PROOF | |
| Initiative Key No.s: 16675_1242 | Section/Sort: Public Notice | Account Exec: Emily | Client Rev. No: 2 | | |
| Publication: Fremantle Cockburn Gazette | Ad Size (HxW): 20cm x 4 Columns | Operator Name: Lindsay | | | |
| Insertion Date: Thurs 30/07/20 | Size (HxW): 20cm x 12.9cm | Proofreader Name: _____ | | | |

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Forthcoming traffic changes between Verde Drive and Kwinana Freeway

In the coming months there will be significant changes to the road network on Armadale Road between Tapper Road and Kwinana Freeway.

Temporary roundabout at Armadale Road, Verde Drive and Tapper Road

Vital works are currently ongoing to construct a temporary roundabout at the intersection of Tapper Road, Verde Drive and Armadale Road, which will help maintain traffic flow following the removal of the traffic signals at this intersection. Subject to weather conditions, we expect this roundabout to be operational in late August.

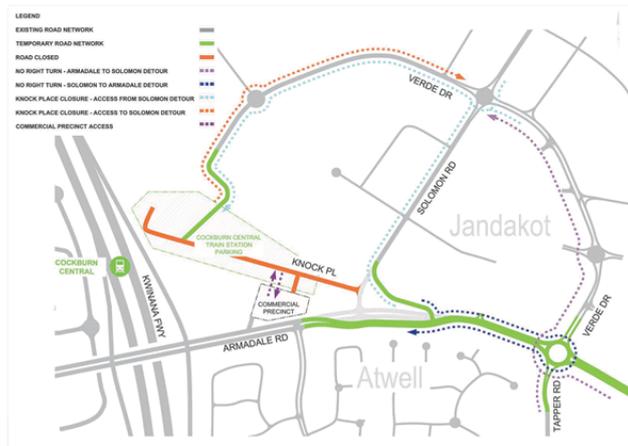
Armadale Road realignment and closure of Knock Place

This change will be followed in August 2020 by a realignment of Armadale Road between Solomon Road and Kwinana Freeway, and the long-term closure of Knock Place, between Solomon Road and Kwinana Freeway. The closure will enable construction of the new intersection of Armadale Road and Solomon Road as well as a new and improved Cockburn Central car park.

Access to commercial properties on Knock Place will be provided via a detour through Verde Drive onto Hutt Link. Future development will include the long-term closure of Hutt Link, where a replacement road to access commercial properties will be maintained.

Public comment on the proposed road closure of Knock Place and associated traffic diversions can be sent to enquiries@mainroads.wa.gov.au addressed to the Armadale Access Alliance, Community and Stakeholder Engagement Team before Thursday 27 August.

The detour map summarises what the road network will look like following the closure of Knock Place in August 2020.



MRWA_16675_1242

16.3 (2020/MINUTE NO 0197) CLIMATE CHANGE STRATEGY 2020-2030**Author(s)** J Harrison**Attachments**

1. Climate Change Community Engagement Report
2. Climate Change Strategy 2020-2030

RECOMMENDATION

That Council adopt the Climate Change Strategy 2020–2030.

COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr K Allen

That the recommendation be adopted.

CARRIED 9/0**Background**

The City of Cockburn has a strong, 20 year history in taking decisive and responsible action on climate change.

In 1999, Cockburn joined the Cities for Climate Protection Program, undertook its first greenhouse gas inventory, and developed an Emissions Reduction Plan.

In 2009 the City undertook its first climate change risk assessment and developed a Climate Change Action Plan.

The *Greenhouse Gas Emission Reduction Strategy* was developed in 2011 and set targets to reduce Council emissions by 2020 from waste, electricity, gas and fuel. In 2013 Council adopted a 2020 renewable energy target.

The City has achieved three out of the five targets set under the previous 2020 strategy:

- Zero Emissions Fleet (achieved)
- 45% cap on waste emissions (achieved)
- 20% renewable energy by 2020 (achieved).

The City has maintained a Zero Emissions Fleet since 2011, through the procurement of accredited carbon offsets supporting Australian native reforestation in Western Australia.

There has been a significant reduction in waste to landfill at Henderson Waste Recovery Park, largely as a result of market competition. Consequently, emissions from waste have remained under the 45% target cap.

The City achieved its renewable energy target in 2018, and currently generates 26% of electricity for Council facilities from solar photovoltaics systems installed on its buildings.



Two of the 2020 targets have not been achieved:

- 20% reduction on electricity and gas emissions (not achieved)
- 10% reduction on streetlight emissions (not achieved)

Electricity and gas emissions have increased over the past few years mainly due to the operational requirements of the Cockburn Aquatic and Recreation Facility.

The number of streetlights in Cockburn has also increased with urban expansion and greenfield developments. Currently, only 2% of Western Power streetlights in Cockburn are LED. The City is actively advocating for large-scale rollout of smart energy efficient lights to reduce the emissions and cost of street lighting.

Submission

N/A

Report

In 2020 the City commenced development of a Climate Change Strategy to supersede the Greenhouse Emissions Reduction Strategy 2011-2021, and the Climate Change Adaptation Plan. The aim of the strategy is to present an integrated approach to address climate change adaptation and mitigation.

In May 2020, public consultation with the Cockburn community on how they would like to see the City demonstrate leadership on climate change action was carried out. The City's residents, businesses, stakeholders and reference groups were all invited to share their vision for a better tomorrow.

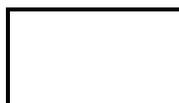
The Cockburn community were very clear on their position of advocating for an aspirational approach to reduce all emissions and protect our unique environment (refer Attachment 2).

The City also undertook a risk assessment and updated its Climate Change Risk Register in 2020.

The City has used information from the community consultation and the risk assessment to develop the Climate Change Strategy 2020-2030 (refer Attachment 1), which includes a resilience roadmap that sets targets to achieve carbon neutrality and increase our adaptive capacity.

The resilience roadmap presents 14 key objectives:

1. Strong leadership
2. Net zero emissions
3. Zero waste to landfill
4. 100% renewable energy
5. Reduce energy consumption
6. Zero emissions fleet
7. Education and collaboration
8. Smart city innovation



9. Waterwise City
10. Conserve biodiversity
11. Coastal adaptation
12. Increase the urban forest
13. Protect community infrastructure
14. Enhance health and wellbeing

Each objective is addressed through a comprehensive action plan that will deliver innovative solutions for energy supply, leadership in waste management and best practice climate adaptation.

Strategic Plans/Policy Implications

Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Reduce adverse outcomes arising from climate change through planning; adaptation, mitigation, infrastructure and ecological management.
- Minimise the City's waste to landfill through reducing, reusing, re-purposing, re-gifting and recycling of waste.

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Listen to, communicate, consult and engage with our residents, businesses and community in a timely, open and collaborative manner.
- Actively advocate and seek regional collaboration focussed on growing the wellbeing and self-sufficiency of the community to better meet their social, environmental and economic needs.

Budget/Financial Implications

The Climate Change Strategy is supported by the City's Greenhouse Action Fund which includes an allocation of \$200,000 each year for initiatives to address climate change. The Greenhouse Action Fund was established in 2011 using ongoing savings from previous emission reduction initiatives.

Many actions in the strategy align to existing strategies or plans which provide guidance on resources and funding. The remaining actions will be subject to annual budget submissions, approved business cases by responsible leaders and budget capacity.

Coordination of the actions, delivery of education programs and the continued investment in researching new initiatives will require the appointment of a full-time Climate Change Officer (currently part-time contract).



It is recommended that a full-time Climate Change Officer be included in the Work Force Plan for appointment in the 2021-2022 financial year to oversee the implementation of this strategy and the climate resilience roadmap.

Legal Implications

If ignored, Climate Change may cause legal, financial, physical and reputational risks to the City. The City is seeking legal advice in collaboration with WALGA, to clarify its liability in the event of coastal climate change risk scenarios.

Community Consultation

A community engagement project titled *Our Sustainable and Climate Resilient Future* was undertaken from 1–28 May 2020.

Over 2000 people visited the Comment on Cockburn page for the climate change consultation project and nearly 1,000 visited the online survey. In total, 309 survey responses were received, seven submissions were made and nine digital workshops were facilitated (including an Elected Member workshop). The City had one of its highest visitation rates on the Comment on Cockburn webpage in response to this project.

The Cockburn community were very clear on their position of advocating for a very aspirational approach for the City to demonstrate leadership regarding sustainability and climate resilience. This means:

- Aiming towards being a carbon neutral City
- Support for action towards reducing greenhouse gas emissions by all emitters
- Investing in education programs to support adaptation initiatives and increase awareness of everyone's environmental impact
- Reducing energy and consumption costs for City operations
- Preservation of the urban forest and enhancement of our unique natural landscape.

Risk Management Implications

The City undertook a risk assessment and updated its Climate Change Risk Register in 2020. The process considered the consequences and likelihood of climate risks using the City's ratings frameworks, which are consistent with AS ISO 31000:2018 *Risk management - Guidelines*.

There are six overarching climate change risks that have the potential to impact the City's service delivery, natural environment, local community and infrastructure:

1. Reduced water availability from decreased rainfall,
2. Biodiversity loss from climate change impacts,
3. Coastal impacts from sea level rise,
4. Urban forest decline from climate change,



5. Community infrastructure damage from climate change impacts,
6. Public health decline from climate change.

To reduce the impact of these risks the City has integrated them into its Risk Management and Safety System (RMSS) and identified key solutions in the Climate Change Strategy.

If Council does not adopt the Climate Change Strategy, the City will not have an adequate response to address the identified climate change risks.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

N/A





City of Cockburn

Our Sustainable & Climate Resilient Future: Community Engagement Outcomes Report

Version 3 Issued: 23 June 2020



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1. Executive Summary

The City of Cockburn sought community input to guide the development of its inaugural Climate Change Strategy. The City engaged Town Team Movement to undertake the engagement process.

The engagement program targeted residents all of ages, businesses, key stakeholders, community and minority groups. As a result, over 2000 people visited the City's project page, "Our Sustainable and Climate Resilient Future" and nearly 1000 visited the online survey. In total, 309 survey responses were received, seven submissions were made, nine digital workshops were facilitated and the City had one of its highest visitation rates on the 'Comment on Cockburn' webpage.

The engagement results reveal that respondents have a clear vision for Cockburn when it comes to a more sustainable and climate resilient future. Respondents are vastly supportive of an Aspirational Approach, where together we aim for best practice in Western Australia and are prepared for some adaptive changes to create a better tomorrow. More specifically, this means:

- Aiming towards being a Carbon Neutral City
- Support for strong action towards reducing greenhouse gas emissions by all emitters
- Investing in education and immersive cultural programs to support the community's own adaptation initiatives and increase awareness of everyone's environment impact
- Reducing energy and consumption costs for council operations
- Preservation and enhancement of our unique natural landscape

A shared environmental vision has been developed having regard to the insightful conversations and results of the engagement program, which states:

The City of Cockburn is a leader in climate resilience and sustainability. Our community provides us with the strength and optimism to be aspirational in our approach to creating a better tomorrow for each and every one of us.

We are aware of our lasting environmental impact. We are committed to taking pride in working together, celebrating our successes and being resilient and adaptive to our changing climate.

In summary, the engagement results provide the aspiration and confidence for the City to be a strong leader and facilitator amongst its state peers to creating an even more sustainable and climate resilient future.

2. Introduction

The City of Cockburn has engaged The Place Team (Town Team Movement) to lead the design and facilitation of an engagement program aimed at seeking feedback from ratepayers, residents and businesses regarding a leadership direction for creating an even more sustainable and climate resilient future. The engagement approach sought to address three key challenges for the City:

- Seek the community's vision for creating a more sustainable and resilient future
- Determine the community's aspiration for the City to demonstrate climate resilient and sustainable leadership
- Gain insight into priority areas for adaptation and resilience

The results and key recommendations of the engagement program will inform the preparation of the City's Climate Change Strategy.

The City's Strategic Community Plan 2016-2026 aims to achieve five core community aspirations as listed in its strategic objectives. The following two objectives are essential to the subject engagement project:

Leading & Listening
Economic, Social & Environmental Responsibility

More specifically, point 5 of 'Leading & Listening' objective states the following:

"Provide for community and civic infrastructure in a planned and sustainable manner, including administration, operations and waste management."

Points 3-6 of 'Economic, Social & Environment Responsibility' objective states:

"Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health."

"Improve the appearance of streetscapes, especially with trees suitable for shade."

"Improve water efficiency, energy efficiency and waste management within the City's buildings and facilities and more broadly in our community."

"Further develop adaptation actions including planning; infrastructure and ecological management to reduce adverse outcomes arising from climate change."

The City has prioritised development of the City's Climate Change Strategy as a key initiative to meet the aspirations identified within the City's Strategic Community Plan 2016-2026. Public and Stakeholder Engagement is critical to informing a future vision, adaptation and resilience measures to guide the Climate Change Strategy.

3. Background

The City of Cockburn has been taking action to adapt to the impact of climate change for more than 20 years.

The City of Cockburn participated in a regional climate change adaptation planning process in conjunction with the South-Metropolitan Regional Council in 2009 to identify regional risks and adaptation actions. Subsequently, the City prepared an operational Climate Change Adaptation Plan, which included a set of actions to manage the specific risks.

The Adaptation Action Plan is due for review in 2020 as part of the development of a Climate Change Strategy. The subject community consultation is undertaken to gain an understanding on how the community would like the City to demonstrate future leadership in climate change.

Some of the key climate achievements completed to date include:

- Largest inventory of solar panels in WA Local Government
- 23% of electricity is sourced from roof top solar
- 100 MW PV and geothermal at the ARC
- Best practise coastal management

The City has also been awarded on multiple occasions for its on-going commitment to sustainability.

This project reflects the City's aim to engage with ratepayers, residents, businesses and relevant stakeholders regarding the future sustainable and climate vision, leadership aspirations, adaptation and resilience priorities and objectives.

The next stages of the project will be to draft the climate change strategy. The adopted strategy is expected to be endorsed by the city in 2020 and include objectives for the next 10-year period.

4. The Community Engagement Process

It was critical that the City implemented a considered approach to engaging with its residents and businesses to better understand the future issues regarding a more sustainable and climate resilient future.

The comprehensive community engagement approach addressed the following matters:

- Developing an understanding of a collective vision for the future of sustainable and climate resilient leadership.
- Understanding the resident, business, staff and elected members aspirations for creating an even more sustainable and climate resilient future.
- Understanding the resident, business and staff challenges or barriers and ideas to overcome these challenges for implementing direct climate and sustainable action.
- Adaptation and Resilient priorities for future implementation (including, bushfire prevention, coastal management, biodiversity protection, water conservation, community health, education and empowerment, etc).
- Understanding the resident and businesses aspirations and direction for future greenhouse gas emission targets.
- Listening and responding to any additional comments.

5. Community Engagement Audience

It is important that the engagement program be accessible and inclusive of all the City's residents and businesses etc, so the engagement process targeted a variety of stakeholders applying new and innovative engagement methods to understand how best to achieve this.

In addition to seeking input from residents and businesses, the following specific reference groups were targeted and invited to participate in virtual and face-to-face workshops;

- Children's Reference Group
- Youth Advisory Committee
- Business and Industry Groups and organisations
- Aboriginal Reference Group
- Disabilities Reference Group
- Community and Stakeholder Groups
- City of Cockburn Staff
- Elected Members

Please note that due to the COVID-19 crisis, some face-to-face conversations and workshops were modified, and new approaches were initiated to reach our diverse audiences using innovative virtual engagement tools and practising safe social distancing at all times. In all cases, every attempt was made to reach out to each of the identified groups above for relevant input and positive feedback was received stating that virtual workshops were accessible, easy to follow and more convenient than face-to-face methods. Participants even asked for more virtual tools to be implemented in future engagement projects.

6. Engagement Methodology

The engagement period started on 1 May 2020 and concluded on 28 May 2020. Public surveys, marketing content and an online project page was setup during this period to allow residents to provide feedback. In total, engagement reached over 30,000 ratepayers, residents, businesses and relevant stakeholders.

The project was communicated in a number of ways, including following:

- Email to a random sample of 5000 ratepayers
- E-News (various) to over 7000 subscribers
- Social media posts and advertisements with a reach of over 25,000 people
- Targeted social media posts and advertisements to youth audience with reach more than 5,000
- E-mail with workshops invitations, survey and detailed information sent to City databases including seniors centre database, cultural database, schools database, youth database, business database, environment database and 23 City of Cockburn resident groups / associations
- A dedicated page on the City's engagement website, Comment on Cockburn, housing project information, Q&A board, workshop and listening post dates and registration forms, and a survey
- Marketing and promotion driving visitors to Comment on Cockburn including billboards and e-newsletters
- Translated language text in Hindi, Mandarin/Chinese and Filipino/Tagalog for multicultural social media groups
- Direct contact with key stakeholders: WALGA, WALGA Sustainability Officer Network, DWER, Cockburn Sound Coastal Alliance, Coastcare / Perth NRM, Native Arc, Cockburn Wetlands Education Centre, Friends of Groups, Millenium Kids, Teachers involved in prior sustainability projects / initiatives
- Posters with QR codes for display at high school and electronic copies sent to P&C / Teachers
- Two advertisements in the Cockburn Gazette
- Other marketing and promotion

The following direct initiatives were undertaken to engage with the diverse ratepayers, residents, businesses and relevant stakeholders within the City of Cockburn:

- Phone conversations and feedback with members of the City's Aboriginal Reference Group
- One virtual consultation workshop with the City's Children's Reference Group
- One virtual consultation workshop with the City's Youth Advisory Group
- One face-to-face consultation workshop with the City's Youth Advisory Group
- One virtual consultation workshop with the City's Disability Reference Group
- One virtual consultation workshop with the Business and Industry members and organisations located within the City's LGA boundary
- One virtual consultation workshop with general community members
- Two virtual community listening posts for community members to ask general questions and seek feedback on past and existing sustainability initiatives
- One virtual consultation workshop with the City's staff
- One virtual consultation workshop with elected members
- General email feedback and ideas relating to sustainability and resilience planning

7. Engagement Results

The following is an analysis of the survey undertaken and discussion outcomes, organised by the general public (via survey feedback) and each stakeholder group.

General Survey Results

The engagement included a public community survey open to all residents for a 28-day comment period between 1-28 May 2020. In total, more than 2,000 people visited the Comment on Cockburn webpage during the consultation period (including many people visiting for the first time) and 309 public submissions were received, which is considered a good sample size for analysing the sentiment and needs of the population within the City.

153 people also visited the 'Frequently Asked Questions' page, which is a positive tool contributing to a stronger understanding of sustainability and climate resilience at the City.

| | |
|------------------------------------|---|
| | <ul style="list-style-type: none"> • 'Climate' – respondents are generally supportive of investing in climate resilience, however there is some confusion about education and facts. • 'Sustainable' – respondents overwhelmingly are positive and aspirational about creating a more sustainable environment. • 'Water' – respondents are generally concerned about water use and advocate for a more 'water wise' environment. |
| <p>Common Key phrases include:</p> | <p><i>"Greater tree canopy in Hamilton Hill, less grass in Manning Park and more trees please, renewable energy used and supported wherever possible".</i></p> <p><i>"More nature areas and grassed and treed areas for coolness... Grants to retroactively fit houses with energy efficient features of all kinds."</i></p> <p><i>"I would like to see more renewable energy in Cockburn"</i></p> <p><i>"Congrats to the Horticulture staff at CoC, great work. Please plant more native vegetation across our wonderful local environment."</i></p> <p><i>"Investment in local resident incentives for sustainable living."</i></p> <p><i>"To create benchmarks for Western Australian suburbs in this field. To be creatively and actively looking for ways to be kind to the environment and research what other countries are successfully achieving and to be petitioning to federal government for more positive changes."</i></p> <p><i>"An environment that sustains both the natural and human life within in's boundaries and co-operates in the wider community to promote the well-being of the residents now and into the future."</i></p> <p><i>"Managing natural resources, harness the use of renewable energy and building efficient, low-carbon suburbs. Enforce effective waste management and low-carbon, energy-efficient transport."</i></p> |



| | |
|--|--|
| | <p><i>"I would like to see a place where we live in balance and with an awareness of how we impact our environment. Where the community is encouraged to be more aware, offered information and encouraged to make sound choices that are able to be sustained."</i></p> |
|--|--|

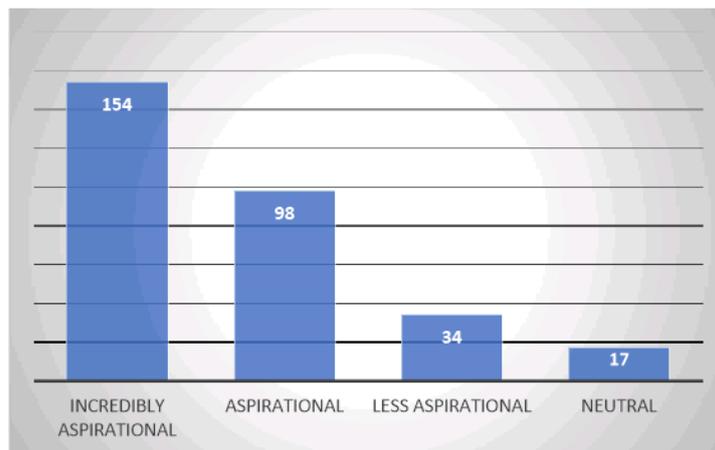
Graph 1: Community level of support for reducing Council's Greenhouse Gas Emissions

| Response | Operations Response | Transport Response | Local Business Response | Residents Response | Industry response |
|--|---------------------|--------------------|-------------------------|--------------------|-------------------|
| No, Cockburn should not focus on this, | 4.58% | 9.15% | 9.90% | 11.44% | 9.15% |
| Not bothered, | 3.92% | 2.61% | 1.65% | 2.29% | 1.96% |
| Possibly, but only if it doesn't come at a cost, | 20.92% | 21.24% | 28.38% | 27.12% | 15.69% |
| Yes definitely, this should be a priority. | 70.59% | 66.99% | 60.07% | 59.15% | 73.20% |

Key Findings:

- On average, Approx 66% of respondents indicated 'Yes definitely, this should be a priority' across all emitters.
- On average, only 8.84% of respondents indicated 'No, Cockburn should not focus on this' across all emitters.
- The survey clearly demonstrates support for reducing greenhouse gas emissions as a priority across all emitters.

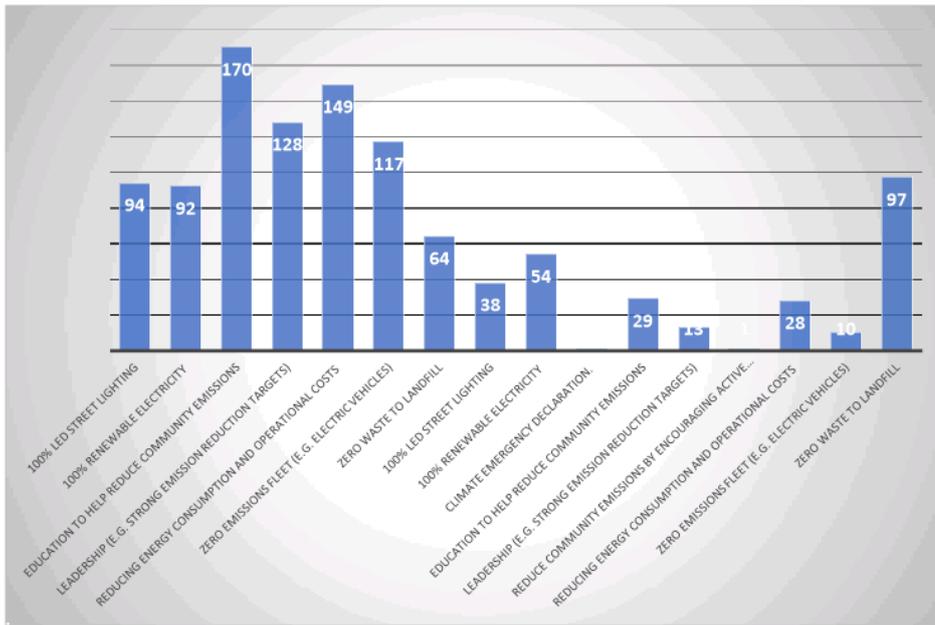
Graph 2: Community aspirations for the City to demonstrate leadership for creating a sustainable and resilient future



Key Findings:

- The community respondents have voted in favour of an 'incredibly aspirational' approach to 'aspirational' approach to creating a sustainable and climate resilient future.
- The City should aim to be best practice leaders in Western Australia and National leaders where possible.

Graph 3: Rating the importance of focus areas to reduce carbon emissions



Key Findings:

- 'Education to help reduce community emissions' is the most popular response with 170 counts.
- 'Reducing energy consumption and operational costs' is the second most popular response with 149 counts.
- 'Leadership (i.e. strong emission reduction targets)' is the third most popular response with 128 counts.



Table 2: How would you rate the importance of the adaptation and resilience objectives listed below?

| Rate the Importance | Inform the community to help them adapt to impacts of a changing climate | Reduce the heat island effect by planting more street trees | Protect infrastructure (e.g. drainage and roads) against severe storms: heat and flooding | Increase community resilience to the health impacts of a changing climate | Increase community resilience to heatwaves | Environmental conservation to maintain biodiversity | Water reduction in Council facilities | Water management to maintain key parklands and streetscapes | Emergency management planning to prepare for climate change impacts | Bushfire management planning to limit adverse effects on the community | Managed coastal retreat (a staged approach to moving foreshore areas and infrastructure further inland to adapt to sea level rise and associated erosion) | Protecting the coastline against sea level rise and erosion |
|----------------------|--|---|---|---|--|---|---------------------------------------|---|---|--|---|---|
| Not at all important | 2 | 9 | 16 | 48 | 46 | 15 | 19 | 6 | 40 | 11 | 42 | 27 |
| Somewhat important | 8 | 45 | 138 | 132 | 130 | 63 | 123 | 89 | 115 | 91 | 131 | 95 |
| Unsure | 3 | 0 | 4 | 15 | 18 | 3 | 7 | 2 | 8 | 3 | 12 | 5 |
| Very important | 289 | 251 | 147 | 107 | 109 | 221 | 156 | 207 | 137 | 195 | 117 | 178 |

Key Findings:

- 'Inform the community to help them adapt to impacts of a changing climate' was rated the most important adaptation and resilience objective.
- 'Reducing the heat island effect by planting more street trees', 'water management to maintain key parklands and streetscapes' and 'bushfire management planning to limit adverse effects on the community' were the next most important adaptation and resilience objectives.
- 'Increase community resilience to the health impacts of a changing climate', 'increase community resilience to heatwaves' and 'managed coastal retreat' were the least important rated adaptation and resilience objectives.

Table 3: Do you support the following greenhouse gas emission reduction targets?

| | 100% renewable energy for Council operations by 2030 | Net zero Council emissions (Carbon Neutral) by 2030 | Net zero community emissions (Carbon Neutral) by 2050 |
|--------------|--|---|---|
| I don't mind | 34 | 37 | 31 |
| No | 43 | 42 | 56 |
| Yes | 224 | 219 | 210 |

Key Findings:

- The community respondents are in favour of aspirational greenhouse gas emission reduction targets.

Table 4: Are you aware of any current sustainability initiatives in Cockburn?

| | | |
|-----|-----|--------|
| No | 135 | 44.12% |
| Yes | 171 | 55.88% |

Key Findings:

- 44% of respondents are not aware of any current sustainability initiatives in Cockburn.
- It is recommended that the City better communicated current and future sustainability initiatives to the wider community.

Table 5: Which initiative/s are you proud of and would like to see continue?

| Current Initiatives | Count of Which initiative/s are you proud of and would like to see continue? | Count of Which initiative/s are you proud of and would like to see continue?2 |
|---|--|---|
| Green waste verge collections | 144 | 7.02% |
| 3 bin household waste system | 142 | 6.72% |
| bird bath | 132 | 6.67% |
| native plants | 132 | 6.67% |
| waterwise verge | 132 | 6.67% |
| Rebates and subsidies | 132 | 6.47% |
| Rooftop solar on council buildings | 131 | 5.05% |
| Bushland management | 126 | 6.11% |
| Sustainable living workshops and events | 120 | 4.90% |
| Environmental education program | 106 | 5.31% |

| Current Initiatives | Count of Which initiative/s are you proud of and would like to see continue? | Count of Which initiative/s are you proud of and would like to see continue?2 |
|------------------------------------|--|---|
| Electric vehicle charging stations | 102 | 4.50% |
| Carbon offsets | 101 | 5.10% |
| Free home energy water audits | 101 | 5.10% |
| Grant programs | 96 | 4.75% |
| Electric staff vehicle fleet | 95 | 4.80% |
| Zero emissions fleet | 95 | 4.50% |
| Coastal adaptation program | 92 | 4.60% |
| Grand Total | 1979 | 100.00% |

Key Findings:

- 17 initiatives were highlighted by respondents, which the community are proud of and would like to see continue.
- The most popular initiatives to continue include 'green waste verge collections', '3 bin household waste system', 'bird bath', 'native plants', 'waterwise verge' and rebates and subsidies.
- The most popular initiatives are all programs, which are accessible to residents and improve the sustainable outcomes of households.

Table 6: General themes for proposed new initiatives

| # | Theme |
|---|---|
| 1 | Recycling |
| | <ul style="list-style-type: none"> • Extensive reuse/recycling of wastewater to irrigate all parks and gardens using Woodman Pt wastewater to help achieve zero ocean discharge. • Facilitating recycling projects (fogo, education, etc) • Reduction in waste going to Landfill • More bins / collection facilities for batteries, aerosols and other dangerous goods • Provide more compost bins for residents • Trial new initiatives: Community-led repurpose, reuse and recycling station in the new Treeby community centre, repair café, multiple terracycle collection boxes, lending library, etc. |
| 2 | Reduce Emissions / Alternative Power |
| | <ul style="list-style-type: none"> • Removing power poles and convert to underground power • Reduce industry emissions • Assist businesses become more sustainable • Honeycomb solar electric community grids • Green waste collection switched to onsite mulching • Making residents and households more accountable for waste • Stronger environmental enforcement of building approvals and advocating for tougher regulation. Enforce renewable materials and sustainable housing • Net zero community emissions (carbon neutral) by 2030 • Trial alternative energy technology • Repair cafes and other pop-up services to encourage reuse • 100% renewable electricity |

| | |
|---|---|
| | <ul style="list-style-type: none"> • Reduce traffic movements to reduce emissions • Improving masterplanning processes and planning policies to reduce heat island impact • Solar panels, water tanks for residents. Less passive grass spaces in parks • Free solar panels for schools • Facilitating and supporting community leaders in reducing carbon emissions |
| 3 | Offer Rebates |
| | <ul style="list-style-type: none"> • Home batter storage systems • Encourage solar panels, water/waste reduction and electrical and vehicle uptake for businesses and residents • Making businesses financially responsible for waste • Electric bikes for workers and schools • 'Green' grants for builders to construct and retrofit more sustainable housing • Electric vehicle owners / drivers • Renewable energies • Cloth nappies • Community energy funds for community groups to install renewables • Free garden startup kits • Native plants |
| 4 | Education |
| | <ul style="list-style-type: none"> • Making it easier to achieve zero waste lifestyles • Better engagement with schools and community groups – organise trash and litter clean-ups and recognise Green Heroes • More education for households • Small discount on rates to complete online sustainable education course • Less verge collections, more education • Communicate actual facts regards to climate change, coastal erosion, sea level rise, etc • Cat trapping in bushland areas – educate community to assist • Benefits of solar power • Bee education and subsidies • Road signage to warn of wildlife crossings |
| 5 | Tree Planting / Preservation |
| | <ul style="list-style-type: none"> • Protect biodiversity • More sustainable public landscapes with canopy coverages • Producing trees – fruits and vegetables • Street plantings to reduce heat island effect • Cockburn Community Wildlife Corridor • Native plant / tree policy • Advocate State and Federal government to save endangered natural habitats • Mandatory native verge plantings on commercial properties • Prevent demolition of existing healthy trees in new masterplanned communities • Stronger leadership to retain significant trees, bushland, vegetation |
| 6 | Walkable Active Transport |
| | <ul style="list-style-type: none"> • Bold action to promote public transport, not more roads to support cars • Incentives to use public transport and alternative active transport options • More dedicated bike paths and cycle connections • More pedestrian footpaths |



Table 7: Ideas to support local businesses prosper in a world transitioning to a low emissions economy

| # | Theme |
|---|--|
| 1 | Rebates / Incentives |
| | <ul style="list-style-type: none"> • Incentives for commercial landlords to provide solar panels, LED lighting, banning single use plastic, water energy, rain water collection, etc. I.e. Powerledger • Reducing council rates and charges to provide more opportunities to invest in the environment • Encourage solar and wind energy generation • Rewards for being environmentally responsible • Advocate for carbon credits • Promote businesses that are making positive sustainable choices • Assisting to complete sustainable grant applications • Special logo / certification for sustainable businesses – possibly a reduction in rates • Encourage alternative education • Incentives at planning stage of commercial projects to promote sustainability |
| 2 | Educate, Innovate & Communicate |
| | <ul style="list-style-type: none"> • Promote 'living local' so people support local businesses • Sustainable business management • Work together – use our businesses to support each other and encourage to make sustainable choices • Education and financial advice to support sustainability • Green waste management • Energy audits • Promote energy storage • Practical cost saving options, which does not cost a lot of money • Mentoring, grants demonstration 'what's in it for me' • Engagement and awareness support • Access to training and facilities • Inspire local businesses to innovate with sustainable technology i.e. competition • Help identify opportunities and resources with supply chains and materials • Identify circular economies between businesses • Composting for hospitality businesses |

Additional Comments:

- Sustainability is not a local government issue, concentrate on core services – water, waste, roads
- Stop developing foreshore areas
- State government issue
- Business require a reliable alternative power to coal, either natural gas or nuclear.
- Don't spend more ratepayers money on this
- Don't regulate businesses, it destroys investment and motivation
- No more food trucks from outside City
- Be a world leader in sustainability
- Solar smart roads
- Showing better support and education for staff to make better sustainable decisions.
- Join other neighbouring councils in carbon neutral activities
- Localised energy networks
- Encourage innovation such as 3D printing, robotic manufacturing, etc.



Table 8: Ideas to support residents and families reduce their energy use and build resilience

| # | Theme |
|---|--|
| 1 | <p style="text-align: center;">Rebates / Incentives</p> <ul style="list-style-type: none"> • Incentives for solar power and vote for electrical power suppliers to be brought into WA • Reduce rates and promote residents to invest in sustainability • Sponsor solar batteries • Subsidise double windows scheme and other insulation measures • Relaxation of rules regarding greywater, tiny homes, alternative energy production, etc • Free energy audits • Incentives for rain water harvesting, native plantings, reducing waste, recycling, roof painting, retrofitting houses, LED lighting, etc • Smarter public landscapes and tree canopy coverage • Encourage local food production and community 'energy' hubs • Encourage transition to a plant based diet • Support programs such as living smart and assist low income residents with solar installations and energy saving mechanisms • Mandatory sustainable housing for new development |
| 2 | <p style="text-align: center;">Educate, Innovate & Communicate</p> <ul style="list-style-type: none"> • Education regarding sustainable building design and sustainable home builders • Household challenges / competitions • Demonstrate energy efficient products. Trade in old products • Workshops for educating residents • More programs, education and presence at events • Open houses, education at schools, etc • Battery, printer and ewaste stations should be at all schools • Community fun days to collect rubbish, etc • Smart meters and accessible data to track energy use and consumption • Energy Audits and education • On-going communications in local newspapers, social media, etc • Quick and easy tips for residents. Improved communications for how to use 3 bin system, etc • Testing new innovative ideas to engage and educate • Facilitate community connections i.e. farmers market • Reward households doing the right thing • Participatory frameworks and involve community in managing common resources • Encourage more residents to plant trees and consider a community 'street tree ambassador' • Support community urban farms • Promote renewable energy sources – i.e. hydroelectricity, wind farms, etc |
| 3 | <p style="text-align: center;">Stronger Sustainable Regulation</p> <ul style="list-style-type: none"> • Ensure all buildings are atleast 6 stars NatHERS for design, construction and on-going maintenance • Audit all homes and provide incentives • Advocate for stronger building regulations to State and Federal government • Mandate energy efficiency for council buildings and households • Also remain sensible with future policies and regulation |



Additional Comments

- Not a local government issue. Residents and families should be able to make their own decisions.
- The council should set the example and buy into sustainable energy sources and practises. Show the community how it can be done.
- The market should govern incentives and sustainable product pricing.

Table 9: What is your wish for a better tomorrow?

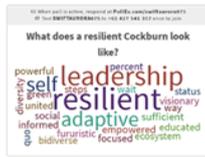
| # | Theme |
|---|---|
| 1 | Meeting the challenges for the next Generation |
| | <ul style="list-style-type: none"> • Living life sustainable, everyone, everywhere • People understanding sustainability, young and old • For my grandson to have a clean environment • Looking after our wildlife • Greener and cooler suburbs – reducing road widths, renewable energy, electric vehicles, community gardens, stormwater management and more sustainable public open space • Increased action against carbon emitters and general waste • Community resilience • Affordable clean energy • All of government takes the climate seriously • Low energy prices • Less pollution of the ocean • Community battery hubs • We belong to the land; the land does not belong to us. I wish that we could live like this • Investment in sustainable projects that people will benefit, feel comfortable and not cause financial hardship |
| 2 | Improved Well-Being |
| | <ul style="list-style-type: none"> • Better safety and well-being • A more respectful world • More bringing neighbours together • Less pollution, less stress, and people that look out for each other • Working together as a community • Not driven by politics • Real kindness and connection to land • Reduction in levels of inequality within our society • A community working and living together with our environment in balance |
| 3 | Financial Security |
| | <ul style="list-style-type: none"> • To be employed, healthy and financially secure. • More incentives so sustainability is affordable for everyone. |
| 4 | Traditional Council Values |
| | <ul style="list-style-type: none"> • Get back to being a real council • Less government initiatives to control peoples lives. Provide essential services, do them well, be polite and respectful • Listen to the people who live in Cockburn • Brighter street lighting to encourage more community activities |



Feedback from the City of Cockburn Staff Workshop

The engagement included a virtual workshop targeted at City administration staff on Monday 6 April 2020.

Word Cloud 2: What does a better tomorrow look like in Cockburn?



Responses

- social Visionary futuristic United ecosystem focused
- Informed Resilient Renewable Leadership Educated
- Diversity and bidiverse leadership empowering responsible
- Strong community connection Adaptive Taking powerful steps
- reliance Environmentally friendly interconnected carbon neutral
- Cooler resilient Self sufficient empowered Pivot Education
- healthy communication Adaptive resilient green
- Substainable Collaborative self sufficient thriving leadership
- empowered independent Biodiverse No status quo
- behaviour change Neutral wellbeing 100y percent renewable
- Adaptive prosperous Self sufficient good science communication
- Proactive Leading the way Responsive interconnected
- community empowered strong partnerships Future proof
- biodiverse
- Anticipate and and proactively deal with challenges, not just wait for them to happen
- Strong sustainable

Where should Council focus its efforts to reduce energy use and carbon emissions? Prioritise top 3

1. Leadership i.e. strong emission reduction targets
2. Zero waste to landfill
3. 100% renewable electricity



Graph 4: How aspirational should the approach be to creating a sustainable and climate resilient future?



What are your adaption and resilience objectives?

1. Environment conservation to maintain biodiversity
2. Inform the community to help them adapt to Climate Change impacts
3. Coastal retreat against sea level rise and erosion
4. Water management to maintain key parklands and streetscapes
5. Reduce the heat island effect by planting more street trees

Graph 5: Should the City of Cockburn focus its efforts on reducing community and industry efforts?

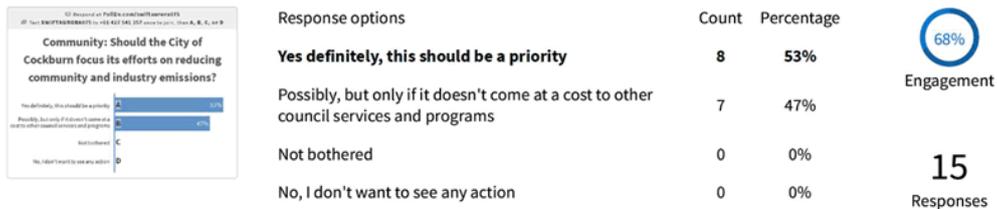


Table 10: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| |
|--|
| <p>Leadership by City of Cockburn</p> <p>Policy</p> <ul style="list-style-type: none"> • Create a target for the City to be a state leader. • Street Tree policy which outlines defined rules to be applied across the entire city. • Targets set and financial support to meet targets. Be strong leaders to action and drive change. • Providing a positive response and being positive about the future • Going beyond regulations - setting new benchmarks • Provide leadership and direction • Practical actions such as conserving water • Lack of policy direction is creating loopholes that residents and developers are able taking advantage of and avoiding taking real action. Also, a lack of enforcement of regulations. A committed and defined set of consequences for rules broken may act as a deterrent. • Executive management to implement or action policies or directions. • Policy - reviewing local planning policies • Create a unified departmental approach to climate change actions, between all state government departments. |
|--|

- Trial options and new initiatives to understand what works best in Cockburn
- Set realistic targets which are achievable,
- Create deterrents to people doing the wrong thing.
- Policy & State Government advocacy
- Improving our Regional partnerships - Cockburn Sound Coastal Alliance
- Legislation needs to be updated to meet current demands and needs
- Advocate for change to address some of those issues. Urban built form has a huge impact on the fossil fuels extraction.

Funding

- Allocating adequate funding and resources
- Balancing and prioritising budget. Trade-offs for a more sustainable environment. Understanding where the community believes the balance should be too. i.e. participatory budgets to discuss climate initiatives.

Team Work

- Empowering and educating staff
- Demonstrating strong leadership – walking the talk
- Bring community along with us and help them undertake action at home
- Show community how they can be involved
- Community behaviour change, science communication and strong communication through education. Be inclusive about who we consult with on this top including CALD communities.
- Unified approach across each departmental with a dedicated officer to facilitate and monitor actions.
- Councillors can be a challenge to communicate climate change. We need more education for councillors.
- Educate technical officers at the City with regard to the climate change actions. Often the City staff are not informed.
- Important to have diversity of engagement. Understand everyone's perspective, including minority groups. i.e. Disability reference group, children's reference group.
- Consistent leadership and communications
- Making the environment a real priority
- Identify behaviour change and tools which will enable these changes to occur.

Operations

- We operate a landfill which is 60% of emissions. Landfill is a big challenge.
- Loss of biodiversity due to greenfield development - Our staff do not have the tools or regulatory power to inform planning issues. Policy does not address biodiversity.
- Large vehicle fleet - how can we offset? - improvements in policy - where does the organization see itself? - what we buy - the life cycle cost of fleet - how do we be leaders in this area. Too many utes.
- Dedicate a staff member to drive and monitor actions and impact on climate change.
- Appoint an urban change officer.
- Reinstate the climate change officer position and have that team really driving the message that we are leaders in this area.
- The city needs to work more strongly with our building sector.

22



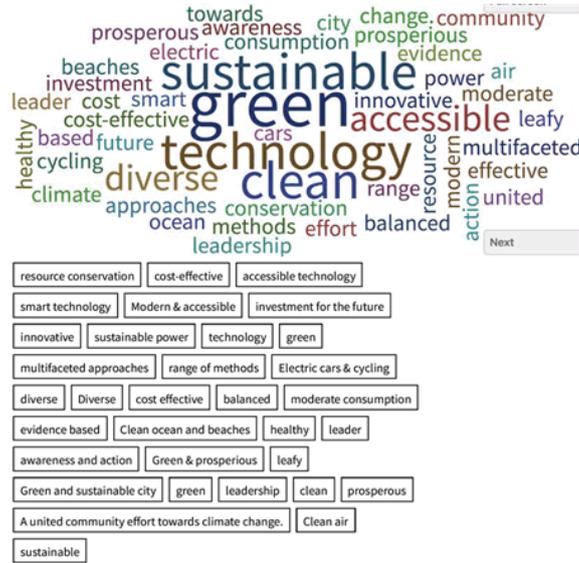
| | |
|---|---|
| <ul style="list-style-type: none"> • Drive better community outcomes in relation to the built environment. How we design and source products. • Street lighting - the cost of street lighting is a huge impact on the City. Needs to be a real change of direction. Needs to be addressed by State Government also. Need to collaborate. Needs to be more sustainable. Particularly as we grow our night-time sporting activities. | |
| <p>Challenges</p> <p><u>Coast and Water</u></p> <ul style="list-style-type: none"> • Coastal vulnerability • Water availability is going to be a restraint moving forward. • Limited amount of ground water for public open space and street trees. Scheme water system is going to be restrained by growth. • Loss of water also impacting on our wetlands also. <p><u>Trees and Vegetation</u></p> <ul style="list-style-type: none"> • Lack of street trees impacting public spaces. • Walkability - urban heat - urban forest strategy - particular in greenfield developments. • The prevalence of bushfires - what learnings can we take from other regions and how can we apply them here? How do we best manage the bushfires going forward? <p><u>Other</u></p> <ul style="list-style-type: none"> • The impact of cultural and ecotourism sectors. Unique culture and environment. How can we better protect and then better capitalise? This is a missing link. Tend to look at issues in isolation. • Cultural and environmental uniqueness of Cockburn is something that could be very much capitalised from an economic perspective. | <p>Other Opportunities</p> <ul style="list-style-type: none"> • Always the first thing we discuss. All levels of the organization. More visible. • Take some risks and INNOVATE. • Using more recycled materials in infrastructure projects (i.e. roads and asphalt) • Hamilton Hill High School redevelopment is recycling materials wherever possible. A good demonstration project • Sustainability grants • Park furniture |



Feedback from the City of Cockburn Disability Reference Group Workshop

The engagement included a virtual workshop with the City's Disability Reference Group (DRG) on Monday 4 May 2020.

Word Cloud 3: What does a better tomorrow look like in Cockburn?



Word Cloud 4: What should sustainability leadership look like at the City of Cockburn?

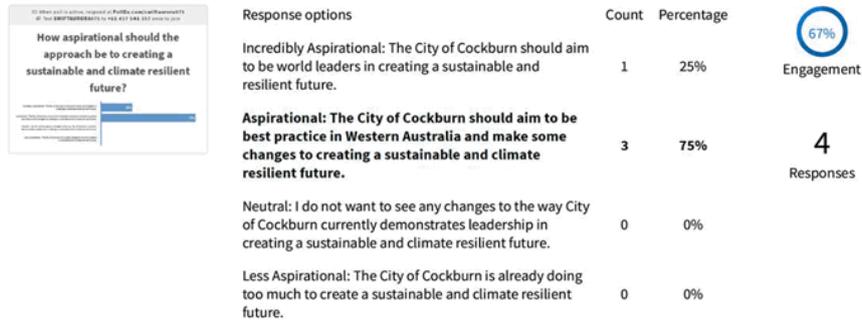


Where should Council focus its efforts to reduce energy use and carbon emissions?
Prioritise top 3

1. Leadership i.e. strong emission targets
2. Reducing energy consumption and operational costs
2. 100% LED street lighting



Graph 6: How aspirational should the approach be to creating a sustainable and climate resilient future?



Aspirations Summary:

- There is a danger in being too aspirational. Need to remain clear and focused.
- Actions need to remain practical and able to be implemented

Table 11: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| General Comments |
|---|
| <ul style="list-style-type: none"> • How much of climate change efforts have discussed real project life financial costs of initiatives / implementation? • Let's make sure the projects we are implementing are creating the intended climate change impact and not just causing more energy consumption in another field. Think about the direct and indirect costs. • I would like to know more about the choices we make regarding logical household and industry consumption • I value the trees and wildlife in Cockburn and protection of the environment. • We need to do a better job of considering the environment when implementing new infrastructure, greenfield suburbs, etc. • How does individual household consumption impact collective action? • This is still a new topic for me. I'm still learning and want to know more. I'm interested in household consumption. Are our efforts really making a difference? • I would like to see more tree planting – if it make a positive difference • More sustainability education • What is industry and business contribution? • I would like to see a move to electric cars • I appreciate the natural environment. |

| Challenges | Challenges for People with a Disability |
|---|--|
| <ul style="list-style-type: none"> • Expense – Cost • Communications – messaging – need a strong and clear direction • How we frame our approach – what is our approach? Sustainability? It's seems a bit unclear – need a strong purpose • What's 'our' contribution vs natural change • Creating a clean society – drop the debates, we all want a clean environment – moving forward • Knowledge and Education • Household renewable and consumption – providing pathways for residents <ul style="list-style-type: none"> ○ How to deal with waste ○ These are the true catalysts for behavioural change ○ Where do we start in our households? • Lack of awareness of accurate and objective data – help us determine factual knowledge and cost of suggested solutions • Being aware of direct and indirect costs • We need a strong focus and priority towards climate change and meeting agendas • What is our problem? What is our purpose • Moving past the 'blame game' • There seems to be 'other' forces not in line with sustainability – i.e. development, infrastructure, etc – need to remain forward thinking • All industries need to be 'at the table' and buy-in | <ul style="list-style-type: none"> • Affordability of action – a lot of new technologies can be too expensive – disability pension can be difficult to undertake economic decisions • Subsidies of programs can help overcome these challenges • Accessible information for people with disabilities – empower community to support sustainability • Reliable information, research – better communications • Be mindful not to unintentionally disadvantage people with disabilities – i.e. new technologies and infrastructure • Inclusion – everyone is included in the sustainability agenda – not leaving anyone behind in decision making • NDIS providers / plans could be improved to help with independence, waste and better education • Resources for NDIS to include in efforts and strategies • Included for support workers – daily shopping – people more aware will help make better choices in the way we shop |



Feedback from the City of Cockburn Children Reference Group Workshop

The engagement included a virtual workshop with the City's Children Reference Group (DRG) on Monday 4 May 2020.

What are you already doing and learning about to protect the environment?

- Recycling
- No Smoking or littering
- Compost – use to make fertiliser in the garden and helps grow veggies
- Wind Farm – need more wind farms
- Take shorter showers
- Being vegetarian – good because you grow more veggies – don't harm animals and eat processed meat
- Greywater – recycle our water and give to plants to make food
- Donate all our clothes and toys to op-shops



What should the future of our environment look like?

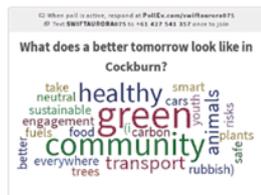
- Use verges to plant more veggies
- More solar panels and wind farms – help power during shortages
- Electric cars – less cars, more people
- Creating more water bins to collect and reuse water
- Using car parks for solar panels and bigger trees
- Planting a new tree every time an old tree is cut down
- More signage to recycle and not use plastic
- No plastic bags – more reusable bags
- Protect the environment with signs
- No littering
- Water tank
- Battery – power for 10 days
- Kids can grow and learn how to grow veggies at school – don't need to go to shops
- The most important thing to do now is to stop pollution. Stop emissions.
- No more toys with happy meals or with purchases.



Feedback from the City of Cockburn Youth Advisory Committee Workshop

The engagement included a virtual workshop with the City's Youth Advisory Committee on Monday 6 May 2020.

Word Cloud 5: What does a better tomorrow look like in Cockburn?

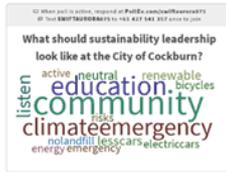


Responses

- animals transport food sustainable youth
- plants and animals everywhere community renewable healthy
- carbon neutral empowerment healthy green engagement
- community green green green fuels for cars (i.e. rubbish)
- community smart
- More connected. More healthy. More innovative. More trees. Better transport.
- community engagement take risks safe communicate green
- innovative



Word Cloud 6: What should sustainability leadership look like at the City of Cockburn?



Responses

| | | | | |
|------------------------|---------------------|-----------------------|------------------|----------------|
| nolandfill | bicycles | active in community | lesscars | education |
| education | electriccars | listening | ecodiversity | lesscars |
| collaborativeeducation | climateemergency | climateemergency | | |
| diversity | 100%renewableenergy | climateemergency | renewable | |
| climate emergency | solarpanels | 100% renewable energy | | |
| carbon neutral | carbon neutral | youth | taking risks | humble |
| education | biodiversity | accountability | consultation | |
| environment | diversity | listen | shared decisions | accountability |
| community | engagement | engagement | asking | accountability |
| community | listen to youth | consultation | accountability | funding |
| listening to community | | | | |

100%

Engagement

45

Responses

Graph 7: How aspirational should the approach be to creating a sustainable and climate resilient future?



Response options

Incredibly Aspirational: The City of Cockburn should aim to be world leaders in creating a sustainable and resilient future.

Count Percentage

1 25%

Aspirational: The City of Cockburn should aim to be best practice in Western Australia and make some changes to creating a sustainable and climate resilient future.

3 75%

Neutral: I do not want to see any changes to the way City of Cockburn currently demonstrates leadership in creating a sustainable and climate resilient future.

0 0%

Less Aspirational: The City of Cockburn is already doing too much to create a sustainable and climate resilient future.

0 0%

57%

Engagement

4

Responses

Table 12: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| General Comments | Opportunities |
|---|---|
| <ul style="list-style-type: none"> Investigate more innovative methods for communicating and engaging with young people – and more frequently I work at a restaurant and we through away a lot of excessive waste and single-use products – this disturbs me, because a lot of these products are so easy to change | <ul style="list-style-type: none"> Better communication with young people Councillors really do listen and we enjoy them coming to our meetings More informal catch ups and young people-led events – Freedom of Speech Capturing education during middle school years – working with school in |

| | |
|--|--|
| <ul style="list-style-type: none"> ○ It's not something that costs a lot of money, it just requires a lot of community effort ● I feel it's my duty to help build a positive and resilient future and make a positive difference ● Water reduction initiatives should extend to the wider community ● Climate Change & Sustainability initiatives also creates more beautiful places | <p>more hardcore ways – make it fun, awesome, and innovative</p> <ul style="list-style-type: none"> ● Create a program for young people that are not able to attend today's session – continuing the conversation ● I would like to know more about government, their role in creating positive change ● More surveys and on-going discussions about sustainability |
| <p>Challenges</p> <ul style="list-style-type: none"> ● Different stakeholders with different opinions – balance different aspects and competing priorities / issues ● Affordability ● Awareness – climate change deniers ● Education should ensure facts are communicated ● People need to understand the consequences of doing nothing ● How do we prevent catastrophes ● More information about volunteering, being positive and information about political parties environmentally aligned – on the agenda ● Finance and cost of everything ● This is an emergency and should be put forward in budgets ● Getting the whole community engaged ● A real resistance to take risks – to be innovative we need to take risks ● Councillors and council can learn from young people to take more risks – the community needs to allow the council to take the risks ● Precedent that council is all powerful and can't make mistakes – it's okay to make mistakes and it's so important – we (youth) need to know that it's okay to make mistakes | <p>Challenges for Young People</p> <ul style="list-style-type: none"> ● There is a diverse range of young people – obstacles are sometimes there own situations – education issues, family issues, accessing needs ● Communities within communities with diverse needs – some communities environment is not a priority, because they don't have safe place to live, eat and study ● Young people don't get as much say as other people – engagement like this is great to have their say but not many young people are aware of these platforms – more promotion at school and social media ● Need to empower youth – we are very passionate – youth also most passionate about climate change – Imagine what we could do if we were empowered to do more and have a more positive impact on sustainability ● Need to say 'Yes, you are being listened to. You do have an active role in creating a sustainable future'. ● Reassurance they can engage in community events – planting trees, have more of a say in these conversations ● Young people dont necessarily know what local government can do and how young people can influence decision making |



Where should Council focus its efforts to reduce energy use and carbon emissions?

Prioritise top 3

1. Zero waste to landfill
2. Education to help reduce community emissions
3. 100% renewable electricity
4. 100% LED street lighting

Which of the following adaptation and resilience objectives are most important to you?

Prioritise top 3

1. Manage coastal retreat
2. Protecting the coastline against sea level rise and erosion
3. Emergency management planning to limit adverse effects on the community
4. Water management to maintain key parklands and streetscapes
5. Water reduction in council facilities
6. Environmental conservation to maintain biodiversity
7. Increase community resilience to the health impacts of a changing climate

Note: 2-7 were all chosen equal amount of times.

Additional General Feedback

- Investigate more innovative methods for communicating and engaging with young people – and more frequently
- I work at a restaurant and we through away a lot of excessive waste and single-use products – this disturbs me, because a lot of these products are so easy to change
 - It's not something that costs a lot of money, it just requires a lot of community effort
- I feel it's my duty to help build a positive and resilient future and make a positive difference
- Water reduction initiatives should extend to the wider community
- Climate Change & Sustainability initiatives also creates more beautiful places

Feedback from the City of Cockburn Business & Industry Stakeholder Workshop

The engagement included a virtual workshop targeted at business and industry stakeholders on Wednesday 20 May 2020.

Table 13: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| | |
|--|--|
| <p>General Comments</p> <ul style="list-style-type: none"> • if we continue looking at cost as a barrier, we are stuck <ul style="list-style-type: none"> ○ For our business (organic collective), environmental sustainability is core ○ We take on the expense of experimenting with new practices and ideas ○ i.e. Running vehicles on recycled veg oil ○ We've always used recycled boxes for delivery – concerned about health during COVID • Partnered with trillion trees and offset material use • Quick wins to build confidence and momentum <ul style="list-style-type: none"> ○ Greening the city and planting more trees ○ Tips and fact sheets for residents and businesses • More LED street lighting | |
| <p>Challenges</p> <p>Cost and Education</p> <ul style="list-style-type: none"> • Sustainability is not being followed through in construction industry • Builders are either unaware of sustainability products or they are too expensive to use • Also a perception that sustainability is going to be cost prohibitive • Businesses are often time poor, so education will need to be more carefully considered and innovative. • Sustainability currently feels too complicated. Nothing is going to happen unless people have something they can actually do. • City needs to break this down and make it more accessible. • Current workshops are only being attended by converted. Possibly consider incentives for attending workshops <p>Policy: Construction & Urban Planning</p> <ul style="list-style-type: none"> • Sustainability not a priority for new development | <p>Opportunities</p> <p>Leadership Business Network</p> <ul style="list-style-type: none"> • Being more pro-active and empowered - Needs to be driven by business, shouldn't wait for someone to tell us what to do • Need to create a business network to support sustainability initiatives • A collective progressive effort • More investment in local sustainable networks such as 'Trillion Trees' • Important to communicate better with businesses – people don't know what they don't know • Need more informed decision making • Often people are interested but don't know what to do next • Council can take lead in workshops and information sessions • Sharing case studies via local paper, website. Celebrate successes. Big or small. <p>Education</p> <ul style="list-style-type: none"> • Stronger collective intention through education – helping people to develop |



| | |
|---|---|
| <ul style="list-style-type: none"> • Need more sustainable architects to inform development regulation before planning • State and Federal Leadership – stronger targets • Not very strong on current enforcement. If it doesn't meet sustainable criteria, it just shouldn't be approved. | <p>a stronger intention – and allow solutions to be developed</p> <ul style="list-style-type: none"> • This will facilitate innovation • Sustainable house day / expo / open house – opportunities to learn and participate in existing events <p>Incentives</p> <ul style="list-style-type: none"> • Sustainable materials on building projects • Germany –Government incentives for insulation – saves on energy usage • Also provided huge boost to construction industry – education – innovative methods • In WA sustainable materials are more expensive because they are not widely used – once more projects start happening and more demand, it will become cheaper – double glazed windows is a great example • Council could create a list of more sustainable products <p>Communications and Branding</p> <ul style="list-style-type: none"> • Rebrand the Recycle Shop at the tip, make cooler and more accessible (remove stigma) • Allow a non-profit or social enterprise to collect, repair, repurpose and sell goods at the tip • Make the "Sustainability Shop" off-site (not at the tip - people don't want to go to the tip) |
|---|---|

In which areas should the City of Cockburn focus their attention to reduce carbon emissions?

1. Zero waste to landfill
2. Education to help reduce community emissions
3. 100% renewable electricity & 100% LED street lighting



Graph 9: How aspirational should the approach be to creating a sustainable and climate resilient future?

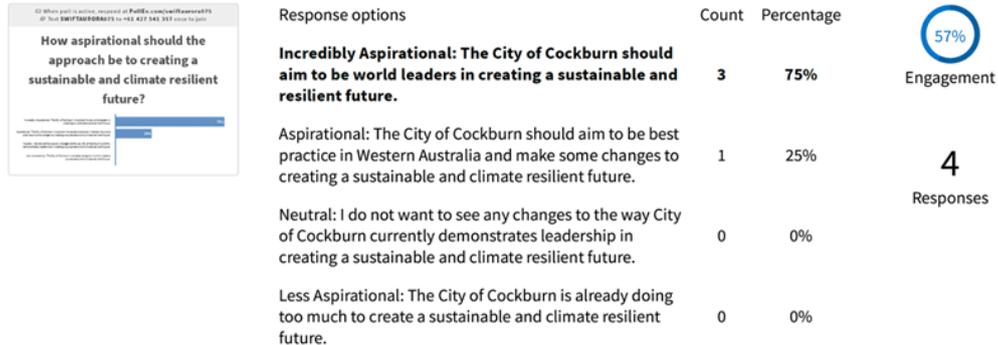


Table 14: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| | |
|--|---|
| <p>General Comments</p> <p>Community crowd funding and investment in battery and energy trading</p> <p>There is opportunities and trials in WA from Western Power and Synergy</p> <p>Need more community buy-in and support</p> <p>Need to advocate for policy and legislation changes to make this more accessible</p> <p>There are small changes and baby steps to get this started – move to LED street lighting</p> <p>Gaps in the Engagement Program and Questions</p> <p>There has not been a focus on social and economic wellbeing</p> <p>I would like to see support for community-based agriculture</p> <p>Focus on engagement has been to minimise impact and not about transitioning to future of agriculture and energy</p> <p>Too much grass and passive spaces – I want to see more trees</p> | |
| <p>General Challenges</p> <p>Leadership</p> <p>Policy & Urban Planning</p> <p>Communications and Branding</p> <ul style="list-style-type: none"> Not enough accessible information on website Need more community members engaged in the topic | <p>Opportunities</p> <p>Walk the Talk</p> <ul style="list-style-type: none"> if you don't live it yourself, it's tough to get others on-board – i.e. CEO's plastic challenge – City of Canning Need to have all elected members on-board – strong leadership – I would like the city to declare a 'climate emergency' – scary term, however reflects we need to be taking action now – we should |



| | |
|--|--|
| <ul style="list-style-type: none"> • More engagement and information to attract further momentum • Some simple visuals regarding or video snapshot of engagement results • Messaging and communications is a barrier – community doesn't really understand what the City is doing • Need more local discussion with each community – use the resident groups to the City's advantage • Create more forums for people to get involved • I'm really proud of the work being completed by the City and I wish it was more visible | <p>take climate as same urgency as COVID</p> <ul style="list-style-type: none"> ○ There is a lot of material for taking community along the journey – Climate Declaration <ul style="list-style-type: none"> • Stronger decision making <p>Community Empowerment</p> <ul style="list-style-type: none"> • A community advocacy group would be great – I would like to see a transition team in Cockburn • People are focused on their own agenda, and not focused enough as a collective • Possibly be a facilitator to support transition town – grants, help where necessary, etc |
|--|--|

In which areas should the City of Cockburn focus their efforts to reduce carbon emissions?

1. Zero Emissions fleet
2. 100% renewable electricity
3. Leadership

Which of the following adaptation and resilience objectives are most important to you?

1. Environmental conservation to maintain biodiversity
2. Protect infrastructure against severe storms, heat and flooding

City of Cockburn Aboriginal Reference Group

The engagement included the City's Aboriginal Reference Group (ARG). Whilst a workshop was unfortunately not able to be organised due to the COVID-19 crisis, phone conversations were facilitated by the City's engagement project team.



Table 15: Opportunities and Challenges for demonstrating Sustainable and Climate Resilience Leadership Discussion

| General Comments | Opportunities |
|---|---|
| <ul style="list-style-type: none"> • Less car and single plastic use – more traditional living off land • More connection with country and being connected in the community • We want to protect nature, animals and beautiful landscape • Continuing our story for taking care of country – next generation to be the caretakers • A better culture and environment – diversity and looking after land • Creating more regenerative environments and learning from Aboriginal culture • Cockburn is leading the way with council-led initiatives and RAP.\ • Let's continue to build on the success of sustainable initiatives at Cockburn | <p>Cultural Education & Innovation</p> <ul style="list-style-type: none"> • Not about reinventing the wheel, Aboriginal culture has so many useful resources for working sustainably with country. • More community training to care for country – there are plenty of Aboriginal programs to share with the City. Programs should tackle multiple demographics, young and old. • Existing cultural training programs need to be better enforced and leaders need to be held accountable for taking it seriously. • Learning should be about cultural education and taught using more immersive methods i.e. visiting cultural sites, bush medicine, controlled burns, etc. • RAP should be considered as part of the strategy • Aboriginals definitely deserve a stronger voice – we have strong views regarding sustainability and education – i.e. telling story of country, learning about native bush tucker foods – and people enjoy learning about it. <p>Sustainable Reference Group</p> <ul style="list-style-type: none"> • Create a reference group with educated community members, not just elected members • Create strong leadership that does not shy away from difficult questions and responses |



| Challenges | Challenges for Aboriginal People |
|---|--|
| <p>Community Leadership</p> <ul style="list-style-type: none"> • Need to create a greater sense of climate urgency – communicating that it's everybody's responsibility • Need to have more trust in our community to take ownership of actions – the City is often very hesitant and conservative • We are not always understanding a holistic perspective. We need to be more inviting and understanding of diverse perspectives, we all have a role to play. <p>Education</p> <ul style="list-style-type: none"> • Not enough education, especially with young people. We need to educate a better way of living. It's devastating what is happening in our environment. I.e. clearing of land, negative impacts on wellbeing, animals, loss of shade, etc. • Allow our young people to be more open to learning and getting involved. • Aboriginal language being used more often • We need to show data with the real impacts of our actions. <p>Communications</p> <ul style="list-style-type: none"> • More cleaning and planting days and better marketing of events • Let us know how to get more involved? How can we help? <p>Existing Leadership</p> <ul style="list-style-type: none"> • I don't always trust our leaders response • Not saying anything and not making decisions can also be wrong • Advocate for stronger state and federal leadership | <ul style="list-style-type: none"> • I feel minority groups are not represented in decision-making. Sometimes our issues are dismissed by certain elected member leaders • Reference groups have diverse representation and can work together to create new innovative ideas • I don't feel the wider community are always informed and educated regarding aboriginal knowledge. We can help change the environment for the better, we have to work together, it's important for everyone's health • Systematic racism is still relevant in Cockburn. Some policies and procedures are not always inclusive and mindful of everyone. • Some staff have diluted our opinions, and have lost the meaning in some of our perspectives. |



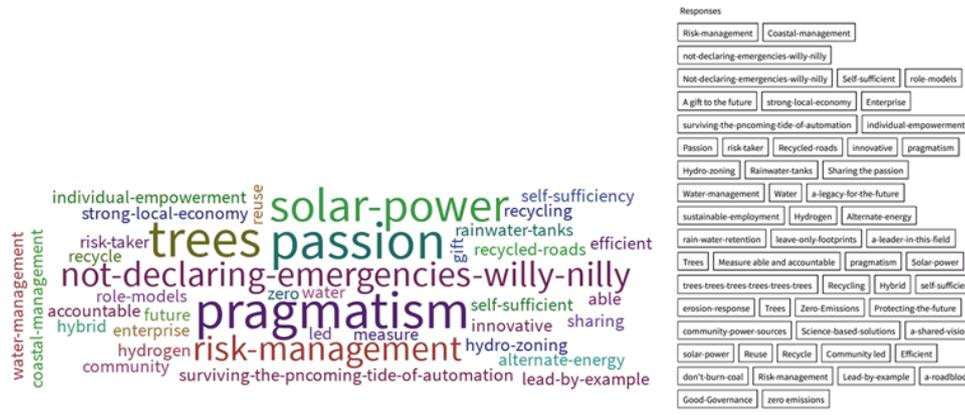
Feedback from the City of Cockburn Elected Members Workshop

The engagement included a virtual workshop targeted at elected members on Thursday 28 May 2020.

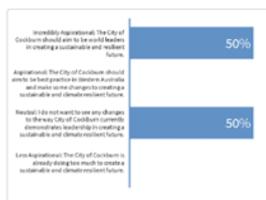
Word Cloud 12: What does a better tomorrow look like in Cockburn?



Word Cloud 13: What should sustainability leadership look like at the City of Cockburn?



Graph 10: How aspirational should the approach be to creating a sustainable and climate resilient future?



| Response options | Count | Percentage |
|--|-------|------------|
| Incredibly Aspirational: The City of Cockburn should aim to be world leaders in creating a sustainable and climate resilient future. | 4 | 50% |
| Aspirational: The City of Cockburn should aim to be best practice in Western Australia and make some changes to creating a sustainable and climate resilient future. | 0 | 0% |
| Neutral: I do not want to see any changes to the way City of Cockburn currently demonstrates leadership in creating a sustainable and climate resilient future. | 4 | 50% |
| Less Aspirational: The City of Cockburn is already doing too much to create a sustainable and climate resilient future. | 0 | 0% |

In which areas should the City of Cockburn focus their efforts to reduce carbon emissions?

1. Zero Emissions Fleet
2. 100% Renewable Electricity
3. Leadership

Which of the following adaptation and resilience objectives are most important to you?

1. Environmental conservation to maintain biodiversity
2. Protect infrastructure against severe storms, heat and flooding

Table 16: Elected members reflected the following themes in the discussion section

| # | Theme |
|---|--|
| 1 | <p>Communications and Engagement</p> <ul style="list-style-type: none"> • Defining "sustainability" and "climate resilience" is important to start the conversation, as councillors had different interpretations and meanings, sometimes at odds and unsure where the shared path was • Sharing knowledge and understanding throughout the community was a key aim that most councillors voiced, closing the knowledge gap <ol style="list-style-type: none"> 1. Sharing without 'looking down' on those who don't know or don't understand 2. Sharing knowledge from Subject Matter Experts 3. Reduce/remove emotion from dialogue 4. Invest appropriate resources to achieve grassroots movement and collective understanding • Community-led initiatives are viable • Speak to whole of community in communications – the people who might not 'get it' or have the resources (time, money) to easily participate in initiatives |

| | |
|---|--|
| 2 | Legislation and Infrastructure |
| | <ul style="list-style-type: none"> • Coordinated response across Local, State and Federal government to reduce wasted effort and spending • Reduction of Red Tape and EPA • Urban infill policies, tree requirements, public open space • Infrastructure for the future • Infrastructure that support native flora and fauna, wildlife, erosion |
| 3 | Cost Vs Impact |
| | <ul style="list-style-type: none"> • What is the best bang for buck? • Technology – when to utilise, viability vs cost • What are the upfront costs, and what do we have to stomach now for future benefits? |
| 4 | Resources |
| | <ul style="list-style-type: none"> • Don't need to reinvent the wheel • Look to UN Sustainability Goals, Local Gov NSW and other local councils for resources |
| 5 | Action Now and Later |
| | <ul style="list-style-type: none"> • Getting started is the hard part, taking the first step • Is the council the main blocker? • Start now, but have a vision for 15-20 years down the track |

Additional Notes:

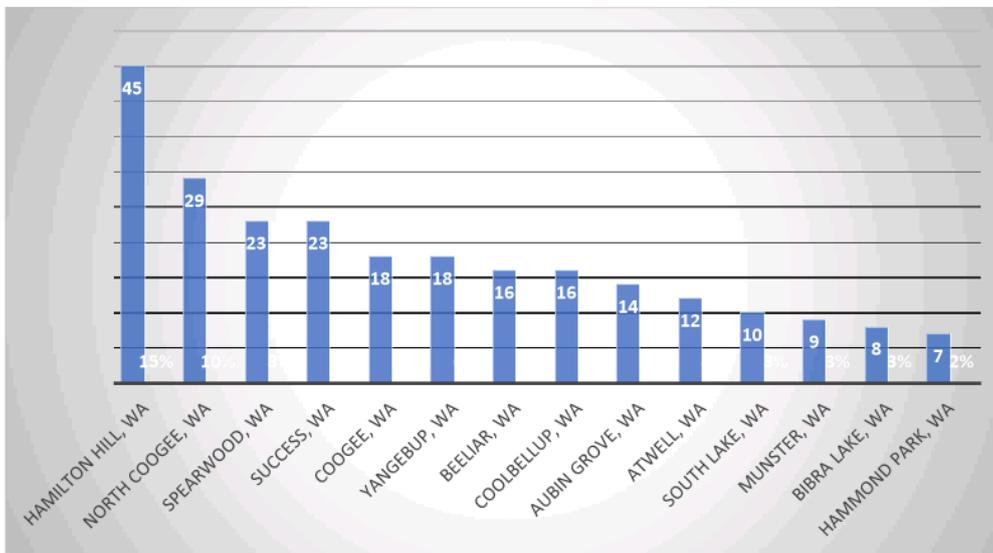
One councillor also noted that the sustainability work council staff have done to date is excellent, which resulted in a collective moment of pride and positive comments from councillors.

7. About the Community Survey Respondents

The following tables highlight demographics and spatial information regarding our survey respondents.



Graph 11: Suburbs of Survey Respondents



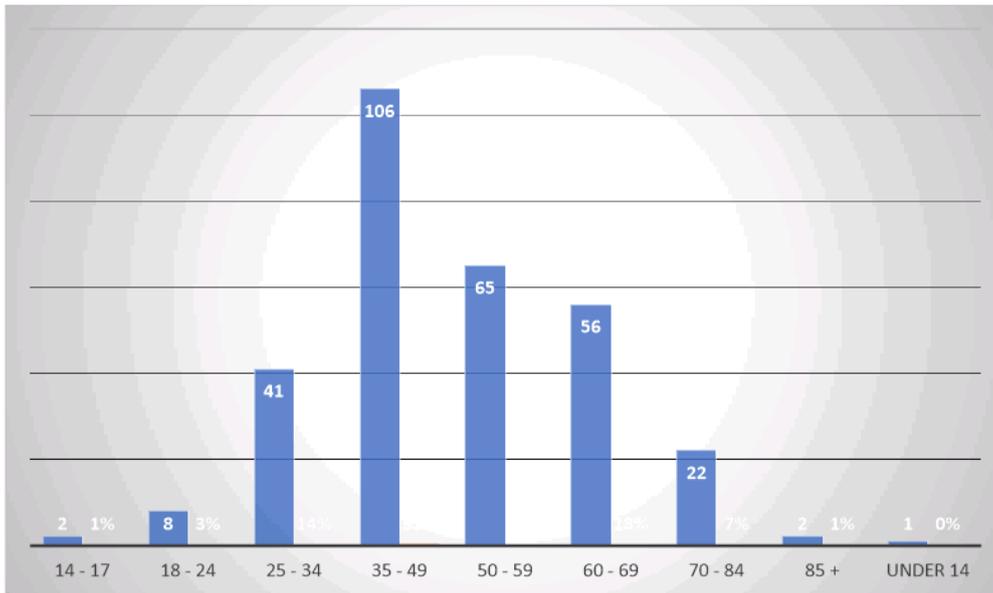
Key Findings:

- Hamilton Hill had the most survey respondents with 45 or 15% of the total respondents.
- 20 additional suburbs recorded 5 or less respondents (equal or less than 1%) including Cockburn Central, Jandakot, North Lake, Treeby, Banjup, Leeming, Beaconsfield, Wandi, Wattleup, Ashfield, Bertram, Bibra Lake, Booragoon, Fremantle, Henderson, Lake Coogee, Manning, Menora, Scarborough, White Gum Valley.

Table 17: Gender of Survey Respondents

| GENDER | | |
|----------------------|------------|-------------|
| Alternative identity | 1 | 0% |
| Female | 170 | 56% |
| Male | 125 | 41% |
| Prefer not to say | 6 | 2% |
| Grand Total | 302 | 100% |

Graph 12: Age of Survey Respondents



Key Findings:

- The most common age of survey respondents was 35-49 with 106 or 35% of respondents and 50-59 with 65 or 21% of respondents.
- Only 18% of respondents recorded an age under 35.

Table 18: Survey Respondents Relationship to Cockburn

| RELATIONSHIP TO COCKBURN | | |
|--|------------|----------------|
| I live in Cockburn, I own property in Cockburn | 115 | 39% |
| I live in Cockburn | 105 | 35% |
| I own property in Cockburn | 23 | 8% |
| I live in Cockburn, I own property in Cockburn, I work in Cockburn | 18 | 6% |
| I live in Cockburn, I own property in Cockburn, I own/operate a business in Cockburn | 16 | 5% |
| I work in Cockburn | 13 | 4% |
| I live in Cockburn, I work in Cockburn | 7 | 2% |
| Grand Total | 297 | 100.00% |

Key Findings:

- A majority of respondents (74%) are homeowners and/or live in Cockburn.
- 17% of respondents own/operate a business or work in Cockburn.



8. Email Submissions

9 email submissions were received during the open public comment period. Below is a summary of the key points.

Department of Water and Environmental Regulation (DWER):

- DWER is committed to working with all sectors of WA economy towards achieving net zero greenhouse gas emissions by 2050 and 28% by 2030.
- DWER is currently preparing the State Climate Policy, and will present a whole of government policy framework to address climate resilience and support the low carbon transition
- Department is committed to assisting the City to secure necessary water supplies, implement water use efficiencies, support further development, parks and recreational facilities.
- Department recommends seeking opportunity through the future Climate Change Strategy to achieve the following neighbourhood design outcomes inclusive of:
 - Trees and plant species within POS and street verges to maximise cooling;
 - Retention of native vegetation areas;
 - Hydro-zoning of parks to best optimise water resources
 - Opportunities to utilise alternative water sources in new developments;
 - Best use of drainage water for recharge and the environment;
 - Conversion of traditional drainage infrastructure to spaces or assets that achieve both water quality and amenity outcomes;
 - Combining urban cooling assets with drainage infrastructure e.g. street trees designed as tree pits to treat stormwater runoff.

Resident Comments:

- Engage in strong and well-established strategies to protect the environment, such as:
 - Planning guidelines for new development to stipulate green practises
 - Require vegetation for new development
 - Developers responsible for two years to maintain landscaping
 - Provide all residents with a new compostable green kitchen waste bin
 - Increase density coverage of ground cover plants
 - Increase tree planting by 50% of crown land
- Tree planting on all street verge in front of all houses
- Convert the fireworks show in Manning Park by a sustainable light show instead. Also, that the event take place further away from animal habitats
- Stronger protection legislation for large old tuart trees on private land. These trees are irreplaceable habitat for native spaces and provide significant cooling. A large amount of tree canopy has been lost already due to recent development.

9. Additional Engagement Regarding Climate Resilience and Sustainability

CY O'Connor Beach Coastal Erosion Management Engagement:

What we did: The City is developing an erosion mitigation strategy for the protection of C.Y. O'Connor Beach. Before embarking on a design (there are a number of ways this could be carried out), the City sought to understand the community's aspirations for the future of C.Y. O'Connor Beach and their perspective on the issue.

How: We engaged in a variety of ways, including pop-ups, on-site workshop, submissions (and survey). Attached is a snapshot of the results.

Why relevant to this project: This feedback is relevant to the objective of "Adapting to the effects of sea level rise."

Additional Engagement Summary Attached: CY O'Connor / Consultation results infographic.jpg

Foreshore Management Plan for Coogee Beach:

What we did: The City has a commitment to develop a Foreshore Management Plan for Coogee Beach foreshore. The Management Plan considers how we will manage the foreshore and adapt to risks in the long term. To do so, we wanted to know how the community use the beach, what they most value and how they would feel about the management of assets in terms of responding to the effects of a receding shoreline through a "managed retreat".

How: We engaged in a variety of ways, including on-site pop ups at the beach, face to face meetings, submissions and a survey.

Why relevant to this project: This feedback is relevant to the objective of "Managed coastal retreat in response to sea level rise"

The Foreshore Management Plan is currently under development.

Community Score Card:

What we did: The City carries out a community score card on different facets to see how well the community think we are doing. One of the questions asked this year was whether the community wants to the City to take more action on Climate Change.

How: This was undertaken by a market research company, who investigated how well we do in a variety of areas.

Why relevant to this project: This feedback is relevant as it indicates if climate leadership and action is supported by the community.

Additional Engagement Summary Attached: MarketScoreCard.PNG



10. Engagement Summary

The comprehensive community engagement process undertaken for this project, which included innovative online engagement, face to face meetings, emails, letters, workshops, phone interviews and a hard copy and online survey open to all residents. The engagement process was open for all ratepayers for a 28-day comment period between 1 May 2020 and 28 May 2020.

Over 2000 people visited the project page to learn more about the project and how they could get involved.

In total, 309 completed surveys and 9 email submissions were received, which is considered an good sample size for analysing the sentiment and needs of the City's population.

In addition to general information being sent to residents and businesses, the following specific groups were engaged;

- Youth Advisory Group
- Business and Industry Groups and organisations
- Children's Reference Group
- Aboriginal Reference Group
- Disabilities Reference Group
- Community and Stakeholder Groups
- City of Cockburn Staff
- Elected Members

The following is a key summary of the survey engagement results:

Community respondents have stated:

- Definitive support for reducing carbon emissions as a priority (for all emitters)
- An aspirational to incredibly aspirational approach for creating a more climate resilient and sustainable future in Cockburn
- The following top 5 areas to focus reducing carbon emissions:
 1. Education to help reduce community emissions
 2. Reducing energy consumption and operational costs
 3. Leadership (i.e. strong emission reduction targets)
 4. Zero emissions fleet
 5. Zero waste to landfill
- The most important adaptation and resilience objectives are:
 1. Inform the community to help them adapt to impacts of a changing climate
 2. Reducing the heat island effect by planting more trees
 3. Water management to maintain key parklands and streetscapes
 4. Bushfire management planning to limit adverse effects on the community
- In favour of all recommended greenhouse gas emission reduction targets
- Only 44% are aware of any current sustainability initiatives in Cockburn
- The most popular current initiatives to continue are:
 1. Green waste verge collections
 2. 3 bin household waste system
 3. Bird bath
 4. Native planting

5. Waterwise verge
 6. Rebates and subsidies
 7. Rooftop solar on council buildings
- The following topics proposed for new initiatives:
 - Recycling
 - Reduce emissions / alternative power
 - Offer rebates
 - Education
 - Tree planting / preservation
 - Active transport
 - The following topics to support local businesses proper in a world transitioning to a low emissions economy:
 - Rebates / incentives
 - Education, innovation and communication
 - The following topics to support residents and families reduce their energy use and build resilience:
 - Rebates / incentives
 - Education, innovation and communication
 - Stronger sustainable regulation
 - The following topics for a better tomorrow:
 - Meeting the challenges for the next generation
 - Improved social well-being
 - Financial security
 - Maintain traditional council values

The following is a key summary of the targeted workshop results with the aim of assisting the City demonstrate sustainable and climate resilient leadership:

Current Challenges include:

Environmental Vulnerabilities:

- Understanding the climate vulnerabilities i.e. coastal vulnerability, water availability, bushfires, etc
- Lack of street trees and native vegetation
- Maintaining the cultural and environmental uniqueness of Cockburn

Affordability

- Affordability for all community members to act in a sustainable manner
- Being mindful not to unintentionally disadvantage people with disabilities, at-risk people and minority groups

Lack of Accurate & Accessible Information

- Lack of awareness of accurate and objective data
- How to prevent further catastrophies?
- Available information regarding volunteering
- Getting the entire community engaged

Lack of Sustainable Leadership

- Conservative politicians and lack of sustainable leadership (mostly state and federal leadership)
- Our residents and businesses are often too time poor to attend workshops and engagement events
- Lack of policy enforcement
- Lack of definition

Cultural Engagement

- Lack of connection with country and current aboriginal programs
- Minority groups are not always considered in decision making

Future Opportunities include:

Engagement, Education and Empowerment

- Teamwork to better inform and empower council staff to demonstrate strong leadership
- Rebranding and marketing sustainability
- Community empowerment of sustainably focused organisations
- Cultural innovation, working in better partnership with aboriginal leaders and existing training programs
- Utilising existing resources such as UN Sustainability goals, etc

Strong and Resilient Leadership

- Strong policy to inform sustainable and climate resilient outcomes
- Funding and resources to implement important initiatives, as identified by the community
- Leadership Business Network
- Council should take a lead and 'walk the talk'
- Defining a clear and concise approach and interpretation to demonstrating climate resilience and sustainable leadership

Expanding Existing Resources

- Limiting waste into our landfill
- Subsidies and rates rebates
- Trialling new innovative initiatives with businesses and residents
- Creating more regenerative environments

11. Recommendations

Vision and Values Statement

The City of Cockburn is a leader in climate resilience and sustainability. Our community provides us with the strength and optimism to be aspirational in our approach to creating a better tomorrow for each and every one of us.

We are aware of our environmental impact. We are committed to taking pride in working together, celebrating our success and being resilient and adaptive to our changing climate. We aim to be a true caretaker of land in Cockburn.

The **core values** to include:

- **Working Together:** Leadership, education, creative engagement and strong communications will unite our community and lead us towards a healthy and resilient future.
- **Reducing all Greenhouse Gas Emissions:** A commitment to becoming a leader in reducing greenhouse gas emissions throughout the City, which is supported by our community.
- **Adapting to become an even more Resilient & Sustainable Cockburn:** Strengthening our existing initiatives and trialling new programs and incentives, which support our businesses, council operations and residents to better care for our land and adapt to a changing climate.

Key Recommendation #1: Leadership in action, we walk the talk

The City strengthens its position as a leader in becoming a carbon neutral city by 2030 and implementing sustainable initiatives that make a positive impact to our environment and well-being. This includes consideration and implementation across the City's operations, council buildings, and public land, including native bushlands. The City leads by example and works in partnership with businesses, construction and landscape industries and residents across the State. We walk the talk.

Recommended Deliverables:

- Setting strong targets and reducing carbon emissions as a priority (for all emitters). This includes reducing consumption and net zero carbon emissions for all council buildings and operations
- Reducing energy consumption and operational costs
- Zero emissions fleet
- Zero waste to landfill
- Teamwork to better inform and empower council staff to demonstrate strong leadership
- Implementing strong local policy to inform sustainable and climate resilient outcomes
- Advocating for strong state and federal leadership to inform sustainable and climate resilient outcomes
- Working with the planning, building and construction industries to be more aware of sustainable products and outcomes
- Incentives that assist business and industry adapt to a changing climate and reducing carbon emissions

50

Key Recommendation #2 Education & empowerment, we are all caretakers of our Land

The City continues to expand its educational programs and works with local aboriginal and community organisations to support local programs. Our aim is to work together to create a climate resilient and sustainable city for everyone. This includes a stronger understanding of aboriginal culture, a more involved presence at community events to promote local sustainable issues and stronger communications advocating the facts of our changing climate and what everyone can do to help create a better tomorrow.

The City also continues to work with minority and at-risk groups to ensure no one is left behind. Everyone has the ability to contribute and feel empowered to be a leader in caring for our land.

Recommended Deliverables:

- Education to help reduce community emissions and setting realistic targets.
- Strengthen existing programs, which educate the wider community (including young, old, disabled, etc) to increase recycling, reduce single-use plastic, native plantings, reducing water useage, community gardens, etc.
- Work with existing aboriginal leaders to increase immersive cultural programs that focus on caring for land such as bush tucker foods, caring for animals, controlled burns and understanding the long history of Cockburn (in accordance with the City's Reconciliation Action Plan)
- Engage and empower residents and community members adapt to the impacts of a changing climate
- Continue popular community-focused initiatives such as:
 - Green waste verge collections
 - 3 bin household waste system
 - Bird bath
 - Native planting
 - Waterwise verge
 - Rebates and subsidies
 - Tree planting
- Continue engaging with reference groups and be careful not to unintentionally disadvantage people with disabilities, at-risk people and minority groups

Key Recommendation #3: Adapt & Care, working towards an even more climate resilient and sustainable tomorrow

Develop a program of activities, which focus on the preservation and celebrate the uniqueness of the Cockburn natural environment. Understand the climate is changing and adaptive and responsible measures are required to continue caring for country, including native bushlands, coastal environments, streetscapes and local habitat. We aim to create a future tomorrow the next generation is proud of.

Recommended Deliverables:

- Continue best practise sustainable coastal management to protect our coast from further erosion
- Water management to maintain key parklands and streetscapes
- Bushfire management planning to limit adverse effects on the community
- Utilising existing resources such as UN Sustainability goals, etc
- Trialling new innovative initiatives with businesses and residents
- Creating more regenerative environments

- Investigate rebates and incentive, which promote sustainable development of new and existing households
- Working with the community to reduce the heat island effect by planting more street trees
- Initiatives that create regenerative outcomes for the environment and community

Key Recommendation #4: Define & Communicate

Creating a clear and concise definition of climate resilience and what it means to the City and its community.

Recommended Deliverables:

- Develop an Adaption and Climate Resilient Action Plan. Include a strong definition and direction for inspiring council staff and the community to understanding the meaning of 'climate resilience'
- Report back to the community with outcomes of the engagement program and a clear action framework, as mandated by the community
- Develop strong visuals and a climate resilient and sustainability communications plan aimed at communicating successful programs and encouraging community to get more involved
- Rebranding and marketing sustainability to create a more informed community





City of Cockburn Climate Change Strategy 2020-2030



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Acknowledgement of Country

Cockburn Nyungar moort Beeljar boodja-k kaadadjiny. Koora, yeyi, benang baalap nidja boodja-k kaaradjiny.

Ngalak kaadatj dayin boodja, kep wer malayin. Ngalak kaadatj koora koora wer yeyi ngalang birdiya

City of Cockburn acknowledges the Nyungar people of Beeljar boodja. Long ago, now and in the future they care for country.

We acknowledge a continuing connection to land, waters and culture and pay our respects to the Elders, past, present and emerging



Executive summary

Climate change has significant social, economic and legal implications for local government. Cockburn is already experiencing the effects with increased coastal erosion, higher summer temperatures, more severe heatwaves and a longer bushfire season.

The City has a critical role in responding to climate change through its responsibilities for land use planning, emergency management, ownership of public infrastructure and delivery of community services.

The vision of this strategy is for the City of Cockburn to continue to be a leader in climate resilience and sustainability. We aspire to become a carbon neutral City and commit to working with the community to adapt to our changing climate.

The City sought leadership direction from the community on how to create a sustainable and climate resilient future. The community shared their vision, aspirations and insight on actions we can take for a better tomorrow.

The City has used this vision to develop a resilience roadmap with 14 objectives to achieve carbon neutrality and increase our adaptive capacity.

- | | | | |
|--|---|--|---|
| <p>1. STRONG LEADERSHIP</p> |  | <p>8. EDUCATION AND COLLABORATION</p> |  |
| <p>2. NET ZERO EMISSIONS</p> |  | <p>9. WATERWISE CITY</p> |  |
| <p>3. 100% RENEWABLE ENERGY</p> |  | <p>10. CONSERVE BIODIVERSITY</p> |  |
| <p>4. ZERO WASTE TO LANDFILL</p> |  | <p>11. COASTAL ADAPTATION</p> |  |
| <p>5. REDUCE ENERGY CONSUMPTION</p> |  | <p>12. INCREASE THE URBAN FOREST</p> |  |
| <p>6. ZERO EMISSIONS FLEET</p> |  | <p>13. PROTECT COMMUNITY INFRASTRUCTURE</p> |  |
| <p>7. SMART CITY INNOVATION</p> |  | <p>14. ENHANCE HEALTH AND WELLBEING</p> |  |

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Introduction

The impacts of climate change are already upon us. More frequent and intensive heatwaves, bushfires and coastal erosion are impacting our natural and built environment. These impacts are predicted to increase in the future and pose risks to our infrastructure and communities.

Local governments are on the frontline in addressing climate change and must ensure that adaptation responses are suitably tailored to the specific risks in their area, and that local communities and stakeholders are consulted and involved in adaptation efforts.

An effective response to climate change requires an integrated approach that considers both mitigation and adaptation.

Climate change mitigation includes actions to reduce and remove anthropogenic (human) causes of greenhouse gas emissions.

Climate change adaptation involves taking practical action to reduce risks from climate impacts and responding to those consequences that cannot be avoided.

Climate resilience is being prepared to readily respond and adapt to future uncertainties, and having the capacity to maintain the essential functions and identity of the City.



Image 1: Solar photovoltaic system (1MW) on the Cockburn ARC

Review of achievements to date

The City of Cockburn has a strong, 20 year history in taking decisive and responsible action on climate change.

In 1999, Cockburn joined the Cities for Climate Protection Program (CPP) and undertook its first greenhouse gas inventory. A corporate emissions reduction plan was developed in 2001 to guide the City through the 5 CPP milestones.

In 2009 the City undertook its first climate change risk assessment and developed a Climate Change Action Plan. The Greenhouse Gas Emission Reduction Strategy was adopted in 2011 and inspired another decade of climate action.

This document is the City's first Climate Change Strategy. It addresses both mitigation and adaptation and supersedes the Greenhouse Gas Emission Reduction Strategy and Climate Change Adaptation Plan.

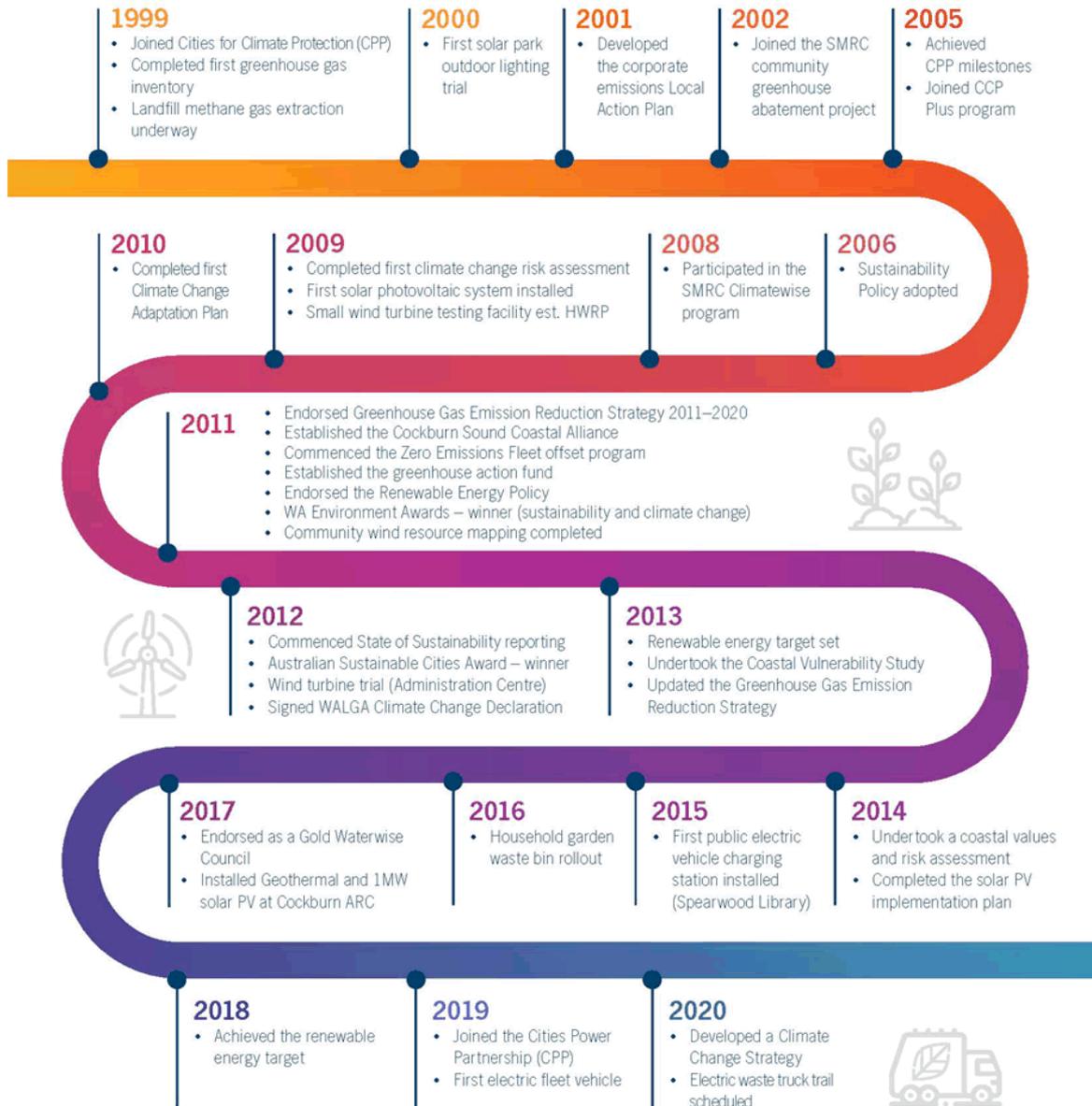


Image 2: The City of Cockburn’s journey of climate change action



Vision

The City of Cockburn will continue to be a leader in climate resilience and sustainability. We aim to be a carbon neutral City and take pride in working together to adapt to our changing climate, now and into the future.

