

# **CITY OF COCKBURN**



**ORDINARY COUNCIL**

**AGENDA PAPER**

**FOR**

**THURSDAY, 9 OCTOBER 2014**

# CITY OF COCKBURN

## SUMMARY OF AGENDA TO BE PRESENTED TO THE ORDINARY COUNCIL MEETING TO BE HELD ON THURSDAY, 9 OCTOBER 2014 AT 7:00 PM

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**CITY OF COCKBURN****AGENDA TO BE PRESENTED TO THE ORDINARY  
COUNCIL MEETING TO BE HELD ON  
THURSDAY, 9 OCTOBER 2014 AT 7:00 PM**

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1. **DECLARATION OF MEETING**
2. **APPOINTMENT OF PRESIDING MEMBER (If required)**
3. **DISCLAIMER (To be read aloud by Presiding Member)**

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

4. **ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)**
5. **APOLOGIES AND LEAVE OF ABSENCE**
6. **ACTION TAKEN ON PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE**  
Nil
7. **PUBLIC QUESTION TIME**
8. **CONFIRMATION OF MINUTES**
  - 8.1 **(OCM 9/10/2014) - ORDINARY COUNCIL MEETING - 11/09/2014**

**RECOMMENDATION**

That Council adopt the Minutes of the Ordinary Council Meeting held on Thursday, 11 September 2014, as a true and accurate record.

**COUNCIL DECISION**

**9. WRITTEN REQUESTS FOR LEAVE OF ABSENCE**

Nil

**10. DEPUTATIONS AND PETITIONS**

**11. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)**

Nil

**12. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER**

**13. COUNCIL MATTERS**

**13.1 (OCM 9/10/2014) - WA LOCAL GOVERNMENT ASSOCIATION (WALGA) - LOCAL GOVERNMENT ACT 1995 - POLL PROVISIONS (089/004) (D GREEN) (ATTACH)**

**RECOMMENDATION**

That Council advises WALGA that it supports an extension to the poll provisions of the Local Government Act 1995 to enable the community of a local government to demand a poll where proposed boundary adjustments to the District will result in a variation to the District affecting the lesser number of 10%, or 250, rateable properties, in that District.

**COUNCIL DECISION**

**Background**

Correspondence has been received from WALGA seeking the input of member local governments on resolutions from the Annual General Meeting (AGM) relative to the poll provisions of the Local Government Act 1995 (the Act), otherwise known as the "Dadour" provisions.

The intent of the WALGA AGM resolutions was due to the ongoing concern that the state government is seeking to circumvent the ability of communities to initiate a poll of electors where proposals made to the Local Government Advisory Board (LGAB) result in the abolition of, or “significant” boundary adjustment to, a local government District.

WALGA State Council has subsequently resolved at its 3 September 2014 meeting to seek members` input to a number of options which could be used to ensure that the mechanisms currently being utilised to “exploit” the perceived intent of the Dadour provisions of the Act are drafted in such a way that will entrench the capacity of local government communities to access the poll provisions of the Act, where any “significant” boundary adjustments are contemplated by any proposal.

### **Submission**

N/A

### **Report**

The current local government “reform” process has raised the concerns of local governments and their communities whereby the state government is being perceived as “manipulating” the provisions of the Act to enable “boundary adjustments” to achieve its reform agenda, thereby avoiding the capacity of some communities to demand a poll of electors where a District is either abolished, or “significantly affected”.

The belief that the “Dadour” provisions would apply to any local government as a “last line of defence” for communities to access as a means of testing elector sentiment in the current reform environment has led to the call for these poll provisions to be not only clarified in their current form, but also extended to ensure that communities cannot be omitted from the process in future.

Given the overwhelming support for these provisions to be enhanced in the legislation, WALGA has proposed a series of options for individual Councils to consider supporting, in an attempt to ensure the retention and clarity of the poll provisions are recognised.

The options, as shown in the attachment, are that any community should have the right to demand a poll of electors in the following circumstances:

1. Under **any** boundary change proposal

**OR**

2. With a significant variation in population, rateable properties or revenue by:
  - a. 10%
  - b. 25%
  - c. 50%

While there are practical restrictions in each of these options, it is clear that there is a high level of concern within the local government sector that the attempt by the state government to manipulate outcomes by circumventing the “intention” of the “Dadour” provisions in the current metropolitan local government reform process represents a fundamental breach of faith to those affected communities who would be denied the right to call for a poll under the current statute.

This concern has also been shared by the government’s partners (WA Nationals), the opposition WA Labor Party and even some of the government’s own members. This level of concern in the political sphere seems now to be affecting the release of the final recommendations of the Minister, causing even greater uncertainty within the sector.

Having carefully considered the relevant legislative impacts the proposed WALGA options provide, it is suggested that the intent of part b) of its Motion could be better aligned to current provisions by referring to the same numerical circumstances as mentioned in the current Act (i.e. Schedule 2.1 clause 8. (3)) where the lesser of 250, or 10%, of affected electors can demand a poll where “amalgamations” are recommended. It is also recommended that only “rateable properties” be included as being “affected”, given the relationship between electors and rateable properties is more tangible than general data sets, such as “population”, or “revenue”, as proposed in the WALGA Motion.

To maintain that relevance, it is recommended that Council’s response reflects those figures, even though it varies slightly from the options provided by WALGA, which are portrayed in percentage terms only.

The WALGA response template gives flexibility to include this variance of feedback, while maintaining the original intent to ensure the right of communities to demand a poll where there is a “significant” change to the local government District boundaries involved in any proposal submitted for assessment by the LGAB.

### **Strategic Plan/Policy Implications**

#### **Leading & Listening**

- Effective advocacy that builds and manages relationships with all stakeholders.

#### **Budget/Financial Implications**

N/A

#### **Legal Implications**

Schedule 2.1 of the Local Government Act 1995, refers.

#### **Community Consultation**

WALGA is seeking direct input from member Councils by 31 October 2014.

#### **Attachment(s)**

WALGA 'Infopage'.

#### **Advice to Proponent(s)/Submissioners**

The Proponent has been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

#### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **14. PLANNING AND DEVELOPMENT DIVISION ISSUES**

### **14.1 (OCM 9/10/2014) - CONSIDERATION TO ADOPT BUSHFIRE HAZARD ASSESSMENT MAPS - APPLICANT: CITY OF COCKBURN - OWNER: VARIOUS (110/112) (C HOSSEN) (ATTACH)**

#### **RECOMMENDATION**

That Council:

- (1) adopt the Bushfire Hazard Assessment Maps as shown in Attachment 1 in accordance with proposed Clause 6.6.5 (b) of City of Cockburn Town Planning Scheme No. 3 ("Scheme") without modification, subject to the gazettal of Amendment No. 92 (Bush Prone Areas);
- (2) in accordance with draft clause 6.6.6 of the Scheme, and

subject to the gazettal of Amendment No. 92, publish a notice of the adoption of the Bushfire Hazard Assessment Maps in (1) above; and

(3) advise all submitters of Council's decision accordingly.

## COUNCIL DECISION

### Background

At its Ordinary Council Meeting held on 12 April 2012 Council resolved to initiate Amendment No. 92 to City of Cockburn Town Planning Scheme No. 3 ("Scheme"). Amendment 92 was then subsequently advertised and presented to Council for final adoption on 9 May 2013.

The purpose of the amendment was to place a Special Control Area over land currently zoned Rural, Resource, Rural Living and Conservation under the Scheme, dealing with bushfire risk management through the planning process. The amendment also proposed a number of alterations and additions to the Scheme Text.

Amendment No. 92 outlines the procedures to be followed in the preparation of a Bushfire Hazard Assessment Map. The City has in anticipation of the approval of the amendment by the Hon. Minister, prepared and advertised the Bushfire Hazard Assessment Maps. These are shown in Attachment 1.

The purpose of this report is for Council to consider for adoption, without modification, the Bushfire Hazard Assessment Maps. This is of course subject to the approval by the Hon Minister and gazettal of Amendment No. 92, which at the time of this report was in its final stages.

### Submission

The Bushfire Hazard Assessment Maps have been prepared by the City's Strategic Planning Business Unit.

## Report

### Status of the Bushfire Hazard Assessment Map

Draft clause 6.6.1 of the City's Scheme states that, a Bushfire Prone Area means any area located in the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone, identified by the local government and shown on a Bushfire Hazard Assessment Map. Further to this, draft Clause 6.6.3 states that a Bushfire Hazard Assessment Map shall indicate bushfire prone areas.