Strategic Community Plan 2020 - 2030

The Climate Change Strategy is aligned with many objectives of the Strategic Community Plan 2020 – 2030 which aims to sustainably manage the City's resources, protect the environment, build capacity and seek collaboration.

The primary climate change objective aims to 'reduce adverse outcomes arising from climate change through planning, adaptation, mitigation, infrastructure and ecological management'.

Sustainability Policy

The Sustainability Policy is the umbrella policy for climate action and includes 11 principles to guide Elected Members and staff in all aspects of decision-making, planning, operations and program delivery.

The 'Climate Change Action' principle outlines the City's commitment to action on climate change, ensuring that mitigation and adaptation actions are equitable and consistent with the aims of the United Nations (UN) sustainable development goals.

The UN sustainable development goals are a universal call to action to end poverty, protect the planet and improve the lives and prospects of everyone, everywhere. The 17 Goals have been adopted by all UN member states, including Australia as part of the 2030 Agenda for Sustainable Development.

Climate change impacts in Cockburn

Climate change is happening now, and its occurrence is supported by extensive scientific research undertaken collectively around the world. There is consensus that our climate has changed as a result of additional greenhouse gas emissions in the atmosphere caused from human activities such as land clearing, agriculture and the burning of fossil fuels.

Under a changing climate¹, it is expected that by the end of this century, Cockburn will experience:

- **Higher temperatures:** Double the number of hot days per annum and heatwaves twice as long
- **Less rainfall:** Dry conditions may be experienced for an additional 2.3 months each year which will significantly reduce groundwater availability.
- **Sea level rise:** 0.9m increase in sea level²
- **More extreme weather:** Increase in the number of high fire weather danger days, and more high-intensity storms.

These changes are likely to pose significant risks to the safety and wellbeing of the community, the natural and built environment and will impact the liveability and amenity of our City. Council will need to plan and adapt to these changes.

Detailed information on the climate models used to predict changes to the City's climate are provided in the *City of Cockburn Climate Change Risk Assessment*, available on the City's website.

The role of local government

The City is responsible for managing the climate risks to its assets (road infrastructure, parks, reserves, public buildings, recreation facilities) and service delivery (waste collection, library and community services, public open space, roads construction, etc).

Local governments are also responsible for setting targets to reduce emissions from their operations, incorporating climate change considerations in policies and contributing appropriate resources to prepare, respond and recover from climate change impacts.

Councils are best placed to provide localised information about relevant climate risks to help the community build resilience and increase adaptive capacity.

¹ Based on a high emissions IPCC climate change scenario for 2090

² 0.9m sea level rise by 2110 based on coastal hazard modelling aligned with State Coastal Planning Policy

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Community consultation

We asked the Cockburn community how they would like to see the City demonstrate leadership on climate change action.

Residents, business, stakeholders, and reference groups were invited during May 2020, to share their vision for a better tomorrow. Over 2000 people visited the project webpage and nearly 1000 visited the online survey. In total, 309 survey responses were received, seven submissions were made, and nine digital workshops were facilitated. The City had one of its highest visitation rates on the 'Comment on Cockburn' webpage. The consultation was demographically representative of the diverse Cockburn community.

The Cockburn community were very clear on their position of advocating for a very aspirational approach for the City to demonstrate leadership regarding sustainability and climate resilience. This means:

- Aiming towards being a carbon neutral City
- Support for action towards reducing greenhouse gas emissions by all emitters
- Investing in education programs to support adaptation initiatives and increase awareness of everyone's environmental impact
- Reducing energy and consumption costs for City operations
- Preservation of the urban forest and enhancement of our unique natural landscape

A shared environmental vision was developed having regard to the insightful conversations and results of the community engagement program, which states:

The City of Cockburn is a leader in climate resilience and sustainability. Our community provides us with the strength and optimism to be aspirational in our approach to creating a better tomorrow for each and every one of us. We are aware of our lasting environmental impact. We are committed to taking pride in working together, celebrating our success and being resilient and adaptive to our changing climate.

The engagement results provide the aspiration and confidence for the City to be a strong leader and facilitator to creating an even more sustainable and climate resilient future. The *Climate Change Consultation Report* is available on the City's website.



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Greenhouse gas emissions in Cockburn

Community emissions profile

The carbon footprint for the entire Cockburn municipality is estimated to be over 2.5 million tonnes of carbon dioxide equivalent (tCO₂-e) per annum,³

Industry is by far the biggest greenhouse gas emitter in Cockburn (68%). The next highest sectors are on-road transport and domestic air travel (15%), residential (12%), followed by commercial (3%).

The City of Cockburn accounts for 1.4% of the total emissions for the municipality, which includes waste emissions from the Henderson Waste Recovery Park.

Corporate emissions profile⁴

The City’s operational activities generate 35,000 tCO₂-e per annum. This equates to 1.4% of emissions in the Cockburn municipality.

The majority of emissions (60%) are generated from solid waste to landfill. Purchased electricity is the second largest source of emissions (29%) and includes electricity used in street lighting, facilities, parks and sporting facilities. Street lighting contributes to almost half of the City’s electricity consumption at a cost of \$2.7 million per annum. Additionally, the City manages over 150 facilities and 320 parks which use electricity to maintain core functions. The remaining 11% of emissions are generated by fuel combustion (vehicles and gas).

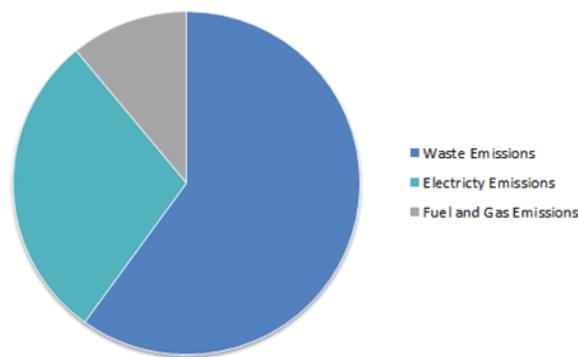


Image 3: City of Cockburn corporate emissions profile

³ Cockburn 2017 municipal emissions, Snapshot Community Climate Tool, Ironbark Sustainability

⁴ The 2020 corporate emissions profile will be updated and published in the public version of the strategy (as soon as 2019/20 data is available). Figures may vary +/- 5%



Climate change mitigation

Although we are already experiencing the impacts of climate change in Cockburn, we have the solutions to help address this challenge.

'Climate change mitigation' is the term used to describe actions to reduce greenhouse gas emissions.

The Cockburn community have told us that they would like the City to be a strong leader and aim towards being a carbon neutral City which means reducing greenhouse gas emissions by all emitters.

This strategy presents net zero emissions targets for the City (to achieve by 2030) and the community (to achieve by 2050). These targets are supported by the community and are in line with leading Councils around Australia.

The emission reduction targets are addressed in the 'climate resilience roadmap' (image 4) and supported by the following objectives:

-  1. Net zero emissions
-  2. Zero waste to landfill
-  3. 100% renewable energy
-  4. Reduce energy consumption
-  5. Zero Emissions fleet
-  6. Smart city innovation

The City aims to take responsible action to reduce its carbon footprint and achieve net zero-emissions as soon as practical. Net zero emissions is akin to 'carbon neutrality' and means that the emissions are balanced out to equal zero.

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Climate change risk assessment

There are six overarching climate change risks that have the potential to impact the City's service delivery, natural environmental, local community and infrastructure (table 2):

1. Reduced water availability from decreased rainfall
2. Biodiversity loss from climate change impacts
3. Coastal impacts from sea level rise
4. Urban forest decline from climate change
5. Community infrastructure damage from climate change impacts
6. Public health decline from climate change

The City undertook a risk assessment and updated its climate change risk register in 2020⁵. The process considered the consequences and likelihood of 18 climate risks using the City's ratings frameworks, which are consistent with *AS ISO 31000:2018 Risk management Guidelines*. The 18 risks have been grouped into their areas of impact and consolidated into the six overarching risks stated above.

The risk assessment considered high and low emissions scenarios, using current, 2050 and 2090 predictions, based on IPCC⁶ research and data from CoastAdapt⁷. Detailed information on the risk assessment methodology and results are provided in the *City of Cockburn 2020 Climate Change Risk Assessment*⁸ on the City's website.

Climate change adaptation

To reduce the impact of these risks and increase adaptive capacity, the City has integrated the six overarching climate risks into its Risk Management and Safety System (RMSS) and identified key solutions in the climate resilience roadmap.

There are six adaptation objectives that will help the City reduce the impacts of climate change:

1. Waterwise City
2. Conserve biodiversity
3. Coastal adaptation
4. Increase the urban forest
5. Protect community infrastructure
6. Enhance health and wellbeing

⁵ City of Cockburn Climate Change Risk Assessment, Ernst and Young, 2020, www.cockburn.wa.gov.au

⁶ The Intergovernmental Panel on Climate Change (IPCC) assessment reports are the most trusted forum for global climate science. The IPCC fifth assessment report published in 2014, is the most current report

⁷ CoastAdapt utilises CSIRO datasets and IPCC scenarios to model climate change and impacts. CoastAdapt was developed by NCCARF with funding from the Australian Government. Data sourced June 2020.

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| Risk Title | Risk Description | Risk Rating | Responsibility | Adaptation objective |
|---|--|-------------|---------------------------------|---|
| Reduced water availability from decreased rainfall | Decreased liveability, reduced water availability, loss of urban vegetation and biodiversity caused by climate change impacts (decreased rainfall) | Substantial | Manager Parks and Environment | Waterwise City  |
| Biodiversity loss from climate change | Damage to or loss of biodiversity and natural habitat caused by climate change impacts (decreased rainfall and increased bushfires, temperatures and extreme weather events) | High | Manager Parks and Environment | Conserve biodiversity  |
| Coastal impacts from sea level rise | Legal liability and damage to or loss of natural environment, infrastructure and coastal land, caused by sea level rise | Substantial | Manager Infrastructure Services | Coastal adaptation  |
| Urban forest decline from climate change | Urban forest decline caused by climate change impacts (increased temperatures and decreased rainfall) | Substantial | Manager Parks and Environment | Increase the urban forest  |
| Community infrastructure damage from climate change | Damage to or loss of infrastructure and increased demand for electricity and water, as a result of climate change impacts (increased bushfires, extreme weather events and temperatures) | Extreme | Manager Infrastructure Services | Protect community infrastructure  |
| Public health decline from climate change | Reduced public safety, health and wellbeing caused by climate change impacts (changes to rainfall and increased bushfires, temperatures and extreme weather events) | Extreme | Manager Environmental Health | Enhance health and wellbeing  |

Table 1: Climate change risks to the City of Cockburn

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Climate resilience roadmap

In May 2020 the City sought leadership direction from the community on how we can create a better tomorrow.

The City has used this vision, and information from the risk assessment to develop a 'resilience roadmap' that sets aspirational targets to achieve carbon neutrality and increase our resilience to climate change.

The resilience roadmap presents 14 objectives that demonstrate leadership, encourage collaboration, reduce emissions (mitigation) and increase our adaptive capacity (adaptation).

Each objective is addressed through a comprehensive action plan (page 17) that will deliver innovative solutions for energy supply, showcase world leadership in waste management, and ensure best practice adaptation.



Image 4: Climate resilience roadmap





1. Strong leadership

Leadership in action is about setting aspirational targets, listening and working together, and increasing our adaptive capacity by strengthening our existing programs and trialing new technology



2. Net zero emissions

The City aims to achieve net zero *corporate* emissions by 2030 and will work with the Cockburn community and the State Government to achieve net zero *community* emissions by 2050.



3. Transition to 100% renewable electricity

The City aspires to progressively add more renewable sources to its energy portfolio and transition to 100% renewable energy by 2030.



4. Zero waste to landfill

The City's vision for responsible waste management is for 100% of non-hazardous waste to be diverted from landfill by 2030.



5. Reduce energy consumption

A key step in the resilience roadmap is to decrease the energy demand and cost of the City's street lighting, parks and facilities by improving efficiency and optimising performance.



6. Zero emissions fleet

The City has maintained a Zero Emissions Fleet since 2011 with carbon offsets, and aims to improve its efficiency and reduce the risks of fuel price volatility by exploring advances in electric and hydrogen powered vehicles.



7. Smart City Innovation

The City will invest in innovative technology-based solutions to meet the challenges of climate change and ensure the City is better connected, safer and more resilient.



8. Education and collaboration

The City acknowledges the shared responsibility of climate change and will provide information on local climate change impacts and work with the community, business and stakeholders to take action and build resilience.



9. Waterwise City

The City is experiencing the impact of declining rainfall and aims to improve water security by maintaining its Gold Waterwise Council status and implementing the Water Efficiency Action Plan.



10. Conserve biodiversity

Climate change has the potential to cause damage to, or loss of, biodiversity and natural habitat. The City aims to increase the resilience of natural ecosystems through the Natural Area Management Strategy.



11. Coastal adaptation

Sea level rise is expected to increase shoreline erosion and inundation which may cause loss of natural habitat and infrastructure. The City will reduce these risks and legal liability by implementing planning controls, engineering solutions and monitoring in line with the Coastal Adaptation Plan and foreshore management plans.



12. Increase the urban forest

Increased temperatures, pests, disease and reduced water availability pose a risk to the urban forest. The City aims to increase tree canopy, maintain water security and improve livability by implementing planning controls, the Urban Forest Plan and Water Efficiency Action Plan.



13. Protect community infrastructure

Climate Change may cause damage to, and loss of, infrastructure as a result of bushfires, extreme weather and temperatures. The City will reduce these risks by implementing the Bushfire Risk Management Plan and best practice infrastructure management.



14. Enhance health and wellbeing

Climate change presents a risk to the health, safety and wellness of our community as a result of increased bushfires, heatwaves and extreme weather events. The City will manage these impacts by implementing the Public Health Plan and Bushfire Risk Management Plan.



Climate change action plan

The climate change action plan has been developed with direction from the community on how the City can create a sustainable and climate resilient future.

The plan provides information on the 14 key objectives of the climate resilience roadmap and a comprehensive set of actions to help eliminate greenhouse gas emissions and increase the City's adaptive capacity.



Image 5: electric vehicle charging station at Cockburn Central





1. Strong leadership

One of the community’s main aspirations for a sustainable and climate resilient future, is to see the City demonstrate strong leadership⁸.

The City will achieve this by:

- Listening and working together – with the community through education, creative engagement and communication towards a healthy and resilient future
- Setting aspirational targets - to reduce all greenhouse gas emissions throughout the City, supported by our community
- Increasing our adaptive capacity - by strengthening our existing initiatives, innovating to take advantage of emerging opportunities, trialing new technology and developing programs to support our businesses and residents to better care for our land and adapt to a changing climate.

| | Actions for strong leadership | Leader | Related Plan | Timeframe | Resource | Measure of success |
|-----|---|-------------------------------|---|----------------------------|----------|---|
| 1.1 | Endorse the 2020 Climate Change Strategy. Commit to net zero emissions | Council | Climate Change Strategy 2020 - 2030 | Now (2020 to 2021) | New | Strategy adopted by Council |
| 1.2 | Establish a Climate Change Working group to explore opportunities and technology | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Climate Change Working group established |
| 1.3 | Continue investment in the Greenhouse Action Fund using financial savings from energy reduction and renewable energy projects | Executive | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Greenhouse action fund allocated |
| 1.4 | Employ a full-time Climate Change Officer to deliver this Strategy | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Climate Change Officer employed |
| 1.5 | Review the Climate Change Strategy in 2025 and update the action plans | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | New | Climate Change Strategy reviewed |
| 1.6 | Continue to implement the Fossil Free Investment preference within the City’s Investment Policy. | Manager Finance | City of Cockburn Investment of funds Policy | Medium Term (2024 to 2027) | Existing | Funds are invested in accordance to policy. |
| 1.7 | Promoting the City’s initiatives and achievements (ie case studies) to encourage others to follow our lead | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Information shared to other Councils |

Table 2: Action plan for strong leadership

⁸ City of Cockburn, Climate Change Community Engagement Outcomes Report, June 2020, www.cockburn.wa.gov.au





2. Net zero emissions

Reducing greenhouse gas emissions is a major challenge that will require strong action over the next decade to ensure we avoid the worst impacts of climate change.

Pathway to Net Zero Emissions

Net zero emissions can be achieved by first reducing emissions to as-close-to zero as possible by improving energy efficiency, fuel switching, reducing waste to landfill and sourcing energy from renewable sources. Any remaining emissions would be balanced with an equivalent amount of carbon removal, such as the purchase of accredited carbon offsets to remove Greenhouse Gas Emissions from the atmosphere

Corporate Emission Reduction Target

The City of Cockburn aims to achieve net zero emissions by 2030 and will transition to source 100% of electricity for its facilities from renewable sources.

The corporate target relates specifically to Council operations which comprise 1.3 percent of total emissions in the Cockburn municipality. This target is considered to be best practice in Australia and necessary to avoid the worst impacts of climate change.

To achieve net zero corporate emissions by 2030, the City will:

- Establish a 'Climate Active'⁹ emissions baseline, annually monitor and report emissions
- Eliminate waste to landfill
- Transition to 100% renewable energy
- Reduce energy consumption and operational costs
- Transition to energy efficient public lighting
- Maintain a zero emissions fleet
- Invest in accredited carbon offsets for residual emissions.

Community Emission Reduction Goal

The City will work with the Cockburn Community and the State Government to achieve net zero emissions by 2050.

This goal recognises the shared responsibility for action on climate change and aligns to the Western Australian Government's proposed emission reduction target. Governments at all levels, businesses, communities and individuals all have important roles to play in reducing emissions.

⁹ Climate Active certification is awarded to businesses and organisations that have reached carbon neutrality

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The City acknowledges that it is well placed to educate, facilitate and support the community, but ultimately the state government, residents and business must take action themselves to achieve net zero emissions.

To assist the community in achieving net zero emissions by 2050, the City will:

- Provide households with the information they need to reduce energy, water and waste
- Develop an Economic Development Framework and action plan to assist local business reduce their energy consumption and prosper in a low emissions economy
- Issue an open letter to the state government, to acknowledge the shared responsibility in working towards net zero emissions
- Advocate for improvements to public transport
- Provide infrastructure such as electric vehicle charging stations and shared paths to facilitate sustainable and active transport

| | Actions to achieve net zero emissions | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|---|-------------------------------|-------------------------------------|----------------------------|----------|---|
| 2.1 | Undertake an annual 'Climate Aligned' greenhouse gas emission corporate inventory and report on performance against targets | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Annual inventory and report completed |
| 2.2 | Implement the climate resilience roadmap to reduce corporate emissions as much as practical by eliminating waste to landfill, transition to 100% renewable energy, and reduce energy consumption. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Implementation of resilience roadmap |
| 2.3 | Invest in ethical, accredited carbon offsets for residual emissions that can't be reduced by 2030. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Long Term (2027 to 2030) | Existing | Investment on accredited carbon offsets |
| 2.4 | Issue an open letter to the state government, to acknowledge the shared responsibility in working towards net zero emissions | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short term (2021 to 2023) | New | Letter issued |
| 2.5 | Assist the community in achieving net zero emissions by 2050 through education and collaboration | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Implementation of resilience roadmap |
| 2.6 | Establish partnerships to assist industry achieve net zero emissions by 2050 | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Implementation of resilience roadmap |

Table 3: Action plan for net zero emissions

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3. Transition to 100% renewable electricity

For decades, scientific communities around the world have been warning about the climate impacts that will be felt if we don't move away from the consumption of coal, oil and gas for energy¹⁰.

As the world's sunniest and windiest inhabited continent, Australia has everything it could ever need to reduce emissions to zero and benefit from the transition¹¹.

The City currently receives the majority of its energy supply from the South West Interconnected System (SWIS). The City generates around 23% of its own renewable energy from decentralized solar installations on the City's buildings.

Currently energy regulation barriers in WA restrict local governments from establishing power purchase agreements (PPAs) for non-contestable sites and restrict access to the grid for large-scale renewable energy projects. A fundamental regulatory change is required to enable the City to decarbonise its electricity supply.

Greenhouse gas emissions from energy use (gas, electricity and fuel) contribute 40% of the City's emissions. This energy is used to power essential services including street lighting, irrigation bores in parks, as well as facilities including our libraries, community buildings and the Cockburn ARC.

The City aspires to progressively add more renewable sources to its energy portfolio and by 2030 transition to 100% renewable energy by sourcing electricity from:

- small to medium scale rooftop solar on occupied council facilities
- large scale solar on the Cockburn ARC, Operations Centre and Administration Building
- a renewable energy park at the Cockburn Resource Recovery Park
- energy storage devices including hydrogen cells and electric vehicles
- organic 'waste from energy'

In addition, the City will advocate for the contestability of electricity supply in WA to enable the procurement of renewable energy.

¹⁰ Primed for action: A resilient recovery for Australia, Climate Council of Australia, 2020

¹¹ Super-Power: Australia's Low-Carbon Opportunity, Ross Garnaut 2019

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| | Actions to achieve 100% renewable electricity | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|---|---------------------------------|-------------------------------------|----------------------------|----------|---------------------------------------|
| 3.1 | Install solar photovoltaic systems on all major council facilities. Continue to investigate the feasibility of battery storage. | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | PV systems installed |
| 3.2 | In collaboration with WALGA, advocate for: a) contestability of street lighting and all Council accounts b) improved grid access for large scale renewable projects as well as community driven projects. | Executive | Climate Change Strategy 2020 - 2030 | Ongoing | New | Electricity market reform |
| 3.3 | Seek funding for the development of a 'Renewable Energy Park' including wind turbines, solar photovoltaics and hydrogen at the Cockburn Resource Recovery Precinct | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | Existing | Funding obtained |
| 3.4 | Investigate off-grid energy options e.g. transfer electricity and hydrogen from the Renewable Energy Park to facilities i.e Cockburn ARC. | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | Existing | Feasibility study completed |
| 3.5 | Continue to replace petrol driven tools, such as chainsaws, blowers and hedge trimmers, with rechargeable battery options and power them with renewable energy | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Replacement program completed |
| 3.6 | Phase out gas appliances and changeover to electric powered appliances which can be powered by renewable energy | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Replacement program completed |
| 3.7 | Secure an agreement with a retailer to procure renewable electricity | Manager Financial Services | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | Existing | Agreement established |
| 3.8 | Support community groups in leased facilities to install renewable energy infrastructure through the sustainability grants program. | Manager Parks and Environment | Climate Change Strategy | Ongoing | New | Sustainability grant criteria amended |

Table 4: Action plan for 100% renewable electricity

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4. Zero waste to landfill

Methane emissions from landfill at Henderson Waste Recovery Park (HWRP) contribute to 60 percent of the City’s total greenhouse gas emissions. In October 2019, the City temporarily diverted household general waste to HWRP landfill following withdrawal from the Southern Metropolitan Regional Council. From June 2022, general waste will be sent to the energy from waste plant in East Rockingham. Landfill emissions may increase during this transition period as a result of increased tonnage being sent to HWRP.

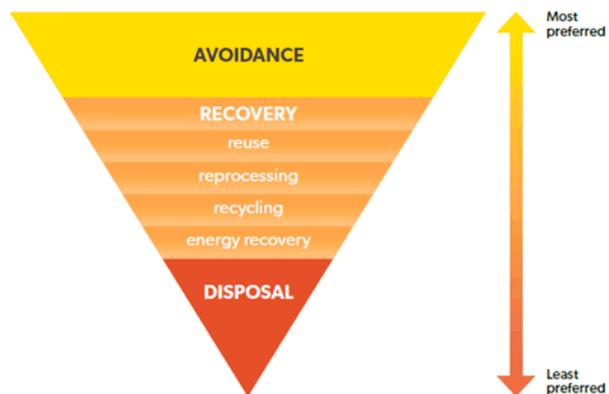
The City’s vision for responsible waste management is for 100% of non-hazardous waste to be diverted from landfill by 2030.

This will be achieved by applying the WA State Government’s waste hierarchy (see image 6 below), with avoidance of waste being the most preferred option. The City will invest in research-based behaviour change programs to encourage community and industry to generate less waste. An integral component of City’s waste management strategy is a strong community engagement program which will be supported by a best practice community drop off facility, recycle shop and education center.

The disposal of waste to landfill is the least preferred option in the hierarchy, as it recovers the least value from materials that are disposed. The City recognises that there will be a small percentage of hazardous and toxic material (eg asbestos) that can only be safely disposed of within a specialised landfill site. The Zero Waste to landfill objective excludes hazardous waste.

Henderson Waste Recovery Park will be transformed in the coming years to become the Cockburn Resource Recovery Park (CRRP) and will include a renewable energy park with methane gas capture, wind, solar and hydrogen technology. Partnerships are being explored to enable complimentary waste processors to take up leased land on site which will contribute to a circular economy. By 2030, the City plans to cap all landfill cells at the CRRP and transition to a waste transfer facility with zero waste to landfill.

Image 6: Waste Hierarchy¹²



¹² Waste Avoidance and Resource Recovery Strategy 2030

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| | Actions to achieve zero waste to landfill | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|---|---------------------|----------------|----------------------------|----------|---|
| 4.1 | Eliminate organic waste to landfill by complete product separation at CRRP and by diverting waste to the energy from waste facility. | Waste Manager | Waste Strategy | Long Term (2027 to 2030) | Existing | CRRP completed. Energy from Waste commenced |
| 4.2 | Improve resource recovery and education through construction of a best practice community drop off facility, reuse shop and education centre at CRRP. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Community education facility constructed |
| 4.3 | Deliver research-based behaviour change programs to encourage the community to generate less waste and adopt leading waste separation practices. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Behaviour change programs developed |
| 4.4 | Establish partnerships and offer land for lease at CRRP to complimentary waste businesses to further recover and process valuable waste streams onsite. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Lease arrangements established |
| 4.5 | Increase recovery from verge side collections through improved processes. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Recovery rates increased |
| 4.6 | Continue partnership landfill gas and power generator. Address legacy and fugitive emissions through landfill capping and by installing extensive methane capture pipework. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Cells capped. Landfill gas and power partnership continued. |
| 4.7 | Investigate opportunities to facilitate a circular economy within Cockburn | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Feasibility study completed |
| 4.8 | Continue to report landfill emissions under the National greenhouse Emissions reporting Scheme (NGERs) and monitor federal government policy in respect to a price on carbon. | Waste Manager | Waste Strategy | Ongoing | Existing | NGERs report submitted annually. |
| 4.9 | Provide container deposit scheme collection points and reduce single use plastics at City events | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | Existing | Wastewise event policy adopted. |
| 4.10 | Increase the use of recycled and sustainable materials within road construction. | Manager Engineering | Waste Strategy | Ongoing | Existing | Recycled material use increased. |

Table 5: Action plan for zero waste to landfill

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5. Reduce energy consumption

Community Facilities

The design and operation of our facilities has long-term implications on City's energy use and associated greenhouse gas emissions. A key step in the transition to net zero carbon emissions is to decrease the energy demand of our facilities by improving efficiency and optimising performance.

The City manages over 150 facilities and 320 parks which use electricity and gas to power appliances, lighting, pumps, heating, cooling and more.

The City is committed to implementing best practice in facility management. This includes implementing policy for sustainable buildings, data management, monitoring, reporting and active management of buildings to optimize energy efficiency.

The installation of solar power systems and application of environmentally sustainable design for all new Council facilities will significantly reduce future emissions, while lowering utility costs and enhancing comfort for building occupants.

Street lighting

Street lighting is a significant source of greenhouse gas emissions, contributing to almost half of the City's electricity consumption at a cost of \$2.7 million per annum.

Currently it is difficult for Local Governments in WA to influence the electricity consumption from street lighting, as the majority of lights are under the operational control of Western Power, who determine which luminaire is fitted to each light and their operational hours.

In other Australian states the Distribution Network Service Providers (DNSPs) and councils have come to an agreement whereby streetlights have been converted to LED on a large scale. Councils are gaining significant financial and emissions savings with leading councils now seeking extra savings by installing smart lighting.

Over half of the City's streetlight stock is 80W Mercury Vapour luminaires (58%), which present great opportunity for a changeover to 17W LED which can reduce electricity consumption by 82%.

The City will continue to advocate for technology replacement, contestability of street lighting and will work with Western Power to facilitate a roll out of more energy efficient street lighting.

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| | Actions to reduce energy consumption | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|---|---|---------------------------------------|----------------------------|----------|--|
| 5.1 | Design all new Council buildings to best practice Environmentally Sustainable Design (ESD) by allocating 3% of the project budget towards innovative ESD | Executive | Sustainability Policy | Ongoing | Existing | ESD incorporated into new builds (over \$1m) |
| 5.2 | Investigate opportunities for carbon positive buildings | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Report complete |
| 5.3 | Implement environmentally and financially sustainable methodology for government facilities management. | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | New | Best practice implemented |
| 5.4 | Support community groups in leased facilities to undertake energy efficient upgrades through sustainability grants | Manager Recreation and Community Safety | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Sustainability grant criteria amended |
| 5.5 | Undertake energy audits of all major community facilities, identify a schedule of works and develop building optimization plans to reduce operating costs, better manage energy demand and reduce consumption | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | Ongoing | Energy audit program delivered |
| 5.6 | Investigate opportunities to offset the carbon emissions from City of Cockburn organised events | Manager Corporate Communications | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | Existing | Feasibility report completed |
| 5.7 | Switch to energy efficient street lighting by working with WALGA and Wester Power (WP) to upgrade WP streetlights and upgrade Council owned streetlighting to LED in parks, marina, public access ways, etc. | Executive | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Smart street light rollout complete |
| 5.8 | Update the Public Open Space Strategy to make it a requirement for all parks and streetscape lighting to be LED | Manager Parks and Environment | Public Open Space Strategy 2014 -2024 | Medium Term (2024 to 2027) | Existing | Public Open Space Strategy updated |
| 5.9 | Continue to monitor the City's energy use via the online integrated monitoring platform and integrate real-time data | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Regular monitoring and inventory report completed. |

Table 6: Action plan to reduce energy consumption

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6. Zero emissions fleet

The City has operational requirements for a range of vehicles, plant and equipment which deliver essential services such as waste collection, recycling and maintenance of the City's parks and reserves.

Fleet emissions from consumption of petrol and diesel fuel contribute 11 percent of the City's total emissions. Almost 80 percent of these emissions are from heavy fleet including waste trucks.

The City has maintained a Zero emissions fleet since 2011, by purchasing accredited carbon offsets that support local WA projects including biodiverse tree planting in the wheatbelt. Going forward the City aims to improve the efficiency of its fleet and transition to electric and hydrogen powered vehicles.

Innovations in the vehicle industry and declining battery costs will soon put Electric Vehicles (EVs) on par with conventional fuel powered vehicles. EVs will be the most significant technology change in passenger vehicles. Over the next five years, it is expected that most of the barriers to adoption will be addressed, with the price of EVs expecting to drop below their conventional counterparts by as early as 2022.

Electric vehicles will play an important role in the City's transition to 100% renewable energy. EVs can also form part of the City's power system as they use, generate and store electricity.

Technological advances in hydrogen fuel cell vehicles may also present opportunities, particularly with waste vehicles.

Replacing the fleet's energy source from fuel, to electricity and hydrogen, can help reduce the risks of fuel price volatility. Integrating battery storage and renewables into the energy mix will help to enhance energy security as the City can generate and store its own fuel source onsite.

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| | Actions for a Zero Emissions Fleet | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|---|--------------------------------|-------------------------------------|---------------------------|----------|--------------------------------------|
| 6.1 | Develop a best practice Council fleet policy and ensure fleet purchases meet strict Greenhouse Gas emissions requirements. Incentivise the selection of low or "zero" emission vehicles. Set a requirement for the proportion of fleet vehicles to be EV. | Manager Infrastructure Service | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Policy updated |
| 6.2 | Continue to offset residual emissions from the City's Fleet by purchasing accredited carbon offsets | Manager Infrastructure Service | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | Existing | Offsets procured |
| 6.3 | Develop a plan to adopt new technology to transition Council's fleet to electric vehicles or hydrogen and power these by 100% renewable energy | Manager Infrastructure Service | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Plan endorsed and implemented |
| 6.4 | Undertake a 'Green Hydrogen for City of Cockburn' feasibility study for solar hydrogen production for waste collection and light vehicle fleets. | Manager Infrastructure Service | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | Existing | Feasibility study completed |
| 6.5 | Continue to provide fast-charging infrastructure throughout the City at key locations for electric vehicles | Manager Infrastructure Service | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | EV charging infrastructure installed |
| 6.6 | Quantify the amount of carbon sequestration through the City's annual planting program to offset residual fleet emissions | Manager Parks and Environment | Urban Forest Plan 2018 - 2028 | Short Term (2021 to 2023) | New | Calculations completed |
| 6.7 | Upgrade the staff vehicle booking system, including the ability to facilitate carpooling to meetings and for commute to work. | Manager Human Resources | Climate Change Strategy 2020 - 2030 | Now (2020 to 2021) | Existing | Vehicle booking system commissioned |
| 6.8 | Investigate incentives to encourage staff to switch to more sustainable transport to commute to work. | Manager Engineering | Integrated Transport Strategy | Short Term (2021 to 2023) | Existing | Incentives approved and implemented |

Table 7: Action plan for a zero emissions fleet

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7. Smart city innovation

The City of Cockburn aspires to be recognised as a digital transformation leader and has made a commitment to embrace leading edge practices, enhance digital literacy, and apply smart technology¹³.

A smart City is one that uses information and technology to increase operational efficiency, improve communication and enhance liveability.

There are new approaches, innovative technologies and smart infrastructure that can help meet the challenges of climate change and ensure a more sustainable and resilient future.

Sensors and integrated technology providing real time data can be used in decision making to improve livability of the City i.e. by monitoring urban heat, water use and waste levels. Smart lighting can also help to reduce energy demand and costs.

| | Actions for smart city innovation | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|---|---------------------------------|-------------------------------------|---------------------------|----------|---|
| 7.1 | Review and implement the Digital Strategy 2019-2023 | Executive | Digital Strategy 2019-2023 | Short Term (2021 to 2023) | Existing | Strategy reviewed and implemented |
| 7.2 | Develop an Innovation Framework | Executive | Corporate Business Plan | Short Term (2021 to 2023) | Existing | Framework endorsed by Council |
| 7.3 | Pilot smart street lighting e.g technology with environmental sensors, diming technology, metering, traffic measurement. Seek funding to accelerate a City wide replacement program | Executive | Digital Strategy 2019-2023 | Short Term (2021 to 2023) | Existing | Pilot completed City wide rollout complete |
| 7.4 | Advocate for a fast-tracked roll-out of residential Advanced Meter Infrastructure to help establish a smart grid and facilitate a virtual power plant in the community | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Advanced Meter Infrastructure installed |
| 7.5 | Support western Power in the installation of Community Storage (large scale batteries) to meet network needs and facilitate a smart grid. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | New | Community Storage Batteries installed |
| 7.6 | Continue to provide electric vehicle charging infrastructure to facilitate sustainable transport | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | New | EV charging infrastructure installed |

¹³ City of Cockburn Digital Strategy 2019 -2023, www.cockburn.wa.gov.au

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| | Actions for smart city innovation | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|--|---------------------------------|-------------------------------------|----------------------------|----------|--|
| 7.7 | Investigate usage-pricing technology and pay-as-you-throw programs for municipal waste | Waste Manager | Waste Strategy | Long Term (2027 to 2030) | New | Feasibility study completed |
| 7.8 | Investigate smart technology to reduce litter (e.g. microplastics) on the Cockburn coastline. | Waste Manager | Waste Strategy | Medium Term (2024 to 2027) | New | Feasibility study completed |
| 7.9 | Investigate reverse vending machines for household waste to engage the community and provide incentives to reduce waste | Waste Manager | Waste Strategy | Short Term (2021 to 2023) | New | Investigation completed. Incentives provide. |
| 7.10 | Implement smart building controls to monitor and manage City's facilities remotely (e.g. to enable switch off of lights and appliances) | Manager Infrastructure Services | Digital Strategy 2019-2023 | Now (2020 to 2021) | Existing | Intelligent building controls installed |
| 7.11 | Explore innovative ways to utilise renewable energy technology to establish a local network of energy generators, peer-to-peer trading and micro grids. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Feasibility study completed |
| 7.12 | Undertake a 'Green Hydrogen for City of Cockburn' feasibility study to examine cogeneration opportunities for electricity and heat production. | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Study completed |
| 7.13 | Investigate design requirements for road signage and line markings to accommodate future advances in transport including driverless and autonomous electric vehicles | Manager Engineering | Integrated Transport Strategy | Long Term (2027 to 2030) | Existing | Investigations completed |
| 7.14 | Encourage smart city infrastructure in land developments (e.g. Hamilton Hill High School redevelopment) | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Advice provided to land developers |

Table 8: Action plan for smart city innovation

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8. Education and collaboration

Education and collaboration are essential elements in the City's response to climate change. Through education, the City can help the community and business understand the specific climate risks to our region, which can empower them to take action to reduce emissions and build resilience by being prepared to adapt to the impacts.

The City acknowledges the shared responsibility of climate change. Collaboration with stakeholders, the community and business, enables the City to pool resources to tackle an issue that isn't defined by borders.

Early childhood and school programs

The City provides grants and scholarships to support environmental education and subsidised access to a wide range of excursions, festivals and programs e.g.

- environmental education and sustainability grants
- school holiday programs
- early learning programs
- professional development sponsorships
- climate change excursions.

Community engagement programs

The City offers a wide range of grants, rebates and engagement programs to residents on sustainability, environmental protection and climate change, including:

- sustainability workshops and events
- community litter prevention programs
- citizen science programs
- sustainability and landowner biodiversity grants
- free mulch and street trees
- rebates and subsidies, free home energy and water audits.

Helping local business prosper

The City is developing an Economic Development Framework and action plan that will outline how the City can help business thrive. The City supports local business and industry through:

- grant funding, support for start-ups and innovation in business
- business capacity building
- support for business-to-business growth and competitive supply chains
- business advisory support, events and online resources
- promoting a 'shop local' message
- advocating for a full range of education and training opportunities
- facilitating a thriving tourism and ecotourism industry.

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Collaboration and Partnerships

The City maintains strong partnerships with the Melville Cockburn Chamber of Commerce (MCCC), Jandakot Airport Chamber of Commerce, Small Business Development Corporation (SBDC), South West Group, and Business Foundations to help build a strong and sustainable economy

In 2012 the City signed the WALGA Climate Change declaration and continues to work with this peak body to advocate for changes in regulation and policy to enable local government to deliver its core services in a sustainable manner. The City is also active in the WALGA CHRMAP (Coastal Hazard Risk Management and Adaptation Planning) Forum, which brings the state's coastal local governments together to collaborate and advocate for better responses to sea level rise and coastal hazards.

The City is actively working with the Department of Fire and Emergency Services (DFES) to mitigate the risk and impact that fire may have on our community.

Cities Power Partnership

Cockburn joined the Climate Council's Cities Power Partnership (CPP) in 2019. CPP is Australia's largest climate change program with over 125 councils. CPP provides opportunity to contribute to the national knowledge hub and collaborate with councils.

Participating councils are required to identify five power partnership pledges. The City has selected the following five action pledges which are addressed within this strategy:

1. Set city-level renewable energy or emissions reduction targets
2. Roll out energy efficient lighting across the municipality
3. Identify opportunities to turn organic waste into electricity
4. Install renewable energy (solar PV and battery storage) on council buildings
5. Power council operations by renewable energy, and set targets to increase the level of renewable power for council operations over time.

Climate Clever

The City joined ClimateClever in 2020 through the Local Government (LGA) Partnership Program. ClimateClever helps empower school, students and households to reduce their carbon footprint and save money. Residents and Schools in Cockburn are encouraged to participate and can receive a 50% discount on Climate clever apps.



Cockburn Sound Coastal Alliance

The City is a founding member of the Cockburn Sound Coastal Alliance (CSCA). The CSCA aims to take a regional approach to understanding and adapting to coastal hazards, so that methodologies to address climate change risks are consistent within the geographical area of Cockburn Sound and Owen Anchorage.

| | Actions for education and collaboration | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|--|---------------------------------|-------------------------------------|---------------------------|----------|--|
| 8.1 | Continue environmental education programs to build awareness and understanding of climate risks and resilience. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Education programs delivered |
| 8.2 | Continue collaboration with the Cockburn Sound Coastal Alliance | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Partnership continued |
| 8.3 | Continue the Cities Power Partnership Champion membership | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | Existing | Membership maintained |
| 8.4 | Work with WALGA to advocate for changes to policy and legislation | Executive | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Partnership continued |
| 8.5 | Support the implementation of State Government climate policy | Executive | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Support continued |
| 8.6 | Continue to implement the buy local purchasing policy | Manager Financial Services | Purchasing Policy | Ongoing | Existing | Buy local policy implemented |
| 8.7 | Implement sustainable behaviour change programs and provide households with the information and tools they need to reduce energy, water and waste consumption. | Manager Parks and Environment | Sustainability Strategy 2017 - 2022 | Ongoing | Existing | Sustainable living events program delivered. |
| 8.8 | Continue to fund the Home Eco Audits program to facilitate personalised actions for households to reduce carbon emissions | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Ongoing | Existing | Home eco audits program delivered |
| 8.9 | Prepare an Economic Development Framework that outlines how the City can build resilience, help business proposer in a low emissions economy and create new jobs, particularly via: organics waste management, | Manager Strategic Planning | Corporate Business Plan | Short Term (2021 to 2023) | New | Economic Development Framework adopted |

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| | Actions for education and collaboration | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|--|-------------------------------|-------------------------------------|---------------------------|----------|---------------------------------------|
| | renewable energy infrastructure, ecosystem restoration, active transport, energy efficiency and electric vehicle infrastructure. | | | | | |
| 8.10 | Continue to encourage sustainable transport use such as public transport, walking and cycling through the 'Your Move' program, transport planning and design | Manager Engineering | Integrated Transport Plan | Ongoing | Existing | Integrated transport plan implemented |
| 8.11 | Investigate how the City can support the community and local business develop, own, or benefit from renewable energy projects. E.g. bulk buys. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Report complete |
| 8.12 | Explore community ownership models for large-scale renewable energy infrastructure. | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Report complete |

Table 9: Action plan for education and collaboration





9. Waterwise city

Cockburn's water security has already been influenced by climate change and associated local decline in rainfall. This has resulted in reduced ground water availability, which may decrease liveability with loss of urban vegetation and a decline in biodiversity.

The City is actively managing the impacts of reduce water availability for its operations. Many of the City's groundwater resource areas are exhausted and with further reductions in groundwater allocations anticipated.

In order to maintain local amenity through neighborhood parklands, high quality sporting grounds and verges, the City is reducing water use by rationalising irrigation. This is achieved through hydrozoning, planting drought tolerant native species and using central controlled irrigation systems. This is a primary example of climate change adaptation in action.

Waterwise Council Program

The City has been endorsed by the Water Corporation as a Gold Waterwise Council since 2015. This program supports councils to improve water efficiency and gives recognition to the City's initiatives to reduce water use at all facilities, buildings and public open spaces. The Water Efficiency Action Plan (WEAP) guides the City to monitor water use, deliver on identified goals, targets and sets actions that improve efficiency in groundwater use and reduce scheme water consumption.

| | Actions for a waterwise City | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|-----|--|-------------------------------|---|---------------------------|----------|--|
| 9.1 | Maintain Gold waterwise Council Status. Aim for platinum endorsement. | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Ongoing | Existing | Gold waterwise Council Status maintained |
| 9.2 | Review, update and implement the Water Efficiency Action Plan to address climate change | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Now (2020 to 2021) | New | Plan updated |
| 9.3 | Establish a waterwise working group to advance the WEAP and water sensitive urban design | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Now (2020 to 2021) | New | Working group establish |
| 9.4 | Reduce community and corporate water consumption | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Short Term (2021 to 2023) | Existing | Water consumption reduced |

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| | | | | | | |
|------|--|---|---|----------------------------|----------|--|
| 9.5 | Increase water literacy and continue to offer incentives to encourage water efficiency in households | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Ongoing | Existing | Education program and incentives delivered |
| 9.6 | Continue to reduce Council groundwater abstraction and explore use of alternative water sources | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Now (2020 to 2021) | Existing | Groundwater usage reduced. Alternatives identified |
| 9.7 | Maintain dialogue with the Water Corporation to enhance the outcomes of major stormwater drainage systems on Wetlands | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Now (2020 to 2021) | Existing | Partnership maintained |
| 9.8 | Implement Water Sensitive Urban Design initiatives e.g. groundwater replenishment, Policy or guidelines ,aquifer recharge trials and groundwater interception drain at Port Coogee | Manager Parks and Environment | Drainage Management Strategy 2018-2028 | Now (2020 to 2021) | New | WSUD Initiatives implemented |
| 9.9 | Continue to monitor water use and sources for public open space and adjust regimes | Manager Parks and Environment | Water Operating Strategy | Ongoing | Existing | Water Operating Strategy implemented |
| 9.10 | Continue to investigate and implement opportunities to reduce irrigated surface area and improve irrigation design e.g. hydrozoning. | Manager Parks and Environment | Water Operating Strategy | Ongoing | Existing | Irrigation reduced |
| 9.11 | Continue to implement water saving measures at the ARC and water audits to identify savings. | Manager Recreation and Community Safety | Water Efficiency Action Plan 2016 -2020 | Ongoing | Existing | Water consumption reduced |
| 9.12 | Conduct water audits at the top 5 water using facilities/locations. | Manager Parks and Environment | Water Efficiency Action Plan 2016 -2020 | Short Term (2021 to 2023) | Existing | Audits completed |
| 9.13 | Require best practice strategies for new urban development and existing storm water drainage management systems, to be capable of handling greater flows considering latest climate science. | Manager Engineering | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Review undertaken |

Table 10: Action plan for a waterwise city

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10. Conserve biodiversity

The City of Cockburn manages 92 bushland reserves, spanning 1,189 hectares, which includes coastal, wetland and upland areas. Many of the City's reserves are located within three regional parks – Beeliar, Jandakot and Woodman Point.

As well as bushland reserves, the City manages a number of ecological linkages in developed areas which provide important movement passages for fauna and habitat for flora. There are two chains of wetlands that run through the heart of the City, including an internationally listed Ramsar wetland, Thompson Lake.

Cockburn's biodiversity is part of the South West Botanical Province of Western Australia, which is recognised as one of the world's top 25 biodiversity hotspots.

Climate change projections suggest that Cockburn will experience an increase in the occurrence of extreme weather and fire-prone conditions which can threaten biodiversity. Increase in temperatures, decreased rainfall and drier conditions can also result in changes to the distribution and occurrence of fauna and flora, due to shifting climate envelopes and changes to the natural cycles of flora and fauna, and in turn biodiversity loss.

Additionally, coastal erosion and salt water intrusion may also cause loss of habitat and reduced ecosystem function in coastal dunes and wetlands such as Manning Lake.

The City has a number of measures in place to increase the resilience of these ecosystems, including flora and fauna monitoring, weed and pest management as outlined in the Natural Area Management Strategy.

Detailed information on climate change risks to biodiversity, current controls, and actions are provided in the City of Cockburn Climate Change Risk Assessment.

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| | Actions to conserve biodiversity | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|---|---|--|---------------------------|----------|---|
| 10.1 | Implement the Natural Area Management Strategy. | Manager Parks and Environment | Natural Area Management Strategy 2018 - 2022 | Now (2020 to 2021) | Existing | Strategy implemented |
| 10.2 | Update the City's Bushfire Risk Management Plan and Conservation specific Fire Response Plans to reduce adverse impacts upon biodiversity. | Manager Recreation and Community Safety | Bushfire Risk Management Plan 2015 - 2020 | Now (2020 to 2021) | Existing | Bushfire Risk Management Plan updated |
| 10.3 | Continue to help to protect at risk species by increasing protected bushland areas, and relocating species that are under threat to suitable areas | Manager Parks and Environment | Natural Area Management Strategy 2018 - 2022 | Now (2020 to 2021) | Existing | Relation programs implemented |
| 10.4 | Continue to investigate and implement alternative species of plants, and trees for new parks/ovals and old parks/ovals requiring upgrading/replacing | Manager Parks and Environment | Natural Area Management Strategy 2018 - 2022 | Now (2020 to 2021) | Existing | Plant selection lists updated |
| 10.5 | Continue to monitor wetlands and conservation areas and adapt management regimes to build resilience and maintain ecosystem function | Manager Parks and Environment | Natural Area Management Strategy 2018 - 2022 | Now (2020 to 2021) | Existing | Monitoring and adaptation regimes implemented |
| 10.6 | Provide education programs/campaigns to assist the public in understanding biodiversity loss and implementing biodiversity preservation strategies in their own gardens | Manager Parks and Environment | Natural Area Management Strategy 2018 - 2022 | Short Term (2021 to 2023) | Existing | Education campaigns delivered |

Table 11: Action plan to conserve biodiversity

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11. Coastal adaptation

Sea level rise is expected to result in increased shoreline erosion and more frequent inundation of low-lying land. Salinity and salt water intrusion to aquifers may also cause damage to, or loss of natural environment and habitat, low-lying buildings and infrastructure.

Increased erosion due to sea level rise is expected to be the first and most significant impact of sea level rise, likely to be acutely observable by the middle of the century. All of the City's sandy shoreline is expected to progressively recede to varying degrees, resulting in permanent loss of coastal land and adjoining assets if no action is taken.

Occasional inundation (coastal flooding) of some low lying coastal areas is likely to occur later in the 21st century, impacting locations such as parts of North Coogee and much of the Woodman Point reserves. The City should be considerate of legal liability and reputational risk in relation to development approval and planning schemes for low-lying areas at high risk of inundation.

Damage to coastal structures as a result of increased storm intensity as well as salt water intrusion and elevated groundwater levels in coastal areas are also likely to have material negative impacts however the magnitude of these hazards are difficult to predict and have not been assessed in detail at this time.

These risks may result in a reduction of public amenity, loss of natural environment and damage to infrastructure. The City has a number of measures in place to reduce these risks including the Coastal Adaptation Plan, and a Coastal Monitoring Program.

Coastal Adaptation Plan

The Coastal Adaptation Plan (CAP) was developed in 2016 as part of the Cockburn Sound Coastal Vulnerability & Flexible Adaptation Pathways Project, using information from the Coastal Vulnerability Study and Values and Risk Assessment. The CAP guides the City in managing risks and adapting to coastal changes in a sustainable and flexible manner.

The Coastal Adaptation Plan adopts a flexible adaptation pathway approach, which aims to implement management actions using risk-based triggers whilst prioritising measures that address the coastal risk without limiting future adaptation strategy options. Specific recommended actions for the City to take in the immediate (15-year) planning horizon are provided to manage risks of coastal erosion and inundation. Recommendations for actions further afield are also provided, on the understanding that these may evolve with periodic review of the CAP to reflect future changes in community values, scientific understanding and future events.

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| | Actions for coastal adaptation | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|--|---------------------------------|------------------------------|----------------------------|----------|--|
| 11.1 | Review and implement the Coastal Adaptation Plan with latest climate science, scenario mapping and WALGA recommendations. | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Ongoing | Existing | Coastal Adaptation Plan reviewed |
| 11.2 | Review and maintain ongoing coastal monitoring program to identify early changes to shoreline conditions and risks | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Ongoing | Existing | Coastal monitoring program delivered |
| 11.3 | Receive legal advice to clarify the liability of the City in the event of coastal climate change risk scenarios | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Short Term (2021 to 2023) | Existing | Legal advice received |
| 11.4 | Engage with the community and stakeholders to raise awareness of coastal risks and increase collaboration | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Short Term (2021 to 2023) | Existing | Community engagement program delivered |
| 11.5 | Prepare site specific foreshore management plans to provide an implementation framework for coastal adaptation measures, including managed retreat or defence as appropriate to the site | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Short Term (2021 to 2023) | Existing | foreshore management plans implemented |
| 11.6 | Design and implement coastal adaptation measures as required and in alignment with the Coastal Adaptation Plan and foreshore management plans | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Ongoing | Existing | Coastal adaptation measure implemented |
| 11.7 | Review and update planning schemes and controls according to latest coastal hazard information and legal advice and best-practice coastal engineering. | Manager Strategic planning | Coastal Adaptation Plan 2016 | Medium Term (2024 to 2027) | New | Planning schemes and controls updated |
| 11.8 | Design for climate resilience and with consideration to the most appropriate sea level rise predictions for any expansion of Marina infrastructure or the development of other coastal structures. | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Ongoing | Existing | Coastal and marina infrastructure designed appropriately |
| 11.9 | Advocate for State and Commonwealth funding towards coastal adaptation measures | Manager Infrastructure Services | Coastal Adaptation Plan 2016 | Ongoing | New | Funding obtained |

Table 12: Action plan for coastal adaptation

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12. Increase the urban forest

Trees, bushland, parks and gardens are central to the character and liveability of the City. The current value of the City's street trees is over \$137 million. These valuable assets help to cool our suburbs, improve the health and wellbeing of our residents, provide habitat, increase amenity and improve property values.

The climate change risk assessment identified a substantial risk to the City's urban forest as a result of increased temperatures and decreased rainfall which will cause heat, pest and water stress on vegetation and the urban forest.

Urban Forest Plan 2018- 2028

The *Urban Forest Plan 2018- 2028* recognises the impacts of climate change and guides the management of streetscapes and public open space to improve the liveability of our City. The plan aims to expand canopy cover through the City's tree planting program, promote resilience by increasing diversity and protect green infrastructure by strengthening development controls.

Planning mechanisms

During the past decade Cockburn has experienced rapid clearing of vegetation due to urban development. The City is currently looking at ways to improve retention of mature trees across the City as part of the draft *Local Planning Strategy*, by acknowledging their environmental value, increase the local amenity and in combating the heat island effect. As part of this, the City is considering how it can identify, protect and enhance ecological corridors across the City. The approach and opportunity to do this depends on the zoning and the development/subdivision potential and the planning of the area.

In 2020 the City amended *Local Planning Policy 1.2 Residential Design Guidelines* as part of the *Better Neighbourhoods, Better Homes* project. The policy mandates garden areas for new grouped dwellings to accommodate a 3x3m minimum deep soil zone for a tree. It is hoped that this will increase neighborhood tree canopy cover.

The City also maintains a significant tree register within the Local Government Inventory (local heritage survey). The register is intended to protect trees of cultural heritage significance with a requirement for planning approval prior to removal or significant pruning, as per the town planning scheme provision.

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| | Actions to increase urban forest | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|---|-------------------------------|---|----------------------------|----------|---|
| 12.1 | Implement the Urban Forest Plan 2018- 2028 to increase canopy cover and enhance resilience of the City urban forest. | Manager Parks and Environment | Urban Forest Plan 2018 - 2028 | Short Term (2021 to 2023) | Existing | Urban Forest Plan implemented |
| 12.2 | Update the Local Planning Strategy to improve retention of mature trees in Cockburn | Manager Strategic Planning | Local Planning Strategy | Now (2020 to 2021) | Existing | Local Planning Strategy updated |
| 12.3 | Adopt the Local Planning Policy 1.2 Residential Design Guidelines to mandate deep soil zone for trees in new grouped dwellings. | Manager Strategic Planning | Local Planning Policy 1.2 | Now (2020 to 2021) | Existing | Local Planning Policy amended |
| 12.4 | Create a plan for climate resilient green spaces to reduce irrigated grass and increase hydro zoning and tree canopy in identified streetscapes and public open space | Manager Parks and Environment | Urban Forest Plan 2018 - 2028 | Medium Term (2024 to 2027) | New | Plan for climate resilient green spaces developed |
| 12.5 | Continue to maintain the significant tree register | Manager Strategic Planning | Local Planning Policy 4.4 Heritage Conservation Design Guidelines | Ongoing | Existing | Significant tree register maintained |

Table 13: Action plan to increase the urban forest





13. Protect community infrastructure

Climate change has the potential to cause damage to community infrastructure as a result of increased bushfires, higher temperatures and extreme weather events.

Collectively these impacts may result in:

- Financial loss to the City through increased insurance premiums, maintenance and repair costs
- Reduced public safety, health and wellness.
- Legal, financial and reputational damage to the City
- Power outage impacts on transport infrastructure such as traffic lights causing traffic congestion and delays.
- Increased operational costs and peak energy demand for utilities.

Bush fires currently pose the greatest climate risk to properties, buildings and other infrastructure located within the City.

The City has a number of measures in place to reduce the impact of bushfires, including vegetation and fuel load management in bushland reserves, facility-centric evacuation plans, fire management plans for building design approval, and community education programs to improve public emergency preparedness.

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| | Actions to protect community infrastructure | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|--|---|--|----------------------------|----------|-------------------------------------|
| 13.1 | Review capacity of existing Council buildings to withstand more severe storms and retrofit as appropriate and if required to meet updated building codes | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Now (2021 to 2022) | New | Review completed |
| 13.2 | Review the frequency of reactive and preventative maintenance performed on the City's infrastructure assets to identify potential gaps in service and develop an environmentally and financially sustainable methodology for maintenance, renewal and repair | Manager Infrastructure Services | Climate Change Strategy 2020 - 2030 | Medium Term (2024 to 2027) | New | Review completed |
| 13.3 | Consistent with Planning Policy provisions continue to ensure all proposed Structure Plans are accompanied and informed by a Bushfire Management Plan where required. | Manager Strategic Planning | Bushfire Risk Management Plan 2015 -2020 | Ongoing | Existing | Bushfire Management Plans developed |
| 13.4 | Ensure all City owned buildings (within Bushfire Prone Areas) have Bushfire Risk Assessment completed. | Manager Recreation and Community Safety | Bushfire Risk Management Plan 2015 -2020 | Ongoing | Existing | Risk Assessment completed |
| 13.5 | Consistent with Planning Policy provisions continue to ensure that the new building design approval process (within Bushfire Prone Areas) incorporates bush fire management. | Manager Recreation and Community Safety | Bushfire Risk Management Plan 2015 -2020 | Ongoing | Existing | Bush fire management undertaken |
| 13.6 | Design building for climate resilience and improve energy management, through implementation of ESD guidelines | Manager Infrastructure Services | Sustainability Policy | Now (2020 to 2021) | Existing | Sustainability Policy implemented |
| 13.7 | Continue to implement Drainage Management Strategy 2018 - 2028. | Manager Engineering | Drainage Management Strategy 2018 - 2028 | Now (2020 to 2021) | Existing | Reviewed to manage climate risks |

Table 14: Action plan to protect community infrastructure

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14. Enhance health and wellbeing

The City of Cockburn has a new vision to make Cockburn the best place to be. Our world-class livability, healthy communities and abundance of natural assets make our City the ideal place to live, work and play.

Climate change has the potential to impact public safety, health and wellness as a result of increased bushfires, heatwaves, higher temperatures, less rainfall, extreme weather events

The impacts of changes to these climate variables can result in:

- increased pressure on emergency and social services
- reduced air quality from smoke following bushfire events
- increase in mosquito borne disease, food poisoning cases or nuisance species
- heat stress and reduced wellbeing
- reduced liveability of the City
- greater demand for resources to accommodate displaced persons
- cancellation or postponement of public events organised by the City
- legal, financial and reputational damage to the City
- increased downtime during hot weather or extreme weather.

Public Health Plan

The City is currently reviewing and updating its Public Health Plan, which will address climate change impacts and include measures to enhance the health and wellbeing of the Cockburn community.

Local Emergency Risk Management Plan

The City's Local Emergency Risk Management Plan describes emergencies that are likely to occur, defines roles and responsibilities within the community, list resources, and coordinate operations and activities. The Plan also identifies agreements between local governments and emergency management agencies.

Bushfire Risk Management Plan 2015 - 2020

This Bushfire Risk Management Plan provides a basis for reducing the risk of fire, reducing the fuel load and increasing community awareness on bushfire risks. The plan acknowledges the impacts of climate change and details a program of coordinated multi-agency treatments to address this risk.

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| | Actions to enhance health and wellbeing | Leader | Related Plan | Timeframe | Resource | Measure of Success |
|------|--|---|---|---------------------------|----------|--|
| 14.1 | Undertake a climate change health vulnerability assessment and map vulnerable residents and areas. | Manager Environmental Health | Public Health Plan | Short Term (2021 to 2023) | New | Vulnerability assessment completed |
| 14.2 | Review, update and implement the Public Health Plan including actions that reduce climate change risk to public health including heat stress, increases in mosquito and vector borne disease, food poisoning, nuisance species, bushfires, pandemics extreme weather events, etc | Manager Environmental Health | Public Health Plan | Short Term (2021 to 2023) | New | Public Health plan reviewed, updated and implemented |
| 14.3 | Continue to update and implement shade strategy for community facilities, playgrounds, parks and beaches. | Manager Parks and Environment | Playground Shade Sail Strategy 2013-2023 | Ongoing | Existing | Shade Strategy implemented |
| 14.4 | Review, update and implement the Bushfire Risk Management Plan and Local Emergency Risk Management Plan. | Manager Recreation and Community Safety | Bushfire Risk Management Plan 2015 - 2020 Local Emergency Risk Management Plan | Short Term (2020 to 2021) | New | Plans updated |
| 14.5 | Provide education programs to assist the public prepare for emergency situations. | Manager Recreation and Community Safety | Local Emergency Risk Management Plan | Ongoing | Existing | Education Programs implemented |
| 14.6 | Review existing warning systems and identify potential gaps and opportunities for improvement. | Manager Recreation and Community Safety | Local Emergency Risk Management Plan | Short Term (2021 to 2023) | Existing | Review completed |
| 14.7 | Provide updated information to assist the public understand the impacts of climate changes and empower them to take action to build resilience | Manager Parks and Environment | Climate Change Strategy 2020 - 2030 | Short Term (2021 to 2023) | New | Education resource developed |

Table 15: Action plan to enhance health and wellbeing

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Resourcing

The action plan is supported by the City's Greenhouse Action Fund which includes an allocation of \$200,000 each year. The Greenhouse Action Fund was established in 2011 using ongoing savings from previous emission reduction initiatives. To date the fund has financed over \$2 million in projects including a 1MV PV systems and geothermal for the Cockburn Aquatic and Recreation Centre.

The City will also explore grant opportunities from the state and federal government to further advance the action plan.

Many actions in this strategy align to existing strategies or plans which provide guidance on resources and funding. The remaining actions will be subject to annual budget submissions, approved business cases by responsible leaders and budget capacity.

Coordination of the actions, delivery of education programs and the continued investment in researching new initiatives will require the appointment of a full-time Climate Change Officer (currently part-time contract).

It is recommended a full-time Climate Change Officer be included in the Work Force Plan for appointment in the 2021- 2022 financial year to oversee the implementation of this strategy and the climate resilience roadmap.

Monitoring and review

This strategy will be reviewed in 2025 to stay up to date with changes in policy, technology and climate change science. Annual greenhouse gas emission inventory reports will continue to be produced to measure the City’s carbon footprint.

The City recognises that risk management is an ongoing process, and will monitor and review climate change risks and actions as new data becomes available.



Image 7: Coastal erosion monitoring at C Y O’Connor Beach



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17. COMMUNITY SERVICES DIVISION ISSUES

17.1 (2020/MINUTE NO 0198) ADOPTION OF THE ANIMAL MANAGEMENT AND EXERCISE PLAN 2020-25

Author(s) M Emery

Attachments

1. Animal Management and Exercise Plan 2020-2025
2. Community Feedback - Woodman Point Dog Area
3. Stakeholder Feedback - Woodman Point Dog Area
4. Environmental Study – Woodman Point Dog Area
5. Petition - Bishop Park
6. Petition - Allendale Park
7. Community Consultation Report
8. Beeliar Primary School Letter of Support

RECOMMENDATION

That Council:

- (1) adopt the Animal Management and Exercise Plan 2020-2025 (*Attachment 1*);
- (2) notes the Environmental Assessment of shore nesting birds along Woodman Point;
- (3) provides 28 days public notice (as defined in section 1.7 of the *Local Government Act 1995*) of its intention to add:
 - a. Reserve 46840- Allendale Park, North Lake as a dogs off-leash exercise area; and
 - b. Woodman Point Beach, Coogee as a dogs prohibited area from south of the Ammunition Jetty (adjacent to John Graham Reserve) to where it intersects with the current dogs off-leash area.
- (4) notes that all other reserves not mentioned within Recommendation 3, but listed within the Alternative Recommendation of OCM 9 July 2020, Item 17.1, are reclassified accordingly;
- (5) be presented, at a later date, a proposed Animal Management Local Law and notes as part of the drafting process, that the City's other local laws may be presented at the same time, for the purposes of removing duplication between the existing and proposed local laws;
- (6) be presented a report, at a later date, to review the effectiveness of Phase 1 off-leash dog exercise areas; and
- (7) consider a budget allocation of \$85,000 as part of the 2020-2021 mid-year budget review process to fund action items listed within the Animal Management and Exercise Plan for 2020-2025.

TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL

COUNCIL DECISION

MOVED Cr M Separovich SECONDED Cr P Corke

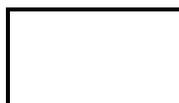
That the recommendation be adopted.

CARRIED BY ABSOLUTE MAJORITY OF COUNCIL 8/1**Background**

In July 2020, Council endorsed the release of the draft Animal Management and Exercise Plan (AMEP) for purposes community consultation and in particular, resolved:

That Council:

- (1) *Endorses the Draft Animal Management and Exercise Plan 2020–2025 for the purposes of a public comment period, subject to the following amendments:*
 - i. *Delete the proposal to add the beach directly north of Caledonia Loop, described as a portion of Property Number 2212003, as a dogs on-leash only area, thus retaining the area as a “dog prohibited” zone,*
 - ii. *Delete the proposal to add Portion of Reserve 24306-Coogee Beach, Coogee, as a dog on-leash only area, thus retaining the area as a “dog prohibited” zone,*
 - iii. *Make a Local Law to incorporate Stages 1 and 2 of the proposed Cat Management Strategy for 2021, to prohibit cats from all Regional Parks, City managed conservation areas and any identified additional bushland areas in the City which are known habitats for vulnerable fauna,*
 - iv. *Delete Stage 3 of the proposed Cat Management Strategy for 2025, and*
 - v. *Enact Stage 4 of the proposed cat management strategy as soon as practicable, but by no later than 2025, regardless of whether the amendments proposed to the Cat Act 2011 in this Plan are approved;*
- (2) *Notes that the final Animal Management and Exercise Plan 2020–2025, together with community and stakeholder feedback received during the public comment period, will be presented to Council in September 2020;*
- (3) *Gives 28 days public notice (as defined in section 1.7 of the Local Government Act 1995 of its intention to add the following reserves:*
as dog off-leash dog exercise areas:
 - a. *Reserve 45286 – Beeliar Reserve, Beeliar,*
 - b. *Reserve 48963 – Aubin Grove Reserve, Aubin Grove,*
 - c. *Reserve 39265 – Santich Park, Lake Coogee,*
 - d. *Lot 9000 Plantagenet Cres, – Goodchild Park, Hamilton Hill,*



- e. *Woodman Point Beach, Coogee, from south of the Ammunition Jetty (adjacent to John Graham Reserve) to where it intersects with the current dogs off leash area, as defined in Attachment 3,*
 - f. *Reserve 50600 – Colorado Park; and*
- (4) *Requires the following reports to be provided for further consideration by Council:*
- i. *The potential for a 24 month trial period to change the Woodman Point Beach from an “off leash area” to a “dog prohibited area” during the Fairy Tern nesting season, on dates to be identified each year,*
 - ii. *Methods to encourage cat registration and sterilisation, and*
 - iii. *Options for helping residents create cat containment areas including, but not limited to:*
 - a. *workshops with cat owners, and*
 - b. *cost subsidies from Council.*

Since this time, the public comment period has been completed on the Draft AMEP, with the Plan being amended to reflect key community feedback and concerns raised during community consultation between 30 July and 26 August 2020.

As such, the final Animal Management and Exercise Plan is now presented to Council for consideration.

Submission

N/A

Report

The growth in dog and cat ownership within the City will mean there will be an estimated 25,000 dogs and 20,000 cats within the City by 2025, a growth of 52% above the currently registered population of dogs and over 600% of cats currently.

Without a well-considered plan to manage this growth and competing priorities of public open space by a growing (human) population, there is an ever-growing need for the City to manage dogs and cats, while providing adequate facilities for dog, cat, and non-animal owners alike.

The AMEP has four (4) key strategic objectives, for Cockburn to apply best practice to animal management (refer Attachment 1).

(1) Encourage Responsible Dog Ownership

Based on a series of community workshops, the AMEP recommends a multipronged approach to encourage responsible dog ownership.

The objective is supported by increased proactive educational style enforcement patrols to ensure existing laws are observed and the behaviour of a few does not adversely impact on the majority of law-abiding dog owners.



In support of this approach, the AMEP recommends a traffic light style signage system simplifying the areas where a dog can and cannot be. Both the increased proactive patrolling by Rangers and the traffic light system has had significant support by the community throughout the consultation phase.

To improve an owner’s capacity to control their dogs, the plan recommends and budgets for increased dog training for residents and educational campaigns about how to effectively control pets in public spaces. The intent behind these types of initiatives is to provide the tools needed for dog owners to become more responsible for their pets, and also improve on the owner and pet relationship. The proposal to subsidise training is also supported by the community, and showed approximately 85% in favour of this type of program.

(2) Provide Sufficient, Safe Spaces for Pets, People and Wildlife

The AMEP recommends significant changes to the use of public open space for exercising dogs within the City. Salient changes include the rezoning of areas that have traditionally been on-leash to off-leash, when not in use by sporting groups, and recommends changes to the Woodman Point beach areas.

Key changes in detail are:

Coastline:

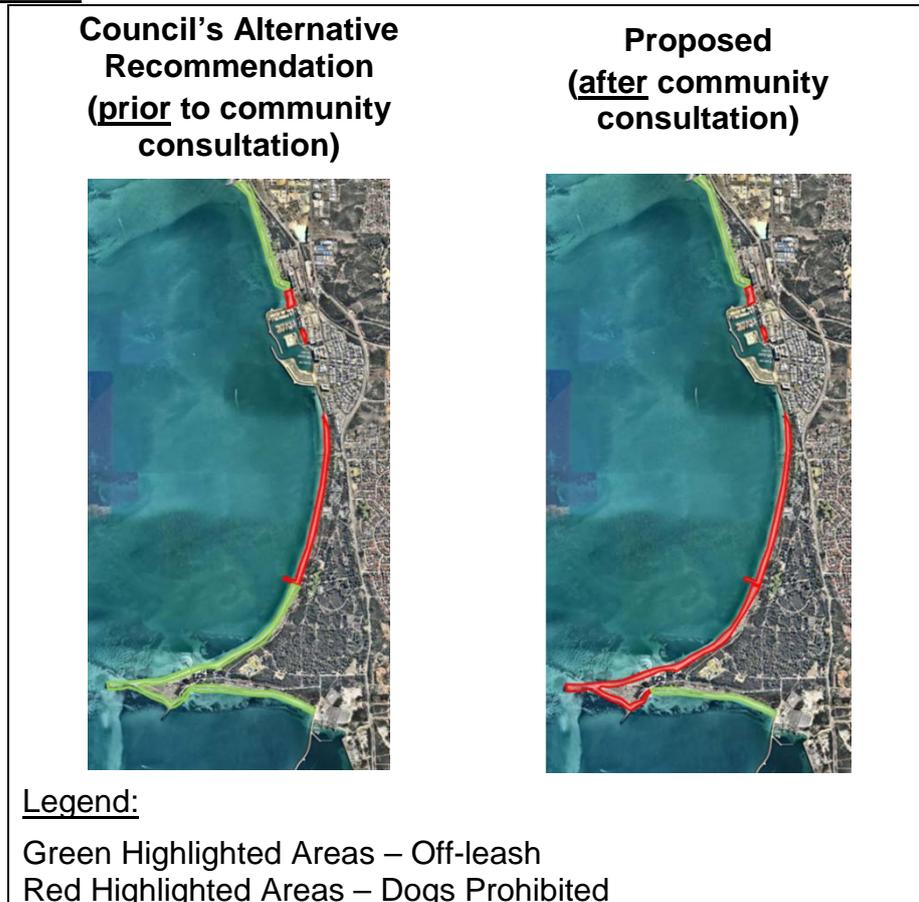


Image One – Coastal usage by dogs



As shown within Image One (1) above, the AMEP approved for public comment by Council at its July 2020 Ordinary Council Meeting shows the section of Woodman Point bound by the Ammo Jetty to the north and the Jervois Bay dog beach to the south as a proposed off-leash dog area.

Since releasing the proposal to the community for comment, the City have been inundated with feedback by members of the community. Most feedback received about Woodman Point has been by email and outside of the formal online survey (Comment on Cockburn). Provided to this report, is a table of community concerns regarding this change (refer Attachment 2). The City have provided a brief commentary of these community submissions.

The majority of emails received have raised significant concerns about possible environmental impacts of the proposed changes, mainly the impact on shorebirds.

In addition, the City has also received several letters from key stakeholders and interested parties (refer Attachment 3) from;

- Department of Local Government, Sport and Cultural Industries (DLGSCI);
- Department of Biodiversity, Conservation and Attractions;
- Woodman Point Regional Park Community Advisory Committee (two (2) submissions);
- Birdlife Australia;
- Conservation Council of Western Australia (Western Australian Fairy Tern Network); and
- Native ARC Inc.

All of these stakeholders have strongly objected to the proposal for the portion of Woodman Point changing to an off-leash area.

Further to the above received correspondence, City Officers engaged Integrate Sustainability, a consultancy company specialising in environmental research. The engagement of this consultancy service was in accordance with the July OCM Alternative Recommendation. Integrate Sustainability assessed the feasibility of leaving this section of the beach closed during the Fairy Tern nesting season.

The report 'Environmental Impact Assessment of Proposed Off-Leash Dog Beach at Woodman Point' (refer Attachment 4) from Integrate Sustainability recommends the use of the beach be reverted to a dog prohibited area. The report's recommendation is based on;

- The Fairy Tern nesting season and beach closure coincides with peak beach access periods over the summer months;
- Other shorebirds, including migratory species listed under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act) (Cth), also utilise this stretch of beach;



- There is significant potential that the change to “off leash” will modify, destroy, remove or decrease the availability or quality of habitat for Fairy Terns, other shorebirds and migratory shorebirds and triggers the need to refer the action under the EPBC Act;
- Rezoning is not in alignment with the Woodman Point Regional Park Management Plan.

Based on the findings of the above-mentioned report and the consultation from key stakeholders, the City have amended the proposed plan to alter Woodman Point to a dog prohibited area.

Reserves:

The AMEP recommends a three-phase approach to deregulating some large public open spaces to dog owners to exercise their pets when reserves or parks are not being used by a recognised sporting group or school activities.

During the community consultation of the AMEP, the City received a petition (refer Attachment 5) to reclassify Bishop Park, Spearwood from off-leash to on-leash. The petition is based on a recent dog attack, where the dog was uncontrolled by their owner, left the park and attacked and killed a neighbouring resident’s cat.

A review of Bishop Park shows that since 2013 there have been two reported incidents to the City’s Rangers, both were for dog wandering. Considering the period of which Bishop Park has been a designed off-leash area, these two incidents indicate the rarity of complaints specific to the park.

When compared to the daily usage of Bishop Park by dog owners, an immediate change to the zoning could be seen as disproportionate. Based on the reported rarity of events, the size of the park and usage, it is recommended that Bishop Park remains off-leash.

Image Two (2) below, provides an overview of the proposed methodology within the AMEP to progressively roll-out future off-leash dog exercise areas throughout the City.



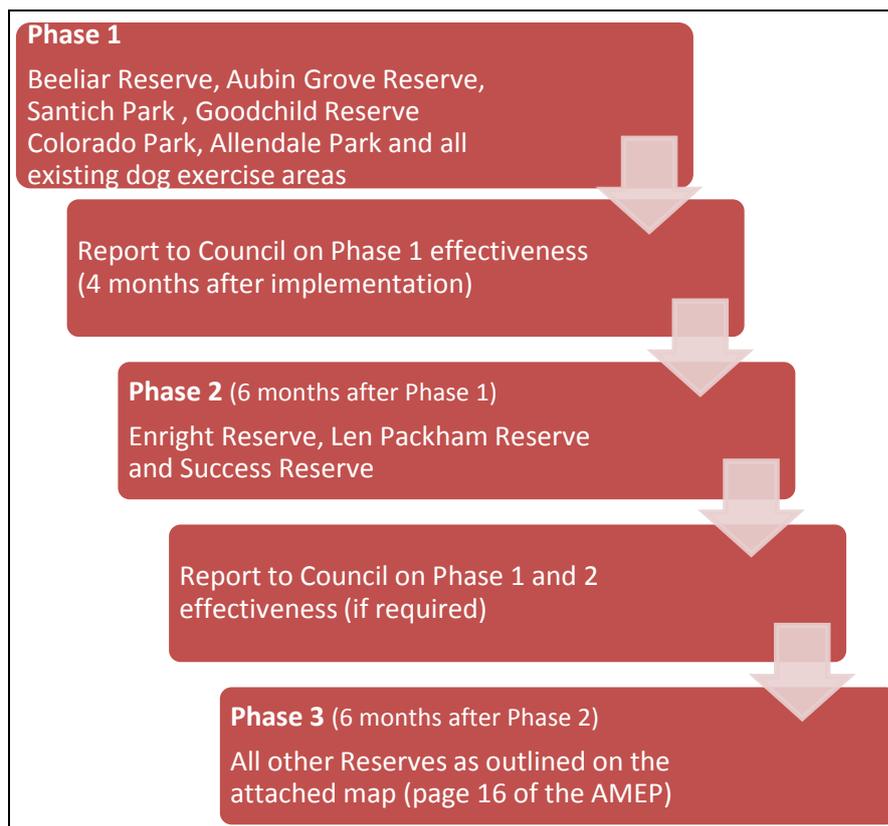


Image Two – Phased Approach

Further to the deregulation of the above active reserves, the AMEP also proposes the construction of two dog parks at Radonich (Beeliar) and MacFaull (Spearwood) Parks in 2021/22 and 2022/23 respectively.

In 2021, further detailed design in conjunction with residents neighbouring these parks will be undertaken. Pending final community consultation, the overall design may include an unfenced portion to encourage responsible dog owners' use of the park.

During the community consultation phase of the AMEP, the City received a petition (refer Attachment 6) from residents supporting the change of Allendale Park, North Lake, from a dog on-leash area to an off-leash dog exercise area. Upon review, City Officers support this rezoning. The Officers' support is based on other usable parks within the area, the size of the Reserve in relation to other areas, and the use of the land by numerous dog owners.

(3) Manage Feral and Domestic Cats

The AMEP has made significant recommendations regarding the overall management of cats within the City. The AMEP, with support of an amended Council Recommendation, proposes the creation of a Local Cat Law via a two-stage process. Should the proposed Local Cat Law come into effect, owners of cats will be required to keep cats confined to their properties, unless under effective control, and prohibited absolutely from all Conservation Reserves and significant bushland areas.

The AMEP also recommends two types of trapping: community based and conservation-based trapping by City Officers or contractors. Both of these programs will go hand in hand with a community education campaign about responsible cat ownership. An already previously funded cat program will commence in September 2020 to provide information to cat owners about the wellbeing of cats and their environmental impacts.

(4) Encourage Smart City, Digital Initiatives for Animal Management

Currently, City Rangers are leading the way in the use of digital technology. Future potential applications of digital innovations include:

Objective 1 - Encourage responsible dog ownership

- Provide subsidised dog and cat microchipping;
- Install geo-fenced areas with RFID readers to tag dogs entering dog prohibited areas;
- Improve the Ranger's ability to integrate internal data relating to previous dog offence history; and
- Provide dog registration kiosks at dog-friendly community events and dog exercise areas.

Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife

- Install CCTV cameras and analytics programs to monitor the usage of fenced dog parks;
- Provide solar-powered sensor lights for dog owners using fenced dog parks afterhours; and
- Install outdoor dog wash stations at CY O'Connor Beach.

Objective 3 - Manage feral and domestic cats

- Install smart trapping and GPS tagging of traps within conservation reserves.

Strategic Plans/Policy Implications

Environmental Responsibility

A leader in environmental management that enhances and sustainably manages our local natural areas and resources.

- Sustainably manage our environment by protecting and enhancing our unique natural coastal, bushland, wetlands areas and native wildlife.
- Provide accessible high-quality open spaces and parks for community benefit.



Community, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

- Facilitate and advocate for increased community safety.
- Provide community, sport, recreational, and cultural facilities and infrastructure to meet our community needs.

Budget/Financial Implications

The development of a five year implementation plan has been included within the proposed AMEP. The overall expenditure outlined within the Draft Plan over 5 years is approximately \$550,000. Due to the limitation in external funding for animal control, there is little opportunity to explore grants to assist in subsidising the implementation of identified expenditure. Should external opportunities present themselves, staff will undertake best endeavours to apply and receive funding.

The below table is a summary of proposed expenditure.

| Objective | 2020-21 | 2021-22 | 2022-23 | 2023-24 | 2024-25 | Total |
|---|------------------|------------------|-----------------|------------------|------------------|------------------|
| Objective 1 - Encourage responsible dog ownership | \$60,000 | \$70,000 | \$55,000 | \$40,000 | \$40,000 | \$265,000 |
| Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife | \$75,000 | \$43,000 | \$40,000 | \$0 | \$0 | \$158,000 |
| Objective 3 - Manage feral and domestic cats | \$0 | \$23,000 | \$0 | \$0 | \$0 | \$23,000 |
| Objective 4 - Embrace Smart City initiatives in animal management | \$0 | \$15,000 | \$0 | \$70,000 | \$60,000 | \$145,000 |
| Total | \$135,000 | \$151,000 | \$95,000 | \$110,000 | \$100,000 | \$591,000 |

The AMEP proposes many projects to begin within the 2020-21 financial year. Some of these identified projects were not initially funded within the 2020-21 municipal budget, and as such, this report recommends a budget adjustment of \$85,000 to be considered by Council at the 2020-21 mid-year budget review. Approximately \$50,000 of the proposed costs identified within 2020-21 can be funded from the existing approved budget.



Legal Implications

The AMEP has several proposed action items which involve numerous legal processes before they come into effect as listed below.

Changing Off-Leash/On-Leash Dog Areas

Pursuant to section 31(3A) of the *Dog Act 1976*, the creation of dog off-leash areas will require an Absolute Majority of Council to enact changes. Once the Council adopts the new locations, a period of 28 days statutory advertising will commence shortly afterwards. After the statutory period ends the locations are specified accordingly.

Signage is not a legal requirement but is considered best practice.

Woodman Point Dog Off-Leash Area

Any action that is likely to generate a significant impact to threatened species requires approval of the Federal Minister for the Environment. If approval is not obtained an offence may be committed under the *Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act)*.

Under the EPBC Act, some species of shorebirds which have been recorded at Woodman Point are listed as threatened species.

Should Council continue to seek a dog off-leash exercise area at Woodman Point, Ministerial approval should be sought prior to implementing any changes.

Creation of Local Cat Laws

The creation of a new Local Law is set out in section 3.12 of the *Local Government Act 1995*.

The Act requires a detailed process to be followed and that copies of proposed laws are publicly advertised and forwarded to the Minister for Local Government and other relevant State Ministers to fulfil these requirements. The Department of Local Government, Sport and Cultural Industries examines the proposed local laws on behalf of the Minister.

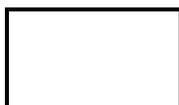
Community Consultation

During development of the AMEP, there were a number of public consultation processes completed. This is in addition to the significant amount of public consultation which occurred as part of the development of the AMEP.

The engagement process has included three stages:

Stage 1 - AMEP Development

The first stage was to hold four workshops with a focus group of dog owners, cat owners and non-animal owners. The workshops provided the basis to identify the priority issues and the development of a brief to engage an external consultant to complete further in-depth engagement.



Stage 2 – AMEP Community needs and sentiment

Stage 2 of the engagement process was undertaken by the external consultant (Catalyse) that included:

- An online survey through the City of Cockburn “Comment on Cockburn” portal;
- Internal staff workshops;
- Community workshop;
- Meeting with other local governments; and
- Research of trends on dog and cat management nationally and internationally.

In addition to the general promotion of the survey, invitations were sent to 2,000 randomly selected households (1,000 by mail and 1,000 by email). 373 residents subsequently completed the survey.

Stage 3 – Community Consultation on the Draft AMEP

Following endorsement by Council, the draft AMEP was released for community consultation for 28 days via the online Comment on Cockburn portal. During this period, the City received over 500 submissions, over 400 of which were through Comment on Cockburn and attached (*Attachment 7*) to this report.

The majority of survey respondents supported the proposed strategies contained within the Animal Management and Exercise Plan, with the exception of the proposal to ‘Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash’ and investigate the potential to have the area as a ‘dog prohibited zone’ during the Fairy Tern nesting season.” In addition to the survey the majority of email submissions received also opposed this change and requested that the Woodman Point area be made a dog prohibited area.

In addition to the online survey, the City’s officers have;

- Sent letters to all residents who reside within 100 metres of parks (except Allendale Park, North Lake) that are proposed within Phase 1 of the off-leash dog exercise areas.
- Held Community listening posts at;
 - Macfaull Park
 - Radonich Park
 - Woodman Point Beach
 - Cockburn Gateways
- Undertaken a Facebook Live event;
- Created an online promotional video for community promotion of the AMEP consultation (advertised on social media);
- Liaised with Beeliar and Aubin Grove Primary Schools; and
- Requested input from all sporting groups that use parks and reserves effected by the AMEP.

During the above consultation, Beeliar Primary School has also provided an attached (*Attachment 8*) letter of support.



Risk Management Implications

If Council decides not to adopt the proposed Animal Management and Exercise Plan 2020-2025, there is a potential “Substantial” level of associated brand/reputational risk as the feedback provided by the community on the Draft Plan has been supportive overall.

Should the AMEP not be adopted Woodman Point beach will become an off-leash area, as the 28 day notification period as per Council’s resolution in July has now passed. This creates a greater risk of environmental damage as highlighted within *Attachment 4 – Environment Study - Woodman Point Dog Area*. Should this damage occur, there may be “significant” community concern and possible compliance and legal implications under the EPBC Act.

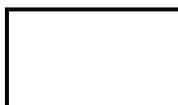
The requirements of the proposed Local Cat Laws have been initially reviewed against the powers afforded to the City by the *Cat Act 2011*. On initial review the suggested cat control measures appear to be lawful, however, all new local laws are reviewed by the Joint Standing Committee after gazettal, if at this time the laws are found to be in contradiction of State laws, the local law would be repealed. Should the local law be repealed by the Committee there may be brand reputational risk.

Advice to Proponent(s)/Submitters

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 10 September 2020 Ordinary Council Meeting.

Implications of Section 3.18(3) *Local Government Act 1995*

Nil





City of Cockburn Animal Management and Exercise Plan 2020–2025



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Executive Summary

The City understands that pets are an important part of people's lives and that they contribute to enhanced wellbeing. Australia has one of the highest rates of animal ownership in the world, with 67% of households owning a dog or cat. Without proper management, previously domesticated animals such as cats, can destroy the native environment, and without proper control, dogs can become a nuisance and dangerous to the public.

Effectively managing the threats posed by pet species requires commitment, a coordinated effort, and action from a range of key stakeholders.

Through increased development, population growth and animal-friendly facilities, the City of Cockburn is committed to educating the community and visitors about their obligations as responsible pet owners, so that animals and the community can coexist harmoniously.

Responsible pet ownership goes further than caring for the health, welfare and safety of our pets. Animal owners are socially and legally responsible for managing their pets in such a way that ensures their animals are compliant with all relevant laws and do not create a nuisance, whether in terms of impact to the community or the environment.

This Plan includes strategic objectives, guiding principles and priority outcomes to direct the coordination of animal management for the City of Cockburn.



Introduction

The purpose of the Animal Management and Exercise Plan 2020-2025 is to guide the City's approach to promoting responsible pet ownership and ensuring that facilities are appropriately accessible and equipped to manage the growing population of pets within the City.

Furthermore, the plan identifies opportunities to assist dog and cat owners to understand their responsibilities while also informing the general community about the City's role and future priorities. The plan also recommends key changes to simplify the dog on-leash versus off-leash areas within City Reserves, Parks and the Coastline.

Aims and Objectives

The aim of this Plan is to support the City of Cockburn to achieve its vision for Cockburn to be the best place to be. Responsible pet ownership and thoughtful and sustainable animal management will help the City of Cockburn becomes the place to be for dogs and cats too.

The objectives of this plan are:

1. Encourage responsible dog ownership;
2. Provide sufficient, safe spaces for pets, people and wildlife;
3. Manage feral and domestic cats; and
4. Embrace smart city initiatives in animal management.

Links to the Strategic Community Plan 2020 – 2030

The Animal Management & Exercise Plan supports the following key objectives in the Strategic Community Plan 2020-2030:

Table 1 Links to the Strategic Community Plan 2020 – 2030

| | |
|--|--|
| Community, Lifestyle and Security | 3.2 Facilitate and advocate for increased community safety . |
| Environmental Responsibility | 2.1 Sustainably manage our environment by protecting and enhancing our unique natural coastal, bushland, wetlands areas and native wildlife. 2.3 Provide accessible high-quality open spaces and parks for community benefit. |

Legislation and Animal Management

Administration of animal management within Western Australia is set out in applicable legislation, predominantly the *Dog Act 1976*, the *Cat Act 2011*, and the City of Cockburn Consolidated Local Laws. This legislation authorises the City to enforce compliance, and respond to or act on animal complaints and incidents, in a manner that is consistent with its powers as granted in legislation. This legislation also directs Council to collect revenue through fee-based services such as animal registration and impounding.

Dog owners must meet the following legal requirements;

- 1) Abide by the *Dog Act 1976* and the City of Cockburn (Local Government Act) Local Laws 2000;
- 2) Ensure their dog has a microchip;
- 3) Current registration from three months of age and that it wears a collar with a registration tag at all times;
- 4) Keep their dog on-leash at all times in public places, unless posted signs indicate otherwise;
- 5) Do not allow their dog to enter areas signposted as 'dogs prohibited' and keep their dog under effective control in designated off-leash areas;
- 6) Pick up and correctly dispose of all animal waste;
- 7) Observe special conditions for owning a greyhound or declared dangerous and restricted breed dog;
- 8) Unless approved, do not keep more than two dogs on their property; and
- 9) Ensure their dog is securely confined within their property.

Cat owners must meet the following legal requirements;

- 1) Abide by the *Cat Act 2011* and the City of Cockburn (Local Government Act) Local Laws 2000;
- 2) Ensure their cat has a microchip, is sterilized (desexed), and has current registration from six months of age;
- 3) Ensure their cat wears a collar with its registration tag at all times in public places; and
- 4) Do not keep more than three cats on their property.

Animal welfare is also a key concern for the community. In WA, animal welfare is legislated by the *Animal Welfare Act 2002* and administered by the Royal Society for the Prevention of Cruelty to Animals (RSPCA) through its Inspectors.



Context of Animal Owners Within the City of Cockburn

According to RSPCA data, 38% of households own a dog, and 29% of households own a cat in Australia. On average, dog owners will have 1.3 dogs, and cat owners will have 1.4 cats. It is estimated that there were 21,579 dogs and 17,735 cats within the City of Cockburn in 2019. By 2024, it is forecast that there will be 24,296 dogs and 19,968 cats in the local area.

Legislation requires that all dogs over three months and all cats over six months must be registered and microchipped and cats must also be sterilised. Analysing current registrations against RSPCA statistics shows that many dogs and cats are not registered, microchipped or sterilised. In 2019, it was estimated that 61% of dogs were registered and 17% of cats were registered in the City of Cockburn. While dog registrations have been increasing, cat registrations have been declining.

Table 2 Animal Registration (2017 – 19) Summary Within the City of Cockburn

| City of Cockburn | | 2017 | 2018 | 2019 | Trend |
|---|--------------------------------|--------|--------|--------|-------|
|  | Estimated number of dogs | 20,575 | 21,087 | 21,579 | ↑ |
| | Registered dogs | 11,986 | 12,467 | 13,159 | ↑ |
| | Estimated % of dogs registered | 58% | 59% | 61% | ↑ |
|  | Estimated number of cats | 16,909 | 17,331 | 17,735 | ↑ |
| | Registered cats | 4310 | 3,905 | 3,088 | ↓ |
| | Estimated % of cats registered | 25% | 23% | 17% | ↓ |

There is a need to reverse the downward trend in cat registrations and accelerate growth in dog registrations. In accordance with legislation, the City is aiming for all dogs and cats to be registered.

Community and Stakeholder Consultation

At the beginning of 2019, City Officers undertook extensive consultation with members of the community, which included animal owning and non-animal owning residents, key stakeholders groups, and local government authorities across Australia. The objective of the consultation was to seek an understanding of community needs for services and improve the accessibility and compliance of dog and cat owners within public open spaces.

Further complementing the series of community workshops, an external consultant was engaged to oversee an online survey and dedicated workshop to finalise the community’s views on proposed recommendations within this Plan. Views of more than 600 residents and City of Cockburn employees were gathered. The engagement program included:

- A postal and online survey, and a community workshop with a representative sample of dog owners and non-dog owners from across the City;
- In-depth interviews with opinion leaders;
- Meetings and workshops with internal stakeholders in animal management, parks and reserves, safety and community engagement officers employed by the City of Cockburn; and
- Social media engagement via the City’s Facebook page and online engagement portal.

A complete report of these findings is attached as Appendix one (1) to this Plan.

The key themes which arose from the Community Engagement Process were:

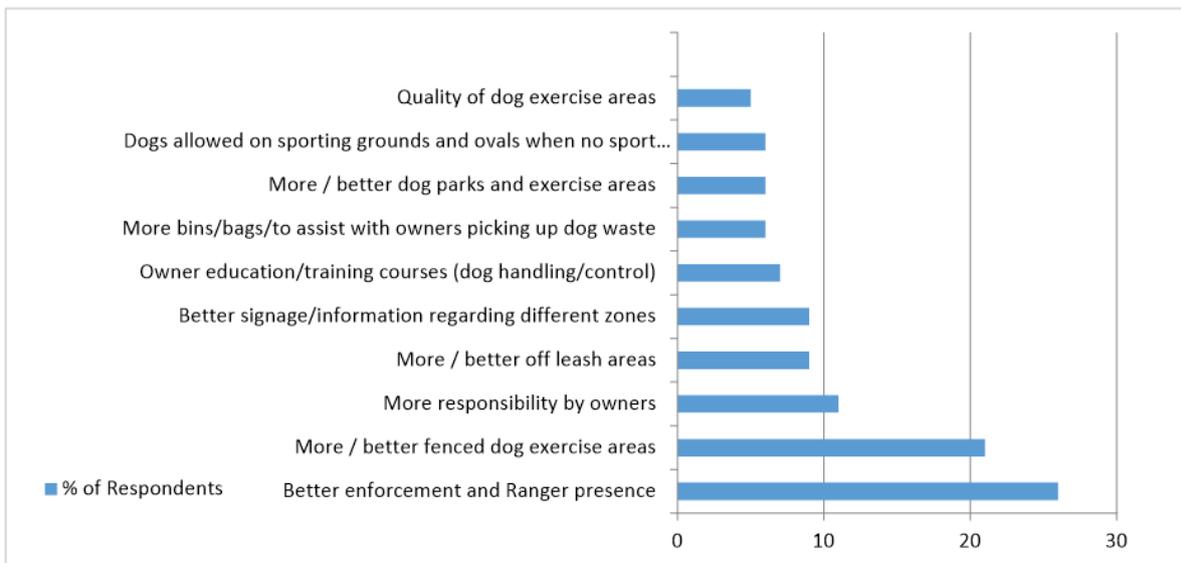


Chart 1 Community Priorities for Animal Management



Key Strategic Objectives

Objective 1 - Encourage Responsible Dog Ownership

Dog owners and non-dog owners have different needs, values and expectations. There are conflicting demands across the community between people who are comfortable with dogs versus those who are not. As the population grows, the potential for conflict is likely to rise, and this may include dogs versus children, dogs versus sport, dogs versus dogs, and dogs versus wildlife.