Therefore the establishment of Bushfire Hazard Assessment Maps are a vital instrument to achieve a suitable 'head-of-power' to enforce the AS3959-2009 bushfire building standard and complimentary planning responses to the identified bushfire risk in the relevant Bushfire Prone Area.

The Council recommendation notes that Scheme Amendment No. 92 is yet to receive Ministerial endorsement and be gazetted in line with the requirements of the *Town Planning Regulations 1967*. Therefore the City is required to await the gazettal of the amendment before formally implementing the requirements, including the adoption of any Bushfire Hazard Assessment Map.

### Identification of Bushfire Prone Land

Council at its meeting on 12 April 2012 endorsed the following methodology for use on any future Bushfire Hazard Assessment Map:

- Inclusion of identified native vegetation of 1 Ha or greater (by aerial photograph);
- Identification of native vegetation less than 1 Ha in size but within 50m of identified native vegetation (>1 Ha);
- Buffering of all the above by 100m (shown in different colour from main hazard area).

For the purpose of registering an accurate assessment of bushfire risk; the identification of bushfire prone areas includes land meeting the first two criteria but not within area covered by Bushfire Hazard Assessment Map, including areas of native vegetation within neighbouring Local Government Areas.

Subsequently, the Office of Bushfire Risk Management, an independent branch of the Department of Fire and Emergency Services, released the State Bushfire Mapping Standard. In response to this and the need for a consistent approach to the mapping of bushfire risk, the Council through the Delegated Authorities, Policy and Position Statements Committee adopted the State Bushfire Mapping

Standard as the new benchmark for mapping risks under any Bushfire Hazard Assessment Map.

The level of conflict between the two utilised approaches is minimal and has not lead to any dramatic change in the area covered by a Bushfire Hazard Assessment Map. This change is reflected in the City's Bushfire Prone Areas Local Planning Policy adopted, subject to the gazettal of Scheme Amendment No. 92, by Council at its September 2014 OCM.

### Public Consultation

In accordance with the requirements of draft Clause 6.6.4 of the City's Scheme, the Bushfire Hazard Assessment Maps were advertised for a period of 21 days to affected residents, state authorities with the required notice also placed in the Cockburn Gazette. Approximately 1,100 landowners were written to as part of the consultation period.

A total of 2 submissions were received at the close of the 21 day advertising period. The submissions raised no new issues in respect of implementing the bushfire prone planning controls.

The City has previously written to all landowners within the Resource, Rural and Rural Living three (3) times regarding the initiative to implement Bushfire Prone Areas. The response rate to all bulk mail outs has consistently been low; averaging approximately 0.1% of the total letters mailed out.

### Conclusion

It is considered that the proposed Bushfire Hazard Assessment Map will provide clear and consistent advice to residents and landowners as to the extent of Bushfire Prone Areas within the City of Cockburn. Moreover, the Bushfire Hazard Assessment Maps provide consistency between the requirements of Amendment No. 92 and the soon to be approved State Bushfire Prone Areas Map.

It is therefore recommended that Council proceed to adopt the Bushfire Hazard Assessment Maps as outlined above.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

- Development that is soundly balanced between new and existing areas.

### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

### **Leading & Listening**

- Effective and constructive dialogue with all City stakeholders.

### **Budget/Financial Implications**

There are nil direct financial impacts faced by the Local Government. It is noted, as it has in all previous reports on Bushfire Prone Areas, that there are financial impacts associated with higher building costs in order to address bushfire prone areas.

### **Legal Implications**

*Planning and Development Act 2005*  
City of Cockburn Town Planning Scheme No. 3  
*Town Planning Regulations 1967*

### **Community Consultation**

In accordance with the requirements of draft Clause 6.6.4 of the City's Scheme the Bushfire Hazard Assessment Maps were advertised for a period of 21 days to affected residents, state authorities with the required notice also placed in the Cockburn Gazette.

### **Attachment(s)**

1. Bushfire Hazard Assessment Maps
2. Schedule of Submissions

### **Advice to Proponent(s)/Submissioners**

Those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**14.2 (OCM 9/10/2014) - ADOPTION OF DRAFT HOUSING AFFORDABILITY AND DIVERSITY STRATEGY FOR COMMUNITY CONSULTATION (110/089) (D DI RENZO) (ATTACH)**

**RECOMMENDATION**

That Council:

- (1) adopt the Housing Affordability and Diversity Strategy for final approval as included at Attachment 1; and
- (2) advise those who have made a submission of Council's decision.

**COUNCIL DECISION**

**Background**

The City of Cockburn, like much of Western Australia, is facing a significant challenge in housing affordability.

There has been a growing focus on the requirement for action and cooperation across all levels of government to address housing affordability issues, particularly evidenced by the Council of Australian Governments ("COAG") National Affordable Housing Agreement 2009. The National Affordable Housing Agreement aims to ensure that all Australians have access to affordable, safe and sustainable housing.

Local Government has an important role to play in facilitating affordable and diverse housing. The City has recognised the importance of this issue in the Strategic Community Plan which identified the provision of diverse housing to respond to changing needs as a key objective. Access to secure, appropriate and affordable housing is a fundamental requirement and an essential component of an inclusive and sustainable city.

Examination of housing affordability and diversity often occurs as part of a local housing strategy. These generally comprise an analysis of local housing supply and demand, future oriented demographic and market trends, as well as policy statements and recommendations for planning processes, town planning schemes, and development controls.

The City's approach has been to develop urban revitalisation strategies which serve the function of a local housing strategy. The City has adopted two urban revitalisation strategies - the Phoenix Central Revitalisation Strategy and Hamilton Hill Revitalisation Strategy, and project planning has commenced for the Coolbellup Revitalisation Strategy.

This approach has been successful in the City, and in accordance with the City's Strategic Community Plan this approach is proposed to continue.

However, in addition to the preparation of urban revitalisation strategies, it is considered that the issue of housing affordability and diversity needs to be examined across the whole of the City. It was therefore proposed that a Housing Affordability and Diversity Strategy be prepared.

Council at its meeting of 8 May 2014 resolved to adopt the Housing Affordability and Diversity Strategy ("Draft Strategy").

The Draft Strategy was subsequently advertised for public comment for a period of 30 days ending on 31 July 2014.

The purpose of this report is for Council to consider the submissions received and make a decision regarding the final approval of the Strategy.

### **Submission**

N/A

### **Report**

The key objectives of the Draft Strategy are:

- a) To provide households with access to housing that is appropriate to their needs in terms of size, physical attributes and location.
- b) To provide housing that is affordable to households of varying financial capacity.
- c) To provide a variety of housing types in locations that have good accessibility to public transport, and essential services.
- d) To promote affordable living, taking into consideration the total cost of living in a dwelling, including energy and water consumption, the price of transport to access employment and essential services, and other daily needs impacted by location.

The Draft Strategy includes a housing needs assessment which examines and analyses demographic projections, with a focus on household composition and size, and age structure. This is followed by an assessment of the current and projected housing stock, and urban form, to determine the appropriateness for current and future households.

To address housing affordability 'low and moderate income' households have been defined to assess and plan for the housing needs of these households. The issue of homelessness was also examined.

A market assessment has enabled household incomes to be assessed against housing prices and rental costs to ascertain housing affordability. Census data has been supplemented with real estate data and other research that has been undertaken to analyse trends on housing prices and rents.

From this assessment a number of key issues were highlighted, and the Strategy identifies mechanisms for addressing the key findings, and identifies a number of actions under the headings of 'Planning Mechanisms'; 'Partnerships'; and 'Leadership, Advocacy and Communication' (see Attachment 1).

The key findings of the assessment undertaken in the Strategy were:

#### Housing stock mismatch

The City's housing stock of predominately large detached dwellings will not provide a good range of options for future households which are getting smaller, and will be predominately one and two person households.

#### Need for compact urban form

The City should continue to strive towards a more compact urban form in existing and new areas, creating walkable, mixed use neighbourhoods, and dwellings with good accessibility to public transport and essential services.

#### Declining housing affordability

Housing affordability is declining to the point where housing is becoming unaffordable for low and moderate income earners, and more households are in housing stress. This has negative impacts for the whole community.

### Cost of living impacts for low income households

While all households are impacted by increasing costs of living, it is low income households that are most affected.

### Need for adaptable housing (Universal Housing Design)

There is a lack of private and public adaptable housing in the City of Cockburn and Perth Metropolitan area generally. This means many people, particularly elderly people and those with disabilities, face living in inappropriate housing or requiring costly modifications to their dwellings.

### Demand for Aged Care Facilities

The ageing population, particularly the increase in people over 70 years of age, will see an increased demand for aged care facilities for those who can no longer live independently, and it is likely that this demand will outstrip supply.

### Shortage of crisis accommodation

There is an identified shortage of crisis accommodation in the City of Cockburn, and this is an important issue given the trend towards increasing levels of homelessness (which includes people living in inappropriate housing).

### Community Consultation

The draft Strategy was advertised for public comment for a period of 30 days and a total of seven submissions were received. All submissions are set out and addressed in the Schedule of Submissions found at Attachment 2.

Four of the submissions that were received wholly supported the Strategy. This included submissions from two landowners, Shelter WA and Disability Services Commission.

The Building Commission's submission requests that adoption of such initiatives does not lead to an increase in construction requirements more stringent than the applicable building standards within the Building Regulations 2012 (i.e primarily those contained in the Building Code of Australia). The Strategy does not include any requirements that will lead to an increase in construction requirements. Rather the actions of the Strategy are simply seeking to better inform the community of the Liveable Design Guidelines and sustainable principles of design to assist them in their selection and design of new dwellings that are appropriate to their needs.

Two of the submissions supported the intent of the Strategy (and the majority of proposed actions) but raised some specific issues/concerns. Housing Industry of Australia (“HIA”) and the Property Council of Australia did not support the following action:

- k) *Continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning.*

This action is merely seeking a comprehensive review of planning mechanisms at a state government level, which should include all possible options. Given the extent of the housing affordability problem it is considered appropriate to examine mandatory inclusionary zoning as an option. Any review of the planning framework should include consideration of all options to determine whether they are appropriate. It is therefore recommended that this action remain in the Strategy.

HIA supported the Strategy overall, but raised concerns about what they perceived to be proposed ‘mandatory measures’, including mandatory requirements for universally accessible dwellings. This assumption is incorrect and there are no proposed mandatory requirements for universally accessible dwellings. The Strategy is only seeking to ‘promote’ the Liveable Design Guidelines which is an excellent online resource for the community and anyone considering building an accessible dwelling. HIA have stated in their submission that they support the promotion of this program.

#### Consultation with Disability Services Commission

The Strategy identifies that people with disabilities are vulnerable to housing stress and homelessness, and often find it difficult to obtain affordable, appropriate housing. They are more likely to be renting than other households, and there is an identified shortage of universally accessible dwellings in both the private and public dwelling stock.

During the community consultation period the City liaised extensively with the Disability Services Commission to discuss the housing needs of people with disabilities to address this issue.

This discussion has drawn attention to some specific issues regarding housing and ancillary dwellings for people with disabilities. These issues and proposed actions are discussed below, and are proposed to be included in the Strategy.

### Special Purpose Dwellings

The Residential Design Codes (“R-Codes”) provides for three types of Special Purpose Dwellings:

- Ancillary Dwellings
- Single Bedroom Dwellings
- Aged and Dependent Dwellings

The objectives of these dwellings are to:

- Ensure that residential development is provided to accommodate people with or without special needs.
- To provide ancillary accommodation which is independent or semi-independent to residents of the single house.
- To ensure that dwellings for the aged and people with special needs can be provided within residential areas.
- To provide opportunities for affordable housing.

### Ancillary Dwellings

The R-Codes provides for the development of ancillary dwellings for people who live either independently or semi-independently of the residents of a single house, sharing some site facilities and services.

Approval for development pursuant to the R-Codes, including ancillary dwellings, can be obtained in two ways:

1. Compliance with the ‘Deemed to Comply’ provisions; or
2. Demonstrated consistency with the ‘Design Principles.

For ancillary dwellings the ‘Deemed to Comply’ requirements primarily require:

- the lot is greater than 450m<sup>2</sup>;
- maximum plot ratio of 70m<sup>2</sup> (amongst other things).

According to Disability Services Commission, in circumstances where the occupant of an ancillary dwelling has a disability, particularly a physical disability, a plot ratio of 70m<sup>2</sup> may not be adequate to meet their needs. Additional space requirements may arise to provide for:

- comfortable wheelchair access around the dwelling
- accommodating equipment, such as hoists and aids
- a larger bathroom for wheelchair accessibility and use of aids
- small additional separate bathroom facilities (shower, toilet, basin) for a carer to use

- 'breakout' space for a carer who needs to be present in the dwelling but at times needs their own space

Applicants have the option to meet the 'design principles', which do not include a maximum floor area. This provides the option to seek consideration under the 'Design Principles', providing justification for a larger ancillary dwelling.

Where development does not satisfy the 'deemed to comply' provisions, it must be demonstrated that the following 'Design Principle' is achieved:

*P1 Ancillary dwelling for people who live either independently or semi-dependently to the residents of the single house, sharing some site facilities and services and without compromising the amenity of surrounding properties.*

Such proposals are assessed on case by case basis. However, it is considered that providing further guidance, in the form of a local planning policy, would assist proponents in seeking consideration under the 'Design Principles'. Such a policy would also assist Council in determining the appropriateness of proposals.

The policy should stipulate the considerations and criteria for considering ancillary dwellings larger than 70m<sup>2</sup>, and specify the information that is required to be submitted to Council.

In this regard consideration should be given to allowing increases to the plot ratio of an ancillary dwelling (generally up to around 100m<sup>2</sup>) where the ancillary dwelling is built to the 'deemed-to-comply' standards set out in the R-Codes for 'Aged and Dependent' dwellings (i.e. universally accessible).

Requiring any ancillary dwelling that is greater than 70m<sup>2</sup> to be universally accessible will ensure that it is ideally suited to the needs of occupants with a disability. Importantly it will contribute to the stock of universally accessible dwellings which have been identified through the Strategy as being in very short supply. Such dwellings have the potential to provide affordable accessible dwellings to meet the needs of people with (or without) disabilities.

It is still very important that ancillary dwellings remain 'ancillary' to the main dwelling, and that they remain as smaller, affordable dwellings that generally accommodate only one or two people to ensure they do not compromise the amenity of the surrounding properties.

To summarise, the local planning policy should seek to:

- Ensure that ancillary dwellings are able to meet the needs of people with disabilities, whilst still being ancillary to the main dwelling.
- Strongly encourage the development of universally accessible ancillary dwellings.
- Ensure ancillary dwellings do not compromise the amenity of the surrounding properties and the streetscape.
- Provide further guidance on the 'design principles' set out in the R-Codes for ancillary dwellings, particularly in relation to plot ratio.

It is proposed that the Strategy include the above discussion and include a new action to prepare a new local planning policy accordingly. The local planning policy itself will explore these matters in further detail, and will be subject to Council consideration, community consultation, and discussion with Disability Services Commission.

#### Aged and Dependent Dwellings

The R-Codes provides for the development of aged and dependent dwellings which are built specifically for a person who is either over 55 years of age or has a disability.

For aged and dependent dwellings (and single bedroom dwellings) the R-Codes allow a variation to the minimum and average lot sizes for subdivision and development, reducing the required site area by one third. This means that there is the potential for more dwellings to be developed than would ordinarily be permitted, proving an incentive for universally accessible dwellings to be built.

The R-Codes set out the design requirements for these dwellings (ie. minimum doorway and corridor widths; level entry ways; bathroom requirements; accessible paths from car parking area and the street etc.

The draft Strategy has identified a strong need for adaptable/universally accessible dwellings.

The 'deemed to comply' requirements of the R-codes require that aged and dependent dwellings are occupied by at least one person who is aged and/or dependent. They require that a memorial be placed on the title of these dwellings (Section 70A notification), stipulating this requirement, which then applies in perpetuity to all subsequent occupiers.

It is considered that this requirement for a memorial to be placed on the title is a significant disincentive for developers to build aged and

dependent dwellings, as it potentially reduces the value of the dwelling. Even people who are aged and/or dependent may be reluctant to build or purchase such dwellings because they are aware that the restriction on occupancy may be a problem for resale or future rental potential. Consequently, with the exception of aged care providers, few aged and dependent dwellings are being built in the City of Cockburn.

It is considered that if the memorial restricting the occupancy of aged and dependent dwelling was not required this would make this incentive significantly more attractive, and would assist in increasing the stock of universally accessible dwellings. With this modification it is accepted that the dwelling could be used by anyone, however importantly the stock of universally accessible dwellings is being increased.