To enable people, pets and wildlife to coexist, there is a need for greater promotion of responsible pet ownership guidelines, participation in dog obedience training and stronger enforcement to encourage responsible behaviour.

The main community concerns are:

- **Keeping dogs out of 'Dogs Prohibited' areas** - a higher concern for non-dog owners (84 points) versus dog owners (50 points).
- **Owner supervision and control of dogs in off-lead areas** - a higher concern for non-dog owners (82 points) versus dog owners (66 points).
- **Dog behaviour** - dogs responding to their owners' commands, not jumping or running up to people or other dogs, etc. A higher concern for non-dog owners (82 points) versus dog owners (55 points).
- **Management of dog waste** – one of the top concerns (81 points within the online community survey) across the community with similar levels of concern among dog owners and non-dog owners. The City's effectiveness score in this area was only 45 out of 100.
- **Ensuring dogs are kept on-lead in on-lead areas** - a higher concern for non-dog owners (78 points) versus dog owners (45 points).

With the exception of managing dog waste, non-dog owners expressed greater concern than dog owners for responsible dog ownership indicators in all the main community concerns.

Community Sentiment - Enforcement

Community consultation identified a strong sentiment for an increase in enforcement by City Rangers, by both dog and non-dog owners.

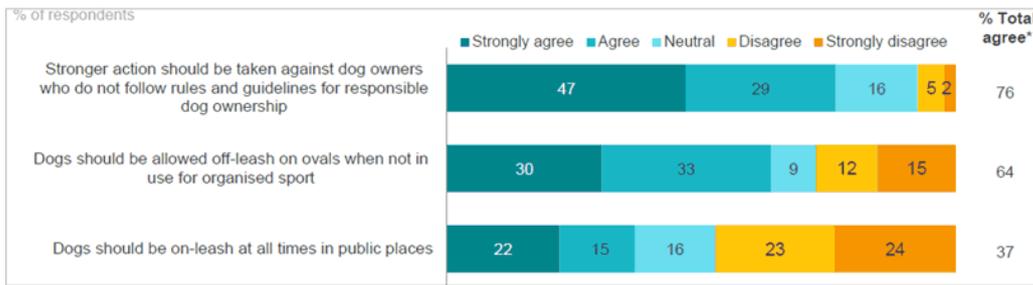


Chart 2 Community Sentiment for Animal Management

Respondents from the community consultation were asked to provide a suggested 'Council Action Plan'. As seen within Chart three (3) below, stronger enforcement by City Rangers is seen as the most supportive action to be undertaken, closely followed by improved and additional dog parks.

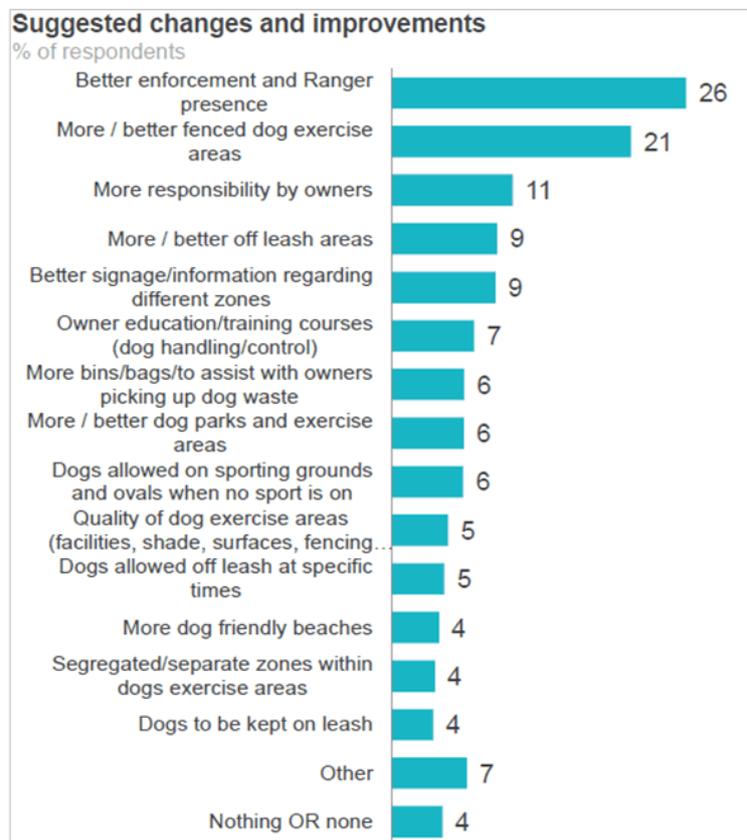


Chart 3 Community Created Council Action Plan

To encourage more responsible pet ownership, there is a requirement to:

- **Increase enforcement** – 93% of non-dog owners and 72% of dog owners would like stronger action taken against dog owners who do not follow responsible dog



ownership rules and regulations.

- **Improve communication of responsible pet ownership guidelines** – only 45% of respondents feel there is sufficient information provided on the guidelines for responsible pet ownership.
- **Improve promotion of dog obedience training** – while 71% of dog owners say they have taken their dog to obedience training, only 23% of dog owners and 17% of non-dog owners feel there is sufficient promotion and provision of dog obedience training.
- **Improve signage** – 23% of respondents feel there is insufficient signage to indicate where dogs are permitted on and off leash.

A traffic light system of signage has been developed as a result of these findings. The roll-out of this system will be completed at all prominent reserves and dog-friendly locations. Within the table three (3) below is a proposed version of the traffic light style signage.

Table 3 Proposed Traffic Light Style System

| | | |
|---|--|---|
|  |  |  |
| <p>Dog off-leash area. Dogs may be off-leash within the area, but effective control by the owner is still required. Dogs must be held by a leash when;</p> <ul style="list-style-type: none"> • designated sporting fields are in use; • designated school activity in progress • city officers are using power tools or mowing; or <p>At the direction of a City Officer.</p> | <p>Dog on-leash area. Dogs allowed within the area but must be on leash. Unless within an enclosed dog park.</p> | <p>Dog Prohibited area. Dogs are not allowed within the area either on or off-leash. Registered assistance dogs are exempted.</p> |

Mobile signage will also be further investigated and implemented to highlight key patrol areas for City Rangers as part of their proactive enforcement patrols. Some community suggestions through consultation highlighted the need to install dog etiquette style signage in dog exercise areas, to educate dog owners, and promote interaction between owners their pets.



Objective 2 - Provide sufficient, safe spaces for dogs, people and wildlife

Within the City of Cockburn there is a myriad of areas where dogs can exercise, and owners can socialise. The needs of dogs, dog owners and non-dog owners vary greatly, and the City caters for different needs with a mix of dogs prohibited parks, reserves and beaches, dogs on leash parks, reserves and beaches, and dogs off-leash exercise areas, including five fenced dog exercise parks.

As part of the Plan's community consultation, there have been strong and varied views on the appropriateness of some of these areas, but also a 'cutting of red tape' approach to allowing responsible owners to walk and exercise their dogs.

The Plan has broken this objective into key areas:

- Dogs access along the coastline;
- Off-leash areas within reserves; and
- Enclosed dog parks.

Dogs Access Along the Coastline

Coastline Usage:

Community feedback has shown a strong desire to maintain dog access to the City's coastal areas and beaches. The community survey showed CY O'Connor Beach is the most popular beach to visit with a dog (56%), followed by Jervois Bay Dog Beach (40%) and Woodman Point (20%). Although dogs are prohibited on Coogee Beach, 15% of dog owners surveyed have visited this beach with their dog in the past 12 months.

When surveyed, 42% of non-dog owners are unhappy with designated dog access zones along the coast. They would like dog owners to be more responsible for their pets and for greater enforcement of on-leash requirements. The practicality of constant enforcement within this area is difficult, and will likely require a significant increase in Ranger resources to maintain a strong enforcement capability along the coast, especially during the summer months.

Environmental Impacts:

Research has shown that dogs on a leash often have the same environmental impact on sea nesting birds, as those not on a leash. The Department of Biodiversity, Conservation and Attractions, and members of the community are concerned about the impact of dogs on Fairy Terns. Fairy Terns are known to nest at Woodman Point and are listed as vulnerable under both state and federal legislation and thus are a protected species. In

Western Australia, predation from domestic and feral animals such as dogs, cats and foxes has reduced population numbers and breeding success.

Further concern has been raised by BirdLife Australia, a prominent bird protection organisation in Australia. Recent research into the Woodman Point area has shown the beach is an ideal habitat for a wide range of migratory and resident shorebirds, including species like migratory Ruddy Turnstone, Grey Plovers, and the Grey-Tailed Tattler. Up to 26 species of shorebird have been recorded at the site. The area also could potentially provide breeding habitat for resident beach-nesting birds, including Red-Capped Plovers and Oystercatchers, though the amount of human activity and the frequent presence of dogs has likely impacted birds selecting the area to breed in recent years. Making the area dog free, as well as measures taken to facilitate Fairy Tern breeding will also benefit other beach-nesting species.

Research has shown dog walking (both leashed and unleashed) is a major disturbance to shorebirds, due to their need to spend large amounts of time feeding in the shallows and exposed sand flats. They do this to build body condition in preparation for their long migration to the northern arctic to breed, and to recover when they return to Australia.

Council at its July 2020 meeting requested Woodman Point be changed from an on-leash area to off-leash area. The change was due to long-standing history of dogs being allowed at Woodman Point, and current usage suggests there a few dogs that use this stretch of beach.

Humans and dogs compete with shorebirds for the use of these spaces to walk, run and fish. There is a growing body of research indicating that frequent disturbance can significantly impact shorebird feeding behaviour, causing birds to waste energy in repeated flight to get out of the path of dogs and their owners. In some cases, this can cause birds to stop using a site altogether.

The general community is highly concerned about keeping dogs away from wildlife and nesting birds. The level of concern is 85 points among non-dog owners and 75 points among dog owners.

Community Awareness:

There is relatively low community awareness of designated dog exercise zones along Cockburn's coast. Only 59% of dog owners and 68% of non-dog owners were aware of the zones when shown image 1 widely used by the City on its website and billboards.

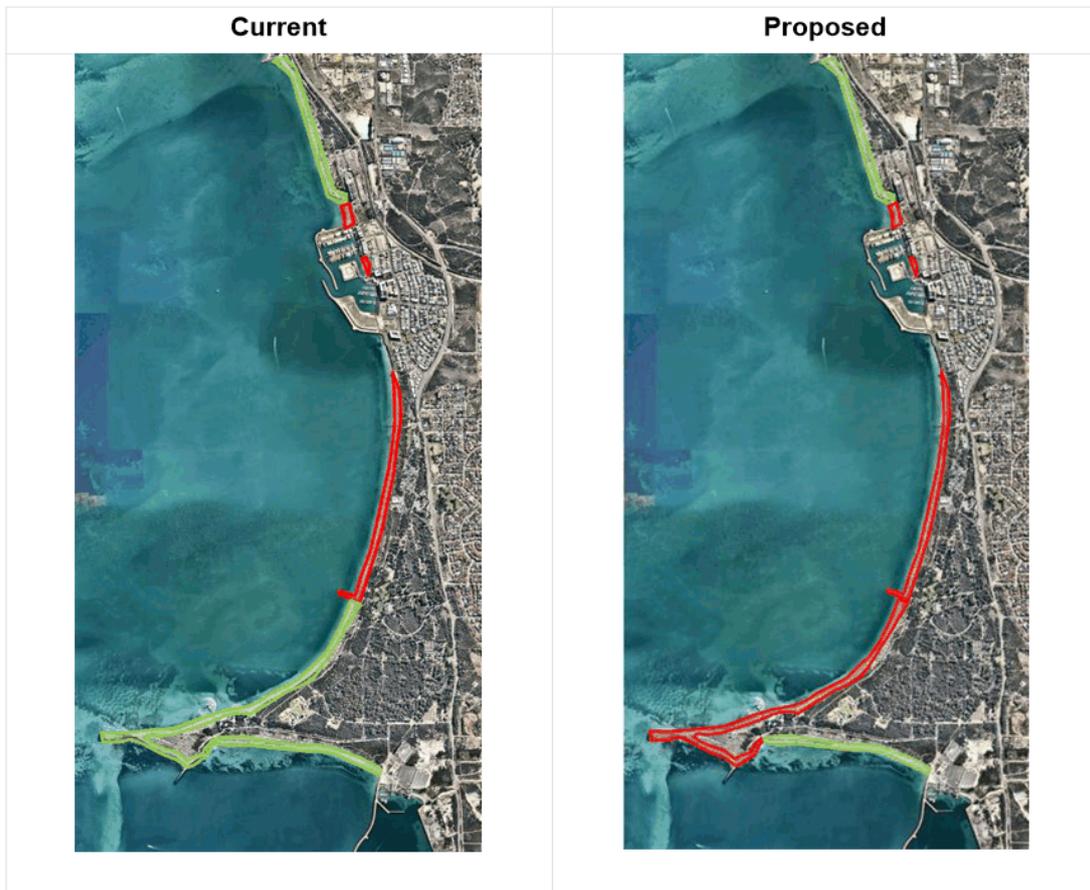
Based on feedback from the community and considering the Council decision, the Plan recommends changes to the coastal areas. The proposed changes are highlighted in table four (4) below.

Image 1 Public Promotion Image



Table 4 Current and Proposed Coastal Changes

| Legend | |
|---|-----------------|
|  | Dogs prohibited |
|  | Dog off-leash |
|  | Dogs on-leash |



| Port Coogee and CY O'Conner (No change) | |
|---|--|
| Current | Proposed |
|  |  |
| Coogee Beach (No change) | |
| Current | Proposed |
|  |  |
| Woodman Point | |
| Current | Proposed |
|  |  |



Off-Leash Areas within Reserves

Only 39% of respondents were happy with the availability and mix of on-leash and off-leash, fenced, and dogs prohibited areas. 68% of non-dog owners think dogs should be on a leash at all times in public places, while only 29% of dog owners feel the same way.

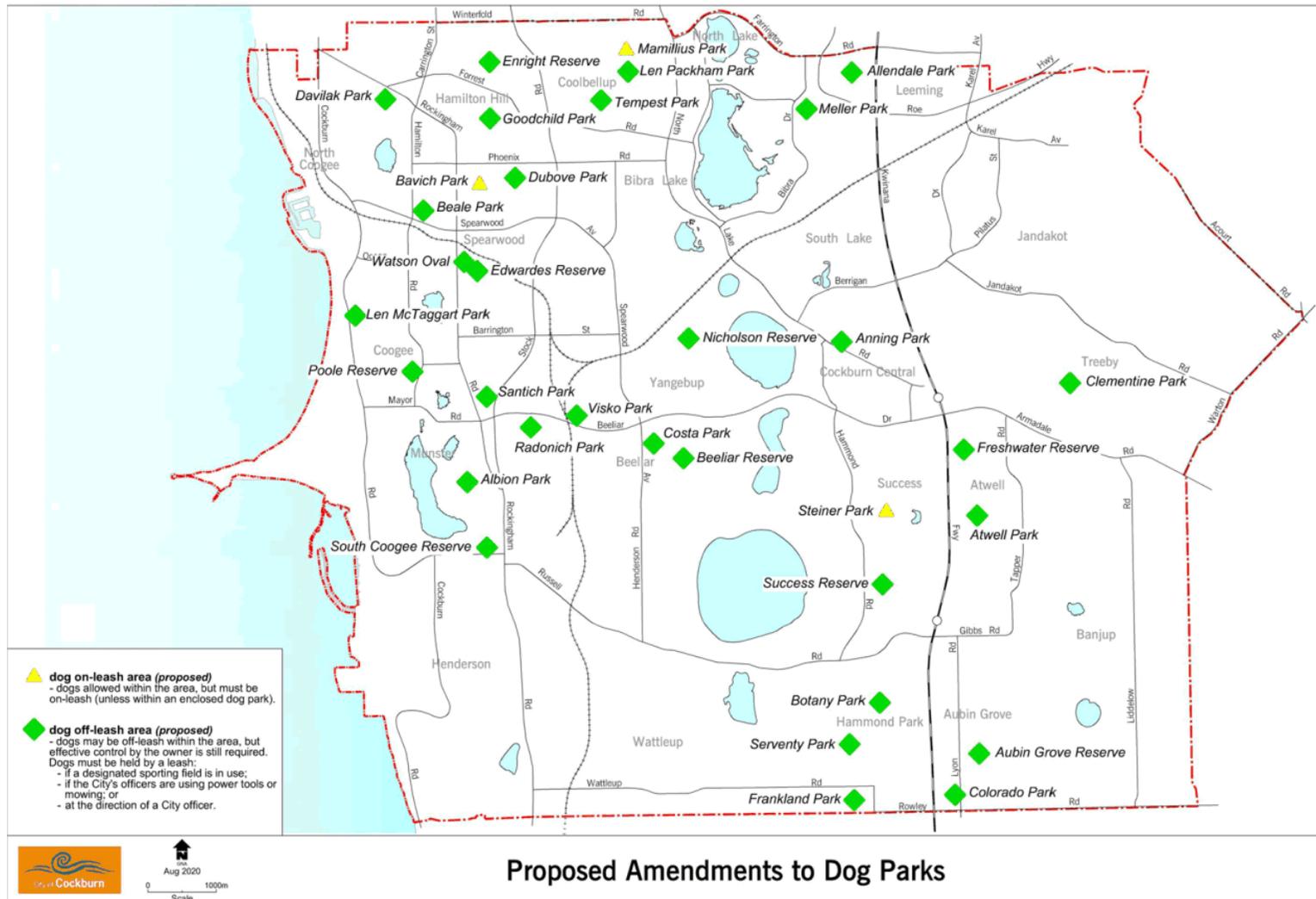
Residents are fortunate to have a number of large and well-maintained reserves throughout the City, which for the most part require owners to have their dogs on a leash. Community feedback shows this seems to be over-restrictive, especially considering most reserves are not heavily utilised at times when most dogs are walked.

Of those surveyed, 73% of dog owners think dogs should be allowed off-leash when ovals are not in use for organised sport, versus only 25% of non-dog owners. The main concern amongst dog and non-dog owners alike was the removal of dog waste. Upon review of current complaints relating to off-leash reserves, there does not appear to be an increasing trend of more dog waste left by owners.

To meet the growing community's need to change regulations relating to where and when dogs can be off-leash, this Plan proposes expanding the use of the traffic light style of signage recommended within this Plan, and expand the City's designated dog areas into three categories:

- Red – Dog prohibited area. Dogs are not allowed in this area either on or off-leash. Registered assistance dogs are exempted.
- Yellow - Dogs are allowed in this area; however, they must be leashed, unless within an enclosed dog park.
- Green - Dogs may be off-leash in this area, but effective control by the owner is still required. Dogs must be held by a leash;
 - When designated sporting fields are in use;
 - When designated school activity in progress;
 - When City Officers are using power tools or mowing; or
 - At the direction of a City Officer.





As part of a change management process, this Plan recommends an initial trial of this system as outlined in image two (2) below.

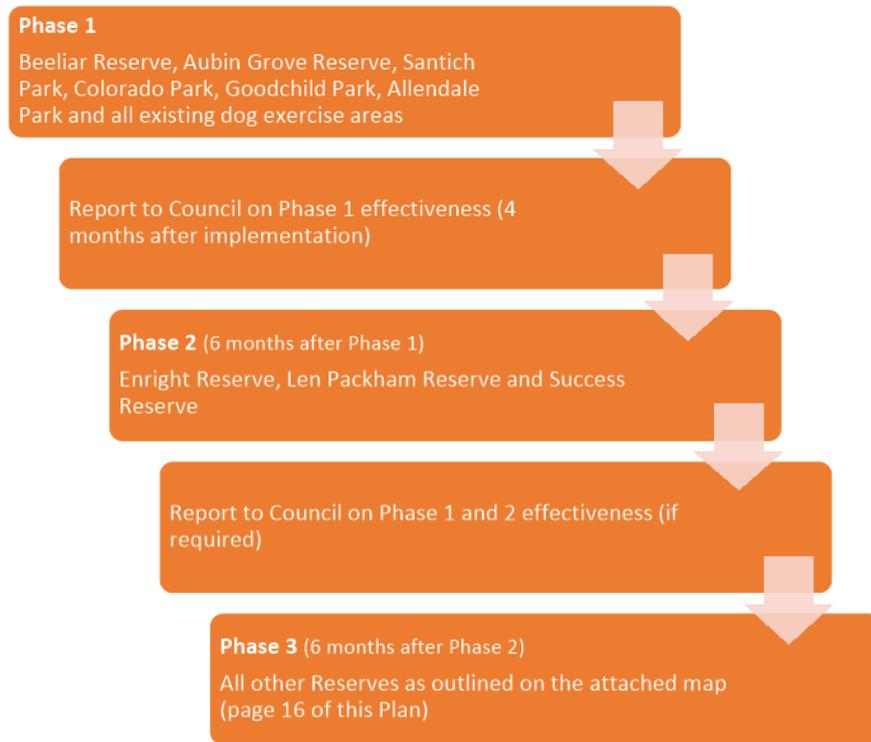


Image 2 Active Reserve Phased Approach

Enclosed Dog Parks

Enclosed dog parks are on-trend, and an increasing number of Local Governments are opening new parks. In addition to parks with separate areas for small and large dogs, most enclosures also include agility equipment and water fountains.

Although dog exercise parks are growing in popularity, there are recognised challenges with enclosed dog parks, mainly the cost of maintenance (requiring approximately \$50,000 replacement turf every 18 months), and a lack of supervision by owners. In the South Australia Dog and Cat Management Board’s *Unleashed: A Guide to Successful Dog Parks*, long, linear enclosed dog exercise areas are recommended to encourage owners to walk or run alongside their dog to maintain supervision. Other concerns with enclosed dog parks areas include:



- dog owners treating the enclosures as a 'set and forget' environment where owners are distracted by socializing or being on their phone, without maintaining appropriate supervision of their dogs,
- ease of access to facilities - walking distance from home,
- poorly designed areas, lack of shade, inappropriate to contain smaller dogs,
- a view that fenced areas give dog owners a false sense of security and cause some to pay less attention to the whereabouts of their dog,
- need for better equipment, more stimulating plants at parks,
- lack of maintenance - grass, gates and replacement dog waste bag dispensers.

The following assessment criteria is recommended to evaluate the proposed locations of future dog exercise areas.

| Dog Exercise Areas Assessment Criteria | |
|---|---|
| Dog Owner Ratio | <ul style="list-style-type: none"> • A dog park should be considered where there is approximately 3,000 dogs within 5km of a park. |
| Demographics | <ul style="list-style-type: none"> • Population size, profile, housing density. • Number of dog registrations, dog profile (age, type, etc.). |
| Current situation | <ul style="list-style-type: none"> • Current access to off-leash areas, walking / driving distance • Current usage, satisfaction. |
| Community needs | <ul style="list-style-type: none"> • level of demand, user profile, expected frequency of use. • Fencing only installed to assist with dog, bicyclist and pedestrian safety . |
| Appropriateness of location | <ul style="list-style-type: none"> • Size of area - min 1.5 ha. • Land ownership - owned or vested to the City of Cockburn. • Site features – drainage, some natural vegetation and topography, shade, natural barriers / fencing, waste disposal options and access to a water source for fountains. • Environmental / biodiversity impacts. • Accessibility – within walking distance of residential areas, accessible by road, footpaths or trails, and adequate parking provisions. • Safety - not isolated, able to be monitored, and good lighting. • Proximity to complimentary activities – for casual surveillance and shared infrastructure and shared costs (parking, lighting, toilets, general waste bins, water, etc). |
| Costs | <ul style="list-style-type: none"> • Cost of establishment and maintenance. |

18% of dog owners feel there are sufficient fenced dog exercise areas in the City of Cockburn; lower among owners of large dogs (15%) versus small dogs (23%). Most would prefer separate exercise areas for big and small or timid dogs. Based on these findings, the Plan recommends using the above guidelines, in a multi-pronged approach;

1. Construct two new enclosed dog parks within the City;
2. Investigate removing unnecessary fencing at existing and proposed parks to promote better dog and non-dog owner interaction; and
3. Improve sensory and education material within existing parks.

Macfaull and Radonich Park were selected due to the distance away from existing enclosed dog parks in the Central and Eastern Wards of the City boundaries. The City officers highlight the need that further specific community consultation with neighbouring residents should be undertaken prior to any construction of these proposed parks.

With the construction of two dog parks at Macfaull and Radonich Park, most of the City's residents would live within a 3km catchment of a dog park, as shown within image three (3) below.

Establishment of a Memorial For Pets That Had Past

In 2019, Council sought further exploration of the creation of a pet memorial within the City for owners of deceased pets to pay their respects. Research by the City's Officers has shown a number of pet cremation services are provided within the Perth metropolitan area. Post cremation, many dog owners elect to either store or dispose of ashes at various locations or in private ceremonies.

Within the Perth metropolitan area, the Shenton Park Dogs Refuge has options of burial and spreading of ashes within their Memorial Garden. The use of the refuge incurs fees of between \$90-\$690, depending on the type of burial or spreading of ashes.

Although initial research would suggest there are no other formal memorials within the metropolitan area, the spreading of ashes is considered common and due to the small quantity and the inert nature of ash, does not pose any environmental health impacts

As part of the Plan, it is recommended that further investigation is done in consultation with Landgate about renaming the City's enclosed dog parks after fallen pets from the Australian Defence Force and other public services.

Objective 3 - Manage Feral and Domestic Cats

Feral and domestic cats are the same species, however, feral cats live and reproduce in the wild with minimal or no reliance on humans. Feral cats are predominantly solitary and nocturnal, spending most of the day in the safety of a shelter such as a rabbit burrow, log or rock pile. They are carnivores, surviving by scavenging or hunting for small mammals, birds, reptiles, amphibians, fish and insects. They can carry infectious diseases which can be transmitted to native animals, domestic pets and humans.

Feral cats threaten the survival of over 100 native species in Australia. They have caused the extinction of some ground-dwelling birds and small to medium-sized mammals. They are a major cause of decline for many land-based endangered animals such as the Bilby, Bandicoot, Bettong and Numbat.

The impact of feral cats is exacerbated by free-roaming domesticated cats, many of which frequent conservation areas. Many native animal species are struggling to survive and so reducing the number killed by feral and free-roaming domesticated cats will assist their populations to grow.

Creating Cat Control Areas

To mitigate the threat to native wildlife, the City has explored the creation of cat prohibited areas. Other local governments have successfully initiated such areas within the Perth metropolitan area.

Currently in the City, pet cats trapped in public areas, including conservation areas, are impounded, and if microchipped, their owners are notified. When collected, the owners pay a small impoundment fee. Where owners cannot be contacted, the impounded cats are sent to the Cat Haven for rehoming where possible. Cats that are not microchipped and deemed to be feral are humanely euthanized.

For the past 12 months, staff from Environmental and Ranger Services, have been investigating a number of options to strengthen the current City of Cockburn Local Laws 2000 that relate to cat management.

A Murdoch University student was engaged, via the City's intern program, to research the approach other Councils have taken to control cats and protect wildlife. The aim was to identify cat laws that would be suitable for the City to adopt. The scope of the research included discussions with other internal staff, including Rangers, to assess the ramifications of implementing specific laws in terms of staffing and financial impacts.



Based on the investigation to date and report produced by the intern, it is recommended that the City adopt a similar approach to other local governments, by enacting laws that prohibit cats in conservation reserves. This would mean that under the proposed Local Law if any registered cat is caught in one of the City's conservation areas, the owner would be committing an offence and be subject to a fine.

It is recommended that the City take a staged approach (Image 4 below) over a period of time to enact amended Cat Management Laws over and above those required by the WA *Cat Act 2011*. The proposed implementation is scheduled for over five years to ensure a smooth transition throughout the community and provide adequate time for laws to pass through relevant agencies.



Image 4 Staged Cat Control Areas

Stage 1: Enact Laws to Prohibit Cats from Selected Areas (Regional Parks)

The City has more than 80 conservation areas that it manages. There are also numerous conservation areas throughout the City that are managed by the Department of Conservation, Biodiversity and Attractions (DBCA). Logically any cat control laws must apply to conservation areas managed by both the City, DBCA and other land identified that is known for habiting vulnerable fauna. It is suggested that Stage 1 is the implementation of cat management laws that prohibit cats from being anywhere within all City managed Conservation Reserves, Regional Parks and any identified additional bushland areas within the City. The areas are shown in Image 5 (below) and highlight the cat prohibited areas proposed by phase 1.



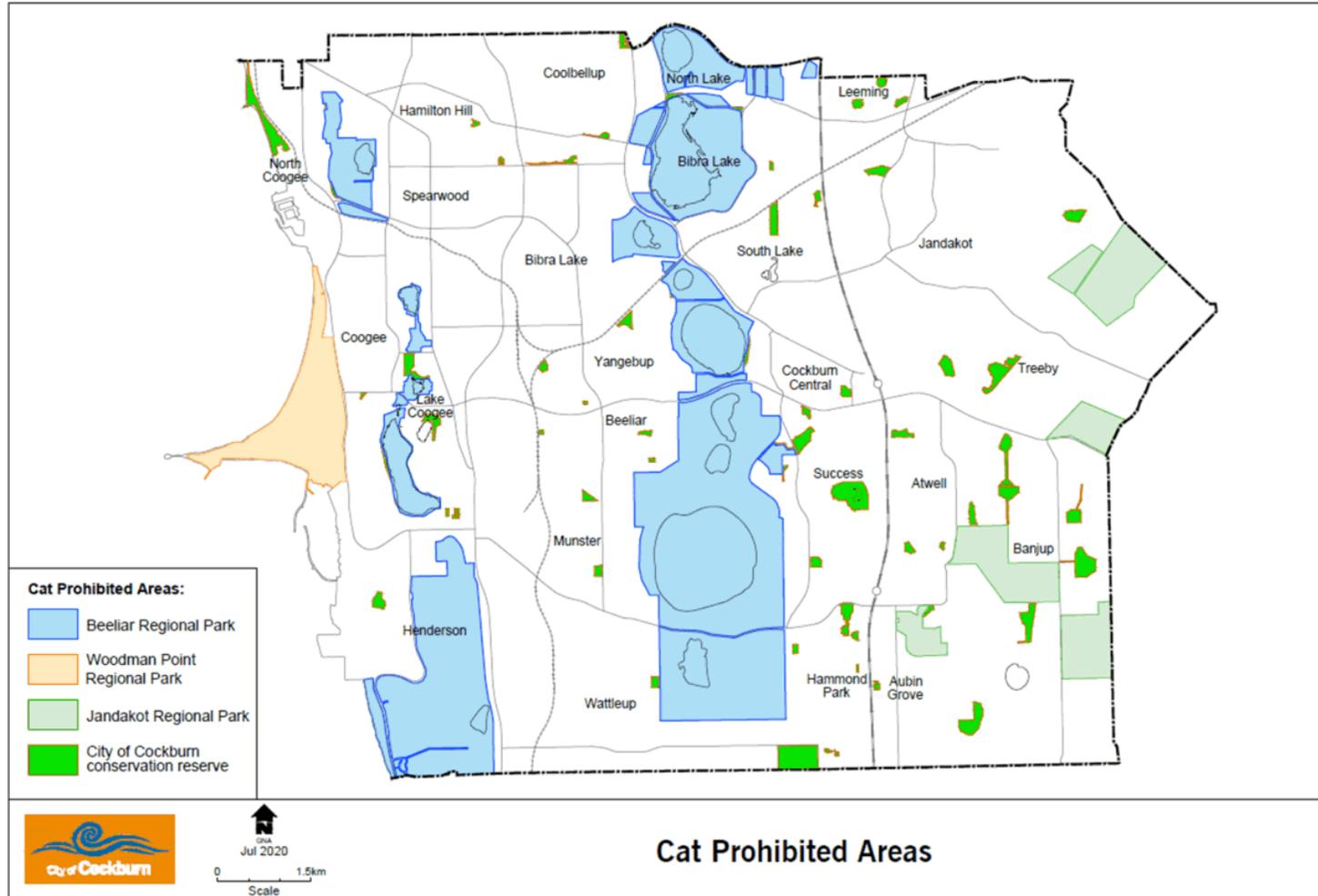


Image 5 Stage 1 - Cat Control Areas



The establishment of the regional parks as prohibited areas for cats is consistent with, and supported by, each of the Regional Park Management Plans (Beeliar RPMP- Pages32, Jandakot RPMP – Page 33, Woodman Point RPMP – Page 25, 26).

Stage 2: Enact Cat Management Laws similar to those that apply to dogs.

Stage 2 would see the City enact Local Laws for cat control based on similar requirements for dogs. The ability to lake such a law will be within the confines of the existing *Cat Act 2011*. The proposed Local Law would require cats to be retained within the owners property at all times.

On implementation of the Local Laws, the City will investigate options for cat owners, including workshops for educating cat owners and subsidies for cat confinement enclosures.

The City will lobby the State government, with the assistance of WALGA, to have the current *Cat Act 2011* amended to allow Councils to implement Cat Management Laws similar those that apply to dogs, ensuring cat management laws would be consistent across the State.

Community Based Trapping

Community based trapping is an effective tool to trap and re-home stray cats and reduce their impact in conversation reserves. Trapping has been trialed and adopted by leading Local Governments in Australia and in the City of Cockburn since 2017.

Further promotion of cat trapping could be used in conjunction with changes to the City's Local Law. A renewed public promotional campaign will assist in increasing community awareness of this service.

Conservation Reserve Trapping

The need for a comprehensive cat trapping program for City reserves is increasing. The population of feral cats is increasing, to the detriment of the native fauna and ecological function. To effectively control the unprecedented growth in the cat population, the City will need to engage additional contractors or appoint a trainee Ranger to undertake cat control in conservation areas. Identifying trapping locations is fluid and requires ongoing changes to address the growing cat population.

Conservation based trapping would align with the proposed cat control areas outlined in this plan.

Objective 4 - Embrace Smart City initiatives in animal management

Currently, City Rangers are leading the way in the use of digital technology with the application of mobile body cameras and other new technology. Future potential applications of digital innovations include:

- Improved data collection and database management of dog and cat owners;
- Digital registrations and automatic renewals;
- Targeted communication with pet owners;
- Reuniting or rehoming lost and unwanted animals;
- Dealing with barking dog issues via digital sound monitors;
- Accessing pet registration data in a mobile environment; and
- Remotely monitoring dog bag dispensers.

With the emergence of technology and strong community support for the City to continue to use technology to innovate, the following concepts have been developed;

Objective 1 - Encourage responsible dog and cat ownership

- Provide subsidized dog and cat microchipping;
- Install geo-fenced areas with RFID readers to tag dogs entering into dog prohibited areas;
- Improve the Ranger's ability to integrate internal data relating to previous dog offence history; and
- Provide dog registration kiosk's at dog-friendly community events and dog exercise areas.

Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife

- Install CCTV cameras and analytics programs to monitor the usage of fenced dog parks,
- Provide solar-powered sensor lights for dog owners using fenced dog parks after hours,
- Install outdoor dog wash stations at dog parks and CY- O'Connor Beach,
- Explore the use of waste collection remote vehicles

Objective 3 - Manage feral and domestic cats

- Install smart trapping and GPS tagging of traps within conservation reserves.
- Provide subsidized cat microchipping

Action Plan

Upon review of the myriad of initiatives proposed by the City, a multi-year action plan has been developed to implement the proposed changes within the Plan.



| Objective 1 - Encourage Responsible Dog Ownership | | | | | |
|---|---|---------------------------------------|--|---------------------------|--|
| Action No. | Actions | Leader | Timing | Cost | Measure of Success |
| <i>Keeping dogs out of 'dogs prohibited' areas</i> | | | | | |
| 1.1 | Increasing the City's casual pool of Rangers to patrol dog prohibited areas proactively. | Rangers and Community Safety Services | Seasonal – November – March of each year | \$40,000 annually | An overall reduction in dogs sighted in dog prohibited areas. |
| <i>Owner supervision and control of dogs in off-lead areas</i> | | | | | |
| 1.2 | Create public education program 'No it's not okay' focusing on singling out the common catch phase and reinforce control of dogs is the owner's responsibility, not another dog owner. | Rangers and Community Safety Services | Q1 - 2021 | \$20,000 | Increased public awareness. |
| 1.3 | Increase dog and cat registration checks in parks, reserves and beaches and home audits, targeting suburbs with low registration levels. Undertaken by existing Ranger team until 2023 financial year, and subsequent expansion as part of the Workforce Plan deliberation process. | Rangers and Community Safety Services | Ongoing | Existing Municipal budget | Increased registration compliance through follow-up action undertaken. |
| 1.4 | Develop Council Policy – for standardising the issuing of animal control infringements. | Rangers and Community Safety Services | Q3 - 2021 | Nil | Consistency of infringements issued, highlighting the need for community compliance. |
| <i>Dog behaviour - (dogs responding to their owners' commands, not jumping or running up to people or other dogs)</i> | | | | | |
| 1.5 | Partner with animal behavioural specialists to develop and administer an educational campaign. | Rangers and Community Safety Services | Q3 – 2022 | \$15,000 | Number of participants engage with the program. |
| 1.6 | Explore expanding animal subsidies for dog desexing. | Rangers and Community Safety Services | Q2 – 2021 | Existing Municipal budget | Expanding the current subsidies to meet expanding community needs. |
| <i>Management of dog waste</i> | | | | | |
| 1.7 | Review and improve access to dog waste stations (bags and bins). | Waste Services | Q3 2022 | \$30,000 | Reduction in reported waste. |
| <i>Ensuring dogs are kept on-lead in on-lead areas</i> | | | | | |
| Action item listed in 1.1 above. | | | | | |

| Objective 2 - Provide Sufficient, Safe Spaces for Dogs, People and Wildlife | | | | | |
|--|---|---------------------------------------|--|----------|---|
| Action No. | Actions | Leader | Timing | Cost | Measure of Success |
| <i>Coastline Usage</i> | | | | | |
| 2.1 | Change the usage of Woodman Point from 'on-leash' to 'dogs prohibited' | Rangers and Community Safety Services | Q3 - 2020 | Nil | Successful change of the area. |
| 2.2 | Provide clearer delineation markers between the areas zoned along the coastline areas. | Rangers and Community Safety Services | Q3 - 2020 | \$10,000 | Increased awareness of beach goes about where a dog can and cannot walk. |
| <i>Environmental Impacts</i> | | | | | |
| Action item listed within 2.1 (a) above. | | | | | |
| 2.3 | Assess possible restrictions of dogs in City managed conservation reserves. | Environmental Services | Q2 – 2024 | Nil | Review undertaken to understand the effectiveness of possible restrictions. |
| <i>Community Awareness</i> | | | | | |
| 2.4 | Implement a traffic light style dog designated areas. | Rangers and Community Safety Services | Commencing in Q3 – 2020 (aligned to action item 2.5 of this Action Plan) | \$45,000 | Increased community awareness of dog areas, resulting in improved compliance by dog owners. |
| <i>Off-leash areas within Reserves</i> | | | | | |
| 2.5 | Commence Phase 1 of the new dog areas. | Rangers and Community Safety Services | Q3 – 2020 | Nil | Improved utilisation of reserves by dog owners, with minimal disturbance to the community and sporting clubs. |
| 2.6 | Provide Council a report into the effectiveness of Phase 1 dog areas, and make a recommendation if Phase 2 and 3 should go ahead. | Rangers and Community Safety Services | Q4 – 2020 | Nil | Provide adequate report to Council to make an informed decision. |
| 2.7 | Commence Phase 2 of the new dog areas. | Rangers and Community Safety Services | Q2 – 2021 | Nil | Improved utilisation of reserves by dog owners, with minimal disturbance to the community and sporting clubs. |
| Action No. | Actions | Leader | Timing | Cost | Measure of Success |
| 2.8 | Commence Phase 3 of the new dog areas. | Rangers and Community Safety Services | Q4 - 2021 | Nil | Improved utilisation of reserves by dog owners, with minimal disturbance to the community and sporting clubs. |



| <i>Enclosed Dog Parks</i> | | | | | |
|---------------------------|--|---|-----------|----------|---|
| 2.9 | Review existing enclosed dog exercise areas, and improve fencing, shade, provision of sensory plants, agility equipment, waste disposal, drinking water stations. | Rangers and Community Safety Services/ Parks Services | Q4 – 2020 | \$20,000 | Improved overall use of the current enclosed dog parks. |
| 2.10 | Explore the renaming of enclosed dog parks after fallen service and other notable dogs | Rangers and Community Safety Services/ Planning | Q1 – 2021 | Nil | The implementation and naming of parks after community consultation. |
| 2.11 | Conduct targeted community engagement on the proposed construction of two new enclosed dog parks within MacFaul Park and Radonich Park with consideration to being partially unfenced. | Rangers and Community Safety Services | Q2 – 2021 | \$3,000 | Quality analysis to provide Council ability to provide an informed decision on if the projects should go ahead. |
| 2.12 | Construction of Radonich Park. | Rangers and Community Safety Services/ Parks Services | Q4 – 2021 | \$40,000 | Dog Park constructed to the meet the desires and needs of the community. Providing a safe place for owners to interact with their pets in a safe and well-planned area. |
| 2.13 | Construction of MacFaul Park. | Rangers and Community Safety Services/ Parks Services | Q3 – 2022 | \$40,000 | |

| Objective 3 - Manage Feral and Domestic Cats | | | | | |
|---|---|--|-----------------------|-----------------------------|--|
| Action No. | Actions | Leader | Timing | Cost | Measure of Success |
| Creating Cat Control Areas | | | | | |
| 3.1 | Review the City's Consolidated Local Laws 2000 and report to Council on possible amendments to include cat control areas. | Rangers and Community Safety Services/ Environment Services | December 2020 | Nil | Provide Council options to introduce new provisions within a Local Law to enact cat control areas. |
| 3.2 | Implement Stage 1 of the proposed Cat Control Areas | Rangers and Community Safety Services/ Environment Services | Q3 - 2021 | Nil | Implementation and promotion of stage 1 of the Cat Control Local Law |
| 3.3 | Public promotion and education of phase 2 implementation | Rangers and Community Safety Services/ Environment Services | Q3 (2022) – Q1 (2023) | \$10,000 | Public education of impending changes to cat restrictions and cat owners comply with the new restrictions. |
| 3.4 | Implement Stage 2 of the proposed Cat Control Areas | Rangers and Community Safety Services | Q3 - 2023 | Nil | Implementation and promotion of stage 2 of the Cat Control Local Laws. |
| Community Based Trapping | | | | | |
| 3.8 | Purchase of additional cat traps to loan to residents wanting to assist in the residential trapping of feral cats. | Rangers and Community Safety Services | Q1 -2022 | \$5,000 | Take up on the hire of cat traps by the community. |
| Conservation Reserve Trapping | | | | | |
| 3.6 | Forms part of the duties of a proposed Aboriginal Ranger traineeship program. | Rangers and Community Safety Services/ Environment Services | December – 2020 | Funded outside of this plan | Increase in proactive patrolling by Rangers and the setting of traps within conservation reserves |



| Objective 4 - Embrace Smart City initiatives in Animal Management | | | | | |
|--|--|---------------------------------------|-----------|----------------------------------|---|
| Action No. | Actions | Leader | Timing | Cost | Measure of Success |
| <i>Objective 1 - Encourage responsible dog and cat ownership</i> | | | | | |
| 4.1 | Install geo-fenced areas with RFID readers to tag dogs entering into dog prohibited areas. | Rangers and Community Safety Services | Q1 -2024 | \$30,000 | Effective monitoring of dog prohibited areas, while reducing human resource commitment. |
| 4.2 | Provide dog registration kiosks at dog friendly community events and dog exercise areas. | Rangers and Community Safety Services | Q4 - 2023 | \$10,000 | Number of on-site registrations completed at City managed events. |
| 4.3 | Install CCTV cameras and analytics programmes to monitor the usage of dog parks. | Rangers and Community Safety Services | Q3 – 2023 | \$30,000 | Increased dog usage intelligence created and used to change and amend dog park designs. |
| <i>Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife</i> | | | | | |
| 4.4 | Provide solar powered sensor lights for dog owners wishing to use dog parks after hours. | Rangers and Community Safety Services | Q2 – 2025 | \$60,000 | Increase use of dog parks at dusk. |
| 4.5 | Install outdoor dog wash stations at CY-O'Connor Beach. | Rangers and Community Safety Services | Q3 -2021 | \$15,000 | Usage of the facility by dog owners. |
| <i>Objective 3 – Manage Feral and Domestic Cats</i> | | | | | |
| 4.6 | Install smart trapping and GPS tagging of traps within conservation reserves | Rangers and Community Safety Services | Q3 - 2022 | Included within existing budgets | Improved management controls within conservation reserves |



Financial

| Action Items | 2020-21 | 2021-22 | 2022-23 | 2023-24 | 2024-25 | Total Project cost |
|--|-----------------------------|--------------|-------------|--------------|---------------|---------------------|
| Objective 1 - Encourage responsible dog and cat ownership | | | | | | |
| Increasing the City's casual pool of Rangers to proactively patrol dog prohibited areas. | \$40,000.00 | \$40,000.00 | \$40,000.00 | \$40,000.00 | \$40,000.00 | \$200,000.00 |
| 'No it's not okay' program | \$20,000.00 | | | | | \$20,000.00 |
| Increase dog and cat registration checks in parks, reserves and beaches and home audits, targeting suburbs with low registration levels. Undertaken by existing Ranger team until 2023 financial year. | Funded outside of this plan | | | | | |
| Partner with Animal Behavioural Specialists to develop and administer an educational campaign. | \$0.00 | \$0.00 | \$15,000.00 | \$0.00 | \$0.00 | \$15,000.00 |
| Review and improve access to dog waste stations (bags and bins). | \$0.00 | \$30,000.00 | \$0.00 | \$0.00 | \$0.00 | \$30,000.00 |
| Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife | | | | | | |
| Provide clearer delineation markers between the areas zoned along the coastline areas. | \$10,000.00 | \$0.00 | \$0.00 | \$0.00 | \$0.00 | \$10,000.00 |
| Implement a traffic light style dog designated areas. | \$45,000.00 | \$0.00 | \$0.00 | \$0.00 | \$0.00 | \$45,000.00 |
| Review existing enclosed dog exercise areas, and improve fencing, shade, provision of sensory plants, agility equipment, waste disposal, drinking water stations. | \$20,000.00 | \$0.00 | \$0.00 | \$0.00 | \$0.00 | \$20,000.00 |
| Conduct targeted community engagement on the proposed construction of two new unfenced dog parks within MacFaul Park and Radonich Park. | \$0.00 | \$3,000.00 | \$0.00 | \$0.00 | \$0.00 | \$3,000.00 |
| Construction of Radonich Park. | \$0.00 | \$40,000.00 | \$0.00 | \$0.00 | \$0.00 | \$40,000.00 |
| Construction of MacFaul Park. | \$0.00 | \$0.00 | \$40,000.00 | \$0.00 | \$0.00 | \$40,000.00 |
| Objective 3 - Manage Feral and Comestic Cats | | | | | | |
| Public promotion and education of phase 2 implementation . | \$0.00 | \$10,000.00 | \$0.00 | \$0.00 | \$0.00 | \$10,000.00 |
| Promote ethical based community cat trapping. | \$0.00 | \$8,000.00 | \$0.00 | \$0.00 | \$0.00 | \$8,000.00 |
| Purchase of additional cat traps to loan to residents wanting to assist in the residential trapping of feral cats. | \$0.00 | \$5,000.00 | \$0.00 | \$0.00 | \$0.00 | \$5,000.00 |
| Forms part of the duties of a proposed Aboriginal Ranger traineeship program. | Funded outside of this plan | | | | | |
| Objective 4 - Embrace Smart City initiatives in animal management | | | | | | |
| Install geo-fenced areas with RFID readers to tag dogs entering into dog prohibited areas. | \$0.00 | \$0.00 | \$0.00 | \$30,000.00 | \$0.00 | \$30,000.00 |
| Provide dog registration kiosks at dog friendly community events and dog exercise areas. | \$0.00 | \$0.00 | \$0.00 | \$10,000.00 | \$0.00 | \$10,000.00 |
| Install CCTV cameras and analytics programmes to monitor the usage of dog parks. | \$0.00 | \$0.00 | \$0.00 | \$30,000.00 | \$0.00 | \$30,000.00 |
| Provide solar powered sensor lights for dog owners wishing to use dog parks after hours. | \$0.00 | \$0.00 | \$0.00 | \$0.00 | \$60,000.00 | \$60,000.00 |
| Install outdoor dog wash stations within dog parks and CY- O'Connor Beach. | \$0.00 | \$15,000.00 | \$0.00 | \$0.00 | \$0.00 | \$15,000.00 |
| SUB TOTALS: | \$135,000.00 | \$151,000.00 | \$95,000.00 | \$110,000.00 | \$100,000.00 | |
| | | | | | TOTAL: | \$591,000.00 |



Animal Management and Exercise Plan Gantt Chart

| TASK NAME | START DATE | END DATE | 2020 | | | | 2021 | | | | 2022 | | | | 2023 | | | | 2024 | | | | 2025 | | | | |
|---|------------|----------|------|----|----|----|------|----|----|----|------|----|----|----|------|----|----|----|------|----|----|----|------|----|----|----|--|
| | | | Q1 | Q2 | Q3 | Q4 | |
| Objective 1 - Encourage responsible dog and cat ownership | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Increasing the City's casual pool of Rangers to proactively patrol dog prohibited areas. | Q3 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 'No it's not okay' program | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Increase dog and cat registration checks | Q1 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Partner with Animal Behavioural Specialists to develop and administer an educational campaign. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Review and improve access to dog waste stations (bags and bins). | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Objective 2 - Provide sufficient, safe spaces for pets, people and wildlife | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Change the usage of a. the portion of the beach south of the Ammo Jetty (Woodman Point) from dogs on lead to dogs prohibited, and b. the portion of beach near Chelydra point North Coogee from dogs prohibited to dogs on leash. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Provide clearer delineation markers between the areas zoned along the coastline areas. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Assess possible restrictions of dogs in City managed Conservation Reserves. | Q2 | Q2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Implement a traffic light style dog designated areas. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Commence Phase 1 of the new dog areas. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Provide Council a report into the effectiveness of Phase 1 dog areas, and make recommendation if Phase 2 and 3 should go ahead. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Explore the renaming of enclosed dog parks | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Commence Phase 2 of the new dog areas. | Q2 | Q2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Commence Phase 3 of the new dog areas. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Review existing enclosed dog exercise areas, and improve fencing, shade, provision of sensory plants, agility equipment, waste disposal, drinking water stations. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Conduct targeted community engagement on the proposed construction of two new unfenced dog parks within MacFaul Park and Radonich Park. | Q2 | Q2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Construction of Radonich Park. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Construction of MacFaul Park. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Objective 3 - Manage feral cats | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Review the City's Consolidated Local Laws 2000 and report to Council on possible amendments to include cat control areas. | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Undertake community consultation on the community's desire for cat control areas. | Q1 | Q2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Council be presented an amended Consolidated Local Law with the inclusion of cat control areas. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Promote ethical based community cat trapping. | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Purchase of additional cat traps to loan to residents wanting to assist in the residential trapping of feral cats. | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Forms part of the duties of a proposed Aboriginal Ranger traineeship program. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Objective 4 - Embrace Smart City initiatives in animal management | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Install geo-fenced areas with RFID readers to tag dogs entering into dog prohibited areas. | Q1 | Q1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Provide dog registration kiosks at dog friendly community events and dog exercise areas. | Q4 | Q4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Install CCTV cameras and analytics programmes to monitor the usage of dog parks. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Provide solar powered sensor lights for dog owners wishing to use dog parks after hours. | Q2 | Q2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Install outdoor dog wash stations within dog parks and CY- O'Connor Beach. | Q3 | Q3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| Install smart trapping and GPS tagging of traps within conservation reserves | | | | | | | | | | | | | | | | | | | | | | | | | | | |



Performance Measures

The City will determine the success of the Animal Management and Exercise Plan 2020-2025 through the following measures and targets. These will be assessed at the end of the life of the plan via community survey.

| Performance Outcomes | 2019 Current | | 2025 Target | |
|---|--------------|---------------|-------------|---------------|
| Overall Performance | | | | |
| Domestic animal control (dogs and cats) | 64 | | 70 | |
| Feral animal control (feral cats, foxes and rabbits) | 65 | | 70 | |
| Responsible pet ownership | | | | |
| % of dogs registered and microchipped | 61% | | 70% | |
| % of cats registered, microchipped and sterilized | 17% | | 30% | |
| % of dog owners who have attended dog obedience training (source: CATALYSE® Community Perceptions Survey) | 71% | | 80% | |
| Key Performance Measures | Dog owner | Non-dog owner | Dog owner | Non-dog owner |
| Happy with the availability and mix of on-leash, off-leash, fenced and dogs prohibited areas | 38% | 39% | 50% | 50% |
| Sufficient information about responsible dog management rules and guidelines | 46% | 38% | 60% | 40% |
| Sufficient promotion and provision of dog obedience training | 23% | 17% | 40% | 30% |
| Sufficient signage indicating where dogs are permitted on and off-leash | 59% | 50% | 80% | 80% |
| Perceived effectiveness (score out of 100) | Dog owner | Non-dog owner | Dog owner | Non-dog owner |
| Management of dog waste | 48 | 33 | 60 | 60 |
| Ensuring dogs are kept on-leash in on-leash areas | 44 | 20 | 60 | 60 |
| Owner supervision and control of dogs in off-leash areas | 38 | 18 | 50 | 50 |
| Keeping dogs out of 'dogs prohibited' areas | 47 | 24 | 60 | 60 |
| Keeping dogs away from wildlife and nesting birds | 50 | 25 | 60 | 60 |
| Dog behaviour (dogs responding to their owners' commands) | 36 | 19 | 50 | 50 |



Reporting and Revision

The Animal Managements Plan will be reviewed in 5 years (2025) to ascertain the action plan objectives that have been completed and the inclusion of any new objectives which have arisen since commencement.



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Animal Management and Exercise Plan 2020-2025

Summary Submission Sentiment

| Objective | Action | Support | Oppose | Undecided |
|--|---|---------|--------|-----------|
| 1. Encourage Responsible Dog Ownership | Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. | 4 | | |
| | Create public education program 'No it's not okay' to improve owner supervision and control of dogs in off-leash areas | 2 | | |
| | Increase dog and cat registration checks in parks, reserves and beaches and home audits | 1 | | |
| | Explore expanding animal subsidies for dog desexing | | | |
| | Partner with animal behavioural specialists to develop and administer an educational campaign | 2 | | |
| | Review and improve access to dog waste stations (bags and bins) | 1 | | |
| | Develop Council Policy for standardising the issuing of animal control infringements | 3 | | |
| 2. Provide Sufficient, Safe Spaces for Dogs, People and Wildlife | Change the usage of Woodman Point from 'on-leash' to 'off-leash' and investigate the potential to have the area as a "dog prohibited zone" during the Fairy Tern nesting season | 5 | 115 | |
| | Provide clearer delineation markers between the dog areas zoned along the coastline | 3 | 1 | |
| | Assess possible restrictions of dogs in City managed conservation reserves | 8 | | |
| | Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas) | 4 | | |
| | Trial use of sporting reserves as off-leash areas when sporting fields are not in use. | 2 | 4 | 1 |
| | Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. | | 8 | |
| | Review existing enclosed dog exercise areas and improve fencing, shade provision of sensory plants, agility equipment, waste disposal and drinking water stations. | 1 | | |
| | Conduct targeted community engagement on the proposed construction of new unfenced dog parks within Macfaull Park and Radonich Park | | | |
| | Provide a new unfenced dog park at Macfaull Park | | | |
| | Provide a new unfenced dog park at Radonich Park | | | |
| 3. Manage Feral and Domestic Cats | Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2 | 8 | 2 | |
| | Purchase additional cat traps to loan to residents wanting to assist in the residential trapping of feral cats | 1 | | |
| | Conservation reserve trapping | 1 | | |
| | Review cat laws to include cat control areas, using a staged approach | 1 | | |
| | Stage 1: (2021) Enact Laws to prohibit cats from all City managed Conservation Reserves, Regional Parks and any identified additional bushland areas in the City | 8 | 2 | |
| | Stage 2: (2023) Enact laws for cat control based on similar requirements for dogs (requiring cats to be retained within the owners property at all times) | 9 | 3 | |
| | On implementation of the laws, investigate options for cat owners, including educational workshops and subsidies for cat confinement enclosures | 1 | | |
| 4. Embrace Smart City Initiatives in Animal Management | Install geo-fenced areas with readers that can track dogs entering into dog prohibited areas | | | |
| | Provide dog registration kiosks at dog friendly community events and dog exercise areas | | | |
| | Install CCTV cameras and analytics programs to monitor usage of dog parks | | | |
| | Provide solar powered sensor lights for dog owners wishing to use dog parks after hours | | | |
| | Install outdoor dog wash stations within dog parks and C.Y. O'Connor Beach | | | |



SUMMARY OF THEMES**Key findings**

155 submissions were recorded in relation to the Animal Management and Exercise Plan. Common themes observed were:

- Opposition to allowing dogs 'off-leash' at Woodman Point
- Some opposition to allowing dogs at Woodman Point altogether
- Support for increased ranger patrols at parks
- Support for changes to the management of domestic cats

The Action "*Change the usage of Woodman Point from 'on-leash' to 'off-leash' and investigate the potential to have the area as a 'dog prohibited zone' during the Fairy Tern nesting season*" found under Objective 2 in the Animal Management and Exercise Plan was strongly opposed (74% of all submissions).

Common concerns regarding this action included the impact of dogs on wildlife (particularly birdlife), the potential danger of 'off-leash' dogs to children and concerns about dogs disrupting recreational activities.

Some respondents (approximately 6) noted that they generally supported the Plan, with the exception of making Woodman Point an 'off-leash' area. Opposition to proposed changes regarding Woodman Point was the most dominant theme in submissions.

Objective 1

In terms of Objective 1 the Action to "*Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas*" received the most support. However, many supportive submissions noted that they wanted an increase of Ranger patrols in dog permitted areas, not just dog prohibited areas. A few respondents also noted, in addition to increased patrols, that animal control infringements should be enforced more regularly. Education programs campaigns also received some support, as did increased registration checks and reviewing access to dog waste stations. No respondents opposed the Actions listed under Objection 1.

Objective 2

Regarding the second Objective, 12 submissions objected to sporting fields being used as 'off-leash areas.' 8 of these submissions referred to specific parks they wanted to remain 'on-leash' only. Uncontrolled dogs and dog waste were the main concerns of submissions opposing 'off-leash' sporting reserves. 2 submissions supported this Action, and 1 was undecided.

Approximately 8 submissions supported the Action to "*Assess possible restrictions of dogs in City managed conservation reserves.*" Specifically, most of these submissions asserted that Woodman Point should become a more restricted conservation area in which dogs were prohibited. 3 submissions supported "*clearer delineation markers between the dog areas zoned along the coastline*" and 1 submission objected to this Action. Improvements to fenced-areas were only expressly supported by 1 submission.

Objective 3

Concerning Objective 3 the Actions to "*Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2*" and then enact these laws (under Stage 1 and 2) received a combined 25 submissions. In contrast, 7 submissions opposed these Actions due to issues such as costs. Only 1 submission reflected support for education workshops and subsidies for cat enclosures.

Objective 4

Finally, no submissions commented on the Actions related to Objective 4. This suggests that Objective 4 and related Actions are not very divisive in the community. Nor do they attract notable community support.



Animal Management and Exercise Plan 2020 – 25

| Number | Community Feedback | Objective, Action Reference and Officer Comment |
|--------|---|--|
| 1. | We are members at Jervis Bay Sailing Club and regularly use the beach for sailing and recreation purposes. Our children use the sailing school and race there every Sunday. To have dogs off the leash on the beach would be detrimental to us as users of the facility. We don't need dogs in the water and on the beach out of control and interfering with the sailing programs run by Jervis Bay Sailing Club. The two activities are incompatible as we will have moving boats coming to and from the beach mixed with dogs not with their owners. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose The use of the beach by the Sailing club and other recreational groups makes share use of the beach problematic. When complaints by user groups are received it is logistically problematic for the City's Rangers to respond in a time efficient manner to assist with the complaint. |
| 2. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. Fairy terns and other birds need protection to enable nesting. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 3. | I do not support dogs being allowed to roam at Woodsman Point south area. This area must be allocated to the wellbeing of the breeding/nesting Fairy Terns as they have a right to be un-disturbed at all times. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose The interruption of shore based birds from breeding by dogs has been raised in the environmental report attached to the OCM Report. |
| 4. | I get very upset every time I go to Woodman Point and see dogs chasing birds which have just flown all the way from Siberia. It seems a great shame that we cannot keep a time bit of coast for the waders. As a person who would love future generations to enjoy birdlife, marine life and plant life in the same way that I have, PLEASE could you stop dogs from spoiling Woodman Point. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose If the beachline area of Woodman Point is to remain as an off-leash area. Incidents such as this would not be out of the control of the City's Rangers to prevent. |
| 5. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 6. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 7. | I do not support dogs on the beach at Woodman point south of the ammunition jetty, | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 8. | I do not support dogs on the beach at Woodman Point, south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 9. | I oppose the proposal for dogs to be allowed to be off lead to the area North the Woodman Point Beach. The Woodman Point Beach is regularly frequented by clientele from the Woodman Point Recreation Camp. The Joondalup council extended the Pinnaroo Point Dog Beach South of the Ern Halliday Recreation Camp several years back and this change has significantly impacted the safe delivery of our beach and water programs for school and community groups at our site. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose The Woodman Point Recreational staff have expressed similar concerns when meeting with the City's Officers during the community consultation period. To date, there have been limited reportable dog attaches within the area. |



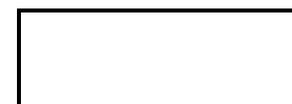
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| <p>For the extra 100 metres or so gained in Hillarys, we have since had incidences of:</p> <ul style="list-style-type: none"> - Dog menace to clientele. - Large dogs aggressively barking at young school children. - Dog owners confronting staff when we have tried to stop dogs peeing or jumping on school children (not all children like dogs). - Dogs grabbing equipment used for activities or clientele gear and running off. - Young children being taken off the beach visibly scared due to having a real fear of dogs. <p>Whilst I am a dog owner, I appreciate that not all dog owners share a common, sensible approach to dealing with their dogs at the beach.</p> <p>Subsequently, some groups have cancelled beach based activities at the Ern Halliday Recreation Camp directly related to the extension of the dog beach at Pinnaroo Point and I suspect this will also be the case at Woodman Point should the proposal be approved.</p> <p>Effectively, less children and groups will be able to enjoy the wonderful area at Woodman Point Beach.</p> | |
| <p>10. BirdLife Australia commends the City of Cockburn for identifying the importance of animal management to protect the natural environment, for proposed cat management laws, and making the draft animal management plan open for public comment</p> <p>However, BirdLife strongly recommends that dogs are prohibited from the beach south of ammunition jetty.</p> <p>BirdLife are concerned by the City's proposal to designate the shoreline of the Woodman Point Regional Park (particularly between the ammunition jetty to and inclusive of the groyne and headland) a "dogs off-leash" zone. It was not appropriate for this to be a "dog on-leash zone" due to the bird conservation values of the shoreline, and further relaxing the zoning will be detrimental to the bird life.</p> <p>It is a proposal that is neither consistent with the Woodman Point Regional Park Management Plan (2010) or with retaining the biodiversity value of the shoreline, for which BirdLife has credible data. It is also unclear why the plan proposes to increase off-leash beach areas, when community consultation called for more fenced dog exercise areas.</p> <p>The Regional Park Management Plan identifies that the Park will be managed for conservation and that "recreation and other uses will be allowed to occur to the extent that they do not adversely impact on the natural environment". Declared threatened flora and fauna are given priority for conservation and "dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park". Your proposal is inconsistent with all these management strategies, providing no priority of protection to threatened migratory shorebirds and Australian Fairy Tern and placing recreational use above protection of the environment by allowing dog access to areas that are not appropriate.</p> <p>BirdLife's extensive bird database (from annual shorebird monitoring) has records of 26 species of migratory and resident shorebirds using Woodman Point. The area provides feeding and roosting/resting habitat for the following federally listed threatened species:</p> <p>Critically Endangered:</p> <p>Great Knot (<i>Calidris tenuirostris</i>)</p> | <p>Objective 1: Actions: Partner with animal behavioural specialists to develop and administer an educational campaign. Implied Sentiment: Support</p> <p>Objective 1: Create public education program 'No it's not okay' to improve owner supervision and control of dogs in off-leash areas. Implied Sentiment: Support</p> <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash' and investigate the potential to have the area as a "dog prohibited zone" during the Fairy Tern nesting season. Implied Sentiment: Oppose</p> <p>Noted. Birdlife's response has also been added to the OCM Report as part of an appendix.</p> |



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| <p>Curlew Sandpiper (<i>Calidris ferruginea</i>)</p> <p>Endangered:</p> <p>Red Knot (<i>Calidris canutus</i>)</p> <p>Lesser Sand Plover (<i>Charadrius mongolus</i>)</p> <p>Vulnerable:</p> <p>Greater Sand Plover (<i>Charadrius leschenaultii</i>)</p> <p>Australian Fairy Tern (<i>Sternula nereis nereis</i>)</p> <p>Migratory Ruddy Turnstone, Grey Plovers and Grey-tailed Tattler regularly use the rocky groyne, and knots, sandpipers and resident Red-capped Plovers prefer the exposed sand and shallows. The area also could provide breeding habitat for resident beach nesting birds including Red-capped Plovers and Oystercatchers (Pied and Sooty), though the amount of human activity and frequent presence of dogs has likely impacted birds selecting the area to breed in recent years. Making the area dog free as well as measures proposed to facilitate Fairy Tern breeding (temporary fencing and feral predator control) will also benefit these beach nesting species and migratory shorebirds.</p> <p>Unleashed dogs on the beach adversely impact the natural environment by disturbing feeding and roosting shorebirds and breeding beach nesting birds such as Fairy Terns, Australian Pied Oystercatchers and Red-capped Plovers, all of which frequent the area. Migratory shorebirds rely on undisturbed feeding and roosting to build body condition to make their long migration to the northern arctic to breed and to recover when they return to Australia. There is a growing body of research indicating that frequent disturbance can significantly impact shorebird feeding behaviour, causing birds to waste energy in repeated flight to get out of the path of dogs and in some cases can cause birds to stop using a productive site all together (Stigner et al. 2016). Dogs off leash can also cause damage to eggs, injury/death of chicks and stressful disturbance to adult breeding beach nesting birds.</p> <p>Due to increased development of and human activity across coastal areas of the Perth metropolitan area, there are now few sites in Perth that provide undisturbed habitat for migratory and resident shorebirds. Consequently, bird numbers have and continue to decline, placing even greater value on places like Woodman Point where a variety of shorebirds can still be seen. This is a natural asset the City of Cockburn should be proud of and keen to manage appropriately so that shorebirds continue to use the site. This management must include prohibiting dogs from the beach. Indeed, there is no point trying to encourage Fairy Terns (and potentially other beach nesting birds) to breed at Woodman Point if you plan to allow dog access. Research has clearly shown that beach nesting birds and dogs do not mix and that best outcomes are achieved by prohibiting dogs (all year round) rather than encouraging and relying on responsible dog ownership/control (see Maguire 2018 - attached). Community engagement and education are important when prohibiting dogs from areas they have had access, and BirdLife would be willing to assist the council with this engagement and messaging</p> | |
| <p>11. I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>Shorebirds are at serious risk when dogs are around. Since settlement river mouths, beaches and other resting and feeding places for shorebirds have been taken over, built on or used for recreation, leaving the shore birds, both local and migratory, with few safe places to be. This is particularly hard for the birds which try to nest along the shoreline, such as the Fairy Terns.</p> <p>Woodman Point is important for the birds, please make it a no go area for dogs.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Object 2: Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support</p> |



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| <p>Coastal systems are dynamic and productive environments that support rich faunal communities worldwide. Critically, coastal habitats are important breeding and feeding sites for a diverse assemblage of birds, who play integral roles in maintaining the transfer of nutrients between terrestrial and aquatic ecosystems (e.g. Anderson and Polis 1999). Despite the importance of these environments, growing human populations and the associated demand for housing, industry and recreational amenities has led to major changes in land use. As a consequence, there have been significant reductions in habitat and resource availability, driving the decline of coastal bird populations (Yasué et al. 2007; Pakanen et al. 2014).</p> <p>Woodman Point is an important breeding, feeding and roosting area for a number of migratory and local bird species, including the Australian Fairy Tern (priority fauna, listed as threatened under state and federal legislation, [Commonwealth of Australia 2019]). Woodman Point, with its extensive sandy beaches and the rocky WAPET Groyne, is a highly visible and attractive feature within the landscape.</p> <p>However, in recent decades, increased human use including dogs on beaches has led to a significant reduction in the number of birds utilising this area (BirdLife Australia 2020).</p> <p>The Woodman Point headland between Cockburn Cement and the Woodman Point carpark has a long history of use, including breeding, by the Australian Fairy Tern. Plans to manage and restore the WAPET Groyne to support future breeding attempts by Fairy Terns is being jointly investigated by the City of Cockburn, Birdlife Australia, Department of Biodiversity, Conservation and Attractions, and the Western</p> <p>Australian Fairy Tern Network. Recent changes allowing 'dogs on leads' in this area is likely to be negatively impacting Fairy Terns, and the proposed changes to make the area an 'off-leash dog area' are likely to be highly detrimental to any nesting attempts. A study by Schlacher et al. (2015) suggests that the "environmental impacts attributable to dogs are more severe than those resulting from other recreational activities, except the highly destructive consequences of driving off-road vehicles (Schlacher et al. 2013d; Weston et al. 2014b)", with the potential for dogs to create a landscape of fear and detrimentally change the behaviour of wildlife species. Under the Woodman Point Regional Park Management Plan 2010 (WPRPMP, Overman et al. 2010),</p> <p>Woodman Point Nature Reserve including the Woodman Point headland, is gazetted as a no-dog area.</p> <p>Dogs are a major threat to coastal wildlife utilising beaches across Australia (Williams et al. 2009; Schlacher et al. 2015; Maguire 2018). Therefore, to maintain the ecological values of the area and to improve outcomes for coastal birds, the beach south of Ammunition Jetty, should completely prohibited to dogs, both leashed and un-leashed. In addition to various parklands, three off-lead dog beaches already exist under the current plan, providing ample opportunity for people to exercise their dogs. Controlling dogs in coastal reserves is essential for maintaining ecosystem function (Schlacher et al. 2015).</p> <p>As land managers, maintaining the ecological values of the natural environment and managing threats to biodiversity in accordance with the WPRPMP (Overman et al. 2010), should be of primary importance. I refer you to the 'Guiding Principles for Conserving the Natural Environment' as set out under the WPRPMP (Overman et al. 2010). Principles 1 – 4 and 8, are particularly pertinent to this argument since the proposed changes are likely to detrimentally impact priority fauna (Australian Fairy Tern) but also have negative outcomes for a diverse arrange of other birds utilising the area, including several threatened migratory shorebirds.</p> <p>1. Conservation and protection of the natural environment</p> <p>Natural processes and biodiversity will be managed to maintain their inherent values. External impacts from human</p> | <p>Noted. The City's engaged consultant has also reaffirmed many points raised by this submission.</p> |
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| <p>use, the surrounding urban area and management practices will be minimised in order to maintain the biodiversity of natural systems over the long-term.</p> <p>2. Park management priorities</p> <p>The Park will be managed for conservation and environmental enhancement. Recreation and other uses will be allowed to occur to the extent that they do not adversely impact on the natural environment.</p> <p>3. Restoration of the natural environment</p> <p>Restoration of the natural environment will be undertaken to protect and maintain biodiversity and natural systems. Areas with high nature conservation value will be considered priorities for restoration.</p> <p>4. Features requiring special protection</p> <p>Declared rare flora, priority and significant flora species, threatened ecological communities, priority fauna and other specially protected fauna will be given priority for conservation and restoration.</p> <p>5. Consistency of management policies</p> <p>The land managers involved in the Park will apply consistent and coordinated management policy.</p> <p>6. Appropriate reserve purpose</p> <p>Reserves within the Park will be assigned an appropriate purpose for the protection of biodiversity and natural systems over the long-term.</p> <p>7. Recognition of cultural and social values</p> <p>The Park will be managed in a way that delivers community benefits by maintaining cultural traditions and places of cultural significance and by providing opportunities for recreation, education and research.</p> <p>8. Precautionary principle</p> <p>If there are threats of serious or irreversible environmental damage, the lack of full scientific certainty should not be used as a reason for postponing measures to prevent environmental degradation.</p> <p>The preservation of Woodman Point as a feeding-, roosting- and breeding- site must be given high priority for the long-term conservation needs of coastal avifauna, but also for human enjoyment.</p> <p>Birdwatching is becoming an increasingly popular past-time across a broad spectrum of the community – a hobby that contributes hundreds of thousands of dollars to the economy (Heathcote 2019). With careful planning and management, there is potential to enhance the wildlife viewing experience at Woodman Point. Particularly, given the diverse spectrum of birds that frequent the area and its popularity among bird watchers.</p> <p>I strongly urge the council to consider the prohibition of dogs on the beach south of Ammunition Jetty, to uphold the WRPMP 2010 as it was originally intended, to manage the area for its strong ecological values and implement measures to protect this important site for wildlife.</p> | |
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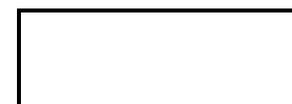
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| 12. | I am a member of the Jervoise Bay sailing club and am strongly opposed to the dog off-lead project at Woodman Point beach. The beach belongs to everyone and should not be taken over by dogs, which are disruptive. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 13. | <p>"I do not support dogs on the beach at Woodman Point south of the ammunition jetty".</p> <p>BirdLife WA has strongly advised the council that dogs not be allowed access at Woodman Point, as this is an important area for shorebirds such as Ruddy Turnstone, Grey Plover, Grey-tailed Tattler, Red-capped Plover to name a few. The area has also been identified to manage for Fairy Tern breeding.</p> <p>Presently dogs are allowed off leash, and this causes considerable disturbance to birds using the area and likely deters beach nesting birds selecting the area to breed.</p> | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 14. | <p>I am in favour of most of the actions identified in the AMP especially the proposed cat laws. Brilliant they are.</p> <p>I do not support the proposal to allow dogs off leash south of the ammunition jetty.</p> <p>This area is an important breeding and resting area for a range of birds, including fairy terns. Many other birds that use this beach are migratory birds flying in from as far away as the artic. This beach is important as it allows these birds to rest and recover prior to heading back to the northern hemisphere to breed.</p> <p>There is no point opening it for 6 months of the year outside the fairy tern nesting time as this time is from Spring to Summer, that would be the peak period for those wanting to use the beach.</p> <p>Our beaches are also used by other native animals such as bandicoots, snakes and lizards.</p> <p>I support this area being completely closed to dogs all year round.</p> <p>Cockburn has approximately 8klm of usable coastline. Allowing dogs access to this area would effectively mean 6.5klm of the 8klm of available coastline is a dog off leash area. That's just not equitable for those that don't want to be around dogs or for our wildlife.</p> <p>The area south of the ammunition jetty is also home to the DPLH camp. Many children visit this camp all year round and use the beach. Allowing dogs off lead in this area puts these children as risk from dog attacks.</p> | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 15. | I would like to state my opposition to dogs being on the beach south of Woodmans Point. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 16. | <p>The Coogee Beach Progress Association (CBPA) understands there has been a Draft Animal Management and Exercise Plan 2020-2025, issued by the City of Cockburn asking for public comments. The CBPA is extremely concerned regarding the new recommendations which will allow dogs off lead south of the Woodman Point ammunition jetty and around the Woodman Point headland. The CBPA on behalf of the community is against this proposal and we urge the council to make this part of the beach a permanent dog free zone.</p> <p>As it stands, this area of coastline is apparently already a dog on lead zone, and this has been a subject of contention for many years. Residents have often complained to our group that dog owners are letting their dogs of lead despite the signs in place prohibiting this action. Complaints also range from dog litter constantly left on the beach and dogs running up to other beach users and jumping up on them. The CBPA have responded by asking the City of Cockburn rangers to monitor the area more closely but this has proven difficult due to the casual timing of the beach activity with</p> | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |



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| | <p>the public, therefore the dog owners continue to let their dogs off the lead.</p> <p>The Woodman Point Regional Park Management Plan 2010 clearly states that; "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park."</p> <p>The proposed recommendation by the City of Cockburn to allow dogs off leads on the beaches south of the Woodman Point ammunition jetty, and around the Woodland Point headland, is in direct conflict with this Woodman Point Regional Management Plan. In addition, the Department of Sport and Recreation Camp at Woodman Point has activities in this area for some 12 months of the year, with many young children in group activities on this section of beach.</p> <p>There is also a current program, supported by the City of Cockburn and the Community to encourage the breeding of Silver Terns on the headland at Woodman Point. Allowing dogs in this area, on or off leads, would totally destroy this program.</p> <p>The CBPA is also aware that a new jetty has been approved for installation approximately 100m south of the existing ammunition jetty (which is to be replaced due to safety concerns) This new jetty will no doubt bring more people and families to the beach creating more issues with dogs interacting with beach goers.</p> <p>The CBPA believes that there is already ample beach areas dedicated for dogs off lead within the local area such as CY O'Connor beach and the beach at western end of the Jervoise Bay Recreational Boating Precinct, and with the new Draft Plan for Managing Cats and Dogs proposed by the City of Cockburn, dog owners will have even more choices to exercise their dogs of lead than ever before.</p> <p>Therefore, prohibiting dogs south of the ammunition jetty, and around the Woodman Point Headland, is the safest and most preferred option, and the CBPA hope you will consider the strong community opposition to this draft proposal.</p> | |
| <p>17.</p> | <p>I have consulted with some of my fellow directors; we have no objections to the area south of Woodman Point Jetty changing to dogs off-leash. From my experience on the beach, a fair proportion of dog owners are already letting their dogs off the leash in this area. Formalising this arrangement will probably be a welcome move.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Support</p> |
| <p>18.</p> | <p>I would like to make the following comments on the draft Animal Management Plan.</p> <ul style="list-style-type: none"> • City of Cockburn has approximately 9320 metres of public accessible beach, • Currently, approximately 6800 m or nearly 75% of public accessible beach the City allows dogs (in both leash and off leash locations). • Public has only 2500 metres of beach where dogs are prohibited (approximately 25% of public accessible beach) • The current Woodman Point on-leash area (yellow) is a breeding and roosting site for a range of shorebirds including Grey Plovers, Fairy Terns, Caspian Terns, Red Capped Plovers, Ruddy Turnstones, Pied Oystercatchers. While some species are only present over summer some species such as Pied Oystercatchers are present all year. • Surveys carried out annually by Birdlife Australia show a steady reduction in shorebird numbers at Woodman Point. It is thought that increasing disturbance from dogs is contributing to the loss of shorebird numbers. • Converting the on-leash (Yellow) to off-leash (Green) will lead to the eventual loss of the shorebird populations | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas). Implied Sentiment: Support</p> |



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| | <p>at Woodman Point.</p> <p>Recommendations</p> <ul style="list-style-type: none"> Do not convert the Woodman Point dogs on-leash zone (yellow) to off-leash zone (Green), as it will lead to the eventual loss of shorebirds at Woodman Point due to disturbance by dogs, Convert the Woodman Point on-leash zone (yellow) to a dog prohibited zone (red). | |
| <p>19.</p> | <p>I would like to advise that I do not support dogs on the beach at Woodman Point, south of the ammunition jetty.</p> <p>This is an extremely important area for Shorebirds and Seabirds, such as the endangered Fairy Tern, Far Eastern Curlew, as well as the Grey-tailed Tattler, Ruddy Turnstone, Sanderling and Red-necked Stint.</p> <p>This area is too important to allow dogs on to – this would lead to the destruction of shorebird and seabird nests, as well as the killing of chicks.</p> <p>Please consider this – it would be highly irresponsible to allow dogs on the beach south of the ammunition jetty at Woodman point.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| <p>20.</p> | <p>I am a long term member of Birdlife WA having taken part in the annual Shorebirds Surveys at Rotttnest Island over many years. I am a dog lover and have had the privilege and joy of sharing my life with cherished pets during my life.</p> <p>I am appreciative and very supportive of the policy initiative and many of the solutions to the problems identified in the Plan.</p> <p>The proposal to designate Woodman Point Regional Park shoreline a 'Dogs off-leashes' zone as per the map is in my view untenable. It would breach the fundamental objectives of the Plan in the apparent interests of simplifying procedures for people and pets at the expense of endangered wildlife and natural resources. Further, no credible rationale is provided in the Plan for the change while a significant case against such a move is provided in the City's own signage at Woodman Point Regional Park itself (see attached) and the included commissioned advice from Birdlife WA that very clearly outlines the value and vulnerability of the ecosystem and its natural inhabitants such as migratory and nesting birdlife.</p> <p>Additionally, estimates of the prospective growth of the numbers of dogs in the catchment area while being used to justify changes to manage anticipated social impacts are not applied to the protection of the wildlife which is a primary objective of the Plan. As a five year plan, it takes into consideration population growth and community needs while overlooking our changing climate and the detrimental effects it will have on ecological communities in an area widely recognised as unique and already under siege.</p> <p>2</p> <p>It is a proposal that appears to me to be logically inconsistent and based in part on unattributed research purporting to argue that as environmental damage is caused by both on-leash and off-leash dogs by inference the change would have</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>negligible additional impact on wildlife. All human and animal activity has negative impacts on wildlife and by its very nature off-leash dog activity magnifies those impacts.</p> <p>Finally, it is worth revisiting the core premises of maintaining the status quo eg:</p> <ul style="list-style-type: none"> • The stated objective of the Draft Animal Management Plan is to: [2] Provide sufficient, safe spaces for pets, people and wildlife. • (links 4.2) 'To sustainably manage our environment by protecting, managing and enhancing our unique natural resources.' • 'The general community is highly concerned with keeping dogs away from wildlife and nesting birds. The level of concern 85points among non-dog owners and 75points among dog owners.' <p>These precepts do not underpin the proposal as argued in the Draft and of additional concern is that no detail is provided to support Council's recommendation as reported – on what basis was that edict put forward and confirmed? The resolution appears arbitrary.</p> <p>Summary</p> <p>The case that is made in the City of Cockburn Draft Animal Management Plan 2020-2025 for the extension of the Dogs off-leashes public facility along the Woodman Point Regional Park shoreline as per the proposal and map is not justifiable, or in my view ethical, by any measure. Please refer to the City's own on site signage, the informed Birdlife WA advice and the other references below and retract the proposal unequivocally.</p> | |
| 21. | <p>We have a dog park near us Bartram Rd Success which is well used however there is no parking on this street. Currently users park on the verge and block the footpath. More consideration with regard to this for future planning would be good.</p> | <p>Objective: 2 Action: Unrelated to actions provided in Animal Management and Exercise Plan. The submission relates to available parking at dog parks</p> |
| 22. | <p>BACKGROUND</p> <p>Woodman Point Recreation Camp conducts over 65,000 hours of structured physical activity programs primarily for children on school and community camps, on behalf of its owner agency, the Department of Local Government, Sport and Cultural Industries ('the department'; DLGSC). The camp has operated since 1982, utilising the former Quarantine Station which operated in the area from 1852 until 1979. The beach in this area is heavily utilised by children attending camps at Woodman Point, both during structured and unstructured recreational activities.</p> <p>At its meeting of 13 October 2016 (minute item 20.2), the City of Cockburn proposed the portion of beach coast south of the Coogee Beach Surf Lifesaving Club as a 'dog prohibited area', not including the Woodman Point dog gazetted beach (exercise areas) on the southern side of the peninsula, on either side of the boat ramp launching area. The decision of Council 8 December 2016 reversed this proposition, and rezoned the beach as "dog-on-lead". See location map below.</p> <p>Woodman Point Location Guide – with 2016 "dog-on-lead" zones superimposed in Purple.</p> <p>NB: The red-enclosed area is Woodman Point Recreation Camp</p> <p>Minutes of the 8 December 2016 Council meeting state that a petition of 279 signatures was received supporting the extension of a "Dogs Prohibited" area for the entire foreshore extending south of Coogee Beach Surf Life Saving Club, around to the existing dog off-lead exercise area. There was a survey of beach walkers who had been walking their</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>The City's officers have met with representatives of the DLGSC during the public consultation period who reiterated most of the concerns within the submission.</p> |



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| <p>dogs on beaches for many years, to 'retain' access to walking their dogs on the beach, although they were never formally allowed previously.</p> <p>'Key Stakeholder' comments included Department of Parks and Wildlife (now Department of Biodiversity, Conservation and Attractions - DBCA), Coogee Beach Surf Club, Coogee Beach Progress Association and Cockburn Powerboat Association. As a major stakeholder in the precinct and significant users of the beach in this area (Woodman Point Recreation Camp, DLGSC) we were not provided an opportunity for consultation prior to this proposal. At a quarterly meeting for the Woodman Point Regional Park Community Advisory Committee meeting in November 2016, we were notified by Mr Chris Beaton (City of Cockburn) that Council will be formally ratifying the existing conditions of the DBCA 'Woodman Point Regional Park Management Plan 2010', which defines the agreed location of dog exercising beaches within the locality of the regional park and exclusion of dogs in other areas including the beaches on the northern side of Woodman Point.</p> <p>The decision of Council contrasts the recommendations of the City, DBCA and the local community petition, and as a major landowner and beach user in the precinct, DLGSC were strongly opposed to the Council's decision. The Director for Facilities and Camps at DLGSC at the time, met with Cockburn CEO Stephen Cain to take-up the matter.</p> <p>There were also concerns from another local community stakeholder group, Jervoise Bay Sailing Club, who operate from this beach on the northern side of the Cockburn Cement jetty.</p> <p>In speaking with the City of Cockburn Senior Ranger Bruce Mentz in early 2017, he stated that whilst Council's decision is to allow dogs on-lead, it was difficult to have rangers patrol the beach in this area due to the distance between public access points and their likely beach entry point for the all-terrain vehicles. It is likely that dogs will continue to roam around our beach off-lead with little recourse or penalty, despite the resolution.</p> <p>The "dogs-on-lead" ruling has since presented a heightened risk to the children accessing the beach through our camp, without suitable controls in place to reduce the likelihood or potential consequence of an incident relating to dogs in this locality. As regular operators at this beach, we see many dogs being walked without leads on a daily basis. We have also had some interactions between un-restrained dogs and camp participants (children) on beach and water-based recreational activities. Fortunately, the interactions to date have not resulted in any personal harm or injury.</p> <p>CURRENT STATUS</p> <p>In early 2020, an extensive Animal Management Plan has been drafted by contractors Catalyse, and submitted to Cockburn Council for endorsement. Woodman Point Recreation Camp were included in the consultation for this report, and our concerns were well represented in the final recommendations of the report, which proposed a return to "dogs prohibited" for Woodman Point.</p> <p>At the 9 July 2020 Council meeting, Cr Phoebe Corke proposed the below amendment:</p> <p>17.1 PROPOSED ANIMAL MANAGEMENT AND EXERCISE PLAN 2020 -2025 - Alternative Recommendation – 1</p> <p>(3) gives 28 days public notice (as defined in section 1.7 of the Local Government Act 1995) of its intention to add the following reserves as dog off-leash dog exercise areas:</p> <p>e. Woodman Point Beach, Coogee, from south of the Ammunition Jetty (adjacent to John Graham Reserve) to where it</p> | |
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| <p>intersects with the current dogs off leash area, as defined in Attachment 3,</p> <p>Reason for Decision Regarding point (3) 4- e.</p> <p>On-leash dog beaches simply do not work. People take their dogs to the beach so they can run free, play with other dogs and get some proper exercise in a safe place. This stretch of beach is not over-run with dogs but the majority of the dog owners who walk their dogs there ignore the rules and let them off-lead. This means that there are other beach users who are aggrieved by this - which is, of course, their prerogative. As the beach is advertised as an on-lead area then they are completely within their rights to expect dogs at that beach to be on- lead. Obviously bad behaviour should not be rewarded but the dog owners who frequent this beach mostly go there every day and many have done so for years - since long before the on-lead rule was introduced. If it is designated an off-lead area then the rules will be clear for all concerned.</p> <p>The public consultation period is now open over the month of August based on the above alternate recommendations of Council, with the proposal to be voted upon at the Council meeting on Thursday 10 September 2020.</p> <p>IMPACT</p> <ul style="list-style-type: none"> • The presence of “off-lead” dogs on the beach used by camp participants adds a new risk to the safe conduct of these activities, without suitable controls within the camp’s authority to reduce such a risk. • The camp is likely to see a significant reduction in beach-based programs due to school and community groups not willing to put their participants at elevated risk as part of their duty of care responsibilities. • Woodman Point peninsula is a gazetted Nature Reserve and roaming dogs are likely to disrupt and negatively impact wildlife. • There already exists a dog exercise area on the eastern end of the beach adjacent to the boat launching precinct, and away from the children at the camp. • The proposed area also incorporate the Jervoise Bay Sailing Club, who have previously identified concerns about dogs off-lead moving through their club to gain access to the beach, and the impact this also has on their club activities. • Extensive community engagement and public consultation was undertaken to arrive at the recommendation in the Draft Animal Management and Exercise Plan 2020-2025 supporting the need to have this area revert back to “dogs prohibited”. • The area was made a “dogs-on-lead” area assumedly in error by council in December 2016 and advertised, giving false expectations, and now that error is being further exacerbated. • The City of Cockburn manage the beaches adjoining the park. Making an “off-lead” area this large will make practical management impossible, particularly given the Cockburn Ranger has previously stated as such due to an inability to access the area easily by vehicle. <p>SUMMARY</p> <p>A decision to allow dogs off-lead at this beach is without logical rationale. The Draft Animal Management Plan commissioned by the City of Cockburn which is now before Council, recommends that this beach returns to “dogs prohibited” for a variety of reasons, including increased risk to beach users and camp participants, the likely impact on</p> | |
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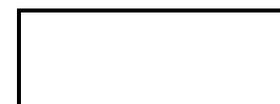
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| | <p>local wildlife within the nature reserve and the close proximity of the existing “dogs off-lead” exercise beach on the entire southern side of the Woodman Point peninsula.</p> <p>I urge Council to reconsider the proposed amendment to the Draft Animal Management Plan and consider the expert advice provided within the report and return the beach on the northern side of Woodman Point to “dogs prohibited”. A letter has been sent to this effect to the Mayor and Councillors of the City of Cockburn for urgent attention.</p> <p>RECOMMENDATIONS</p> <ol style="list-style-type: none"> 1. The department OBJECTS to the suggested alternate proposal that the Woodman Point beach be reclassified to “dogs-off-lead”. 2. The department SUPPORTS the recommendations of the Draft Animal Management Plan and return the beach to a “dogs prohibited” area. | |
| 23. | <ol style="list-style-type: none"> 1. I am strongly opposed to Woodman Point beach being a dog exercise area. I believe the bird habitat would be affected and priority should be given to preserving Woodman Point as an important conservation area. Recreation at the Woodman point camp would also be affected. Dog owners have the option of walking our dogs on-leash along the dual use path in that area right now so the beach front option is unnecessary. 2. No attention is given in the Plan to controlling the number of animals a person can keep in a residential area and the circumstances under which they can house the dogs. Currently dogs are being shut on small balconies of apartments all day long while their owners go to work and I offer the example of a large doberman that was kept for 10 hours a day in a very tiny front yard on hot artificial grass. The dog became so aggressive it was frightening to walk past. These poor dogs become extremely lonely, stressed and bark all day long. 3. I would also ask the Council to take into consideration the negative impact of more signage on the beach front. The beachfront and cycle path is being subjected to more and more signage as the Council reacts to more and more calls to control and regulate. Signage has never and will never solve the problem of bad behaviour and I urge the Council to utilise education programmes through social media rather than signage. Should it be that legal requirements may force you to apply signage could it, at the very least, be consolidated with one map showing allowable areas for fishing, dogs, safety compliance etc. | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Sentiment: Support</p> <p>Objective: 2 Action: Provide clearer delineation markers between the dog areas zoned along the coastline. Implied Sentiment: Oppose</p> <ol style="list-style-type: none"> 1. Noted. 2. The AMEP does not mention the number of dogs limited to a property, as this is governed by the City’s Consolidated Local Law. Animal welfare is not governed by the City of Cockburn, as it falls under the Animal Welfare Act which is monitored by the RSPCA. 3. Signage along the coastline is a key consideration and phase of the proposed traffic light system of signs. |
| 24. | <p>Please don’t allow dogs off leash on the beach south of the ammo jetty at Woodman Point.</p> <p>I am a kayaker and we see birds being hugely disturbed with dogs on leash, they have no chance if they can be chased. There are some delightful pairs of oystercatchers on the beach and they will be destroyed by dogs off leash. Bird life is amazing in that area and we would so appreciate the council maintaining the leash requirement.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |
| 25. | <p>I am writing to advise you that I feel very strongly on this matter and do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>Woodman Point is an important area for shorebirds such as Ruddy Turnstone, Grey Plover, Grey-tailed Tattler, and Red-capped Plover to name a few. As well as many Terns, including Roseate Terns visiting the area and the area has also been identified to manage for Fairy Tern breeding.</p> <p>The Council proposal that dogs are allowed off leash at Woodman Point, will cause considerable disturbance to birds using the area and likely deter beach nesting birds selecting the area to breed. There are other options that could be</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |



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| | <p>used for dogs, which will not impact an important area for native birds.</p> <p>Please make the right decision and keep Woodman Point as dog free as possible!!</p> | |
| 26. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 27. | <p>Please consider not allowing dogs south of ammunition jetty. This is a significant area for many local and migratory bird species and needs protection. I love dogs and love to see them having fun running around on the beach but Woodman's point is not the place for this. We regularly visit to take photos of birds not found in other areas. This could become a major Mecca for enjoying nature if the council takes steps now to start a transition to creating a sanctuary here. Future generations will thank you.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 28. | <p>I note that one of the concerns mentioned in the Animal Management Plan is the impact that dogs are having on the Australian Fairy Tern, <i>Sternula nereis nereis</i>. The area of beach between the Woodman Point Groyne (near the car park) and Cockburn Cement is, historically, an important breeding area for this species. However, due to high levels of disturbance, no attempt to breed has been recorded in the past decade.</p> <p>Nesting attempts were made in November 1997, February 1998, December 1999, December 2007 and December 2009. But in all cases, eggs were disturbed or destroyed by people or four-wheel drive vehicles (Singor, M., pers. comm.). More recently, changes to the beach zoning mean large numbers of people visit this section of beach with their dogs, which is likely to negatively impact Fairy Terns.</p> <p>Fairy Terns and the other birds using the shoreline in this area will benefit from a range of measures and I recommend that a holistic approach be taken to improve the management of this site. Particularly, if the aim is the support nesting attempts by Fairy Terns and to restore the natural values of the area. This includes the prohibition of dogs, which will reduce disturbance, particularly from un-leashed animals, new regulations that restrict cats from the Woodman Point Reserve, trapping of pest animals (cats and foxes) within and around the reserve. Particularly in the pre-breeding season when Fairy Terns and other shorebirds aggregate in the area. Additionally, a community education program, designed to help reduce the impact of beachgoers on feeding, roosting and nesting shorebirds.</p> <p>Dogs have been identified as a major threat to beach-nesting birds (Maguire 2018). In 2018, BirdLife Australia completed 'A review of dog impacts to beach-nesting birds and management solutions' (Maguire 2018). The impact of dogs ranged from disturbance, causing adults to leave their broods unattended to egg and chick predation. No dog areas are listed as being the most effective strategy to protect shorebirds/ beach-nesting birds (Maguire 2018).</p> <p>In Victoria, the prohibition of dogs from a beach on the Mornington Peninsula resulted in a doubling of Hooded Plover fledgling production, compared to the previous decade. For further details, see Maguire (2018).</p> <p>The impact of free-roaming cats on wildlife populations is a significant problem and must also be addressed (Osborne & Williams 1991; Greenwell et al. 2019; Legge et al. 2020). In 2018, an entire colony of Fairy Terns (~220 birds) in Mandurah was destroyed by a single, free roaming desexed cat that depredated adults and chicks over several nights, until eventually, the colony was abandoned (Greenwell et al. 2019). The issue of beach management to protect Fairy Terns and other shorebirds in the area is</p> <p>complex, but adopting a range of measures, as noted above, including the exclusion of dogs, will go a long way towards</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support</p> |