The memorial is not a requirement of the 'design principles'. It is therefore proposed that Local Planning Policy APD12 'Aged and Dependent Persons Dwelling' be amended to include further guidance on the application of the 'design principles' that does not require a memorial to be included on the title. It is considered important that if this were to be accepted that the dwelling size is restricted to a maximum of 100m<sup>2</sup> to ensure the dwelling is contributing both to the stock of smaller, affordable dwellings, whilst also contributing to the stock of universally accessible dwellings.

The 'deemed to comply' requirements of the R-codes state that 'Aged and Dependent Persons Dwellings' should be a minimum of five dwellings within any single development. However the R-Codes identify the option for local governments to adopt a local planning policy that varies this requirement to allow the incentive to be taken up for smaller developments.

It is therefore proposed that the City's Local Planning Policy APD12 'Aged and Dependent Persons Dwelling' also be amended to include removal of the minimum number of aged and dependent dwellings required in any single development.

For many landowners this will provide another option for the addition of a special purpose dwelling by potentially allowing an aged and dependent dwelling to be built behind their house (because of the allowance under the R-Codes to vary the site area), when they otherwise may not have a lot that is large enough to build a second dwelling.

It is considered that removing the minimum number of dwellings for aged and dependent dwellings in conjunction with the removal of the requirement for a memorial on the title would have the following benefits:

- See a significant increase in the uptake of the incentive for aged and dependent dwellings;
- Provide another housing option for many families (that can also be subdivided and placed on a separate title, which is not possible with ancillary dwellings);
- Increase the stock of universally accessible dwellings;
- Increase the stock of smaller, affordable dwellings.

### Single Bedroom Dwellings

The R-Codes provide for development of single bedroom dwellings to provide alternative and affordable housing options for singles or couples.

Through the implementation of Local Planning Policy APD56 'Single Bedroom Dwellings' since its adoption in 2008 staff have identified minor modifications that could be made to the policy to further encourage this type of accommodation within the City.

The 'deemed to comply' requirement limits the floor area of a single bedroom dwelling to 70m<sup>2</sup>. This is considered important, however the restriction on number of rooms capable of use as a bedroom is considered restrictive in today's housing market.

Given that the 'design principles' provides for housing suitable for one or two persons, the limitation of only one room capable for use as a bedroom is considered to prejudice the use of the dwelling for two people other than a couple. There may be many instances where a parent and child, two siblings, two friends/flatmates or other non-couples wish to reside together in a small dwelling without being restricted to one bedroom. It is envisaged that in the majority of instances however, the second room will typically be used as an ancillary or utility type space such as a study, a spare room, an activity room or a guest bedroom. This extra space is consistent with modern expectations and standards in contemporary housing and provides for greater flexibility generally regarding occupancy and use.

This flexibility is unlikely to cause any impact on the amenity of an area or adjoining neighbours; it simply provides a more flexible floor plan that will suit a greater range of smaller household, and it will make the incentive more attractive.

Such proposed changes to the policy were considered by Council in 2011 (Ordinary Meeting 12 May 2011). This included insertion of a new clause providing acceptance of an additional multi-purpose room capable of use as a second bedroom if required where the dwelling complied with the maximum floor area set out in the R-Codes, and

where it provided limited accommodation suitable for one or two persons.

At that time the R-Codes specified a maximum floor area of 60m<sup>2</sup> for single bedroom dwellings, and Council considered that this floor area was not sufficient to adequately accommodate a second bedroom.

The R-Codes have now increased the allowable floor area to 70m<sup>2</sup>, and it is considered that this additional 10m<sup>2</sup> provides adequate space to potentially accommodate a second additional room. It is therefore appropriate to consider modifications to APD56 'Single Bedroom Dwellings' to allow consideration of an additional room capable of use as a bedroom provided that the floor area does not exceed 70m<sup>2</sup>.

#### Summary of Proposed New Strategy Actions

In response to the issues discussed, the following new actions are recommended, and have been incorporated into the Draft Strategy (Section 5.1: Planning Mechanisms) included at Attachment 1.

- Modify Local Planning Policy APD12 'Aged and Dependent Persons' Dwellings' to allow less than five dwellings to be developed in any single development.
- Modify Local Planning Policy APD12 'Aged and Dependent Persons Dwellings' to allow flexibility for Section 70A Notifications not to be included on the certificate of title that require at least one occupant to be a disabled or physically dependent person or aged person (where the plot ratio does not exceed 100m<sup>2</sup>).
- Prepare a new Local Planning Policy for Ancillary Dwellings that sets out criteria for consideration under the 'Design Principles' of the Residential Design Codes (including plot ratio greater than 70m<sup>2</sup>) to ensure ancillary dwellings are able to meet the needs of people with disabilities.
- Modify Local Planning Policy APD56 'Single Bedroom Dwellings' to allow a second multi-purpose room that could be used as a bedroom where the total floor area of the dwelling does not exceed 70m<sup>2</sup>.
- Prepare a 'Housing Options' information kit setting out information and options for special purpose dwellings to assist people understand their housing options.

### Including Performance Indicators

Shelter WA recommended that performance indicators be included to assess whether objectives contained in this document are being achieved.

To summarise, the key actions of the Strategy focus on encouraging:

- Multiple dwellings which are in general smaller and more affordable than detached dwellings.
- Dwellings in mixed use development scenarios to increase the stock of smaller, affordable dwellings in accessible locations.
- Ancillary dwellings that have been found to be the most affordable rental accommodation in the City.
- Smaller dwellings including single bedroom dwellings that are in general more affordable dwellings for rent or purchase, and better match the needs of future households.
- Adaptable dwellings (including aged and dependent dwellings) to provide improved housing options for people with disabilities.

Therefore to measure the success of the Strategy in achieving more diverse and affordable housing it is proposed that a number of performance indicators be set out.

- Increase in the percentage of multiple dwellings approved
- Increase in the percentage of aged and dependent dwellings approved
- Increase in the percentage of single bedroom dwellings approved
- Increase in the percentage of ancillary dwellings approved
- Increase in the percentage of adaptable dwellings approved

### Corporate Strategic Plans

The Strategy will assist in the implementation of actions identified in a number of the City's Corporate Strategic Plans as outlined below.

#### *City of Cockburn Strategic Community Plan 2012-2022*

*Diversity of housing to respond to changing needs and expectations (1.1.4)*

*Ensure our strategic land use planning in the form of: the Local Planning Strategy, Town Planning Scheme, revitalisation strategies and structure plans, achieves a robust planning framework delivering adequate supply and diversity in housing choice.*

#### City of Cockburn Age Friendly City Strategic Plan

One of the key outcomes of the City's Age friendly City Strategic Plan is that the ageing population in the City of Cockburn has access to affordable suitable housing options that allow them to age safely and be socially supported within the community to which they belong.

#### City of Cockburn Youth Services Strategic Plan

The City's Youth Services Strategic Plan identifies that there is insufficient crisis and transitional housing options for young people in Cockburn with Anglicare operating the only service.

#### Conclusion

It is recommended that Council adopt the Strategy for final approval as found at Attachment 1 with the modifications discussed in this report.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Diversity of housing to respond to changing needs and expectations.

#### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.
- Communities that are connected, inclusive and promote intergenerational opportunities.

#### **Budget/Financial Implications**

The project is being funded from municipal funds. Ongoing actions will be funded from municipal funds, and none of the proposed actions are considered to have significant financial impacts.

#### **Legal Implications**

N/A.

## Community Consultation

The Draft Strategy was advertised for a period of 30 days, and all submissions are outlined and addressed at Attachment 2.

## Attachment(s)

1. Draft Housing Affordability and Diversity Strategy (proposed modifications incorporated)
2. Schedule of Submissions

## Advice to Proponent(s)/Submissioners

Submissioners have been advised that this matter is to be considered at the 9 October 2014 Ordinary Meeting of Council.

## Implications of Section 3.18(3) Local Government Act, 1995

Nil.