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| | <p>reducing disturbance and improving outcomes for birds utilising this historically important site.</p> | |
| <p>29.</p> | <p>I urge you to oppose dogs being allowed off the leash in Santich Park.</p> <p>There are enough off-leash areas already.</p> <p>Unfenced dog parks will not prevent dogs from running onto the road or running at people. Using sporting fields as off-leash areas will lead to increased poo around the park and altercations.</p> <p>I am totally opposed to dogs being allowed off leash in Santich Park. It is a nice park where one can go and relax. I go running there and have had dogs on the leash bark and jump at me as I run past. Goodness knows what they would have done if they were off the leash.</p> <p>On another occasion, I was running on the footpath along Asquith Street at Santich Park when a dog, which was off the leash in the park, left the park and ran towards me. It was only when it was half-way across the road did it heed its owner's shouts. At the moment, I can remind the owner that the dog needs to be on a leash.</p> <p>Last week, I took my small dog for a walk around Santich Park. After a nice, quiet walk and as we came to the corner of the park near Shelley/Asquith Street, a woman entered the park with two big dogs on the leash. She let one dog off the leash and it immediately ran towards mine, which did not like the attention and, to avoid an altercation, I had to lift her up and take her out of the park.</p> <p>An elderly neighbour was knocked to the ground on the footpath near the corner of Shelley Way/Asquith Street by a dog which was off the leash, and had been off the leash in the park. The dog's owner didn't help her get up, didn't apologise and simply walked away with the dog. An elderly woman who walks her blind dog in Santich Park was knocked to the ground by a dog off the leash in a park a few minute's walk from Santich Park. The dog's owners left her on the ground, put the dog into their car and drove off. She was later taken to hospital. At the moment, she feels safe walking in Santich Park. I was walking in Santich Park with my young lad not too long ago near the car park when a man entered the park with a German Shepard. He let it off the leash. It immediately ran towards my young lad. It ran around him and back to the owner, who did not restrain it. The dog ran again towards my young lad and this time jumped up at him. When I called the owner to put the dog on a leash, he treated it as a joke and it was only when I pointed out a witness to the incident did he take the dog out of the park. I reported it but the Rangers were not able to locate the dog. Now, when my young lad sees a dog off the leash, he wants to leave the park.</p> <p>On another occasion, I was walking my dog with another man and his small dogs in Santich Park when a woman approached with two big dogs off the leash. One of those attacked one of the other man's dogs and when he pulled his dog up to safety, the woman criticised him for doing so. She was in the wrong.</p> <p>It is now proposed that such dogs as those mentioned above be allowed off the leash. It is farcical and a slap in the face to those dog-owners who have done the right thing over the years by keeping their dogs on the leash. Santich Park has two playgrounds and dogs off the leash will go into them. It will be an unhygienic mess. If there are children playing there when dogs enter, who knows how the dogs will behave around the children. Very often, the owners of these dogs continue walking around the park and are oblivious to where their dogs are and what they're doing.</p> | <p>Objective: 2</p> <p>Actions: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas.</p> <p>Sentiment: Oppose</p> <p>Community consultation suggest there is wider public support for the change. Although the proposal is to allow dog owners to have their dogs off-leash within the Reserve, the owner will be required to ensure their pet is under their effective control. The proactive educational style patrols suggested within the AMEP will ensure this is better educated to dog owners.</p> |



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| | <p>It is also farcical that more Rangers will be recruited to patrol a reduced number of areas where dogs will have to be on the leash. It seems to be a cop-out so that Rangers can avoid having to ensure that current by-laws are being complied with.</p> <p>Why cannot more Rangers be recruited now to ensure dogs are on the leash in such parks as Santich Park?</p> | |
| 30. | <p>I am against the proposal to allow dogs off lead on the north beach of Woodman Point. This is due to the number of children from the Woodman Point Recreation Camp using the beach for their activities.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 31. | <p>Please keep the area from Ammunition Jetty to the Cockburn cement jetty as an on-lead area. I need somewhere I can take my dogs on lead without them being attacked by off lead dogs. If you make the ammunition jetty beach off lead then people will do what they do at the woodman point boat ramp which is open the car door and let the dog run without supervision. This is a family area and that is not appropriate. I can walk my dogs through that area and there is no dog faeces. If this becomes off-lead it will be as bad as CY O'Conner which is so disgusting you can't even walk on the path for all the faeces.</p> <p>A visit to the Cockburn chat Facebook page should highlight the problem with people's mindset around off lead dogs. There are too many dog attacks. Dog attacks create aggressive dogs.</p> <p>Please leave some areas as on-lead so we can all enjoy taking our dogs out in the city of cockburn.</p> <p>I recommend having discussions with animal behaviour experts before you make everywhere off lead. If you make all areas off lead those of us who are responsible with our dogs will have less opportunities to enjoy Cockburn's facilities than those who don't care.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 1 Action: Partner with animal behavioural specialists to develop and administer an educational campaign. Implied Sentiment: Support</p> <p>The suggested benefits of having an on-lead dog beach is not widely supported by the community due to the inadvertent environmental damage done by dogs. The coastline offers many walking paths that are remaining as on-leash areas.</p> |
| 32. | <p>Sounds good. 👍📄</p> | <p>Objectives: All Actions: All Sentiment: Support</p> |
| 33. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 34. | <p>I live opposite the Beelias Oval. I am deeply distressed with the above Proposal.</p> <p>We are Seniors in our 70's. The only place I can exercise and walk around is on the Oval, as we cannot do any rigorous exercises.</p> <p>Yesterday at 11am when I walked around the oval, 2 dogs came charging at me. The owner as usual said they will not harm you, I am terrified of dogs. I told them dogs are to be on leash. She reacted and said they are vicious. Can you imagine if dogs are off leash, everyone will come to let their dogs run loose here.</p> <p>Where do you think I should exercise?</p> <p>There are 2 parks around here: the Costa Park and the Milgun Reserve where dogs are permitted off leash. I never go to these parks. They can exercise their dogs off leash there.</p> <p>Do you put the needs of animals over the needs of us humans?</p> <p>Please do not let the Beelias Oval be Dog Off-leash.</p> | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beelias Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> <p>The proposal to have Beelias (like all other locations) will require the owner to have their pets under effective control. As part of the AMEP a proposed 'no its not ok' program will be created to educate dog owners of other users of public open space.</p> |
| 35. | <p>No dogs should be allowed in apartments, as it is not only noise pollution but also animal cruelty.</p> | <p>Objective: 3 Action: Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2, and then implement Stage 1 and 2</p> |



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| | | <p>Implied Sentiment: Support</p> <p>The ability to restrict owners of apartments to keep dogs is not allowed under the State Dog Act of 1976.</p> |
| | <p>Australia has the worst extinction rate of mammals globally.</p> <p>Cats are responsible for the more small mammal extinctions than both dogs / wildfires and urban development combined.</p> <p>There are a number of small mammal species in the shire such as bandicoots etc and these need to be protected at all reasonable costs.</p> <p>The solution regarding cats.</p> <ol style="list-style-type: none"> 1. All cats to be kept indoors 2. All cats to be sterilized. 3. Registration Fee to have cats 4. All to be microchipped. | <p>Noted. Apart from point one within this response all other elements are an existing legal requirement for cat owners.</p> |
| 36. | <p>I am a dog owner, and my terriers and I love the beach, however I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>There are plenty of places we can walk — but our precious native and migratory birds don't have that option. This area is critical to their long term survival, and they must be protected from as many predators as possible.</p> <p>This isn't an attack on dogs — as I said, I have them myself and love them dearly! — however all it takes is one irresponsible owner, only one dog off-lead, to decimate bird and other native animal populations.</p> <p>Australia has lost so many native animals during the catastrophic bushfires that ravaged the country. We likely will again. We need to protect those who are left.</p> <p>Please help our native animals. We dog walkers can go elsewhere.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 37. | <p>I strongly oppose Woodman point being changed from on-lead to off-lead and would like it to be "dog-prohibited" year round. The Fairy terns need all the protection they can get.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 38. | <p>Most of the Draft Animal Management & Exercise Plan 2020-2025 I think includes excellent great ideas to help protect local fauna, however I have the following concern.</p> <p>I am strongly against the whole of woodmanpoint beach area south of the recreation centre to become dog of lead as is proposed in the draft.</p> <p>This is one of the very few areas (the only one???) where you can still see fairy tern if you are lucky. Loose dogs will not help sustain this.It is also the only place in Perth where I have seen the blue wrens.There is a strong doglobby that lobbies for more of lead, but this is already supported in the proposal in other areas.</p> <p>Cockburn council needs to protect the fairytern and all other birds that are specific in this area!</p> <p>The council talks about prohibiting dogs when birds are in nesting season, but I think when dog owners are in the habit of having their dogs of lead, they will not know about nesting season and or follow these guidelines, and I think the birds need protection all year around.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | I trust the council will protect our local fauna and these rare and special birds that we have here in Cockburn, and keep the local dogs on lead at Woodman point. | |
| 39. | DISAGREE A change in zone for our beach will significantly impact the safe delivery of our beach and water programs for school and community groups at the camp. A dog beach already exists in the area. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 40. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 41. | I am an original resident of Paradise Grove and live opposite the Freshwater Reserve. I am dead-set opposed to making the Reserve an "off-leash" area for dogs for the following reasons; 1. The (soon to be updated) play equipment is popular with families of small children and I feel it is too dangerous to have free roaming dogs and small children in the same area. 2. Increased dog activity in the Reserve will result in increased dog faeces left behind. It would be great if everyone cleaned up after their pet, but the reality is far from it. 3. I have witnessed fights between dogs in the Reserve. Having free roaming dogs will only make this a regular occurrence. 4. Freshwater Reserve is a small park with a playground, soccer area and some room for people to play and exercise. Dogs running free are incompatible with this area. The proposal to increase Ranger activities to patrol "on-leash" and "prohibited" areas is a great idea. More patrols locally and at the beaches would be ideal. The proposed cat laws are welcome and well overdue. There used to be Bandicoots in the park and adjacent bush but the local cats wiped them out years ago. | Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Oppose Objective: 3 Action: Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2, and then implement Stage 1 and 2 Implied Sentiment: Support As part of the AMEP a proposed 'no its not ok' program will be created to educate dog owners of other users of public open space. The program will specifically target the need for co-use of smaller parks by dog and non-dog owners alike. |
| 42. | I am proud to live in Cockburn a shire that cares and listens to it's residents. Yes, some really great ideas there but people cooperation is vital. I think we need higher penalties for those who deliberately ignore the rules, mandatory sterilisation for both cats and dogs. There are far too many surrendered animals out there and basic obedience training should be encouraged if not mandatory. It is NOT a pleasure to walk my dogs the way things are at the moment and it will get worse even in a conservation area like Manning Park. A pet is for life, a huge commitment which people take on to lightly. Animals are not disposable items after all. A greater ranger presence In parks and regular registration checks would be very beneficial and reassuring. The amount of dogs/cats registered is laughable. So many people doing to wrong thing. Also something to consider, if ratepayers live on a large block e.g over 600sqm then three dogs should be considered but if a ratepayer lives on a 250sqm block a limit of one large or two small dogs would be better. Animals need exercise and grass. | Objective 1: Action: Increase dog and cat registration checks in parks, reserves and beaches and home audits Implied Sentiment: Support Objective 1: Action: Develop Council Policy for standardising the issuing of animal control infringements. Implied Sentiment: Support (Submission suggests higher penalties for infringements) |



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| 43. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 44. | I am a big user of all the beaches at Woodman Point for swimming, kite surfing, fishing and so on. I am a member of the Jervoise Bay Sailing club, Cockburn Power Boat association, a Cockburn local and a dog owner. The concept of un leashed dogs on the Northern Beach of Woodman point beach is a bad idea and I strongly oppose it. . Unleashed dogs on the North WP beach does not fit with the main uses and users of the beach. Having unleashed dogs amongst the camp and sailing club activities will have serious impact and presents a significant risk to those involved and the future of the activities. As a dog owner- One unleashed dog beach at Woodman Point is more than adequate in my opinion. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 45. | Thanks for all the hard work in trying to please everyone! I know if I had a dog I would want that socialising aspect with other dog owners with dogs off lead - and agree they must be enforced to PAY ATTENTION and PICK UP My only other concern (as I am now entering the first grandchild stage) is that on very small community grassed areas where dogs and owners socialise is that I don't want toddlers crawling on the grass that lots of dogs have pooped on - even if it has been removed. | Objective: 2 Action: Unrelated to Animal Management and Exercise Plan specified actions. Submission expresses concerns for about dogs on small community parks. Implied Sentiment: Concern |
| 46. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 47. | Cats-include the option for domestic cats to be allowed outside the property of the owner to be walked on a lead. I previously owned a cat and did this on a regular basis. This is responsible cat ownership. I do support domestic cats being inside the owners property at all other times. Dogs-There needs to be greater enforcement and penalties for owners who walk their dogs off the leash in areas where this is not permitted, including obvious signage. I have experienced this on many occasions in my local suburban area. These owners see this as their right and there seems to be no consequence for this behaviour. | Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (however submission suggests patrols include dog permitted on leash areas) Objective: 1 Action: Develop Council Policy for standardising the issuing of animal control infringements. Implied Sentiment: Support (Submission suggests higher penalties for infringements) Objective: 3 Action: Stage 2: (2023) Enact laws for cat control based on similar requirements for dogs (requiring cats to be retained within the owners property at all times). Implied Sentiment: Oppose The prosed cat laws would include a provision to allow owners to walk their cat, providing they do not enter a conservation reserve. |
| 48. | I do not want dogs on the beach at the above location. <i>(Subject line – Woodman Point)</i> I have seen on several times dogs running around out of control. The wild birds are stressed and leave the nesting area. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 49. | I am opposed to the prospect of dogs being able to run off-lead at Woodman Point Beach. This beach is utilised by the students and clients who attend the local recreation camp. Dogs pose a significant risk to the public who are not on leads. Children may be scared of animals and this could make | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |



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| | <p>them very anxious or a dog may run up to a group of kids and then becomes startled and bites a child.</p> <p>Members of the public do not pick up after their dogs and students' participants in activities that requires them to drive into the sand, this will pose a huge safety issue.</p> <p>Kids need outdoor recreation in a safe environment, dogs can be on a lead and still enjoy the beach.</p> | |
| 50. | <p>The Freshwater Park is directly opposite me and the proposed "dogs off leash" council changes directly affect me and all the residents opposite the park. I am disappointed that I have not been better consulted on this matter. I do not under any circumstances want dogs to be allowed off their leash to roam freely in the park. Already we have dogs roaming in the park, secreting on our lawn and walking into our backyard. It is annoying and it is sometimes dangerous as there are many families in the street with small children. We have had dog fights frequently from dog owners having their dogs off their leads. In fact, I would request that dog patrols are increased in the park to make sure they are on their lead. Furthermore, we already have a soccer over and a playground in such a small park area. Dogs off leads and more dog owner traffic is incompatible with this small open space. Please keep dogs on their leads and send increased dog traffic to other more suitable locations.</p> | <p>Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Oppose</p> <p>As part of the AMEP a proposed 'no its not ok' program will be created to educate dog owners of other users of public open space. The program will specifically target the need for co-use of smaller parks by dog and non-dog owners alike.</p> |
| 51. | <p>I wish to express my disagreement with making Aubin Grove Colorado park a dog exercise area. There are too many young children that live in the area along, including my two young daughters, with numerous exercise classes that utilise this park at all hours, it would be unsafe to make it an off leash or dog exercise park. Please do not let this go ahead.</p> | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> <p>Colorado park was subject to strong local community support for the park.</p> |
| 52. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 53. | <p>I do not support this as it is a great migratory bird nesting and arrival point. Dogs would destroy this! They already have owners who disobey the signs . This would be a retrograde step!</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 54. | <p>I am very upset and annoyed to receive a letter saying you are considering making our park a dog off leash area!</p> <p>I am a resident and home owner on Birch Road, my house overlooks Colorado Park. At the present the noise from the dogs in the neighbourhood along with fitness classes is just bearable. This park is not suitable for more dogs and off lead! it is used by many children who are already scared of dogs barking at them - I've seen many scared children including my own. There is no barrier between our house and the park and I am already sick of seeing dog shit in the park when I walk about, let alone the noise. Other dog parks in neighbouring suburbs are much further away from residential properties. Please don't ruin this park for the kids who enjoy the open space to kick a footy and play, bringing in an area for dogs off lead ruins this for everyone.</p> | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> <p>Colorado park was subject to strong local community support for the park.</p> |
| 55. | <p>In support of Birdlife Western Australia I do not support dogs on the beach at Woodman Point south of the ammunition jetty. For reasons indicated in the draft....</p> <p>Research has shown that dogs on a leash often have the same environmental impact on sea nesting birds, as those not on a leash. The Department of Biodiversity, Conservation and Attractions, and members of the community are concerned about the impact of dogs on Fairy Terns. Fairy Terns are known to nest at Woodman Point and are listed as vulnerable under both state and federal legislation and thus are a protected species. In Western Australia, predation from domestic and feral animals such as dogs, cats and foxes has reduced population numbers and breeding success. Further concern has been raised by BirdLife Australia, a prominent bird protection organisation in Australia. Recent</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>research into the Woodman Point area has shown the beach is an ideal habitat for a wide range of migratory and resident shorebirds, including species like migratory Ruddy Turnstone, Grey Plovers, and the Grey-Tailed Tattler. Up to 26 species of shorebird have been recorded at the site. The area also could potentially provide breeding habitat for resident beach-nesting birds, including Red-Capped Plovers and Oystercatchers, though the amount of human activity and the frequent presence of dogs has likely impacted birds selecting the area to breed in recent years. Making the area dog free, as well as measures taken to facilitate Fairy Tern breeding will also benefit other beach-nesting species. Research has shown dog walking (both leashed and unleashed) is a major disturbance to shorebirds, due to their need to spend large amounts of time feeding in the shallows and exposed sand flats. They do this to build body condition in preparation for their long migration to the northern arctic to breed, and to recover when they return to Australia. There is a growing body of research indicating that frequent disturbance can significantly impact shorebird feeding behaviour, causing birds to waste energy in repeated flight to get out of the path of dogs and their owners. In some cases, this can cause birds to stop using a site altogether.</p> | |
| <p>56.</p> | <p>We are inundated with dogs in our North Coogee South Beach end. I rarely see a dog on a lead. They are left to wander free by the majority of pet owners. Into people's gardens annoying, dogs on leads and excreting in front gardens and on paths. When you remind people to pick up the poop or ask them if they'd like a doggy bag, they become defensive.</p> <p>The same laws are disregarded by the horse owners.</p> <p>We rarely see rangers in the area.</p> <p>It would be great to see dogs on leads in all areas.</p> <p>There is a real misunderstanding of dog exercise areas in our estate.</p> <p>It would be great to see dog laws enforced.</p> | <p>Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (however submission suggests patrols include dog permitted on leash areas)</p> <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| | <p>I do not support dogs on the beach at Woodman Point, south of the ammunition jetty.</p> <p>Domesticated dogs threaten the conservation of beach nesting birds through disturbance and destruction of eggs and chicks.</p> <p>Harassment by dogs repeatedly interrupts birds ability to feed and rest putting them at risk during long distance migration</p> | |
| <p>57.</p> | <p>I am the closest resident to Woodman Point Beach, and I am personally opposed to the Dogs off Lead alternate proposal for Woodman Point beach. I have lived at this location for 12 years. I also owned a dog until it recently passed away.</p> <p>Many school and community groups, along with individuals and families, access this beach for recreation everyday - myself included.</p> <p>I was opposed to the 'dogs on lead' ruling of 2016. Since then, i have seen owners ignoring the rules and letting their dogs run off lead on a daily basis. That's understandable as dogs should be able to run free on a beach, where it is designated as a dog exercise beach. There has been a dog exercise beach on the southern side of Woodman Point for many years. AT its closest point, this beach is a mere 200m from the beach now proposed to be off-lead. There is no rationale for this addition to the some 1200m already available to dog owners on the peninsula.</p> <p>Please consider the regular beach goes, school and community groups, businesses and clubs operating in this location</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | for many years, and do not allow this absurd recommendation to be passed. | |
| 58. | <p>A member of Jervoise Bay Sailing Club strongly am against the proposal to allow dogs off lead in the Woodman Point Beach area.</p> <p>That part of the beach is widely used by local beach goes and sailing club events.</p> <p>Having dogs run free will have a impact on the beach uses and interfere with sailing programs.</p> <p>There is a dog area just on the South side of Woodman Point beach so there is no reason for one on the North Side which is a well used area by people.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 59. | <p>The rare fairy tern needs protection.</p> <p>Dogs off their leash will cause much disturbance to this bird's breeding process.</p> <p>In fact dog's in the fairy terns breeding ground should be banned especially south of the Woodman point.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support</p> |
| 60. | <p>I am writing to express my concern over the proposed changes to the dog off lead areas around Woodman Point.</p> <p>I am a dog owner and frequent the dog off leash exercise beach on the south side of the Point. This is more than adequate for exercising my dog. I don't take her along the dog on lead section of the beach to the north as it is populated by families, the sailing club and the participants at the Woodman Point Recreation Camp. Even though I consider my dog friendly I would hate for something to occur with a child.</p> <p>I have also noticed that many people ignore the on lead sign and let their dogs run free harassing people on the beach which I find very disrespectful and inconsiderate. There are many people within our community that have genuine fear of animals and dog owners need to be more aware of this. Because they aren't I would suggest the on lead section of the beach be returned to a dog prohibited area.</p> <p>My stance is that the current off lead section to the South is perfect. All other areas should be prohibited to dogs. Keep our families safe and consider the needs of the participants of the sailing club and recreation camp. I can foresee it would be a huge risk to these organisations if dogs were allowed to roam free.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 61. | <p>Please do not let dogs on Woodman point beach south of the ammunition jetty Woodman Point is a well-known birding site throughout the world!</p> <p>You are lucky to have such a place in your shire boundaries.</p> <p>There are only few remaining birding sites of this nature left in the Perth metro.</p> <p>Dogs can exercise in parks and other less venerable locations.</p> <p>Leave Woodman point to the birds please they don't have anywhere else to go.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 62. | <p>I am writing to register my formal objection to the proposal to allow dogs off the lead, or for that matter on the lead, in the Woodman Point Regional coastal area, particularly south of the Woodman Point jetty.</p> <p>This area of beach has in recent years already been subjected to increasing numbers of dogs on leads, as well as a</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>number of irresponsible dog owners who let their dogs off the lead. In the past 5 years it has been very clear that the number of fairy terns and other migratory birds have been declining.</p> <p>I have walked and run along this stretch of beach for 30 years and in recent years the number of people walking their dogs off the lead has increased and I have witnessed dogs chasing birds, chasing other dogs and at times running up to people and defecating on the beach and owners often leaving the droppings behind. These irresponsible dog owners are reminded by myself and others that dogs need to be on a lead and their mess cleared, which has on occasion resulted in being abused and the dog allowed to continue off the lead. It is only a matter of time before a native animal is hurt, or there is a serious dog attack on another dog, an attack on a person, or an altercation between non dog and dog owners.</p> <p>I was recently attacked by two dogs which were both on a lead and I required treatment by a doctor and a course of antibiotics. The attack occurred in a City of Cockburn public open space, where dogs are allowed on a lead and I dread to think how bad the situation would have been had the dogs not been on a lead and worse still, had the attack been on a child, or one of the many native animals and birds in that area. It is a reminder that even the best intentioned dog owner is not guaranteed to be able to maintain 100% control of their dog even when on a lead, so it is a significant concern what potential damage a dog can do when not on a lead.</p> <p>The other major concern with the Draft Management Plan proposal is that both the south and north sides just near the jetty are a very popular part of the beach for families with children, particularly in summer time. Having dogs off the lead anywhere near that area could be a significant risk for families. From personal experience, I know that dog attacks can occur even when unprovoked and it is also very clear in the City's management plan that 'The practicality of constant enforcement within this area is difficult, and will likely require a significant increase in Ranger resources to maintain a strong enforcement capability along the coast, especially during the summer months'. Is this something that the City wants to spend additional resources on when there are currently alternative dog exercise areas that cater for dogs off the lead in the area, or worse still not manage to get the surveillance balance right and risk a serious incident occurring on a public beach?</p> <p>We are extremely fortunate to currently have different areas designated for various uses and enjoyed by all parties responsibly along our amazing stretch of coastline. There are currently designated dog exercising areas along the beach at CY O'Connor and further down the beach at the southern section of Woodman Point between the WAPET and the Woodman Point Groyne's. If someone wants to interact with dogs and their owners, then they can frequent those designated dog beach areas.</p> <p>To allow dogs on or off the lead in the Woodman Point Regional Park coastal area where there are known migratory birds, conservation and wildlife hotspots and people enjoying and interacting with the natural environment responsibly, (which very often include elderly persons and children), it is a highly irresponsible move by the City to introduce dogs on or off the lead in this area and unnecessary when there are other dog exercise areas in the near vicinity. We are extremely fortunate to have such a beautiful section of coastline that can be enjoyed by all parties in a responsible way, please keep it that way.</p> | |
| 63. | <p>I am writing in relation to the proposed 'dogs off leash beach'.</p> <p>I am opposed to this occurring as it impacts the SAFE operation of the use by Woodman Point Recreation Camp – which is fantastic to see communities and school groups utilising that beach area in a safe environment without stressing about animals interfering or attacking.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>A dog exercise beach already exists on the southern side of Woodman Point, a mere 200m from our beach at its closest point, so therefore there is no need for any more room for the dog beach, as much as I am an animal (dog) lover and owner myself. I believe it is about balance and having a fair amount of space for the multiple uses between dog beach areas and general public and recreation camp use. The last thing I'd want to see was if a dog bit a child due to the council approving dogs off leash in this area. Safety first!</p> | |
| <p>64.</p> | <p>I live in the Atwell area and have noticed the lack of dog exercise areas, specifically fenced areas. I currently use the new park on Abelia Road in Treeby/Caleeya and it would be wonderful to have a similar fenced dog park in the Atwell suburb and I was wondering if the council would consider building one at the Brenchley park area on Brenchley drive Atwell. Currently this area isn't used for any other purpose and is Already grassed. I don't believe any parking would be required as there is the atwell oval close by with an existing car park</p> | <p>Objective: 2 Action: Submission does not directly related to Animal Management and Exercise e4 Plan actions. Submission proposes additional fenced dog exercise areas. Implied Sentiment: Support</p> <p>The AMEP recommends the proposal of two additional parks within the western ward due to lack of pet friendly infrastructure. Within a 3km radius of Atwell their a number of enclosed dogs parks already been established.</p> |
| <p>65.</p> | <p>With respect to managing domestic and feral cats I would like to add to my comment that I do appreciate that cats can kill a significant amount of wildlife. So perhaps if a cat owner lives close to a conservation or bushland area and their cat is shown to visit these areas, they should be required to keep the cat confined to their property.</p> | <p>Objective: 3 Action: Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2, and enact Stage 1 and 2 Implied Sentiment: Support</p> |
| <p>66.</p> | <p>I thought I would share my views and thoughts about the dog beach proposals at Woodman Point Nature Reserve.</p> <p>I have been a regular visitor to this site for over 30 years and have seen many changes during that time.</p> <p>My interest has been in research concerning the migrating shorebirds that use this location and these comments are made without prejudice. My research has shown an 80% decline in shorebirds in this reserve over the past 25 years.</p> <p>I remember when you could occasionally see seals on the beach; shorebirds and seabirds used to breed there and dolphins would chase schools of herring right up to the shoreline.</p> <p>You are undoubtedly familiar with the Woodman Point Regional Park Management Plan 2010.</p> <p>I am not sure if the City of Cockburn can override undertakings and strategic objectives that were laid down in the Woodman Point Regional Park Management Plan 2010 endorsed by a number of Departments. This plan is still in force to the best of my knowledge?</p> <p>The Woodman Point Regional Park Management Plan 2010 was signed off by</p> <p>Conservation Commission of Western Australia, Department of Environment and Conservation, City of Cockburn, Department of Transport and the Department of Sport and Recreation.</p> <p>The Management Plan is quite clear on what level of dog access is permitted in the different sections of the Woodman Point Regional Park and who is responsible in enforcing these. Part of the Woodman Point Headland is classified as a Conservation Reserve (Reserve 49220).</p> <p>I have attached parts of the Management Plan as reference.</p> <p>It clearly states dogs are not permitted in the nature reserve (Reserve 49220) and it is a joint responsibility of the City of Cockburn and Department of Environment and Conservation to ensure public compliance.</p> <p>I would be very interested to know how many infringements notices the Council rangers have issued in respect to the Woodman Point Nature Reserve. Having visited that area for many years I know for a fact that dogs run rampant throughout the Nature Reserve and no enforcement takes place. The only time I have seen rangers is when they were having their lunch in the parking lot overlooking Jervoise Bay.</p> | <p>Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (however submission suggests patrols include dog permitted areas)</p> <p>Objective 1: Action: Develop Council Policy for standardising the issuing of animal control infringements. Implied Sentiment: Support (Submission suggests penalties be enforced more frequently)</p> <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Provide clearer delineation markers between the dog areas zoned along the coastline. Implied Sentiment: Support</p> |



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| <p>The proposal to allow dogs off-leash in the Conservation area comprising Woodman Point Headland is beyond my comprehension as is allowing the area for kite surfing. These activities will destroy the last vestiges of conservation value this reserve has. Your own research shows this beach is at the lower end of the scale when measured for dog usage. Placing it off-limits to dogs will therefore have minimal impact.</p> <p>Considering Jervoise Bay is already a large dog beach area and if you include the beach area to the north of the Cockburn Cement works as a dog beach, my proposal to have the Woodman Point Headland Reserve off limits to all dogs does not seem unreasonable and a very small concession in the overall picture. Especially when we are looking at sustainable conservation in a rapidly growing urban setting in this day and age.</p> <p>My suggestions are:</p> <ul style="list-style-type: none"> • Woodman Point Conservation Reserve (Reserve 49220) is off-limits to all dogs and ideally having the top part fenced off during the peak shorebird migration season and Fairy Tern breeding season (October to February). Fencing to be placed where the dune vegetation starts. • Clear signage be installed at the start of Woodman Point View Road informing visitors where designated dog beaches are. You cannot expect the public to abide by any regulations unless they are clearly signs posted. There used to be signage at the start of Woodman Point View Road and at the beach entrances at the car parks. Often wondered why they were removed. • Reduce the number of parking bays at Woodman Point fishing groyne overlooking Jervoise Bay <p>This will relieve the pressure on the conservation reserve. This will be compensated by providing additional parking bays along Woodman Point View Road.</p> <ul style="list-style-type: none"> • The beach area to the north of the Cockburn Cement Ltd shell processing plant to the Woodman Point jetty at the John Graham Recreation Reserve has limited conservation value and is suitable for a dog beach in my opinion. Access to this area would need to be improved as access is presently difficult. • Better access to the beach between the Cockburn Cement Ltd shell processing plant and the jetty at John Graham Recreation Reserve. Maybe improved access can be created by a southward pointing walking track to the beach from the John Graham Recreation Reserve parking lot. Additional access to the beach be considered from the end of O’Kane Court or at the Jervoise Bay Sailing Club. • The car park bay areas to be enlarged along Woodman Point View and sign posted as dog beaches. The beach area facing Jervoise Bay is already a dog beach and improved carparking, dog facilities and signage can direct dog owners to this beach and improve it’s utilisation. Thereby keeping dog owners away from the Woodman Point Conservation Reserve. • Genuine enforcement of the regulations needs to be carried out. Maybe providing an educational period of three months when only warnings are issued before taking a harder line. Having a ranger frequent the area over weekends and holiday periods will be beneficial. Work schedule could be negotiated so they have two days off during the week? • I recommend that a CCTV mast be installed at the carpark near the Woodman Point fishing groyne. I have in the early hours (3.00 am) of the morning over summer found tanker trucks in the car park on the point pumping waste directly into Jervoise Bay. I suspect considering the time, these have been illegal waste dumping’s and likely toxic. In case you wondered just before sun rise is a good time to monitor shorebird numbers. | |
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| 67. | I would like to oppose the Dogs off lead proposal on Woodman Point beach and have this recorded. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 68. | I often talk to customers who frequent the beach and have had a lot of feedback regarding the dog beach. Many people seem to prefer to go to CY O Connor beach so that their dogs can be off leash. I guess its quite difficult to exercise the dog when they can't go for a swim. I would be in favour of allowing dogs off leash in that area. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Support |
| 69. | I am writing to oppose the Woodman Point Beach section being made into a Dog Off Lead Beach. My children both participate in sailing at Jervoise Bay sailing club and my youngest son has ASD and a significant fear of dogs approaching him. This would effect his enjoyment and most likely participation in the program. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 70. | <p>I just completed the survey but was disappointed not to find a general 'other comments' area where I could do my brain dump and so I'm going to give you my thoughts here.</p> <p>First, thanks very much for the opportunity to comment on this draft management plan. It's an area I have a lot of interest (and stake) in. I love animals but I love native wildlife too and these two things CAN coexist.</p> <p>Second, broadly speaking I support most of the ideas in the plan and so please view the following feedback as 'ancillary' – just ideas/observations I thought might be useful, and no-one's ever asked me. There might be a bit of repetition with my survey feedback bc I wasn't sure if they'd be read by the same people.</p> <p>Okay, here we go...</p> <p>TERMINOLOGY: 'desexing' – this has traditionally meant neutering but I'd love to see some more allowance for different kind of 'sterilization' in the management plan language. My dog, for example, is vasectomized but not neutered because science has shown that most animals live longer, healthier lives with their full set of hormones, particularly large breeds. Neutring a dog or cat at 3 months of age robs them of that for the rest of their lives and can be very detrimental for large breeds particularly. So, please could the language be reconsidered to allow for people who choose other forms of sterilization.</p> <p>In the Cat management area, the restriction around wearing a collar is 'in public places'. Could this please be added to the dog management one too? It is safer to leave dogs unattended in back yards without collars on (avoids snagging/choking hazards). Makes the rules consistent/simpler.</p> <p>Something to consider in the community education is that it's not just aggressive or anxious dogs that are a problem in public areas. Hyper-social dogs and/or dogs who have not been correctly socialized to observe polite social distancing are just as much of a problem to non-dog-lovers or people with anxious dogs. When a dog comes running at me, my concern is not just 'what is that dog going to do' but 'what is MY dog going to do'. Even a friendly dog can cause a problem for an anxious dog (and its anxious owner). ALL DOGS SHOULD SOCIAL DISTANCE and if t hey don't' do it naturally then the owners should be required to enforce it.</p> <p>Cat collars – could there be some thought given to collars having to be the 'snap off' kind if they're compulsory. I know that probably flies in the face of your own rules but they're very inhumane when they get snagged and choke the cat. Microchipping should be enough to identify a found/trapped cat without needing a collar. Not really sure what the benefit of a collar is, actually. People don't tend to catch up and return wandering cats the way they do dogs.</p> <p>Would love to see all dog areas have geo-fencing installed (presume it works off the tags or the chip??) not just the formally fenced areas.</p> | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> <p>Note: Submission suggests additional actions. The submission also notes a general support for most Objections and Action in the Animal Management and Exercise Plan. A number of parts within the submission will be taken under consideration as part of the operational roll-out of the Plan.</p> <p>The usage of areas such as Water Corporation land would require approval from the land manager. The sites specified within the submission would unlikely be supported by land manager due security, safety and infrastructure onsite.</p> |



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| <p>Item 1.5 – could it please be remembered that socialization training is not just for pups. I would love to have somewhere (fenced) where I could go with my anxious adult dog to give him social opportunities in a safe environment delivered by someone who knows animal behaviour. Adult classes would be awesome.</p> <p>Beeliar oval – If Beeliar Oval becomes off-lead, I will no longer be able to use it. It’s that simple. I don’t think this is a) safe or b) appropriate to be a dog off lead area 100% of the time. It’s a pretty busy road and people (including busses) roar along there. Unless you’re prepared to fence it all in (are you? That might change my view), it’s one place that I can go with my anxious dog (on lead) and enjoy a long walk without every other dog in the neighbourhood running up to us. People simply DON’T control their dogs, even when they can. They assume everyone there is there for the same reason they are – free, wild, runfest. The reality for many people is that that is a nightmare for walking an anxious or reactive dog. If you could introduce a odds/evens day – eg: even days of the month it’s leads on, odd days of the month it’s leads off then everyone has a chance to enjoy the space safely. But that’s just too messy. I’d rather see Beeliar Oval stay ‘on lead’ space and a new run area established nearby.</p> <p>Alternative ‘run’ area - The draft management plan talks about the best dog run areas being long and skinny. But how \$\$ they are to upkeep. I wanted to suggest an alternative location to Beeliar Oval. There is a long, skinny area really only used by dirt bike riders currently that runs between THOMPSONS LAKE and the WATERCORP RESERVOIR LAND up at the top of Beeliar/Meve where Mannikin Heights does a dogleg. This is outside the Regional Park and also outside Water Corps land. I assume it’s an old road reserve since it aligns with Lorimer. It is long and skinny and could mean people could run their dogs (or throw a ball) from Mannikin down to Lorimer. Because it is already fenced on either side, all it would need is a bit of fencing at top and bottom and some casual landscaping to make it a bit tidier. It’s not a grassed space but a bit of degraded bushland.</p> <p>To encourage more people to rehome their animals – Maybe there could be a rates rebate for anyone who registers their dogs/cats for life. So there’s a tangible \$\$ reward and not just the satisfaction of following rules.</p> <p>Subsidies for cat ‘enclosures’ – for the latter part of the plan, it would be great to see this extended not just to enclosures (‘cat runs’) but to ‘containment solutions’. For example I’ve just spent \$2000 on Oscillot spinning paddles for my fences so that my back yard can be (effectively) a contained cat run, and keep other cats out. I would love to have got a subsidy for that and not just a netted run. Or fence extensions. Or that kind of thing. Broaden definition basically.</p> <p>That’s it! Sorry for the unformed mind dump but it was a long document to read and I figure that the team working on it will know where my comments above belong.</p> | |
| <p>71. I am writing to express my opposition to the proposal currently being considered by the City of Cockburn to allow for ‘Dogs off-lead’ beach on the northern coast of Woodman Point.</p> <p>There already exists a ‘Dogs off-lead’ beach on the southern side of Woodman Point as well as numerous kilometres of ‘Dogs on-lead’ beaches around the southern coast of Woodman Point as well as an area on the northern coast. The Council’s Draft Animal Management and Exercise Plan for Community Consultation, page 11, indicates that community feedback shows that Woodman Point, despite its current ‘Dog off lead’ southern beach, is one of the least popular beach areas along the City’s coastline. One would question the need to provide more beaches with less restrictions in the area for dog owners when those that already exist are not well utilised.</p> <p>I furthermore wholeheartedly support the proposal to create a ‘Dogs Prohibited’ beach on the beach area adjacent to the Woodman Point Recreation Camp, especially during the Fairy Tern nesting season. Council should be doing everything possible to ensure that vulnerable wildlife within its jurisdiction can thrive in an environment free of threat</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support</p> |



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| | <p>and the potential harm that may be caused by dogs 'on-leash', and the absolute havoc that could be caused by dogs 'off-lead'.</p> <p>In addition, this area of beach is used extensively by the Woodman Point Recreation Camp for water activities and water training that form an integral part of the Camp's recreation program. The Camp is extremely well utilised by the community and especially by many schools throughout the school year. It would enhance the delivery of the Camp's activities should this stretch of beach adjacent to the camp be reclassified as 'Dogs Prohibited'.</p> <p>This is a pristine area of coastline with unique and vulnerable wildlife that should be protected at all costs. I urge Council to pass the proposal to make this stretch of beach 'Dog Prohibited'.</p> | |
| 72. | <p>I do not support the plan to allow dogs on leash or off leash to be in areas where migrating birds are landing and roosting and nesting in this important bird area There are so many places where people take dogs eg Bunnings</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 73. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 74. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 75. | <p>I was just alerted to possible changes to the dog management plans which will impact our residence. Initially I was told that freshwater reserve was being considered to be a fenced in dog park but upon inspection of the draft linked on your Facebook page it looks like it will be just an off lead area.</p> <p>I do have some concerns regarding the west section of grass here (I live at 18 paradise grove, with my western neighbour being this grass) as currently we already have a lot of off leash dogs coming up to our fence and stirring up both my dog and my back neighbours dogs (they are worse for barking) constantly, being home through COVID this has been especially noticeable. My concern lies in the fact that an off lead park would lead to more of this and the possibility of my young children being harassed/attacked through the fence. At this point I would likely need to put up a solid fence to counter this which I shouldn't have to do, it's something that we have discussed already due to the high volume of dogs that already harass Ollie behind the fence.</p> <p>I'm just wondering if there will be any provisions for such changes to assist in any transition, unfortunately only having two houses that directly border the park means it won't affect anyone else.</p> <p>I'm actually torn on this proposal, as a dog owner it would be great to have an off-lead park right there and more off lead spaces are certainly required. But as a parent of a 2yo and one due this month, the possibility of extra off lead dogs would reduce my desire to use the playground at times which is something we bought this property for. The concerns I have for dogs crossing the road and harassing other residents dogs or vice versa, which happens already does worry me as well.</p> | <p>Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Undecided</p> <p>All off-leash areas will be actively monitored by the City's Rangers and where trends start to emerge the Rangers will try to educate dog owners prior to taking enforcement action.</p> |
| 76. | <p>Regards the Dogs off Lead proposal for North Beach of Woodman Point.</p> <p>Perth Modern School among many other department schools have annual camps at the Woodman Point Recreation Camp in the City of Cockburn.</p> <p>I feel that a dog on lead would be appropriate middle ground and if I had to choose between the two proposals would go with Dogs Prohibited. As a school, we can not justify the risk of dog bite from dogs off lead and would have to cancel</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>all beach activities at the Camp. This would significantly affect the activities run and I am guessing for all schools that use the site. Considering there is an off lead spot in the area there would be no need to put another one in the high child use area.</p> <p>I do not support dogs on the beach South of the Ammunition Jetty at Woodman Point</p> | |
| 77. | <p>This is regarding the letter dated 5 August 2020 'Have your say: proposed dog off-leash area - Beeliar reserve'.</p> <p>I strongly object to this proposal. We do not wish to have the dogs off leash on this oval.</p> <p>I like to go for a walk or exercise on this oval. I had several incidents even now when the dogs are supposed to be on the leash. See some of them in the list below .</p> <ol style="list-style-type: none"> 1. While walking the scary looking dog came running towards me frantically barking. I was scared to death. The dog owner kept laughing. 2. Once, my husband reminded the dog owner when the dog came at pounding at me, the dog owner started abusing us and followed us insisting we should apologise and followed us for 500m to see where we live. 3. My son went for a jogging and the dog started running behind him and almost about to catch his foot. my son was very scared and has stopped jogging on the oval ever since then and jogs on the streets. That is ridiculous. 4. They DO NOT pick up the dog pooh. 5. One of my friend's little puppy (on the leash) was attached by the big dog (off leash) <p>Often the dog owners are very irresponsible and they insist that their dog is friendly. However I am not comfortable with dogs especially when they are above my knee height. We have called several times to council and complained, however the ranger cannot turn up in time. I do not wish to talk to the dog owners and get into arguments with them. Neither I want to wait until the dog bites me and I get into more trouble, or call police. There is a dog park just two lanes away and one on the hill. I have my right to enjoy the oval fearlessly. Hence I STRONGLY OBJECT to allow dogs off leash on this ground. Rather I would request council to have ranger more often on the oval and issue hefty fines if the dog is off leash.</p> | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beeliar Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> |
| 78. | <p>I Do NOT support dogs on Woodman's Point as they interfere with Birdlife. Especially Fairy Terns</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 79. | <p>As with all surveys all the questions asked are not always possible to answer, as I am handicaped I have to rely on Dog walkers as they are police officers who love the dogs I rely on them to comply with the rules, on one or two occasions one of the dogs normally exhausted from chasing a ball makes its home by its self just down the road, the only other time one of the dogs escaped due the fence being damaged by storm, but lucky for me he was near home and came home when I called him, of course they are chipped, desexed and registered. They both went though Puppy school and Rascal was requested by the school to come for extra visits as he was a good influence on the other dogs.</p> <p>With respect to the cats , Max the ragdoll spends his day either under a tree by the front door or in the back garden, the other one Scottie spends his time in the back garden as he could not jump the fence, on odd occasions he will walk along the fence top around the property I have seen Max do this once or twice, they do not catch birds but will stalk Doves in the back garden but never catch them, I believe Rascal may have caught one he is very fast., and I have found the odd chick on the ground which has fallen out of a nest. I think my cats are scared of heights if they do not have a</p> | <p>Submission does not imply support or opposition for specific Objective or Actions of the Animal Management and Exercise Plan</p> |



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| | roof under them. | |
| 80. | <p>The beach at Woodman Point is an important area for birds that migrate between the sub-Arctic circle where they breed and beaches such as Woodman Point where there are long standing records of these birds foraging and roosting during the summer months. These species are listed internationally as endangered and protected under legislation. This is also a beach where Fairy Tern, a species listed as threatened in Australia, are known to nest. They are particularly vulnerable to uncontrolled dogs. Dogs running up and down beaches disturbs birds and eventually they go and don't return.</p> <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 81. | <p>I am a dog owner. However I don't support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 82. | <p>I am concerned about the plan in your Draft Animal Management and Exercise Plan that proposes to turn most of Woodman Point beach into an off-leash area, meaning more beach access for dogs than people in a place where older people walk daily.</p> <p>I'm not a dog hater, but off-leash dogs completely change the nature of the beach, causing mess and potentially attacking people and destroying the nesting sites of shore birds that lay their eggs in the sand.</p> <p>There is already a huge off-leash area and that isn't used very much, so I am very confused as to why this would be made even larger. With this in mind, is it really necessary to create Australia's largest off-leash dog area at Woodman Point?</p> <p>As Australia battles COVID-19, with many Perth people separated from loved ones in the eastern states, it's important for not only the natural environment but also for people's mental health that we make space for quiet reflection and ensure the beach remains a haven for people of all ages.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 83. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty. That's a crazy idea, we need to protect areas where birds nest!</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 84. | <p>In response to your email invitation to comment on the draft animal management plan...</p> <p>Re cats, please bring them into line with dog rules on roaming, so fed up with domestic cats in our native garden, especially after dark, fighting, killing, peeing....they should not be tolerated outside of their home boundary at all, ever, as soon as possible. We are only one block from Bibra Lake reserve, and have had wild life establish in our garden only to 'disappear'. Cats do not belong in our environment or on our verges, we have high fences which does not deter their foulling of our vegie patch inside our yard.</p> <p>Re dogs in eastern Bibra Lake, please please make Mellor oval an off lead dog exercise area outside of school hours (as Blue Gum Montessori does use the oval) & sporting activities. It is the only good flat exercise area for dogs to romp in our area, & safe for elderly with no slopes, with short turf that doesnt hide tiger snakes which are a huge danger around the lake. The fact that much of our dog community already uses it for off lead dog exercising is indication enough of the need, & it is a great social focus for a great number of both dogs & considerate owners. I do support rangers in fining any dog not wearing a collar, which is a daily occurrence on the oval, and of course owners caught not</p> | <p>Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Support (Submission specifically supports Mellor Oval being used as an off-leash area)</p> <p>Objective: 3 Action: Draft a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2, and enact these laws Implied Sentiment: Support</p> |



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| | picking up their dog's excreta. | |
| 85. | I am writing this email in response to the Draft Animal Management & Exercise Plan stating that I do not support dogs on the beach at Woodman Point south of the ammunition jetty as this is a vital area for seabirds. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 86. | I want to register my opposition to the 'Dogs off-lead' proposal on Woodman Point beach. The proposal will have a negative impact on the water programs for camp and pose a safety risk. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 87. | Cats eat 5 million native animals per night in Australia (CSIRO info) Wildlife is more important than cats. As a cat owner I totally support any regulation that keeps cats indoors at all times. I also believe any cat caught outside should be put down. | Objective: 3 Action: Stage 2: (2023) Enact laws for cat control based on similar requirements for dogs (requiring cats to be retained within the owners property at all times). Implied Sentiment: Support |
| 88. | Personally I'm not a huge fan of non-enclosed off leash areas. Two main reasons- 1.some people are scared of dogs and having unleashed dogs running around would stop them being able to use the ovals for fitness activities. 2. There are a lot of dogs in the community who really don't react well to other dogs approaching them where are the people who own these dogs supposed to walk their dogs, most dog owners can't stop their dogs once they've decided to bound at another dog. I am however a huge fan of the traffic lights system I just think sporting reserves should be yellow. Being a dog owner myself its not a issue at all to take my dogs to an off lead area. I personally use the one in Honeywood. | Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Oppose Objective: 2 Action: Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas). Implied Sentiment: Support |
| 89. | Whilst I wish to object to the current increase to the 'off lead' area extension at Woodman Point Beach I have tried to submit this objection through the on line 'report a problem' and not succeeded – could you please forward this email to the appropriate council officer/area. I am a local resident based in Buchanan Rise, both my wife and I use the beach area at least once or twice a day most days of the week. I wish to object to the current proposal of increasing the dog 'off lead' area for the following reasons: I have witnessed in the past an aggressive owner refusing to put their dogs on a lead when asked to do so. I have seen children (and adults) shy away from running dogs off leads. I have not seen any policing of the situation of dogs 'off leads' in the 'on lead' area. It may be unrealistic to expect a 'Ranger' to attend to a current incident whilst it is happening due to being elsewhere in the city limits, so policing of incidents is very limited I believe. We have witnessed an increase in dog excrement in the past twelve months along the path from Cockburn Road to the | Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (However, submission calls for increase in patrols in permitted dog areas as well as dog prohibited areas) Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |



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| | <p>beach area, also an increase in the area of Fairbairn Road.</p> <p>I have witnessed someone throwing a yellow doggie bag into the bushes in the area of Cockburn Road with numerous left on the ground – this I have reported before through the council ‘on line’ system and had no follow up too.</p> <p>There are no Dog dirt bags or refuse points seen on my daily walk from Buchanan Rise all of the way to the beach area and none at all on the beach access points.</p> <p>This current proposal of increasing ‘off lead’ access is only going to exacerbate current issues and prohibit more families from enjoying the magnificent beach we should be enjoying without the worry of a dog running up to us and snapping or dog dirt on the beach.</p> | |
| 90. | <p>I do not support dogs on the beach South of the Ammunition Jetty at Woodman Point</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |
| 91. | <p>Woodman Point</p> <p>I am concerned regarding the proposal around Woodman point in relation to dog access. Due to the site being a fairy tern nesting habitat the area should be a no dog zone all year round not just for 6 months. Whilst its great to keep dogs out during nesting season the reality is people will still go there outside the 6 month exclusion. It is very secluded so it will be very hard to police. All it takes is one dog at one time to destroy a few nests and the already endangered birds fate becomes more dire. Not to mention impacts on other foreshore dwelling and nesting fauna. There are plenty of other areas for dogs to exercise in Cockburn but not anywhere else these birds can nest.</p> <p>Cat Laws</p> <p>I support the cat laws but feel stage 1 and 2 could be more combined. Still have a slow approach keeping cats out of natural areas first and then contain them to property but feel more incentives could take place at the same time.</p> <p>e.g. free cat rego for a year during the transition, subsidies for cat runs</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> <p>Objective: 3 Action: Draft (and then enact) a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2 Implied Sentiment: Support</p> |
| 92. | <p>Having read through the proposals for dogs and cats I am completely gobsmacked at the proposal to allow dogs off lead to an additional 5km of the beach from Ammo Jetty to the Woodman point boat ramp. Even at the current time, there is a daily issue with dogs uncontrolled running up to the beach users, sometimes in an aggressive manner. This part of the beach is used predominantly for exercise, walking, running, etc, and needs to be made dog free, not a dog playground and toilet. The beach at the northern end of Coogee gets so congested due to the Lifesaving club activities that the southern area has become the default exercise section predominantly by a more senior-aged demographic. There are currently little-used dog off-lead areas on beaches and no need for more, who is looking after the rights of the beach users who want a quiet walk or sit uninterrupted by uncontrolled dogs. I am sure that most dog owners don't want other aggressive dogs free to attack theirs, there are constant savage attacks on dog off lead beaches that are well reported on social media if you take the time to read. I would have hoped that you all as elected members can see this is an ill-thought-out proposal that needs to be revised</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |
| 93. | <p>We do not support any dogs on the beach at Woodman Point south of the ammunition jetty – on or off leads. There are too many dogs on beaches and they disturb the decreasing no. of shore and waterbirds that visit the area.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |



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| <p>94. To reiterate the comment I made on my survey, I would like to suggest the implementation of a 'badge system', whereby green badges are presented to dogs that are able to demonstrate basic obedience, such as:</p> <ul style="list-style-type: none"> • solid recall, even with distractions around • calm demeanour when off-lead • friendly towards other dogs/people <p>The test would need to be mandatory for all dog owners, and coordinated by the council - I'd happily pay a fee to participate.</p> <p>Whilst I understand this would be a huge administrative task for the City of Cockburn, I believe it would put you ahead of neighbouring councils in terms of a proactive approach to responsible dog ownership, as well as ensuring safer environments for dogs, people and wildlife.</p> <p>I am a member of Dogs West, and both my dogs are highly trained in Agility, Obedience and Tricks, yet I often avoid certain areas because of unruly dogs that have had no training. I am not referring to dog parks - these are easy enough for me to avoid, I am more concerned with beaches and parks/green areas where my dogs can roam freely without being fenced in.</p> <p>Whilst I commend your initiative to allow off-leash dogs on sporting fields/ovals when there are no sporting events taking place, I do think there needs to be something in place to protect well-trained dogs. There are so many uncontrolled dogs in our suburb, and I have seen too many fights that could have been prevented. My own dogs have narrowly escaped many unwanted confrontations, and my friend even broke her femur at C Y O'Connor beach after three dogs charged into her (they weren't aggressive, they were chasing each other, but didn't listen to the owner's recall).</p> <p>Most of these incidents could have been avoided with solid recall, but many owners don't have this trained skill before allowing their dog to be unleashed.</p> <p>Even if a dog is friendly and non-aggressive, it is still dangerous to allow it to be off-lead without a solid recall for a number of reasons, including:</p> <ul style="list-style-type: none"> • the dog could run into a road • the dog could run up to a reactive on-leash dog and get bitten • the dog could accidentally run into a baby/child and knock them over • the dog could run up to a person who is scared of dogs • the dog could chase wildlife <p>I understand you will be running an educational programme regarding responsible dog ownership, but I wonder if this suggested 'badge' system will go further and help society regulate itself? Owners who have been approved a green badge will hopefully proudly display their dog is trained enough to be safely off-lead, and the rest of us will know our dogs are going to be safely mixing with controlled dogs.</p> | <p>Note: Submission suggests alternative Action relating to Objective 1.</p> |
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| 95. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. I believe this should be retained as a wildlife only zone, for the sake of birds and other native specie endemic to the area. I am a coastal and marine science student at Curtin University, and I believe it is of the utmost importance that we keep this space pet free. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 96. | I wish to oppose amendment 17.1(1)(3)e - Dogs on Woodman Point Beach. "Dog owners Ignoring the current rules" is no excuse for changing the rules. Children and small dog owners use this area and the open slather approach for any size dog off lead creates unacceptable risk to other users and wildlife | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 97. | <p>I have been made aware the City of Cockburn are considering a proposal that would allow dogs to exercise off-lead along the north beach of Woodman Point – The beach used by Woodman Point Recreation Camp, myself and a number of children. I believe a Draft Animal Management and Exercise Plan is before the council recommending this stretch of beach be 'Dogs Prohibited', but one counsellor has proposed an alternate recommendation of 'dogs off-lead' which has been endorsed for public comment.</p> <p>A dog exercise beach already exists on the southern side of Woodman Point, a mere 200m from this beach at its closest point that I utilise myself and would see no need to endanger children and others in a highly trafficked area of the beach by allowing this area to continue further.</p> <p>I myself only a few months ago had been approached by a couple accessing through the JBSC grassed area with a large (I would guess at around 60kg) intimidating dog off the lead and running strait for me. The owners did not react and let their dog continue with complete disregard for others.</p> <p>I would hope that common sense prevails with regards to this matter and you see fit to continue providing safe areas that are utilised by small children, a large number of persons for walking/swimming and watercraft activities (that could potentially startle animals and cause them to react adversely).</p> | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 98. | An idea for the dog park could be time slots for private sessions. People can book a time slot (30min/1 hour) for just their dogs and friends if possible. For those with anxious/ not dog friendly dogs! Although some dogs are friendly they can become anxious and therefore become aggressive (fear aggression) this also puts the dog in a bad situation and dogs can be worked up for days. My two would love to play at a dog park but irresponsible owners and my girls anxiety prohibits this! Having a dog park that we can book a private session in would be awesome and help a lot! A small fee could be charged to ensure people do the right things and even if it's only run on weekends and weekdays are free for all! This may also help with the dog fights that can occur as people want to get their dogs out and off lead in a safe space but the dog struggles with other dogs or can be picky. My girl is very anxious and fear aggressive and although is friendly towards most other dogs if two dogs are playing she gets anxious and tries to break it up which can cause fights. Hopefully this is something that can be thought out about and put into action. We'd be going every weekend! | Note: Submission suggests alternative Action in relation to Object 2. |
| 99. | I would like you to know that I do not support dogs on the beach at Woodman Point, South off the Ammunition Jetty. As too many dogs have been seen chasing wildlife. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 100. | I use woodman point beach to exercise my dog at least 6 times a week, we need to keep this beach dog friendly and keep a section of the beach for off the lead exercising. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Support |
| 101. | I am writing to express my opposition to the proposal currently being considered by the City of Cockburn to allow for 'Dogs off-lead' beach on the northern coast of Woodman Point. There already exists a 'Dogs off-lead' beach on the southern side of Woodman Point as well as numerous kilometres of | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |



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| | <p>'Dogs on-lead' beaches around the southern coast of Woodman Point as well as an area on the northern coast. The Council's Draft Animal Management and Exercise Plan for Community Consultation, page 11, indicates that community feedback shows that Woodman Point, despite its current 'Dog off lead' southern beach, is one of the least popular beach areas along the City's coastline. One would question the need to provide more beaches with less restrictions in the area for dog owners when those that already exist are not well utilised.</p> <p>I furthermore wholeheartedly support the proposal to create a 'Dogs Prohibited' beach on the beach area adjacent to the Woodman Point Recreation Camp, especially during the Fairy Tern nesting season. Council should be doing everything possible to ensure that vulnerable wildlife within its jurisdiction can thrive in an environment free of threat and the potential harm that may be caused by dogs 'on-leash', and the absolute havoc that could be caused by dogs 'off-lead'.</p> <p>In addition, this area of beach is used extensively by the Woodman Point Recreation Camp for water activities and water training that form an integral part of the Camp's recreation program. The Camp is extremely well utilised by the community and especially by many schools throughout the school year. It would enhance the delivery of the Camp's activities should this stretch of beach adjacent to the camp be reclassified as 'Dogs Prohibited'.</p> <p>This is a pristine area of coastline with unique and vulnerable wildlife that should be protected at all costs. I urge Council to pass the proposal to make this stretch of beach 'Dog Prohibited'.</p> | <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support</p> |
| 102. | <p>Both my wife and I oppose the use of the beach areas north or south of the old ammunition jetty for unleashed dogs.</p> <p>We walk the beaches regularly and find the occasional unleashed dog can be intimidating when they run up - maybe it is just for a pat but who knows!</p> <p>A lot of dog owners are quite ignorant and do not care for other beach users.</p> <p>Love dogs but, not here!</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 103. | <p>I oppose the proposal to make Woodman's point beach an "off the lead" dog beach.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 104. | <p>You propably receive a lot of emails and I hardly write to the council, but the the animal management and exercise plan for 2020-2025 does concern me.</p> <p>I am surprised that the council does consider to open up more areas around Woodman Point for dogs off and on leash. I am a Cockburn resident living in Hamilton Hill. I am proud of the beautiful and tranquil beaches in my area. Me as a regular beach goer I feel very upset about many dog owners not behaving responsible about the rules along the beaches. Dogs are seen in areas where dogs are not allowed, dogs are off leash where they're supposed to be on leash, and some dog owners do not clean up after their dogs. When I dare to remind a dog owner I get insulted or ridiculed. To contact a ranger in that moment does not make sense because the dog and owner are long time gone. And I've never seen a ranger at the beach except during the covid 19 restrictions for beach goers.</p> <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>This is an important area for shorebirds such as Ruddy Turnstone, Grey Plover, Grey-tailed Tattler, Red-capped Plover to name a few. The area has also been identified to manage for Fairy Tern breeding.</p> <p>Dogs allowed off leash at Woodman Point will cause considerable disturbance to birds using the area and likely deter</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | beach nesting birds selecting the area to breed. | |
| 105. | Written comments Doc Set Id; 9665995 – I am against the proposal to make Woodman Point Beach dogs off lead. I am a dog owner. I enjoy taking him to Jervoise Bay off-lead. There is plenty of parking, toilets, even a café. I also enjoy taking my grandchildren fishing and birdwatching near Woodman Spit. Dogs that are supposed to be on lead often disturb and frighten us. We all have right to feel safe. Some owners are inconsiderate and abusive. This whole area should be prohibited to dogs. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose (Submission calls for the area to be dog prohibited) |
| 106. | Written comments doc set id; 9665994 – We are against the proposal to make Woodman Point Beach off lead for dogs. We used to enjoy walking, swimming, spotting crabs with our grandchildren. Unfortunately, during the last few year, dogs that are supposed to be on-lead have been racing around swimming, freely – poo is left on the beach. There are lots of young families in summer. It has become unsafe and a health hazard. This is an unfair and biased proposal. Dogs should be prohibited. There are already designated off-lead beaches – C Y O'Connor and Jervoise Bay. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose (Submission calls for the area to be dog prohibited) |
| 107. | Written comments doc set id 9665991 – Re-proposal to make Woodman Point Beach dogs off-lead. We are against this proposal. Council has done well in estimating the increase in the number of dogs, cats by 2025. However, they have omitted to estimate the increase in the number of PEOPLE. Coogee beach and surf club are crowded in summer. Families with young children use the beach on the southern side of Amm. Jetty. By 2025 there will be even more! This proposal would render this beach inaccessible. It's bad enough already with dogs that are supposed to be on-lease jumping, pooing freely. With the existing Jervoise Bay off-lead, this would create about 4.5kms of off-lead! Plus C Y O'Connor. This is inconsiderate and out of proportion. Are the rest of us to be restricted to the 2kms stretch between the jetty and Port Coogee? Woodman Point beach needs to be for PEOPLE and WILDLIFE only. Dogs prohibited. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose (Submission calls for the area to be dog prohibited) |
| 108. | Written comments doc set id; 9665990 – We strongly oppose the proposal to make Woodman Point Beach off-lead for dogs. We walk here regularly, more often in summer, as Coogee Beach and the surf club become very busy. Dogs (supposed to be on-lead) have jumped and lunged at us on numerous occasions. Is this "effective" control? It is frightening! Sometimes there are people with walk frames and wheelchairs near the jetty, also people with disabilities. We all have a right to enjoy the beach and feel safe. No dogs here please. There are already designated off-lead beaches on either side – Jervoise Bay and C.Y O'Connor. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 109. | I would also like to include my views on changes to the cat laws. I really love my cat and I really love the wetlands I visit most mornings. I watch the many bird activities around the lakes, especially the results of the breeding seasons. Many chicks, ducks, swans etc are beginning to emerge from their nest at the moment. I must say I have never seen any half eaten wildlife on my walks. This doesn't mean I don't believe there is some damage done by wild cats and foxes. I believe the solution to the problem is for Cockburn to fence all its wetlands to protect what is said to be destroyed by domestic and wild animals. The fences could have gate access for all who wish to enter the areas. This would provide a safe and happy habitat from foxes and wild cats. It would also take the pressure off the family cat which your proposed changes will create. Cats are very valued members of families and give a great deal more than they take. | Note: Submission suggests alternative action (fencing Conservation Reserves) in relation to Objective 3. |
| 110. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. Parks, on land which has already been cleared, are a much more appropriate place for exercising dogs. We allow our dog off lead to run and chase a ball, without disturbing other park users, but we would never allow our dog to chase the wildlife at our beach. Please show | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |

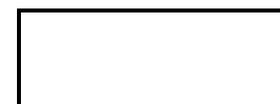
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| | respect for the needs of our precious breeding birds. | |
| 111. | I do not support the plan to allow dogs on the beach at Woodman Point south of ammunition jetty. It's important to conserve the area for bird breeding. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 112. | Please note that I do not support dogs on the beach at Woodman Point south of the ammunition jetty. I am both a dog owner and a bird observer and fear for the beach breeding and feeding birds of our coastlines. Especially in the breeding and migration season the birds are under increasing threats from disturbance and loss of feeding grounds. Surely having habitat within urban areas is something to be cherished. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 113. | I do not support the proposed unleashing of dogs on the beach at Woodman Point south of the ammunition jetty. This will cause considerable disturbance to birds using the area and likely deter beach nesting birds from selecting this area to breed. Thank you for the opportunity to comment. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 114. | I have been informed that the City of Cockburn May be thinking of allowing dogs to be let off their leash at Woodman point. I do not support dogs on the beach at Woodman Point south of the ammunition jetty. It is a vulnerable area for native wildlife with many rare birds nesting on the ground there and vulnerable to being killed or disturbed by dogs. It's so important in this world to allow areas of our landscape to be used by creatures that call these areas home. Who are we to disrupt them just for our leisure pursuits? Let us lead by example | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 115. | We feel that the "Dogs off-lead" proposal would interfere with the programs run at the Woodman Point on the beach. We feel that the "Dogs off-lead" proposal would make the Woodman Point beach an unsafe environment for children who use the campsite. We understand owners needed to walk to their dog through this area on a lead to the dog beach 200m metres away, but feel that for the safety of the children and the campsite, that the laws don't change. We are opposed to the 'Dogs off-lead' proposal on Woodman Point beach. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 116. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. This area is important to shorebirds and dogs will disrupt their breeding etc. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 117. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. I believe that dogs should not be allowed access at Woodman Point. This is an important area for shorebirds such as Ruddy Turnstones, Grey Plover, Grey-tailed Tattler, Red-capped Plover and others. Dogs off the leash will also disrupt and harm Fairy Tern breeding. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 118. | I would like to express my opposition to the proposed changes regarding dogs walking off leash at Woodman point. There is already a lot of disturbance at the Spit because of fishing people leaving their rubbish and just being there, if dogs are allowed off leash on the beach, there will be nowhere for birds to rest. Many species of waders and sea birds visit the area and it is extremely important in my opinion to protect them. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 119. | I understand that in the above plan the Council is proposing dogs are allowed off leash at Woodman Point. This will cause considerable disturbance to birds using the area and is likely deter beach nesting birds selecting the area to | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |



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| | <p>breed.</p> <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | |
| 120. | <p>"We do not support dogs on the beach at Woodman Point south of the ammunition jetty."</p> <p>Now that we are retired we love to travel around our wonderful state, spending time bush and beach walking and relaxing in bush settings; as bird lovers we enjoy the peace and quiet of just sitting watching birds in all settings. Nothing disturbs birds more than an animal 'on the loose'. It has been shown that birds use at least 6 times more energy when taking off in a panic than normal flight takeoff. Shore nesting birds are at continual risk but the presence of uncontrolled animals in their midst would be catastrophic, therefore we wish to add our voices against the proposal to allow unrestrained dogs at Woodman Point.</p> <p>It is stated that the purpose of the plan is to encourage responsible dog and cat ownership - responsibility involves keeping the animal under control, does it not?</p> <p>So we repeat "We do not support dogs on the beach at Woodman Point south of the ammunition jetty."</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 121. | <p>It goes without saying that dogs on parks and ovals where little athletics are being conducted either as training or competition can be very problematic. Firstly there are the very inconsiderate owners who don't clean up feacies left by the dogs with the obvious health and 'mess' risks. As well, many dogs tend to become excited and some can pose a biting or tripping risk to children exercising.</p> <p>This being said, I believe that our Clubs do not object to dogs being excercised at either of the two venues mentioned above with the appropriate control measures in place.</p> <p>For both of our venues the green traffic light system would be appropriate; that is Dogs allowed off-leash except when certain activities like sport are being undertaken when on-leash must be strictly exercised.</p> | <p>Objective: 2 Action: Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas). Implied Sentiment: Support</p> <p>Objective: 2 Action: Trial use of sporting reserves as off-leash areas when sporting fields are not in use. Implied Sentiment: Support (Dependent on traffic light system and support from sporting clubs)</p> |
| 122. | <p>I do not support Dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>This is such an important area for shorebirds who really are simply running out of coastal space to "feed up" over our warmer weather without disturbance. Fairy Terns are desperate for somewhere "safe" to breed.</p> <p>Please consider an alternate plan.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 123. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 124. | <p>I oppose the proposal for the extension of the dogs off lead zone on the Northern Side of the Woodman Point peninsula.</p> <p>The Woodman Point Beach is regularly frequented by clientele from the Woodman Point Recreation Camp. I spent a number of years working at the campsite as both instructor and Assistant Manager and believe that this proposal is not ecologically sustainable (given the sea birds and other wildlife in the area) and will have a significant negative impact on the current beach users from both the recreation camp and caravan park.</p> <p>The lack of direct beach access to this section of coastline will also prove problematic and the cost to council for the additional provision & maintenance of bins required for dog waste will also be significant.</p> <p>The City of Joondalup extended the Hillarys Beach Dog Exercise Area South of the Ern Halliday Recreation Camp in 2010 and over the past 10 years this change has significantly impacted the safe delivery of our beach and water programs for school and community groups at our site and I would urge the City of Cockburn to not follow the same path.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>For the extra 100 metres or so gained in Hillarys, we have since had incidences of:</p> <ul style="list-style-type: none"> - Dog menace to clientele. - Large dogs aggressively barking at young school children. - Dog owners confronting staff when we have tried to stop dogs peeing or jumping on school children (not all children like dogs). - Dogs grabbing equipment used for activities or clientele gear and running off. - Young children being taken off the beach visibly scared due to having a real fear of dogs. <p>Whilst I am a dog owner and understand the community benefits of such a facility, I appreciate that not all dog owners share a common, sensible approach to dealing with their dogs at the beach. We often have dog owners come to the camp office looking for dogs that clearly are not under control and have left the beach area.</p> <p>Subsequently, some groups have cancelled beach based activities at the Ern Halliday Recreation Camp directly related to the extension of the dog beach at Hillarys and I suspect this will also be the case at Woodman Point should the proposal be approved.</p> <p>Effectively, less children and groups will be outdoors activity recreating on the wonderful area at Woodman Point beach.</p> | |
| 125. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 126. | I do not support dogs on the beach at Woodman Point south of the ammunition jetty. The area is extremely important for feeding and nesting shorebirds. Dogs are predators and will destroy these nesting sites and scare off these birds. Please can you try to prioritise our dwindling wildlife before we have none left. Thank you. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 127. | <p>I strongly recommend that dogs should not be permitted in the Woodman Point area, so as to protect our native Fairy Terns and the remaining natural habitat there. I do not think a seasonal approach to dogs in this area will be followed by the public. As a resident and rate payer (and dog owner) in Cockburn I am very grateful to the Environment Team protecting our local reserves and native wildlife.</p> <p>I am looking forward to the fenced dog exercise area and endorse the stronger responsibilities placed on cat owners. I believe that there is no need to stage the cat management changes, as shown in the draft, as stage 2 is important to enact as soon as possible.</p> <p>Thanks for your excellent services in the conservation of our precious natural environment and the ecosystems within.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Review existing enclosed dog exercise areas and improve fencing, shade provision of sensory plants, agility equipment, waste disposal and drinking water stations. Implied Sentiment: Support</p> <p>Objective: 3 Action: Draft, and then enact, a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2. Implied Sentiment: Support</p> |
| 128. | Please note that I do not support dogs on the beach at Woodman Point south of the ammunition jetty. | Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose |
| 129. | <p>I am writing to express my deep concern and dissatisfaction with the Draft Animal Management and Exercise Plan 2020-2025. My main concern is the proposal to increase the amount of coastal dog off-lead exercise areas.</p> <p>I also have serious concerns about how decisions around increasing off-leash coastal dog exercise areas have been</p> | Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (Submission support this action given it is extended to dog permitted areas) |



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| <p>made to date. From the feedback I've received from Council officers and current and past Councillors, most feedback over the years has been against increasing the amount of dog-of-lead exercise areas around Woodman Point and Coogee Beach. So, I was stunned to see that the plan proposes to increase it even further.</p> <p>My concerns:</p> <ul style="list-style-type: none"> • The proposal would create more beach for dogs than people. The plan states that dog ownership in the city is increasing, with a projected number of 24,296 dogs by 2024. Well, the number of humans is also increasing with a projected population of 131,713 in 2024. Why are dogs more important than people? • Residents seem to think that they can become the owner of a large dog/s, and expect the City to provide them with ample off-lead exercise areas. We are living in a city – if people want this lifestyle, they should move to a regional or rural area. • Page 12 of the plan states that Woodman Point has a long history of being off-leash. This is absolutely false – it was only changed to off-leash in 2016. This decision was made with very little community consultation. • The plan states that, in 2019, officers undertook extensive consultation with members of the community, including animal owning and non-animal owning residents. My mum attended one of the two workshops. She was the only non-dog owner among 59 dog owners. How can important decisions be made based on such poor community consultation? Dog owners and non-dog owners views must be equally represented in order for an informed decision to be made. Why doesn't the summary of community consultation in the report present a breakdown of the proportion of dog owners and non-dog owners who participated? • The plan states that "community feedback has shown a strong desire to maintain dog access to the City's coastal areas and beaches". This is because the community consultation was skewed towards dog-owners. • The plan states that a complete report of the community consultation findings is in Appendix 1 but there is no Appendix 1. • Dogs destroy our natural environment as much as cats. The plan acknowledges that Woodman Point provides habitat for up to 26 species of shore birds, and on-leash dogs can do as much damage as on-leash dogs. The proposal to turn the beach into a "dog prohibited" area during Fairy Tern nesting season is absolutely ridiculous. Fairy Terns generally breed from October to February. Some populations are known to occupy a single area all year round. The City cannot even enforce on-leash areas, so how will it keep dogs entirely out of a secluded beach for part of the year? Also, the plan acknowledges that people are confused about which rules apply where. • For the past four years, dog owners have been openly flouting the rules at Woodman Point and Coogee Beach. From just 20 to 24 December 2019 (around four hours in total), I observed a total of 31 dogs off-lead in the on-lead area. Many of these dogs ran straight up to me and jumped on me. See the attached collage. I was viciously attacked by a 'nice' dog several years ago. So it is very difficult for me to feel safe to use the beach south of Ammunition Jetty. When I politely request that people put their dog on a leash, most of the time I get an aggressive response. • The most important theme from the community consultation was actually "better enforcement and ranger presence". So it seems that everyone, including dog owners, recognise that there are many irresponsible dog owners out there and that it is extremely difficult for officers to effectively patrol this area of beach. The plan even acknowledges the impracticality of enforcement on the beach. • Why wasn't a question asked in the previous community consultation around no increases to coastal off-leash exercise areas? • Only 23% of dog owners and 17% of non-dog owners feel there is sufficient promotion and provision of dog obedience training. Why should the City provide dog obedience training? This is the responsibility of dog owners. If you can't afford this, then you shouldn't be able to afford food, veterinary bills and other dog-related expenses. • Trialling the increased allocation of off-leash dog exercise area in Quarter 3 2020 is ridiculous. This is a very quiet time with very few humans or dogs. • Its proposed that the plan will be reviewed in five years. The plan proposes very significant changes to amenity | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Provide clearer delineation markers between the dog areas zoned along the coastline. Implied Sentiment: Support</p> <p>Objective: 2 Action: Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas). Implied Sentiment: Support</p> |
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| | <p>and access, and so should be reviewed annually.</p> <p>What I support:</p> <ul style="list-style-type: none"> • The traffic light system and clearer signage. • Increasing the City's casual pool of rangers to patrol dog prohibited areas proactively. <p>Council officers, including those in the compliance and environmental services departments, are strongly opposed to the proposed increase to off-leash dog exercise areas for all of the reasons I've outlined in this email. These officers are on the ground delivering community services, monitoring our biodiversity and talking with residents. Surely their views should be considered as well? It is extremely frustrating when Councillors do not consider the views of staff (I'm speaking from experience). Also, staff from the State Government's Parks and Wildlife Services are also scathing of the proposal, as well as the 2016 decision to increase the amount of off-leash area. Local government authorities need to work with State Government to successfully protect our biodiversity.</p> <p>What I would like to see:</p> <ul style="list-style-type: none"> • The amount of off-leash dog exercise areas to be reduced from what is currently available. I think that Jervoise Bay should be the only off-leash and the area south of Woodman Point to revert back to a dog prohibited area. <p>Prior to 2016, Jervoise Bay was a three-kilometre off-leash dog exercise area. It is a suitable off-leash area as it is naturally contained by the groynes. I always remember it to be quiet. That's why it was nonsensical for Council to increase it in 2016.</p> <p>The beach has to be an off-leash exercise area or a dogs prohibited area - there is no in-between. Dog owners will always flout the rules and it is impossible to effectively patrol this beach.</p> <p>In south-east Melbourne, there are six dog-friendly beaches from Port Melbourne in the north to Mentone in the south. These six sites service a total of 355,603 people (combined population of the Cities of Port Phillip, Bayside, Kingston). These areas are small and dogs are only allowed off-lead at certain times of the day. So, Jervoise Bay and CY O'Connor Beach provide a huge amount of off-leash dog area and much more than many other parts of Australia.</p> <p>Since 2016, Council's decision-making processes around this issue has been very questionable. There has been a gradual increase in off-leash dog exercise areas with poor community consultation methods and a lack of transparency around decision making processes. You guys are meant to be listening to the community. What is going on here??</p> <p>I would like to have a response from each of you.</p> <p>I am considering making a Freedom for Information request to understand how the decisions around off-leash coastal dog exercise areas at the City of Cockburn are being made.</p> | |
| 130. | <p>Please, please, please ban dogs from the beach at the Woodman Point Ammunition Jetty,</p> <p>The irresponsible owners are more the problem, not the dogs, but you must ban dogs to protect wildlife and the pristine beach and dunes.</p> <p>Many thanks for the opportunity to voice this and we hope you will listen to the quiet majority who do not want dogs on an important Regional Park.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support (Submission proposes dogs be banned from Ammunition Jetty)</p> |



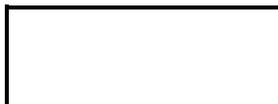
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| 131. | <p>I do NOT support dogs on the beach At Woodman Point South of the Ammunition jetty. Please think of our endangered birds.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 132. | <p>I am a frequent visitor to Woodman Point, to walk and to watch the variety of birdlife. I do not support the current proposal for allowing unleashed dogs at Woodman Point. This location is an important area for many shorebirds, as well as nesting Fairy Terns. The proposal states that there will be temporary signage and prohibition during the breeding season but I feel that that will probably be ignored by many people who are used to taking their dogs onto the beach at other times of the year. The current situation is already abused, I have been there many times when owners have dogs off the leash and have been chased down the beach by a large dog on one occasion.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 133. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty. This beach is used by migratory waders such as Grey Plovers, Ruddy Turnstones for foraging. It is also used by Red-capped Plovers and is a potential nesting site for these small beach nesting birds. Fairy Terns use the beach for resting in between foraging flights. The beach is a potential nesting site for Fairy Terns if dogs are prohibited from this beach.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 134. | <p>As per our letter to council on 9 July 2020 regarding the recommendations for 17.1 Proposed Animal Management and Exercise Plan 2020-2025 the wish to reiterate that the WPRPCAC gave a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 as it is consistent with the Woodman Point Regional Park Management Plan and the presence of dogs has been known to negatively impact the wildlife (i.e. nesting fairy terns) as well as children's activities at the Woodman Point Recreation Centre.</p> <p>The Woodman Point Beach must be returned to a dog prohibited area for the following reasons:</p> <ul style="list-style-type: none"> • The Woodman Point Regional Park Management Plan (WPRPMP) was endorsed by the City of Cockburn • The WPRPMP was gazetted in 2010 • page 26 of the WPRPMP states: "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park. Dogs are not permitted on the grassed areas of John Graham Recreation Reserve because they may cause a nuisance to visitors." • There already exists a dog exercise area on the eastern end of the beach adjacent to the boat launching precinct. • There have been many incidents of the children at the Woodman Point Recreation camp being negatively impacted by dogs off lead on the beach • The consultant hired by City of Cockburn undertook expert work and provided sound recommendations for why this area needs to have the dogs removed • The area was made a dogs-on-lead area in error by council and advertised, giving false expectations and now that error is being further exacerbated • Woodman Point contains a nature reserve and conservation park vested in the Conservation and Parks Commission • The city of Cockburn manages the beaches adjoining the park. • It is correct to say that on-leash dog beaches simply do not work • The reasons given for the council decision are flawed. The beach should never have been advertised as allowing dogs on-lead as this is against the management plan. It was a dog prohibited beach for much longer than it has been dogs | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support (Submission proposes dogs be banned from Ammunition Jetty)</p> |



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| | <p>on-lead. The children from the Woodman Point Recreation Centre have used this beach for much longer than dog walkers and more frequently and in larger numbers. The children do not have alternative areas to go but the dog walkers do. The main reason for removing dogs is much broader and based in a gazetted management plan endorsed by the City of Cockburn, then just for nesting Fairy Terns. It is already demonstrated that once dog walkers get access to an area, they refuse to give it up, so requesting they restrict their use during Fairy Tern nesting season will be an abysmal failure.</p> <p>For all these reasons, the WPRPCAC give a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 and completely reject the alternative recommendation 17.1 (1) (3) e.</p> | |
| | <p>I wish to comment on the suggestion to change Woodman point Beach from "on-leash" to "off leash" for dog exercise.</p> <ul style="list-style-type: none"> ➤ Dog's behavior off-leash can be unpredictable. They become boisterous and run at or jump on children and older walkers. Walkers and families enjoying the beach should be free to walk comfortably and safely. ➤ Woodman Point is a nature reserve – dogs tend to run in and out of the sand dunes disturbing fauna and flora. Their owners do not have effective control and often ignore the dog's toileting, neglecting to pick up the poo. ➤ There are numerous exercise areas for dogs in Cockburn, including a long stretch of beach north of the Coogee Marina and power station and a strip on the southern part of Woodman Point – Jervoise Bay. ➤ Signatures below of members of group who do not support the changes to "off leash" | |
| <p>135.</p> | <p>In addition to my completed survey online I would like to add the following comments;</p> <ul style="list-style-type: none"> • If the Council plans to increase the number of areas where dogs are allowed to be "off-leash", then the areas that are zoned "on-leash" need to be more obvious, or there needs to be stricter/harsher penalties enforced to those that don't adhere to the "on-leash" signage. I propose the following in "on-leash" zones: <ul style="list-style-type: none"> o Increased 'obvious' signage ☒ many areas zoned "on-leash" have minimal signage and are rarely adhered to (such as Bakers Square and Dixon Reserve inside fenced playground, Hanlon reserve park in playground) o Signage to include "penalties up to \$x" o Fencing around children's play equipment o Increased presence of Rangers in "on-leash" areas (as proposed) • I also feel that the Council could improve owner accountability and education by implementing the following; <ul style="list-style-type: none"> o enforcing fines on owners who do not adhere to signage o include information to pet owners on owner responsibility and penalties in local paper and Cockburn Newsletter <p>I am a local home owner/resident with three small children. We live on a small rear-strata lot with a small outdoor</p> | <p>Objective: 1 Action: Increase the City's casual pool of Rangers to proactively patrol dog prohibited areas. Implied Sentiment: Support (Given that dog permitted areas also see an increase in patrols)</p> <p>Objective: 2 Action: Provide clearer delineation markers between the dog areas zoned along the coastline. Implied Sentiment: Support</p> |



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| | <p>space. Our local parks are a place where we enjoy exercise, playing together as a family, socialising with friends and other community members, and watching the local birdlife. We choose to take our children to parks that are "on-leash" or "dogs prohibited" zoned for health and safety reasons. While my husband and I love dogs, we do not own a dog and therefore our children are apprehensive around them. We are constantly educating our kids about safety around animals (domestic or otherwise).</p> <p>We wish dog owners would show the same courtesy toward young families, however we often find owners not adhering to signage at "on-leash" or "dogs prohibited" parks and playgrounds. When I have expressed my concern to dog owners, I generally get the following responses; "our dog is fine with kids" or "I didn't know there was a sign", or "I don't have any bags with me" (when not picking up after their dog). My husband walks home from the bus stop every day via Bakers Square and every day he sees a dog owner not adhering to the "on-leash" signage. He has asked numerous times for dog owners to pick-up after their dog when it has been to the toilet in the sand at the playground.</p> <p>I definitely feel that dog owners should have the right to exercise their dogs off-leash. However, I feel the "on-leash" and "dogs prohibited" areas need to be adhered to, to ensure the safety and wellbeing of young families.</p> | |
| <p>136.</p> | <p>I am a dog owner and love taking my good boi out for walks in parks and at the beach. However, when it comes to dogs, beach and nesting birds, me and my boy can stay away; we have other exercise options; shore birds don't have other nesting options.</p> <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>I urge this council to make a decision that considers the ability of humans to be able to go to other exercise areas and shore birds to nest and breed in areas that are not specific to their survival. Please protect shore birds for the future and do not allow dog on the beach at Woodman Point south of the ammunition jetty.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| <p>137.</p> | <p>I do not support dogs being allowed on Woodman Point Beach. This area must be designated as "dog prohibited" to protect the wildlife and the people in the area. That is what the Woodman Point Regional Park management plan supports.</p> <p>The City of Cockburn says the objective of the Animal Management Plan is for safe spaces for pets, people and nature. Woodman Point is important for nature and people, pets can exercise in many places. Fairy Terns don't have other places to go.</p> <p>Please be a responsible Council and do the right thing, not the popular thing.</p> | <p>Objective: 2 Action: Provide clearer delineation markers between the dog areas zoned along the coastline. Implied Sentiment: Support</p> |
| <p>138.</p> | <p>I am a City of Cockburn rate payer and pet owner.</p> <p>I have two cats and two dogs.</p> <p>I am happy that CoC is providing more dog exercise opportunities, but I am very disappointed about the cat restrictions.</p> <p>Implementing such change is significant and will cause a lot of people a great deal of stress, and in some cases financial hard ship.</p> <p>People are already having to deal with financial pressures due to Covid.</p> <p>I hope CoC are basing their decision on research, rather than a persons personal beliefs. I tend to think it is the later as</p> | <p>Objective: 3 Action: Draft, and then enact, a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2. Implied Sentiment: Oppose3</p> <p>Note: Submission supports dog-related actions under the Animal Management and Exercise Plan</p> |



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| | <p>no other council has similar laws, where domestic cats have to remain on premise 24/7.</p> <p>What studies has CoC conducted and what information has been referred to come to such a conclusion that cats should be kept within the confines of a property.</p> <p>Have these findings been made public, if so where can I obtain a copy.</p> <p>What makes CoC so different to other councils?</p> <p>CoC encourages residential and commercial development to boost their bank balance, but these decisions have irreparable long term effects on the local wildlife.</p> <p>By allowing such developments the natural habitats and wildlife that the proposed laws are trying to protect are being decimated. So to compare the impacts of development and the effect domestic cats have on the natural environment is laughable and not even comparable.</p> <p>Maybe CoC should cut back on residential and commercial development if they have real concerns about nature.</p> <p>What conflict of interest measures have been put in place to ensure council members and staff don't benefit from the introduction of such laws.</p> <p>How can you reassure your constituents that council members or staff wont profit or gain a benefit directly or indirectly as a result of the cat laws being introduced, through cat enclosure / fencing services?</p> <p>Then there is the planning issue. What rules is CoC going to impose when building these enclosures/structures. Are people going to have to pay to go through the planning process etc?</p> <p>I believe you would receive more support for the laws if cats still had freedom to move around during the day, but were prohibited from entering nature reserves and were subject to a night time curfew.</p> | |
| 139. | <p>"I do not support dogs on the beach at Woodman Point south of the ammunition jetty."</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 140. | <p>Thank you for the opportunity to comment on the City of Cockburn's (the city) proposed Animal Management and Exercise Plan. The Department of Biodiversity, Conservation and Attractions (DBCA) has reviewed the plan and acknowledges the work the city is taking in managing domestic animals in their jurisdiction. DBCA commends the city's objectives in relation to the management of cats, including the review of the cat laws and the introduction of cat prohibited areas. This will have a positive impact and provide protection to the conservation values of reserves. The staged approach that is proposed is well considered and will provide the community with opportunity to adapt. In relation to the management of dogs, DBCA does hold concerns with the proposed changes to the management of dogs along the beach at Woodman Point.</p> <p>Woodman Point Regional Park (the park) consists of multiple reserves managed by a number of State government agencies and the City of Cockburn. A majority of the land</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| <p>tenure within the regional park is managed by DBCA as Nature Reserve and Conservation Park for the protection of the biodiversity and recreation values. The Conservation Park, extends to the high water mark, with the remaining beach managed by the city. This boundary is not delineated and there may be impacts on the values of the Conservation Park from the proposed off-leash dog area.</p> <p>In 2016, DBCA provided comments to the city expressing concerns with the proposal to allow dogs on-leash along the beach adjoining the park, noting likely impacts to wildlife and conflicts with user groups. In addition, it was highlighted that the Woodman Point Regional Park Management Plan 2010 states that "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park." The management plan was endorsed by the city and represents the commitment from all stakeholders in managing the park cooperatively. DBCA's views were shared by other key stakeholders in the park such as the Department of Local Government, Sport and Cultural Industries who manage the recreation camp within the park. These views were not reflected in the Council's decision to designate the beach as on-leash dog area.</p> <p>In early 2020, the city engaged a consultant to undertake a review and develop an Animal Management Plan for the city's jurisdiction. Given the majority of the land within the park is managed by DBCA, there was opportunity for DBCA to provide comment, along with the other key stakeholders in the park with a vested interest. DBCA's understanding is the plan that was presented to the city recommended the beach area along the northern extent of the park be designated as a dog prohibited area. The draft provided for comment does not reflect this understanding.</p> <p>The current designated off-leash exercise area along the southern side of Woodman Point View is supported by DBCA to remain. This location provides a suitable and sufficient area to maintain as the dog off-leash area as it is located away from the Nature Reserve and the foreshore that supports the migratory and shorebird population. This area is also not near to any major recreation sites with minimal potential conflicts with other user groups.</p> <p>The beach within (and adjoining) the Conservation Park includes nesting and roosting areas for Australian Fairy Tern (<i>Sternula nereis nereis</i>) a threatened species listed with a rank of Vulnerable under the State Biodiversity Conservation Act 2016 and the Commonwealth Environment Protection and Biodiversity Conservation Act 1999. One of the main threats identified for the Australian Fairy Tern is the disturbance to breeding (and roosting), and loss of breeding habitat as the result of disturbance by humans and domestic pets.</p> <p>In addition, there are several other threatened and migratory shorebirds species that are also likely to be impacted from the proposed change in this draft plan. This includes Red-Capped Plovers and Oystercatchers, numerous shorebird species such as Ruddy Turnstones, Grey Plovers, and the Grey-Tailed Tattler. Other listed species such as the Critically Endangered Great Knot (<i>Calidris tenuirostris</i>) and Curlew Sandpiper (<i>Calidris ferruginea</i>), the Endangered Red Knot (<i>Calidris canutus</i>) and Lesser Sand Plover (<i>Charadrius mongolus</i>) and the Vulnerable Greater Sand Plover (<i>Charadrius leschenaultii</i>) have also been recorded at Woodman Point.</p> <p>In addition to the likely impacts to wildlife and other conservation values of the park, conflicts between user groups that recreate in the park are important consider. John Graham Reserve, part of Woodman Point Regional Park, attracts</p> | |
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| | <p>more than 600,000 visitors per annum for the purpose of recreation, including picnicking and exercising. John Graham Reserve is currently a dog prohibited area and the proposed change may result in large numbers of dog owners seeking to use the carparks and facilities at the reserve to access the beach causing ongoing management and enforcement issues for DBCA and detract from the enjoyment of the reserve.</p> <p>DBCA does not support the proposal in the draft Animal Management and Exercise Plan for the beach area adjacent to Woodman Point Regional Park be changed from an on-leash area to off-leash area. DBCA considers it more appropriate to make this area 'dogs prohibited' as previously advised and recommended in the draft Animal Management Plan developed by the consultant and recommended to Council by the City Administration. This recommendation is aligned with the park's management plan and DBCA's management of the adjacent area.</p> <p>DBCA will continue working with the City and other interested stakeholders to ensure that Woodman Point Regional Park continues to support and protect the State's significant biodiversity values and provide a range of recreational opportunities</p> | |
| <p>141.</p> | <p>I would like to put forward our disapproval to the Dogs-off-leads on the Woodmans Point Beach. As we use the beach all through summer as a member of the Jervoice Bay Yacht Club we would like to see it as a "DOG FREE BEACH" at least between the holiday camp area to the cement works jetty all year-round. There is nothing worse than finding a spot on the best beach in the metro area and finding dog poo on the sand or dogs running out of control up and down the beach and the owners don't care and NO rangers to police the situation. We have been going to the club now for 5 years and have never seen a ranger at this beach area.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| <p>142.</p> | <p>I endorse the proposed CoC cat laws. The millions of native animals murdered every year is well known. This is a welcome move to protect the parks in CoC.</p> | <p>Objective: 3 Action: Draft, and then enact, a proposed Cat Control Local Law to prohibit cats in Accordance with Stage 1 and 2. Implied Sentiment: Support</p> |
| <p>143.</p> | <p>I have recently been made aware the Cockburn Council is revising their Animal Management Plan with the possibility of allowing dogs off the leash to exercise at Woodman Point.</p> <p>This same area has also been identified as a breeding site for the Fairy Terns. This species of bird nests in the sand and their breeding will be put at great risk if dogs and people use this area. As quoted on the website of the Australian Government Department of Agriculture, Water and Environment The Fairy Tern is already listed as</p> <p>Listed as Endangered (National Parks and Wildlife Act 1972 (South Australia): January 2020 list) as <i>Sternula nereis nereis</i></p> <p>TAS: Listed as Vulnerable (Threatened Species Protection Act 1995 (Tasmania): June 2019 list) as <i>Sternula nereis nereis</i></p> <p>VIC: Listed as Threatened (Flora and Fauna Guarantee Act 1988 (Victoria): November 2019 list) as <i>Sterna nereis nereis</i></p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>WA: Listed as Vulnerable (Biodiversity Conservation Act 2016 (Western Australia): September 2018 list) as <i>Sternula nereis nereis</i></p> <p>I request the Cockburn Council to continue with their Environmental responsibility and consider the detrimental impact to our birdlife by allowing dogs off the leash.</p> <p>This will cause considerable disturbance to birds using the area and likely deter beach nesting birds selecting the area to breed.</p> <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty.</p> <p>This area is also an important area for other shorebirds such as Ruddy Turnstones, Grey Plover, Grey-tailed Tattler, Red-capped Plover to name a few.</p> | |
| 144. | <p>To me it seems a fair and reasonable plan with simple to follow guidelines, especially the traffic light system.</p> <p>Our club uses the Macfaull Park Disc Golf course, which I realise is not a designated sporting reserve, for events and we happily co-exist with the many dog walkers who use the park. The "unclaimed" dog droppings issue is certainly noticeable at MacFaull Park and at times can cause problems for disc golfers. I think this problem is not specific to this park though.</p> <p>We have a policy of no dogs during bigger tournaments, that is players can't bring their own dogs. For our league days at MacFaull Park we notice the many dogs around but haven't had a serious issue that I recall.</p> <p>I always stress to the players that it's a shared facility and that we need to be aware and respectful of other users. We don't throw if there is any prospect of a dangerous situation.</p> | <p>Objective: 2 Action: Implement a traffic light style signage system for dog designated areas (red - dogs prohibited, amber - dogs on lead and green - dog exercise areas). Implied Sentiment: Support</p> |
| 145. | <p>I support the plan for a total off the leash for dogs as designated from the Southern /eastern side of Woodsman Point-up to and around the Western Groin,breakwater.</p> <p>I am a long term resident of Cockburn and have taken my dogs regularly to this area for over 25 years.</p> <p>With the councils supporting and providing doggy bags and disposal bins it makes for a very dog and people friendly beach area.</p> <p>In general the regular walkers take pride in our beach and pick up an enormous amount of drift litter. Plastics, glass, abandoned fishing gear,and whatever else has found its way into the ocean.</p> <p>This also assists with managing water contamination While these are small contributions, it must go some way in protecting sea life and keeping our Woodies beach a much nicer place to walk our dogs and maintain community pride and fellowship.</p> | <p>Objective : 1 Action: Review and improve access to dog waste stations (bags and bins). Implied Sentiment: Support</p> <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Support</p> |
| 146. | <p>Whilst I support the plan in general, I have significant concerns regarding the area near Ammunition Jetty at Woodman Point being identified as an off-lead dog area.</p> <p>The Woodman Point Regional Park contains a wide variety of ecosystems including tracts of undisturbed coastal vegetation that support local and migratory birds. A total of 93 species have been recorded at Woodman Point. Of these, 36 species are shore-birds and other waterbirds including the Australian Fairy Tern which is a threatened species.</p> <p>Introducing an off-lead area for dogs at Woodman Point would not only compromise remaining vegetation that is crucial habitat for these species but it will likely result in ongoing predation of our precious wildlife.</p> <p>Western Australia's biodiversity and wildlife are already declining at alarming rates therefore I hope more consideration is given to this matter.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Note: Submission supports most of the Plan, with the exception of the Woodman Point changes.</p> |



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| <p>147.</p> | <p>As per our letter to council on 9 July 2020 regarding the recommendations for 17.1 Proposed Animal Management and Exercise Plan 2020-2025 we wish to reiterate that the Woodman Point Regional Park Community Advisory Committee (WPRPCAC) gave a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 as it is consistent with the Woodman Point Regional Park Management Plan 2010 (WPRPMP) and the presence of dogs has been known to negatively impact wildlife (i.e. nesting fairy terns and other migratory birds) as well as conflict with children's activities when using the beach at the Woodman Point Recreation Centre.</p> <p>The WPRPCAC strongly believes the Woodman Point Beach south of the ammunition jetty should be returned to a dog prohibited area for the following reasons:</p> <ul style="list-style-type: none"> • The current proposal is inconsistent with the WPRPMP which was endorsed by the City of Cockburn prior to its gazettal in 2010. • Page 26 of the WPRPMP states: "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park. Dogs are not permitted on the grassed areas of John Graham Recreation Reserve because they may cause a nuisance to visitors." • Domestic animals including dogs have been known to negatively impact on nesting Australian Fairy Terns at Woodman Point, and there are many other migratory birds and shorebirds that will be impacted by dogs off lead. • There already exists a dog exercise area on the eastern end of the beach adjacent to the boat launching precinct, which is adequate in size and does not compete or generate conflict with other user groups. • There have been many incidents of the children at the Woodman Point Recreation camp being negatively impacted by dogs off lead on the beach, despite the area currently zoned as dogs on lead. • The consultant hired by City of Cockburn to develop the animal management plan undertook expert work and provided sound recommendations for why the beach to the south of the ammunition jetty should be dogs prohibited. • The area was made a dogs-on-lead area in error by council and advertised, giving false expectations and now that error is being further exacerbated • Woodman Point contains a nature reserve and conservation park vested in the | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> <p>Objective: 2 Action: Assess possible restrictions of dogs in City managed conservation reserves. Implied Sentiment: Support (Submission calls for Woodman Point to be dog prohibited)</p> |
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| | <p>Conservation and Parks Commission and dogs off lead can easily stray into these areas disturbing wildlife.</p> <ul style="list-style-type: none"> • The children from the Woodman Point Recreation Centre have used this beach for many years and do not have alternative areas to go. They should have the right to access the beach adjacent to the camp without being harassed by uncontrolled dogs. • It is already demonstrated that once dog walkers get access to an area, they refuse to give it up, so requesting they restrict their use during Fairy Tern nesting season will not be practicable. <p>For all these reasons, the WPRPCAC give a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 and strongly disagree with the alternative recommendation 17.1 (1) (3) e.</p> |
| <p>148. Support of and concerns around the Animal Management Plan</p> <p>This plan is framed as an “Animal Management Plan”, that is one that intends to address past resident complaints through a long-term future control strategy. This itself is concerning. Given that circumstances, community needs, ecologies, community perceptions, and demographics are constantly on the move, to set in a five year model that is both ‘complaint based’ and not therefore constructive, or appreciation based, and to set in one that refers to a handful of narratives, is to fail to include and respect the changing geography of animals, people and society.</p> <p>I feel overall the tone of this report could benefit from less messages and proposals that are punitive, policing, and restrictive. “Specific control measures, “enforcement’ ‘controlling dogs and cats’, ‘community safety’ ‘threats posed by pet species’ ‘proactive enforcement patrols’ to one that is honouring, inclusive and representative of the role our furry family have in our lives. Our community is a shared one, shared by humans, dogs and other species.. Dogs are known as ‘man’s’ best friend and are important and loved members of our family, they are not menaces or threats, to be targeted by the city.</p> <p>Dogs and Cats</p> <p>It is problematic that this report proposes to address ostensible issues of ‘dog’ and ‘cat’ management under the one umbrella since dogs and cats do not belong in the same conversation. Domestic cats whilst also loved by their owners, are a predatory species, that regularly are seen across the city, roaming the streets at nights, and in other people’s gardens and front and back yards. They kill inordinate numbers of local bird species and lizards on a daily basis. They are not otherwise walked and do not in this way form part of the broader social landscape.</p> <p>Dogs display none of these qualities, they do not roam the streets at night, are not to be found in other people’s gardens or yards, and only in exceptional circumstances do they harm birds or lizards. They are generally deeply loved family members, are vital companion animals and a joy to the wider community, most of whom appreciate and love dogs.</p> <p>In addition, and ironically, whilst this report sets out to detail animal management of cats and dogs, there seems to be an over-emphasis on dogs and little to no mention of domestic cats in the entire report, except for an occasional mention of feral cats, with the proposition of trapping in conservation areas (agreed, good idea). Domestic cats, their suburban behaviour, their movement patterns, predation patterns, and impact on wildlife are not mentioned in this report or in the Catalyse survey and results. It is apparent in fact that no questions were asked in the community survey</p> | <p>Note: Submission does not directly express support for or against specific actions of the plan. Submission does express support for dogs to continue to be permitted to Woodman Point and supports more education opportunities (regarding dog behaviour and conservation).</p> |



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| <p>regarding cat behaviour and every question asked, of the 373 who chose to respond (out of a random selection of 2000) and the 226 who were solicited by the authors to respond, referred to dog presence and behaviour.</p> <p>In addition, no questions were asked that invited the survey respondents to reflect or comment on the value dogs bring to the community, to their owners and ways in which we can care for and foster a positive dog culture, and educate the public about the significance of dogs, the value of dogs, and the ways in which we (humans) can be conscious of dogs needs in a shared space and thus develop a co-literacy surrounding the space that we share with other animals. Important considerations on this front are things such as people understanding to not come too close to dogs on scooters, skateboards and bikes, (more of an issue now with electric bikes), as this frightens and alarms nearly every dog. Nearly all the questions asked were loaded towards soliciting feedback about more controls and restrictions on dogs. In terms of the questions posed this seemed to be a survey designed to solicit monitoring and punitive action towards dogs. However, most respondents fortunately did and do not agree with the need for or desirability of such actions.</p> <p>There is also no evidence to suggest that dogs are a threat to community safety. For most dog owners and most of the public, they are the opposite, a source of safety, companionship and joy. I know this, as I walk my dog every day and he is now 10 years old. In all that time, he has brought countless amounts of joy to others on our walks, and it is only the rare (and often vocal) individual you meet, who does not like dogs. Most people in Australian society are dog lovers.</p> <p>The Emery report refers to national and international research, as a broad sweeping statement, and South Australia in particular. This is surprising as in SA dogs are allowed on every beach. Across the rest of the world, particularly in the UK, Europe and America, dogs are recognised and respected as a vital part of the community and of family, allowed at beaches, parks, in restaurants, cafes, hotels, on public transport. A happy dog is a well socialised dog. And to have dogs be able to move freely and welcomed in the community is as important for the dog as it is for the dog owner. The restrictions placed on a dog in moving in the community, are restrictions on freedom and community you also place on the dog owner.</p> <p>Misrepresentations and Inconsistencies</p> <ul style="list-style-type: none"> • The report by M Emery and co, cites as an action, the inclusion of two 'extra' dog -parks in the entire City, Mc Faull Park and Radonich park. However, McFaull park is already a dog exercise area, so it is not a new addition. • The report by M Emery and co, cites as an action, the inclusion of a dog wash station at CY O'Connor – there is already a dog wash station at CY O'Connor. However, there is none at any other dog beaches in the City of Cockburn. It would be helpful to have a dog wash station at Ammunition Jetty Beach • The report shows the Jervois bay area as currently off-lead. It is currently on-lead. <p>☐ The report by M Emery and co, misrepresents the main community concerns as indicated by the Catalyse survey. Whilst the data shows that the main concerns are waste, and wildlife protection, (as prompted by the limit and scope of the questions asked in the survey). the report by Emery presents the main concerns as waste and 'keeping</p> | |
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| <p>dogs out of dog prohibited areas', and 'owner control of dogs" (see page 395). The Catalyse survey also reports community satisfaction with these same things – city management of waste and protection for wildlife and nesting birds. Alongside 85 percent satisfaction all round with the City's management of dogs.</p> <ul style="list-style-type: none"> • Whilst nearly three quarters of all respondents in the Catalyse survey cited they were happy with the coastal areas provided for dogs and their owners, when asked about a proposed council action plan their main concerns and requests of those surveyed, are not enough off leash coastal areas for dogs, not enough coastal areas for dogs per-se, and not enough off leash areas as a whole. There are also multiple requests in the survey responses, cited by Catalyse, to make Woodman Point/ Ammunition jetty beach a leash free area for dogs (see page 439). <p>▣ The report by Catalyse, cites most of the community being happy with the signage available (see page 442), at variance with the survey results M Emery and Co are proposing an extensive, extremely expensive and resource intensive signage campaign. Additionally, the city already suffers from a surfeit of signage that is both visually unappealing and often patronising to responsible members of the public. More signage is not necessary or desirable.</p> <ul style="list-style-type: none"> • The report by Catalyse cites 85 percent of those surveyed giving the City a current positive rating for management of dogs in public places (see page 424), which begs the question as to why more management actions would be needed? • The report by M Emery is proposing actions that are at variance with the survey response. The suggestion of geo fencing RFDIS tags, CCTV cameras and monitoring practises are aggressive, invasive and policing manoeuvres that are both offensive and an extreme invasion of privacy. They are not warranted by the survey results and are not actions that are welcoming of and caring towards dogs or their owners. <p>Significance of Ammunition Jetty as a dog beach and inadequacy of other areas</p> <p>Ammunition Jetty beach is the only stretch of long uninterrupted coastline available to dogs and their owners to enjoy in the City of Cockburn. All other parts of the coastline of this kind dogs are prohibited at.</p> <p>I understand the initial proposal was to close it to dogs, but now the proposal is to have it off leash with the prospect of closing it at tern nesting times. I am in principle in agreeance with this new proposal but feel it would be more valuable from an educational and conservation perspective, to close the beach in nesting times, to any external form of disruption.</p> <p>The report cites that the key concerns raised by the respondents were to do with dog waste not being picked up by owners, and to do with protection of wildlife and nesting birds. At the same time this is what the respondents said the council perform most effectively at. Yet nesting birds (the terns) are used as the rationale to close the beach to dogs, citing 'community concern" (and not citing community satisfaction at the same time) as the reason, alongside an unsupported claim about DBCA having a wide-ranging concern about terns.</p> <p>In terms of information I have become aware of regarding terns behaviour and needs closing this beach to dogs, is not a necessary or sufficient condition for protecting terns nesting sites. The issue is much more complex than this. If your concern is to protect terns nests , then you have to close the beach at nesting times to humans (who have an equal if not more significant impact) as their footfall is heavier than dogs, there are more humans than dogs on the beach, children run up to birds and dig in the sand, and kite surfers at this time are a significant problem for birds. And an all year-round greater threat are the predatory animals, namely cats and foxes. In addition, people swimming are one of the biggest threats to shore birds, who forage close to the shore, and who perceive humans to be a predator, thus a</p> | |
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threat, making their area of foraging no longer a safe haven.