### 14.3 (OCM 9/10/2014) - LOCAL STRUCTURE PLAN FOR LOT 6 (NO. 210) HAMMOND ROAD, SUCCESS - OWNER: CHRISTINE JULIA ARMSON - APPLICANT: TPG TOWN PLANNING URBAN DESIGN AND HERITAGE ON BEHALF OF NATALIE JARDIM (PROSPECTIVE PURCHASER) (110/108) (L SANTORIELLO) (ATTACH)

#### RECOMMENDATION

That Council:

- (1) in pursuance of Clause 6.2.9.1 (a) of the City of Cockburn Town Planning Scheme No. 3 ("Scheme") adopt the proposed Local Structure Plan subject to the following modifications:
  1. Insert an additional section within Part 1 Section 5.0 "*Land Use*" (all following sections within Part 1 are to be renumbered accordingly) and include the title "*Residential Density*" with the following text: "*Residential densities applicable to the Structure Plan area shall be those residential densities shown on the Structure Plan Map*".
  2. Insert an additional subsection within Part 1 titled "*Designated Bushfire Prone Areas – Construction Standards*" following 5.2 'Detailed Area Plans' and insert the following additional text: "*This Structure Plan is supported by a Bushfire Management Plan (BMP) located in Appendix 4 "Bushfire Prone Planning - Bushfire Management Plan Lot 6 (210) Hammond Road, Success Project number 14110". Any land falling within 100 metres of a bushfire hazard identified*

*in the BMP is designated as a Bushfire Prone Area for the purpose of the Building Code of Australia”.*

3. Replace the text within Part 1 section 5.1 (2) with the following: *“Building setbacks and construction standards required to achieve a Bushfire Attack Level (BAL) 29 or lower in accordance with Australian Standards (AS3959-2009): Construction of buildings in bushfire prone areas.”*
4. Modify the text within Part 1 section 6.2 (2) as follows: *“In respect of applications for the subdivision of land the Council shall recommend to the Western Australian Planning Commission that condition(s) be imposed requiring the implementation of the Bushfire Management Plan (Appendix 4) which has been prepared as part of this Local Structure Plan.”*
5. Replace the text within Part 1 section 6.2 (3) with the following: *“No Class 1, 2, 3 or 10a structures (as defined by the Building Code of Australia) shall be approved or constructed within the area identified as ‘No Building Zone’ on the Local Structure Plan Map including minor projections and structures appurtenant to dwellings such as carports, garages, verandas, patios and outbuildings. But does not include barriers such as driveways, lawns or pathways as outlined in Appendix 4 Bushfire Management Plan. Class 1, 2, 3 or 10a structures are to be wholly contained in the BAL 29, 19 and 12.5 areas as identified on figure 9 of Appendix 4 BMP. No Class 1, 2, 3 or 10a structures are permitted within the BAL 40 or FZ areas of figure 9”.*
6. Modify Appendix 4 (BMP) section 7.1 by deleting this text *‘Department of Parks and Wildlife Reserve are to maintain parkland to the north and west to comply with Building Protection Zone requirements.’*
7. Modify the Local Structure Plan Map (Plan 1), Figure 2, Figure 3 and Figure 4 by reducing the size of the ‘Structure Plan Area’ by excluding the ‘Other Regional Road’ (Hammond Road – In Blue)”.
8. Replace the word “access” with “use” in Part 2 section 5.4 paragraph 3 and replace the word “an” with “and” under dot point 4.
9. Appendix 3 – Engineering Servicing Report shall be updated to address the ‘outstanding issues’ identified within the Department of Water (DoW) letter received during the Local

Structure Plan advertising period dated 27 August 2014 (ref:RF3769-04) to the satisfaction of the City of Cockburn.

10. Modify Part 1 section 6.2 (1) by inserting “(c) *A mosquito management plan*”.
  11. Modify Part 1 section 5.1 by including “3. *A heightened risk of mosquito born disease in the area*”.
- (2) endorse the Schedule of Submissions prepared in respect of the proposed Local Structure Plan for Lot 6 (No. 210) Hammond Road, Success (Attachment 4);
  - (3) in pursuance of Clause 6.2.10.1 of the Scheme forward the Local Structure Plan to the Western Australian Planning Commission within 7 days of Councils adoption; and
  - (4) advise the proponent of Council’s decision.

## COUNCIL DECISION

### Background

The proposed Local Structure Plan (“LSP”) was formally lodged with the City of Cockburn on 14 August 2014.

In accordance with Clause 6.2.7.2 of City of Cockburn Town Planning Scheme No. 3 (“Scheme”), the Structure Plan was referred to the Western Australian Planning Commission (“WAPC”) for comment. The LSP was then advertised for public comment for a period of 21 days as required by the Scheme pursuant to Clause 6.2.8.

The purpose of this report is to consider for adoption the proposed Local Structure Plan for Lot 6 (No.210) Hammond Road, Success. A copy of the LSP map is included as Attachment 3 of this report.

### Submission

The proposed Local Structure Plan was prepared by TPG Town Planning, Urban Design and Heritage on behalf of Natalie Jardim.

## Report

### Planning Background

The subject land is roughly 2.02 hectares in area and generally bounded by Hammond Road to the east. To the north and west of the subject land is an area of Beeliar Regional Park, which comprises Branch Circus Wetland (Conservation Category Wetland (“CCW”)) and forms part of Bush Forever Site No. 391.

The subject site is zoned ‘Urban’ under the Metropolitan Region Scheme (“MRS”) and ‘Development’ under the City of Cockburn Town Planning Scheme No. 3 (“Scheme”). The subject land is also located within Development Area 13 (“DA 13”), Development Contribution Area No. 1 (“DCA 1”) and Development Contribution Area No. 13 (“DCA 13”).

Pursuant to Clause 6.2.4 and Schedule 11 of the Scheme, a structure plan is required to be prepared and adopted to guide future subdivision and development.

### Wetland Buffer

The subject land (Lot 6) comprises part of a Conservation Category Wetland (CCW) known as Branch Circus Wetland, as identified in the Department’s Geomorphic Wetlands Swan Coastal Plain dataset. CCWs are wetlands that support a high level of ecological attributes and functions, and are the highest priority for protection.

In accordance with the Environmental Protection Authority’s (EPA’s) *Guidance Statement No. 33 – Environmental Guidance for Planning and Development* (EPA 2008), the EPA requires a 50 metre buffer to protect the ecological functionality of CCWs and to protect wetlands from proposed land use change.

The applicant met with the City of Cockburn officers and the officers of the Department of Parks and Wildlife (DPaW) in early September to determine the extent of the buffer for this site. It was agreed that a reduced buffer, in the context of this site, was considered appropriate (refer to Attachment 3) provided the buffer is revegetated with appropriate native vegetation species of local provenance.

The LPS report has since been revised (September 2014) to mandate the requirement of a ‘*Wetland rehabilitation and landscaping strategy*’ as a condition of subdivision or development approval.

Specifically, the Wetland rehabilitation and landscaping strategy will address issues such as protection of remnant vegetation, access

provisions and strategies to control weeds and to revegetate the buffer to the CCW.

### Bushfire Prone Areas

The native vegetation on Lot 6 and the land to the north are classified as a 'bushfire hazard' within the accompanying Bushfire Management Plan (Appendix 4).

The BMP was prepared in accordance with the Western Australian Planning Commissions' draft *Planning for Bushfire Management Guidelines (May 2014)* and draft *State Planning Policy 3.7 'Planning for Bushfire Risk Management'*.

The revised guidelines specify that *'proposals within extreme bushfire hazard areas or areas where a BAL-40 (Bushfire Attack Level) or BAL-FZ assessment would exist are not to be supported.'*

Objective 4 of the draft guidelines specifies that planning proposals are to *'achieve a responsible and balanced approach between bushfire risk management and management measures, and landscape amenity and biodiversity conservation objectives.'*

Figure 9 *'Proposed Subdivision and BAL determination'* of the BMP provides the indicative Bushfire Attack Levels based on the classification of the native vegetation and the slope of the land beneath the vegetation. This figure includes BAL-FZ and BAL-40.

In order to ensure compliance with the draft Bushfire Risk Management Guidelines the LSP Map has been amended to include a 'No Building Zone' reflective of the BAL-FZ and BAL-40 areas.

The 'No Building Zone' prohibits the approval of any part of a residential dwelling within the areas which require BAL-40 and above construction standards.

### Conclusion

The subject land is described as Lot 6 (No. 210) Hammond Road, Success. The site is approximately 2 hectares in area and contains a Conservation Category Wetland associated with the Beeliar Regional Park.

The Local Structure Plan (LSP) will facilitate future subdivision of the wetland to be transferred from private landholdings to the Crown. In addition the City of Cockburn will secure an area of public open space adjacent to the wetland which will be serviced by a new local road.