In addition, the key area of population density and diversity of bird life is the point itself. Birds love congregating on peninsulas and points, as any bird lover would know and observe. If any area was to have a dog restriction zone applied to it, it should be here (alongside a human restriction), rather than the beachside of Woodman point.

A more effective and education oriented proposal to protect the terns, alongside being a better use of the funding allocated to this report, would be the establishment of a fairy tern breeding sanctuary, south of the Cockburn cement, shell terminal, (an area not frequented by dog owners or dogs) as the Fremantle council has done with Rous Head. This would meet and respect the needs of all creatures, birds, dogs and dog owners. I suggested this to a tern expert and she said this would be a very important step in protection of and recognition of the terns.

The report cites education as an important value. Yet none of the suggestions in the report are education oriented. They are simply about signs, restrictions, fines, control and penalties. An authentic public education opportunity would be to shut any nesting bird beachline, to the public for the most crucial time of nesting, this is usually for a few weeks of the year. This would express and communicate value for the wildlife and facilitate learning about the habits and patterns of shorebirds. To close off dogs from enjoying this beach and yet allowing humans and children to walk, dig and tramp along it at crucial times, is not reflecting any value or respect for bird life and patterns of nesting, or reflecting the value dogs, nature and wilderness, and sharing this together – has for humans and dogs alike.

In addition, the fairy terns do not nest on this beach every year, their patterns vary. So, what is required is a monitoring of when they are nesting, and a willingness to close the beach to humans, kite surfers, and dogs at this time, or a certain part of the beach. Then this can be reopened to dogs and their humans once this is over.

On a positive note, given that the other key concerns raised by those surveyed, was that there were NOT ENOUGH off leash coastal areas for dogs, or areas in general for letting them off leash in a primarily safe and free environment, it is heartening to see that this beach has now been put forward as an off leash area.

Humans and Dogs and Beaches

It is important that dog parents have a beach that is close and local, safe and that you can enjoy as a family and friends' unit. Dog beaches are not just about exercise, they are about being in your community with your community, and coming to know and love places, and being able to enjoy, swim and leisure there with your dog. Jervois bay does not provide this, it is not a swimming beach, it is flanked by industry on either side. There is also for large parts of the year, piles of seaweed at the water's edge. The water is very shallow (sometimes sealions come in). And the sand is quite dirty.

There is no parkland to gather on adjacent to the beach, it is essentially a car park, and then the beach. There is no shade. It has very low amenity as a public beach, other than for the kite surfers and fisher people.

And again, it is at a significant distance from residents of Spearwood and Coogee, you need to head kilometres down Cockburn road, and then you turn off and it is a further three kilometres down the beach road before you come to the beach itself. There is limited parking, so it cannot accommodate more people coming for the purpose of dog walks.

In its remoteness it is also not an ideal beach for a woman on her own, once the sun goes down it starts to feel unsafe.

It is not a beach therefore that can be really enjoyed at length on summer evenings, and it is not a meeting place for friends, with each other and with our dogs, as it is not a desirable swimming beach, and no-one unless they are a kite surfer or fisher, or the occasional dog walker as fore mentioned, chooses to go there.

I am not suggesting that this therefore should NOT be dog friendly, but are illustrating the importance of Ammunition Jetty beach as a local dog beach, that includes all of the amenity (parklands, parking, socialising areas, a swimming



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| | <p>beach), that Woodman point lacks.</p> <p>This beach uniquely includes a beautiful parkland you can rest at and have a picnic at, after your walk and swim. It is a beautiful little sandy path (fenced) that takes you down to the beach through the dunes, and it is a stunning 45-minute return walk to the mole and back. I meet many people on my walk, many dog lovers, and the same regular dog parents, who also go down there with their pooch nearly every evening.</p> <p>I know and take joy in the bird life, the pelicans, the terns, a flock of Carnaby's that have recently been flying over the dunes in a group of about thirty. I strongly feel that those who know and love the beach, who have visited it and cared for it every other day over years, are the ones who need to be key in the decision process of ongoing care for and inclusive community at this beach.</p> <p>What is at most important here is the fact that our coastline is a shared space, this is the nature of complex urban and ecological environments. It is shared by the birdlife, sea life, dune life, human life and dog life. Taking one out of the equation on the premise that it protects all the others reveals a failure to respect the community, and a failure in understanding the ecology.</p> <p>In sum, I thank you for your decision to keep Ammunition Jetty and Woodman point as places we can love and enjoy with our furry friends, with appropriate restrictions when needed. I cannot overestimate the importance of the right and also the privilege of enjoying our natural wonders with our animals and to be able to have the fluidity in our lives to meet our family and friends in places together, with the other members of our family, our dogs. Nearly all dog lovers are animal lovers, so respecting other animals is part of the equation. I think what is needed most is to educate people about the impacts of their own behaviour, on birds and other creatures, rather than restricting and penalising.</p> | |
| 149. | <p>I do not support dogs on the beach at Woodman Point south of the ammunition jetty. As a dog owner and bird lover, I think it is fair we keep some areas for wildlife. They were here first.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |
| 150. | <p>I would like to say that I am in full support for the following parts of the Plan:</p> <ul style="list-style-type: none"> - Purchase additional cat traps to loan to residents wanting to assist in the residential trapping of feral cats - Conservation reserve trapping - Review cat laws to include cat control areas, using a staged approach: <p>Stage 1 (2021): Enact Laws to prohibit cats from all City managed Conservation Reserves, Regional Parks and any identified additional bushland areas in the City</p> <p>Stage 2 (2023): Enact laws for cat control based on similar requirements for dogs (requiring cats to be retained within the owners property at all times). Investigate options for cat owners e.g. educational workshops and subsidies for cat confinement enclosures.</p> <ul style="list-style-type: none"> - Change Woodman Point from "on-leash" to "off-leash" and investigate possibility of it being "dog prohibited" during Fairy Tern nesting season <p>My reasons for supporting Woodman Point being a "Dog Prohibited" area during Fairy Tern nesting season is that I do volunteer work for Birdlife Australia including participating in the Great Cocky Count on an annual basis since 2014, and participating several years in the Rainbow Lorikeet Roost Count. I receive their newsletter and have read about their concerns for the Fairy Terns nests on that beach. I think it is entirely appropriate to help these birds continue to</p> | <p>Objective: 1 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash' and investigate the potential to have the area as a "dog prohibited zone" during the Fairy Tern nesting season. Implied Sentiment: Support</p> <p>Objective: 3 Actions: All Implied Sentiment: Support</p> |



reproduce safely on that beach and many people can't control their dogs even when on a leash and people also often don't put their dogs on a leash in specified dog leash areas – I see this all the time at Bibra Lake and North Lake! So people can't be trusted to adequately control their dogs.

Regarding the Cat Laws I would like to take this opportunity to outline the numerous problems I've had with my neighbours' roaming pet cats trespassing on my property over many years. I transformed my large property and large verge from grass to native habitat specifically for rare and engendered species and now my garden provides food, water and shelter for 23 species of birds: (Carnaby's Black Cockatoos, Brown Goshawks, Boobook owls, Red capped parrots, Ring necked parrots, Mudlarks, Galahs, Corellas, Red Wattle birds, Brown Honeyeaters, Singing Honeyeaters, New Holland Honeyeaters, Kookaburras, Laughing doves, other doves, Pardalotes, Willy Wagtails, Rainbow Bee Eaters, Pacific Black duck, Cuckoo Shrike, Rainbow Lorikeet, Raven, Magpies). More than 6 species of reptiles: (Bobtails, Lizards: Western Bearded Dragons, dugites, Skinks, geckos). And 2 species of Frogs: Motorbike frogs & Banjo frogs. I also have breeding populations of bandicoots and their joeys living on my property and using my property every day and night for foraging and water.

I've had a contained pet cat for 15 years living in an outdoor enclosure in my garden so she couldn't hunt and this is why my property has this many species of wildlife. However despite my best efforts to do the right thing for wildlife and my cat, my Neighbours' cats roam my garden and verge day and night and they use it as a hunting ground or area for sleeping even when I scare them away! Some local councils have brought in a night curfew but this doesn't work as the cats hunt Day and night, not just night. Over the years I have had to consistently chase my neighbours' cats off my property and I have witnessed them many, many times stalking and hunting the birds, bandicoots and frogs. The bandicoot joeys often disappear from my property altogether not long after they leave their Mother's pouch as they are so small and vulnerable to predation.

My experience is that cat containment is most beneficial for cats' health and welfare. My cat has not once been hit by a car, she's lived several years longer than all my other cats who were not contained, and she has never caught many of the feline diseases that are prevalent including Feline HIV. The only time she has ever been in a cat fight and got injured was when a neighbours' cat trespassed on my property and in order to defend her territory my cat escaped off her harness and chased it. This resulted in a very expensive vet bill for me as a result of a trespassing cat!

I have tried to speak with my neighbours over the years about the problems with their roaming cats and each time they have either become hostile no matter how tactfully I broach the subject, or they have refused to believe their cat is hunting (despite me witnessing it), and/or they say to me they are doing all the "right things" by sterilising, vaccinating, and microchipping their cat. They tell me - why should they contain their cats to their property when it is not the law. So it needs to be the Law.

The problem isn't just the hunting, it's their sheer presence walking around or sleeping on my property and verge that scares away birds and bandicoots that forage during the day and night and there isn't much for them to eat in suburbia as most people just have lawns, so this has a big impact especially for bandicoots that can't fly away to another area to feed.

Also I used to have two cats and I had to give away one of my beloved cats because my neighbours' desexed male cat was trespassing and spraying around the outside of my house, causing my cat to spray inside my house to try to mark her territory and keep it away. My neighbour refused to believe it was her cat and I tried many expensive vet approaches to stop my cat spraying but none worked so I had to give her away which was devastating for me. All because of a trespassing cat.

I think cat containment is best throughout the City because then it is easier to identify responsibility and liability if a cat is caught off its property rather than just having cats be banned from being in bushland and parks. I think it's best to



trap domestic and feral cats in all bush reserves and parks to protect native wildlife. I also agree with making traps available for residents to borrow so that I can catch the ones that trespass on my property hunting the wildlife and causing problems for my cat.

A lot of people seem to think cat containment is “too hard” but it is definitely not. A handyman helped me (but in retrospect I could’ve done it myself) to net a section of my roofed verandah with a couch for my cat to sit on and a cat door so she can get inside the house. It was very cheap (approx \$150) and most of the materials were bought from Bunnings. It was very cheap compared to the vet bills I would have had to pay if my cat got hit by a car or had cat fights. I trained my cat to walk on a harness through the streets in the evenings which she loves, and I place her on her harness in areas of the garden affixed to a garden stake so she can hang out where she doesn’t affect the birds and animals.

These are some cheap and easy containment options:

- There is the product called Oscillot <https://oscillot.com.au/>

There are also several Facebook groups dedicated to DIY cat enclosures where people ask for advice and others help them out.

- one is Bunnings Cat Hacks Australia focusing on using simple and cheap materials from Bunnings and there are many examples of portable style enclosures for people renting!

<https://www.facebook.com/groups/250488372605075/?ref=share>

- another group is created by a company that makes and sells various types of equipment for cat enclosures that people can either set up themselves or hire a handyman- they’re a more “upmarket” style

<https://www.facebook.com/groups/CatEnclosuresAus/?ref=share>

There are two other groups containing a mixture of cat enclosure ideas:

- Cat Enclosure Ideas - Western Australia

<https://www.facebook.com/groups/279644759164166/?ref=share>

- Cat Enclosures (an international group)

<https://www.facebook.com/groups/279644759164166/?ref=share>

So just in case you’re getting resistance from people including renters/landlords about cat enclosures being “too hard” these groups show there are very many options to suit all types.

I think it would be very helpful for the City to provide workshops and incentives and perhaps home visits to properties with easy cat containment options. Landlords and renters might need the most assistance, but there are many options in the above groups.

I’m from Sydney originally and I watched the wildlife decrease there year by year and I think Perth is very lucky to have all the wildlife it has. We need to act now before it’s too late, especially with more and more high density housing and less and less trees and bush.

I hope the City can also consider more grants and workshops to assist people to transform their verges from grass to native gardens with plants specifically to feed rare species. Verges might be the only place for plants in high density areas.



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| | <p>Dogs are legally required to be contained to their properties to protect wildlife and people. I think it is high time that cats were legally required to be contained to their owners' properties too. As residents we have support from Rangers when there are problems with neighbours' dogs but when there are problems with neighbours' cats there are no laws enabling residents to have a win-win outcome and very little support from Rangers. This needs to change.</p> | |
| <p>151.</p> | <p>I would like to submit my support for "Dogs to be prohibited" for the stretch of beach in front of the Woodman's Point Camp area.</p> <p>I strongly oppose this area becoming a dogs off leash area as there are plenty of dogs off leash areas within the local area. I also believe altering this area to be a dogs off leash area could pose a risk for children accessing the Woodman's Point facility.</p> <p>My main comment on your draft is a huge opposition to taking away the "On Lead" dog beach area at Woodman Point. For the love of dogs, WHY!?!?</p> <p>There are SO many off lead areas, parks, beaches available to those who want to use them. But there is only ONE beach that is on lead only. Just one! There are so many dog owners like me who for whatever reason, want a safe dog beach to take their dog ON LEAD and not be rushed by off lead dogs. So many fear reactive dogs, anxious dogs, previously attacked dogs - many many reasons.</p> <p>Why would you take away the ONE on lead area we get to use? Please leave those like us just ONE beach!</p> <p>Off lead dogs are everywhere. They get dedicated parks. They get CY O'Connor beach, south beach, much of woodman point. Now you're giving them sporting reserves too. They have taken over Manning park, so perhaps that's next?</p> <p>Why punish those of us who are doing the right thing by taking our on lead dog beach away? My girl is the sweetest dog you'd ever meet, but she's fear reactive after being attacked. She LOVES the beach at Woody's.</p> <p>I have no issue with making dogs prohibited during nesting season to protect birds, I am ALL about that! Please just leave us one ON LEAD beach!!</p> <p>If as you say, your objective 2 of the draft plan is to "provide sufficient, safe spaces for dogs, people and wildlife." then this goes completely against that. By taking away the only on lead dog beach, you are taking away our safe space and a safe way for our girl to enjoy the beach that she dearly loves.</p> <p>I urge you to truly reconsider this, or if it is off the table at woodmans point, then please consider designating another beach area on lead only.</p> <p>As a side note, I truly hope Manning Park never becomes off lead area, and I would really love to see more signage there stating it is lead only, as there are constantly those breaking the rules. There never seems to be a ranger presence here and the signage is very poor. Many people have control of their dogs, but many dont, and some are even regularly using the area as a space for throwing their dog balls which is not appropriate.</p> <p>I have filled in your comment survey on this matter, but I am also emailing to put forward the names and comments of some other community members on your facebook post, as the council has said they will not take those comments into account. They have given permission to me to put their names and comments forward in my email. You can see their original comments on your City of Cockburn facebook post relating to the draft.</p> <p>Jackie Gray, Rae Blake and Natasha King would all like to see better monitoring of ON lead areas so those of us with our on lead dogs can relax and enjoy using the spaces appropriately.</p> <p>Kim Lindsay has stated "Please keep the beach at WOODMAN POINT 'on lead' - we have junior sailors on that beach and</p> | <p>Objective: 1 Action: Create public education program 'No it's not okay' to improve owner supervision and control of dogs in off-leash areas. Implied Sentiment: Support</p> <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>have enough trouble with the dogs that are meant to be on lead"</p> <p>Robyn Feaver has said "I would like to have areas kept dogs on lead at woodman point too. Had one lady Woodman point with 3 dogs offlead and tried attacking my dog --- when mentioned theyre meant to be on leads, she voiced "well they're fine at home"</p> <p>On that note - an education campaign for those determined off lead dog owners in on lead areas who constantly state "my dog isnt a problem, theyre friendly" or "Its ok, my dogs friendly" or "My dogs got no issues, if yours does you should go home" - would be great! As they obviously dont actually have a good understanding of dog socialisation, or understand the actual issue with this.</p> <p>I don't believe we are the minority, I know there are many other people in the same boat with anxious or reactive dogs. We now feel the onus is on making more and more areas off lead which leaves us with little left.</p> <p>I hope others have spoken up on this issue. And even if you do believe we are in the minority, we are full rate payers, so our voice should still be heard.</p> | |
| <p>152.</p> | <p>I would like to formally object to the proposal to convert Beelias Reserve (off Lakefront Ave, Beelias) to a dog off-leash park under both a trial- and/or permanent-basis.</p> <p>We are a family with young children who live on Lakefront Ave, Beelias and are frequent visitors to the oval, playground and nearby lake. The reasons for our objection include:</p> <ol style="list-style-type: none"> 1. The area is heavily used by young families who are drawn to the playground, the paths around the oval and the lake for riding/scooting. 2. My children are young and as with many others of their age, are afraid of dogs, especially large ones which form the majority of what we see at the oval. 3. Beelias Reserve is currently supposed to be an on-leash area for dogs and is signposted as such. However owners frequently take their dogs to the oval and the dogs are not leashed. The dogs run up to children and owners rarely show any concern for the feelings of others. Reassurances of "Oh, he's just saying hello" or "he wouldn't hurt anyone" as the dog rushes up to a small child do nothing to alleviate the fear. Only restraining the dog addresses this issue. <ul style="list-style-type: none"> • Based on the points 1-3 above, we feel that having dogs off the leash in an area which is designed to accommodate young families (ie. playground & school) contravenes one of the key objectives from the Strategic Community Plan under Community Lifestyle and Security quoted in "the Plan" - Provide safe places and activities for residents and visitors to relax and socialize. 4. Not all dog owners are responsible and pick up their pets' waste – that's just an unfortunate fact. If dogs are allowed off the leash at the oval, the likelihood of finding faeces more widespread on the oval and the surrounds will increase. If owners cannot currently pick up their dogs waste on the leash, they will not chase their dog down to collect their waste from across the 5. With the oval being used for play and sporting activities, plus being utilized by the nearby Beelias Primary School, this presents a health hazard to those people utilizing the oval. <ul style="list-style-type: none"> • Based on points 4-5, the proposal contravenes the second key objective quoted in "the Plan" from the Strategic | <p>Objective: 2 Action: Phase 1 trial use of sporting reserves as off-leash areas to include: Beelias Reserve, Aubin Grove Reserve, Santich Park, Goodchild Park, Colorado Park and all existing dog exercise areas. Report back to Council on the effectiveness of this trial. If effective, roll out at other suitable locations. Implied Sentiment: Oppose</p> <p>Note: Submission suggests alternative actions relating to Objects 1 and 2 not mentioned in the Animal Management and Exercise Plan.</p> |



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| | <p>Community Plan under Economic, Social and Environmental Responsibility - 4.3 Sustainably manage our environment by protecting, managing and enhancing our unique natural resources and minimising risks to human health.</p> <p>6. We have lived on Lakefront Ave for more than 14years and I could count on one hand the number of times I've seen a Ranger at the oval, and have never seen them speak to any dog owners to enforce the dogs-on-leash rule.</p> <ul style="list-style-type: none"> Based on this, we have now faith whatsoever in the City's plan to successfully encourage responsible dog ownership in this area, with regards to being courteous to other park users or cleaning up their pets' waste. <p>I note that "the Plan" forecasted the number of dogs in Cockburn to increase to 24,000 by 2024 which is quoted as one of the drivers for changing Beeliar Reserve (among others) to an off-leash area. I would challenge the Council to consider the following points when formulating their management plan to better serve both dog-owners and non-dog owners within the Cockburn community:</p> <ul style="list-style-type: none"> If the number of dogs in Cockburn is increasing due to new housing developments/urban sprawl and therefore new people moving into the Cockburn, then consider whether accommodating greenspace/dog exercise areas into the housing developments should be mandatory under Town Planning. If the number of dogs is forecast to increase because existing residents are purchasing more dogs (ie. increasing from 1 to 2, or applying to increase household from 2 to 3 or more), then the Council should consider restricting the number of dogs per household. Perhaps even lowering the limit from two dogs per household to one (accepting that this could not be applied retrospectively but definitely going forward) and/or not accepting any more than two under any circumstances. If there are insufficient facilities to accommodate the forecast number of 24,000 by 2024, then corrective action to restrict dogs in the area should be applied rather than impose dog off-leash areas on the rest of the community. Finally, if more off-leash areas are essential, consideration should be given to utilising existing 'boring' greenspaces, which are used less by young families and children, to dog off-leash areas. An example of this is Costa Park on the corner of Gecko Tce and Bluebush Ave, Beeliar which has no play equipment for children and is currently underutilised. These areas are more appropriate for dogs, leaving Beeliar Oval for families and young children. | |
| <p>153.</p> | <p>To Cockburn Council – proposal to make the beach from ammo jetty to Woodmans point a no leash dog beach. I ask the council to carefully consider this move, and vote strongly against it. I am a daily summer swimmer, and a walker, along Coogee beach, since council have made the beach, from ammo jetty onwards, a dog on leash beach, that stretch of beach, is a no go for me, as I fear dogs, even on leashes.</p> <p>How many times, have I seen a dog, with its owner no leash, playing on Coogee beach, which is a no dog at all beach, they know this, and yet they (owners) just ignore the laws. It is bad enough, that the beach in question – is a dog on leash beach, and yet Cockburn council wants, the dogs, not to be on a leash, so they can around freely, wherever they want, these dogs – (unless something has changed) cannot read – and do not know when to stop, and go no further – onto Coogee Beach.</p> <p>How many times have I heard someone say to me "he won't hurt you" even though they are leashed and go for you.</p> <p>Is council prepared to have rangers permanently patrolling the beach, as like I have said, these dogs do not know, where the boundary is and stop?</p> <p>It will be just a matter of time before a dog runs after some, (just having fun) and attacks them, the dogs will get excited and will not listen to their owners, as I have witnessed before.</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from 'on-leash' to 'off-leash.' Implied Sentiment: Oppose</p> |



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| | <p>Who will be to blame then? Owner ?council? or will the blame fall on the person or, heaven forbid, a child having fun on the beach, designated for humans.</p> <p>If this proposal goes ahead, there will be more beach for these dogs, than for us humans. I ask council is this normal and right, there are already, as far as I know 3 beaches, designated for dogs, in the local area, the dog owners, can go there, and leave us humans – some environment and peace on the beach.</p> | |
| <p>154.</p> | <p>I STRONGLY OPPOSE THE PROPOSAL TO OPEN A BEACH south of the Woodman’s Jetty FOR DOGS WITHOUT LEASH</p> <p>The CoogeeWoodman’s beach is the only "dog-free" beach here. An oasis for people who for different reasons(don't like or are afraid of dogs..) do not want to have dogs running free at the beach.</p> <p>There are beaches both to the south and to the north of CoogeeWoodman’s beach there dog’s owner can exercise theirs dogs. They have also right to walk dogs (on the leash) south of Woodman’s Jetty. Is it not enough?</p> <p>It seems like people working at Cockburn Council favour more the owners of dogs but did not take into account the right for people to be at the beach free from dogs.</p> <p>Maybe your proposal to have Coogee Beach free for dogs is due to the fact that you lack the rangers to control that the rules (dogs with leash) are followed. You are shifting the responsibility to us.</p> <p>How do you ensure my safety on the Coogee beach (where I am walking every day), in case the dog (running without leash!) approaches me or even worse jump on me?</p> <p>I am terribly afraid of dogs and in this case I’m not sure how my body will react. Council should take into account that many people living here has a different background from countries where dogs are trained to be aggressive. Your decision to have a beach free for dogs will force me to use a pepper spray in order to protect myself and that will create huge conflict here.</p> <p>Who will clean after dogs defecting on the beach? Dogs owners will not control where running dogs leave the poo.</p> <p>Dogs WILL run on the dunes where "...access is restricted to protect natural environment and costal vegetation.... The dunes are ...important feeding, breeding and roosting grounds for variety of shorebirds /l.</p> <p>Such information can we find on the boards along the entire walking path i</p> | <p>Objective: 2 Action: Change the usage of Woodman Point from ‘on-leash’ to ‘off-leash.’ Implied Sentiment: Oppose</p> |



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| | <p>Woodman's. Don't you care about that anymore?</p> <p>If dogs off-leash will be allowed on the Coogee beach, I don't see why other people cannot have cat on leash on the beach?!</p> <p>Leave the only remained stretch of beach free from pets, if you don't want create conflicts.</p> <p>i- .</p> <p>~</p> <p>We, people, need a beach free from running dogs.</p> | |
| <p>155.</p> | <p>Poole Reserve has been my local park for the past 23+ years. Whilst it has been a leashed dog park many people don't adhere to the law. But many people do. Firstly, a ratepayer should be able to visit their local park and feel safe doing so. Parks are there to provide a pleasant and safe experience for all.</p> <p>Poole Reserve has just been upgraded with; a tree promenade, native gardens, children's [playground, exercise equipment, Gazebo, tables with seating and a flying fox. Some of these changes were suggested by myself. The park will be beautiful when completed. Thank you Cockburn.</p> <p>The park should now attract a lot more children and families and for that reason. I have real concerns for dogs being off lead because;</p> <ul style="list-style-type: none"> • Dogs love to use the sand pit to do their business and the rest of the park • Dogs love to dig in gardens • Dogs love to socialize enthusiastically • Some dogs become aggressive with other dogs on leads • Some dogs are overprotective of their owners and their children • Some owners occupy themselves with mobiles and ignore their dogs excreting all over the place • Some owners occupy themselves with mobiles and leave their young children unsupervised. • Some people use the exercise equipment and let their dogs roam free <p>If you allow the park to be overrun with owners using their mobiles, children running loose and dogs unsupervised there is real concern for a very unsafe environment for everyone including those dogs which do the right thing.</p> <p>There are a lot of people who wish to visit their local park without any interaction with animals especially dogs which show over enthusiasm or in some cases aggression. They may just want to exercise or get fresh air without any kind of threat or interaction with others or their uncontrolled animals. For their own personal reasons. They should have this right.</p> <p>Whilst there are a few who do the right whilst the signs say "Dogs must be on leads" there and quite a lot who allow their dogs to do the above. If the rules change permanently you will discriminate against those who should be able to enjoy life as much as those who are too lazy to do the right thing by everyone. I am 65 and walk my dog on a lead most mornings – if I can obey the rules I'm sure everyone can.</p> <p>Please reconsider the use of this park and let it be available for everyone, not just a few who are too lazy and inconsiderate to do the right thing. Dogs need to be controlled on leads for the safety, enjoyment and wellbeing of</p> | <p>Objective: 2</p> <p>Action: Submission calls for more signage and for Poole Reserve to remain an on-leash dog park.</p> <p>Implied Sentiment: Concern</p> |



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| | <p>everyone.</p> <p>Please take the time to consider; Have you ever been walking when two bull terriers have charged you and your small dog?</p> <p>Have you ever had to try protecting your leashed dog from the aggression of other dogs who are running free – their owners on the other side of the park? It is terrifying and heart breaking. It is also very confronting when dog owners become very abusive because you have asked them to do the right thing.</p> <p>Please make the park available to everyone. And please make your signage a lot more obvious to all who do visit the park.</p> | |
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Department of Local Government, Sport and Cultural Industries

Our Ref 20/3136
Enquiries Matt Hayes
Phone 9492 9795
Email Matthew.hayes@dlgsc.wa.gov.au

City of Cockburn Council
PO Box 1215
BIBRA LAKE DC WA 6965

Stamp: CITY OF COCKBURN, DOC Set, 8 1 AUG 2020, SUBJECT 144/003, RETENTION 54.277, PROPERTY, APP, ACTION 011/011, 011/016

Dear Mayor Howlett and Councillors

DRAFT CITY OF COCKBURN ANIMAL MANAGEMENT PLAN

The Department of Local Government, Sport and Cultural Industries (DLGSC) note with concern the recommendation of the City of Cockburn Council at its Ordinary Meeting of 9 July 2020, that the beach at Woodman Point be zoned as "dogs off-lead".

The department operates Woodman Point Recreation Camp at this location, servicing a large volume of metropolitan and regional school and community groups throughout the year. The camp has operated since 1982, utilising the former Quarantine Station which operated in the area from 1852 until 1979. The beach in this area is heavily utilised by children attending camps at Woodman Point, both during structured and unstructured recreational activities.

In December 2016, despite the recommendations of City of Cockburn officers, key stakeholders, local land managers and the majority of community respondents, the beaches of the Woodman Point peninsula up unto the 'Munitions Jetty' at John Graham Reserve, was re-zoned as "dogs on-lead". Our department was not consulted as part of this process. Following this decision, our department submitted its objections to the Chief Executive Officer of the City, West Ward Councillor Michael Separovich and the Woodman Point Regional Park Community Advisory Committee.

Since that time, there have been numerous instances of unwelcomed contact by dogs to children from the camp whilst participating in recreation activities. Fortunately, there has been no instance of injury to-date.

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A decision to allow dogs off-lead at this beach is without logical rationale. The Draft Animal Management Plan commissioned by the City of Cockburn which is now before Council, recommends that this beach returns to "dogs prohibited" for a variety of reasons, including increased risk to beach users and camp participants, the likely impact on local wildlife within the nature reserve and the close proximity of the existing "dogs off-lead" exercise beach on the entire southern side of the Woodman Point peninsula.

I urge Council to reconsider the proposed amendment to the Draft Animal Management Plan and consider the expert advice provided within the report and return the beach on the northern side of Woodman Point to "dogs prohibited".

I invite West Ward Councillors Michael Separovich, Phoebe Corke and Kevin Allen, to meet with the Manager of Woodman Point Recreation Camp, Matt Hayes, to discuss the matter further in the hope of finding a suitable resolution for the camp, environment and local community. Matt Hayes can be contacted on 9492 9795 or matthew.hayes@dlgsc.wa.gov.au.

Yours sincerely



Kim Ellwood
Executive Director – Sport and Recreation

28 July 2020



Department of **Biodiversity,
Conservation and Attractions**



Enquiries: Tim fisher
Phone: 08 9442 0300
Email: timothy.fisher@dbca.wa.gov.au

City of Cockburn
9 Coleville Crescent
SPEARWOOD WA 6163

comment@cockburn.wa.gov.au

To Whom it May Concern

DRAFT ANIMAL MANAGEMENT AND EXERCISE MANAGEMENT PLAN 2020-2025

Thank you for the opportunity to comment on the City of Cockburn's (the city) proposed Animal Management and Exercise Plan. The Department of Biodiversity, Conservation and Attractions (DBCA) has reviewed the plan and acknowledges the work the city is taking in managing domestic animals in their jurisdiction.

DBCA commends the city's objectives in relation to the management of cats, including the review of the cat laws and the introduction of cat prohibited areas. This will have a positive impact and provide protection to the conservation values of reserves. The staged approach that is proposed is well considered and will provide the community with opportunity to adapt. In relation to the management of dogs, DBCA does hold concerns with the proposed changes to the management of dogs along the beach at Woodman Point.

Woodman Point Regional Park (the park) consists of multiple reserves managed by a number of State government agencies and the City of Cockburn. A majority of the land tenure within the regional park is managed by DBCA as Nature Reserve and Conservation Park for the protection of the biodiversity and recreation values. The Conservation Park, extends to the high water mark, with the remaining beach managed by the city. This boundary is not delineated and there may be impacts on the values of the Conservation Park from the proposed off-leash dog area.

In 2016, DBCA provided comments to the city expressing concerns with the proposal to allow dogs on-leash along the beach adjoining the park, noting likely impacts to wildlife and conflicts with user groups. In addition, it was highlighted that the *Woodman Point Regional Park Management Plan 2010* states that "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park." The management plan was endorsed by the city and represents the commitment from all stakeholders in managing the park cooperatively. DBCA's views were shared by other key stakeholders in the park such as the Department of Local Government, Sport and Cultural Industries who manage the recreation camp within the park. These views were not reflected in the Council's decision to designate the beach as on-leash dog area.

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DBCA Submission on Draft Animal Management Plan - City of Cockburn v2

20/10/2020



In early 2020, the city engaged a consultant to undertake a review and develop an Animal Management Plan for the city's jurisdiction. Given the majority of the land within the park is managed by DBCA, there was opportunity for DBCA to provide comment, along with the other key stakeholders in the park with a vested interest. DBCA's understanding is the plan that was presented to the city recommended the beach area along the northern extent of the park be designated as a dog prohibited area. The draft provided for comment does not reflect this understanding.

The current designated off-leash exercise area along the southern side of Woodman Point View is supported by DBCA to remain.. This location provides a suitable and sufficient area to maintain as the dog off-leash area as it is located away from the Nature Reserve and the foreshore that supports the migratory and shorebird population. This area is also not near to any major recreation sites with minimal potential conflicts with other user groups.

The beach within (and adjoining) the Conservation Park includes nesting and roosting areas for Australian Fairy Tern (*Sternula nereis nereis*) a threatened species listed with a rank of Vulnerable under the State *Biodiversity Conservation Act 2016* and the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999*. One of the main threats identified for the Australian Fairy Tern is the disturbance to breeding (and roosting), and loss of breeding habitat as the result of disturbance by humans and domestic pets.

In addition, there are several other threatened and migratory shorebirds species that are also likely to be impacted from the proposed change in this draft plan. This includes Red-Capped Plovers and Oystercatchers, numerous shorebird species such as Ruddy Turnstones, Grey Plovers, and the Grey-Tailed Tattler. Other listed species such as the Critically Endangered Great Knot (*Calidris tenuirostris*) and Curlew Sandpiper (*Calidris ferruginea*), the Endangered Red Knot (*Calidris canutus*) and Lesser Sand Plover (*Charadrius mongolus*) and the Vulnerable Greater Sand Plover (*Charadrius leschenaultii*) have also been recorded at Woodman Point.

In addition to the likely impacts to wildlife and other conservation values of the park, conflicts between user groups that recreate in the park are important consider. John Graham Reserve, part of Woodman Point Regional Park, attracts more than 600,000 visitors per annum for the purpose of recreation, including picnicking and exercising. John Graham Reserve is currently a dog prohibited area and the proposed change may result in large numbers of dog owners seeking to use the carparks and facilities at the reserve to access the beach causing ongoing management and enforcement issues for DBCA and detract from the enjoyment of the reserve.

DBCA does not support the proposal in the draft Animal Management and Exercise Plan for the beach area adjacent to Woodman Point Regional Park be changed from an on-leash area to off-leash area. DBCA considers it more appropriate to make this area 'dogs prohibited' as previously advised and recommended in the draft Animal Management Plan developed by the consultant and recommended to Council by the City Administration. This recommendation is aligned with the park's management plan and DBCA's management of the adjacent area.

DBCA will continue working with the City and other interested stakeholders to ensure that Woodman Point Regional Park continues to support and protect the State's significant biodiversity values and provide a range of recreational opportunities

If you would like to discuss the contents of this letter in more detail please feel free to contact DBCA's Regional Parks Unit Manager, Mr Tim Fisher on 9442 0300.

Yours sincerely



Benson Todd
REGIONAL MANAGER

25 August 2020



**WOODMAN POINT REGIONAL PARK
COMMUNITY ADVISORY COMMITTEE**

C/- 19 Commercial Street
SOUTH FREMANTLE
WA 6162



City of Cockburn Council members
Email: avasile@cockburn.wa.gov.au

Dear Councillors,

9 July 2020 Ordinary Meeting of Council Agenda Items 16.1 and 17.1

The Woodman Point Regional Park Community Advisory Committee (WPRPCAC) has been established to provide a regular forum for community members to have input into the management and planning of the Woodman Point Regional Park. The Committee contains representatives of State and local government and community members. The Minister for the Environment has endorsed the appointment of the community representatives on the Committee.

On behalf of the WPRPCAC we highly commend the City of Cockburn for its initiative to implement animal management measures which will assist in the protection of and reduction to disturbance to native wildlife.

Regarding the recommendations for 16.1 Options for the Control of Roaming Cats, the WPRPCAC wholly endorse these especially the proposed tenure-blind approach to include all Regional Parks within the City of Cockburn (Woodman Point, Jandakot, Beeliar) in considerations for the Cat Prohibited Areas designations.

Regarding the recommendations for 17.1 Proposed Animal Management and Exercise Plan 2020-2025 the WPRPCAC support most of the recommendations, with a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 as it is consistent with the Woodman Point Regional Park Management Plan and the presence of dogs has been known to negatively impact the wildlife (i.e. nesting fairy terns) as well as children's activities at the Woodman Point Recreation Centre.

The WPRPCAC do not support the expansion of the dogs on-leash area as per recommendation (3) e on a portion of Reserve 24306 – Coogee Beach, Coogee as defined in Attachment 3, and wish for the current area to be retained as a dog prohibited area. It is preferred the status quo remain for dogs in this area due to the high use by families for picnics and recreation activities.

Yours sincerely

A handwritten signature in black ink, appearing to read 'J. Robinson'.

James Robinson
Chair, Woodman Point Regional Park Community Advisory Committee
9 July 2020

WOODMAN POINT REGIONAL PARK COMMUNITY ADVISORY COMMITTEE

C/- 19 Commercial Street
SOUTH FREMANTLE
WA 6162



City of Cockburn Council members
Email: avasile@cockburn.wa.gov.au

Dear Councillors,

Re: 9 July 2020 Council Alternative Motion 17.1 (1) (3) e - Add Woodman Point Beach as off-lead dog exercise area.

As per our letter to council on 9 July 2020 regarding the recommendations for 17.1 Proposed Animal Management and Exercise Plan 2020-2025 we wish to reiterate that the Woodman Point Regional Park Community Advisory Committee (WPRPCAC) gave a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 as it is consistent with the *Woodman Point Regional Park Management Plan 2010* (WPRPMP) and the presence of dogs has been known to negatively impact wildlife (i.e. nesting fairy terns and other migratory birds) as well as conflict with children's activities when using the beach at the Woodman Point Recreation Centre.

The WPRPCAC strongly believes the Woodman Point Beach south of the ammunition jetty should be returned to a dog prohibited area for the following reasons:

- The current proposal is inconsistent with the WPRPMP which was endorsed by the City of Cockburn prior to its gazettal in 2010.
- Page 26 of the WPRPMP states: "At Woodman Point, dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park. Dogs are not permitted on the grassed areas of John Graham Recreation Reserve because they may cause a nuisance to visitors."
- Domestic animals including dogs have been known to negatively impact on nesting Australian Fairy Terns at Woodman Point, and there are many other migratory birds and shorebirds that will be impacted by dogs off lead.
- There already exists a dog exercise area on the eastern end of the beach adjacent to the boat launching precinct, which is adequate in size and does not compete or generate conflict with other user groups.
- There have been many incidents of the children at the Woodman Point Recreation camp being negatively impacted by dogs off lead on the beach, despite the area currently zoned as dogs on lead.
- The consultant hired by City of Cockburn to develop the animal management plan undertook expert work and provided sound recommendations for why the beach to the south of the ammunition jetty should be dogs prohibited.
- The area was made a dogs-on-lead area in error by council and advertised, giving false expectations and now that error is being further exacerbated
- Woodman Point contains a nature reserve and conservation park vested in the Conservation and Parks Commission and dogs off lead can easily stray into these areas disturbing wildlife.



- The children from the Woodman Point Recreation Centre have used this beach for many years and do not have alternative areas to go. They should have the right to access the beach adjacent to the camp without being harassed by uncontrolled dogs.
- It is already demonstrated that once dog walkers get access to an area, they refuse to give it up, so requesting they restrict their use during Fairy Tern nesting season will not be practicable.

For all these reasons, the WPRPCAC give a definitive approval of recommendation (1) f as a dog prohibited area on Woodman Point Beach, Coogee and surrounding area as defined in Attachment 3 and strongly disagree with the alternative recommendation 17.1 (1) (3) e.

Yours sincerely



James Robinson
Chair, Woodman Point Regional Park Community Advisory Committee
26 August 2020



18 August 2020

City of Cockburn
9 Coleville Crescent
SPEARWOOD WA 6163
comment@cockburn.wa.gov.au

Re: draft Animal Management and Exercise Plan 2020-2025

BirdLife Australia commends the City of Cockburn for identifying the importance of animal management to protect the natural environment, for proposed cat management laws, and making the draft animal management plan open for public comment

However, BirdLife strongly recommends that dogs are prohibited from the beach south of ammunition jetty.

BirdLife are concerned by the City's proposal to designate the shoreline of the Woodman Point Regional Park (particularly between the ammunition jetty to and inclusive of the groyne and headland) a "dogs off-leash" zone. It was not appropriate for this to be a "dog on-leash zone" due to the bird conservation values of the shoreline, and further relaxing the zoning will be detrimental to the bird life.

It is a proposal that is neither consistent with the *Woodman Point Regional Park Management Plan* (2010) or with retaining the biodiversity value of the shoreline, for which BirdLife has credible data. It is also unclear why the plan proposes to increase off-leash beach areas, when community consultation called for more fenced dog exercise areas.

The Regional Park Management Plan identifies that the Park will be managed for conservation and that "recreation and other uses will be allowed to occur to the extent that they do not adversely impact on the natural environment". Declared threatened flora and fauna are given priority for conservation and "dogs are not permitted in the nature reserve, on the headland nor the beaches adjoining the Park". Your proposal is inconsistent with all these management strategies, providing no priority of protection to threatened migratory shorebirds and Australian Fairy Tern and placing recreational use above protection of the environment by allowing dog access to areas that are not appropriate.

BirdLife's extensive bird database (from annual shorebird monitoring) has records of 26 species of migratory and resident shorebirds using Woodman Point. The area provides feeding and roosting/resting habitat for the following federally listed threatened species:

Critically Endangered:

Great Knot (*Calidris tenuirostris*)
Curlew Sandpiper (*Calidris ferruginea*)

Endangered:

Red Knot (*Calidris canutus*)
Lesser Sand Plover (*Charadrius mongolus*)

Vulnerable:Greater Sand Plover (*Charadrius leschenaultii*)Australian Fairy Tern (*Sternula nereis nereis*)

Migratory Ruddy Turnstone, Grey Plovers and Grey-tailed Tattler regularly use the rocky groyne, and knots, sandpipers and resident Red-capped Plovers prefer the exposed sand and shallows. The area also could provide breeding habitat for resident beach nesting birds including Red-capped Plovers and Oystercatchers (Pied and Sooty), though the amount of human activity and frequent presence of dogs has likely impacted birds selecting the area to breed in recent years. Making the area dog free as well as measures proposed to facilitate Fairy Tern breeding (temporary fencing and feral predator control) will also benefit these beach nesting species and migratory shorebirds.

Unleashed dogs on the beach adversely impact the natural environment by disturbing feeding and roosting shorebirds and breeding beach nesting birds such as Fairy Terns, Australian Pied Oystercatchers and Red-capped Plovers, all of which frequent the area. Migratory shorebirds rely on undisturbed feeding and roosting to build body condition to make their long migration to the northern arctic to breed and to recover when they return to Australia. There is a growing body of research indicating that frequent disturbance can significantly impact shorebird feeding behaviour, causing birds to waste energy in repeated flight to get out of the path of dogs and in some cases can cause birds to stop using a productive site all together (Stigner et al. 2016). Dogs off leash can also cause damage to eggs, injury/death of chicks and stressful disturbance to adult breeding beach nesting birds.

Due to increased development of and human activity across coastal areas of the Perth metropolitan area, there are now few sites in Perth that provide undisturbed habitat for migratory and resident shorebirds. Consequently, bird numbers have and continue to decline, placing even greater value on places like Woodman Point where a variety of shorebirds can still be seen. This is a natural asset the City of Cockburn should be proud of and keen to manage appropriately so that shorebirds continue to use the site. This management must include prohibiting dogs from the beach. Indeed, there is no point trying to encourage Fairy Terns (and potentially other beach nesting birds) to breed at Woodman Point if you plan to allow dog access. Research has clearly shown that beach nesting birds and dogs do not mix and that best outcomes are achieved by prohibiting dogs (all year round) rather than encouraging and relying on responsible dog ownership/control (see Maguire 2018 - attached). Community engagement and education are important when prohibiting dogs from areas they have had access, and BirdLife would be willing to assist the council with this engagement and messaging.

Please do not hesitate to contact me on 0409033126 or vicki.stokes@birdlife.org.au if you require further information.

Yours Sincerely



Dr Vicki Stokes
WA Program Manager
BirdLife Australia

Maguire, G. 2018. *A Review of Dog Impacts to Beach-besting Birds and Management Solutions*. BirdLife Australia Report.

Stigner, MG., Beyer, HL., Klein, CJ. and Fuller, RA. 2016. Reconciling recreational use and conservation values in a coastal protected area. **Journal of Applied Ecology** 53, 1206-1214.



conservation council of western australia (inc.) abn 35 982 476 107
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29 May 2020

Christopher Beaton
City of Cockburn
9 Coleville Crescent
Spearwood WA 6163
(Via email: cbeaton@cockburn.wa.gov.au)

Dear Chris,

I write with respect to the Animal Management Plan and the proposed changes to prohibit dogs from a section of beach between south of Ammo Jetty and Jervoise Bay, adjoining Woodman Point Reserve.

I note that one of the concerns mentioned in the Animal Management Plan is the impact that dogs are having on the Australian Fairy Tern, *Sternula nereis nereis*. The area of beach between the Woodman Point Groyne (near the car park) and Cockburn Cement is, historically, an important breeding area for this species. However, due to high levels of disturbance, no attempt to breed has been recorded in the past decade.

Nesting attempts were made in November 1997, February 1998, December 1999, December 2007 and December 2009. But in all cases, eggs were disturbed or destroyed by people or four-wheel drive vehicles (Singor, M., pers. comm.). More recently, changes to the beach zoning mean large numbers of people visit this section of beach with their dogs, which is likely to negatively impact Fairy Terns.

Fairy Terns and the other birds using the shoreline in this area will benefit from a range of measures and I recommend that a holistic approach be taken to improve the management of this site. Particularly, if the aim is the support nesting attempts by Fairy Terns and to restore the natural values of the area. This includes the prohibition of dogs, which will reduce disturbance, particularly from un-leashed animals, new regulations that restrict cats from the Woodman Point Reserve, trapping of pest animals (cats and foxes) within and around the reserve. Particularly in the pre-breeding season when Fairy Terns and other shorebirds aggregate in the area. Additionally, a community education program, designed to help reduce the impact of beachgoers on feeding, roosting and nesting shorebirds.



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Dogs have been identified as a major threat to beach-nesting birds (Maguire 2018). In 2018, BirdLife Australia completed 'A review of dog impacts to beach-nesting birds and management solutions' (Maguire 2018). The impact of dogs ranged from disturbance, causing adults to leave their broods unattended to egg and chick predation. No dog areas are listed as being the most effective strategy to protect shorebirds/ beach-nesting birds (Maguire 2018). In Victoria, the prohibition of dogs from a beach on the Mornington Peninsula resulted in a doubling of Hooded Plover fledgling production, compared to the previous decade. For further details, see Maguire (2018).

The impact of free-roaming cats on wildlife populations is a significant problem and must also be addressed (Osborne & Williams 1991; Greenwell et al. 2019; Legge et al. 2020). In 2018, an entire colony of Fairy Terns (~220 birds) in Mandurah was destroyed by a single, free roaming desexed cat that depredated adults and chicks over several nights, until eventually, the colony was abandoned (Greenwell et al. 2019).

The issue of beach management to protect Fairy Terns and other shorebirds in the area is complex, but adopting a range of measures, as noted above, will go a long way towards reducing disturbance and improving outcomes for birds utilising this historically important site.

Should you have any questions, feel free to contact me.

Yours sincerely,

Dr Nic Dunlop
Citizen Science Coordinator
Conservation Council of Western Australia

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To whom it may concern,

I am writing in reference of the City of Cockburn's Draft Animal Management Plan. Whilst I support the plan in general, I have significant concerns regarding the area near Ammunition Jetty at Woodman Point being identified as an off-lead dog area.

The Woodman Point Regional Park contains a wide variety of ecosystems including tracts of undisturbed coastal vegetation that support local and migratory birds. A total of 93 species have been recorded at Woodman Point. Of these, 36 species are shore-birds and other waterbirds including the Australian Fairy Tern which is a threatened species.

Introducing an off-lead area for dogs at Woodman Point would not only compromise remaining vegetation that is crucial habitat for these species but it will likely result in ongoing predation of our precious wildlife.

Western Australia's biodiversity and wildlife are already declining at alarming rates therefore I hope more consideration is given to this matter.

Kind Regards,

Dean

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City of Cockburn - Woodman Point EIA

Environmental Impact Assessment of
Proposed Off-Leash Dog Beach at
Woodman Point

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Environmental Impact Assessment of Proposed Off-Leash Dog Beach at Woodman Point

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Executive Summary

A Draft Animal Management and Exercise Plan (AMP) has recently been submitted to the City of Cockburn Council (the Council) for their approval; to guide the City's approach to promoting responsible pet ownership and to ensure the facilities are appropriately equipped and accessible for the City's growing population of pets. Within the AMP, a trial period has been suggested to change the zoning to a 2.7km stretch of beach south of Ammunition Jetty at Woodman Point for 24-months as a 'dog off-leash' area and becoming a 'dog prohibited' area during the Australian Fairy Tern (*Sternula nereis nereis*) nesting season (October to March, with the months to be identified each year).

Integrate Sustainability Pty Ltd (ISPL) was engaged by the City of Cockburn to undertake an impact assessment and prepare a report on the feasibility of a 2.7km section of beach at Woodman Point being closed to dogs during the Fairy Tern nesting season. An environmental impact assessment approach was utilised to assess the feasibility. The EIA prepared by ISPL for the City of Cockburn has been completed by:

- Undertaking a site visit;
- Completing a desktop assessment on the environmental values at Woodman Point with a particular focus on coastal habitat and use by Fairy Terns and other shorebirds;
- Identification of potential environmental impacts associated with the rezoning of the beach and completing a risk assessment to outline the likelihood and severity of these impacts occurring;
- Consultation with key stakeholders (Fairy Tern Network, Birdlife Australia, CCWA); and
- Provision of recommendations based on the risk assessment.

Woodman Point provides known habitat for a range of shorebirds and migratory shorebirds which are listed under the *Biodiversity Conservation Act 2016* (WA) and/or the *Environment Protection and Biodiversity Conservation Act 1999* (Cth). In particular, the following listed species have been recorded in recent years:

- Great Knot (*Calidris tenuirostris*, critically endangered);
- Curlew Sandpiper (*Calidris ferruginea*, critically endangered);
- Red Knot (*Calidris canutus*, endangered);
- Lesser Sand Plover (*Charadrius mongolus*, endangered);
- Greater Sand Plover (*Charadrius leschenaultia*, vulnerable);
- Australian Fairy Tern (*Sternula nereis nereis*; vulnerable); and
- 25 listed migratory shorebirds.

Fairy Terns are known to have previously used the area between Cockburn Cement and the Carpark at Woodman Point Headland for breeding; however, the increase in human traffic, recreation and dog access are likely to have contributed to reduced breeding attempts and breeding success (CCWA Pers Comm, 2020; Birdlife Australia Pers Comm, 2020; Pers Comm C. Greenwell, 2020).

The risk assessment ranked potential impacts to Fairy Terns and other beach-nesting shorebirds as high due to habitat degradation, disturbance and predation which is likely to increase with use of the beach by people and dogs even if the beach is closed to dogs over the summer months. Public safety impacts were also ranked as high due to the isolation and incompatible recreational activities which occur along this stretch of beach.

Allowing dog access to a 2.7km section of beach from Ammunition Jetty to the Carpark at the Woodman Point Headland with the exception of during the Fairy Tern nesting season between October to March, is unfeasible for the following reasons:



City of Cockburn - Woodman Point EIA
Environmental Impact Assessment of Proposed Off-Leash Dog Beach at Woodman Point

- Shorebird populations, including Fairy Terns, may be adversely impacted by:
 - Loss of breeding, roosting and foraging habitat;
 - Increased predation;
 - Increased disturbance rendering the site unsuitable;
- Fairy Tern nesting season and beach closure coincides with peak beach access periods over the summer months;
- Other shorebirds, including migratory species listed under the EPBC Act, also utilise this stretch of beach;
- There is significant potential that impacts will modify, destroy, remove or decrease the availability or quality of habitat for Fairy Terns, other shorebirds and migratory shorebirds and triggers the need to refer the action under Part 9 of the *Environment Protection and Biodiversity Conservation Act 1999* (Cth);
- Re-zoning is not in alignment with the Woodman Point Regional Park Management Plan;
- The area is isolated making it difficult to patrol and manage beach closures;
- Litter may become an issue along the beach and bins could only be installed and emptied at the entry point near the Ammunition Jetty;
- Closing the beach for the summer months could lead to confusion and be unpopular amongst dog owners; and
- The possibility of a dog attack would be increased if dogs were permitted off-leash increasing public safety issues and incompatible recreational activities with the Woodman Point Recreational Camp. Isolation is also a primary concern as access by emergency vehicles in the event of a severe dog attack or other emergency is limited.

Based on this assessment, it is recommended that the beach south of Ammunition Jetty to the Carpark at Woodman Point Headland be zoned as a 'no dog' area. This recommendation is in line with the Woodman Point Regional Park Management Plan and would provide a safe refuge with conservation benefits for Fairy Terns, shorebirds and seabirds.





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1 Introduction

Integrate Sustainability Pty Ltd (ISPL) was engaged by the City of Cockburn to undertake an impact assessment and prepare a report on the feasibility of a 2.7km section of beach at Woodman Point being closed to dogs during the Australian Fairy Tern (*Sternula nereis nereis*) nesting season. Three subspecies of Fairy Terns are recognised, these being the New Zealand Fairy Tern, the New Caledonian Fairy Tern and the Australian Fairy Tern. The term Fairy Tern is used throughout this document and refers to the Australian Fairy Tern (*Sternula nereis nereis*) which is listed as Vulnerable under both the *Biodiversity Conservation Act 2016* (WA) (BC Act) and *Environment Protection and Biodiversity Conservation Act 1999* (Cth), (the EPBC Act).

1.1 Project overview

A Draft Animal Management and Exercise Plan (AMP) has recently been submitted to the City of Cockburn Council (the Council) for their approval; to guide the City's approach to promoting responsible pet ownership and to ensure the facilities are appropriately equipped and accessible for the City's growing population of pets (City of Cockburn, 2020). One of the recommendations put forward in the AMP was to prohibit dogs on a portion of beach south of Ammunition Jetty and over the Woodman Point Headland within the Woodman Point Regional Park for the purpose of protecting potential Fairy Tern nesting habitat (Figure 1.1). This section of beach is currently designated as a 'dog on-leash' area (Figure 1.2). At its July 2020 meeting, the Council requested Woodman Point be changed from a 'dog on-leash' area to a 'dog off-leash' area, as outlined in the Draft AMP currently open for public comment (City of Cockburn, 2020). A trial period has been suggested for changing the access to this stretch of beach for 24-months to be a 'dog off-leash' area, but becoming a 'dog prohibited' area during the Fairy Tern nesting season (October to March, with the months to be identified each year). The purpose of this report is to assess the feasibility of the recommendation to change the zoning of the beach as per the trial, with a particular focus on Fairy Terns and other shorebirds for which this stretch of beach provides habitat.





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 Environmental Impact Assessment of Proposed Off-Leash Dog Beach at Woodman Point



Figure 1.1 Woodman Point location of beach re-zoning





Figure 1.2 Woodman Point current and proposed beach dog access. Red – No access, Yellow – On-leash, Green – Off-leash (City of Cockburn, 2020)

1.2 Approach and methodology

An environmental impact assessment approach was utilised to assess the feasibility of closing a 2.7km section of beach at Woodman Point to dogs during the Fairy Tern nesting season. Environmental Impact Assessment (EIA) is a process which involves examining and identifying potential consequences or impacts to the environment should proposed activities be implemented (Wathern, 2013). EIA is used as a decision making and planning tool for urban development as it can provide decision makers with an indication of the consequences related to an activity. The EIA process includes initial identification of the environmental values of the area proposed for development or activity; either through a desktop assessment summarising the information already known about the area, or using targeted surveys where the environmental values are unknown or there are potential significant environmental impacts.

This is followed by identification and discussion of the environmental risks associated with environmental values or characteristics of the proposed development. Where risks are identified, management measures are also recommended with the aim of minimising the environmental impacts should the development or activity proceed. This information is then used by decision-makers to determine whether the project or activity can be appropriately managed and should proceed; or if it will result in a significant impact to the environment and should not be implemented.

The EIA prepared by ISPL for the City of Cockburn has been completed by:

- Undertaking a site visit;
- Completing a desktop assessment on the environmental values at Woodman Point with a particular focus on coastal habitat and use by Fairy Terns and other shorebirds;
- Identification of potential environmental impacts associated with the rezoning of the beach and completing a risk assessment to outline the likelihood and severity of these impacts occurring;
- Consultation with key stakeholders (Fairy Tern Network, Birdlife Australia, CCWA); and
- Provision of recommendations based on the risk assessment.

2 Environmental desktop review

As part of the EIA process, a desktop review was completed for the Woodman Point area to identify known and potential environmental values. These are described in the subsections below.

2.1 Woodman Point Regional Park

Woodman Point Regional Park is located approximately 9km south of Fremantle and 22km south of Perth in the City of Cockburn (Conservation Commission, 2010). The Park covers an area of 255ha and is situated on a narrow beach ridge plain extending to a peninsula (Conservation Commission, 2010). Woodman Point itself separates Owen Anchorage from Jervois Bay and marks the northern extent of the Cockburn Sound



(Conservation Commission, 2010). The Park can be accessed by vehicles via Cockburn Road. The long-term vision and strategy for the Park are:

Vision: "Woodman Point Regional Park will be a well-managed coastal park supporting and preserving species and habitat diversity in a sustainable manner. The Park will provide for the conservation and preservation of cultural heritage values, as well as providing for the recreational needs of the community, in a visually harmonious way" (Conservation Commission, 2010).

Strategy: "Manage the Park for conservation and allow recreation and other uses to occur to the extent that they do not adversely impact on other Park values." (Conservation Commission, 2010).

2.1.1 Landform

The landform of Woodman Point Regional Park is characterised by gently undulating coastal dunes and swales (Conservation Commission, 2010). The beach ridge characterising Woodman Point was formed from sand deposition over thousands of years (Conservation Commission, 2010). East of the coastline is the Tamala Limestone comprised of a superficial layer of limestone deposition over sand which form a series of ridge and valley systems representing former dunes (Conservation Commission, 2010).

2.1.2 Flora and vegetation

The floristic communities of the Swan Coastal Plain were classified by Gibson *et al* (1994); and the flora and vegetation of Woodman Point Regional Park represents coastal floristic communities of the Quindalup dune system which have remained relatively intact since European settlement (Conservation Commission, 2010). Five floristic communities occur within the Woodman Point Regional Park and are widely distributed across the Swan Coastal Plain:

- Coastal shrublands on shallow sands;
- *Acacia* shrublands on taller dunes;
- *Callitris preissii* (Rottneest Cyprus) and/or *Melaleuca lanceolata* forests and woodlands;
- *Eucalyptus gomphocephala* (Tuart) and/or *Agonis flexuosa* (Peppermint) woodlands; and
- Woodlands and shrublands on Holocene Dunes. (Conservation Commission, 2010; Gibson, et al., 1994).

2.1.3 Terrestrial fauna

The variety of vegetation communities provide a range of habitats for terrestrial fauna. There is a diverse species composition of avifauna ranging from woodland/bush birds, trans-equatorial migratory birds, waterbirds and shorebirds (Conservation Commission, 2010). Quenda are also known to occur within the Park along with a range of reptiles (Conservation Commission, 2010).

2.1.4 Marine and coastal areas

The coastline frames Woodman Point Regional Park and is a key feature of the Park's environmental and recreational values (Figure 2.1). The coastal vegetation, dune systems and sandy beaches are a good representation of the natural coastal environment pre-development in the Perth region. The coastal foreshore provides habitat for a diverse bird population including trans-migratory birds and breeding habitat for local shorebirds (Conservation Commission, 2010). Wind and waves are driving forces behind currents and sediment movement. Seasonal trends in wind and waves influence sand deposition on the beaches as well as erosion. Artificial groyne structures have started to lead to an accumulation of sediment on the northern side of the point while there is some evidence of erosion along the Jervoise Bay shoreline (Conservation Commission, 2010).



Figure 2.1 Woodman Point characteristic coastal features

2.1.5 Aboriginal and European Heritage

A search of the Aboriginal Heritage Inquiry System identifies Woodman Point (ID15841) as a registered Aboriginal site of mythical significance. This area was traditionally used for Cobbler fishing and mussel gathering. Local plants in the area such as the quandong (*Santalum acuminatum*) were used in jam making and also hold cultural significance (Conservation Commission, 2010).

Woodman Point is also considered historically important, as European settlers used this as a quarantine station for people disembarking ships in the late 1800's. The quarantine station was later used for troops during the First World War and later for migrants to fumigate their clothes and luggage in the 1950's (Conservation Commission, 2010). Ammunition stores were built to house explosives during the Second World War (Conservation Commission, 2010). The buildings which make up the quarantine station and magazine storage, a number of tuart trees near Cockburn Road, the Coogee Beach and Jetty and Ammunition Jetty are included in the City of Cockburn Municipal Inventory as Heritage Places (Conservation Commission, 2010).

2.1.6 Recreation and community use

Woodman Point provides a range of areas for community use and recreation. The beach between Ammunition Jetty and Woodman Point Headland includes beach access for swimming, snorkelling and sailing. The Woodman Point Recreation Camp conducts over 65,000 hours of structured physical activity programs primarily for children on school and community camps on behalf of its owner agency, the WA Department of Local Government, Sport and Cultural Industries. The beach in this area is heavily utilised by children attending camps at Woodman Point, both during structured and unstructured recreational activities. From Woodman Point Headland to the boating precinct, the beach and groyne can be accessed for fishing, swimming and off-leash dog exercise.





2.2 Shorebirds at Woodman Point

2.2.1 What are shorebirds?

Shorebirds or waders are a diverse group of birds associated with wetland and coastal habitats where they wade in shallow water and feed along the shore (DBCA, 2017). Shorebirds can be further divided into two groups – those which are resident and spend their entire life in Australia and those which are migratory coming to feeding grounds in Australia each year from September to March (Department of Environment and Climate Change, 2008). Most migratory shorebirds make an annual return journey of thousands of kilometres between breeding grounds in the northern hemisphere and non-breeding grounds in the southern hemisphere (Weller & Lee, 2017). Coastal, wetland and estuarine habitats in Australia provide important habitat for resident shorebird populations and also as a resting and feeding ground for migratory shorebirds (Weller & Lee, 2017). Figure 2.2 shows typical shorebird habitat at Woodman Point.

Seabirds differ from shorebirds in that they forage at sea, only coming ashore to roost and nest (DBCA, 2017). Shorebirds and seabirds can often be seen in flocks alongside each other (DBCA, 2017). Woodman Point also provides resting and roosting habitat for seabirds.



Figure 2.2 Shorebird habitat at Woodman Point

Common shorebirds of the Perth region and which have been known to utilise habitat at Woodman Point from regional database (NatureMap, 2020; Birddata, 2020; Urban Bushland Council, 2020) are:

- Pied Oystercatcher (*Haematopus longirostris*);
- Banded Stilt (*Cladorhynchus leucocephalus*);
- Banded Lapwing (*Vanellus tricolor*);
- Black Fronted Dotterel (*Elseya melanops*);
- Grey Plover (*Pluvialis squatarola*);
- Red-capped Plover (*Charadrius ruficapillus*); and
- Sooty Oystercatcher (*Haematopus fuliginosus*).

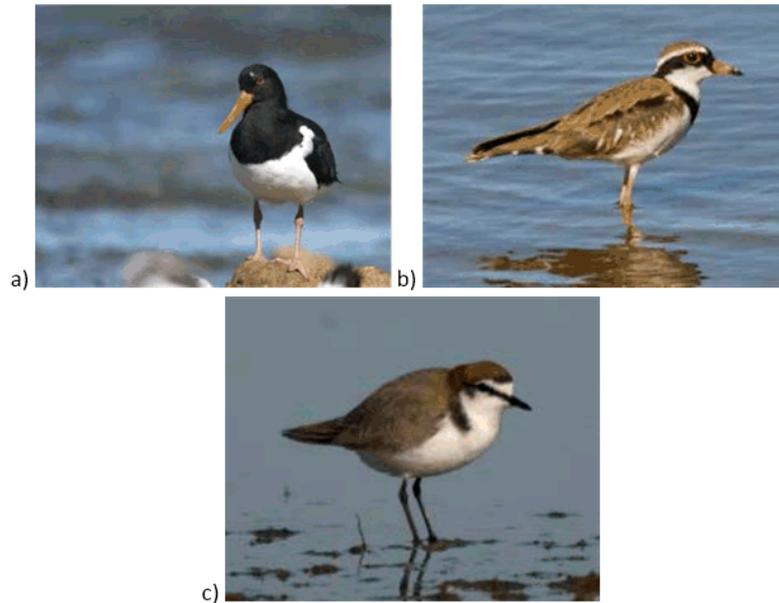


Figure 2.3 Common Shorebirds of Woodman Point – a) Pied Oystercatcher, b) Black Fronted Dotterel c) Red-Capped Plover (Perth Birds and Bush, 2020)

2.2.2 Beach nesting shorebirds

Many resident shorebirds spend their entire life cycle (foraging, roosting and breeding) along the coastlines and beaches of Australia. Beach nesting birds build their nests directly on beaches, estuaries, dunes or rocky shores above the high tide mark (Maguire, 2018; Maguire, 2008). Sand, shells and stones provide an ideal camouflage for eggs and mottled chicks (Maguire, 2008).

The key breeding habitat for beach nesting birds is also highly favoured by people for recreation, particular during the summer months which coincide with the breeding season of many beach nesting birds. These added pressures contribute to an already low breeding success rate (Maguire, 2008). Figure 2.4 shows how well eggs and chicks camouflage on the beach.



Figure 2.4 Typical example of the nest of a beach-nesting shorebird (Fairy Tern nest at Point Walter, Image: Corlette, 2020)





2.2.3 Conservation significant shorebirds of Woodman Point

A NatureMap report for Woodman point identified 131 birds with the potential to occur at Woodman Point, of these, 37 species are shorebirds or waterbirds (NatureMap, 2020). Many shorebirds are of conservation significance due to either their low populations and threats facing their persistence or their migratory status in Australia which is recognised under several international agreements (e.g. JAMBA, CAMBA, ROKAMBA¹).

Table 2.1 provides a list of conservation significant shorebirds and seabirds recorded at Woodman Point, or for which Woodman Point provides suitable habitat. The complete list from NatureMap and the Protected Matters Search Tool are provided as Appendix A. Species which have been recorded and lodged through Birdlife Australia's Birddata portal have also been recorded. It is important to note that Birddata also collates citizen science data and identifications may not have been verified by a fauna specialist.

Table 2.1. Conservation significant shorebirds and migratory birds (NatureMap, 2020; Birddata, 2020; DAWE, 2020)

| Species | Conservation Status | | Recorded (Birddata) |
|--|---------------------|--------------|---------------------|
| | WA | Commonwealth | |
| Eastern Curlew (<i>Numenius madagascariensis</i>) | Cr | Cr | X |
| Curlew Sandpiper (<i>Calidris ferruginea</i>) | Cr | Cr | X |
| Great Knot (<i>Calidris tenuirostris</i>) | Cr | Cr | X |
| Lesser Sand Plover (<i>Charadrius mongolus</i>) | En | En | X |
| Red Knot (<i>Calidris canutus</i>) | En | En / IA | X |
| Greater Sand Plover (<i>Charadrius leschenaultii</i>) | Vu | Vu / IA | X |
| Fairy Tern (<i>Sternula nereis nereis</i>) | Vu | Vu | X |
| Flesh-footed Shearwater (<i>Ardenna carneipes</i>) | Vu | IA | X |
| Hooded Plover (<i>Thinornis rubricollis</i>) | P4 | | X |
| Grey-tailed Tattler (<i>Tringa brevipes</i>) | P4 / MI | IA | X |
| Brown Skua (<i>Stercorarius antarcticus</i>) | P4 | | X |
| Common Sandpiper (<i>Actitis hypoleucos</i>) | MI | IA | X |
| Wedge-tailed Shearwater (<i>Ardenna pacifica</i>) | MI | IA | X |
| Ruddy Turnstone (<i>Arenaria interpres</i>) | MI | IA | X |
| Sanderling (<i>Calidris alba</i>) | MI | IA | X |
| Red-necked Stint (<i>Calidris ruficollis</i>) | MI | IA | X |
| Caspian Tern (<i>Hydroprogne caspia</i>) | MI | IA | X |
| Bar-tailed Godwit (<i>Limosa lapponica</i>) | MI | IA | X |
| Southern Giant Petrel (<i>Macronectes giganteus</i>) | MI | En / IA | X |
| Northern Giant Petrel (<i>Macronectes halli</i>) | MI | Vu / IA | X |
| Whimbrel (<i>Numenius phaeopus</i>) | MI | IA | X |
| Wilson's Storm-Petrel (<i>Oceanites oceanicus</i>) | MI | IA | X |
| Bridled Tern (<i>Onychoprion anaethetus</i>) | MI | IA | X |
| Pacific Golden Plover (<i>Pluvialis fulva</i>) | MI | IA | X |
| Grey Plover (<i>Pluvialis squatarola</i>) | MI | IA | X |
| Arctic jaeger (<i>Stercorarius parasiticus</i>) | MI | IA | X |
| Pomarine Jaeger (<i>Stercorarius pomarinus</i>) | MI | IA | X |
| Roseate Tern (<i>Sterna dougallii</i>) | MI | IA | X |
| Common Tern (<i>Sterna hirundo</i>) | MI | IA | X |
| Little Tern (<i>Sternula albifrons</i>) | MI | IA | |
| Crested Tern (<i>Thalasseus bergii</i>) | MI | IA | X |
| Common Greenshank (<i>Tringa nebularia</i>) | MI | IA | X |
| Marsh Sandpiper (<i>Tringa stagnatilis</i>) | MI | IA | X |
| Sharp-tailed Sandpiper (<i>Calidris acuminata</i>) | MI | IA | X |
| Terek Sandpiper (<i>Xenus cinereus</i>) | MI | IA | X |

¹ JAMBA – Japan-Australia Migratory Bird Agreement. CAMBA – China-Australia Migratory Bird Agreement. ROKAMBA – Republic of Korea-Australia Migratory Bird Agreement.

For more information see: <https://www.environment.gov.au/biodiversity/migratory-species/migratory-birds>



Cr – Critically endangered, En – Endangered, Vu – Vulnerable, P4 – Priority 4, MI – Migratory, IA – International Agreement

2.2.4 Australian Fairy Terns

The Australian Fairy Tern (*Sternula nereis nereis*, Figure 2.5) is endemic to Australia and nests in colonies located along shorelines, coastal lagoons and salt lakes with sparse vegetation and where their main prey, small schooling fish (such as whitebait and sardines), are naturally abundant (Greenwell, et al., 2019; Dunlop, 2018). The Fairy Tern is listed as Vulnerable under both the *Biodiversity Conservation Act 2016* (WA) and the *Environment Protection and Biodiversity Conservation Act 1999* (Cth).

In south-west Australia, Fairy Terns are partially migratory, spending winter months (June to August) along the north-west coast and establishing breeding colonies along the south-west coast from October to March (Dunlop, 2018). Peak breeding activity occurs from mid-November to early January (Dunlop, 2018).

Fairy Terns forage by plunge diving for small schooling fish within close proximity of nests (Dunlop, 2018). Habitat critical to the survival of the Fairy Tern is considered to include any habitat, within its range, where the species is known or likely to breed or forage (Commonwealth of Australia, 2019). The principal threats to Fairy Terns in south-western Australia are disturbances to breeding colonies and habitat loss (Dunlop, 2018).



Figure 2.5 Fairy Tern (*Sternula nereis nereis*) (image: C. Greenwell)

At Woodman Point, the stretch of beach between Woodman Point carpark and Cockburn Cement is of breeding significance for Fairy Terns (Figure 2.6). Fairy Terns are known to have used this area for breeding, however, the increase in human traffic, recreation and dog access have reduced breeding attempts and breeding success (CCWA Pers Comm, 2020; Birdlife Australia Pers Comm, 2020; Pers Comm C. Greenwell, 2020).

Fairy Terns are known to have attempted to nest in this location in November 1997, February 1998, December 1999, December 2007 and December 2009 (CCWA Pers Comm, 2020). In all cases eggs were destroyed by people or four-wheel drives and the breeding attempt was unsuccessful (CCWA Pers Comm, 2020). There has been no record of breeding activity by Fairy Terns in the last decade however, Fairy Terns are still regularly sighted at Woodman Point during the summer months (October to April) (CCWA Pers Comm, 2020). Figure 2.6 and Figure 2.7 present data from Birdlife Australia's Birdata Portal of Fairy Tern sightings at Woodman Point indicating that Fairy Terns are a regular visitor to the area (Birdata, 2020).



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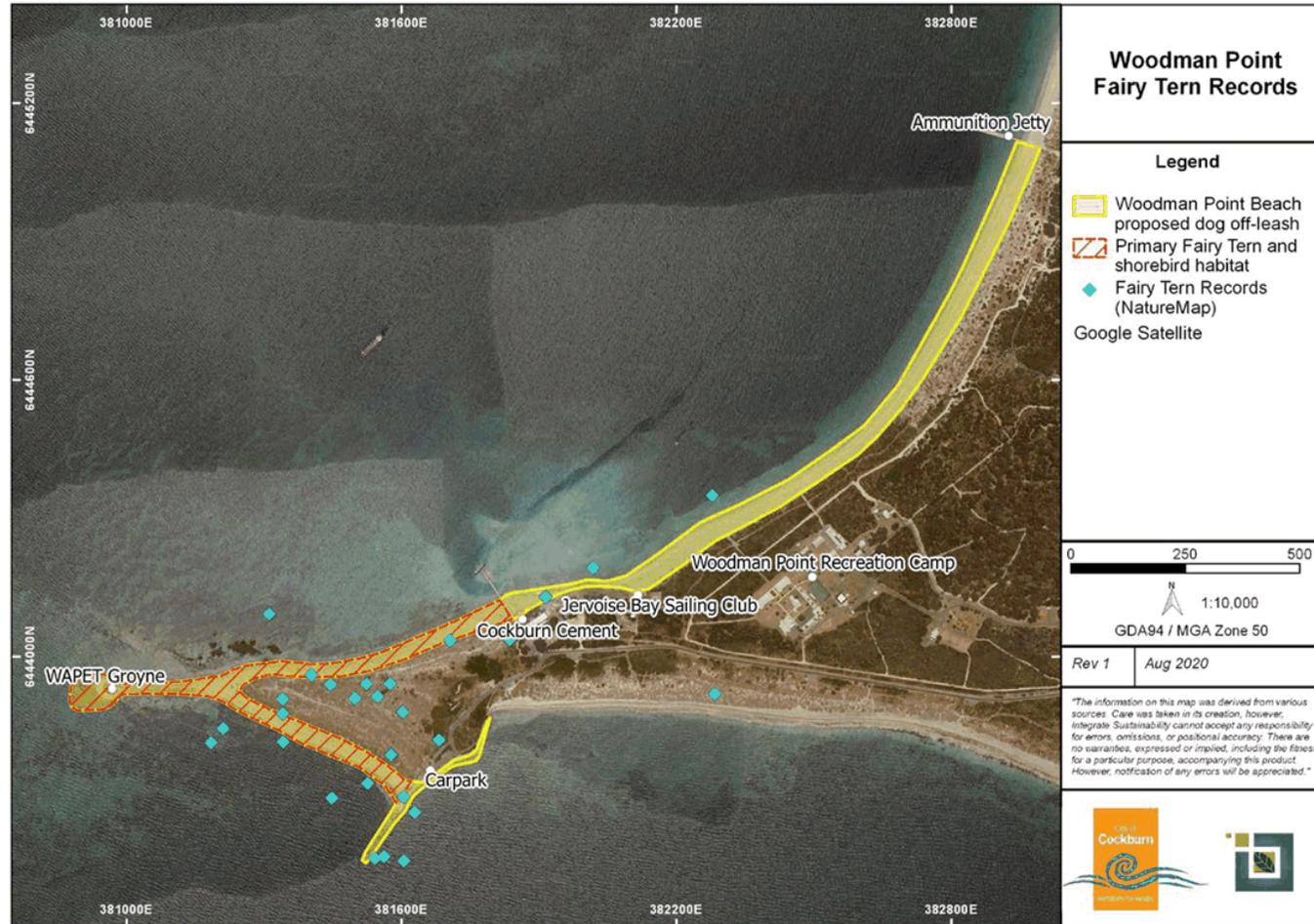


Figure 2.6 Fairy Tern (*Sternula nereis nereis*) records at Woodman Point (NatureMap, 2020)



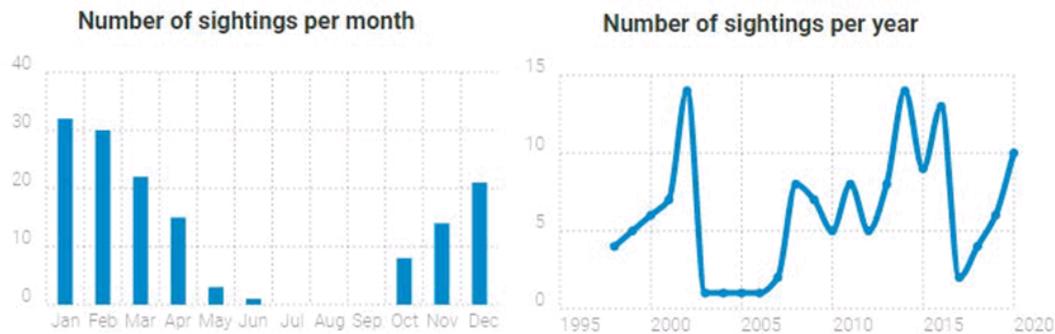


Figure 2.7 Fairy Tern sightings by month and year at Woodman Point (Birddata, 2020)

2.2.5 Other conservation significant shorebirds and migratory birds at Woodman Point

Table 2.1 highlights that other conservation significant shorebirds have also been recorded at Woodman Point and are likely to occur in the future. In particular, the following listed species have been recorded in recent years:

- Great Knot (*Calidris tenuirostris*, critically endangered);
- Curlew Sandpiper (*Calidris ferruginea*, critically endangered);
- Red Knot (*Calidris canutus*, endangered);
- Lesser Sand Plover (*Charadrius mongolus*, endangered); and
- Greater Sand Plover (*Charadrius leschenaultia*, vulnerable).

Other resident shorebirds like the Pied Oystercatcher, Sooty Oystercatcher, Hooded Plover and Red-capped Plover may also utilise the beach and coastal areas for nesting. Human and dog activity in the area has reduced the selection of this site as breeding grounds in recent years (Birdlife Australia Pers Comm, 2020).

2.2.6 Threats to shorebirds

One of the most significant threats to shorebirds is the loss of wetland and coastal habitat. On the Swan Coastal Plain up to 75% of wetlands have been filled or drained and coastal areas have been heavily developed (Weller & Lee, 2017). As described by Weller & Lee (2017), other threats to shorebirds and migratory shorebirds include:

- Habitat loss or modification of stop-over areas within migratory flight paths;
- Foraging habitat modification from weed infestations, pollution and altered hydrological regimes;
- Disturbance during foraging and roosting periods causing birds to move from an area at high energetic cost potentially compromising the birds ability to undertake a migratory flight; and
- Unsustainable harvesting of baitfish and other prey species.

In addition, beach nesting shorebirds face an additional suite of threats described by Maguire (2018) as including:

- Breeding habitat loss and modification from weed infestations, coastal developments, sea level rise and beach or dune erosion;
- Crushing of nests, eggs and chicks by people, horses and vehicles;
- Predation of eggs and chicks from cats, foxes, dogs and birds of prey; and
- Disturbance to nesting pairs leaving unattended eggs and chicks which can cause starvation of chicks and even abandonment of the breeding attempt.



Figure 2.8 shows dog and human activity along the stretch of beach at Woodman Point which is shorebird breeding habitat.

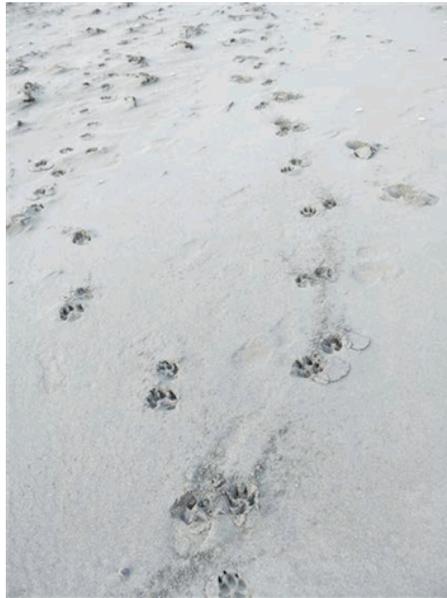


Figure 2.8 Evidence of dogs within shorebird habitat at Woodman Point

3 Environmental impact assessment

It is important to note that Fairy Terns and shorebirds have already been somewhat impacted by increased activity along this section of beach at Woodman Point. This environmental impact assessment has been completed to assess the further impacts to Fairy Terns and other shorebirds which utilise a 2.7km section of beach at Woodman Point under the following scenario:

- The 2.7km section of beach from Ammunition Jetty to the Carpark at the Woodman Point Headland will be zoned a 'dog off-leash' exercise area and will be closed to dogs during the Australian Fairy Tern (*Sternula nereis nereis*) nesting season which, for this purpose, is assumed to be the full extent of the months from October to March.

Consideration has also been given to impacts on public safety, operational activities/management and legal obligations under the above scenario.

The following risk matrix has been used to classify and rank the impacts. Definitions from Birdlife Australia risk assessments (Commonwealth of Australia, 2019; Weller & Lee, 2017) have been used or refined for the purpose of this risk assessment.



Table 3.1. Risk matrix

| | | Consequence | | |
|-------------|--|-------------|----------|--------|
| | | Minor | Moderate | Major |
| Likelihood | Very Likely | Medium | High | High |
| | Likely | Low | Medium | High |
| | Unlikely | Low | Low | Medium |
| Definitions | <p>Consequence:</p> <p><u>Minor</u> – Impacts will be short-term (0-6 months) and minor in scale on habitat; and abundance of Fairy Tern and other shorebirds remains the same or reduces temporarily.</p> <p><u>Moderate</u> – Impacts will have a medium-term effect (lasting up to 18 months) on Fairy Terns and other shorebirds, with localised loss/degradation of habitat and some displacement of fauna or decrease in shorebird abundance.</p> <p><u>Major</u> – Impacts will have a long-term effect (lasting 2 years or more) and widespread impact on habitat, and persistent decrease in Fairy Tern and other shorebird abundance.</p> <p>Likelihood:</p> <p><u>Unlikely</u> – event/impact has a little to nil chance of occurring.</p> <p><u>Likely</u> – event/impact has a relative chance of occurring.</p> <p><u>Very Likely</u> – event/impact will occur.</p> <p>Impact Rating:</p> <p><u>Low</u> – No or small impact to Fairy Terns and other shorebirds from the current baseline (e.g. habitat quality and/or area remains the same as the baseline, numbers of Fairy Terns and shorebirds using Woodman Point stays the same as the baseline).</p> <p><u>Medium</u> – Moderate impact to the Fairy Terns and other shorebirds from the current baseline (e.g. numbers of Fairy Terns and shorebirds using Woodman Point decreases, habitat quality and/or area has changed but can still be utilised for foraging and roosting, local population is reduced).</p> <p><u>High</u> – Significant impact to Fairy Terns and other shorebirds from the current baseline (e.g. Fairy Terns and shorebirds stop using Woodman Point, habitat is no longer conducive for foraging, roosting or breeding, loss of local populations).</p> | | | |





Table 3.2. Risk assessment

| Site | Value | Impact | Likelihood | Consequence | Rating | Comments |
|---|---|--|-------------|-------------|---|--|
| Woodman Point Beach South of Ammo Jetty – off-leash dog exercise with beach closure from October to March | <i>Fairy Terns and beach-nesting shorebirds</i> | | | | | |
| | Foraging and roosting. | Degradation of suitable habitat. | Likely | Moderate | Medium | <ul style="list-style-type: none"> Beach access during the winter months will lead to the degradation of habitat. |
| | | Birds are deterred from foraging and roosting at Woodman Point. | Likely | Major | High | |
| | Breeding habitat and breeding attempts. | Degradation of suitable habitat. | Likely | Moderate | Medium | <ul style="list-style-type: none"> People with dogs are likely to disregard signage and fencing. Beach closure during the summer months may be confusing as this is when people are more likely to frequent the beach. |
| | | Birds are deterred from nesting due to foot traffic along the beach or the presence of perceived threats (dogs, feral cats, people). | Likely | Major | High | |
| | | Predation or destruction of nests, eggs or chicks (from feral cats, dogs and people). | Likely | Major | High | |
| | | Disturbance from dogs and people (causing flight) during courtship and early nesting period deterring the breeding attempt. | Likely | Major | High | |
| | | Disturbance from people and unleashed dogs (causing flight) and abandonment of eggs / chicks. | Likely | Major | High | |
| | <i>Other shorebirds and migratory shorebirds</i> | | | | | |
| | Foraging and roosting habitat. | Degradation of suitable habitat. | Likely | Moderate | Medium | <ul style="list-style-type: none"> People with dogs are likely to disregard signage and fencing. Beach closure during the summer months may be confusing as this is when people are more likely to frequent the beach. |
| | | Disturbance from dogs and people (causing flight) of roosting and resting migratory birds. | Likely | Moderate | Medium | |
| | | Birds are deterred from roosting due to foot traffic along the beach / groyne or the presence of perceived threats (dogs, feral cats, people). | Unlikely | Major | Medium | |
| | <i>Community</i> | | | | | |
| | Public safety. | Incompatible recreation activities along the beach (off-leash dogs, children at Woodman Point Recreation Camp, sailing club). | Very Likely | Major | High | <ul style="list-style-type: none"> Community conflict. Potential liability for the City of Cockburn. |
| Dog attack. | | Likely | Major | High | | |
| Limited accessibility in case of an emergency. | | Likely | Major | High | | |
| Regulation. | Disregard for signs, fencing and other mechanisms | Very Likely | Moderate | High | <ul style="list-style-type: none"> Isolation and access to the | |





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| Site | Value | Impact | Likelihood | Consequence | Rating | Comments |
|-----------------------------------|---|---|-------------|-------------|--------|---|
| | | to limit or restrict access | | | | beach is limited and challenging to police |
| <i>Environment and Aesthetics</i> | | | | | | |
| | Environmental Approvals. | Non-compliance with EPBC Act. | Likely | Major | High | <ul style="list-style-type: none"> Rezoning may trigger 'controlled action' under the EPBC Act requiring referral and formal assessment. |
| | Woodman Point Regional Management Plan. | Rezoning incompatible with the vision and strategy of the Plan. | Very Likely | Minor | Medium | <ul style="list-style-type: none"> Misalignment. |
| | Aesthetics. | Increased litter including dog excrement and plastic bags. | Very Likely | Minor | Medium | <ul style="list-style-type: none"> Community may not recognise the conservation value of the area. |
| | | Increased use of the beach reduces the visual aesthetic and natural value to the community. | Likely | Minor | Low | |





3.1 Discussion of impacts

3.1.1 Impacts on Fairy Terns and beach nesting shorebirds

3.1.1.1 Habitat Degradation

Loss of habitat, in particular breeding habitat is one of the greatest threats to Fairy Terns and beach-nesting shorebirds (Commonwealth of Australia, 2019). Shorelines and coastal areas are also one of the most favoured locations for development and recreation which puts added pressure on beach-nesting species as suitable habitat may be lost or become subject to greater disturbance and predation (Commonwealth of Australia, 2019).

This can lead to a situation where Fairy Terns and other beach nesting birds are prevented from breeding successfully leading to the further decline of populations (Dunlop, 2018). Fairy Terns are known to have utilised Woodman Point for breeding habitat in the past and breeding attempts have been recorded in the 90's and early 2000's (CCWA Pers Comm, 2020). Increased foot traffic and access to the section of beach south of Ammunition Jetty will further reduce the likelihood of Fairy Terns selecting the site for breeding in the future.

3.1.1.2 Disturbance to breeding colonies

In this context, disturbance refers to the behaviour and physiological response of an animal to an anthropogenic stimulus (such as dogs and people) (Glover, et al., 2011). Disturbance to breeding colonies of Fairy Terns, particularly during the courtship, pre-laying and early incubation stages has been found to lead to the abandonment of the nesting attempt (Dunlop, 2018). This forces pairs into further sub-optimal or hazardous nesting locations and a lower chance of breeding success later in the season (Dunlop, 2018).

Domestic dogs are known to chase adult shorebirds and beach nesting birds which can impact the birds' ability to rest, seek food and can also lead to prolonged absences from eggs and chicks (Maguire, 2018). Chasing and unpredictable movement, proximity and speed of unrestrained dogs are traits that do not lead to shorebirds adapting to the presence of dogs (Maguire, 2018). Rather, those attributes promote increased sensitisation, enhanced response frequencies and response intensities, meaning shorebirds are more likely to take flight more often (Maguire, 2018).

In addition, research has also shown that walkers accompanied by dogs often evoke greater flight responses from shorebirds than walkers alone (Maguire, 2018). This suggests that dogs are seen by shorebirds as much more of a threat than people, as dogs are more likely to catch and kill them or their chicks (Maguire, 2018). The highest frequencies of shorebird nest absences are often in response to people accompanied by unleashed dogs, and is more than double that of people approaching alone or with a leashed dog (Maguire, 2018).

3.1.1.3 Predation

Predation on eggs and chicks is a major impact affecting Fairy Terns and other beach nesting shorebirds (Maguire, 2008; Maguire, 2018). Dogs, cats and foxes have all been attributed to shorebird egg and chick predation (Maguire, 2008; Williams, et al., 2009; Greenwell, et al., 2019). The presence of a high number of predators can also impact the feeding of chicks causing prolonged nest absences compromising survival rates (Maguire, 2008).

In Mandurah, a single cat was responsible for the depredation of a colony of nesting Fairy Terns (~220 birds) over several nights resulting in the abandonment of the colony and breeding attempt (Greenwell, et al., 2019). This demonstrates the importance of safe breeding habitat for Fairy Terns and beach nesting shorebirds to ensure the persistence of populations.





3.1.1.4 Management considerations

In conservation situations where habitat suitability, disturbance and predation are concerns for Fairy Tern populations and other beach nesting shorebirds; management measures have been successfully implemented to protect beach nesting shorebirds and improve breeding success. This includes enhancing the attractiveness of potential breeding sites through the application of additional shell material, signage with temporary fencing to restrict access, predator control programs and education to increase public awareness (Commonwealth of Australia, 2019; Department of Environment and Climate Change, 2008; Dunlop, 2018; Maguire, 2008).

At Rous Head in Fremantle, a Fairy Tern breeding sanctuary has been established with an artificial beach constructed from dredge spoil and shell material (Fremantle Ports, 2020), see Figure 3.1. Due to the location of the sanctuary within the Fremantle Port, the area has been fenced and access by people and dogs can be successfully managed (Fremantle Ports, 2020). This has seen successful breeding by Fairy Terns over the last six years since its establishment in 2013 (Fremantle Ports, 2020).

Further to this, the establishment of temporary fencing and signage at the Point Walter spit in the 2018 breeding season saw 70 pairs of breeding Fairy Terns utilise the area (Corlette, 2020). In the 2019 breeding season the fencing was moved 50m closer inland, increasing the area available for Fairy Terns and decreasing disturbance which saw 130 breeding pairs use the site (Corlette, 2020).



Figure 3.1 Fairy Tern sanctuary at Rous Head, Fremantle Port (Dunlop, 2018)

3.1.2 Impacts on migratory shorebirds

Migratory shorebirds rely on undisturbed feeding and roosting sites to build body condition to make their long migratory flights (Weller & Lee, 2017). Safe foraging and roosting habitat within coastal areas is vital for migratory shorebirds to rest, feed and accumulate the energy resource required to successfully migrate back to breeding grounds in the northern hemisphere (Weller & Lee, 2017).

Disturbance to migratory shorebirds from dogs, people and other stimulus imposes a high energetic cost, potentially compromising the ability for the birds to build sufficient energy reserves for migration (DBCA, 2018). Continued disturbance at a site can render the habitat as unsuitable (DBCA, 2018).



Studies have found the presence of people at foraging and roosting sites can reduce the time migratory shorebirds spend consuming prey and reduce the number of migratory shorebirds using the area; both of which can compromise the ability for migratory shorebirds to complete a migration (DBCA, 2018; Martin, et al., 2014).

3.1.3 Misalignment with Woodman Point Regional Management Plan

Under the Woodman Point Regional Management Plan (2010), the beach south from Ammunition Jetty and the Woodman Point Headland, is gazetted as a no-dog area. The environmental impacts of dogs to coastal wildlife utilising beaches across Australia has been well documented (Maguire, 2008; Dunlop, 2018; Maguire, 2018; Schlacher, et al., 2015). Allowing dog access, both leashed and off-leash, is a misalignment with this Plan. It would also compromise the Vision and Strategy of the Park which focuses on conservation and preservation of environmental values (Conservation Commission, 2010).

3.1.4 Public safety

Concerns have been raised on previous occasions about the safety of children that utilise the Woodman Point Recreation Camp (Pers Comm C.Beaton, 2020). The camp conducts over 65,000 hours of structured physical activity programs primarily for children on school and community camps, on behalf of its owner agency, the Department of Local Government, Sport and Cultural Industries. The camp has operated since 1982, utilising the former Quarantine Station which operated in the area from 1852 until 1979.

The beach in this area is heavily utilised by children attending camps at Woodman Point, both during structured and unstructured recreational activities. The presence of off-leash dogs on the beach used by camp participants adds a new risk to the safe conduct of these activities, without suitable controls within the camp's authority to reduce such a risk (Pers Comm C.Beaton, 2020).

Other beach users may also be at risk from dog attack. The beach is isolated which may adversely impact the ability of emergency vehicles to access the site should a dog attack or other emergency occur. This could impose legal liability issues for the City of Cockburn.

3.1.5 Operational Activities and Management

It is currently proposed that the off-leash dog exercise beach will be closed to dogs during the Fairy Tern nesting season between October to March each year during the 24-month trial period. This would allow dogs to access the beach for five months over the winter and be closed over summer. Closing the beach to dogs during the Fairy Tern nesting season coincides with peak beach use over the summer months, a time when most dog owners would have an expectation that they can access the beach. This is likely to be unpopular with dog owners.

3.1.5.1 Communication

Signage and advertising could be used to inform dog owners; however, it would be assumed that owners caught with their dog off-leash during the summer months would likely suggest that they were not aware of the restrictions. Signage may not always be seen and not everyone will notice or take heed of advertising or social media. Research completed by the City of Cockburn has also shown that there is a low compliance with dog leashing regulations with many dog owners allowing their dogs to be off-leash even when in on-leash areas (City of Cockburn, 2020). This is likely to be more common in isolated areas such as the beach south of Ammunition Jetty. The isolation of this beach also makes it difficult for Rangers to regulate and monitor given the limited access points to the beach. To enforce the seasonal closure more resources would need to be directed to the area which may not be available to adequately implement these measures (Pers Comm C.Beaton, 2020).





3.1.5.2 Litter

Increased beach usage, particularly with off-leash dog access, has the potential to increase litter along the stretch of beach south of Ammunition Jetty. Most dog owners do the right thing and collect their dogs waste and dispose of it properly. Unfortunately, there are dog owners who either do not collect their dogs waste or collect it and leave the bag on the beach as they are not prepared to carry it to the nearest bin. Access to bins on this section of the beach would be limited and bins would only be stationed at the entry points to the beach as access is already restricted. This means that more than 2.7km of beach would have no rubbish bins in place and limited patrols from Rangers to enforce the litter laws (Pers Comm C.Beaton, 2020).

3.1.6 Controlled Action under the EPBC Act

Under the EPBC Act, an action which is likely to have, will have or has a significant impact on a 'Matter of National Environmental Significance' may result in a "Controlled Action" requiring approval by the Federal Minister of the Environment (DoEE, 2013). A matter of national environmental significance includes species which are listed as nationally threatened and migratory species (DoEE, 2013). Under the EPBC Act, shorebirds which have been recorded at Woodman Point and are listed as threatened species include:

- Great Knot (*Calidris tenuirostris*, critically endangered);
- Curlew Sandpiper (*Calidris ferruginea*, critically endangered);
- Red Knot (*Calidris canutus*, endangered);
- Lesser Sand Plover (*Charadrius mongolus*, endangered);
- Greater Sand Plover (*Charadrius leschenaultia*, vulnerable); and
- Fairy Tern (*Sternula nereis nereis*, vulnerable).

A variety of migratory shorebirds also utilise and have been previously recorded at Woodman Point and fall into the category of 'Matters of National Environmental Significance' under the EPBC Act (see Table 2.1).

Within the significant impact criteria of the EPBC Act, an action is likely to have a 'significant impact' on a vulnerable species (such as Fairy Terns) if there is a real chance it will:

- Lead to a long-term decrease in the size of an important population of a species;
- Reduce the area of occupancy of an important population;
- Fragment an existing important population into two or more populations;
- Adversely affect habitat critical to the survival of a species;
- Disrupt the breeding cycle of an important population;
- Modify, destroy, remove or isolate or decrease the availability or quality of habitat to the extent that the species is likely to decline;
- Result in invasive species that are harmful to a vulnerable species becoming established in the vulnerable species' habitat;
- Introduce disease that may cause the species to decline; or
- Interfere substantially with the recovery of the species. (DoEE, 2013).

The proposed action (to zone the 2.7km section of beach from Ammunition Jetty to the Carpark at the Woodman Point Headland a 'dog off-leash' exercise area that is closed to dogs during the Fairy Tern nesting season from October to March) has the potential to adversely impact on Fairy Terns and other listed shorebirds. Due to increased development and human activity across coastal areas in the Perth region, there are now few sites that provide suitable habitat for resident and migratory shorebirds (Birdlife Australia Pers Comm, 2020). There is significant potential that a 'dog off-leash' exercise zone could "modify, destroy, remove or isolate or decrease the availability or quality of habitat to the extent that Fairy Terns, resident and





migratory shorebird species is likely to decline". This may trigger the need for referral to the Department of Agriculture, Water and the Environment for assessment under the EPBC Act.

Should the City of Cockburn decide not to refer the activity and choose to implement, there is the potential that:

- A member of the public could refer the activity resulting in an unplanned assessment process, prevent further action occurring until a decision is reached; or
- Civil or criminal penalties may apply for breaches of the EPBC Act including failure to refer an action. A member of the public can report the action if they believe it breaches the EPBC Act commencing an investigation.

4 Conclusions and recommendations

4.1 Conclusion

The risk assessment ranked potential impacts to Fairy Terns and other beach-nesting shorebirds as high. This primarily relates to habitat degradation, disturbance and predation which is likely to increase with use of the beach by people and dogs even if the beach is closed to dogs over the summer months. These impacts may trigger the need for referral to the Department of Agriculture, Water and the Environment for assessment under Part 9 of the EPBC Act.

Furthermore, the public safety impacts were ranked as high due to the isolation and incompatible recreational activities which occur along this stretch of beach.

In summary, allowing dog access to a 2.7km section of beach from Ammunition Jetty to the Carpark at the Woodman Point Headland with the exception of during the Fairy Tern (*Sternula nereis nereis*) nesting season between October to March, is unfeasible for the following reasons:

- Shorebird populations, including Fairy Terns, may be adversely impacted by:
 - Loss of breeding, roosting and foraging habitat;
 - Increased predation;
 - Increased disturbance rendering the site unsuitable;
- Fairy Tern nesting season and beach closure coincides with peak beach access periods over the summer months;
- Other shorebirds, including migratory species listed under the EPBC Act, also utilise this stretch of beach;
- There is significant potential that impacts will modify, destroy, remove or decrease the availability or quality of habitat for Fairy Terns, other shorebirds and migratory shorebirds and triggers the need to refer the action under the EPBC Act;
- Re-zoning is not in alignment with the Woodman Point Regional Park Management Plan;
- The area is isolated making it difficult to patrol and manage beach closures;
- Litter may become an issue along the beach and bins could only be installed and emptied at the entry point near the Ammunition Jetty;
- Closing the beach for the summer months could lead to confusion and be unpopular amongst dog owners; and
- The possibility of a dog attack would be increased if dogs were permitted off-leash increasing public safety issues and incompatible recreational activities with the Woodman Point Recreational Camp. Isolation is also a primary concern as access by emergency vehicles in the event of a severe dog attack or other emergency is limited.





4.2 Recommendation

Based on this assessment, it is recommended that the beach south of Ammunition Jetty to the Carpark at Woodman Point Headland be zoned as a 'no dog' area.

Having the beach designated as a 'no dog' area would provide a safe refuge for Fairy Terns, shorebirds and seabirds as well as give dog owners a clear understanding that the area is a 'no dog' area regardless of the season. In addition, it would minimise the need to allocate additional resources to police the area and to install and monitor rubbish bins as well as address public safety concerns around potential for dog attacks and the Woodman Point Recreational Camp. This recommendation is in line with the Woodman Point Regional Park Management Plan and the Vision and Strategy for Woodman Point documented within this Plan.

As outlined in Section 3.1.1.4, alternative management measures for further protection of Fairy Tern breeding habitat are available and have been successful when implemented at Rous Head and Point Walter.

The recommendation to make the 2.7km section of beach south of Ammunition Jetty a 'no-dog' area will assist with Fairy Tern conservation and protection of important breeding and foraging habitat for Fairy Terns and other resident and migratory shorebirds. It is likely to encourage Fairy Terns to return to the area for breeding.

Any further management measures to improve Fairy Tern breeding habitat at Woodman Point will also provide conservation benefits for other beach-nesting shorebirds including Red-Capped Plovers and Sooty Oystercatchers as well as migratory shorebirds.





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Appendix A – Baseline Data

A1 – NatureMap Report

A2 – Protected Matters Search Tool Report

A3 – Birdata Species List





Appendix B – Stakeholder Feedback Letters Re: Draft Animal Management and Exercise Plan

B1 – Birdlife Australia

B2 – Conservation Council of WA

B3 – Claire Greenwell





Appendix C – Figures





PETITION FORM

| |
|-----------------------|
| CITY OF COCKBURN |
| DOC Set |
| 26 AUG 2020 |
| SUBJECT 082/002 |
| RETENTION 25-17)30 |
| PROPERTY 3314396 |
| APP |

To: Chief Executive Officer
 City of Cockburn
 9 Coleville Crescent
 SPEARWOOD WA 6163

PO Box 1215
 BIBRA LAKE WA DC 6965
 ACTION 004/011
 FYI - DANIEL ARMSTRONG
 FYI - JO FIORI
 FYI - BERNIE PINTO

We, the undersigned electors of the City of Cockburn request (clearly define the purpose)

Dog exercise area at Bishop Park, Huxley Place Spearwood, to be re-zoned as a family friendly park. Dogs should not be allowed to roam freely off leads in close proximity to a children's playground area.

for the following reasons (provide summary of reasons supporting requests)

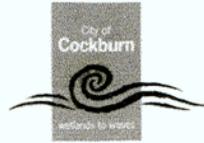
- To make park safe for families and children to access playground without fear of being injured by freely roaming dogs.
- Frequent movement of non local vehicles into quiet small cul-de-sac and parking on park lawn as there are no bollards
- Unprovoked "dog attacks" on local animals as per letter of complaint by initiator of petition lodged on 1st August 2020
- Disruption to native wildlife

As the initiator of this petition, my name is Foshko Katalina
 For further information I can be contacted via email maria_rosina_katal@hotmail.co
 or by telephone





| Name | Address | Signature |
|--------------------------|-----------------------------|--------------------|
| DINA BATTEL | 3 HUXLEY PLACE SPEARWOOD | <i>[Signature]</i> |
| C. BATTEL | 3 HUXLEY PLACE, SPEARWOOD. | <i>[Signature]</i> |
| M. BATTEL | 3 HUXLEY PLACE SPEARWOOD | <i>[Signature]</i> |
| J. GALLOWAY | 2 HUXLEY PLACE SPEARWOOD | <i>[Signature]</i> |
| M. Antonio | 7 Huxley Place, Spearwood | <i>[Signature]</i> |
| N. Antonio | 7 Huxley Place Spearwood | <i>[Signature]</i> |
| B. KURILJ | 6 HUXLEY PLACE SPEARWOOD | <i>[Signature]</i> |
| E. Gibson | 6 Huxley Place Spearwood | <i>[Signature]</i> |
| K. GREER | 10 HUXLEY PLACE " | <i>[Signature]</i> |
| J. BOZANICH | 12 HUXLEY PL. SPARWOOD | <i>[Signature]</i> |
| Judy Bozanič | 12 Huxley PL Spearwood. | <i>[Signature]</i> |
| A. VUKOVAC | 16 HUXLEY PLACE, SPEARWOOD | <i>[Signature]</i> |
| J. Vukovac | 16 HUXLEY PLACE | <i>[Signature]</i> |
| T. Vukovac | 2 AMALFI CREST YANKEEBUR | <i>[Signature]</i> |
| M. ALAJBEI | 22 HUXLEY PL SPEARWOOD | <i>[Signature]</i> |
| E. ALAJBEI | 22 HUXLEY PL SPEARWOOD | <i>[Signature]</i> |
| M. R. ... | 24 HUXLEY SPEARWOOD | <i>[Signature]</i> |
| Kelly Bombardieri | 28 Huxley Place Spearwood | <i>[Signature]</i> |
| | 17 MARVELL SPEARWOOD | <i>[Signature]</i> |
| ANTE JURICEV | 6 BOSWELL PLACE SPEARWOOD | <i>[Signature]</i> |
| Luce Juricev | 6 BOSWELL PL. SPEARWOOD | <i>[Signature]</i> |
| Monica Fistonich | 4 Boswell Place, Spearwood. | <i>[Signature]</i> |
| 13 MARVELL AV. SPEARWOOD | | <i>[Signature]</i> |
| LIZ SICON | 15 MARVELL AVE SPEARWOOD | <i>[Signature]</i> |
| LIZ CORREIA | 21 MARVELL AVE SPEARWOOD | <i>[Signature]</i> |



| Name | Address | Signature |
|------------------------|---|----------------------------|
| M. CORREIA | 21 MARVEL AVE | <i>M. Correia</i> |
| I MIHALJ | 26 MARVEL AVE | <i>I. Mihalj</i> |
| A. MIHALJ | 26 MARVEL AVE | <i>A. Mihalj</i> |
| Marija Gorcin | 24 Marvel HVE | <i>Marija Gorcin</i> |
| F. Carino | 34 Huxley Place | <i>F. Carino</i> |
| C. Carino | 34 Huxley Place | <i>C. Carino</i> |
| 2/8/20 Kitty | 38 Huxley Pl, Spearwood | <i>Kitty</i> |
| MARIA KATHALINIC | 5 Huxley Place SPEARWOOD | <i>M. Katalinic</i> |
| JOSKO KATALINIC | 5 Huxley Place SPEARWOOD | <i>Katalinic Josko</i> |
| IVAN ANDRIJIC | 14 HUXLEY PLACE SPEARWOOD | <i>Ivan Andrijic</i> |
| Mira and J. Zubovic | 14 HUXLEY PLACE SPEARWOOD and 12A LEAFSIDE WAY SPEARWOOD | <i>Mira and J. Zubovic</i> |
| C. Galloway | 2 Huxley Place Spearwood | <i>C. Galloway</i> |
| MARK THOMAS | 1 HUXLEY PLACE SPEARWOOD | <i>Mark Thomas</i> |
| Hubert Thomas | 1 Huxley Place Spearwood | <i>Hubert Thomas</i> |
| JOE CAPPELLI | 12 BARRETT STREET SPEARWOOD | <i>Joe Capelli</i> |
| GARY McWIMMER | BARRETT ST SPEARWOOD | <i>Gary McWimmer</i> |
| TOMMY VIDUOLIC | 12 BOVICI ST SPEARWOOD | <i>Tommy Vidolic</i> |
| Ashlee Bini | 14 Barrett street spearwood | <i>Ashlee Bini</i> |
| Beth Richard | 16 Barrett St Spearwood | <i>Beth Richard</i> |
| Judy Pigg | 29 Barrett St Spearwood | <i>Judy Pigg</i> |
| Olissa Fazio | 35 Barrett Street Spearwood | <i>Olissa Fazio</i> |
| Maria Soriente | 8 Buchan Spearwood | <i>Maria Soriente</i> |
| angela Camar | 6 Buchan close | <i>Angela Camar</i> |





| Name | Address | Signature |
|------------------|-------------------------------|-------------|
| John Camara | 6 Buchan close SPEARWOOD. | [Signature] |
| John Williams | 4 Buchan Ct Sp. | [Signature] |
| Ines BABIC | 2 Buchan cl Spw | Babic |
| Anika Lisman | 5 Buchan Close Spearwood | [Signature] |
| Christine Lisman | 5 Buchan Close Spearwood | [Signature] |
| BULLIANO | 21 THACKERAY ST SPEARWOOD | [Signature] |
| EUGENIA | 21 THACKERAY ST SPEARWOOD | [Signature] |
| T. DAURIZIO | 20 THACKERAY ST SPEARWOOD | [Signature] |
| F. DAURIZIO | 20 Thackeray Street Spearwood | [Signature] |
| NICK JAKOVICH | 18 THACKERAY ST SPEARWOOD | [Signature] |
| DAN JOKUSICH | 14 THACKERAY ST | [Signature] |
| ANIKKA JAKOVICH | 6 THACKERAY ST SPEARWOOD | [Signature] |
| VERINA BAVONE | 6 THACKERAY ST SPEARWOOD | [Signature] |
| Tiffany Harsy | 18 Buchan close | [Signature] |
| J Sulentic | 16 Buchan Close | [Signature] |
| MARIL KATAJNE | 5 Huxley place Spearwood | [Signature] |
| Felicija MORA | 29 NEWTON RD SP. | [Signature] |
| M. LA MACCHA | 70 Newton St Spearwood | [Signature] |
| N. JERKOVIC | 70 NEWTON ST SP | [Signature] |
| CC. macho | 51 Newton Street Spearwood | [Signature] |
| N. CAVLINA | 314A ROCKINGHAM RD. SPEARWOOD | [Signature] |
| A. Cavlina | 314A Rockingham Rd. Spearwood | [Signature] |
| C. Katalinic | 4 Crizen Way, Lake Woogel. | [Signature] |
| A. BIZZANTINO | 5 Huxley Place Spearwood | [Signature] |
| Rose AMARA | 19 MARQUEL AVIE SPEARWOOD | [Signature] |

Josko & Maria Katalinic
5 Huxley Place
Spearwood, WA 6163
0422307735
maria_rosina_katal@hotmail.com
30th July 2020

Cockburn City Council
9 Coleville Crescent
Spearwood WA 6163
08 9411 3444
cusotmer@cockburn.wa.gov.au

Dear Cockburn City Council,

I am writing to express my concern regarding the Exercise Dog Area at Bishop Park in Spearwood. At approximately 1:30 pm on Wednesday the 29th July 2020, two dogs brutally attacked and killed our 16 year old family pet cat, 'Mitsy' at the front of our house whilst she was sleeping. We can be grateful and consider ourselves lucky as only fifteen minutes earlier our 3 year old grandson Jake was playing at the front our house with her and could have been attacked by the two dogs and ended in a tragedy.

I'm extremely distressed by what happened and feel I need to take the necessary steps to ensure this does not happen again. I am aware that Bishop Park has been zoned an Exercise Dog Park but this is with the rule that 'If you are using an off-lead dog exercise area, your dog is under control at all times to prevent conflict with other dogs or people' and, 'It is the owners' responsibility to ensure the dog collar is worn and a lead is available' as documented on the Cockburn City Council website <https://www.cockburn.wa.gov.au/DogExercise>.

As a resident of the area I have witnessed people arriving in their cars throughout the day and release their dogs to roam free in the park without collars, leads or control of their owners. There are elderly people in our neighbourhood who are afraid to walk in our park. The park has an ungated children's playground area. We have used this in the past with our grandchildren and on many occasions have had to turn back home when going for a walk of fear that they may be injured by dogs freely roaming the area.

Please we need some serious action to change this park to a family friendly park not a "Dog Exercise Park" as it is located in a small neighbourhood with children's playground equipment. We are left traumatized and distressed especially my wife who witnessed the whole attack on our beloved Mitsy. This sort of dog behaviour has to be stopped and the owner punished. I have already reported and given the statement with the Cockburn City Council Ranger regarding the incident.

Attached is a petition with residents in our neighbourhood in support of re- zoning Bishop Park as a non-exercise dog park area.

Sincerely,

Josko and Maria Katalinic

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓ ↓ 30/7/2020 ↓

| Name | Address | Phone | Signature |
|---------------------|-------------------|--------------|--------------------------|
| ① WENDY GLANCE | 40A WESTERWAY TCE | 0410654808 | <i>W Glance</i> |
| ② JOHN GIALLORENZO | 44 Westerway Tce | 0417 033 844 | <i>J. Giallorenzo</i> |
| ③ ANGIE GIALLORENZO | 44 Westerway Tce | 0438 310 935 | <i>Angie Giallorenzo</i> |
| ④ SONIA Mc Coy | 62 ALLENDALE ENT | 0423 623 955 | <i>Sonia</i> |
| ⑤ Heil Gill | 37 Westerway Tce | 0421 468 494 | <i>Heil Gill</i> |
| ⑥ Joliti Gill | 37 Westerway Tce | 0402 400 173 | <i>Joliti</i> |
| ⑦ Elizabeth Norrish | 35 Westerway Tce | 0423 491 912 | <i>Elizabeth</i> |
| ⑧ Winterlands | 31 Westerway Tce | 0418 313 553 | <i>Winterlands</i> |
| ⑨ Hannah Lambert | 31 Westerway Tce | 0418 433 511 | <i>Hannah Lambert</i> |
| ⑩ Teremy Lee | 17 Westerway Tce | 0401 761 968 | <i>Teremy Lee</i> |
| ⑪ Lucy Reeve | 7 WESTERWAY TCE | 0405 632 329 | <i>Lucy Reeve</i> |
| ⑫ Gian Rove | 7 WESTERWAY TCE | 0405 58 223 | <i>Gian Rove</i> |
| ⑬ Steve Hill | 1 Westerway Tce | 0448 555 513 | <i>Steve Hill</i> |
| ⑭ Helen Hill | 1 Westerway Tce | 04100 50 253 | <i>Helen Hill</i> |

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓

↓ 30/7/2020 ↓

| Name | Address | Phone | Signature |
|-----------------------|-------------------|--------------|-----------|
| 15 Joan Bler | 14 Westerway Tce | 0450 445161 | |
| 16 Gio Ryan | 16 Westerway Tce | 0414 763385 | |
| 17 Natalie Car | 22 Westerway Tce | 0433 202155 | |
| 18 Gareth Nightingale | 30 Westerway Tce | 0417 424160 | |
| 19 Marie McDermott | 32 Westerway Tce | 0403 902523 | |
| 20 Steve McDermott | " " | " " | |
| 21 Michael Lacovic | 34 Westerway | 6400 344635 | |
| 22 Teresa Lacovic | 34 Westerway Tce | 0400 212 847 | |
| 23 WUSE PAUS. | 30 WESTERWAY TCE | 0438 35543 | |
| 24 Mick Fairbairn | 36 WESTERWAY TCE | 0418 937790 | |
| 25 Connor Fairbairn | 36 Westerway TCE | 0455 882780 | |
| 26 Jayden Fairbairn | 36 Westerway TCE | 0427 782820 | |
| 27 Olivia McCarthy | 36 WESTERWAY TCE | 0435 05 9691 | |
| 28 Stephanie Day | 38A Westerway Tce | 0424 567683 | |
| 29 Shaniqa Lin | 38A Westerway Tce | 0439 934552 | |



Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓ ↓ 29/7/2020 ↓

| Name | Address | Phone | Signature |
|------------------|--------------------------------------|--------------|----------------------|
| 30 Tina Kendall | North lake 10 Cheshunt Gardens | 0433833450 | <i>Tina Kendall</i> |
| 31 Margaret | 12 Cheshunt Gardens | 93355777 | <i>Margaret</i> |
| 32 Mel Fialco | 22 Cheshunt Gardens Murdoch Chase | 9439361397 | <i>Mel Fialco</i> |
| 33 S. Carmichael | 15 Cheshunt Cdr | 0434239888 | <i>S. Carmichael</i> |
| 34 Jill Reid | 13 Cheshunt Gdns | 0438319347 | <i>Jill Reid</i> |
| 35 Ben Reid | 13 Cheshunt Gdns | 0415520669 | <i>Ben Reid</i> |
| 36 Andrew Ross | 7 CHESHUNT GDNS | 0421 871 461 | <i>A Ross</i> |
| 37 YONNA ROSS | 7 CHESHUNT GDNS | 0421522679 | <i>Yonna Ross</i> |
| 38 K. LIE | 5A — — | — | <i>K. Lie</i> |
| 39 James Browne | 3 CHESHUNT | 0466 994616 | <i>James Browne</i> |
| 40 Katalin Dobos | 3 Cheshunt Gdns | 0424085953 | <i>Katalin Dobos</i> |
| 41 Teck Goh | 6 Westerway Way | 0421 891215 | <i>T. Goh</i> |
| 42 Yin Sito | — — | 0411 649 275 | <i>Yin Sito</i> |
| 43 KEVIN CANTON | 10 WESTERWAY TERRACE | 0417926543 | <i>Kevin Canton</i> |
| 44 Helen Brown | 12 Westerway Terrace | 0409 114 781 | <i>Helen Brown</i> |

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓

21/1/2020 ↓

| Name | Address | Phone | Signature |
|------------------------|------------------------------------|--------------|-------------------|
| 45) Trent O'Neil | 34 Peterborough Circle Northlake | 0413766270 | <i>Trent</i> |
| 46) Sunny Gao | 36B Peterborough Circle North Lake | 0451 117 985 | <i>Sunny</i> |
| 47) Ben Neenan | 38 Peterborough Cir Northlake | 0433 700435 | <i>Ben</i> |
| 48) Frank De Luca | 42 Peterborough Cir North Lake | 0432740327 | <i>Frank</i> |
| 49) Anne Marie De Luca | 42 Peterborough Cir North Lake | 0411281300 | <i>Anne Marie</i> |
| 50) Bruno Giallorenzo | 46 Peterborough | 0422313736 | <i>Bruno</i> |
| 51) Michelle Cheah | 48 Peterborough Circle North Lake | 0439393138 | <i>Michelle</i> |
| 52) Paul Cheah | " (as above) | " (as above) | <i>Paul</i> |
| 53) Daniel Wong | 56 Peterborough Cr North Lake | 0402 944 028 | <i>Daniel</i> |
| 54) JOE SCATA | 58 PETERBOROUGH CIRCLE | 0459 326 716 | <i>Joe</i> |
| 55) Pravin Paul | 60 Peterborough Cr. | 0401 259 630 | <i>Pravin</i> |
| 56) PRIYA ANN | 60 Peterborough Cr. | 0413376184 | <i>Priya</i> |
| 57) SUCHITA PRAVIN | 60 Peterborough Cir | 0434132257 | <i>Suchita</i> |
| 58) Rya Smith | 62 Peterborough Cir | 0419872451 | <i>Rya</i> |



Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓

25/7/2020 ↓

| Name | Address | Phone | Signature |
|-------------------|------------------------------------|--------------|-----------|
| 59) GAVIN JONES | 7 PETERBOROUGH CIRCLE NORTH LAKE | 0414 946 691 | |
| 60) JANE JONES | 7 PETERBOROUGH CIRCLE NORTH LAKE | 0414 946 683 | |
| 61) DIANA EXTN | 5 Peterborough Circle North Lake | 0417 908 990 | |
| 62) Kath Wills | 1 Peterborough Circle N/Lake | 0409 201 542 | |
| 63) LUCAS BROWNE | 8 PETERBOROUGH CIRCLE N/LAKE | 0459 207 92 | |
| 64) JASAL LAI | 18 PETERBOROUGH CIRCLE N/LAKE | 0400 211 171 | |
| 65) Julian Tan | 22 Peterborough Circle, North Lake | 0412 166 300 | |
| 66) GEOFF SMITH | 27 PETERBOROUGH CIR N/LAKE | 0438 377 098 | |
| 67) TERRY RIBWOOD | 59 PETERBOROUGH CIRCLE | 0403 040 952 | |
| 68) DENIS ALLEN | 35 PETERBOROUGH CIRCLE | 0427 579 036 | |
| 69) Sunny Lee | 26, PETERBOROUGH CIRCLE | 04 30533898 | |
| 70) Angela Gitero | 28 Peterborough Circle | 0405456201 | |
| 71) Sam Kapke | 30 Peterborough Cir | 0421 461 375 | |
| 72) Kate Farrell | 32 Peterborough Cir | 0412 171 301 | |
| 73) KENTU FARREK | 32 PETERBOROUGH C | 0455660027 | |

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

(North Lake)

25/7/2020

| Name | Address | Phone | Signature |
|------------------|----------------------------|-----------------------|--------------------------|
| 73 Bev+Graeme | 69 Peterborough Circle | 0400365386 | Bev |
| 75 James Lo | 57 Peterborough Circle | 0430283990 | James |
| 76 Annix Huang | " | 0421234679 | Annix |
| 77 David | 55 PETERBOROUGH | 0404114146515 | David |
| 78 Carolyn Lee | 51 Peterborough | 0412300400 | Carolyn |
| 9 | 43 Peterborough | 0422222251 | Eric Campbell |
| 79 Eric Campbell | 43 Peterborough | 0462914758 | Eric |
| 80 Ken Campbell | 43 Peterborough | 0415787561 | Ken |
| 81 Max Zhang | 23 Peterborough | 0430505278 | Max |
| 82 Zoe Clark | 21 Peterborough | 0413366763 | Zoe |
| 83 Nick Baker | 19 Peterborough | 0448073390 | Nick |
| 84 Kenneth Baker | 19 Peterborough | 0425541146 | Kenneth |
| 85 Helen Le | 19 Peterborough Cir | 0479173949 | Helen |
| 86 Simon Wong | 15 Peterborough Cir | 0406477888 | Simon |
| 87 Greg Barnes | 9 Peterborough Circle | 0414443566 | Greg |



Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

| Name | Address | Phone | Date | Signature |
|---------------------|------------------------|------------|-----------|-------------|
| (88) Michael Ngea | 95 Peterborough Cl | 0433314408 | 25/7/2020 | M Ngea |
| (89) Russel Stewart | 6 MAYBERRY Crt | 9332-8504 | 25/7/2020 | R Stewart |
| (90) Dean Stewart | 6 Mayberry Crt | 0409375884 | 25/7 | D Stewart |
| (91) JOHN BONG | 8 MAYBERRY CT | 0411236330 | 25/7/2020 | J Bong |
| (92) Vicki Powell | 10 MARGERY CT | 0423151179 | 25/7 | V Powell |
| (93) Ricardina Sugi | 14 Mayberry Crt | 0410633349 | 25/7 | R Sugi |
| (94) Kanchana & Saj | 91 Peterborough Circle | 0421026770 | 25/7 | K & S |
| (95) DAMIR ANDRIJK | 85 PETERBOROUGH CIRCLE | 0417084140 | 25/7 | D Andrijk |
| (96) MARIJA ANDRIJK | 85 PETERBOROUGH CIRCLE | 0407993553 | 25/7 | M Andrijk |
| (97) Johanna Smith | 62 Peterborough Circle | 0406096962 | 25/7 | J Smith |
| (98) Jomy Zachariuk | 83 Peterborough circle | 0450647762 | 25/7 | J Zachariuk |
| (99) Sheya Vangher | " | " | 25/7 | S Vangher |
| (100) Vivian Wong | 77 Peterborough Circle | 0403850389 | 25/7 | V Wong |
| (101) William | 71 Peterborough Crt | 0403227828 | 25/7 | W |
| (102) Garam Isob | 69 Peterborough | 0437797945 | 25/7 | G Isob |

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

29/7/2020 ↓

| Name | Address | Phone | Signature |
|---------------------------|------------------------------------|-----------------------|------------------------|
| 103 IVIS NG | 89 Peterborough Circle | 0418903683 | [Signature] |
| Kathryn Dalley | 72 Peterborough Circle. | 0414545457 | [Signature] |
| Isabel Dalley | " | " | [Signature] |
| 104 SYLVIA BETTS-HEINE | 76 Peterborough Circle, North Lake | 0407 424 987 | [Signature] |
| 105 JACK MOIR | 88 PETERBOROUGH | 0437511200 | [Signature] |
| 106 Lyndie Trewean | 2 Scottsdale Ct | 6180 1548 | [Signature] |
| 107 Phil Trewean | 2 Scottsdale Ct North Lake | 6180 1548 | [Signature] |
| 108 Elsy Gil | 4 Scottsdale | 0435554510 | [Signature] |
| 109 NEIL JACKSON | " | " | [Signature] |
| 110 LINDA ANTONKICH | 12 Scottsdale Cvt | 0893121953 | [Signature] |
| 111 Jobb Wong | 107 Peterborough Circle | 0415264005 | [Signature] |
| 112 SEBASTIAN | PETERBOROUGH 74 Circle | 0424799976 | [Signature] |
| 113 SOTOS | ROSSIDES | 2 GRANTON GRN | [Signature] |
| 114 MIKE | DESZOVICH | 4 GRANTON GRN | [Signature] |
| 115 Maddy/Stephen | 2 Keltan Ave | 0414317250 | [Signature] |



Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓

6/28/17/2020 ↓

| Name | Address | Phone | Signature |
|----------------------|------------------|-------------|--------------------|
| 116) Bev Flintoff | 6 Felton | 9332 5582 | <i>[Signature]</i> |
| 117) B Cioulis | 8 Felton Ave | 0400243 782 | <i>[Signature]</i> |
| 118) T. Hodgkin | 10 Felton | 0413923913 | <i>[Signature]</i> |
| 119) Todd Hodgkin | 10 Felton Ave | 0447328655 | <i>[Signature]</i> |
| 120) Richard Cheung | 97 Peterboro Cir | 9313 8593 | <i>[Signature]</i> |
| 121) D. Kapachia | 22 Allendale Ent | 0421 897679 | <i>[Signature]</i> |
| 122) Shana Wang | 20 Allendale Ent | 0401939350 | <i>[Signature]</i> |
| 123) Nancy Forjilio | 19 Allendale Ent | 0419830677 | <i>[Signature]</i> |
| 124) Helen Todd | 18 Allendale ent | - | <i>[Signature]</i> |
| 125) STEVEN HEWEL | 16 Allendale Ent | 0449976889 | <i>[Signature]</i> |
| 126) Res strong | 3 Allendale ent. | 0411 884534 | <i>[Signature]</i> |
| 127) Bev Pusitz | 7 Allendale Ent | 0400231862 | <i>[Signature]</i> |
| 128) Andy Castel. | 7 Allendale Ent | 0457942463 | <i>[Signature]</i> |
| 129) Barbara Oston | 9 Allendale ent | 0432 959791 | B. Oston |
| 130) Takako Iwahashi | 8 Chestnut Glen | 0401 971173 | Takako |

Petition to Support: Request for a dog friendly, off the leash area (for controlled dogs) at the top/upper section of Allendale Park in Murdoch Chase Estate (North Lake). Lower flat area at bottom of hill near playground to remain dog on leash.

↓ North Lake ↓

↓ 28/7/2020 ↓

| Name | Address | Phone | Signature |
|------------------------|---|------------|-------------------------|
| (131) Sheree Kendall | 10 Chestnut Gardens North Lake | 0413775581 | <i>Sheree Kendall</i> |
| (132) Chbe Kendall | " " | 0413775582 | <i>Chbe Kendall</i> |
| (133) Karan Zilisthane | 25 Peterborough Circle, North Lake | 0438864461 | <i>Karan Zilisthane</i> |
| (134) Samantha Harwood | 1 Peterborough Circle North Lake WA 6163 | 0411546424 | <i>Samantha Harwood</i> |
| (135) Dan Mulcahy | 11 Chestnut Gardens 6163 | 0451102158 | <i>Dan Mulcahy</i> |
| (136) VICKI LAMBERT | 11 CHESTNUT GARDNS 6163 | 0410084615 | <i>Vicki Lambert</i> |
| (137) Samuel Edge | 20 Allendale Int. 6163 | 0401720775 | <i>Samuel Edge</i> |
| \$ | | | |
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Project Report

30 July 2020 - 26 August 2020

Comment on Cockburn

Draft Animal Management & Exercise Plan 2020-2025



Visitors Summary



Highlights

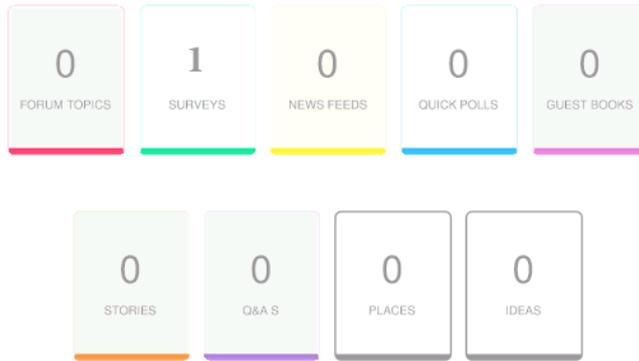
| | | |
|-------------------|----------------------|----------------|
| TOTAL VISITS | MAX VISITORS PER DAY | |
| 4.5 k | 583 | |
| NEW REGISTRATIONS | | |
| 264 | | |
| ENGAGED VISITORS | INFORMED VISITORS | AWARE VISITORS |
| 503 | 2 k | 3.2 k |

| | | | | | |
|---------------------------------|--------------|-----------------------------|------------|------------|-----------|
| Aware Participants | 3,188 | Engaged Participants | 503 | | |
| Aware Actions Performed | Participants | Engaged Actions Performed | Registered | Unverified | Anonymous |
| Visited a Project or Tool Page | 3,188 | Contributed on Forums | 0 | 0 | 0 |
| Informed Participants | 1,975 | Participated in Surveys | 503 | 0 | 0 |
| Informed Actions Performed | Participants | Contributed to Newsfeeds | 0 | 0 | 0 |
| Viewed a video | 0 | Participated in Quick Polls | 0 | 0 | 0 |
| Viewed a photo | 0 | Posted on Guestbooks | 0 | 0 | 0 |
| Downloaded a document | 1,250 | Contributed to Stories | 0 | 0 | 0 |
| Visited the Key Dates page | 0 | Asked Questions | 0 | 0 | 0 |
| Visited an FAQ list Page | 164 | Placed Pins on Places | 0 | 0 | 0 |
| Visited Instagram Page | 0 | Contributed to Ideas | 0 | 0 | 0 |
| Visited Multiple Project Pages | 990 | | | | |
| Contributed to a tool (engaged) | 503 | | | | |



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

ENGAGEMENT TOOLS SUMMARY

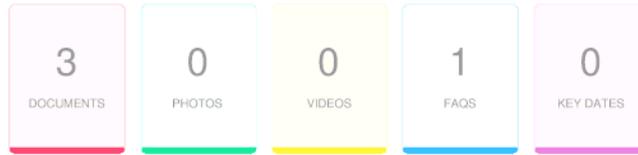


| Tool Type | Engagement Tool Name | Tool Status | Visitors | Contributors | | |
|-------------|----------------------|-------------|----------|--------------|------------|-----------|
| | | | | Registered | Unverified | Anonymous |
| Survey Tool | Comment Form | Archived | 1257 | 501 | 0 | 0 |



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

INFORMATION WIDGET SUMMARY



| Widget Type | Engagement Tool Name | Visitors | Views/Downloads |
|-------------|---|----------|-----------------|
| Document | DRAFT - Animal Management and Exercise Plan | 1027 | 1569 |
| Document | Fact Sheet | 271 | 435 |
| Document | Hard Copy Survey.pdf | 69 | 95 |
| Document | deleted document from | 1 | 1 |
| Faqs | faqs | 164 | 178 |



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

ENGAGEMENT TOOL: SURVEY TOOL

Comment Form

| | | |
|----------------------|-------------------------|--------------------------|
| Visitors 1257 | Contributors 501 | CONTRIBUTIONS 578 |
|----------------------|-------------------------|--------------------------|

Objective 1 of the draft Plan is to encourage responsible dog ownership. Do you support or oppose the following strategies:



Optional question (577 response(s), 0 skipped)

Question type: Likert Question

Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Objective 2 of the draft Plan is to provide sufficient, safe spaces for dogs, people and wildlife. Do you support or oppose ...



Optional question (575 response(s), 2 skipped)

Question type: Likert Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Objective 3 of the draft Plan is to manage feral and domestic cats. Do you support or oppose the following strategies:



Optional question (574 response(s), 3 skipped)

Question type: Likert Question

Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Objective 4 of the draft Plan is to embrace smart city (digital technology) initiatives in animal management. Do you support ...



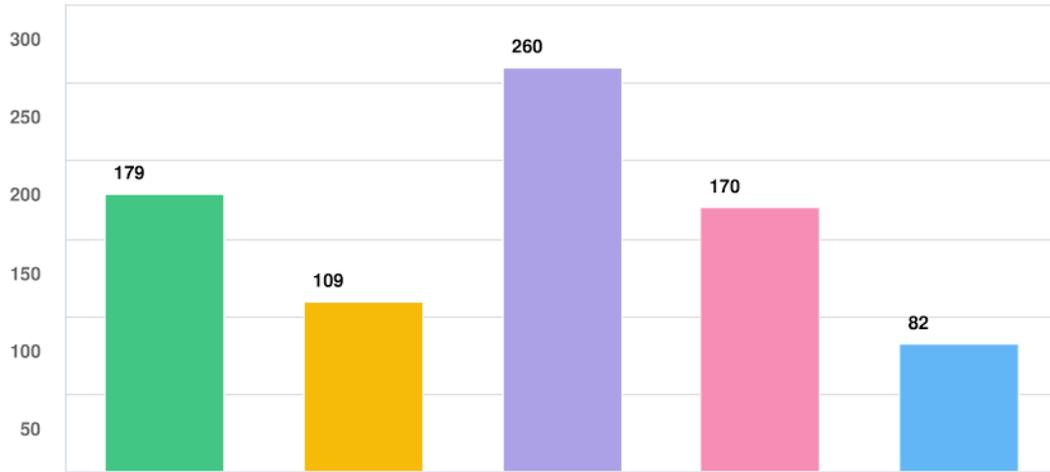
Optional question (574 response(s), 3 skipped)

Question type: Likert Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Which of the following apply to you?



Question options

- None of the above
- Previous Dog Owner
- Dog Owner
- Previous Cat Owner
- Cat Owner

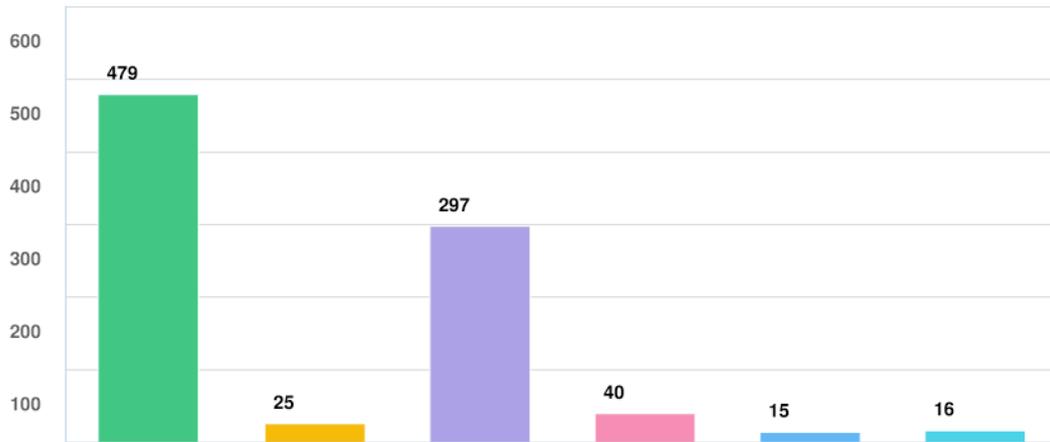
Optional question (572 response(s), 5 skipped)

Question type: Checkbox Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

What is your relationship to the City of Cockburn?



Question options

- Other (please specify)
- City of Cockburn Staff
- Visitor / work in the area
- Ratepayer
- Business owner or manager
- Resident

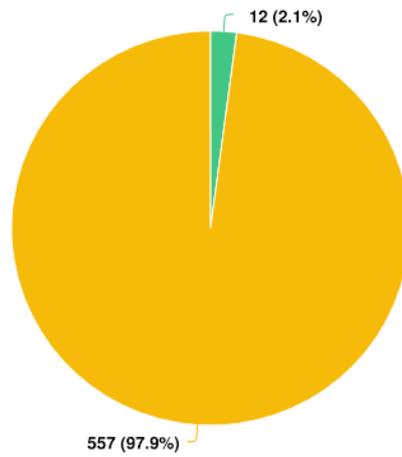
Optional question (574 response(s), 3 skipped)

Question type: Checkbox Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Are you completing this survey on behalf of a club, group or organisation?



Question options

- No
- Yes

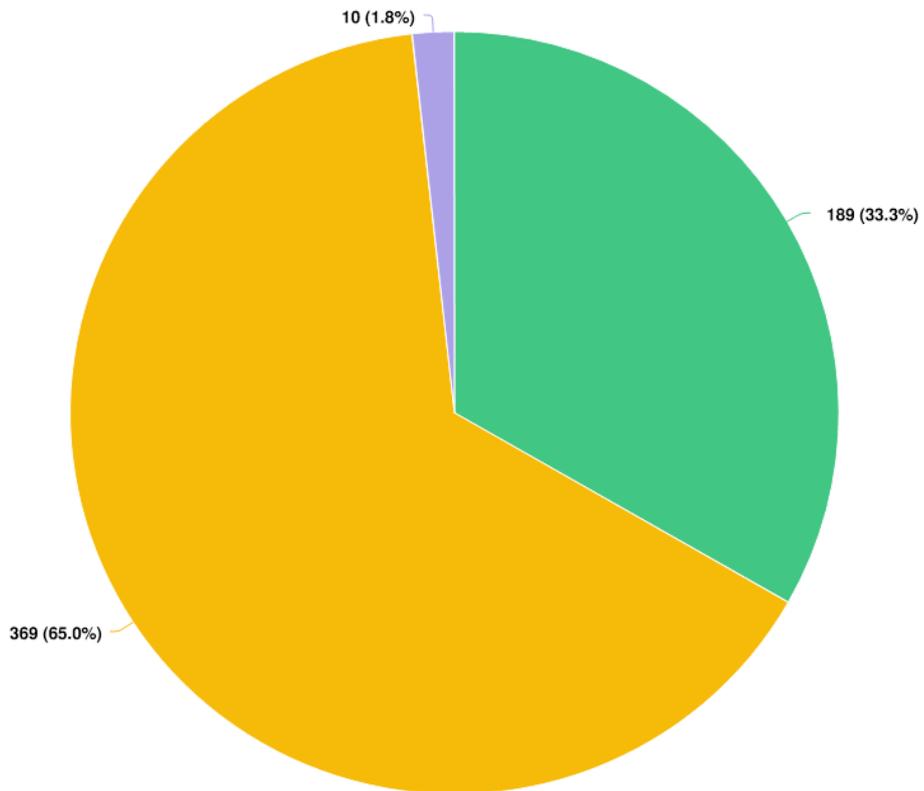
Optional question (569 response(s), 8 skipped)

Question type: Radio Button Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Gender:



Question options

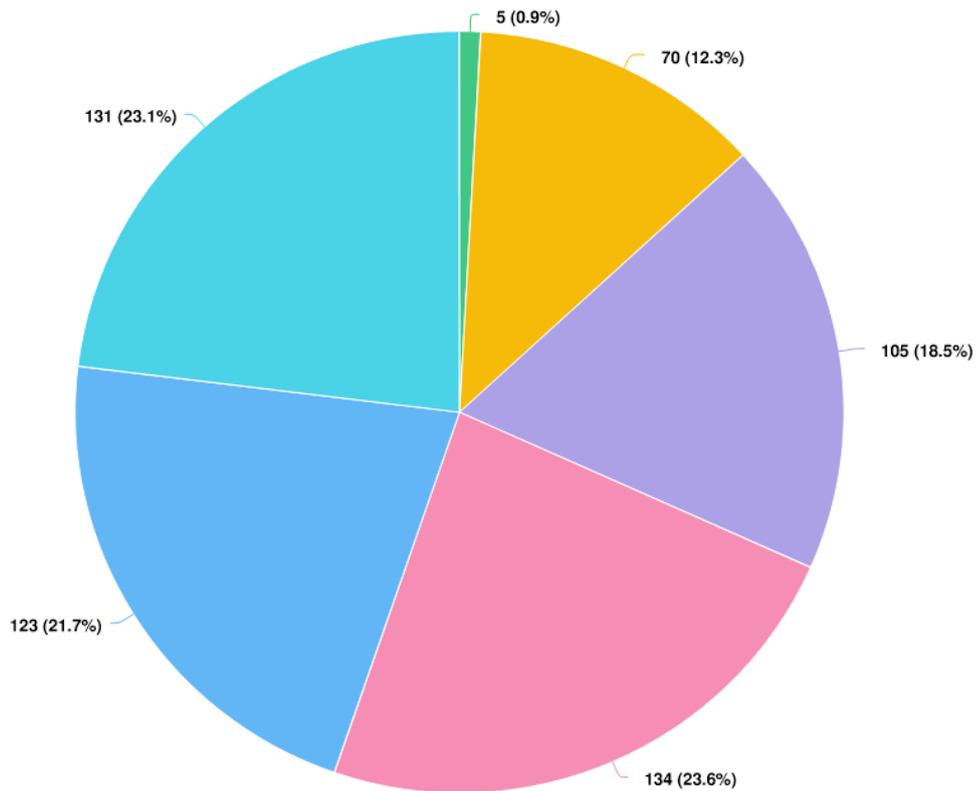
- Other
- Female
- Male

Optional question (568 response(s), 9 skipped)
Question type: Radio Button Question



Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Age bracket:



Question options

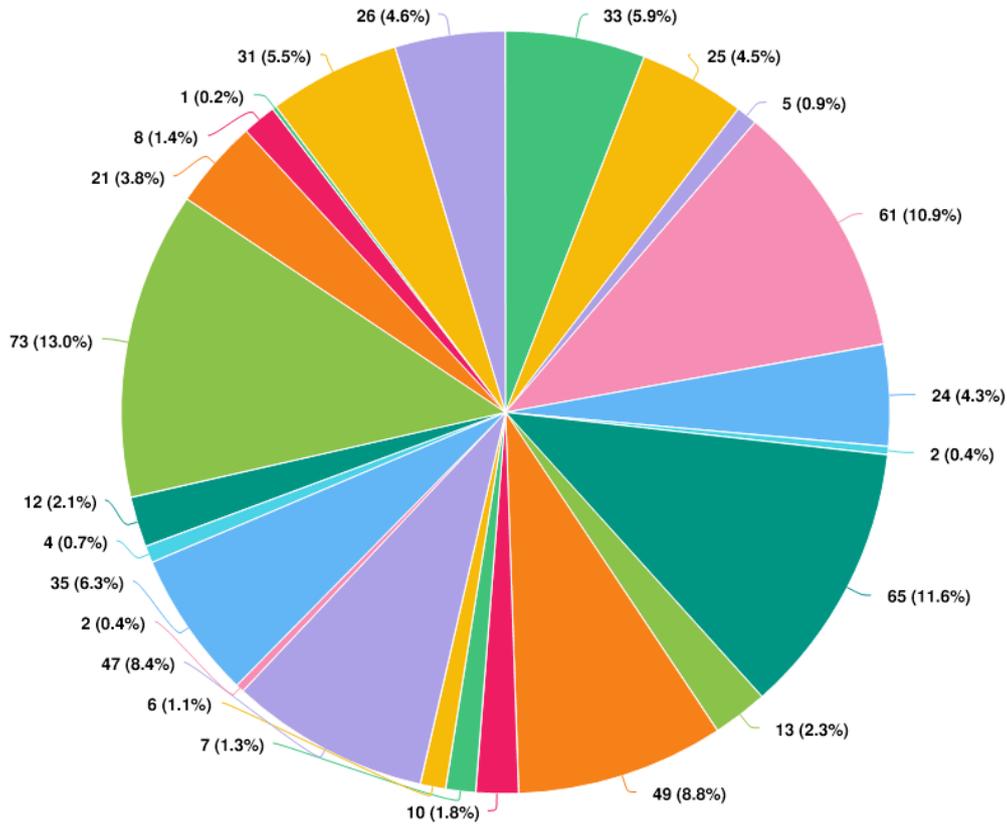
- 65 +
- 55 - 64
- 45 - 54
- 35 - 44
- 25 - 34
- 18 - 24

Optional question (568 response(s), 9 skipped)

Question type: Radio Button Question

Comment on Cockburn : Summary Report for 30 July 2020 to 26 August 2020

Suburb:



Question options

- Other (please specify) Yangebup Wattleup Treeby Success Spearwood South Lake
- North Lake North Coogee Munster Lake Coogee Leeming Jandakot Hammond Park
- Hamilton Hill Coolbellup Coogee Cockburn Central Bibra Lake Beeliar Banjup
- Aubin Grove Atwell

Optional question (560 response(s), 17 skipped)

Question type: Dropdown Question





17 August 2020

City of Cockburn
Mr Michael Emery
9 Coleville Crescent,
Spearwood WA 6163

Dear Michael,

I write on behalf of Beeliar Primary School in support of the City of Cockburn's Draft Animal Management and Exercise Plan.

We understand that one of the objectives of this draft plan that will directly affect our school are to enact further dog off leash areas, namely Beeliar Reserve.

As our school shares this space with the community, we are encouraged that the City is promoting responsible dog ownership and providing sufficient, safe spaces for pets, people and wildlife.

We would like to request, if the draft plan is enacted, the inclusion of extra dog bag stations being installed at Beeliar reserve and dogs are required to be on-leash when a school activity is in progress.

We appreciate the City's ongoing efforts to improve community relations and look forward to continue to form a genuine partnership to make our public spaces more accessible for all.

Sincerely,

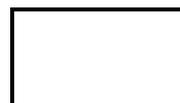
Louise O'Donovan

Principal Beeliar Primary School

A handwritten signature in blue ink, appearing to read 'Louise O'Donovan', is written below the typed name.

18. EXECUTIVE DIVISION ISSUES

Nil



19. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**19.1 (2020/MINUTE NO 0199) BUS SERVICE TO CALLEYA ESTATE**

Author(s) C Sullivan

Attachments N/A

RECOMMENDATION

That a report be prepared to a future Council Meeting on potential bus routes within the Treeby locality and the timing of the provision of those routes, following consultation with the Public Transport Authority and the Minister for Transport's office.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

By email dated 27 August 2020, Cr Lee-Anne Smith submitted the following Notice of Motion:

That Council:

1. liaise with Main Roads Western Australia and the Public Transport Authority to secure a full access at the Ghostgum Avenue and Armadale Road intersection, which will provide the best outcome for the public transport operation within the estate;
2. seek feedback from the Treeby Residents' Association on the proposed potential future bus routes presented within the Treeby Public Transport Strategy and an alternative alignment for bus services provided by the Public Transport Authority for the public transport operations within the estate;
3. write to all residents within the Calleya Estate seeking feedback on the proposed potential future bus routes presented within the Treeby Public Transport Strategy and the alternative alignment for bus services provided by the Public Transport Authority for the public transport operations within the estate;
4. advise the Treeby Residents' Association of the outcome; and
5. write to the Minister for Transport to exercise best endeavours to include the proposed bus route in the next state budget.

Reason

Now more than ever our State government needs to be meeting the increasing demand for travel by encouraging the use of public transport.



In order to get more people on buses and off roads, bus routes need to be easily accessible, direct, and relevant to the needs of local residents.

The provision of a high quality public transport system was an attractive selling point to Calleya Estate residents and an expectation of surrounding Cockburn residents, in order to decrease traffic congestion.

The Officer Recommendation is welcomed, and this alternative takes it one step further, ensuring residents are involved in the process and consulted on the public transport system that suits their needs most.

Submission

N/A

Report

Since early 2018, the matter of the proposed bus service for the Calleya Estate has been discussed with the Public Transport Authority (PTA) and the Treeby Residents Association. Since that time, the development of the estate and the surrounding road network has progressed much further, with the duplication of Armadale Road east of the intersection with Ghostgum Avenue, and the commencement of the duplication of Armadale Road, west of Ghostgum Avenue.

One result of the detail design of the Armadale Road duplication project is the retention of full movement at the upgraded intersection of Armadale Road and Ghostgum Avenue. The City will now recommence discussions with the PTA and the Department of Transport to determine the current status of planning the proposed bus service for the Calleya Estate. A report will be presented to a future Council meeting.

It needs to be recognised from past discussions that the PTA carries out their own public consultation of proposed bus routes or amendments to existing bus routes. The implementation of such routes entirely depends on State funding in future budgets.

Strategic Plans/Policy Implications

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Plan, develop and advocate for safe, sustainable, integrated local transport networks, public transport and regional transport networks.
- Advocate and plan for reduced traffic congestion.



Budget/Financial Implications

None for the City at this time as bus services are funded by the State.

Legal Implications

N/A

Community Consultation

Consultation will be carried out with the PTA, the Department of Transport and the local community of the Calleya Estate, as well as the Treeby Residents' Association. The results will be reported to a future Council Meeting.

Risk Management Implications

Should Council not adopt the recommendation, the matter of the proposed bus service for the Calleya Estate will remain unresolved with the associated uncertainty in the local community.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



**20. NOTICES OF MOTION GIVEN AT THE MEETING FOR
CONSIDERATION AT NEXT MEETING**

Nil



21. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY MEMBERS OR OFFICERS**21.1 (2020/MINUTE NO 0200) NEIGHBOURHOOD WATCH REFERENCE GROUP - NOMINATION**

Author(s) G Bowman
Attachments N/A

RECOMMENDATION

That Council appoint Cr Tom Widenbar as a representative to the Cockburn Neighbourhood Watch Reference Group.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

By email received 17 August 2020, Cr Widenbar requested to be appointed by Council as a delegate to the Neighbourhood Watch Reference Group.

Submission

N/A

Report

In his nomination, Cr Widenbar states the reason for his nomination as: *'I would like to join the Committee to replace Cr Lee Anne Smith following her resignation from the committee in July 2020. I would like to ensure we continue to have representation from Council at the Committee'*.

The Terms of Reference for the Cockburn Neighbourhood Watch Reference Group provide the ability for multiple elected members to be appointed by Council as representatives.

Following Cr Smith's resignation Cr Stone is the only Elected Member who currently sits on the Reference Group.

It is therefore recommended that Council appoint Cr Widenbar as a Council representative on the Cockburn Neighbourhood Watch Reference Group.



Strategic Plans/Policy ImplicationsListening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

N/A

Risk Management Implications

There is a “Low” level of “Brand / Reputation” risk associated with this item.

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



22. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE

22.1 (2020/MINUTE NO 0201) INVESTIGATION- PEDESTRIAN CROSSING ON BEELIAR DRIVE, BEELIAR HIVE SHOPPING CENTRE

| | |
|--------------------|--|
| Author(s) | J Kiurski |
| Attachments | <ol style="list-style-type: none"> 1. Beeliar Drive Pedestrian Crossing Survey 2. Images of Zebra and Wombat Crossing at Mid-Block 3. Images of Mid-Block Signalised Crossing - Puffin and Pelican Crossings 4. Beeliar Drive Pedestrian Crossing Survey |

RECOMMENDATION

That Council monitor demand and reassess the need to upgrade the pedestrian crossing.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

At the 11 June 2020 Ordinary Council Meeting, Cr Stone raised a Matter to be Noted for Investigation, Without Debate as shown below:

Item 22.2 – Investigation – Pedestrian Crossing of Beeliar Drive, Beeliar Hive Shopping Centre

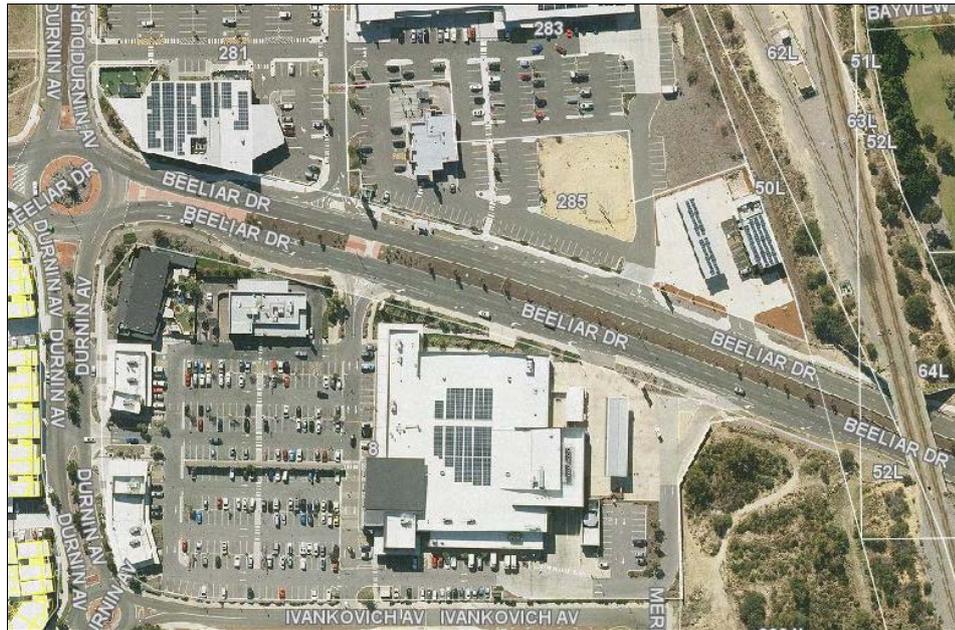
Cr Stone has requested a report to investigate the provision of a safe pedestrian crossing of Beeliar Drive near the Beeliar Hive Shopping Centre.

Reason

Residents of Minori Gardens and the adjacent new development have expressed concerns about the safety of pedestrians crossing Beeliar Drive to and from the day care and primary school.

The aerial photo extract below shows the subject site of this report, which is the section of Beeliar Drive between Durnin Avenue and the Railway Bridge over Beeliar Drive, Beeliar.





Submission

N/A

Report

Pedestrian road crossing facilities are developed as on-road infrastructure or grade separated crossing (overpass or underpass) infrastructure.

State and Local Government have a range of interlinked roles and responsibilities. Government organisations are working together to raise the profile of walking and encourage walking at a strategic level as a healthy and sustainable transport mode.

Main Roads Western Australia (MRWA) is responsible for the care, control and management of the major road and highway network throughout Western Australia. MRWA owes a duty of care to those utilising the road reserve and associated facilities, to ensure they are safe and not exposed to the risk of injury.

MRWA are the approving body for ensuring consistent application of signs and pavement markings on all roads in Western Australia. For this reason, MRWA are involved in the design and approval process of pedestrian facilities on state and local road networks.

Local Governments are responsible for planning, constructing and maintaining much of the pedestrian network, specifically responsible for facilities on local distributor and access roads, and through many parks and reserves.

Councils are also responsible for approving the design of many new residential developments, and regeneration projects for town centres and recreational areas. The design and connectivity of these areas play a major role in the overall walkability of a neighbourhood.



In August 2012, Council approved a mixed-use development on Lot 9001 Beeliar Drive, Beeliar. As a part of this development, the modification of Beeliar Drive to ensure access to and from the site is restricted to left in/left out only by the installation of appropriate traffic management measures was completed.

As part of the development, a pedestrian crossing with cut-through facility in the existing Beeliar Drive median was installed. A footpath within the northern verge of Beeliar Drive connecting this pedestrian crossing to the existing bus stop in the east bound direction was constructed.

The image below shows the as constructed pedestrian facilities across Beeliar Drive in 2013:



In 2015 the City completed the Beeliar Drive Duplication from Durnin Avenue to Spearwood Avenue to improve the road safety and respond to the increase of traffic volume related to the new developments along Beeliar Drive.

At that time, engineering consulting firm GHD had been commissioned by the City to undertake a Road Safety Audit of the design drawings for the Beeliar Drive Duplication (Durnin Avenue to Spearwood Avenue).

The purpose of this road safety audit was to consider the safety aspects of the design and surrounds, identify safety hazards, and recommend any modifications.

It was recommended that a pedestrian crossing just west of the eastbound bus bay (in view of the likely safety implications for vision impaired pedestrians) needed to be established and the northern bus bay could be relocated further east to allow a straight pedestrian crossing alignment of Beeliar Drive.



Safety Audit recommendations were implemented in 2016 and the image below shows current as constructed pedestrian facilities across Beelihar Drive.



The traffic impact assessment completed for the shopping area that was developed on the north side of Beelihar Drive in 2016/2017 did not recommend any changes to the existing pedestrian crossing on Beelihar Drive. A footpath along the north verge of road and through the new parking lot was constructed for that development to augment the pedestrian network.

Existing Road Network

Beelihar Drive is a dual-lane divided carriageway with a large median island. Beelihar Drive is classified as a Distributor A Road in the MRWA Metropolitan Functional Road Hierarchy, and operates under a sign posted speed limit of 70km/h.

Review of traffic counts from June 2019 indicates that Beelihar Drive carried an average weekday traffic flow of 21,741 vehicles per average weekday, with the maximum 85% percentile speed of 75km/h at the above road section.

Crash data at the section of Beelihar Drive between Durnin Avenue and Birchley Road was reviewed. MRWA Crash Analysis Reporting (CAR) data indicates this section of road recorded a total of four (2015-, 2016-2 and 2018-1) crashes during the five-year period ending in December 2019. Review of the detailed crash history indicates that all four crashes are Leaving Driveway (exit from the southern shopping area).

Bus Routes 522, 531 and 532 run along Beelihar Drive with bus stops provided approximately 50m (westbound) and 100m (eastbound) west of the intersection of Durnin Avenue and Beelihar Drive.



There are two mid-block pedestrian crossings, one at the Durnin Avenue/Beeliar Drive intersection, and the other 100m east of Durnin Avenue/Beeliar Drive. They are located within a large raised median that separates Beeliar Drive into two separate carriageways, thus enabling pedestrians to cross the road as two short one-way crossings, using the median as a refuge.

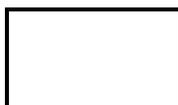


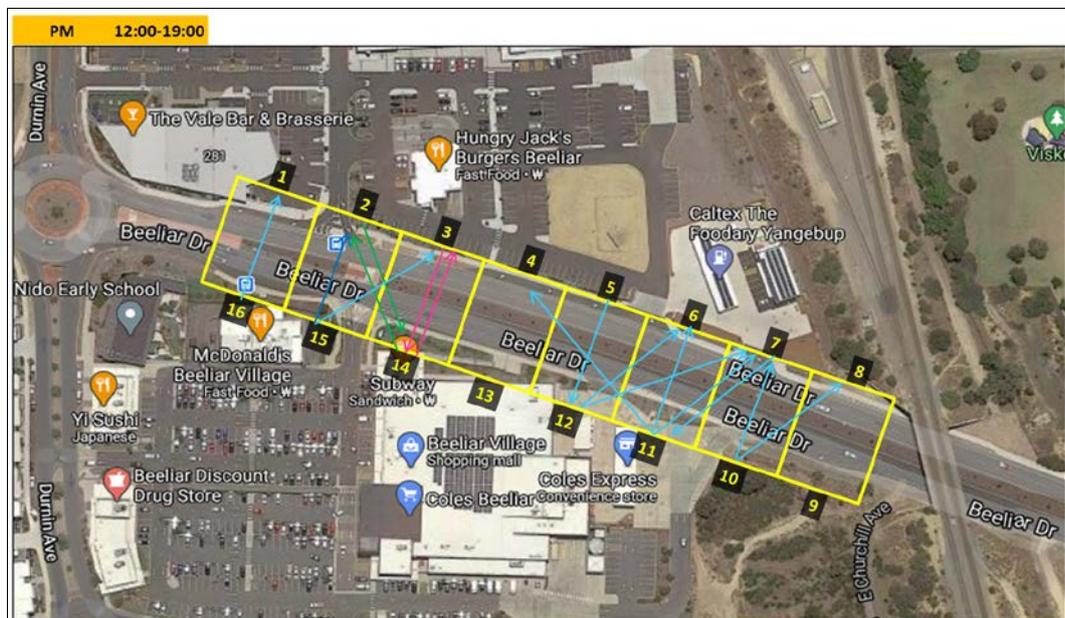
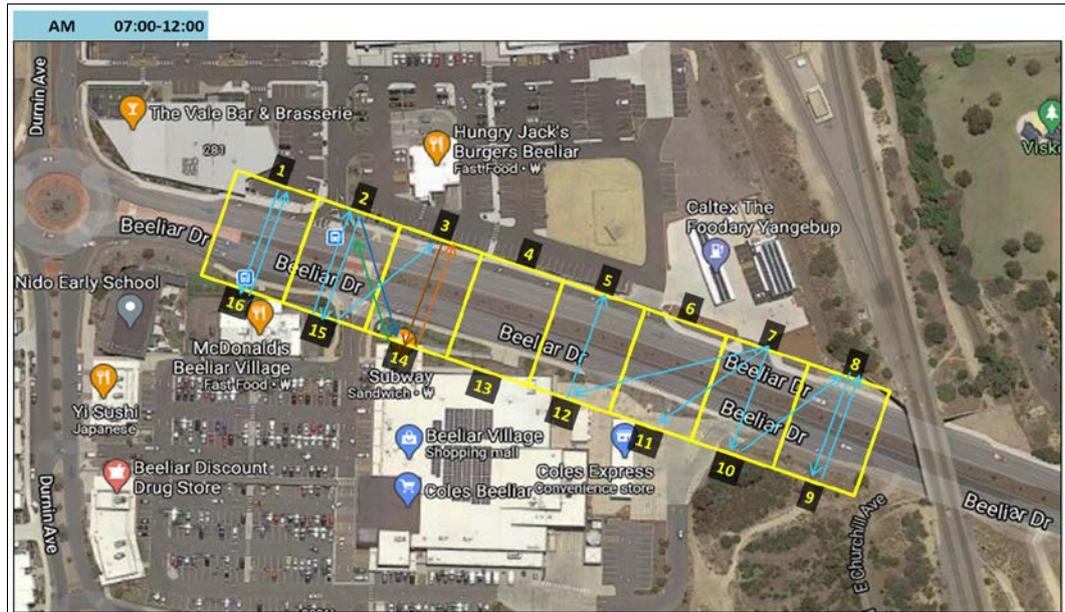
Investigation of Options to Improve Pedestrian Safety

The City has undertaken an investigation of possible options to improve pedestrian safety along the above section of Beeliar Drive. To investigate the need of a mid-block crossing, the City arranged 12-hour pedestrian movement surveys on 20 August 2020.

This section of Beeliar Drive has been separated into 16 zones for the indication of pedestrian movements. The two images below show the AM and PM pedestrian movements between Durnin Avenue and the Railway Bridge over Beeliar Drive.

The 12 hour survey presented 138 crossings eastbound and 112 crossings westbound. Details of the pedestrian movement surveys are included in Attachment 1 for reference.





In addition, the City arranged 12-hour pedestrian movement surveys for Ivankovich Avenue on 20 August 2020 to assess connectivity between Beeliam Drive to and from the day care and primary school on Ivankovich Avenue.

Details of pedestrian movement surveys on Ivankovich Avenue presented as a Site 2 and are included in Attachment 1.

The 12 hour survey presented most of the crossings at AM and PM school time when the parents are using the shopping centre parking facilities and crossing Ivankovich Avenue to access the school.

The image below shows the safe school crossing facilities installed on Ivankovich Avenue.





Option 1: Provide a Pedestrian Bridge at Mid-Block

A pedestrian bridge at mid-block is a grade separation, which provides the highest level of protection for pedestrians and minimises the disruption to road traffic. Grade separated crossings are rarely provided except at freeways and other limited access roads which carry large traffic volumes at high speeds.

Dimensional standards and the vertical clearances to road level for pedestrian bridges must be approved by MRWA.

Any proposed bridge along Beeliar Drive will be assessed for the relevant level of RAV Network access by MRWA Structures Engineering Branch.

The height of the bridge must safely accommodate a load/vehicle height of 4.6m. Overhead clearance and the gradient of ramps and landings will determine the total length of the bridge.

The choice between a bridge or other pedestrian crossing facilities depends upon the particular location. A bridge can be visually obtrusive and generally requires longer ramp sections to clear the road.

Disadvantages for this type of crossing are high capital cost and generally poor patronage due to the level difference and longer walking distance. Many pedestrians prefer to cross at grade, often without using any crossing facility.

Due to natural topography, current width of road reserve and restriction of land from the existing developments on the north and south side of Beeliar Drive, along with the high capital cost indicates a pedestrian bridge at mid-block is not feasible and is not recommended.



Option 2: Provide a Zebra and Wombat Crossing at Mid-block

According to the Planning and Designing for Pedestrians Guidelines, zebra crossings can only be installed on roads with no more than one lane of traffic in each direction and a maximum sign posted speed of 50km/h with maximum 85% percentile speed of 60km/h.

Wombat crossings are generally installed in combination with zebra crossings, also in low speed environments. The crossing has a platform that is in contrast with crossing markings, such as black or red asphalt and 100mm elevation (maximum of 75mm on bus routes). Attachment 2 shows some images of mid-block zebra and wombat crossings.

According to the MRWA mid-block Crossing Warrants, a zebra crossing may be considered if in two separate hours on an average weekday:

- The number of pedestrians crossing in close proximity of the site (generally within 30 m) exceeds 60 per hour ,
- The number of vehicles exceeds 600 per hour (total both directions),
- The product of the number of pedestrians crossing and vehicles passing the site exceeds 90,000 in the same hour.

The recent pedestrian surveys indicate that none of the sites has more than 60 pedestrian movements per hour crossing Beeliar Drive and therefore the first dot point above is not satisfied and a mid-block zebra crossing would not be approved by MRWA.

The large number of pedestrians crossing all four line of Beeliar Drive was 13 between 8.30 -8.45 AM.

Option 3 - Pelican and Puffin Signalised Crossings

The puffin crossing is a development of the pelican crossing. Above ground detectors sense the presence of people crossing the road and adjust the crossing times as required. Extra time is allocated for slower moving pedestrians such as the elderly and people with a disability.

A pelican and puffin crossing may be considered for locations with high pedestrian and traffic volumes which meet MRWA warrants or alternative crossing guidelines for roads with speeds ≤ 70 km/h.

A pelican and puffin crossing may be considered if any of the following conditions exist (extract from Planning and Designing for Pedestrians Guidelines):

- (a) *For each of three hours on an average day:*
 - i. *Pedestrian volumes exceed 350 persons per hour*
 - ii. *Vehicular traffic exceeds 600 vehicles per hour (one direction) or 1000 vehicles per hour (total both directions) where there is a central pedestrian refuge*
- (b) *For each of eight hours on an average day:*
 - i. *The pedestrian volume exceeds 175 persons per hour*
 - ii. *Vehicular traffic exceeds 600 vehicles per hour (one direction) or 1000 vehicles per hour (total both directions) where there is a central pedestrian refuge*



- iii. There is no zebra crossing, footbridge or underpass within a reasonable distance.*
- (c) *At a school where, in two separate one hour periods of a typical school day:*
- i. There are no fewer than 50 persons crossing the roadway*
- ii. At least 600 vehicles pass the site subject*
- iii. The product of the number of pedestrians crossing and vehicles passing in the same hour exceeds 40,000*
- (d) *The pedestrian and traffic volume is sufficient to justify a zebra crossing but pedestrians would be in danger at an 'unprotected' pedestrian crossing. This could be due to the width of the carriageway, traffic speed or traffic volume*
- (e) *A zebra crossing exists and two or more pedestrian accidents of the type susceptible to the correction by signals have occurred on or near the crossing within the past three years*
- (f) *A zebra crossing is justified and pedestrian volumes are very heavy and coincide with high traffic volumes to the extent that excessive delays to road traffic are likely*

The recent pedestrian surveys indicate the existing pedestrian volumes do not satisfy points (a), (b) and (c) above and as a zebra crossing is not justified, points (d), (e) and (f) are not satisfied. Therefore, a mid-block pelican and puffin signalised crossings would not be approved by MRWA. Attachment 3 shows some images of pelican and puffin signalised crossings.

Pelican crossings (Pedestrian Light Control Crossing) are pedestrian activated traffic signals located at mid-block locations. They are used where pedestrian crossing activity is concentrated along short sections of road carrying high traffic volumes.

The signal sequence is similar to signalised intersections except that a flashing yellow phase is included for motorists soon after the display of the flashing red figure commences for pedestrians. Drivers may then proceed through the crossing with caution during the flashing yellow phase but must give way to pedestrians still on the crossing.

Warrants for pelican crossings are discussed in the previous section for Pelican and Puffin Signalised Crossings:

- Roads with a speed limit ≥ 70 km / hr
- Where pedestrians cross over a long section of road
- Where pedestrian crossing volumes and traffic volumes are low
- Close to an intersection (should be combined with a signalised intersection treatment).

Beeliar Drive is a major distributor road and operates under a sign posted speed limit of 70km/h with high traffic volumes. It is considered an inappropriate location for a mid-block signalised crossing with a zebra crossing.



Summary

Based on the analysis and discussions presented within this report, the following conclusions are made:

- The current pedestrian crossing facility across Beeliar Drive between Durnin Avenue and Railway Bridge is appropriate for the current pedestrian crossing demand and traffic flows.
- The school crossing facilities installed on Ivankovich Avenue is appropriate for the current pedestrian crossing demand.
- All pedestrian crossing facilities on Beeliar Drive and Ivankovich Avenue are constructed in according the current standards and approved by MRWA.
- The Traffic Assessment Guideline for Development - Traffic Assessment Guidelines Volume 3 (for Subdivisions) and Volume 5 (Technical Appendix) provide an alternative consideration of the warrant for installing pedestrian priority crossing facilities, and some of them has been assessed within the report.
- Pedestrian bridge at mid-block is not feasible and is not recommended due to natural topography, current width of road reserve and restriction of land from the existing developments on the north and south site of Beeliar Drive.
- Zebra and wombat crossings at mid-block is not recommended due the recent number of pedestrians crossing the road; less than not 60 pedestrian movements per hour, which is the MRWA warrant for this option.
- Pelican and Puffin Signalised Crossings will be supported for the sites when a number of pedestrians per hour exceed 350 persons, which is not supported by the current survey.
- Demand for the pedestrian crossing and the safety of the current pedestrian crossing facilities will be monitored for any upgrade and improvement in the future.

Strategic Plans/Policy Implications

Community, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

Provide community, sport, recreational, and cultural facilities and infrastructure to meet our community needs.

City Growth and Moving Around

Plan, develop and advocate for safe, sustainable, integrated local transport networks, public transport and regional transport networks.

Listening and Leading

Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.



Budget/Financial Implications

All costs of the future survey for the pedestrian crossing demand and safety of the current pedestrian crossing facilities will be covered by the existing budget CW2375 - Traffic Safety Management.

Legal Implications

N/A

Community Consultation

Public consultation with the adjacent or impacted property owners will be carried out if any changes of the current pedestrian crossing facilities are proposed in the future.

Risk Management Implications

The proposed monitoring of the pedestrian crossing will identify any future improvements.