The Bushfire Management Plan approved as part of this proposal will mandate appropriate higher building standards. The inclusion of a 'no building zone' over portions of the LSP Map will allow for increased setback to the wetland for the protection of the native vegetation. Additionally residential development will be restricted to a maximum Bushfire Attack Level of 29 which is consistent with state planning policy.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.

#### **Moving Around**

- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

The required fee was calculated on receipt of the proposed Structure Plan and has been paid by the proponent. There are no other direct financial implications associated with the Proposed LSP.

### **Legal Implications**

Clause 6.2.9.1 of the Scheme requires Council to make a decision on the application within 60 days from the end of the advertising period. The advertising period formally concluded on the 4<sup>th</sup> of September 2014.

### **Community Consultation**

In pursuance of Clause 6.2.8 of the City's Scheme, public consultation was undertaken for a minimum period of 21 days. The advertising period formally commenced on 14 August 2014 and concluded on 4 September 2014.

Advertising included letters to adjoining and affected landowners, within and surrounding the LSP area as well as various Government Agencies and service providers.

Analysis of the submissions has been undertaken and included in the attached Schedule of Submissions (Attachment 4). In total the City received a total of 6 submissions all from Government agencies and service providers. No submissions from the community were received during the advertising period.

#### **Attachment(s)**

1. Context Plan
2. Site, Aerial and Conservation Assets Plan
3. Proposed LSP Map (revised)
4. Schedule of Submissions

#### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

#### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

#### **14.4 (OCM 9/10/2014) - DRAFT MAJOR DEVELOPMENT PLAN FOR JANDAKOT AIRPORT (WAREHOUSE AND OFFICE DEVELOPMENT COMPRISING KMART DISTRIBUTION CENTRE) - LOCATION: JANDAKOT AIRPORT - APPLICANT: JANDAKOT AIRPORT HOLDINGS IN ASSOCIATION WITH KMART AND URBIS (110/01) (A TROSIC) (ATTACH)**

#### **RECOMMENDATION**

That Council: subject to there being no agreement reached on the necessary infrastructure upgrades by 31 October 2014, Council:

- (1) provide written comment on Preliminary Draft Major Development Plan for the Kmart Distribution Centre, recommending that it be refused for the following reasons:
  1. The Major Development Plan does not comply with the 2009 Jandakot Airport Master Plan.
  2. Jandakot Airport Holdings has not developed the road network in accordance with their 2009 Jandakot Airport Master Plan submission or to the City's requirements.
  3. The road network does not adequately cater for the traffic generation from the broader development as was identified by the 2009 Jandakot Airport Master Plan and

- creating a further distribution centre in this location will put additional pressure on existing linkages which cannot cater for the traffic growth.
4. The proposal is inappropriately located, being on the periphery of the site and therefore having potential adverse impacts by way of noise on the amenity of nearby residences.
  5. The proposal orientates itself from a temporary access, which has not been endorsed as a permanent access by the City.
- (2) write separately to the relevant Federal Minister (Minister of Infrastructure and Regional Development), advising them of the current issues associated with the lack of infrastructure coordination and investment which is the responsibility of Jandakot Airport Holdings in the prevailing road network

## COUNCIL DECISION

### Background

The City of Cockburn has been invited to provide comment on the proposed Major Development Plan ("MDP") for a warehouse and office development at Jandakot Airport. This will be a Kmart Distribution Centre, servicing the entire Kmart store network of Western Australia.

This major development represents yet another example of the significant development happening at Jandakot Airport, under the auspices of the 2009 Jandakot Airport Masterplan ('2009 Masterplan'). It is also cognizant of the Draft 2014 Jandakot Airport Masterplan ('2014 Masterplan'), which continues a focus on major development happening around the airport.

As part of the City's opportunity to provide comment, it is important to consider whether this proposed MDP appropriately addresses the requirements set out under Section 91 of the *Airports Act 1996*, as well as the requirements of the 2009 Master Plan.

It is recommended that Council not support the Proposed MDP.

## Submission

The proposed MDP has been submitted by Jandakot Airport Holdings, ('JAH') in association with Kmart and Urbis.

## Report

### Legislative Background

The *Airports Act 1996* and associated Regulations represents Commonwealth legislation. This has a responsibility for the regulation of ownership, management and conduct of major Australian airports. Part 5 of the Act sets out the requirements for land use, planning and building controls.

In accordance with the Act, all major airport development requires a Major Development Plan to be prepared and advertised. The MDP is then submitted to the responsible Federal Minister (Minister of Infrastructure and Regional Development) for assessment. The Minister has the power to approve or to refuse to approve the MDP.

Section 89 of the Act sets out all those activities defined as major airport development. The proposed Kmart Distribution Centre which is the subject of this Proposed MDP is determined to be major airport development as per Section 89(1)(e) of the Act:

*Constructing a new building, where:*

- (i) The building is not wholly or principally for use as a passenger terminal; and*
- (ii) The cost of construction exceeds \$20 million or such higher amount as is prescribed;”*

Based on the estimated construction cost in order of \$40 million, the proposed development is considered a major airport development thereby triggering the need for a MDP.

### MDP Components

The proposed development represents a significant proposal, on 9.7366ha of land located in the northwest corner of the airport nearby Berrigan Drive and rural residences. It is located on a piece of land formed by the corner of Berrigan Drive and Spartan Street, the latter street being a temporary street that was not intended to be a focus for major development to access from.

The development comprises components including:

- A general goods warehouse with a floorspace area of 3.8035ha, with goods-in from the northeast and south-east, and goods out

from the north-west, and bulk floor and racked storage with mechanical sorting in the centre;

- 20 covered receiving bays (south-east side of the building);
- 46 covered container loading bays lanes (north-east side of the building);
- 35 recessed despatch docks (north-west side of the building);
- Outdoor container area;
- A perishable goods coolroom;
- Maintenance room and battery charge room;
- Administration areas, being receiving office, despatch office and main office;
- Staff and visitor car parking;
- Pallet yard;
- Sprinkler tank and pump room.

It is planned to support the entire Kmart network in WA, which currently comprises 23 retail outlets with capacity to cater for planned growth of the store network (currently 5 additional planned). Following the completion of the construction period and once fully operational, the Distribution Centre is estimated to employ up to 120 staff, with a maximum of 80 staff being rostered on at any one time.

The use will operate 24 hours a day. This is important to consider impacts on the amenity of nearby residents within the Resource zone along Berrigan Drive. Amenity impacts are particularly important to consider in respect of the noise of the use, as well as noise associated with freight and general vehicle traffic for the use. It is noted that all truck movements are proposed via a single consolidated access point onto Spartan Street, at the southeast corner of the site.

### Key issues

There are key issues associated with this MDP that are considered to pose an unacceptable level of impact on the surrounding locality. These issues are associated with traffic and noise.

### Traffic

The MDP suggests that the development proposal does not need to propose modifications to the existing Jandakot Airport Mixed Business Precinct road network on the basis that the Kmart distribution centre would not have significant impact on the operation of Jandakot Airport's road network or the immediately surrounding local road network. The City disagrees with this assumption, and contests that the local road network suffers from a lack of investment in the road network that Jandakot Airport Holdings were responsible for as part of the implementation of the 2009 Master Plan.

The transport related impacts of the airport are a particular concern for the City and road users because of the increasing volume of traffic that is being generated by the airport and the fact that the airport still only has a single primary access point via Karel Avenue. A secondary left-in/left out only connection of Spartan Street to Berrigan Drive has been granted as a temporary access until Orion Road connects to the Berrigan Drive/Jandakot Road intersection. The City has also acceded to allowing a signalman to provide traffic control during the PM peak period at the intersection Karel /Berrigan (north) to improve accessibility for JAH until these works are completed. These temporary solutions cannot be sustained in the longer term and provide clear demonstration of the need for investment in the external network as was identified by the 2009 Masterplan.

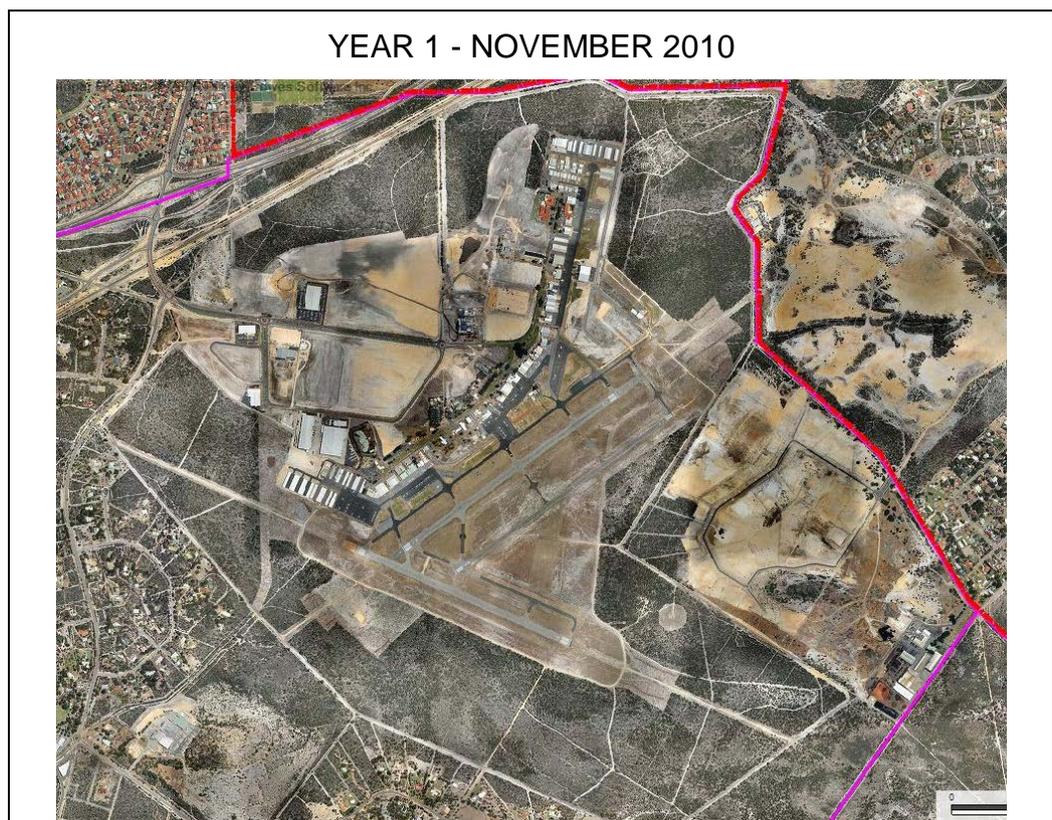
In Section 9.0 Road Access System of the 2009 Master plan a suite of road improvements were presented to address the impacts of the traffic generated by the airport developments onto the surrounding road network.

Those road improvements included:

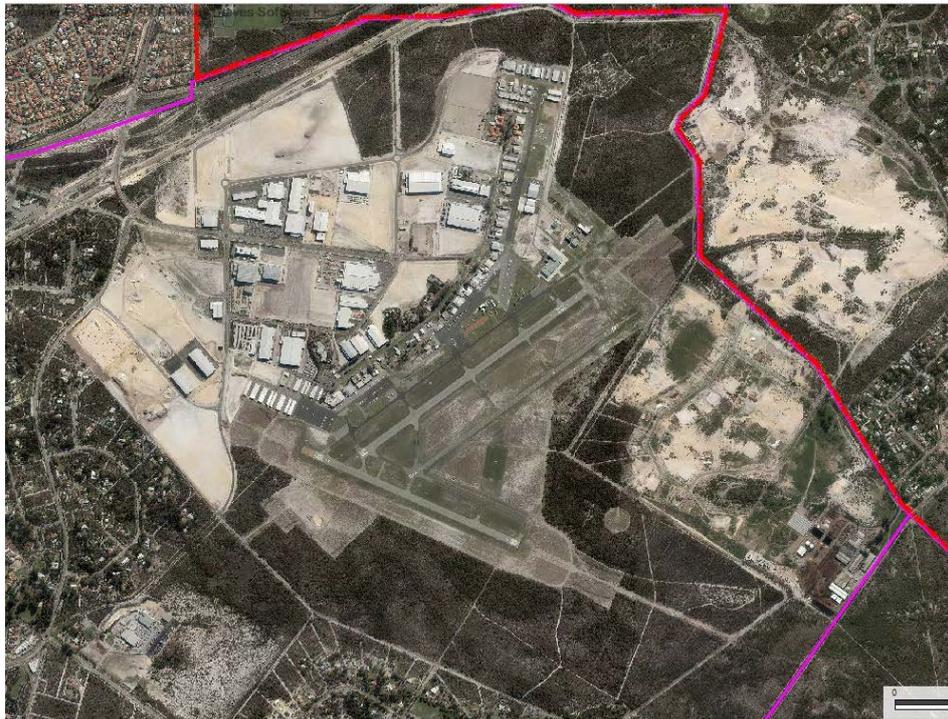
1. Berrigan Drive (Kwinana Freeway to Jandakot Road)
  - Widen existing two-lane divided road to four-lanes divided
2. Berrigan Drive North (Karel Avenue to Jandakot Road)
  - *Upgrade* Berrigan Drive/Karel Avenue intersection from a roundabout to traffic signal control.
  - Erect *Local* Traffic Only signs at either end of this road section.
  - If *necessary*, supplement the above with traffic calming treatments.
3. East Link (Airport to Ranford Road)
  - Construct a new 2-lane arterial road with traffic signal control at Ranford Road.
4. Karel Avenue (Farrington Road to Roe Highway)
  - Upgrade of approach and departure lanes to Roe Highway from two-lane divided road to four-lanes divided.
5. Karel Avenue (Roe Highway to Berrigan Drive)
  - Widen existing two-lane divided road to four-lanes divided.
6. South Link (Extension of Orion Road to Berrigan Drive/Dean Road/Jandakot Road intersection)
  - Construct a new arterial road by extending Orion Road to Berrigan Drive/Jandakot Road and installing suitable traffic control (multi-lane roundabout or traffic signals).

- This includes realigning a section of Berrigan Drive just north of Jandakot Road so that a T-intersection is formed, with the South Link being the continuing road and Berrigan Drive the terminating road.

Since the decision in 2009 the City has been working with JAH to progress the implementation of this suite of road works in accordance with Councils recommendation. Over the years it has been presented with varying traffic modelling data and road design options for intersections prepared by a number of consultants. No significant progress has been made on any of the planned road improvements outside the airport boundary to date. Whilst the City has been prepared to consider the staging of the road network upgrades to be consistent with the rate of development, it has been continually frustrated by the lack of progress on reaching agreement for these necessary works. However, significant development of land for non-aviation commercial use has progressed in that time as can be seen by comparing the aerial photographs.



## YEAR 5 TO DATE



The mix of development that has occurred over the 5 years of the 2009 Master Plan has comprised:

- 8.4ha of warehouse;
- 2.1ha of workshop;
- 1.1ha of office;
- 0.5ha of retail.

In normal circumstances, under development that is regulated in accordance with the City's Town Planning Scheme No. 3, the City would not contemplate granting planning approval for a mix of commercial, retail and industrial uses exceeding 12ha in size with only a limited single access point. The City would have either required all infrastructure upgrades to be delivered upfront, or staged these to coordinate with the rate of development taking place. This highlights the complexity of dealing with a developer on a federal government lease.

The City has been very patient in its dealings with JAH and extended a generous level of goodwill whilst waiting on the implementation of the planned road improvements, by permitting them to:

- operate temporary (since August 2011) traffic management at the Karel Avenue/Berrigan Drive intersection, in weekday PM peak hours to minimise the delays to exiting airport traffic, and
- construct a temporary left-in/left-out access to Berrigan Drive (at Spartan Road). This access was to be available until the south link

road was provided and its future ultimately was to be considered as part of the 2014 Masterplan.

Whilst the City has provided support for the temporary left in / left out access at Spartan Street, it is clear that JAH intend to pursue retention of this link in perpetuity. The 2014 Masterplan identifies this link as forming part of the broader road network supporting the development and the Kmart distribution centre gains clear access from the link. It is important that the City be clear, this link is approved as a temporary link only until the south link road is completed. That approval however can be revisited at any time and can also be withdrawn at Councils discretion.

The issues to do with the access arrangements at Jandakot Airport have been further explored in greater detail later in this OCM agenda at Item 16.3. That item clearly articulates the necessary and priority scope of works which must be delivered to provide for the further development of the Jandakot Airport precinct and also seeks to facilitate a formal agreement which will give clarity and commitment to delivery of these works, which has been lacking to date. These 2 agenda items are linked as access to the development is critical to consideration of this MDP.