Advice to Proponent(s)/Submitters

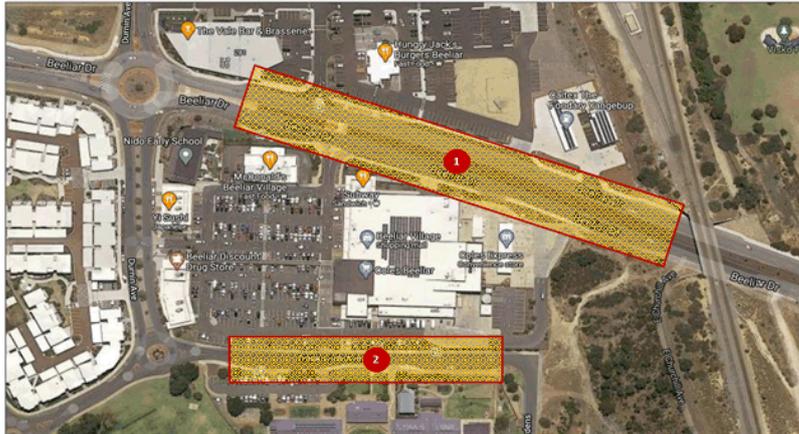
N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



Client City of Cockburn
Location 1. Beeliar Dr
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



[Survey Area]
1. Beeliar Dr
2. Ivankovich Ave



Client City of Cockburn
Location 1. Beelihar Dr
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



- 45 over
- 41-45
- 36-40
- 31-35
- 26-30
- 21-25
- 16-20
- 11-15
- 5-10
- 1-5



Client City of Cockburn
Location 1. Beeliar Dr
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



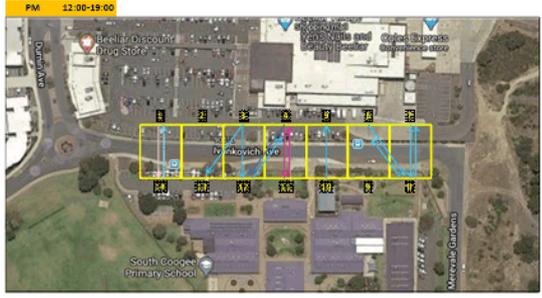
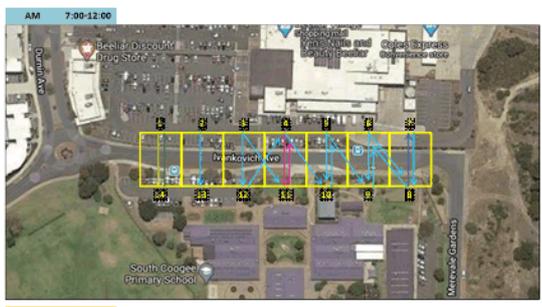
| Location | 1. Beeliar Dr | | | | | |
|----------------|---------------|----------|------------|------------|----------|------------|
| Direction | EB | | | WB | | |
| Time Period | Peds | Cyclists | Total | Peds | Cyclists | Total |
| 7:00 to 7:15 | 1 | 0 | 1 | 0 | 0 | 0 |
| 7:15 to 7:30 | 4 | 0 | 4 | 2 | 0 | 2 |
| 7:30 to 7:45 | 0 | 0 | 0 | 1 | 0 | 1 |
| 7:45 to 8:00 | 1 | 0 | 1 | 1 | 0 | 1 |
| 8:00 to 8:15 | 5 | 0 | 5 | 4 | 0 | 4 |
| 8:15 to 8:30 | 0 | 0 | 0 | 1 | 0 | 1 |
| 8:30 to 8:45 | 2 | 0 | 2 | 11 | 2 | 13 |
| 8:45 to 9:00 | 4 | 0 | 4 | 3 | 0 | 3 |
| 9:00 to 9:15 | 0 | 0 | 0 | 3 | 0 | 3 |
| 9:15 to 9:30 | 2 | 0 | 2 | 0 | 0 | 0 |
| 9:30 to 9:45 | 3 | 0 | 3 | 0 | 0 | 0 |
| 9:45 to 10:00 | 3 | 0 | 3 | 3 | 0 | 3 |
| 10:00 to 10:15 | 0 | 0 | 0 | 2 | 0 | 2 |
| 10:15 to 10:30 | 0 | 0 | 0 | 2 | 0 | 2 |
| 10:30 to 10:45 | 4 | 0 | 4 | 1 | 0 | 1 |
| 10:45 to 11:00 | 3 | 0 | 3 | 4 | 0 | 4 |
| 11:00 to 11:15 | 5 | 0 | 5 | 4 | 0 | 4 |
| 11:15 to 11:30 | 4 | 0 | 4 | 1 | 0 | 1 |
| 11:30 to 11:45 | 3 | 0 | 3 | 2 | 0 | 2 |
| 11:45 to 12:00 | 1 | 0 | 1 | 0 | 0 | 0 |
| 12:00 to 12:15 | 1 | 0 | 1 | 1 | 0 | 1 |
| 12:15 to 12:30 | 4 | 0 | 4 | 3 | 0 | 3 |
| 12:30 to 12:45 | 2 | 0 | 2 | 1 | 0 | 1 |
| 12:45 to 13:00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 13:00 to 13:15 | 1 | 0 | 1 | 0 | 1 | 1 |
| 13:15 to 13:30 | 1 | 1 | 2 | 3 | 0 | 3 |
| 13:30 to 13:45 | 2 | 0 | 2 | 2 | 0 | 2 |
| 13:45 to 14:00 | 2 | 0 | 2 | 0 | 0 | 0 |
| 14:00 to 14:15 | 3 | 0 | 3 | 3 | 0 | 3 |
| 14:15 to 14:30 | 0 | 0 | 0 | 0 | 0 | 0 |
| 14:30 to 14:45 | 6 | 0 | 6 | 6 | 0 | 6 |
| 14:45 to 15:00 | 1 | 0 | 1 | 4 | 0 | 4 |
| 15:00 to 15:15 | 12 | 0 | 12 | 2 | 0 | 2 |
| 15:15 to 15:30 | 6 | 1 | 7 | 2 | 0 | 2 |
| 15:30 to 15:45 | 6 | 0 | 6 | 10 | 0 | 10 |
| 15:45 to 16:00 | 7 | 0 | 7 | 6 | 0 | 6 |
| 16:00 to 16:15 | 9 | 0 | 9 | 3 | 0 | 3 |
| 16:15 to 16:30 | 3 | 0 | 3 | 2 | 0 | 2 |
| 16:30 to 16:45 | 5 | 0 | 5 | 1 | 0 | 1 |
| 16:45 to 17:00 | 8 | 0 | 8 | 0 | 0 | 0 |
| 17:00 to 17:15 | 4 | 0 | 4 | 3 | 0 | 3 |
| 17:15 to 17:30 | 1 | 0 | 1 | 6 | 0 | 6 |
| 17:30 to 17:45 | 2 | 0 | 2 | 0 | 0 | 0 |
| 17:45 to 18:00 | 1 | 0 | 1 | 2 | 0 | 2 |
| 18:00 to 18:15 | 1 | 0 | 1 | 0 | 0 | 0 |
| 18:15 to 18:30 | 3 | 0 | 3 | 2 | 0 | 2 |
| 18:30 to 18:45 | 0 | 0 | 0 | 2 | 0 | 2 |
| 18:45 to 19:00 | 0 | 0 | 0 | 0 | 0 | 0 |
| Totals | 136 | 2 | 138 | 109 | 3 | 112 |



Client City of Cockburn
Location 2. Ivankovich Ave
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



- 100 over
- 45-100
- 41-45
- 36-40
- 31-35
- 26-30
- 21-25
- 16-20
- 11-15
- 5-10
- 1-5



Client City of Cockburn
Location 2. Ivankovich Ave
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



| Location | 2. Ivankovich Ave | | | | | |
|----------------|-------------------|----------|------------|------------|----------|------------|
| Direction | NB | | | SB | | |
| Time Period | Peds | Cyclists | Total | Peds | Cyclists | Total |
| 7:00 to 7:15 | 1 | 0 | 1 | 0 | 0 | 0 |
| 7:15 to 7:30 | 3 | 0 | 3 | 0 | 0 | 0 |
| 7:30 to 7:45 | 1 | 0 | 1 | 3 | 0 | 3 |
| 7:45 to 8:00 | 5 | 0 | 5 | 1 | 0 | 1 |
| 8:00 to 8:15 | 3 | 0 | 3 | 6 | 0 | 6 |
| 8:15 to 8:30 | 1 | 0 | 1 | 40 | 1 | 41 |
| 8:30 to 8:45 | 82 | 1 | 83 | 95 | 2 | 97 |
| 8:45 to 9:00 | 38 | 0 | 38 | 65 | 0 | 65 |
| 9:00 to 9:15 | 6 | 0 | 6 | 5 | 0 | 5 |
| 9:15 to 9:30 | 5 | 0 | 5 | 4 | 0 | 4 |
| 9:30 to 9:45 | 3 | 0 | 3 | 4 | 0 | 4 |
| 9:45 to 10:00 | 1 | 0 | 1 | 1 | 0 | 1 |
| 10:00 to 10:15 | 1 | 0 | 1 | 4 | 0 | 4 |
| 10:15 to 10:30 | 1 | 0 | 1 | 1 | 0 | 1 |
| 10:30 to 10:45 | 11 | 0 | 11 | 0 | 0 | 0 |
| 10:45 to 11:00 | 10 | 0 | 10 | 5 | 0 | 5 |
| 11:00 to 11:15 | 3 | 0 | 3 | 5 | 0 | 5 |
| 11:15 to 11:30 | 1 | 0 | 1 | 4 | 0 | 4 |
| 11:30 to 11:45 | 1 | 0 | 1 | 0 | 0 | 0 |
| 11:45 to 12:00 | 1 | 0 | 1 | 1 | 0 | 1 |
| 12:00 to 12:15 | 2 | 0 | 2 | 1 | 0 | 1 |
| 12:15 to 12:30 | 4 | 0 | 4 | 3 | 0 | 3 |
| 12:30 to 12:45 | 1 | 0 | 1 | 4 | 0 | 4 |
| 12:45 to 13:00 | 11 | 0 | 11 | 7 | 0 | 7 |
| 13:00 to 13:15 | 1 | 0 | 1 | 5 | 0 | 5 |
| 13:15 to 13:30 | 2 | 0 | 2 | 2 | 0 | 2 |
| 13:30 to 13:45 | 1 | 0 | 1 | 0 | 0 | 0 |
| 13:45 to 14:00 | 1 | 0 | 1 | 4 | 0 | 4 |
| 14:00 to 14:15 | 1 | 0 | 1 | 1 | 0 | 1 |
| 14:15 to 14:30 | 5 | 0 | 5 | 2 | 0 | 2 |
| 14:30 to 14:45 | 1 | 0 | 1 | 16 | 0 | 16 |
| 14:45 to 15:00 | 59 | 0 | 59 | 78 | 0 | 78 |
| 15:00 to 15:15 | 184 | 2 | 186 | 12 | 0 | 12 |
| 15:15 to 15:30 | 7 | 0 | 7 | 1 | 0 | 1 |
| 15:30 to 15:45 | 2 | 0 | 2 | 7 | 0 | 7 |
| 15:45 to 16:00 | 2 | 0 | 2 | 6 | 0 | 6 |
| 16:00 to 16:15 | 0 | 0 | 0 | 5 | 0 | 5 |
| 16:15 to 16:30 | 1 | 0 | 1 | 3 | 0 | 3 |
| 16:30 to 16:45 | 4 | 0 | 4 | 2 | 0 | 2 |
| 16:45 to 17:00 | 3 | 0 | 3 | 2 | 0 | 2 |
| 17:00 to 17:15 | 1 | 0 | 1 | 2 | 0 | 2 |
| 17:15 to 17:30 | 0 | 0 | 0 | 1 | 0 | 1 |
| 17:30 to 17:45 | 0 | 0 | 0 | 3 | 0 | 3 |
| 17:45 to 18:00 | 5 | 0 | 5 | 1 | 0 | 1 |
| 18:00 to 18:15 | 0 | 0 | 0 | 4 | 0 | 4 |
| 18:15 to 18:30 | 0 | 0 | 0 | 1 | 0 | 1 |
| 18:30 to 18:45 | 2 | 0 | 2 | 0 | 0 | 0 |
| 18:45 to 19:00 | 1 | 0 | 1 | 0 | 0 | 0 |
| Totals | 479 | 3 | 482 | 417 | 3 | 420 |



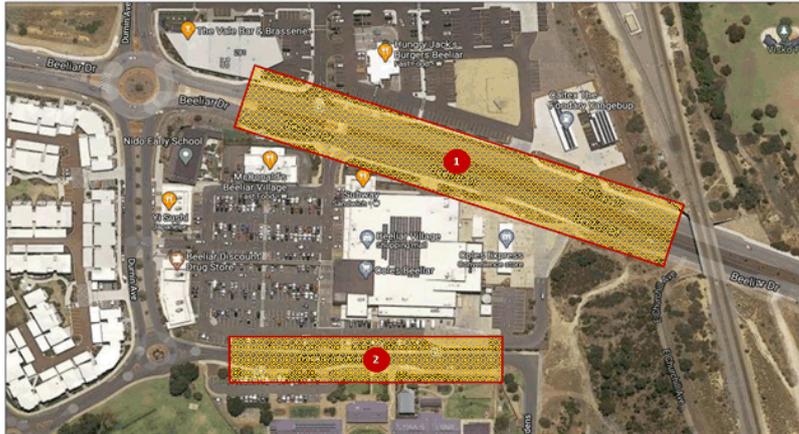
IMAGES OF ZEBRA AND WOMBAT CROSSINGS AT MID-BLOCK



IMAGES OF MID-BLOCK SIGNALISED CROSSINGS - PUFFIN AND PELICAN CROSSINGS



Client City of Cockburn
Location 1. Beeliar Dr
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



[Survey Area]
1. Beeliar Dr
2. Ivankovich Ave



Client City of Cockburn
Location 1. Beelihar Dr
Date Thu, 20th Aug 2020 (7:00-19:00)
Description Pedestrian Survey - Desired Lines



- 45 over
- 41-45
- 36-40
- 31-35
- 26-30
- 21-25
- 16-20
- 11-15
- 5-10
- 1-5



| | |
|--------------------|-----------------------------------|
| Client | City of Cockburn |
| Location | 1. Beeliar Dr |
| Date | Thu, 20th Aug 2020 (7:00-19:00) |
| Description | Pedestrian Survey - Desired Lines |



| Location | 1. Beeliar Dr | | | | | |
|----------------|---------------|----------|------------|------------|----------|------------|
| | EB | | | WB | | |
| Direction | Peds | Cyclists | Total | Peds | Cyclists | Total |
| 7:00 to 7:15 | 1 | 0 | 1 | 0 | 0 | 0 |
| 7:15 to 7:30 | 4 | 0 | 4 | 2 | 0 | 2 |
| 7:30 to 7:45 | 0 | 0 | 0 | 1 | 0 | 1 |
| 7:45 to 8:00 | 1 | 0 | 1 | 1 | 0 | 1 |
| 8:00 to 8:15 | 5 | 0 | 5 | 4 | 0 | 4 |
| 8:15 to 8:30 | 0 | 0 | 0 | 1 | 0 | 1 |
| 8:30 to 8:45 | 2 | 0 | 2 | 11 | 2 | 13 |
| 8:45 to 9:00 | 4 | 0 | 4 | 3 | 0 | 3 |
| 9:00 to 9:15 | 0 | 0 | 0 | 3 | 0 | 3 |
| 9:15 to 9:30 | 2 | 0 | 2 | 0 | 0 | 0 |
| 9:30 to 9:45 | 3 | 0 | 3 | 0 | 0 | 0 |
| 9:45 to 10:00 | 3 | 0 | 3 | 3 | 0 | 3 |
| 10:00 to 10:15 | 0 | 0 | 0 | 2 | 0 | 2 |
| 10:15 to 10:30 | 0 | 0 | 0 | 2 | 0 | 2 |
| 10:30 to 10:45 | 4 | 0 | 4 | 1 | 0 | 1 |
| 10:45 to 11:00 | 3 | 0 | 3 | 4 | 0 | 4 |
| 11:00 to 11:15 | 5 | 0 | 5 | 4 | 0 | 4 |
| 11:15 to 11:30 | 4 | 0 | 4 | 1 | 0 | 1 |
| 11:30 to 11:45 | 3 | 0 | 3 | 2 | 0 | 2 |
| 11:45 to 12:00 | 1 | 0 | 1 | 0 | 0 | 0 |
| 12:00 to 12:15 | 1 | 0 | 1 | 1 | 0 | 1 |
| 12:15 to 12:30 | 4 | 0 | 4 | 3 | 0 | 3 |
| 12:30 to 12:45 | 2 | 0 | 2 | 1 | 0 | 1 |
| 12:45 to 13:00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 13:00 to 13:15 | 1 | 0 | 1 | 0 | 1 | 1 |
| 13:15 to 13:30 | 1 | 1 | 2 | 3 | 0 | 3 |
| 13:30 to 13:45 | 2 | 0 | 2 | 2 | 0 | 2 |
| 13:45 to 14:00 | 2 | 0 | 2 | 0 | 0 | 0 |
| 14:00 to 14:15 | 3 | 0 | 3 | 3 | 0 | 3 |
| 14:15 to 14:30 | 0 | 0 | 0 | 0 | 0 | 0 |
| 14:30 to 14:45 | 6 | 0 | 6 | 6 | 0 | 6 |
| 14:45 to 15:00 | 1 | 0 | 1 | 4 | 0 | 4 |
| 15:00 to 15:15 | 12 | 0 | 12 | 2 | 0 | 2 |
| 15:15 to 15:30 | 6 | 1 | 7 | 2 | 0 | 2 |
| 15:30 to 15:45 | 6 | 0 | 6 | 10 | 0 | 10 |
| 15:45 to 16:00 | 7 | 0 | 7 | 6 | 0 | 6 |
| 16:00 to 16:15 | 9 | 0 | 9 | 3 | 0 | 3 |
| 16:15 to 16:30 | 3 | 0 | 3 | 2 | 0 | 2 |
| 16:30 to 16:45 | 5 | 0 | 5 | 1 | 0 | 1 |
| 16:45 to 17:00 | 8 | 0 | 8 | 0 | 0 | 0 |
| 17:00 to 17:15 | 4 | 0 | 4 | 3 | 0 | 3 |
| 17:15 to 17:30 | 1 | 0 | 1 | 6 | 0 | 6 |
| 17:30 to 17:45 | 2 | 0 | 2 | 0 | 0 | 0 |
| 17:45 to 18:00 | 1 | 0 | 1 | 2 | 0 | 2 |
| 18:00 to 18:15 | 1 | 0 | 1 | 0 | 0 | 0 |
| 18:15 to 18:30 | 3 | 0 | 3 | 2 | 0 | 2 |
| 18:30 to 18:45 | 0 | 0 | 0 | 2 | 0 | 2 |
| 18:45 to 19:00 | 0 | 0 | 0 | 0 | 0 | 0 |
| Totals | 136 | 2 | 138 | 109 | 3 | 112 |

22.2 (2020/MINUTE NO 0202) FEASIBILITY - CONSTRUCTION OF AN ADDITIONAL ROAD LINK INTO THE YANGEBUP AREA**Author(s)** J Kiurski**Attachments** 1. Yangebup Traffic Assessment Options**RECOMMENDATION**

That Council note the report.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0**Background**

At the 13 February 2020 Ordinary Council Meeting, Cr Eva raised a Matter to be Noted for Investigation, Without Debate as shown below:

Cr Eva has requested a report on the feasibility of constructing an additional road link into the Yangebup area.

Reason

There are currently three entry/exit points providing vehicle access/ egress to the Yangebup area – Yangebup Road onto Spearwood Avenue, Osprey Drive onto North Lake Road, and Dunraven Drive onto Beelias Drive. A fourth road link would improve traffic congestion on the existing entry/exit points and provide better access/egress for emergency vehicles.

Traffic congestion has been causing considerable delays during peak hours at these locations, especially during school terms.

Submission

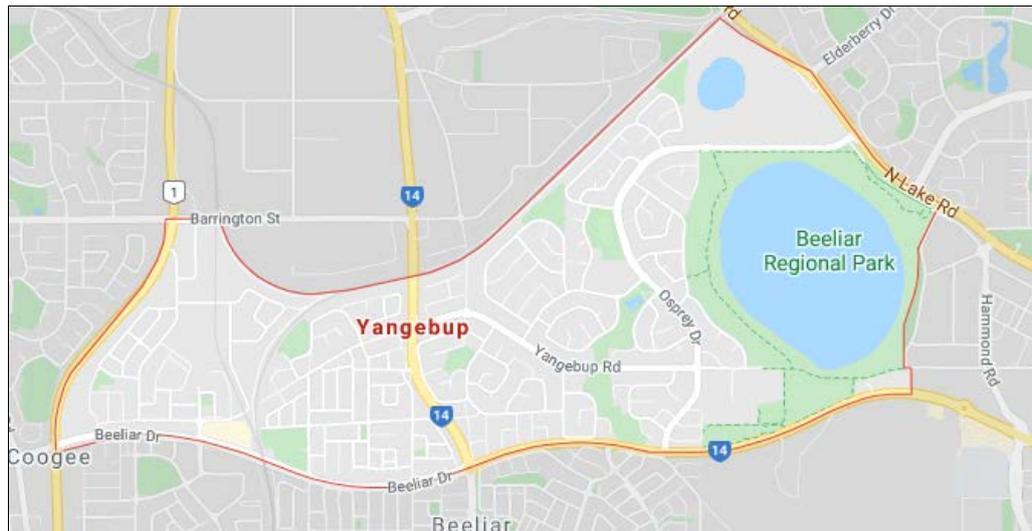
N/A

Report

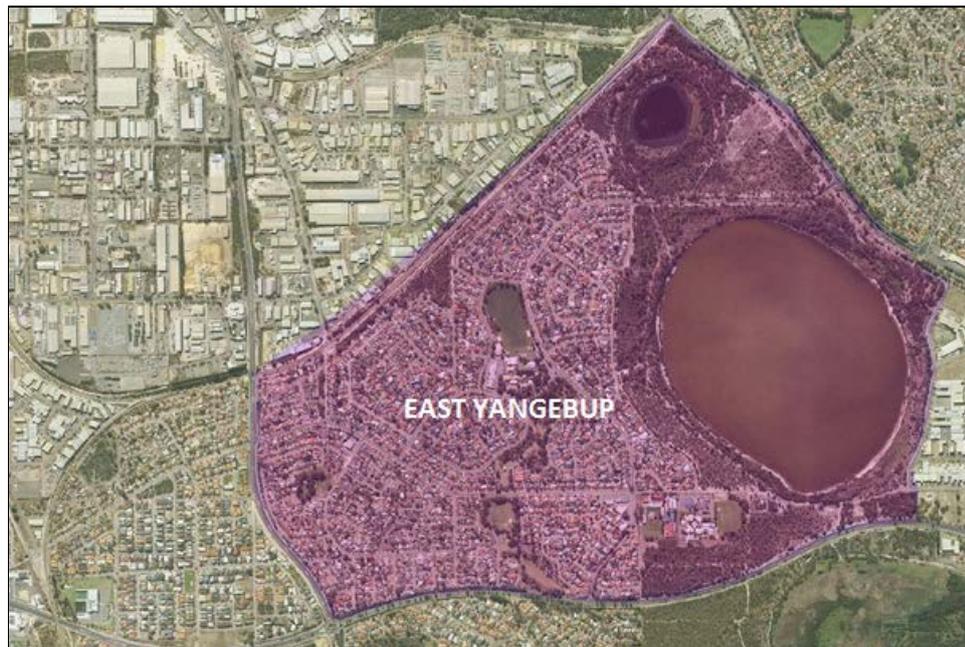
The suburb of Yangebup, as shown in the image below, was originally subdivided in the early 1980s with the majority of homes constructed soon after, making the average Yangebup home almost 40 years old.

The part of Yangebup east of Spearwood Avenue, north of Beelias Drive and west of Osprey Drive, is in close proximity to the Beelias Regional Park and is approximately three kilometres from the City's largest activity centre – Cockburn Central.





The study area of this report is illustrated as East Yangebup, as shown in the image below and is generally bounded by Yangebup Lake, Beelias Drive, Spearwood Avenue, Yangebup Road and the freight rail line to the north.



There are four main road corridors within the study area: Yangebup Road, Dunraven Drive, Osprey Drive and Moorhen Drive.

Until 1990, Yangebup Road was the main east-west link between the Kwinana Freeway in the east and Stock Road in the west. This also included Miguel Road connecting into the Spearwood industrial area via a railway level crossing on the north side of that area.

The passage of time has seen Spearwood Avenue extend to a new arterial road - Beelias Drive - which now extends through to Stock Road and allowed Yangebup Road to function more as a local distributor.



Also during this time Osprey Drive was constructed and then extended through to North Lake Road at a signalised intersection.

These works also saw the deletion of the Miguel Road level crossing and the deletion of a section of Yangebup Road south of Yangebup Lake, through an area of sensitive wetlands. Both of these decisions were intended to separate the adjacent industrial areas to the north and east from the residential areas of Yangebup. The severing of Miguel Road at the railway level crossing eliminated a safety risk between vehicles and rail freight.

The section of Yangebup Road reserve through the sensitive wetlands was absorbed into the Beeliar Regional Park to reduce environmental risk as well as for the reason noted above. Half the bitumen was removed so it could still function as a dual use path. This section of Yangebup Road is no longer shown on the Metropolitan Regional Scheme as a road, it is unallocated crown land. It is zoned as Parks and Recreation.

There are currently three entry/exit points providing vehicle access/egress to the Yangebup area:

- Spearwood Avenue at Yangebup Road
- Beeliar Drive at Dunraven Drive
- Osprey Drive at North Lake Road.

There are currently two established local shopping centres in Yangebup.

- Lakelands Shopping Centre located on the corner of Moorhen Drive and Swallow Drive including an IGA, various medical facilities and food outlets,
- Local centre on the corner of Yangebup Road and Dunraven Drive including a liquor store, various food outlets and a beauty service.

Yangebup has three education facilities within the suburb: Yangebup Primary School, Mater Christi Primary School and Divine Mercy College Primary/Secondary School, including two childcare service facilities. There are also several churches, as well as one sporting facility with club rooms at the Milgun Reserve.

The District Traffic Study (DTS) prepared in 2018 found that without any changes to the City wide road network by 2021, modelling shows widespread congestion with many north-south and east-west links operating at a capacity of 80% or higher.

The 2018 DTS recommends a range of road upgrades however none in immediate proximity to Yangebup. This recognises the significant upgrades completed along Spearwood Avenue, including upgrades to the intersection of Spearwood Avenue and Yangebup Road.

Further upgrades to Beeliar Drive west of Spearwood Avenue were also recently completed.



Importantly the study recognises the need for alternative responses to address congestion including:

- Public transport improvements to rail and bus routes and infrastructure;
- Further encouraging pedestrian and cycle transport modes;
- Examining the impact of peak spreading on road infrastructure requirements;
- Examining the timing and sequencing of development,
- Increasing implementation of Intelligent Transport Systems.

A key consideration for the Yangebup study area is to balance road upgrades/alternative link access to address congestion issues with sustainable alternatives including public transport, walking and cycling.

Access Traffic Assessment

The City engaged GTA Consultants to complete an assessment of the requirements for the future road network servicing the suburb of Yangebup (east of Spearwood Avenue, north of Beeliar Drive and west of North Lake Road). The Eastern Yangebup Access Traffic Assessment is included for reference as Attachment 1.

The purpose of this study was to investigate the impacts on intersections providing access/egress to this part of Yangebup, considering the effects of both internal traffic growth due to additional lots being developed and the increase of external traffic on roads bordering the area.

The existing traffic volume and crash data of the study area have been analysed at the key intersection access points:

- Spearwood Avenue and Yangebup Road
- Beeliar Drive and Dunraven Drive
- Osprey Drive and North Lake Road

The results of the SIDRA analysis will assist the City with determining the best possible treatment(s) and need for an additional link in the time period to the 2031 horizon.

Modelling scenarios for the study allowed for approximately 240 additional lots over the forthcoming years and the traffic growth on the external arterial roads bordering the area due to development within the wider area.

Two scenarios were modelled for comparison purposes with 2020 as the current or base year and 2031 as the future scenario. Three separate intersections were modelled (refer Attachment 1):

- Spearwood Avenue and Yangebup Road
- Beeliar Drive and Dunraven Drive
- North Lake Road and Osprey Drive

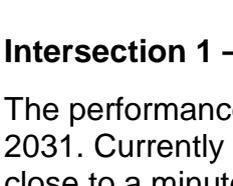
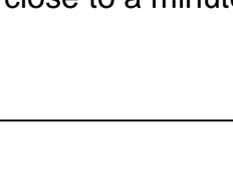


The traffic modelling did not include the reinstatement of the Miguel Road link across the rail corridor or the section of Yangebup Road south of Yangebup Lake. Reinstatement of these links is not believed to be feasible due to:

- The safety and environmental risks are just as relevant today as in the late 1990's when the two roads were severed
- The crossing of the freight rail corridor at Miguel Road would require approval by the PTA which is unlikely not just for the safety reason but the rail freight frequency and length of rolling stock are much increased
- The industrial areas to the east and north are almost fully developed now and hence would reintroduce commercial or heavy vehicles into the residential areas if the two roads were reinstated
- The reinstatement of Yangebup Road would require approval by the WA Planning Commission which is considered to be unlikely due to the reasons above as well as a Town Planning Scheme Amendment as the purpose would be contrary to Parks and Recreation zoning

Summary of Traffic Model Results

The traffic assessment of the current and future road network layout has shown that the road network is expected to operate satisfactorily until 2031 with one exception. The discussions below relate to two fundamental criteria – Degree of Saturation (DoS) and Level of Service (LoS). A description of Levels of Service for intersections appears on the image below, for reference.

| LoS | DESCRIPTION |
|---|--|
|  | A A condition of free flow in which individual drivers are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to manoeuvre within the traffic stream is extremely high, and the general level of comfort and convenience provided is excellent. |
|  | B In the zone of stable flow where drivers still have reasonable freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is a little less than with LoS A. |
|  | C Also in the zone of stable flow, but most drivers are restricted to some extent in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience declines noticeably at this level. |
|  | D Close to the limit of stable flow and approaching unstable flow. All drivers are severely restricted in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is poor, and small increases in traffic flow will generally cause operational problems. |
|  | E Traffic volumes are at or close to capacity, and there is virtually no freedom to select desired speeds or to manoeuvre within the traffic stream. Flow is unstable and minor disturbances within the traffic stream will cause breakdown. |
|  | F In the zone of forced flow, where the amount of traffic approaching the point under consideration exceeds that which can pass it. Flow breakdown occurs, and queuing and delays result. |

Intersection 1 – Spearwood Avenue and Yangebup Road

The performance of this intersection is considered acceptable up to 2031. Currently this has high (DoS) in both peak periods with delays close to a minute in the PM peak.



At this point, there is expected to be delays of approximately 54s for the right turn from Yangebup Road, typically during the period immediately after the completion of a school day (approximately 3:15pm to 3:30pm).

From the tables below, performance of the intersection at Spearwood Avenue and Yangebup Road by 2031 is expected to operate in a similar way as to how it presently operates. This is evidenced by the small increase in DoS and the LoS remains as is.

The determining critical movement for this intersection, the right turn from Yangebup Road, is expected to have slightly longer delays and queues in 2031 compared to the current performance in both the AM and PM peaks. Based on the analysis, there is no need to modify this intersection in the short term or by 2031.

| Approach | Performance Criteria 2020 AM | | | | Performance Criteria 2020 PM | | | |
|-----------------------|------------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| Spearwood Ave (SOUTH) | | | | | | | | |
| Lane 1 | T | 0.30 | | | T | 0.18 | | |
| Lane 2 | T | 0.30 | | | T | 0.18 | | |
| Lane 3 | R | 0.19 | 6 | 9 | R | 0.27 | 8 | 17 |
| Yangebup Road (EAST) | | | | | | | | |
| Lane 1 | L | 0.28 | 9 | 8 | L | 0.47 | 16 | 18 |
| Lane 2 | R | 0.56# | 31 | 19# | R | 0.76# | 29 | 54# |
| Spearwood Ave (NORTH) | | | | | | | | |
| Lane 1 | L | 0.11 | 3 | 7 | L | 0.22 | 7 | 7 |
| Lane 2 | T | 0.16 | | | T | 0.40 | | |
| Lane 3 | T | 0.16 | | | T | 0.40 | | |
| Intersection | ALL | 0.65 | | 4/C | ALL | 0.76 | | 5/F |

Note: # = Worst DoS or Delay defining the overall Intersection DoS or Intersection LoS

| Approach | Performance Criteria 2031AM | | | | Performance Criteria 2031 PM | | | |
|-----------------------|-----------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| Spearwood Ave (SOUTH) | | | | | | | | |
| Lane 1 | T | 0.31 | | | T | 0.19 | | |
| Lane 2 | T | 0.31 | | | T | 0.19 | | |
| Lane 3 | R | 0.20 | 6 | 9 | R | 0.30 | 9 | 18 |
| Yangebup Road (EAST) | | | | | | | | |
| Lane 1 | L | 0.30 | 10 | 8 | L | 0.52 | 19 | 19 |
| Lane 2 | R | 0.72# | 37 | 22# | R | 0.87# | 40 | 74# |
| Spearwood Ave (NORTH) | | | | | | | | |
| Lane 1 | L | 0.12 | 4 | 7 | L | 0.24 | 7 | 7 |
| Lane 2 | T | 0.16 | | | T | 0.41 | | |
| Lane 3 | T | 0.16 | | | T | 0.41 | | |
| Intersection | ALL | 0.72 | | 4/C | ALL | 0.87 | | 6/F |



Intersection 2 – Beeliar Drive and Dunraven Drive

Current performance 2020 in the table below shows the AM peak has long queues on Beeliar Drive with cars stopped to turn right into Dunraven Drive. This then impacts on the ability of vehicles to safely turn right into Beeliar Drive from Dunraven Drive.

| Approach | Performance Criteria 2020AM | | | | Performance Criteria 2020 PM | | | |
|------------------------|-----------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| Beeliar Drive (SOUTH) | | | | | | | | |
| Lane 1 | T | 0.33 | | | T | 0.34 | | |
| Lane 2 | T | 0.33 | | | T | 0.34 | | |
| Lane 3 | R | 0.46# | 17 | 15 | R | 0.34 | 10 | 16 |
| Dunraven Drive (NORTH) | | | | | | | | |
| Lane 1 | L | 0.29 | 9 | 8 | L | 0.37 | 13 | 10 |
| Lane 2 | R | 0.22 | 5 | 40# | R | 0.64# | 17 | 65# |
| Beeliar Drive (WEST) | | | | | | | | |
| Lane 1 | L | 0.25 | 8 | 8 | L | 0.13 | 4 | 7 |
| Lane 2 | T | 0.28 | | | T | 0.34 | | |
| Lane 3 | T | 0.28 | | | T | 0.34 | | |
| Intersection | ALL | 0.46 | | 5E | ALL | 0.64 | | 5/F |

In the afternoon, the critical movement becomes the right turn from Dunraven Drive into Beeliar Drive where delays extend to over two minutes with a high DoS of 0.90. This turn is at LoS F under these conditions.

| Approach | Performance Criteria 2031AM | | | | Performance Criteria 2031 PM | | | |
|------------------------|-----------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| Beeliar Drive (SOUTH) | | | | | | | | |
| Lane 1 | T | 0.42 | | | T | 0.45 | | |
| Lane 2 | T | 0.42 | | | T | 0.45 | | |
| Lane 3 | R | 0.69# | 29 | 29 | R | 0.58 | 18 | 28 |
| Dunraven Drive (NORTH) | | | | | | | | |
| Lane 1 | L | 0.38 | 13 | 13 | L | 0.52 | 19 | 15 |
| Lane 2 | R | 0.42 | 10 | 82# | R | 1.42# | 118 | 507# |
| Beeliar Drive (WEST) | | | | | | | | |
| Lane 1 | L | 0.27 | 9 | 8 | L | 0.14 | 4 | 7 |
| Lane 2 | T | 0.36 | | | T | 0.44 | | |
| Lane 3 | T | 0.36 | | | T | 0.44 | | |
| Intersection | ALL | 0.69 | | 8/F | ALL | 1.42 | | 18/F |

In the GTA report, this intersection is assessed as the DO SOMETHING option and proposes banning the right turn from Dunraven Drive and provide an alternative right turn location introduced on Beeliar Drive near the western bend in Milgun Drive.

The City does not support this proposal, due to the impact this would have on the existing residential area on the north side of Beeliar Drive around Milgun Drive, as well as the physical constraints.

This intersection is the subject of a current design for traffic signal control as approved by Council at the March 2020 OCM, with implementation subject to approval by the MRWA.



Intersection 3 - North Lake Road and Osprey Drive

This intersection currently operates at a satisfactory level of service for both the AM and PM peak periods. Under the current traffic flows and geometry/control, this intersection is considered acceptable.

From the tables below, it is apparent that this intersection is still expected to operate at a satisfactory level of service in both the AM and PM peaks. There is no requirement to modify this intersection in the short term or by 2031.

| Approach | Performance Criteria 2020AM | | | | Performance Criteria 2020 PM | | | |
|-------------------------|-----------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| North Lake Road (SOUTH) | | | | | | | | |
| Lane 1 | L | 0.04 | 2 | 7 | T | 0.07 | 5 | 7 |
| Lane 2 | T | 0.29# | 42 | 7 | T | 0.23 | 43 | 12 |
| Lane 3 | T | 0.29 | 42 | 7 | R | 0.23 | 43 | 12 |
| North Lake Road (NORTH) | | | | | | | | |
| Lane 1 | T | 0.11 | 22 | 4 | L | 0.18 | 43 | 6 |
| Lane 2 | T | 0.11 | 22 | 4 | R | 0.18 | 43 | 6 |
| Lane 3 | R | 0.17 | 18 | 77 | | 0.26# | 36 | 73 |
| Osprey Drive (WEST) | | | | | | | | |
| Lane 1 | L | 0.11 | 1 | 7 | L | 0.08 | 6 | 7 |
| Lane 2 | R | 0.21 | 3 | 75 | T | 0.20 | 30 | 68 |
| Lane 3 | R | 0.21 | 3 | 75 | T | 0.20 | 30 | 68 |
| Intersection | ALL | 0.29 | | 13/B | ALL | 0.26 | | 20/B |

| Approach | Performance Criteria 2031AM | | | | Performance Criteria 2031 PM | | | |
|-------------------------|-----------------------------|-------|-----------|--------------------------------|------------------------------|-------|-----------|--------------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS | Lane | DoS | Queue (m) | Delay (s)/ Intersection LoS |
| North Lake Road (SOUTH) | | | | | | | | |
| Lane 1 | L | 0.04 | 5 | 7 | T | 0.07 | 5 | 7 |
| Lane 2 | T | 0.36# | 34 | 3 | T | 0.29# | 48 | 9 |
| Lane 3 | T | 0.36# | 34 | 3 | R | 0.29# | 48 | 9 |
| North Lake Road (NORTH) | | | | | | | | |
| Lane 1 | T | 0.15 | 31 | 4 | L | 0.23 | 52 | 5 |
| Lane 2 | T | 0.15 | 31 | 4 | R | 0.23 | 52 | 5 |
| Lane 3 | R | 0.35 | 21 | 88 | | 0.29 | 39 | 74 |
| Osprey Drive (WEST) | | | | | | | | |
| Lane 1 | L | 0.14 | 6 | 6 | L | 0.10 | 5 | 6 |
| Lane 2 | R | 0.22 | 26 | 74 | T | 0.28 | 34 | 75 |
| Lane 3 | R | 0.22 | 26 | 74 | T | 0.28 | 34 | 75 |
| Intersection | ALL | 0.36 | | 10/B | ALL | 0.29 | | 15/B |

Mitigation Measures

The current road network is appropriate for the current traffic flows, with the one exception being the intersection of Beeliar Drive and Dunraven Drive.

Intersection 1 – Spearwood Avenue and Yangebup Road

There is no need to modify this intersection in the short term or by 2031.



Intersection 2 – Beeliar Drive and Dunraven Drive

In context of addressing the delays and queues for the critical right turns to and from Dunraven Drive there are three options:

- Banning Movements (right turn from Dunraven Drive into Beeliar Drive)
- Installation of traffic signals
- Installation of a roundabout

Option 1 - banning the right turn from Dunraven Drive is a possibility in the longer term if an alternative right turn could be provided at another location on Beeliar Drive. This option is necessary to consider in the event that the MRWA refuse to approve the proposed traffic signals at the Dunraven/Beeliar intersection.

The GTA report is recommending a possible location near the western bend in Milgun Drive and Beeliar Drive as shown on the image below:



City officers completed a visual assessment of above site and found that there is a significant level difference of two to four metres between Milgun Drive and Beeliar Drive, which would be a significant impact in the construction of an intersection with Beeliar Drive.

Also, Milgun Drive is a local access road which carries less than 1000 vehicles per day on the Average Week Day (latest traffic count was February 2020). To connect to Beeliar Drive would create a major link between Yangebup Road and Beeliar Drive along Milgun Drive which would be a significant impact on the local residential area north of Beeliar Drive.

For these reasons, City officers do not support this proposal.



Option 2 - installation of traffic signals at Beeliar Drive and Dunraven Drive intersection has been considered at the 12 March 2020 Ordinary Council Meeting.

The Council approved the design of signalisation of the intersection and approved funds of \$100,000 for traffic modelling and design of traffic control signals in the 2020/2021 financial year. The project is still subject to approval by the MRWA.

Option 3 – the roundabout installation was not considered viable due to the width of road reserve and size of the roundabout required at this location (expected to be approximately 60m inscribed diameter).

This size roundabout is required as Beeliar Drive is presently a divided dual carriageway road on the Restricted Access Vehicle (RAV) Route for vehicles up the Network 4 size.

Intersection 3 - North Lake Road and Osprey Drive

This is currently a signalised intersection and there is no need to modify this intersection in the short term, or by 2031.

Conclusion

There is currently no requirement to provide an additional road link into the study area pending the reconstruction of the Beeliar/Dunraven intersection.

Strategic Plans/Policy Implications

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Sustainably revitalise urban areas to deliver high levels of amenity and to cater for population growth.
- Advocate and plan for reduced traffic congestion.
- Plan, develop and advocate for safe, sustainable, integrated local transport networks, public transport and regional transport networks.

Budget/Financial Implications

There are no financial impacts on the current budget as the design of the proposed traffic signals at the intersection of Dunraven Drive and Beeliar Drive are included.

Legal Implications

N/A



Community Consultation

Community consultation will be required for the reconstruction of the intersection of Beeliar Drive and Dunraven Drive should the proposed signalisation be approved by the MRWA. This would be in the 2021/22 financial year.

Risk Management Implications

Future monitoring of the access points into the Yangebup area will be required into the future following the modification of the Beeliar/Dunraven intersection to determine any future improvements.

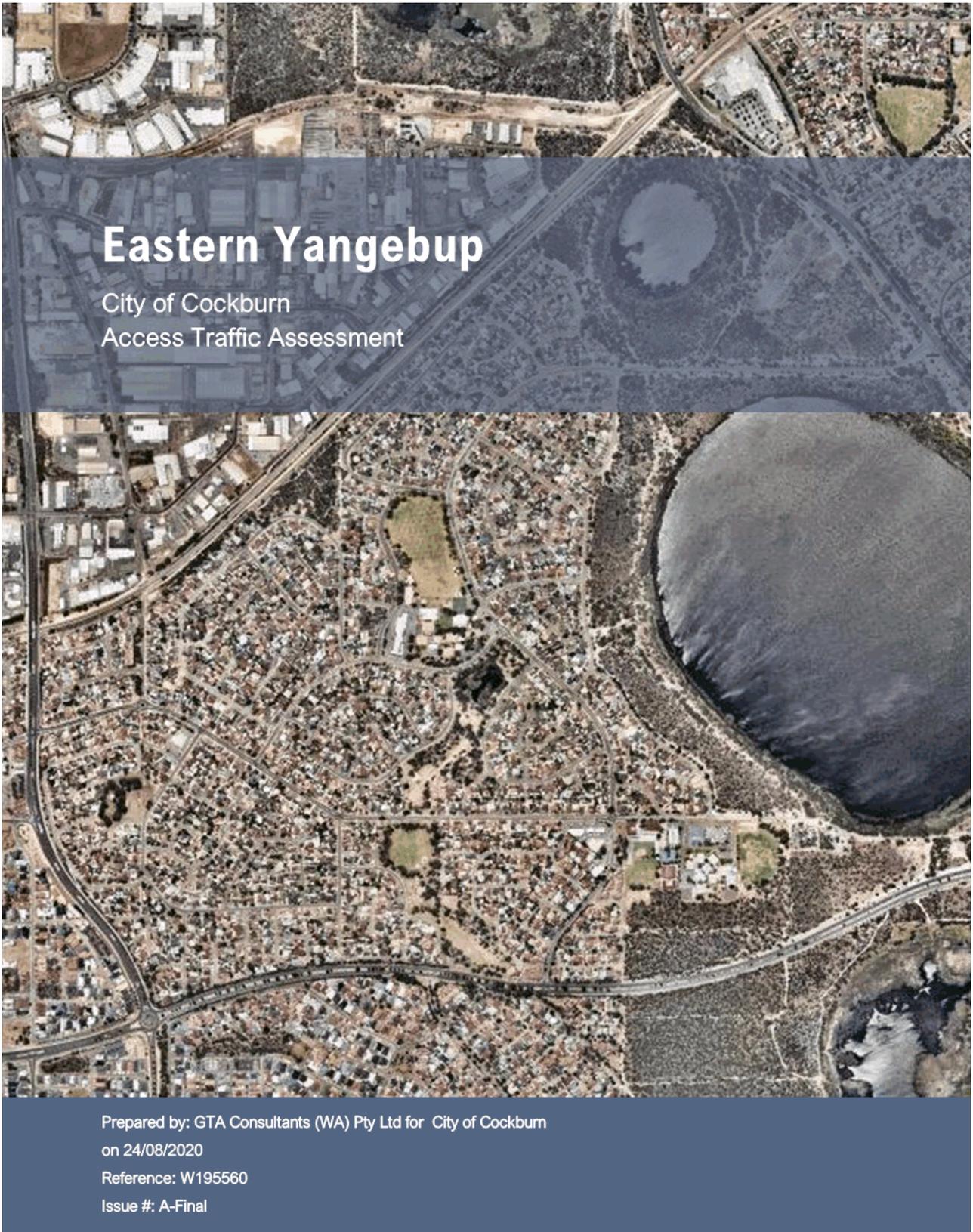
Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil





Eastern Yangebup

City of Cockburn
Access Traffic Assessment

Prepared by: GTA Consultants (WA) Pty Ltd for City of Cockburn
on 24/08/2020
Reference: W195560
Issue #: A-Final



Eastern Yangebup

City of Cockburn Access Traffic Assessment

Client: City of Cockburn
on 24/08/2020
Reference: W195560
Issue #: A-Final

Quality Record

| Issue | Date | Description | Prepared By | Checked By | Approved By | Signed |
|---------|------------|---|-------------|------------|-------------|-----------------|
| A-PrDr | 21/08/2020 | Preliminary Draft prior to review | RD | | | - |
| A-Dr | 23/08/2020 | Draft – updated to address comments from KC | RD | KC | | - |
| A-Final | 24/08/2020 | Final – no edits to Draft | RD | KC | KC | <i>Karalogo</i> |
| | | | | | | |

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EXECUTIVE SUMMARY

1. EXECUTIVE SUMMARY

The City of Cockburn is currently assessing the requirements for the future network servicing the suburb of Yangebup (east of Spearwood Avenue, north of Beelias Drive and west of North Lake Road).

As part of this work, the City's Engineering Services is investigating the impacts of intersections accessing this part of Yangebup with the effects of both internal traffic growth due to additional lots being developed and the increase of external traffic on roads bordering the area.

The purpose of this report is to summarise the results of the assessment primarily undertaken using Sidra Intersection 8 at the key intersection access points:

- Spearwood Avenue and Yangebup Road
- Beelias Drive and Dunraven Drive
- Osprey Drive and North Lake Road

The results of the SIDRA analysis will assist the City with determining the best possible treatment(s) during the progress of time from current 2020 to approximately 2031.

Background

The suburb of Yangebup east of Spearwood Avenue, north of Beelias Drive and west of Osprey Drive has developed over the years as the surrounding regional road network has taken shape and modified previous established road networks.

This area of Yangebup initially had Yangebup Road as the main east-west arterial between the Kwinana Freeway in the east and Stock Road in the west. This also included Miguel Road connecting into the Spearwood industrial area via a railway level crossing on the north side of the area.

Over the years, Spearwood Avenue has seen to extend to a new arterial road and Beelias Drive which now extends from Yangebup Road through to Stock Road and replaced the function of Yangebup Road. Also, this has seen Osprey Drive being built and extended through to North Lake Road at a signalised intersection.

These works have also seen the removal of the Miguel Road level crossing and Yangebup Road south of Yangebup Road, through sensitive wetlands.

This now leaves this part of Yangebup with three access points:

- Spearwood Avenue at Yangebup Road
- Beelias Drive at Dunraven Drive
- Osprey Drive at North Lake Road

With the expected additional 240 single dwelling lots within this area of Yangebup and the additional traffic flows on the arterial roads of Spearwood Avenue, Beelias Drive and North Lake Road surrounding the area, there is a uncertainty that these access points will be insufficient in their present format and perhaps number of access points to meet the demand of the expected traffic flow increases.

EXECUTIVE SUMMARY

Model Options and Scenarios Tested

The City wanted to test if the current number of and form of intersections would be sufficient to meet the current and future traffic demands.

While investigations are agreed to use SIDRA Intersection 8 at this stage of the assessment, it is also understood that if the intersection treatments were to be changed to meet future demands, then the City will undertake the subsequent modelling, approvals and costs associated with future traffic modelling of the intersections at that time.

GTA Consultants has been engaged by the City to complete the Base Model (2020) and Scenario Testing for the forecast year of 2031.

The following intersection options and scenarios have been modelled:

Scenario 1a – Current intersections with current 2020 traffic flows

Scenario 1b – Current and modified intersection(s) with expected 2031 traffic flows (DO NOTHING CASE)

Scenario 2b – Current and modified intersection(s) with expected 2031 traffic flows (DO SOMETHING CASE)

Findings

The traffic assessment of the current and future road network layout has shown that the road network is expected to operate satisfactorily until 2031 with a single exception. The model results are summarised below.

Intersection of Spearwood Avenue and Yangebup Road

This intersection was found to have delays for the right turn movement from Yangebup Road, this being the most critical movement for this intersection. With the modest traffic flow increases expected both on Spearwood Avenue and Yangebup Road, this performance of this intersection is considered acceptable up to accommodate traffic volumes to approximately 2031. At this point, there is expected to be delays of approximately 90 seconds for the right turn from Yangebup Road, and this typically during the period immediately after the completion of a school day (approximately 3:15pm to 3:30pm). Outside of this time, the delays are expected to be somewhat less than this and acceptable. Based on a short period of delay and only during school days, there is no need to modify this intersection in the short term or by 2031.

Intersection of Beelihar Drive and Dunraven Drive

This intersection currently experiences significant delays and queues for the critical right turns to and from Dunraven Drive. In the AM peak, there are currently long queues on Beelihar Drive with cars stopped to turn right into Dunraven Drive. This then impacts on the ability of vehicles in Dunraven Drive to safely turn right onto Beelihar Drive. In the PM peak, the issue becomes the right turn from Dunraven Drive, with long delays which may lead to risk taking manoeuvres due to motorists taking smaller gaps in traffic which may not be achievable.

Intersection of North Lake Road and Osprey Drive

This intersection currently operates at a very good Level of Service (LoS) B during both the AM and PM peak periods and this is expected to be the case in 2031 with increased internal traffic flows and increased external traffic flows. There is no requirement to modify this intersection in the short term or by 2031.

EXECUTIVE SUMMARY

Recommendations

Apart from the intersection of Beeliam Drive and Dunraven Drive, no specific mitigation measures are proposed due to the above assessment as the delays, Levels of Service and queues were all found to be within acceptable limits. There is expected to be slightly longer delays and queues in comparison to the current operation of the road network, but these are all expected to be at acceptable levels.

Intersection of Spearwood Avenue and Yangebup Road

There is no need to modify the intersection of Spearwood Avenue and Yangebup Road in the short term or by 2031 as the delays to the critical right turn from Yangebup Road is considered acceptable. Beyond 2031 there may be the requirement to address the increasing delays experienced by vehicles turning right from Yangebup Road.

Intersection of Beeliam Drive and Dunraven Drive

The issue with the current delays experienced by vehicles turning right from Beeliam Drive into Dunraven Drive is the requirement for the right turn traffic to give way to opposing left turn traffic on the western approach of Beeliam Drive. This left turn has a high volume of traffic and thus reduces the available gaps in traffic for the right turn traffic. To address this, it is suggested that the left turn be placed under Give Way control with a splitter island, similar in format to the left turn island on the northern approach to the intersection of Spearwood Avenue and Yangebup Road. It is recommended that this needs to be implemented in the short term as the performance of the right turn into Dunraven Drive from Beeliam Drive affects both the safety of right turn traffic and through traffic approaching this intersection from the east.

In the longer term as traffic flows increase, delays for vehicles turning right from Dunraven into Beeliam Drive are expected to get to a level which will be unacceptable and lead to safety concerns. This is expected to be in approximately 2-3 years' time based on application of growth rates until the delays for right turns exceeding 90 seconds. By 2031, the delays will be significantly unacceptable.

To address this, it is suggested that one of a possible selection of treatments could be implemented, this being the creation of a new intersection near where Milgun Drive meets Beeliam Drive.

Intersection of North Lake Road and Osprey Drive

There is no need to modify this intersection in the short term or by 2031 as the performance of this intersection is expected to remain acceptable with traffic growth.

INTRODUCTION

2. INTRODUCTION

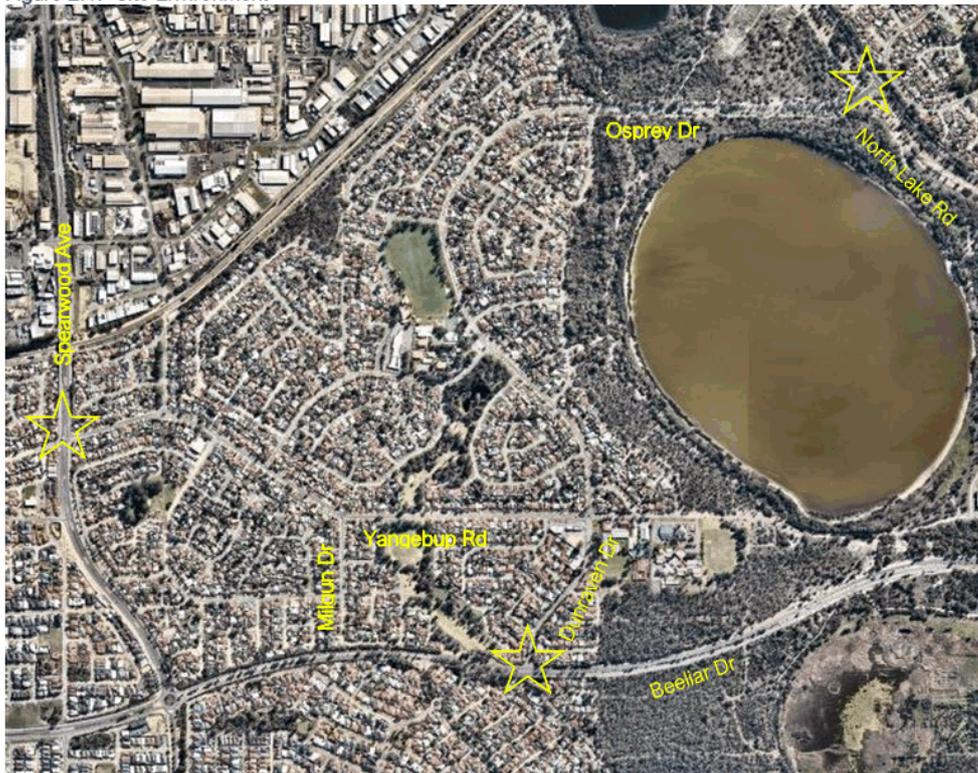
2.1. Project Location and Traffic Modelling Area

2.1.1. Project Location

This report focuses on the traffic impact assessment and road network analysis modelling of the current and future key entry intersections to the suburb of Yangebup bordered by Spearwood Avenue, Beelihar Drive and North Lake Road, as shown in Figure 2.1. Access to and from this part of Yangebup is currently provided by the following:

- Unsignalised T-junction of Spearwood Avenue and Yangebup Road at the western side of the subject area
- Unsignalised T-junction of Beelihar Drive and Dunraven Drive at the southern side of the subject area
- Signalised T-junction of North Lake Road and Osprey Drive at the eastern side of the subject area

Figure 2.1: Site Environment



(map source: Google)

Figure 2.1 also denotes the traffic modelling area of focus for this report with the key focus of the intersections controlling access to and from the area.

INTRODUCTION

Concerns have been aired by the City with regard the ability of this area to be serviced by these three intersections in their present layout with an expected increase in development within the area and with expected traffic flow increases on the three arterial roads bordering the area.

This report has been prepared for the City of Cockburn.



W195560 // **24/08/2020**
Access Traffic Assessment // Issue: A-Final
Eastern Yangebup, City of Cockburn



MODELLING SCENARIOS FOR THE STUDY

3. MODELLING SCENARIOS FOR THE STUDY

As discussed above, the current development within this area of Yangebup is expected to yield approximately 240 additional single dwelling lots over the forthcoming years, these being on larger lots capable of accommodating additional lots under the current zoning.

In addition to this, there is expected to be traffic growth on the external arterial roads bordering the area due to development within the wider City of Cockburn.

Accordingly, the traffic modelling scenarios (years, approximately) to determine the likely traffic impacts of the combination of internal increase in lots and traffic growth on the adjacent arterial road network have been selected as:

- Current Operation of Intersections 2020
- Future Operation of Intersections (Do Nothing) 2031
- Future Operation of Intersections (Do Something) 2031

The year 2031 was chosen as the target year as traffic flows for the arterial road network and has been based on the City of Cockburn's 2018 District Traffic Study. This study had 2031 as the final assessment year.

The traffic operations of the current (2020) and future (2031) intersections were assessed for the following:

- Current Road network:

Intersection 1 Spearwood Avenue/Yangebup Road (western intersection)
 Intersection 2 Beelliar Drive/Dunraven Drive (southern intersection)
 Intersection 3 North Lake Road/Osprey Drive (eastern intersection)

- Future Road network:

Intersection 1 Spearwood Avenue/Yangebup Road (western intersection)
 Intersection 2 Beelliar Drive/Dunraven Drive (southern intersection)
 Intersection 3 North Lake Road/Osprey Drive (eastern intersection)

These intersections were then assessed using Sidra Intersection 8, modelling the operation in both the current 2020 based on current traffic flows observed in August 2020 and then in 2031 with the impacts of both the internal and external growth in traffic flows.

ASSESSED OPTIONS

4. ASSESSED OPTIONS

4.1. Proposed Intersection Modelling Scenarios

4.1.1. Proposed Modelling Scenarios

A total of two scenarios were modelled for comparison purposes with three separate intersections modelled. Table 4.1 defines the scenarios as modelled by GTA in SIDRA Intersection 8. The current layout and accesses were modelled to set a base line and then the 'future' was assessed to examine if the fully developed Yangebup could operate satisfactorily with the new proposed accesses.

If there was found to be any issues with the operation of intersections around the future developed Yangebup, then further assessment was to be undertaken to examine if these intersections could operate satisfactorily to a point and then changes that may then be required subsequent to this.

The initial 'base case' current road network and 'future' networks are summarised below.

Table 4.1: SIDRA Scenarios

| Scenario | 2020 (Base – current "a") | 2031 (Future "b") |
|--|------------------------------|----------------------|
| Scenario 1 – Current Road Network | | |
| Intersection 1 | AM/PM | AM/PM |
| Intersection 2 | AM/PM | AM/PM |
| Intersection 3 | AM/PM | AM/PM |
| Scenario 2 – Do Something | | |
| Intersection 1 | Not assessed | AM/PM |
| Intersection 2 | Not assessed | AM/PM |
| Intersection 3 | Not assessed | AM/PM |

4.1.2. Traffic Growth Assumptions

As discussed previously, there are two elements for the growth in traffic for this area of Yangebup, internal and external.

The City of Cockburn has advised that there is expected to be an additional 240 lots able to be developed within this area of Yangebup under the current zoning. This is expected to be on larger and corner lots and be typical in-fill development.

The traffic flow on the external roads are expected to be through future development within the wider City of Cockburn.

Each of these types of growth have been assumed to be mutually exclusive for the purposes of estimations of future traffic flows.



ASSESSED OPTIONS

The internal increase in the number of lots is expected to yield an approximately 1,920 vehicle trips per day. Some of these trips are expected to be wholly within the subject area due to three schools and a neighbourhood centre also being within the area. A conservative 20% deduction on these trips has been applied and thus there is expected that approximately 1,540 of these trips will be to external locations outside the subject area. This increase in the number of lots has been assumed to occur fully by 2031.

Examining the current derived traffic flows on the three main exit roads (Yangebup Road, Dunraven Drive and Osprey Drive), these roads currently carry approximately 23,400 vehicles per day (vpd). Applying the above additional 1,540 vehicles per day, this works out an expected internal growth rate of approximately +0.58% per annum to 2031.

Internal to External Traffic Growth +0.58% pa

For the external arterial roads, the City of Cockburn's 2018 District Traffic Study was utilised to derive growth rates from 2021 to 2031.

The adopted **external** annual increase in traffic flows for these roads are detailed in Table 4.2 and summarised below:

- **Spearwood Avenue** +0.25% pa
- **Beeliar Drive** +2.74% pa
- **North Lake Road** +3.19% pa

Table 4.2: Derived Annual Arterial Traffic Increases

| Location | 2021 | | 2031 | | 2021 to 2031 Annual Growth | | Adopted Average (pa) |
|--|------|------|------|------|----------------------------|--------|----------------------|
| | AM | PM | AM | PM | AM | PM | |
| Spearwood Avenue North of Beeliar Drive | 3160 | 3047 | 3026 | 3344 | -0.43% | +0.93% | +0.25% |
| Beeliar Drive east of Dunraven Drive | 6908 | 5740 | 9916 | 6861 | +3.68% | +1.80% | +2.74% |
| North Lake Road east of Osprey Drive | 4727 | 4664 | 6497 | 6363 | +3.23% | +3.16% | +3.19% |

Traffic flows provided by the City of Cockburn and counted by Matrix Traffic & Transport Data are provided in Appendix A. These recorded traffic flows were at the intersections surrounding subject area and at two internal intersections. The data was undertaken in February 2020 (City of Cockburn data) and in August 2020 (Matrix data). In addition to this, SCATS data was used for the intersection of North Lake Road and Osprey Drive intersection assessment.

SCENARIO 1A – CURRENT ROAD NETWORK - 2020

5. SCENARIO 1A – CURRENT ROAD NETWORK - 2020

5.1. Scenario 1a - Traffic Assumptions

For the assessment of Scenario 1 – 2020 current intersections and road network, the current traffic flows collated by the City of Cockburn and as surveyed by GTA through Matrix Traffic & Transport Data and SCATS data were utilised.

5.2. Scenario 1a - Traffic Performance Assessment

5.2.1. Layouts (2020 Base)

The geometric layouts of the intersections for the Sidra assessment of the current operations are based on the current road layout configurations. These are described below.

The proposed modifications to the current road access are discussed below.

Intersection 1 Spearwood Avenue/Yangebup Road (western access)

This intersection is presently an all movement T-junction intersection with Spearwood Avenue the priority road and Yangebup Road the terminating road. The Yangebup Road approach is controlled by Give Way signage with separate left and right turn lanes. The left turn lane on the Yangebup Road approach is approximately 50m long whilst the right turn lane is a full-length lane. The right turn from Yangebup Road becomes an acceleration lane in Spearwood Avenue with an acceleration length of 135m. Yangebup Road is subject to the general built up speed limit of 50km/h.

Spearwood Avenue is a dual carriageway with left and right turn dedicated lanes into Yangebup Road. It has a 70km/h speed zone. The right turn lane is approximately 85m in length whilst the left turn lane (with Give Way control) is approximately 115m in length.

This intersection was modified in the second half of 2018 with the duplication of Spearwood Avenue. Prior to the duplication, this intersection experienced the following crash history over a 3.5-year period:

| | | |
|----------------------|----|-----------------------|
| • Total | 22 | (approximately 6.3pa) |
| • Right Angle | 9 | (approximately 2.6pa) |
| • Right Turn Through | 3 | (approximately 0.9pa) |
| • Rear End | 8 | (approximately 2.3pa) |

Of these crashes, 6 were casualty crashes (one was a KSI, this being either a fatal or serious injury) and 16 property damage only crashes.

In the 10-month period from March 2019 to December 2019, the following crashes were recorded:

| | | |
|----------------------|---|-----------------------|
| • Total | 9 | (approximately 2.4pa) |
| • Right Angle | 2 | (approximately 2.4pa) |
| • Right Turn Through | 1 | (approximately 1.2pa) |
| • Rear End | 0 | (approximately 0.0pa) |

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Of these crashes ALL were property damage only crashes.

The slight increase in right turn through crashes is possibly related to the increase difficulty picking gaps through two lanes of oncoming traffic compared to the previous single lane of oncoming traffic.

From the above analysis, it is clear that the intersection modifications have eliminated rear end crashes whilst the rate of right angle and right turn through crashes seems to be the same. The overall rate of crashes for the entire intersection has dropped significantly.

Figure 5.1: Intersection of Spearwood Avenue & Yangebup Road



Intersection 2 Beeliar Drive/Dunraven Drive (southern access)

This intersection is presently an all movement T-junction intersection with Beeliar Drive the priority road and Dunraven Drive the terminating road. The Dunraven Drive approach is controlled by Give Way signage with separate left and right turn lanes. The left turn lane on the Dunraven Road approach is approximately 45m long and has a splitter island whilst the right turn lane is full-length. The right turn from Dunraven Drive is into a 7.2m wide median in Beeliar Drive which allows a two-stage crossing or vehicles turning right from Dunraven Drive into Beeliar Drive. Dunraven Drive is subject to the general built up speed limit of 50km/h.

Beeliar Drive is a dual carriageway with left and right turn lanes into Dunraven Drive. It is speed zoned at 70kmh. The right turn lane is approximately 90m in length whilst the left turn lane is approximately 65m in length and this movement has priority over right turn movements from Beeliar Drive.

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This intersection has not been modified in the past 5 years and has experienced the following crash history over this 5-year period:

- Total 17 (approximately 3.4pa)
- Right Angle 4 (approximately 0.8pa)
- Right Turn Through 2 (approximately 0.4pa)
- Rear End 10 (approximately 2.0pa)

Of these crashes, 3 were casualty crashes (Nil KSI) and 14 property damage only crashes.

The right angle and right turn through crashes are probably a function of the difficulty in turning right to and from the intersection due to large delays in peak periods. The rear end crashes may be related to the queues on Beeliar Drive in the AM peak.

Figure 5.2: Intersection of Beeliar Drive & Dunraven Drive



Intersection 3 North Lake Road/Osprey Drive (eastern access)

This intersection is a signalised T-junction with North Lake Road the priority road and Osprey Road the terminating road. North Lake Road is a 4-lane road and is subject to a 70km/h posted speed limit while the Osprey Drive approach is subject to a 60km/h posted speed limit with a double right turn into North Lake Road with of those lanes being a 100m short lane.

The traffic signals operate in a 3-phase format with:

- Phase A Through movements on North Lake Road approaches
- Phase C Osprey Drive Right turn
- Phase E Through movements and Right turn on North Lake Road north/west approach

In peak periods the cycle lengths are approximately 160s in the AM and PM peaks.

There are left turn slip lanes on the Osprey Drive and North Lake Road south/east approach of 90m and 105m length respectively. There is a right turn lane on the north/west approach of approximately 100m length.



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The North Lake Road approaches have mast arm traffic signals installed.

Figure 5.3: Intersection of North Lake Road & Osprey Drive



This intersection has not been modified in the past 5 years and has experienced the following crash history over this 5-year period:

- Total 22 (approximately 4.4pa)
- Right Angle 2 (approximately 0.4pa)
- Right Turn Through 2 (approximately 0.4pa)
- Rear End 15 (approximately 3.0pa)
- Sideswipe 2 (approximately 0.4pa)
- Non-collision 1 (approximately 0.2pa)

Of these crashes, 6 were casualty crashes (one was a KSI) and 16 property damage only crashes.

The high rate of rear end crashes is considered typical of traffic control signals, whilst there are the occasional right and right turn through crashes related to drivers disobeying the traffic control. There are two sideswipe crashes probably related to the double right turn from Osprey Drive into North Lake Road.

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5.2.2. Scenario 1 - Current Operation (2020 Base)

Based on the observed traffic flows and the current geometric road configurations the base line traffic modelling were assessed.

The results of this assessment are summarised in the following tables with copies of the detailed Sidra outputs in Appendix B.

Intersection of Spearwood Avenue and Yangebup Road

Table 5.1: Intersection 1 2020 Existing AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Spearwood Ave (SOUTH) | | | | 0 |
| Lane 1 | T | 0.30 | | |
| Lane 2 | T | 0.30 | | |
| Lane 3 | R | 0.19 | 6 | 9 |
| Yangebup Road (EAST) | | | | |
| Lane 1 | L | 0.28 | 9 | 8 |
| Lane 2 | R | 0.65# | 31 | 19# |
| Spearwood Ave (NORTH) | | | | |
| Lane 1 | L | 0.11 | 3 | 7 |
| Lane 2 | T | 0.16 | | |
| Lane 3 | T | 0.16 | | |
| Intersection | ALL | 0.65 | | 4/C |

Note: # = Worst DoS or Delay defining the overall Intersection DoS or Intersection LoS



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Table 5.2: Intersection 1 2020 Existing PM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Spearwood Ave (SOUTH) | | | | 0 |
| Lane 1 | T | 0.18 | | |
| Lane 2 | T | 0.18 | | |
| Lane 3 | R | 0.27 | 8 | 17 |
| Yangebup Road (EAST) | | | | |
| Lane 1 | L | 0.47 | 16 | 18 |
| Lane 2 | R | 0.76# | 29 | 54# |
| Spearwood Ave (NORTH) | | | | |
| Lane 1 | L | 0.22 | 7 | 7 |
| Lane 2 | T | 0.40 | | |
| Lane 3 | T | 0.40 | | |
| Intersection | ALL | 0.76 | | 5/F |

From the above calibrated assessment, this current intersection at Spearwood Avenue and Yangebup Road operates acceptable in the AM and PM peaks. The critical movement is the right turn from Yangebup Road into Spearwood Avenue. Currently this has high Degree of Saturation (DoS) in both peak periods with delays close to a minute in the PM peak. With these delays the right turn effectively operates at a Level of Service (LoS) C in the AM peak and F in the PM peak. With the delays currently experienced, these were observed to typically occur over a 10-15 minute period between 3:15 to 3:25/3:30pm and most likely related to school traffic exiting the Yangebup Road area. Outside these times, the queues and delays observed were less than the above modelled results.

Intersection of Beelihar Drive and Dunraven Drive

The next intersection assessed was the southern intersection of Beelihar Drive and Dunraven Drive. The results of this assessment are shown in the following two tables.



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Table 5.3: Intersection 2 2020 Existing AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|----------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Dr (EAST) | | | | 0 |
| Lane 1 | T | 0.33 | | |
| Lane 2 | T | 0.33 | | |
| Lane 3 | R | 1.05# | 134 | 113# |
| Dunraven Dr (NORTH) | | | | |
| Lane 1 | L | 0.29 | 9 | 8 |
| Lane 2 | R | 0.32 | 8 | 63 |
| Beeliar Dr (WEST) | | | | |
| Lane 1 | L | 0.18 | | 6 |
| Lane 2 | T | 0.28 | | |
| Lane 3 | T | 0.28 | | |
| Intersection | ALL | 1.05 | | 19/F |

Table 5.4: Intersection 2 2020 Existing PM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|----------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Dr (EAST) | | | | 0 |
| Lane 1 | T | 0.34 | | |
| Lane 2 | T | 0.34 | | |
| Lane 3 | R | 0.71 | 24 | 37 |
| Dunraven Dr (NORTH) | | | | |
| Lane 1 | L | 0.37 | 13 | 10 |
| Lane 2 | R | 0.90# | 31 | 145# |
| Beeliar Dr (WEST) | | | | |
| Lane 1 | L | 0.10 | | 6 |
| Lane 2 | T | 0.34 | | |
| Lane 3 | T | 0.34 | | |
| Intersection | ALL | 0.90 | | 10/F |

This intersection was observed to have two critical issues:

- Right turn from Beeliar Drive into Dunraven Drive
- Right turn from Dunraven Drive into Beeliar Drive

The above calibrated assessments of this intersection performance closely match the observed queues/delays for this intersection in the current peaks.



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In the AM peak there is presently long queues for the right turn from Beelias Drive into Dunraven Drive due to the volumes of traffic on Beelias Drive travelling west to east and also turning left into Dunraven Drive. In the AM peak this is currently approximately 1,250vph. The right turn traffic currently gives way to both through and left turn traffic on the western approach to this intersection. Under this condition, the queues on Beelias Drive turning right typically extend beyond the current 90m long right turn lane and into the right lane of Beelias Drive. This turn is under LoS F under these conditions.

In the afternoon, the critical movement becomes the right turn from Dunraven Drive into Beelias Drive where delays extend to over 2-minutes with a high DoS of 0.90. This turn is at LoS F under these conditions.

Under these current conditions, the current intersection is considered to fail. The long delays for turning right from Dunraven Drive will lead to risk taking manoeuvres whilst the long queues on Beelias Drive will start to lead to rear end crashes and the tail end of the queue backed onto Beelias Drive.

This intersection should be considered for a possible upgrade some of which could be:

- Modification of channelisation (lengthening of right turn lane on Beelias Drive and/or modification of the left turn on the western approach);
- Banning Movements (right turn from Dunraven Drive);
- Installation of traffic signals; or,
- roundabout control.

The final two options are not considered viable as the banning of movements will require longer internal trips within the subject area and may not be acceptable to local residents whilst the provision of a roundabout (expected to be approximately 60m inscribed diameter) would not physically fit within the road reserve without expensive and protracted land acquisition. This size and diameter of the roundabout is required as Beelias Drive is presently on the Restricted Access Vehicle (RAV) Route for vehicles up the Network 4 size.

The provision of traffic signals would most likely not be supported by Main Roads WA as this intersection is on the RAV route and this control would add additional delays to heavy vehicles on Beelias Drive. The only exception would possibly be that if the assessment of future traffic flows would not allow this intersection to operate in any modified form and traffic signals were the only option available to allow this intersection to operate effectively.

The above first option of the change in priority for the left turn movement on the western approach from a free left turn to a left turn slip under Give Way control, would allow right turn traffic on Beelias Drive to then have priority over this traffic instead of the other way round.

This alternative was assessed, and the results are shown in the following two tables.

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Table 5.5: Intersection 2 2020 Existing AM Peak Operating Conditions with Modified Western LT

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.33 | | |
| Lane 2 | T | 0.33 | | |
| Lane 3 | R | 0.46# | 17 | 15 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.29 | 9 | 8 |
| Lane 2 | R | 0.22 | 5 | 40# |
| Beeliar Drive (WEST) | | | | |
| Lane 1 | L | 0.25 | 8 | 8 |
| Lane 2 | T | 0.28 | | |
| Lane 3 | T | 0.28 | | |
| Intersection | ALL | 0.46 | | 5/E |

Table 5.6: Intersection 2 2020 Existing PM Peak Operating Conditions with Modified Western LT

| Approach | Performance Criteria | | | |
|----------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Dr (EAST) | | | | 0 |
| Lane 1 | T | 0.34 | | |
| Lane 2 | T | 0.34 | | |
| Lane 3 | R | 0.34 | 10 | 16 |
| Dunraven Dr (NORTH) | | | | |
| Lane 1 | L | 0.37 | 13 | 10 |
| Lane 2 | R | 0.64# | 17 | 65# |
| Beeliar Dr (WEST) | | | | |
| Lane 1 | L | 0.13 | 4 | 7 |
| Lane 2 | T | 0.34 | | |
| Lane 3 | T | 0.34 | | |
| Intersection | ALL | 0.64 | | 5/F |

With the modification of the left turn on the western approach, the 24 vehicles turning right from Beeliar Drive in the AM peak would have priority over the 310 vehicles turning left from Beeliar Drive. This would free up the traffic flow for the right turn into Dunraven Drive and would then allow gaps for traffic in Dunraven Drive to turn right into Beeliar Drive. The modification of the left turn slip lane on the west approach would also make



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the right turn easier from Dunraven Drive as these drivers would be more sure if a vehicle is indeed turning left into Dunraven Drive and not continuing through the intersection.

So as a short-term win, it is recommended to modify the left turn lane on the western approach to a left turn slip lane under Give Way control.

Intersection of North Lake Road and Osprey Drive

This intersection was assessed utilising the current phase times and traffic flows recorded prior to the intersection being modified with new pavement and thus removing several loops. The traffic counts used were from December 2019. The performance of this intersection under its current format is summarised below.

This assessment was based on the current 160s cycle time and current average phase times. These are:

- Phase A AM:109s PM:97s
- Phase C AM:27s PM:33s
- Phase D AM:25s PM:30s

Table 5.7: Intersection 3 2020 Existing AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|--------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| North Lake Road (SOUTH) | | | | 0 |
| Lane 1 | L | 0.04 | 2 | 7 |
| Lane 2 | T | 0.29# | 42 | 7 |
| Lane 3 | T | 0.29 | 42 | 7 |
| North Lake Road (NORTH) | | | | |
| Lane 1 | T | 0.11 | 22 | 4 |
| Lane 2 | T | 0.11 | 22 | 4 |
| Lane 3 | R | 0.17 | 18 | 77 |
| Osprey Drive (WEST) | | | | |
| Lane 1 | L | 0.11 | 1 | 7 |
| Lane 2 | R | 0.21 | 3 | 75 |
| Lane 3 | R | 0.21 | 3 | 75 |
| Intersection | ALL | 0.29 | | 13/B |



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Table 5.8: Intersection 3 2020 Existing AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|--------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| North Lake Road (SOUTH) | | | | 0 |
| Lane 1 | L | 0.07 | 5 | 7 |
| Lane 2 | T | 0.23 | 43 | 12 |
| Lane 3 | T | 0.23 | 43 | 12 |
| North Lake Road (NORTH) | | | | |
| Lane 1 | T | 0.18 | 43 | 6 |
| Lane 2 | T | 0.18 | 43 | 6 |
| Lane 3 | R | 0.26# | 36 | 73 |
| Osprey Drive (WEST) | | | | |
| Lane 1 | L | 0.08 | 6 | 7 |
| Lane 2 | R | 0.20 | 30 | 68 |
| Lane 3 | R | 0.20 | 30 | 68 |
| Intersection | ALL | 0.26 | | 20/B |

From the above, it is apparent that this intersection currently operates at a very good Level of Service in both the AM and PM peaks. Under the current traffic flows and geometry/control, this intersection is considered acceptable.

Assessment of the overall performance of the road network and entries/exits of the current network surrounding the subject area is satisfactory except for the intersection of Beelihar Drive and Dunraven Drive. All intersections currently operate with acceptable Degrees of Saturation with accompanying **low delays** and **short queues**. These Level of Service principles are discussed in more detail below.

5.3. Level of Services Concepts

The level of service concept describes the quality of traffic service in terms of six levels, designated A to F, with level of service A (LOS A) representing the best operating condition (i.e. at or close to free flow), and level of service F (LOS F) the worst (i.e. forced flow). More specifically:

- *LOS A:* Primarily free flow operations at average travel speeds, usually about 90% of the FFS (free flow speed) for the given street class. Vehicles are completely unimpeded in their ability to manoeuvre within the traffic stream. Control delay at signalised intersections is less than 10 seconds. At non-signalised movements at intersections the average control delay is less than 10 seconds;
- *LOS B:* Reasonably unimpeded operations at average travel speeds, usually about 70% of the FFS for the street class. The ability to manoeuvre within the traffic stream is only slightly restricted, and control delays at signalised intersections are between 10 and 20 seconds. At non-signalised movements at intersections the average control delay is between 10 and 15 seconds;
- *LOS C:* Stable operations; however, ability to manoeuvre and change lanes in mid-block locations may be more restricted than at LOS B, and longer queues, adverse signal coordination, or both may



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contribute to lower average travel speeds of about 50% of the FFS for the street class. Signalised intersection delays are between 20 and 35 seconds. At non-signalised movements at intersections the average control delay is between 15 and 25 seconds;

- *LOS D:* A range in which small increases in flow may cause substantial increases in delay and decreases in travel speed. LOS D may be due to adverse signal progression, inappropriate signal timing, high volumes, or a combination of these factors. Average travel speeds are about 40% of FFS. Signalised intersection delays are between 35 and 55 seconds. At non-signalised movements at intersections the average control delay is between 25 and 35 seconds;
- *LOS E:* Characterised by significant delays and average travel speeds of 33% of the FFS or less. Such operations are caused by a combination of adverse progression, high signal density, high volumes, extensive delays at critical intersections (between 55 and 80 seconds), and inappropriate signal timing. At non-signalised movements at intersections the average control delay is between 35 and 50 seconds; and,
- *LOS F:* Characterised by urban street flow at extremely low speeds, typically 25% to 33% of the FFS. Intersection congestion is likely at critical signalised locations, with high delays (in excess of 80 seconds), high volumes, and extensive queuing. At non-signalised movements at intersections the average control delay is greater than 50 seconds.

In addition to the above:

- Average Delay: is the average of all travel time delays for vehicles through the intersection
- Queue: is the queue length below which 95% of all observed queue lengths fall
- Degree of Saturation: Ratio of the traffic flow to the capacity for that particular lane/movement.

The above has been summarised in Table 5.9.

Table 5.9: Sidra Performance Measures

| LoS | | Intersection Degree of Saturation (DoS, X) | |
|-----|------------|--|-------------------------|
| | | Unsignalised Intersection | Signalised Intersection |
| A | Excellent | <=0.50 | <=0.60 |
| B | Very Good | 0.50-0.70 | 0.60-0.75 |
| C | Good | 0.70-0.80 | 0.75-0.90 |
| D | Acceptable | 0.80-0.90 | 0.90-0.95 |
| E | Poor | 0.90-1.00 | 0.95-1.00 |
| F | Very Poor | >=1.0 | >=1.0 |



SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

6. SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

6.1. Scenario 1b – 2031 Do Nothing Traffic Assumptions

For the future year assessment, the road network was assumed to remain as it is presently the case, with the exception for the intersection of Beeliar Drive and Dunraven Drive. As assessed in the previous section, this intersection should be modified in the short term with the conversion of the left turn lane on the western approach of Beeliar Drive to a left turn slip lane.

The traffic growth for the various movements was based on the expected internal growth due to future development of 240 lots and with the impacts on external traffic growth of the surrounding arterial road network.

These are summarised below once more:

- **Internal to External Traffic Growth** +0.58% pa
- **Spearwood Avenue** +0.25% pa
- **Beeliar Drive** +2.74% pa
- **North Lake Road** +3.19% pa

6.2. Scenario 1b - Traffic Performance Assessment

Based on the expected future traffic flows with the application of the above growth rates from 2020 to 2031 the expected performance of the three (3) access intersections are expected as below.

The results of this assessment are summarised in the following tables with copies of the detailed Sidra outputs at Appendix C.

SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

Intersection of Spearwood Avenue and Yangebup Road

Table 6.1: Intersection 1 2031 Future AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|---------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Spearwood Avenue (SOUTH) | | | | 0 |
| Lane 1 | T | 0.31 | | |
| Lane 2 | T | 0.31 | | |
| Lane 3 | R | 0.20 | 6 | 9 |
| Yangebup Road (EAST) | | | | |
| Lane 1 | L | 0.30 | 10 | 8 |
| Lane 2 | R | 0.72# | 37 | 22# |
| Spearwood Avenue (NORTH) | | | | |
| Lane 1 | L | 0.12 | 4 | 7 |
| Lane 2 | T | 0.16 | | |
| Lane 3 | T | 0.16 | | |
| Intersection | ALL | 0.72 | | 4/C |

Table 6.2: Intersection 1 2031 Future PM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|---------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Spearwood Avenue (SOUTH) | | | | 0 |
| Lane 1 | T | 0.19 | | |
| Lane 2 | T | 0.19 | | |
| Lane 3 | R | 0.30 | 9 | 18 |
| Yangebup Road (EAST) | | | | |
| Lane 1 | L | 0.52 | 19 | 19 |
| Lane 2 | R | 0.87# | 40 | 74# |
| Spearwood Avenue (NORTH) | | | | |
| Lane 1 | L | 0.24 | 7 | 7 |
| Lane 2 | T | 0.41 | | |
| Lane 3 | T | 0.41 | | |
| Intersection | ALL | 0.87 | | 6/F |



SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

From the above assessment, the intersection at Spearwood Avenue and Yangebup Road is expected to operate in a similar way as to how it presently operates. The determining critical movement for this intersection, the right turn from Yangebup Road is expected to have slightly longer delays and queues compared to the current performance in both the AM and PM peaks. Even though the LoS is F, it is presently LOS F in the PM peak and thus is considered acceptable, given this is approximately 10-11 years in the future.

Intersection of Beeliar Drive and Dunraven Drive

The next intersection assessed was the southern intersection of Beeliar Drive and Dunraven Drive. The results of this assessment is shown in the following two tables.

Table 6.3: Intersection 2 2031 Future AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.42 | | |
| Lane 2 | T | 0.42 | | |
| Lane 3 | R | 0.69# | 29 | 24 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.38 | 13 | 11 |
| Lane 2 | R | 0.42 | 10 | 82# |
| Beeliar Drive (WEST) | | | | |
| Lane 1 | L | 0.27 | 9 | 8 |
| Lane 2 | T | 0.36 | | |
| Lane 3 | T | 0.36 | | |
| Intersection | ALL | 0.69 | | 6/F |



SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

Table 6.4: Intersection 2 2031 Future PM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.45 | | |
| Lane 2 | T | 0.45 | | |
| Lane 3 | R | 0.58 | 18 | 28 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.52 | 19 | 15 |
| Lane 2 | R | 1.42# | 118 | 507# |
| Beeliar Drive (WEST) | | | | |
| Lane 1 | L | 0.14 | 4 | 7 |
| Lane 2 | T | 0.44 | | |
| Lane 3 | T | 0.44 | | |
| Intersection | ALL | 1.42 | | 18/F |

This intersection was found to have one critical issue, with this being the right turn movement from Dunraven Drive into Beeliar Drive. In the AM peak the delays are expected to double and to be similar to the expected delays for the previous Spearwood Avenue/Yangebup Road intersection in 2031. However, in the PM peak the delays are expected to be significant with 8-9 minute delays. This would not be acceptable, and intervention would be required. This scenario is assessed in the following section as the DO SOMETHING option.

Intersection of North Lake Road and Osprey Drive

This intersection was assessed utilising the current phase times and increased traffic flows as utilised above.

This assessment was based on the current 160s cycle time and current average phase times. These are:

- Phase A AM:109s PM:97s
- Phase C AM:27s PM:33s
- Phase D AM:25s PM:30s



SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

Table 6.5: Intersection 3 2031 Future AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|--------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| North Lake Road (SOUTH) | | | | 0 |
| Lane 1 | L | 0.04 | 5 | 7 |
| Lane 2 | T | 0.36# | 34 | 3 |
| Lane 3 | T | 0.36# | 34 | 3 |
| North Lake Road (NORTH) | | | | |
| Lane 1 | T | 0.15 | 31 | 4 |
| Lane 2 | T | 0.15 | 31 | 4 |
| Lane 3 | R | 0.35 | 21 | 88 |
| Osprey Drive (WEST) | | | | |
| Lane 1 | L | 0.14 | 6 | 6 |
| Lane 2 | R | 0.22 | 26 | 74 |
| Lane 3 | R | 0.22 | 26 | 74 |
| Intersection | ALL | 0.36 | | 10/B |

Table 6.6: Intersection 3 2031 Future AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|--------------------------------|----------------------|-------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| North Lake Road (SOUTH) | | | | 0 |
| Lane 1 | L | 0.07 | 5 | 7 |
| Lane 2 | T | 0.29# | 48 | 9 |
| Lane 3 | T | 0.29# | 48 | 9 |
| North Lake Road (NORTH) | | | | |
| Lane 1 | T | 0.23 | 52 | 5 |
| Lane 2 | T | 0.23 | 52 | 5 |
| Lane 3 | R | 0.29 | 39 | 74 |
| Osprey Drive (WEST) | | | | |
| Lane 1 | L | 0.10 | 5 | 6 |
| Lane 2 | R | 0.28 | 34 | 75 |
| Lane 3 | R | 0.28 | 34 | 75 |
| Intersection | ALL | 0.29 | | 15/B |

From the above, it is apparent that this intersection is still expected to operate at a very good level of service in both the AM and PM peaks. Under the current traffic flows and geometry/control, this intersection is



SCENARIO 1B – CURRENT ROAD NETWORK – 2031 (DO NOTHING)

considered acceptable and no modifications are required to cater for expected internal traffic growth and external arterial traffic growth.



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SCENARIO 2B –2031 (DO SOMETHING)

7. SCENARIO 2B –2031 (DO SOMETHING)

7.1. Scenario 2b – 2031 Do Nothing Traffic Assumptions

This scenario has the same underlying traffic and network assumptions as per the prior “Do Nothing” assessment.

The only difference is that a treatment is required to be implemented to address the high right turn delays for the right turns from Dunraven Drive into Beelias Drive.

In the context of addressing this wholly at this intersection, there would be three options:

- Banning Movements (right turn from Dunraven Drive);
- Installation of traffic signals; or,
- roundabout control.

Again, the last two options are possibly not considered viable due to previous aforementioned issues. The first option is a possibility in the longer term if an alternative right turn could be provided at another location on Beelias Drive.

One possible location could be near the western bend in Milgun Drive and Beelias Drive. See below

SCENARIO 2B –2031 (DO SOMETHING)

Figure 7.1: Possible Intersection Location



If this intersection were to be provided in a similar format to the modified intersection of Beelbar Drive and Dunraven Drive, there is expected to be a redistribution of traffic flow away from the intersection of Beelbar Drive and Dunraven Drive to this possible new location. No detailed civil engineering assessment or road design has been undertaken, but assuming the intersection could be constructed at this location with a fully indented right turn, the expected performance of the intersection of Beelbar Drive and Dunraven Drive would be similar to the following two tables.

7.2. Scenario 2b - Traffic Performance Assessment

Based on the expected future traffic flows with the application of the growth rates from 2020 to 2031, the expected performance of the access intersections are expected as below.

The results of this assessment are summarised in the following tables with copies of the detailed Sidra outputs in Appendix D.

Only Intersection 2 and the possible new intersection on Beelbar Drive have been assessed below. It is expected that these intersection modifications and a new intersection would have a negligible impact on the intersection of Spearwood Avenue / Yangebup Road and North Lake Road / Osprey Drive. The performance of these intersections would be close to the same as assessed in Section 6.



SCENARIO 2B –2031 (DO SOMETHING)

Table 7.1: Intersection 2 2031 Future AM Peak Operating Conditions with new Intersection/Mod. LT

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beelihar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.42 | | |
| Lane 2 | T | 0.42 | | |
| Lane 3 | R | 0.66# | 26 | 23 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.34 | 11 | 10 |
| Lane 2 | R | 0.10 | 2 | 60# |
| Beelihar Drive (WEST) | | | | |
| Lane 1 | L | 0.22 | 8 | 8 |
| Lane 2 | T | 0.36 | | |
| Lane 3 | T | 0.36 | | |
| Intersection | ALL | 0.66 | | 5/F |

Table 7.2: Intersection 2 2031 Future PM Peak Operating Conditions with new Intersection/Mod. LT

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beelihar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.45 | | |
| Lane 2 | T | 0.45 | | |
| Lane 3 | R | 0.44 | 12 | 25 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.47# | 16 | 14 |
| Lane 2 | R | 0.33 | 7 | 85# |
| Beelihar Drive (WEST) | | | | |
| Lane 1 | L | 0.08 | 2 | 7 |
| Lane 2 | T | 0.44 | | |
| Lane 3 | T | 0.44 | | |
| Intersection | ALL | 0.47 | | 4/F |

The above assessments of this intersection show that the operation is expected to be acceptable in both the AM and PM peak periods. The delays for the right turn from Dunraven Drive are expected to be similar to the delays at the intersection of Spearwood Avenue / Yangebup Road which is also considered acceptable. However, the delays are considered at the limit for this minor right turn movement and any further delays



SCENARIO 2B –2031 (DO SOMETHING)

would be a concern. However, it should be noted that in the PM peak these delays are due to 16 vehicle movements over an hour, which is a very low demand and the DoS is expected to be 0.33, which is also to be excellent based on Sidra Performance Measures.

Beyond this, it is suggested that the right turn could be banned at this intersection and all right turns be undertaken at the possible future intersection at Milgun Drive.

Prior to this banning and with the new intersection and redirected traffic flows, this new intersection is expected to have performance as summarised below.

Table 7.3: New Intersection 2031 Future AM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beelliar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.33# | | |
| Lane 2 | T | 0.33# | | |
| Lane 3 | R | 0.02 | 0.2 | 11 |
| Milgun Drive (NORTH) | | | | |
| Lane 1 | L | 0.03 | 0.3 | 7 |
| Lane 2 | R | 0.12 | 1 | 25# |
| Beelliar Drive (WEST) | | | | |
| Lane 1 | L | 0.03 | 0.4 | 7 |
| Lane 2 | T | 0.28 | | |
| Lane 3 | T | 0.28 | | |
| Intersection | ALL | 0.33 | | 1/D |

SCENARIO 2B –2031 (DO SOMETHING)

Table 7.4: New Intersection Future PM Peak Operating Conditions

| Approach | Performance Criteria | | | |
|-------------------------------|----------------------|-------------|-----------|----------------------------|
| | Lane | DoS | Queue (m) | Delay (s)/Intersection LoS |
| Beeliar Drive (EAST) | | | | 0 |
| Lane 1 | T | 0.34 | | |
| Lane 2 | T | 0.34 | | |
| Lane 3 | R | 0.09 | 1 | 14 |
| Dunraven Drive (NORTH) | | | | |
| Lane 1 | L | 0.04 | 0.4 | 8 |
| Lane 2 | R | 0.43# | 4 | 44# |
| Beeliar Drive (WEST) | | | | |
| Lane 1 | L | 0.05 | 1 | 7 |
| Lane 2 | T | 0.34 | | |
| Lane 3 | T | 0.34 | | |
| Intersection | ALL | 0.43 | | 2/E |

In the afternoon, the critical movement would be the right turn from Milgun Drive into Beeliar Drive where delays are expected to be approximately 45 seconds which then leads to a LoS E and a DoS of 0.43. This intersection would then be acceptable in this proposed format. If the right turn from Dunraven Drive at the intersection of Beeliar Drive and Dunraven Drive were to be banned completely, there is sufficient spare capacity at this intersection to effectively cater for any distributed right turn traffic to this new intersection.



SUMMARY OF TRAFFIC MODEL RESULTS

8. SUMMARY OF TRAFFIC MODEL RESULTS

The traffic assessment of the current and future road network layout has shown that the road network is expected to operate satisfactorily until 2031 with a single exception. The model results are summarised below.

Intersection of Spearwood Avenue and Yangebup Road

This intersection was found to have delays for the right turn movement from Yangebup Road, this being the most critical movement for this intersection. With the modest traffic flow increases expected both on Spearwood Avenue and Yangebup Road, the performance of this intersection is considered acceptable up to approximately 2031. At this point, there is expected to be delays of approximately 90 seconds for the right turn from Yangebup Road, and this typically during the period immediately after the completion of a school day (approximately 3:15pm to 3:30pm). Outside these times, the delays are expected to be somewhat less than this and considered acceptable. Based on this, there is no need to modify this intersection in the short term or by 2031.

Intersection of Beelihar Drive and Dunraven Drive

This intersection currently experiences significant delays and queues for the critical right turns to and from Dunraven Drive. In the AM peak there are currently long queues on Beelihar Drive with vehicles stopped to turn right into Dunraven Drive. This then impacts on the ability of Dunraven Drive vehicles to safely turn right onto Beelihar Drive. In the PM peak, the issue becomes the right turn from Dunraven Drive with long delays which may lead to risk taking manoeuvres by taking smaller gaps in traffic which may not be achievable.

Intersection of North Lake Road and Osprey Drive

This intersection currently operates at a very good level of service, B during both the AM and PM peak periods and this is expected to be the case in 2031 with increased internal traffic flows and increased external traffic flows. There is no requirement to modify this intersection in the short term or by 2031.

MITIGATION MEASURES

9. MITIGATION MEASURES

Apart from the intersection of Beeliar Drive and Dunraven Drive, no specific mitigation measures are proposed as the above assessment found the delays, levels of service, and queues were to be within acceptable limits. There is expected to be slightly longer delays and queues in comparison to the current operation of the road network, but these are all expected to be at satisfactory levels.

Intersection of Spearwood Avenue and Yangebup Road

There was no need to modify this intersection in the short term or by 2031. Beyond 2031, there may be the requirement to address the increasing delays experienced by vehicles turning right from Yangebup Road.

Intersection of Beeliar Drive and Dunraven Drive

The issue with the current delays experienced by vehicles turning right from Beeliar Drive into Dunraven Drive is the requirement for the right turn traffic having to give way to opposing left turn traffic on the western approach of Beeliar Drive. This left turn has a high volume of traffic and thus reduces the available gaps in traffic for the right turn traffic. To address this, it is suggested that the left turn be placed under Give Way control with a splitter island, similar in format to the left turn island on the northern approach to the intersection of Spearwood Avenue and Yangebup Road. This needs to be undertaken in the short term as the performance of the right turn into Dunraven Drive from Beeliar Drive affects both the safety of right turn traffic and through traffic approaching this intersection from the east.

In the longer term as traffic flows increase, delays for vehicle turning right from Dunraven Drive into Beeliar Drive are expected to get to a level which will be unacceptable and lead to safety concerns. This is expected to be in approximately 2-3 years' time based on application of growth rates until the delays for right turns exceed 90 seconds. By 2031 the delays will be significantly unacceptable.

To address this, it is suggested that one of a possible selection of treatments could be implemented with this being the creation of a new intersection near where Milgun Drive meets Beeliar Drive.

Intersection of North Lake Road and Osprey Drive

There is no need to modify this intersection in the short term or by 2031.