Until the south link road and other associated works are completed, the City should not support this development as the road network cannot support major industrial, commercial and retail land use which generates significant consumer, employment and freight traffic demand. If agreement cannot be reached on these necessary road upgrades as recommended at item 16.3, Council must consider the withdrawal of support for the Spartan Street access from Berrigan Drive meaning that this distribution centre (should it be approved) will need to gain access from the internal road network provided by JAH. That outcome will clearly create additional pressure on the internal network.

### Noise

The second issue of concern is the way in which noise impacts have not been examined as part the proposed MDP. The documentation states that:

*“goods from local deliveries will be via side load small vehicle, semi-trailers and B-doubles. Containers on semi-trailers, B-doubles and super-B-double vehicles will be used for the loading of goods from the covered area and rear load semi-trailers and B-doubles will be used for the transport operations at despatch. Goods-in operations are typically limited to 6:00am – 4:00pm, whilst goods-out operations occur across a longer period of day, generally between 5:30am – 8:00pm.”*

It also states:

*“The closest sensitive land use to the subject site is rural-residential development in the suburb of Jandakot to the west of the site with the nearest dwelling being less than 200 metres from the proposed Kmart Distribution Centre building.”*

As part of noise considerations, the documentation also indicates that:

*“in order to create a level site ready for development, considerable earthworks have taken place between the proposed Distribution Centre and Berrigan Drive. The level difference between the established ground level of the site and Berrigan Drive and residential development further to west creates what is termed a natural acoustic and visual barrier.”*

The document then finally contends that this level difference, the setback distance to Berrigan Drive and existing and proposed landscaping result will ensure that there will be no undue impact on the amenity of the surrounding residential development (including from flashing lights and reversing beepers).

This contention is not acceptable to the City. The City would not accept anything less than a detailed noise assessment as part of a proposed development to ensure that it will not have noise impacts on surrounding residences. The lack of a detailed noise assessment as part of this proposed MDP means that it can be concluded that the proposal will not have any undue impact on the surrounding residential/rural amenity. Such wording is also ambiguous, for example what is representative of due versus undue impact?

Where a similar development type is proposed elsewhere within the City of Cockburn, it would be necessary for the proponent to demonstrate that potential emissions would not cause adverse impacts to the residential/rural amenity of other nearby properties. The necessity for a detailed assessment of a proposal is based by the City on the separation distances identified within the Environmental Protection Authority's Guidance Statement No 3 "Separation Distances between Industrial and Sensitive Land Uses", which recommends buffer distances between the boundary of a proposed industrial land use and the boundary of the sensitive land use. The present boundary to boundary set back of the proposed MDP from the nearest sensitive land use is approximately 40m.

A similar land use identified within the EPA Guidance Statement is 'transport vehicles depot', for which the EPA Guidance Statement identifies a 200m buffer distance from sensitive land uses, with the potential for gaseous, noise, dust and odour impacts. There are five

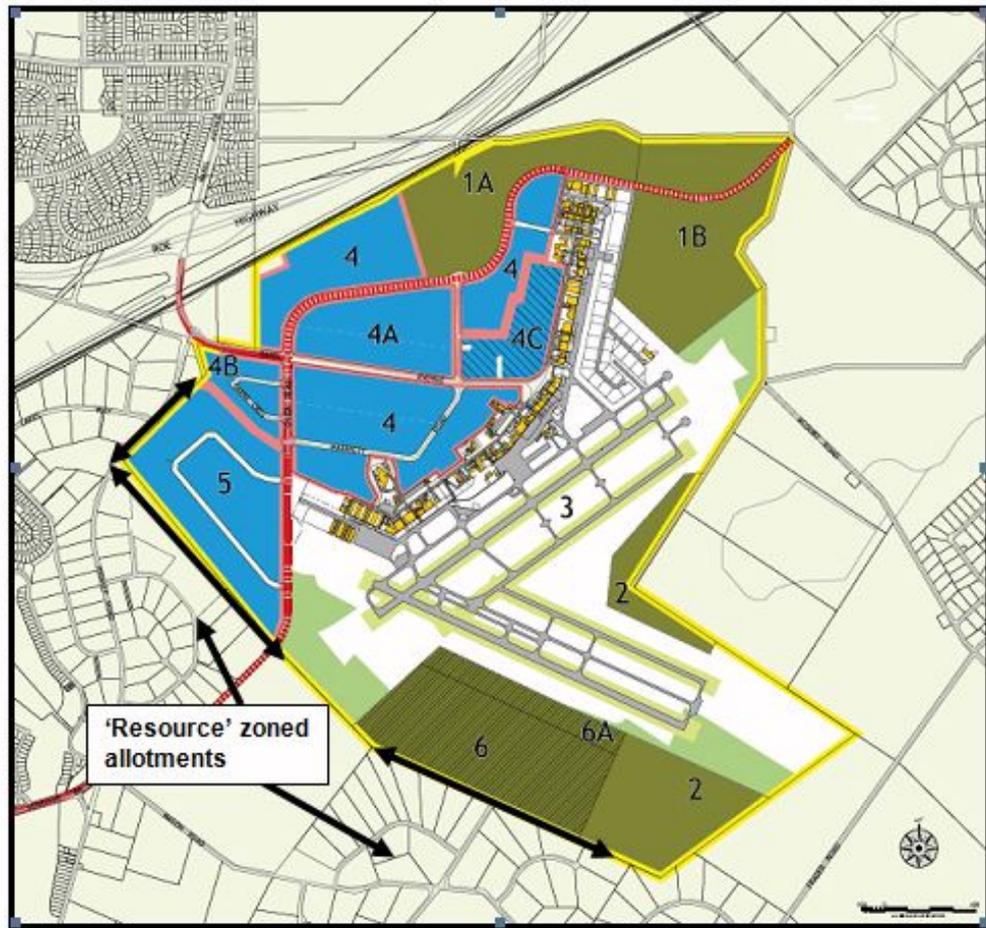
sensitive land uses within this 200m set back from the Western boundary of the Proposed MDP. As a result, a detailed noise assessment should be provided by the proponent to demonstrate that all noise emissions would comply with the assigned noise levels imposed under the Environmental Protection (Noise) Regulations 1997, for all hours of operation.

There would be additional amenity impacts associated with the resultant increase in heavy vehicle traffic on Berrigan Drive and Spartan Street caused by this development. Heavy vehicle traffic occurring at night and in the early morning would be particularly intrusive as a result of the reduced background noise levels within residential and rural areas at these times.

The projected increase in the volume of traffic on Berrigan Drive would also result in an increase in the 'assigned noise levels' (the permitted levels of noise) for the nearby noise sensitive properties for all noise emissions, as the assigned noise levels are related to the volume of traffic on nearby roads.

In addition, where it is necessary for the road infrastructure to be upgraded as a result of this Proposed MDP, there are noise and development implications. If Berrigan Drive (and Spartan Street) require upgrade to support the increased level of heavy vehicle traffic to Jandakot Airport, any upgrade of the road infrastructure has implications for the City under State Planning Policy 5.4, entitled "Road and Rail Transport Noise and Freight Considerations in Land Use Planning" (SPP 5.4). Under SPP 5.4, urban roads carrying more than 20,000 vehicles per day (as well as urban primary distributor roads and primary freight roads), are required to comply with noise criteria, identify measures to reduce noise impacts on existing noise sensitive development and may be required to implement noise attenuation measures to reduce noise impacts. Presently, the road reserve for Berrigan Drive is not of sufficient size to support an increase in the level of road infrastructure.

The above concerns do not represent new information for the applicant. At the time of considering the 2009 Master Plan, important concerns were raised by the City of Cockburn in respect of how the interface between higher intensity commercial development would be managed with the lower intensity 'Resource' zoned lots which adjoin the airport site. This is diagrammatically depicted following:



Issues raised at the time of the 2009 Master Plan included whether the resulting commercial development may have external impacts.

Given the nature of the airport as a specialised centre dealing with logistical warehousing and storage functions, it is reasonable to suggest that some land uses by their nature will have some external impacts. This could include potential noise, traffic and light spill impacts. Given also that the hours of operation for such uses may extend into the night, such impacts may have a higher degree of magnitude than if just restricted to daytime hours.

It was therefore important to ensure that the placement of higher impact commercial uses was not on the boundary, close to sensitive residential development, but alternatively placed internally on the airport site. For example, the placement of the recently approved ALDI