SUMMARY

10. SUMMARY

Based on the analysis and discussions presented within this report, the following conclusions are made:

- The current road network is appropriate for the current traffic flows, with the one exception being the intersection of Beeliam Drive and Dunraven Drive
- This intersection currently has significant delays and long queues on the eastern approach right turn into Dunraven Drive
- This intersection should be modified in the short term with the modification of left turn lane on the western approach to the intersection to a Give Way slip. This will flip the turning priorities and improve the performance of the right turn into Dunraven Drive
- By 2031 with the dual impacts of internal traffic growth due to 240 additional lots and external traffic flows growth on arterial roads, the road network is appropriate for the expected traffic flows, with the one exception again being the intersection of Beeliam Drive and Dunraven Drive
- In this case, the delays expected for the right turn from Dunraven Drive into Beeliam Drive are expected to increase beyond 90 seconds in approximately 2-3 years
- To address this, it is proposed a new intersection be provided near where Milgun Drive approaches Beeliam Drive
- This is expected to reduce the delays for right turns from Dunraven Drive to acceptable levels up to approximately 2031
- By 2031, delays at Dunraven Drive are expected to be excessive again and consideration to the banning of this movement should then be undertaken

APPENDIX: TRAFFIC COUNTS

A. TRAFFIC COUNTS



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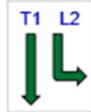
A-2



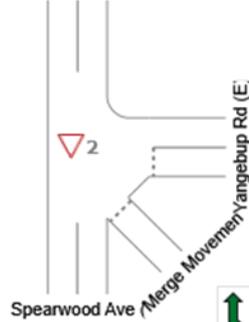
Input Volumes

Volume Display Method: Separate

| | | |
|-----|-----|-----|
| | T1 | L2 |
| Tot | 521 | 145 |
| LV | 466 | 140 |
| HV | 55 | 5 |



Spearwood Ave (N)



| | | | |
|----|-----|-----|----|
| | Tot | LV | HV |
| R2 | 249 | 247 | 2 |
| L2 | 205 | 197 | 8 |



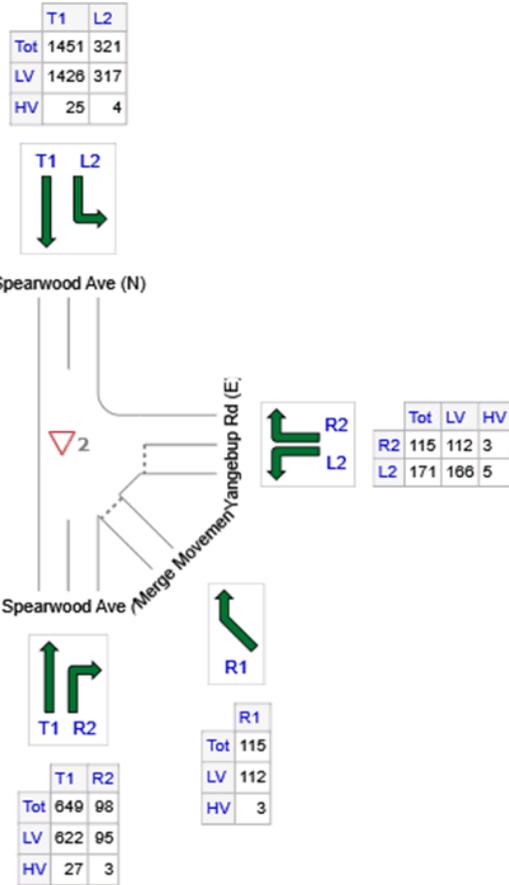
| | |
|----|-----|
| | Tot |
| R1 | 249 |
| LV | 247 |
| HV | 2 |

| | | |
|-----|------|-----|
| | T1 | R2 |
| Tot | 1067 | 150 |
| LV | 1022 | 146 |
| HV | 45 | 4 |

| | All MCs | Light Vehicles (LV) | Heavy Vehicles (HV) |
|----------------------|---------|---------------------|---------------------|
| S: Spearwood Ave (S) | 1217 | 1168 | 49 |
| SE: Merge Movement | 249 | 247 | 2 |
| E: Yangebup Rd (E) | 454 | 444 | 10 |
| N: Spearwood Ave (N) | 666 | 606 | 60 |
| Total | 2586 | 2465 | 121 |

Input Volumes

Volume Display Method: Separate



| | All MCs | Light Vehicles (LV) | Heavy Vehicles (HV) |
|----------------------|---------|---------------------|---------------------|
| S: Spearwood Ave (S) | 747 | 717 | 30 |
| SE: Merge Movement | 115 | 112 | 3 |
| E: Yangebup Rd (E) | 286 | 278 | 8 |
| N: Spearwood Ave (N) | 1772 | 1743 | 29 |
| Total | 2920 | 2850 | 70 |



USER REPORT FOR SITE

Project: Sidra Modelling

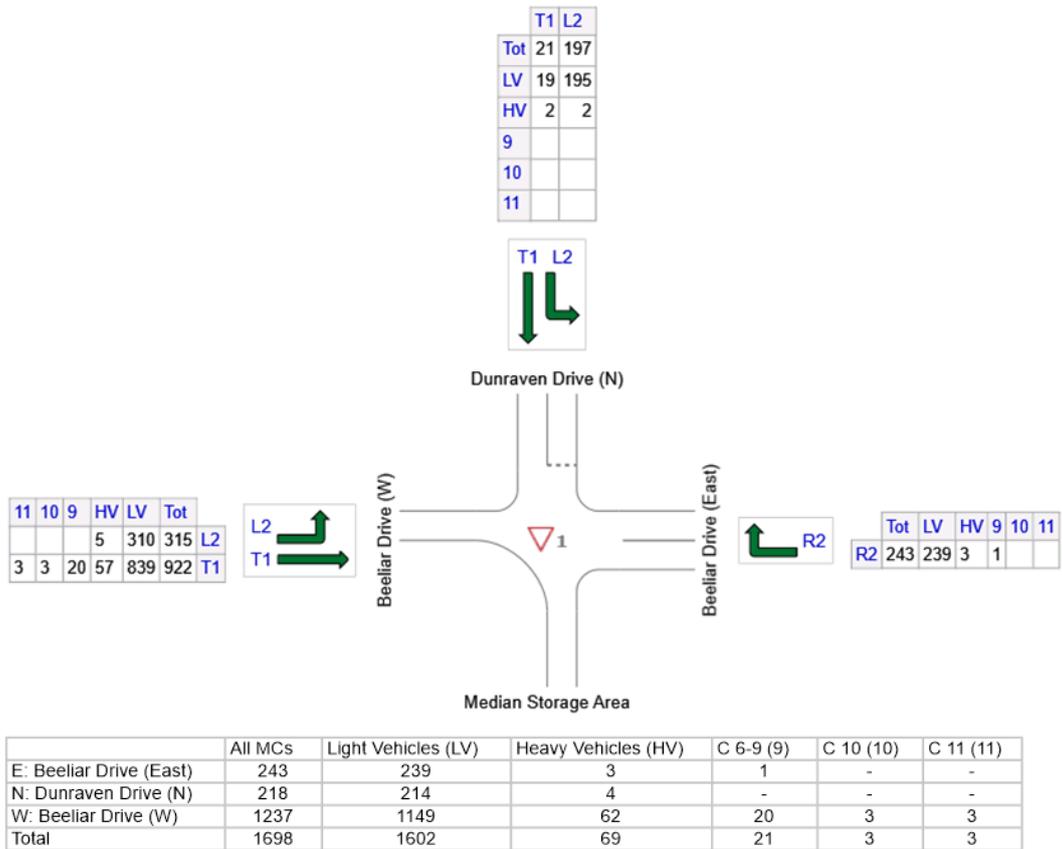
Template: Input Volume Only

Site: [Beeliar Dr & Dunraven Dr (Approach) 2020 AM]

Yangebup
 Site Category: (None)
 Giveway / Yield (Two-Way)

Input Volumes

Volume Display Method: Separate



USER REPORT FOR SITE

 Project: Sidra Modelling

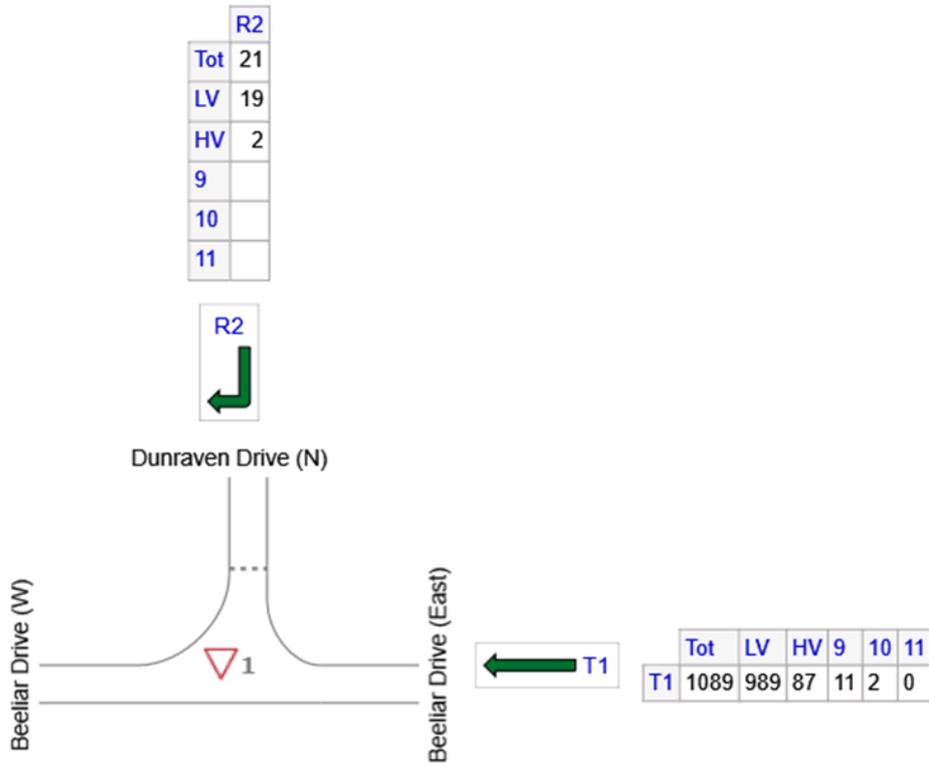
Template: Input Volume Only

 Site: [Beeliar Dr & Dunraven Dr (Median) 2020 AM]

Yangebup
 Site Category: (None)
 Giveaway / Yield (Two-Way)

Input Volumes

Volume Display Method: Separate



| | All MCs | Light Vehicles (LV) | Heavy Vehicles (HV) | C 6-9 (9) | C 10 (10) | C 11 (11) |
|-------------------------|---------|---------------------|---------------------|-----------|-----------|-----------|
| E: Beeliar Drive (East) | 1089 | 989 | 87 | 11 | 2 | 0 |
| N: Dunraven Drive (N) | 21 | 19 | 2 | - | - | - |
| Total | 1110 | 1008 | 89 | 11 | 2 | 0 |



USER REPORT FOR SITE

Project: Sidra Modelling

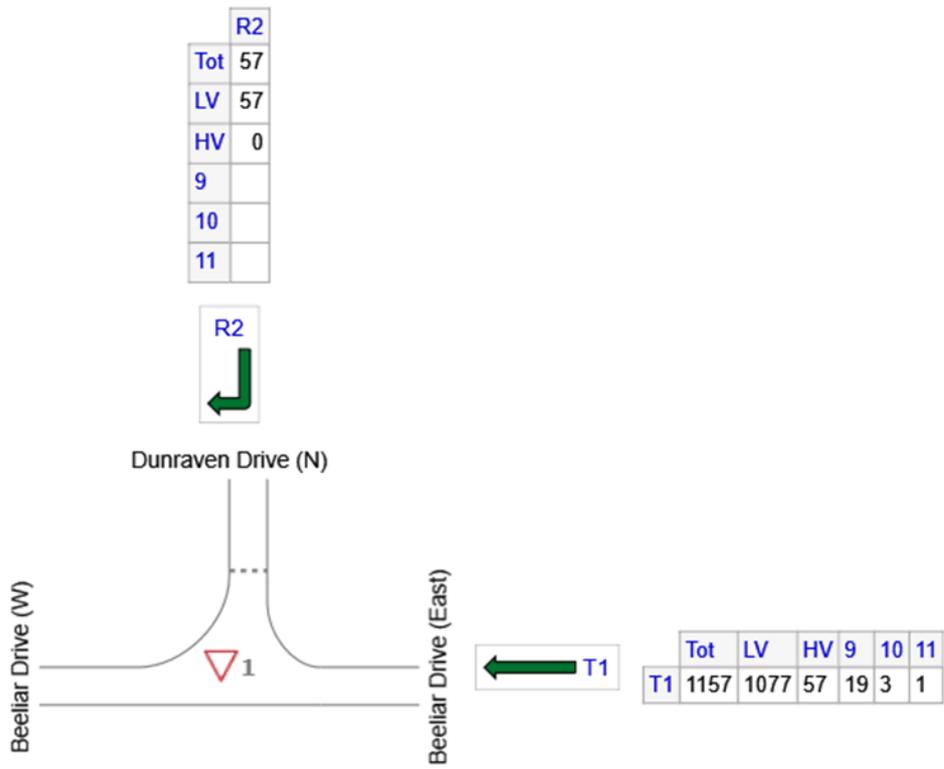
Template: Input Volume Only

Site: [Beeliar Dr & Dunraven Dr (Median) 2020 PM]

Yangebup
 Site Category: (None)
 Giveaway / Yield (Two-Way)

Input Volumes

Volume Display Method: Separate

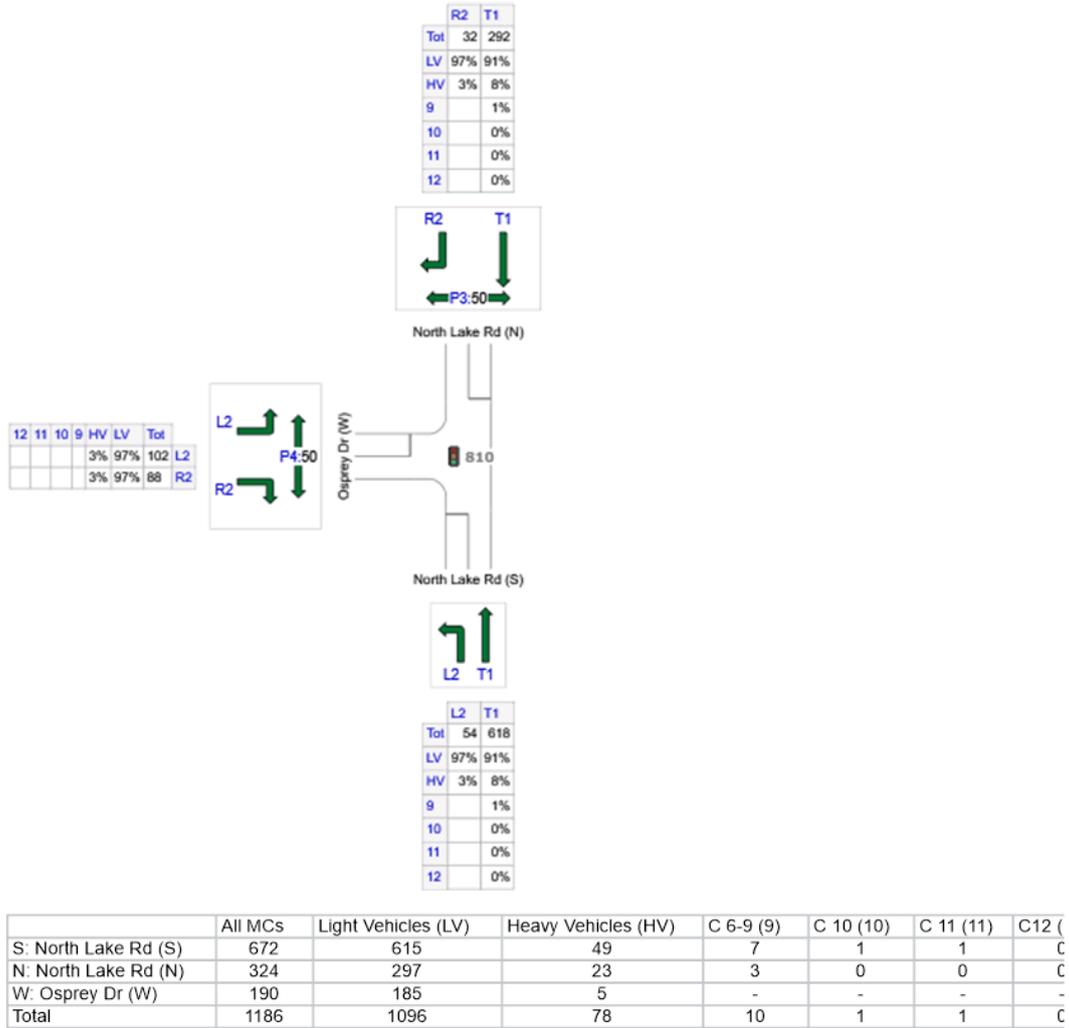


| | All MCs | Light Vehicles (LV) | Heavy Vehicles (HV) | C 6-9 (9) | C 10 (10) | C 11 (11) |
|-------------------------|---------|---------------------|---------------------|-----------|-----------|-----------|
| E: Beeliar Drive (East) | 1157 | 1077 | 57 | 19 | 3 | 1 |
| N: Dunraven Drive (N) | 57 | 57 | 0 | - | - | - |
| Total | 1214 | 1134 | 57 | 19 | 3 | 1 |



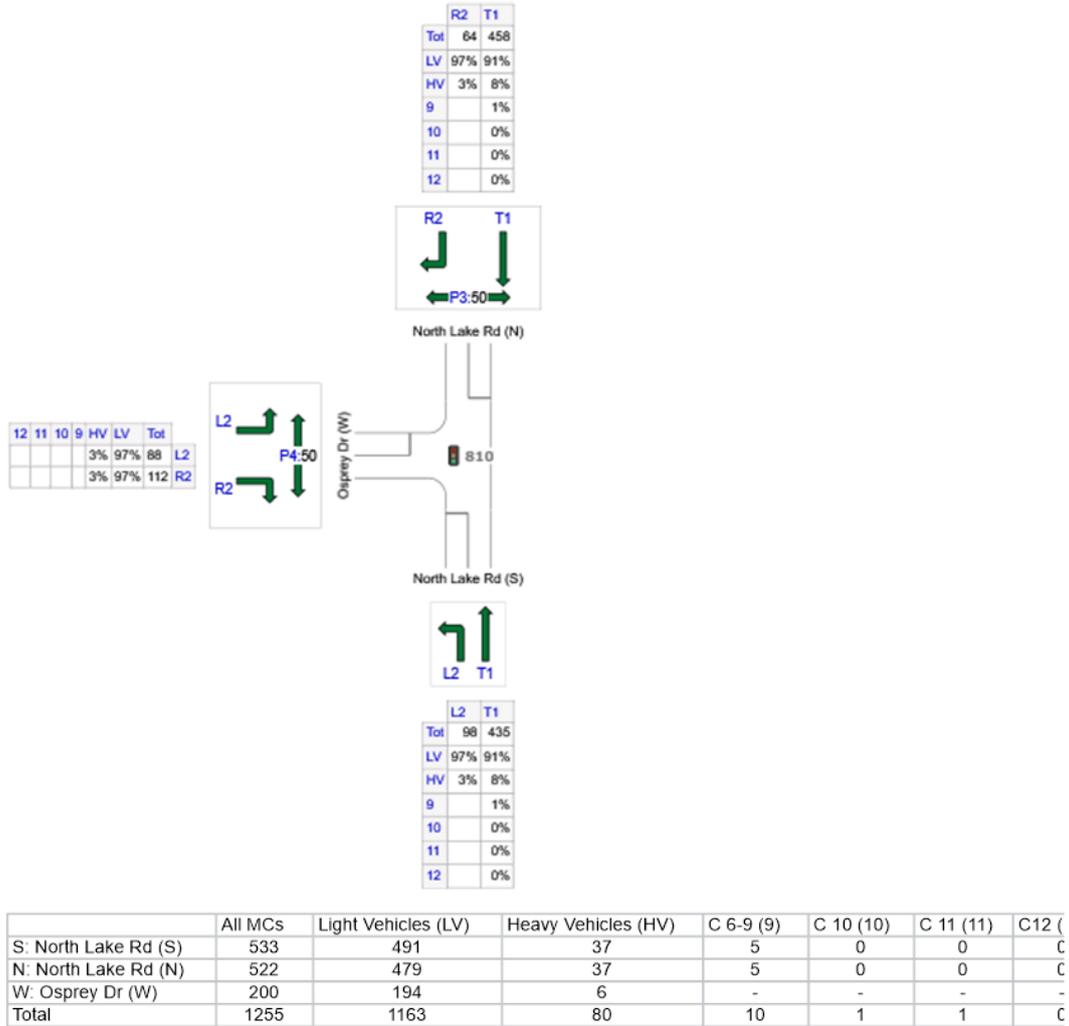
Input Volumes

Volume Display Method: Total and



Input Volumes

Volume Display Method: Total and



APPENDIX: SCENARIO 1A - 2020
SIDRA RESULTS

B. SCENARIO 1A - 2020 SIDRA RESULTS



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Eastern Yangebup, City of Cockburn

B-3



| Lane Use and Performance | | | | | | | | | | | | | |
|---------------------------|--------------|------|------|-----------|------------|---------------|------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: Spearwood Ave (S) | | | | | | | | | | | | | |
| Lane 1 | 564 | 4.2 | 1871 | 0.302 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 559 | 4.2 | 1853 | 0.302 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 158 | 2.7 | 840 | 0.188 | 100 | 9.3 | LOS A | 0.8 | 5.7 | Short | 83 | 0.0 | NA |
| Approach | 1281 | 4.0 | | 0.302 | | 1.2 | NA | 0.8 | 5.7 | | | | |
| SouthEast: Merge Movement | | | | | | | | | | | | | |
| Lane 1 | 262 | 0.8 | 927 | 0.283 | 100 | 3.4 | LOS A | 1.0 | 7.6 | Full | 15 | 0.0 | 0.0 |
| Approach | 262 | 0.8 | | 0.283 | | 3.4 | LOS A | 1.0 | 7.6 | | | | |
| East: Yangebup Rd (E) | | | | | | | | | | | | | |
| Lane 1 | 216 | 3.9 | 777 | 0.278 | 100 | 8.0 | LOS A | 1.2 | 8.9 | Short | 52 | 0.0 | NA |
| Lane 2 | 262 | 0.8 | 404 | 0.649 | 100 | 19.1 | LOS C | 4.2 | 30.8 | Full | 500 | 0.0 | 0.0 |
| Approach | 478 | 2.2 | | 0.649 | | 14.1 | LOS B | 4.2 | 30.8 | | | | |
| North: Spearwood Ave (N) | | | | | | | | | | | | | |
| Lane 1 | 153 | 3.4 | 1423 | 0.107 | 100 | 7.1 | LOS A | 0.4 | 3.4 | Short | 108 | 0.0 | NA |
| Lane 2 | 274 | 10.6 | 1764 | 0.155 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 274 | 10.6 | 1764 | 0.155 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 701 | 9.0 | | 0.155 | | 1.6 | LOS A | 0.4 | 3.4 | | | | |
| Intersection | 2722 | 4.7 | | 0.649 | | 3.8 | NA | 4.2 | 30.8 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | |
|---------------------------|--------------|------|------|-----------|------------|---------------|------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: Spearwood Ave (S) | | | | | | | | | | | | | |
| Lane 1 | 344 | 4.2 | 1872 | 0.184 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 339 | 4.2 | 1847 | 0.184 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 103 | 3.1 | 378 | 0.273 | 100 | 17.4 | LOS C | 1.1 | 8.2 | Short | 83 | 0.0 | NA |
| Approach | 786 | 4.0 | | 0.273 | | 2.3 | NA | 1.1 | 8.2 | | | | |
| SouthEast: Merge Movement | | | | | | | | | | | | | |
| Lane 1 | 121 | 2.6 | 1224 | 0.099 | 100 | 1.6 | LOS A | 0.3 | 2.4 | Full | 15 | 0.0 | 0.0 |
| Approach | 121 | 2.6 | | 0.099 | | 1.6 | LOS A | 0.3 | 2.4 | | | | |
| East: Yangebup Rd (E) | | | | | | | | | | | | | |
| Lane 1 | 180 | 2.9 | 382 | 0.471 | 100 | 17.7 | LOS C | 2.1 | 16.3 | Short | 52 | 0.0 | NA |
| Lane 2 | 121 | 2.6 | 158 | 0.765 | 100 | 54.1 | LOS F | 3.8 | 28.8 | Full | 500 | 0.0 | 0.0 |
| Approach | 301 | 2.8 | | 0.765 | | 32.3 | LOS D | 3.8 | 28.8 | | | | |
| North: Spearwood Ave (N) | | | | | | | | | | | | | |
| Lane 1 | 338 | 1.2 | 1504 | 0.225 | 100 | 7.0 | LOS A | 0.9 | 6.8 | Short | 108 | 0.0 | NA |
| Lane 2 | 764 | 1.7 | 1917 | 0.398 | 100 | 0.1 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 764 | 1.7 | 1917 | 0.398 | 100 | 0.1 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1865 | 1.6 | | 0.398 | | 1.3 | LOS A | 0.9 | 6.8 | | | | |
| Intersection | 3074 | 2.4 | | 0.765 | | 4.6 | NA | 3.8 | 28.8 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 256 | 1.6 | 256 | 1.6 | 555 | 0.461 | 100 | 14.8 | LOS B | 0.9 | 7.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 256 | 1.6 | 256 | 1.6 | | 0.461 | | 14.8 | NA | 0.9 | 7.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 207 | 1.0 | 207 | 1.0 | 708 | 0.293 | 100 | 8.4 | LOS A | 0.5 | 3.7 | Short | 48 | 0.0 | NA |
| Lane 2 | 22 | 9.5 | 22 | 9.5 | 101 | 0.218 | 100 | 39.8 | LOS E | 0.3 | 2.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 229 | 1.8 | 229 | 1.8 | | 0.293 | | 11.4 | LOS B | 0.5 | 3.7 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 332 | 1.6 | 332 | 1.6 | 1339 | 0.248 | 100 | 7.6 | LOS A | 0.5 | 3.4 | Short | 65 | 0.0 | NA |
| Lane 2 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1302 | 7.1 | 1302 | 7.1 | | 0.281 | | 2.0 | LOS A | 0.5 | 3.4 | | | | |
| Intersection | 1787 | 5.7 | 1787 | 5.7 | | 0.461 | | 5.0 | NA | 0.9 | 7.0 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|---------------|--------------|-------------------|------------------|-------------------|--------|-------------|---------------|-------------|----------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn v/c | Lane Util. % | Average Delay sec | Level of Service | 95% Back of Queue | | Lane Config | Lane Length m | Cap. Adj. % | Prob. Block. % |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 256 | 1.6 | 256 | 1.6 | 243 | 1.053 | 100 | 112.9 | LOS F | 17.9 | 134.3 | Full | 500 | 0.0 | 0.0 |
| Approach | 256 | 1.6 | 256 | 1.6 | | 1.053 | | 112.9 | NA | 17.9 | 134.3 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 207 | 1.0 | 207 | 1.0 | 708 | 0.293 | 100 | 8.4 | LOS A | 1.3 | 9.3 | Short | 48 | 0.0 | NA |
| Lane 2 | 22 | 9.5 | 22 | 9.5 | 69 | 0.323 | 100 | 62.8 | LOS F | 1.0 | 7.7 | Full | 500 | 0.0 | 0.0 |
| Approach | 229 | 1.8 | 229 | 1.8 | | 0.323 | | 13.6 | LOS B | 1.3 | 9.3 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 332 | 1.6 | 332 | 1.6 | 1821 | 0.182 | 100 | 6.4 | LOS A | 0.0 | 0.0 | Short | 65 | 0.0 | NA |
| Lane 2 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1302 | 7.1 | 1302 | 7.1 | | 0.281 | | 1.7 | NA | 0.0 | 0.0 | | | | |
| Intersection | 1787 | 5.7 | 1787 | 5.7 | | 1.053 | | 19.1 | NA | 17.9 | 134.3 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|-------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeliar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1146 | 9.2 | 1146 | 9.2 | | 0.326 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 22 | 9.5 | 22 | 9.5 | 1445 | 0.015 | 100 | 2.5 | LOSA | 0.0 | 0.1 | Full | 9 | 0.0 | 0.0 |
| Approach | 22 | 9.5 | 22 | 9.5 | | 0.015 | | 2.5 | LOSA | 0.0 | 0.1 | | | | |
| Intersection | 1168 | 9.2 | 1168 | 9.2 | | 0.326 | | 0.1 | NA | 0.0 | 0.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|-----------|------------|---------------|------------------|-------------------|-----|-------------|-------------|-----------|--------------|--------|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total veh/h | HV % | Total veh/h | | | | | | HV % | Veh | | | | | Dist m |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1146 | 9.2 | 1146 | 9.2 | | 0.326 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 22 | 9.5 | 22 | 9.5 | 1445 | 0.015 | 100 | 2.5 | LOSA | 0.0 | 0.3 | Full | 9 | 0.0 | 0.0 |
| Approach | 22 | 9.5 | 22 | 9.5 | | 0.015 | | 2.5 | LOSA | 0.0 | 0.3 | | | | |
| Intersection | 1168 | 9.2 | 1168 | 9.2 | | 0.326 | | 0.1 | NA | 0.0 | 0.3 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).

Lane LOS values are based on average delay per lane.

Minor Road Approach LOS values are based on average delay for all lanes.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|---------|-------|---------|-------|-----------|------------|---------------|------------------|---------------------|------|-------------|-------------|-----------|--------------|
| | Demand Arrival Flows | | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total | HV | Total | HV | | | | | | Veh | Dist | | | | |
| | veh/h | % veh/h | veh/h | % veh/h | v/c | % | sec | | | | | m | m | % | % |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 146 | 0.7 | 146 | 0.7 | 426 | 0.344 | 100 | 16.3 | LOS C | 0.6 | 4.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 146 | 0.7 | 146 | 0.7 | 0.344 | | 16.3 | NA | 0.6 | 4.1 | | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 223 | 0.9 | 223 | 0.9 | 602 | 0.371 | 100 | 10.4 | LOS B | 0.7 | 5.1 | Short | 48 | 0.0 | NA |
| Lane 2 | 60 | 0.0 | 60 | 0.0 | 93 | 0.643 | 100 | 65.1 | LOS F | 0.9 | 6.8 | Full | 500 | 0.0 | 0.0 |
| Approach | 283 | 0.7 | 283 | 0.7 | 0.643 | | 22.0 | LOS C | 0.9 | 6.8 | | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 184 | 1.7 | 184 | 1.7 | 1459 | 0.126 | 100 | 7.1 | LOS A | 0.2 | 1.6 | Short | 65 | 0.0 | NA |
| Lane 2 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1433 | 4.7 | 1433 | 4.7 | 0.342 | | 0.9 | LOS A | 0.2 | 1.6 | | | | | |
| Intersection | 1862 | 3.8 | 1862 | 3.8 | 0.643 | | 5.4 | NA | 0.9 | 6.8 | | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|---------|-------|---------|-----------|------------|---------------|------------------|-------------------|------|-------------|-------------|-----------|--------------|------|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total | HV | Total | | | | | | HV | Veh | | | | | Dist |
| | veh/h | % veh/h | veh/h | % veh/h | v/c | % | sec | | | | | m | m | % | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 146 | 0.7 | 146 | 0.7 | 206 | 0.710 | 100 | 37.4 | LOS E | 3.2 | 23.6 | Full | 500 | 0.0 | 0.0 |
| Approach | 146 | 0.7 | 146 | 0.7 | 0.710 | | 37.4 | NA | 3.2 | 23.6 | | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 223 | 0.9 | 223 | 0.9 | 602 | 0.371 | 100 | 10.4 | LOS B | 1.7 | 12.7 | Short | 48 | 0.0 | NA |
| Lane 2 | 60 | 0.0 | 60 | 0.0 | 67 | 0.902 | 100 | 144.9 | LOS F | 4.2 | 30.8 | Full | 500 | 0.0 | 0.0 |
| Approach | 283 | 0.7 | 283 | 0.7 | 0.902 | | 38.9 | LOS E | 4.2 | 30.8 | | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 184 | 1.7 | 184 | 1.7 | 1819 | 0.101 | 100 | 6.4 | LOS A | 0.0 | 0.0 | Short | 65 | 0.0 | NA |
| Lane 2 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1433 | 4.7 | 1433 | 4.7 | 0.342 | | 0.9 | NA | 0.0 | 0.0 | | | | | |
| Intersection | 1862 | 3.8 | 1862 | 3.8 | 0.902 | | 9.5 | NA | 4.2 | 30.8 | | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|---------------|--------------|-------------------|------------------|---------------------|--------|-------------|---------------|-------------|----------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn v/c | Lane Util. % | Average Delay sec | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length m | Cap. Adj. % | Prob. Block. % |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeliar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1218 | 6.9 | 1218 | 6.9 | | 0.342 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 60 | 0.0 | 60 | 0.0 | 394 | 0.152 | 100 | 9.0 | LOSA | 0.2 | 1.4 | Full | 9 | 0.0 | 0.0 |
| Approach | 60 | 0.0 | 60 | 0.0 | | 0.152 | | 9.0 | LOSA | 0.2 | 1.4 | | | | |
| Intersection | 1278 | 6.6 | 1278 | 6.6 | | 0.342 | | 0.5 | NA | 0.2 | 1.4 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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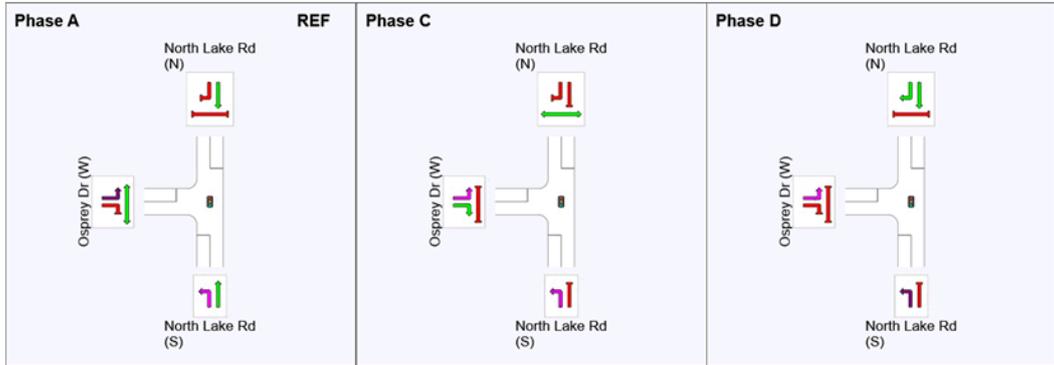
| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|-----------|------------|---------------|------------------|-------------------|-----|-------------|-------------|-----------|--------------|--------|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total veh/h | HV % | Total veh/h | | | | | | HV % | Veh | | | | | Dist m |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1218 | 6.9 | 1218 | 6.9 | | | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 60 | 0.0 | 60 | 0.0 | 394 | 0.152 | 100 | 9.0 | LOSA | 0.5 | 3.4 | Full | 9 | 0.0 | 0.0 |
| Approach | 60 | 0.0 | 60 | 0.0 | | | | 9.0 | LOSA | 0.5 | 3.4 | | | | |
| Intersection | 1278 | 6.6 | 1278 | 6.6 | | | | 0.5 | NA | 0.5 | 3.4 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

| Lane Use and Performance | | | | | | | | | | | | | |
|--------------------------|--------------|------|------|-----------|------------|---------------|------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: North Lake Rd (S) | | | | | | | | | | | | | |
| Lane 1 | 57 | 2.8 | 1609 | 0.035 | 100 | 7.1 | LOS A | 0.3 | 2.0 | Short | 108 | 0.0 | NA |
| Lane 2 | 325 | 9.0 | 1118 | 0.291 | 100 | 13.9 | LOS B | 11.2 | 91.1 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 325 | 9.0 | 1118 | 0.291 | 100 | 13.9 | LOS B | 11.2 | 91.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 707 | 8.5 | | 0.291 | | 13.4 | LOS B | 11.2 | 91.1 | | | | |
| North: North Lake Rd (N) | | | | | | | | | | | | | |
| Lane 1 | 154 | 9.0 | 1392 | 0.110 | 100 | 4.1 | LOS A | 2.7 | 22.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 154 | 9.0 | 1392 | 0.110 | 100 | 4.1 | LOS A | 2.7 | 22.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 34 | 2.8 | 202 | 0.167 | 100 | 77.0 | LOS E | 2.4 | 18.3 | Short | 101 | 0.0 | NA |
| Approach | 341 | 8.4 | | 0.167 | | 11.3 | LOS B | 2.7 | 22.0 | | | | |
| West: Osprey Dr (W) | | | | | | | | | | | | | |
| Lane 1 | 107 | 2.8 | 972 | 0.111 | 100 | 7.3 | LOS A | 1.2 | 9.0 | Short | 93 | 0.0 | NA |
| Lane 2 | 46 | 2.8 | 224 | 0.207 | 100 | 74.5 | LOS E | 3.3 | 25.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 46 | 2.8 | 224 | 0.207 | 100 | 74.5 | LOS E | 3.3 | 25.0 | Short | 88 | 0.0 | NA |
| Approach | 200 | 2.8 | | 0.207 | | 38.4 | LOS D | 3.3 | 25.0 | | | | |
| Intersection | 1248 | 7.6 | | 0.291 | | 16.8 | LOS B | 11.2 | 91.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Intersection and Approach LOS values are based on average delay for all lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Output Phase Sequence

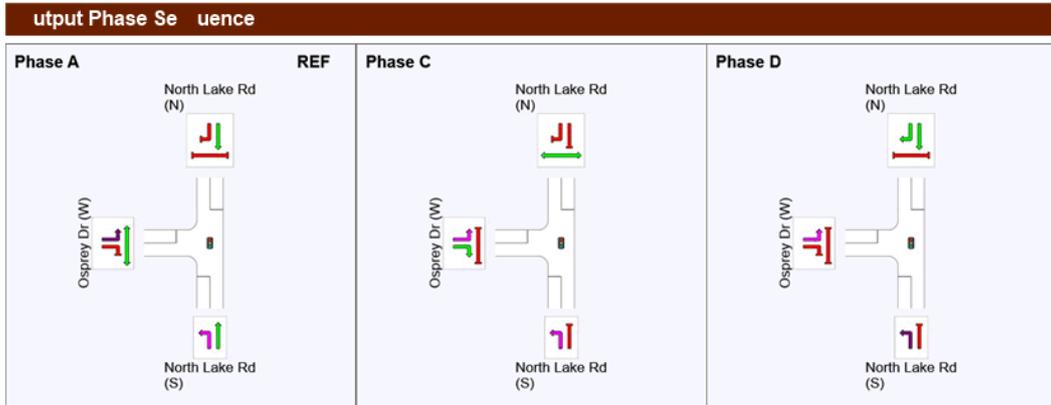


REF: Reference Phase
 VAR: Variable Phase



| Lane Use and Performance | | | | | | | | | | | | | |
|--------------------------|--------------|------|------|-----------|------------|---------------|------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: North Lake Rd (S) | | | | | | | | | | | | | |
| Lane 1 | 103 | 3.2 | 1563 | 0.066 | 100 | 7.3 | LOS A | 0.7 | 5.0 | Short | 108 | 0.0 | NA |
| Lane 2 | 229 | 9.0 | 993 | 0.231 | 100 | 18.5 | LOS B | 8.8 | 71.4 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 229 | 9.0 | 993 | 0.231 | 100 | 18.5 | LOS B | 8.8 | 71.4 | Full | 500 | 0.0 | 0.0 |
| Approach | 561 | 7.9 | | 0.231 | | 16.4 | LOS B | 8.8 | 71.4 | | | | |
| North: North Lake Rd (N) | | | | | | | | | | | | | |
| Lane 1 | 241 | 9.0 | 1324 | 0.182 | 100 | 6.1 | LOS A | 5.3 | 43.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 241 | 9.0 | 1324 | 0.182 | 100 | 6.1 | LOS A | 5.3 | 43.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 67 | 3.2 | 258 | 0.261 | 100 | 72.6 | LOS E | 4.7 | 35.8 | Short | 101 | 0.0 | NA |
| Approach | 549 | 8.3 | | 0.261 | | 14.2 | LOS B | 5.3 | 43.0 | | | | |
| West: Osprey Dr (W) | | | | | | | | | | | | | |
| Lane 1 | 93 | 3.2 | 1143 | 0.081 | 100 | 6.9 | LOS A | 0.8 | 6.4 | Short | 93 | 0.0 | NA |
| Lane 2 | 59 | 3.2 | 292 | 0.202 | 100 | 68.2 | LOS E | 4.0 | 30.3 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 59 | 3.2 | 292 | 0.202 | 100 | 68.2 | LOS E | 4.0 | 30.3 | Short | 88 | 0.0 | NA |
| Approach | 211 | 3.2 | | 0.202 | | 41.2 | LOS D | 4.0 | 30.3 | | | | |
| Intersection | 1321 | 7.3 | | 0.261 | | 19.5 | LOS B | 8.8 | 71.4 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Intersection and Approach LOS values are based on average delay for all lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



REF: Reference Phase
 VAR: Variable Phase



APPENDIX: SCENARIO 1B – 2031
SIDRA RESULTS

C. SCENARIO 1B – 2031 SIDRA RESULTS



W195560 // **24/08/2020**
Access Traffic Assessment // Issue: A-Final
Eastern Yangebup, City of Cockburn

C-4



| Lane Use and Performance | | | | | | | | | | | | | |
|---------------------------|--------------|------|------|-----------|------------|---------------|------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: Spearwood Ave (S) | | | | | | | | | | | | | |
| Lane 1 | 580 | 4.2 | 1871 | 0.310 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 574 | 4.2 | 1852 | 0.310 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 168 | 2.7 | 826 | 0.203 | 100 | 9.4 | LOS A | 0.8 | 6.2 | Short | 83 | 0.0 | NA |
| Approach | 1322 | 4.0 | | 0.310 | | 1.2 | NA | 0.8 | 6.2 | | | | |
| SouthEast: Merge Movement | | | | | | | | | | | | | |
| Lane 1 | 279 | 0.8 | 906 | 0.308 | 100 | 3.6 | LOS A | 1.1 | 8.5 | Full | 15 | 0.0 | 0.0 |
| Approach | 279 | 0.8 | | 0.308 | | 3.6 | LOS A | 1.1 | 8.5 | | | | |
| East: Yangebup Rd (E) | | | | | | | | | | | | | |
| Lane 1 | 230 | 3.9 | 764 | 0.300 | 100 | 8.3 | LOS A | 1.3 | 10.1 | Short | 52 | 0.0 | NA |
| Lane 2 | 279 | 0.8 | 390 | 0.715 | 100 | 21.7 | LOS C | 5.0 | 37.3 | Full | 500 | 0.0 | 0.0 |
| Approach | 508 | 2.2 | | 0.715 | | 15.7 | LOS C | 5.0 | 37.3 | | | | |
| North: Spearwood Ave (N) | | | | | | | | | | | | | |
| Lane 1 | 162 | 3.4 | 1411 | 0.115 | 100 | 7.2 | LOS A | 0.5 | 3.7 | Short | 108 | 0.0 | NA |
| Lane 2 | 282 | 10.6 | 1764 | 0.160 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 282 | 10.6 | 1764 | 0.160 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 726 | 9.0 | | 0.160 | | 1.6 | LOS A | 0.5 | 3.7 | | | | |
| Intersection | 2835 | 4.6 | | 0.715 | | 4.2 | NA | 5.0 | 37.3 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | |
|---------------------------|--------------|------|------|-----------|------------|---------------|---------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: Spearwood Ave (S) | | | | | | | | | | | | | |
| Lane 1 | 366 | 4.2 | 1872 | 0.195 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 361 | 4.2 | 1845 | 0.195 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 110 | 3.1 | 339 | 0.324 | 100 | 19.6 | LOS C | 1.3 | 9.8 | Short | 83 | 0.0 | NA |
| Approach | 836 | 4.0 | | 0.324 | | 2.6 | NA | 1.3 | 9.8 | | | | |
| SouthEast: Merge Movement | | | | | | | | | | | | | |
| Lane 1 | 129 | 2.6 | 1191 | 0.108 | 100 | 1.8 | LOS A | 0.3 | 2.6 | Full | 15 | 0.0 | 0.0 |
| Approach | 129 | 2.6 | | 0.108 | | 1.8 | LOS A | 0.3 | 2.6 | | | | |
| East: Yangebup Rd (E) | | | | | | | | | | | | | |
| Lane 1 | 191 | 2.9 | 348 | 0.550 | 100 | 20.6 | LOS C | 2.6 | 19.8 | Short | 52 | 0.0 | NA |
| Lane 2 | 129 | 2.6 | 138 | 0.936 | 100 | 98.5 | LOS F ¹¹ | 6.7 | 50.7 | Full | 500 | 0.0 | 0.0 |
| Approach | 320 | 2.8 | | 0.936 | | 52.0 | LOS F ¹¹ | 6.7 | 50.7 | | | | |
| North: Spearwood Ave (N) | | | | | | | | | | | | | |
| Lane 1 | 359 | 1.2 | 1497 | 0.240 | 100 | 7.0 | LOS A | 1.0 | 7.4 | Short | 108 | 0.0 | NA |
| Lane 2 | 812 | 1.7 | 1917 | 0.424 | 100 | 0.1 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 812 | 1.7 | 1917 | 0.424 | 100 | 0.1 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1984 | 1.6 | | 0.424 | | 1.3 | LOS A | 1.0 | 7.4 | | | | |
| Intersection | 3270 | 2.4 | | 0.936 | | 6.6 | NA | 6.7 | 50.7 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

¹¹ Level of Service is worse than the Level of Service Target specified in the Parameter Settings dialog.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|---------------|--------------|-------------------|------------------|---------------------|--------|-------------|---------------|-------------|----------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn v/c | Lane Util. % | Average Delay sec | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length m | Cap. Adj. % | Prob. Block. % |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 256 | 1.6 | 256 | 1.6 | 555 | 0.461 | 100 | 14.8 | LOS B | 0.9 | 7.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 256 | 1.6 | 256 | 1.6 | | 0.461 | | 14.8 | NA | 0.9 | 7.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 207 | 1.0 | 207 | 1.0 | 708 | 0.293 | 100 | 8.4 | LOS A | 0.5 | 3.7 | Short | 48 | 0.0 | NA |
| Lane 2 | 22 | 9.5 | 22 | 9.5 | 101 | 0.218 | 100 | 39.8 | LOS E | 0.3 | 2.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 229 | 1.8 | 229 | 1.8 | | 0.293 | | 11.4 | LOS B | 0.5 | 3.7 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 332 | 1.6 | 332 | 1.6 | 1339 | 0.248 | 100 | 7.6 | LOS A | 0.5 | 3.4 | Short | 65 | 0.0 | NA |
| Lane 2 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1302 | 7.1 | 1302 | 7.1 | | 0.281 | | 2.0 | LOS A | 0.5 | 3.4 | | | | |
| Intersection | 1787 | 5.7 | 1787 | 5.7 | | 0.461 | | 5.0 | NA | 0.9 | 7.0 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|-------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeliar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1146 | 9.2 | 1146 | 9.2 | | 0.326 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 22 | 9.5 | 22 | 9.5 | 1445 | 0.015 | 100 | 2.5 | LOSA | 0.0 | 0.1 | Full | 9 | 0.0 | 0.0 |
| Approach | 22 | 9.5 | 22 | 9.5 | | 0.015 | | 2.5 | LOSA | 0.0 | 0.1 | | | | |
| Intersection | 1168 | 9.2 | 1168 | 9.2 | | 0.326 | | 0.1 | NA | 0.0 | 0.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|-----------|------------|---------------|------------------|---------------------|-----|-------------|-------------|-----------|--------------|--------|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total veh/h | HV % | Total veh/h | | | | | | HV % | Veh | | | | | Dist m |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 146 | 0.7 | 146 | 0.7 | 426 | 0.344 | 100 | 16.3 | LOS C | 0.6 | 4.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 146 | 0.7 | 146 | 0.7 | 0.344 | | 16.3 | NA | 0.6 | 4.1 | | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 223 | 0.9 | 223 | 0.9 | 602 | 0.371 | 100 | 10.4 | LOS B | 0.7 | 5.1 | Short | 48 | 0.0 | NA |
| Lane 2 | 60 | 0.0 | 60 | 0.0 | 93 | 0.643 | 100 | 65.1 | LOS F | 0.9 | 6.8 | Full | 500 | 0.0 | 0.0 |
| Approach | 283 | 0.7 | 283 | 0.7 | 0.643 | | 22.0 | LOS C | 0.9 | 6.8 | | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 184 | 1.7 | 184 | 1.7 | 1459 | 0.126 | 100 | 7.1 | LOS A | 0.2 | 1.6 | Short | 65 | 0.0 | NA |
| Lane 2 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1433 | 4.7 | 1433 | 4.7 | 0.342 | | 0.9 | LOS A | 0.2 | 1.6 | | | | | |
| Intersection | 1862 | 3.8 | 1862 | 3.8 | 0.643 | | 5.4 | NA | 0.9 | 6.8 | | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|-------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeliar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1218 | 6.9 | 1218 | 6.9 | | 0.342 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 60 | 0.0 | 60 | 0.0 | 394 | 0.152 | 100 | 9.0 | LOSA | 0.2 | 1.4 | Full | 9 | 0.0 | 0.0 |
| Approach | 60 | 0.0 | 60 | 0.0 | | 0.152 | | 9.0 | LOSA | 0.2 | 1.4 | | | | |
| Intersection | 1278 | 6.6 | 1278 | 6.6 | | 0.342 | | 0.5 | NA | 0.2 | 1.4 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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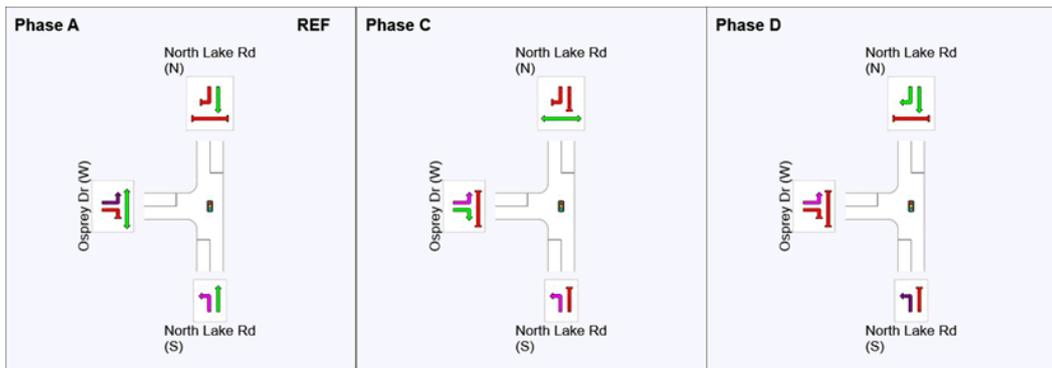


| Lane Use and Performance | | | | | | | | | | | | | |
|--------------------------|--------------|------|------|-----------|------------|---------------|---------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: North Lake Rd (S) | | | | | | | | | | | | | |
| Lane 1 | 60 | 2.8 | 1625 | 0.037 | 100 | 7.1 | LOS A | 0.3 | 2.4 | Short | 108 | 0.0 | NA |
| Lane 2 | 439 | 9.0 | 1213 | 0.362 | 100 | 10.9 | LOS B | 13.9 | 113.2 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 439 | 9.0 | 1213 | 0.362 | 100 | 10.9 | LOS B | 13.9 | 113.2 | Full | 500 | 0.0 | 0.0 |
| Approach | 939 | 8.6 | | 0.362 | | 10.7 | LOS B | 13.9 | 113.2 | | | | |
| North: North Lake Rd (N) | | | | | | | | | | | | | |
| Lane 1 | 208 | 9.0 | 1390 | 0.149 | 100 | 4.3 | LOS A | 3.8 | 30.8 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 208 | 9.0 | 1390 | 0.149 | 100 | 4.3 | LOS A | 3.8 | 30.8 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 36 | 2.8 | 101 | 0.353 | 100 | 88.5 | LOS F ¹¹ | 2.8 | 21.3 | Short | 101 | 0.0 | NA |
| Approach | 451 | 8.5 | | 0.353 | | 11.0 | LOS B | 3.8 | 30.8 | | | | |
| West: Osprey Dr (W) | | | | | | | | | | | | | |
| Lane 1 | 114 | 2.8 | 790 | 0.145 | 100 | 8.0 | LOS A | 1.6 | 11.8 | Short | 93 | 0.0 | NA |
| Lane 2 | 49 | 2.8 | 225 | 0.219 | 100 | 74.1 | LOS E ¹¹ | 3.5 | 26.5 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 49 | 2.8 | 225 | 0.219 | 100 | 74.1 | LOS E ¹¹ | 3.5 | 26.5 | Short | 88 | 0.0 | NA |
| Approach | 213 | 2.8 | | 0.219 | | 38.6 | LOS D | 3.5 | 26.5 | | | | |
| Intersection | 1603 | 7.8 | | 0.362 | | 14.5 | LOS B | 13.9 | 113.2 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab). Lane LOS values are based on average delay per lane. Intersection and Approach LOS values are based on average delay for all lanes. SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay. Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D). HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

¹¹ Level of Service is worse than the Level of Service Target specified in the Parameter Settings dialog.

Output Phase Sequence



REF: Reference Phase
VAR: Variable Phase

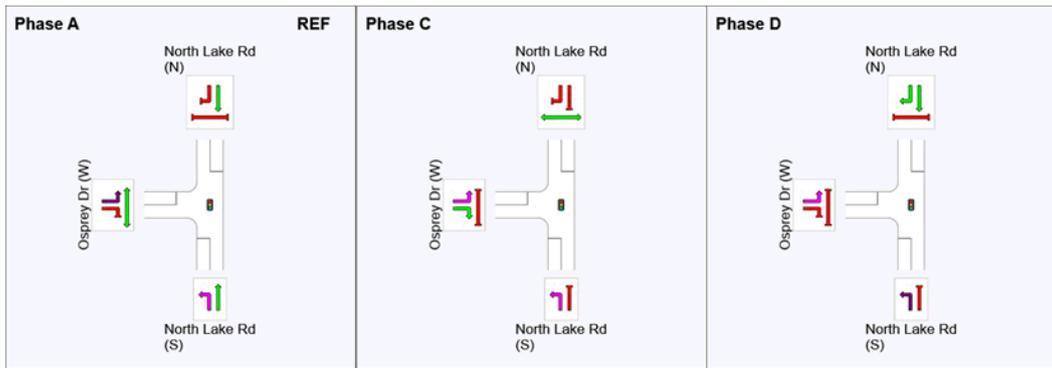


| Lane Use and Performance | | | | | | | | | | | | | |
|--------------------------|--------------|------|------|-----------|------------|---------------|---------------------|-------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | 95% Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| South: North Lake Rd (S) | | | | | | | | | | | | | |
| Lane 1 | 110 | 3.2 | 1562 | 0.070 | 100 | 7.3 | LOS A | 0.7 | 5.4 | Short | 108 | 0.0 | NA |
| Lane 2 | 309 | 8.9 | 1072 | 0.288 | 100 | 15.8 | LOS B | 11.2 | 91.1 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 309 | 8.9 | 1072 | 0.288 | 100 | 15.8 | LOS B | 11.2 | 91.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 728 | 8.0 | | 0.288 | | 14.5 | LOS B | 11.2 | 91.1 | | | | |
| North: North Lake Rd (N) | | | | | | | | | | | | | |
| Lane 1 | 325 | 8.9 | 1393 | 0.233 | 100 | 4.6 | LOS A | 6.4 | 52.1 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 325 | 8.9 | 1393 | 0.233 | 100 | 4.6 | LOS A | 6.4 | 52.1 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 72 | 3.2 | 247 | 0.290 | 100 | 73.9 | LOS E ¹¹ | 5.1 | 38.6 | Short | 101 | 0.0 | NA |
| Approach | 722 | 8.3 | | 0.290 | | 11.5 | LOS B | 6.4 | 52.1 | | | | |
| West: Osprey Dr (W) | | | | | | | | | | | | | |
| Lane 1 | 99 | 3.2 | 1010 | 0.098 | 100 | 7.3 | LOS A | 1.1 | 8.2 | Short | 93 | 0.0 | NA |
| Lane 2 | 63 | 3.2 | 224 | 0.279 | 100 | 74.8 | LOS E ¹¹ | 4.5 | 34.2 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 63 | 3.2 | 224 | 0.279 | 100 | 74.8 | LOS E ¹¹ | 4.5 | 34.2 | Short | 88 | 0.0 | NA |
| Approach | 224 | 3.2 | | 0.279 | | 45.1 | LOS D | 4.5 | 34.2 | | | | |
| Intersection | 1674 | 7.5 | | 0.290 | | 17.3 | LOS B | 11.2 | 91.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab). Lane LOS values are based on average delay per lane. Intersection and Approach LOS values are based on average delay for all lanes. SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay. Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D). HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

¹¹ Level of Service is worse than the Level of Service Target specified in the Parameter Settings dialog.

Output Phase Sequence



REF: Reference Phase
VAR: Variable Phase



APPENDIX: SCENARIO 2B - DO
SOMETHING SIDRA RESULTS

D. SCENARIO 2B - DO SOMETHING SIDRA RESULTS



W195560 // **24/08/2020**
Access Traffic Assessment // Issue: A-Final
Eastern Yangebup, City of Cockburn

D-5



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Arrival Flows | | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beelias Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 231 | 1.7 | 231 | 1.7 | 555 | 0.416 | 100 | 14.4 | LOS B | 0.8 | 6.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 231 | 1.7 | 231 | 1.7 | | 0.416 | | 14.4 | NA | 0.8 | 6.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 169 | 1.1 | 169 | 1.1 | 707 | 0.238 | 100 | 8.0 | LOS A | 0.4 | 2.7 | Short | 48 | 0.0 | NA |
| Lane 2 | 1 | 0.0 | 1 | 0.0 | 127 | 0.008 | 100 | 28.3 | LOS D | 0.0 | 0.1 | Full | 500 | 0.0 | 0.0 |
| Approach | 170 | 1.1 | 170 | 1.1 | | 0.238 | | 8.1 | LOS A | 0.4 | 2.7 | | | | |
| West: Beelias Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 240 | 1.5 | 240 | 1.5 | 1366 | 0.176 | 100 | 7.4 | LOS A | 0.3 | 2.3 | Short | 65 | 0.0 | NA |
| Lane 2 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1210 | 7.5 | 1210 | 7.5 | | 0.281 | | 1.5 | LOS A | 0.3 | 2.3 | | | | |
| Intersection | 1611 | 6.0 | 1611 | 6.0 | | 0.416 | | 4.0 | NA | 0.8 | 6.0 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|-----|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total veh/h | HV % | Total veh/h | | | | | | Veh | Dist m | | | | | |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1146 | 9.2 | 1146 | 9.2 | | 0.326 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 4 | 0.0 | 4 | 0.0 | 1498 | 0.003 | 100 | 2.4 | LOSA | 0.0 | 0.0 | Full | 9 | 0.0 | 0.0 |
| Approach | 4 | 0.0 | 4 | 0.0 | | 0.003 | | 2.4 | LOSA | 0.0 | 0.0 | | | | |
| Intersection | 1151 | 9.1 | 1151 | 9.1 | | 0.326 | | 0.1 | NA | 0.0 | 0.0 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 83 | 1.0 | 83 | 1.0 | 425 | 0.195 | 100 | 14.7 | LOS B | 0.3 | 2.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 83 | 1.0 | 83 | 1.0 | | 0.195 | | 14.7 | NA | 0.3 | 2.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 181 | 1.0 | 181 | 1.0 | 602 | 0.301 | 100 | 9.8 | LOS A | 0.5 | 3.8 | Short | 48 | 0.0 | NA |
| Lane 2 | 3 | 0.0 | 3 | 0.0 | 106 | 0.027 | 100 | 33.8 | LOS D | 0.0 | 0.2 | Full | 500 | 0.0 | 0.0 |
| Approach | 184 | 1.0 | 184 | 1.0 | | 0.301 | | 10.2 | LOS B | 0.5 | 3.8 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 66 | 1.9 | 66 | 1.9 | 1528 | 0.043 | 100 | 6.8 | LOS A | 0.1 | 0.5 | Short | 65 | 0.0 | NA |
| Lane 2 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1315 | 5.0 | 1315 | 5.0 | | 0.342 | | 0.4 | LOS A | 0.1 | 0.5 | | | | |
| Intersection | 1581 | 4.3 | 1581 | 4.3 | | 0.342 | | 2.3 | NA | 0.5 | 3.8 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|------|-------------|------|-----------|------------|---------------|------------------|---------------------|-----|-------------|-------------|-----------|--------------|--------|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total veh/h | HV % | Total veh/h | | | | | | HV % | Veh | | | | | Dist m |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1218 | 6.9 | 1218 | 6.9 | | | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 12 | 0.0 | 12 | 0.0 | 394 | 0.029 | 100 | 8.5 | LOSA | 0.0 | 0.3 | Full | 9 | 0.0 | 0.0 |
| Approach | 12 | 0.0 | 12 | 0.0 | | | | 8.5 | LOSA | 0.0 | 0.3 | | | | |
| Intersection | 1229 | 6.8 | 1229 | 6.8 | | | | 0.1 | NA | 0.0 | 0.3 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
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 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|---------|-------|---------|------|-----------|------------|---------------|------------------|---------------------|------|-------------|-------------|-----------|--------------|
| | Demand Arrival Flows | | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total | HV | Total | HV | | | | | | Veh | Dist | | | | |
| | veh/h | % veh/h | veh/h | % veh/h | v/c | % | sec | | | | | m | m | % | % |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 12 | 0.0 | 12 | 0.0 | 568 | 0.021 | 100 | 11.4 | LOS B | 0.0 | 0.2 | Full | 500 | 0.0 | 0.0 |
| Approach | 12 | 0.0 | 12 | 0.0 | | | | 11.4 | NA | 0.0 | 0.2 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 19 | 0.0 | 19 | 0.0 | 714 | 0.027 | 100 | 7.3 | LOS A | 0.0 | 0.3 | Short | 48 | 0.0 | NA |
| Lane 2 | 4 | 11.8 | 4 | 11.8 | 154 | 0.029 | 100 | 24.1 | LOS C | 0.0 | 0.3 | Full | 500 | 0.0 | 0.0 |
| Approach | 23 | 2.2 | 23 | 2.2 | | | | 10.5 | LOS B | 0.0 | 0.3 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 43 | 2.1 | 43 | 2.1 | 1606 | 0.027 | 100 | 6.6 | LOS A | 0.0 | 0.3 | Short | 65 | 0.0 | NA |
| Lane 2 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 485 | 9.0 | 485 | 9.0 | 1729 | 0.281 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1013 | 8.7 | 1013 | 8.7 | | | | 0.3 | LOS A | 0.0 | 0.3 | | | | |
| Intersection | 1049 | 8.5 | 1049 | 8.5 | | | | 0.7 | NA | 0.0 | 0.3 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
 Lane LOS values are based on average delay per lane.
 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|-------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 573 | 9.2 | 573 | 9.2 | 1759 | 0.326 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1146 | 9.2 | 1146 | 9.2 | | 0.326 | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 18 | 11.8 | 18 | 11.8 | 1434 | 0.012 | 100 | 2.5 | LOSA | 0.0 | 0.1 | Full | 9 | 0.0 | 0.0 |
| Approach | 18 | 11.8 | 18 | 11.8 | | 0.012 | | 2.5 | LOSA | 0.0 | 0.1 | | | | |
| Intersection | 1164 | 9.2 | 1164 | 9.2 | | 0.326 | | 0.1 | NA | 0.0 | 0.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).

Lane LOS values are based on average delay per lane.

Minor Road Approach LOS values are based on average delay for all lanes.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|--------------|------|---------------|------|------|-----------|------------|---------------|------------------|---------------------|--------|-------------|-------------|-----------|--------------|
| | Demand Flows | | Arrival Flows | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. |
| | Total veh/h | HV % | Total veh/h | HV % | | | | | | Veh | Dist m | | | | |
| East: Beeljar Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 28 | 0.0 | 28 | 0.0 | 430 | 0.064 | 100 | 14.0 | LOS B | 0.1 | 0.6 | Full | 500 | 0.0 | 0.0 |
| Approach | 28 | 0.0 | 28 | 0.0 | | 0.064 | | 14.0 | NA | 0.1 | 0.6 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 20 | 0.0 | 20 | 0.0 | 607 | 0.033 | 100 | 8.4 | LOS A | 0.0 | 0.3 | Short | 48 | 0.0 | NA |
| Lane 2 | 12 | 0.0 | 12 | 0.0 | 115 | 0.105 | 100 | 32.6 | LOS D | 0.1 | 0.9 | Full | 500 | 0.0 | 0.0 |
| Approach | 32 | 0.0 | 32 | 0.0 | | 0.105 | | 17.5 | LOS C | 0.1 | 0.9 | | | | |
| West: Beeljar Drive (W) | | | | | | | | | | | | | | | |
| Lane 1 | 44 | 1.4 | 44 | 1.4 | 1594 | 0.028 | 100 | 6.7 | LOS A | 0.0 | 0.3 | Short | 65 | 0.0 | NA |
| Lane 2 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 3 | 624 | 5.1 | 624 | 5.1 | 1828 | 0.342 | 100 | 0.0 | LOS A | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1293 | 5.0 | 1293 | 5.0 | | 0.342 | | 0.3 | LOS A | 0.0 | 0.3 | | | | |
| Intersection | 1352 | 4.8 | 1352 | 4.8 | | 0.342 | | 1.0 | NA | 0.1 | 0.9 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).
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 Minor Road Approach LOS values are based on average delay for all lanes.
 NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



| Lane Use and Performance | | | | | | | | | | | | | | | |
|-----------------------------------|----------------------|---------|-------|---------|-----------|------------|---------------|------------------|---------------------|------|-------------|-------------|-----------|--------------|-----|
| | Demand Arrival Flows | | | Cap. | Deg. Satn | Lane Util. | Average Delay | Level of Service | Aver. Back of Queue | | Lane Config | Lane Length | Cap. Adj. | Prob. Block. | |
| | Total | HV | Total | | | | | | Veh | Dist | | | | | |
| | veh/h | % veh/h | veh/h | % veh/h | v/c | % | sec | | | m | | m | % | % | |
| East: Beeliam Drive (East) | | | | | | | | | | | | | | | |
| Lane 1 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Lane 2 | 609 | 6.9 | 609 | 6.9 | 1780 | 0.342 | 100 | 0.0 | LOSA | 0.0 | 0.0 | Full | 500 | 0.0 | 0.0 |
| Approach | 1218 | 6.9 | 1218 | 6.9 | | | | 0.0 | NA | 0.0 | 0.0 | | | | |
| North: Dunraven Drive (N) | | | | | | | | | | | | | | | |
| Lane 1 | 48 | 0.0 | 48 | 0.0 | 394 | 0.123 | 100 | 8.9 | LOSA | 0.1 | 1.1 | Full | 9 | 0.0 | 0.0 |
| Approach | 48 | 0.0 | 48 | 0.0 | | | | 8.9 | LOSA | 0.1 | 1.1 | | | | |
| Intersection | 1266 | 6.7 | 1266 | 6.7 | | | | 0.4 | NA | 0.1 | 1.1 | | | | |

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Network Data dialog (Network tab).

Lane LOS values are based on average delay per lane.

Minor Road Approach LOS values are based on average delay for all lanes.

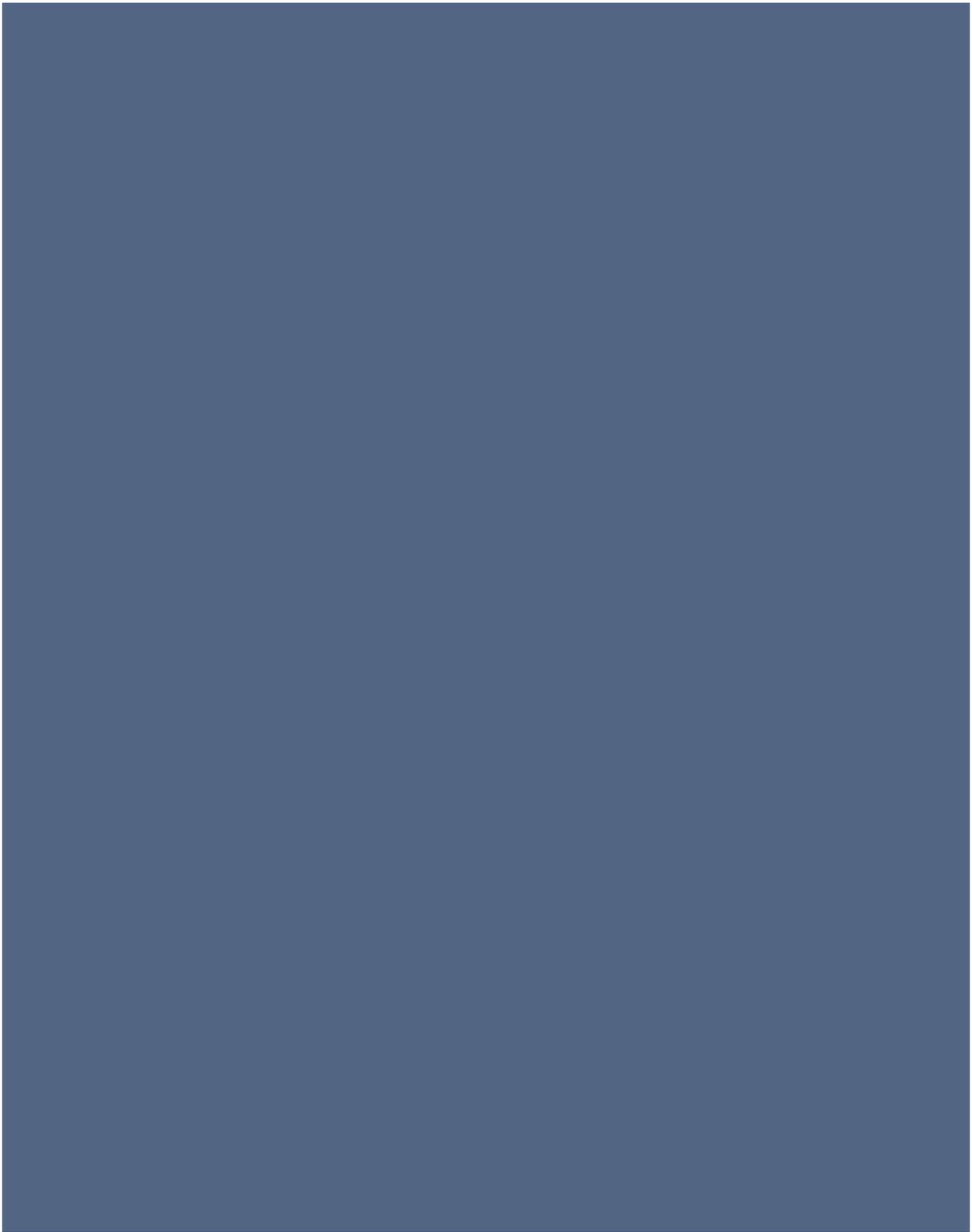
NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road lanes.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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22.3 (2020/MINUTE NO 0203) HISTORY - GLEN IRIS GOLF COURSE

Author(s) C Catherwood
Attachments 1. Zoning History

RECOMMENDATION

That Council notes the report.

COUNCIL DECISION

MOVED Cr K Allen SECONDED Cr P Eva

That the recommendation be adopted.

CARRIED 9/0

Background

At the 9 July 2020 Ordinary Council Meeting, Cr Stone requested that a report be prepared on the Glen Iris Golf Course, including a full history of the zoning, re-zoning, the offer of sale to Council, syndicate details, purchase by developers, and all other relevant background information.

Since the recent sale of the property, a significant amount has interest has been noted from the existing residential community surrounding the golf course land. To assist with keeping the community updated, the City has a dedicated webpage addressing some of the frequently asked questions and also containing information about planning processes and links to the developer's webpage and community engagement officer.

Submission

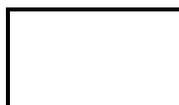
N/A

Report

In response to the above request, information is set out further below:

Zoning History

The City has had three local planning schemes over the years and the zoning history of the land under the earlier (now revoked schemes) is set out in Attachment 1. The changes to the land over time covered an extensive number of scheme amendments. These amendments all occurred to District Zoning Scheme No. 2 which was in place until December 2002, when the current scheme was introduced.



Current Zoning

The current planning scheme which applies to the land is Town Planning Scheme No. 3 (TPS3) and applies the following zones:

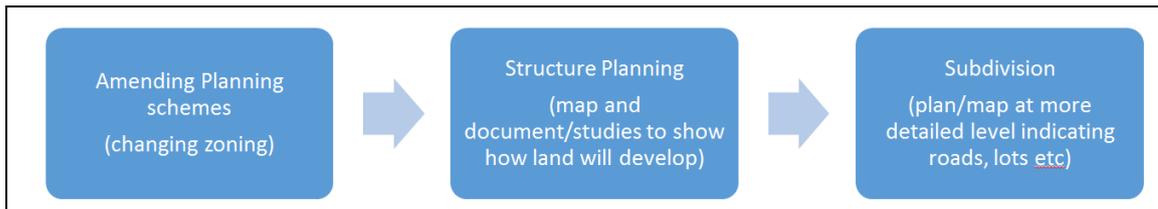
- Former golf course - Special Use Zone described as Special Use 1 (golf course estate, private recreation, hotel, convention centre and associated uses). This is shown outlined in magenta on the planning scheme map extract shown below.
- Former driving range - Residential zone (designated as R40 density).



Re-Zoning

The new landowner has made it known they intend to seek to redevelop the former golf course land. To facilitate a redevelopment, the TPS3 zoning would need to be amended (for the Special Use 1 portion). The driving range is also zoned residential and has a density assigned (ie: R40), meaning a subdivision could be lodged and considered.

There are three key planning processes which apply to redevelopments:



A quick overview of the key processes and common questions is shown in the table below:

| Frequently asked questions | Planning process | | |
|---|----------------------------|--|--|
| | Amending planning scheme/s | Structure Planning | Subdivision |
| Can the landowner apply? | ✓ | ✓ | ✓ |
| Requires consultation with surrounding landowners | ✓ | ✓ | × |
| Where do applications get submitted? | City of Cockburn | City of Cockburn | Department of Planning, Lands and Heritage |
| Average timeframe for a decision | ~10-12 months or longer | ~8-10 months | ~3 months |
| Decision maker | Minister for Planning | Western Australian Planning Commission | Western Australian Planning Commission |



It should be noted that amending the town planning scheme and the structure plan process are able to run concurrently.

In terms of those key processes, there are key issues to be considered at each stage, broadly described in the table below:

| Planning Process/Stages | Key Issues |
|---|---|
| <p>Amending Planning Scheme (rezoning):</p> <ul style="list-style-type: none"> • Initiation by Council • EPA referral • Advertising – public/government agency • Report on submissions • Final decision by Minister | <ul style="list-style-type: none"> • Alignment with state level planning (MRS) • Why the zoning change is being pursued • If changed, what is the appropriate zoning (a development area to enable a structure plan or straight to a particular zone) • Supporting assessments (eg. Environmental, servicing capability, traffic, bushfire, drainage etc) |
| <p>Structure Planning:</p> <ul style="list-style-type: none"> • Applicant preparation • Submission to LG • Assessment of Reg requirements provided (sufficient to advertise?) • Advertising – public/government agency • Report on submissions • Final decision by WAPC | <ul style="list-style-type: none"> • Broad level design and interface with surrounding development • Liaison with adjacent landowners/community • Alignment with state level planning (policies such as Liveable Neighbourhoods) • Supporting assessments (eg. Environmental, servicing capability, traffic, bushfire, drainage etc.) • Matters to be addressed at subdivision level |
| <p>Subdivision:</p> <ul style="list-style-type: none"> • Applicant preparation • Submission to DPLH • Referrals – government agency • DPLH assessment report • Final decision by WAPC | <ul style="list-style-type: none"> • More detailed level design and interface with surrounding development • Alignment with state level planning (policies such as Liveable Neighbourhoods) • Matters to be addressed for lot creation, servicing, civil drawings etc. (applied by conditions) |



Offer of Sale

The City was informally approached by the previous landowners in 2017 seeking to discuss the confidential sale of the Golf Course and whether the City would be interested in negotiating a purchase.

Following a meeting with the landowners it became clear that they considered the land was worth the equivalent residential land value and not the value as an operational golf course or reflective of its current zoning. Before considering any potential purchase the City sought details of the operational and management accounts for the golf course however the landowners were not forthcoming with this information. As a result the City was unable to undertake any due diligence in respect to preparing a business case on its potential purchase. The landowners subsequently proceeded to negotiate with a number of other parties based on the land being developed for residential purposes.

Syndicate details

These are not details provided to the local government as part of the planning process as they have no relevance in terms of planning regulation.

Purchase

The land, following its offer on the open market was purchased by a development company who have indicated their intention to redevelop the land. They have advised that they are not in the business of operating golf courses and did not intend to keep the golf course and associated clubhouse running.

Other Background Information

As a local government, our role in the key planning processes (for redevelopments) can be summarised as follows (the distinction of Council and City is used for clarity between Elected Members and the role of the Administration):

- Council initiates the rezoning process. This is a crucial step in the process which cannot be undertaken by City staff;
- The City determines sufficient information submitted to advertise proposal. This applied in the case of structure plans and is the only use of delegation in the redevelopment process. There are timeframes set under planning regulation to do this (which would not work in with the Council agenda cycle) and the WAPC also has the ability to intervene.
- The City facilitates community engagement and statutory consultation period
- The City provides technical guidance and information on policy/regulation
- The City compiles submissions;



- Council makes recommendation to the decision maker (WAPC) for scheme amendment and structure plans;
- The City makes recommendation to the decision maker (WAPC) for subdivisions (including whether they are aligned to the earlier stages of the planning processes and suitable technical conditions)

It should be clearly noted that the City is not the ultimate decision maker in the planning process for redevelopments, this authority lies with the WAPC.

Conclusion

The above information should be noted. There are no proposals in respect to the former Glen Iris golf course before Council for consideration at this point in time.

Strategic Plans/Policy Implications

Listening and Leading

A community focused, sustainable, accountable and progressive organisation.

- Ensure good governance through transparent and accountable, planning, processes, reporting, policy and decision making.
- Listen to, communicate, consult and engage with our residents, businesses and community in a timely, open and collaborative manner.

Budget/Financial Implications

N/A

Legal Implications

N/A

Community Consultation

Not applicable. This is a report for information on request of an Elected Member, not a proposal.

Risk Management Implications

This report does not form a proposal for which Council's position is being sought. This report of information is recommended to be noted by Council.

It should in no way be perceived as an indication of a position of any kind on any future proposals to be put forth for Council consideration at a later date.

Many community members have directly contacted Elected Members seeking their position on the matter of any future proposals. Elected



Members should keep in mind with considering this report their inability to provide commitments as to support or otherwise on any matter until Council's formal deliberations as part of a Council meeting

Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

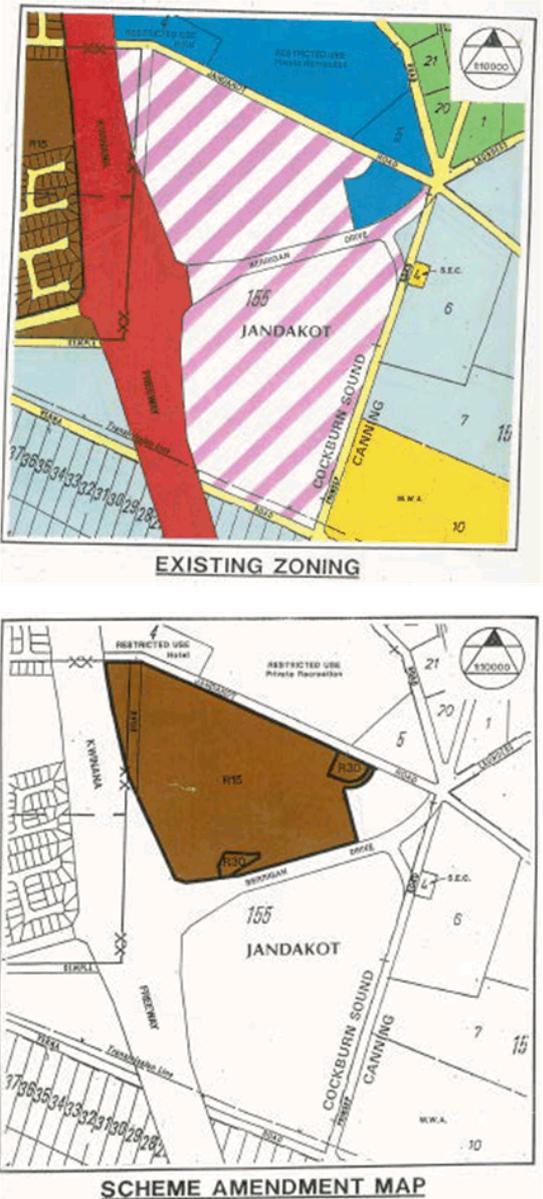
Nil

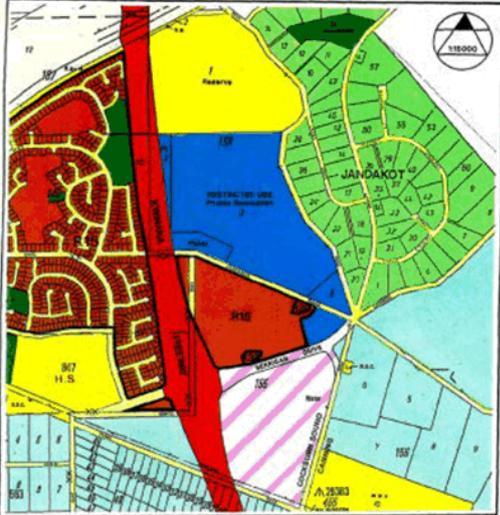
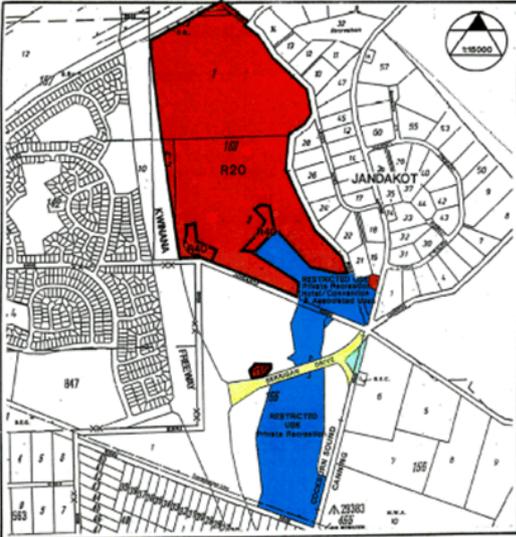


ATTACHMENT ONE - PREVIOUS REZONINGS UNDER DISTRICT ZONING SCHEME NO. 2 (DZS2)

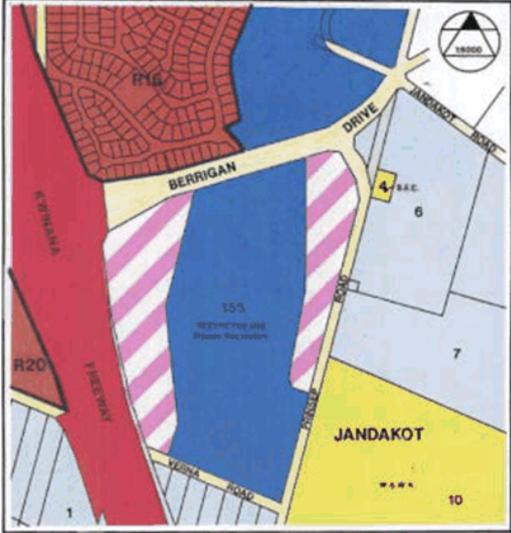
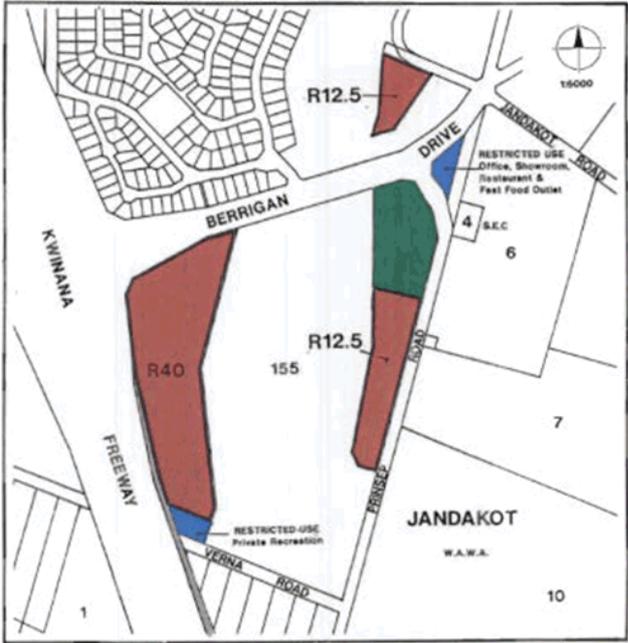
| Amdt # | Description | Date Gazetted |
|--------|--|--|
| 56 | <p>Rezoning portion of Lot 2, 3, 4 and 5 Jandakot Road from Commercial – Restricted Use – Private Recreation and Hotel to Residential R15/12.5, Restricted Use Private Recreation, and Hotel and Commercial.</p> <p>Amendment 56 was supported by the above Concept Plan which was adopted in 1990 as a Structure Plan to guide development.</p> | <p>OCM meeting July 1990 (gazette date not clear)</p> |



| Amdt # | Description | Date Gazetted |
|--------|--|----------------------|
| 64 | <p>Rezoning portion of JAA Lot 155 Jandakot Road and portion of CSL 544 Lot 54 Potters Place, Jandakot from Light Industry to Residential R15 and R30.</p>  <p>EXISTING ZONING</p> <p>SCHEME AMENDMENT MAP</p> | 2 October 1992 |

| Amdt # | Description | Date Gazetted |
|--------|---|-----------------|
| 82 | <p>Rezoning portion of JAA 170, JAA 169, Lots 1 Lakes Way, Lots 3, 4, 5 Jandakot Road and Lot 20 Prinsep Road, portion of JAA 155, and portion of Dean Road, Jandakot Road, Berrigan Drive and Prinsep Road from Public Purposes (Special Use) Reserve, Commercial – Restricted Use – Private Recreation/Hotel, Residential R15 and R30, Light Industry, Commercial, Special Rural and Local Reserve – Local Road to Residential R15, R20 and R40, Commercial Restricted Use – Private Recreation, Commercial – Restricted Use – Private Recreation Hotel Convention Centre and Associated Uses, Commercial, Rural and Local Reserve – Local Road.</p> <div style="text-align: center;">  <p>EXISTING ZONING</p>  <p>SCHEME AMENDMENT MAP</p> </div> | 15 October 1993 |



| Amdt # | Description | Date Gazetted |
|--------|---|---------------|
| 119 | <p>Rezoning portion of Lot 139 and JAA Lot 155 Berrigan Drive, Prinsep Road, Imlah Court, Jandakot from Light Industry, Commercial Restricted Use – Private Recreation and Rural to Residential R12.5 and R40, Commercial – Restricted Use (Office, Showroom, Restaurant & Fast Food Outlet), Parks and Recreation and Railways.</p>  <p style="text-align: center;">EXISTING ZONING</p>  <p style="text-align: center;">SCHEME AMENDMENT MAP</p> | 15 April 1997 |

| Amdt # | Description | Date Gazetted |
|--------|---|------------------|
| 168 | Rezoning portion of Lot 3, Lot 4 and Lot 504 and Dean Road to R20/R40 and R40, Local Reserves | 27 February 1998 |

WHY IS THE DRIVING RANGE ZONED R40?

The concept plan attached to the Amendment No. 82 document shows the driving range as "Driving Range or R40/Convention Centre". It would appear when the time came to develop, they decided on the former. Amendment No. 82 zoned the land "Restricted Use – Private Recreation Hotel/Convention & Associated Use". A subsequent amendment must have rezoned the land to Residential R40.



22.4 (2020/MINUTE NO 0204) FUTURE USES OF UNMADE METROPOLITAN REGION SCHEME (MRS) ROAD RESERVES IN THE NORTHERN AREA OF THE CITY AND ALONG THE COCKBURN COAST DEVELOPMENT AREA

Author(s) C Catherwood

Attachments N/A

RECOMMENDATION

That Council notes the report.

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 9/0

Background

At the 13 August 2020 Ordinary Council Meeting, Cr Corke requested a report on the following:

'Future uses of unmade MRS Road Reserves in the northern area of the City and along the Cockburn Coast Development Area'.

Reason

Now that the eastern portion of the Roe 8 reserve has been reclassified as Class A Reserve it is time to revisit the future uses of the remaining road reserves in the above areas, in consultation with the State authorities which have an interest in these road reserves. There are many projects that have been on hold for far too long.

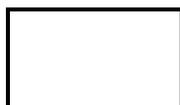
If the future of the road reserves was clearer, decisions as to surrounding land uses and options would be better informed. Furthermore, even the knowledge that nothing will happen for a defined period of time would give residents more certainty.

There are a number of proposals – for a Wildflower Walking Trail, for tree planting in the degraded portion of the Roe 9 reserve just west of Carrington, for revegetation in other locations – that could go ahead if there was clarity about at least the immediate future.

It is an appropriate time to give some certainty to Coolbellup and Hamilton Hill residents by determining what, if any plans, the State authorities have at this time for these road reserves.

Submission

N/A

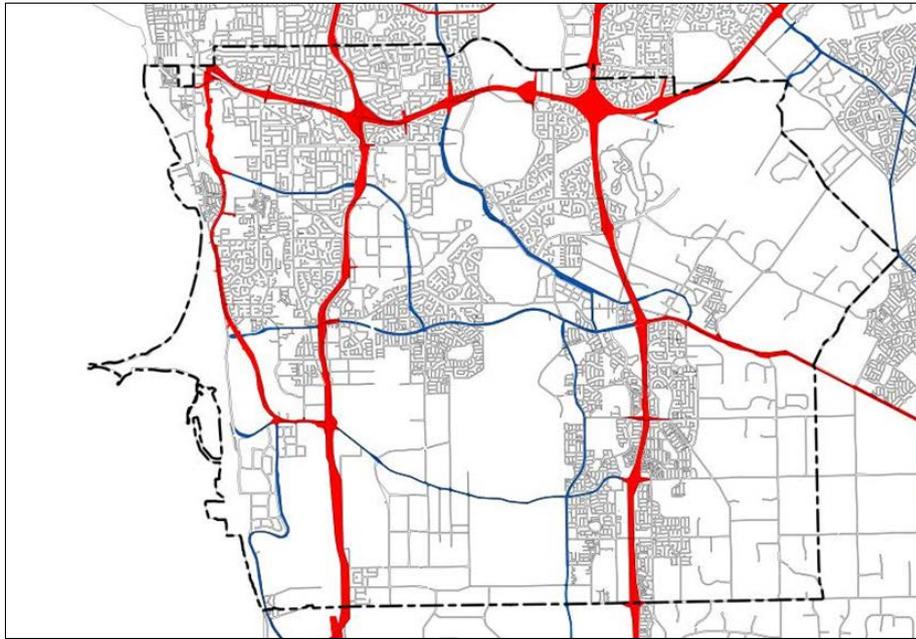


Report

Location of MRS road reserves within the City of Cockburn

There is an extensive network of regional road reservations across the City of Cockburn as shown in the image below.

The Metropolitan Region Scheme (MRS) designates the reservation of the Primary Regional Road (PRR or 'red road') network and the Other Regional Road (ORR or 'blue road') network.



Statutory Context

The PRR network is controlled and provided by Main Roads WA, while the ORR is maintained by the relevant local governments.

The *Main Roads Act 1930* provides for the Governor, on advice of the Commissioner of Main Roads to declare roads as part of either of these networks.

Consideration of the Request

It is important to be clear which network this request relates to:

- the Primary Regional Road network (Main Roads WA); or
- the Other Regional Road network controlled by the local government.

The examples cited in the request are from the PRR network:

- Along the Cockburn Coast Development Area (which is the segment of Cockburn Coast Drive extending Spearwood Avenue to Rollinson Road),
- The northern area of the City (which would be the east-west connection between Rollinson Road and the new Murdoch Link near the freeway, often described as 'Roe 8' and 'Roe 9').



None of these road reserves are within the control of the local government.

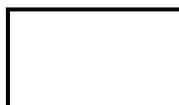
The State Government's position of Roe 8 and 9 is clear – there is no intent for the road to be delivered. In terms of Cockburn Coast Drive, it is the City's understanding it does not appear in forward capital works planning over the coming few decades. This would appear to indicate its delivery is extremely long term, or a lack of intent to build this road at all.

City's Draft Integrated Transport Strategy

The City is preparing a draft Integrated Transport Strategy (ITS) with a view to providing a vision and high-level direction for the City of Cockburn transport network, which will inform policy, advocacy and infrastructure decisions. This is likely to be presented for consideration later in 2020. This document should be considered holistically and it would be premature to determine what outcomes that document will seek. Given this it is recommended that no specific actions be undertaken at this time. The information below represents what some of the likely matters related to the roads subject to this request.

The City is reliant on having a robust and carefully considered road network, in order to provide access to opportunities for residents as well as supporting the diversity of commercial and industrial land uses. Regional connectivity needs to be balanced with local needs, as well as the preservation of areas of social, heritage and environmental value.

Consideration will need to be given to the State Government's *Perth and Peel @3.5 million*, which does not delineate the current reserves known as Roe 8 and Roe 9 contributing to regional connectivity.



The location of the Cockburn Coast Drive reserve is shown on the aerial photo below:



Cockburn Coast Drive has been identified as a future road corridor in the MRS for many years and is reflected in the Cockburn Coast District Structure Plan. All of the more detailed planning for Cockburn Coast (undertaken with MRWA) assumes the delivery of Cockburn Coast Drive as currently reserved, with Cockburn Road handling the interim traffic load until Cockburn Coast Drive is delivered, allowing Cockburn Road to be scaled back to a more pedestrian friendly environment.

The City of Cockburn could work collaboratively with the City of Fremantle and Main Roads WA to resolve its alignment, design and functionality.

The draft ITS can include advocacy items such as the above.

Rehabilitating Roe Highway – Bill relating to the Beelihar Wetlands

The program of works to rehabilitate the Roe 8 Corridor is underway.

Environmental consultants in collaboration with community, scientists, State Government and the City have drafted a 10 year Rehabilitation Management Plan (RMP) for the Corridor, which spans 4.5 kilometres from Bibra Drive, to west of Stock Road.

In November 2018, the Metropolitan *Region Scheme (Beelihar Wetlands) Bill 2018* was introduced to Parliament. The purpose of this bill is to amend the Metropolitan Region Scheme to change the reservation of certain land in the Beelihar wetlands and for related purposes. Its current status is “Legislative Council Second Reading”. This Act comes into operation on the day on which it receives the Royal Assent.

As noted in the Minister for Planning’s Media Statement (Nov 2018), which accompanied tabling of this Bill, consideration of the appropriate zoning for a portion of land reserved for Primary Regional Roads to the

west of North Lake Road will be subject to public consultation on the most appropriate use. This again, is a process which will take time to ensure an appropriate level of community engagement is achieved and will likely attract significant interest.

Changes to the Metropolitan Region Scheme (MRS) Reservations

An amendment to the MRS changes the zoning or reservation of land to allow for a different land use. This would be the process needed to formalise any discontinuation of a road project, other than the section covered by the Bill described above.

In general, a major MRS amendment usually takes 24 months to complete, depending on the environmental assessment process, the number of submissions, and requests for hearings. The Western Australian Planning Commission (WAPC) will consider all submissions before making a recommendation to the Minister for Planning.

Changes to Local Planning Scheme

An amendment to the City's local planning scheme, to reflect changes to the MRS, may be required. This occurs following the gazettal of any MRS amendment. This takes approximately 10-12 months or longer.

Temporary Projects within the Reservations

There is scope for requests to utilise portions of reserved roads for alternative uses. There are two scenarios that may occur:

- Where a road will not be constructed in the near future, rendering some investment worthwhile for an activity or more temporary or short lived infrastructure in the interim;
- Where a road is no longer required and is likely to be returned to another purpose. In this case, it is important that matters such as environmental attributes are protected (as these would no longer be lost to a road in future) but also that any parties undertaking these short term land uses accept they are short term and do not then turn into a fixed constraint which needs to be accommodated in the future planning of the area.

Both of the above, require approval. A recent example is the horse-riding trail at Manning Park, and if approved, the Manning Park Mountain Bike Trail (currently undergoing consultation)

Conclusion

Given the road reserves of interest are within Main Roads WA control, the planning, budgeting and project management of these road reserves cannot be changed by the local government.

The City can collaborate and encourage Main Roads WA to resolve the future of these road reserves and determine if the reserves are still needed, and if they are, when they may be realistically delivered. This



can be captured in the ITS objectives and actions, enabling City Officers to undertake these actions as a matter of course. It is important to understand the results of such discussions would take time to see any impacts on the reservation in the MRS and the City's Local Planning Scheme.

Where not needed, Main Roads WA should advise the Department of Planning, Lands and Heritage. This would enable the State to designate the alternative MRS zone or reserve to be applied as part of a future amendment to the Metropolitan Region Scheme. Local government should be consulted about the alternative zone or reserve to be applied, to ensure it could integrate with the surrounding land.

On the basis of the above it is recommended Council notes the report.

Strategic Plans/Policy Implications

Community, Lifestyle and Security

A vibrant healthy, safe, inclusive and connected community.

- Provide community, sport, recreational, and cultural facilities and infrastructure to meet our community needs.

City Growth and Moving Around

A growing City that is easy to move around and provides great places to live.

- Plan, develop and advocate for safe, sustainable, integrated local transport networks, public transport and regional transport networks.
- Advocate and plan for reduced traffic congestion.

Budget/Financial Implications

Nil

Legal Implications

Main Roads Act 1930

Planning and Development Act 2005

Community Consultation

N/A

Risk Management Implications

This report does not form a proposal for which Council's position is being sought, though it does include a brief discussion about the City's draft ITS which will be presented to Council in future.

This report of information is recommended to be noted by Council.



Advice to Proponent(s)/Submitters

N/A

Implications of Section 3.18(3) *Local Government Act 1995*

Nil



22.5 EXPANSION OF NATURE PLAY COMPONENTS WITHIN THE EXISTING PLAY SPACES ACROSS THE CITY**Author** C Sullivan

Cr Eva has requested that a report be prepared to a future meeting of Council to investigate the expansion of nature play components within existing play spaces across the City.

Reason

Recent community feedback I have received has strongly indicated the desire for expanded nature play opportunities for children. The pop up nature play (loose parts) initiative has proven very popular in the community and has had a positive effect on the health and well-being of particularly the pre-primary age group.

A review of the existing City strategies and plans for public open space areas and the children and family strategy should be undertaken to assess additional nature play opportunities to improve public participation.

22.6 LOCKERS AT CITY OF COCKBURN CONTROLLED BEACH LOCATIONS**Author** C Sullivan

Cr Stone has requested that a report be prepared to a future meeting of Council to investigate the provision of lockers at City of Cockburn controlled beach locations.

Reason

During summer, WA Police acknowledge an increase in theft from vehicles at our popular beach locations.

The provision of beach lockers during peak times may assist in the reduction of crime in Cockburn, which according to the annual community scorecard, is still a major concern of residents.



23. CONFIDENTIAL BUSINESS

DECLARATION OF INTEREST

The Presiding Member advised the meeting that three Declarations of Interest had been received for this item:

A Declaration of Impartiality Interest submitted by Mayor Logan Howlett, pursuant to Regulation 11 *Local Government (Rules of Conduct) Regulations 2007*. The nature of the interest being that Mayor Howlett was a named respondent in the Fair Work Commission Hearing.

A Declaration of Impartiality Interest submitted by Cr Kevin Allen, pursuant to Regulation 11 *Local Government (Rules of Conduct) Regulations 2007*. The nature of the interest being that Allen was a named respondent in the Fair Work Commission Hearing.

A Declaration of Impartiality Interest submitted by Cr Lee-Anne Smith, pursuant to Regulation 11 *Local Government (Rules of Conduct) Regulations 2007*. The nature of the interest being that Cr Smith was a named respondent in the Fair Work Commission Hearing.

Cr Kevin Allen left the meeting at 9.45pm.

(2020/MINUTE NO 0205) MEETING TO PROCEED BEHIND CLOSED DOORS

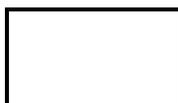
COUNCIL DECISION

MOVED Cr C Stone SECONDED Cr M Separovich

That, pursuant to Section 5.23(2)(a) of the *Local Government Act 1995*, the Council meeting proceeds behind closed doors to consider Item 23.1.

LOST 3/5

Cr Kevin Allen returned to the meeting at 9.47pm.



23.1 (2020/MINUTE NO 0206) INVESTIGATION - FAIR WORK COMMISSION HEARING

This report and its attachments are **CONFIDENTIAL** in accordance with Section 5.23(2) (a) of the *Local Government Act 1995*, which relates to the following:

(a) *a matter affecting an employee or employees.*

COUNCIL DECISION

MOVED Cr P Corke SECONDED Cr M Separovich

That the recommendation contained in the Confidential Report be adopted.

CARRIED 5/4

23.2 (2020/MINUTE NO 0207) ENTER INTO AN AGREEMENT TO SUBLEASE AND ENTER INTO A SUBLEASE WITH INTERNATIONAL GOLF INVESTMENTS - BAKER COURT, NORTH LAKE

This report and its attachments are **CONFIDENTIAL** in accordance with Section 5.23(2) (c) of the *Local Government Act 1995*, which relates to the following:

(c) *a contract entered into, or which may be entered into, by the local government and which relates to a matter to be discussed at the meeting.*

COUNCIL DECISION

MOVED Cr C Terblanche SECONDED Deputy Mayor L Kirkwood

That the recommendation contained in the Confidential Report be adopted.

CARRIED 9/0

24. (2020/MINUTE NO 0208) RESOLUTION OF COMPLIANCE

RECOMMENDATION

That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

COUNCIL DECISION

MOVED Cr P Eva SECONDED Deputy Mayor L Kirkwood

That the recommendation be adopted.

CARRIED 6/3

Cr Separovich and Cr Stone requested that their vote against the motion be recorded in the minutes.

25. CLOSURE OF MEETING

The meeting closed at 9.55pm.

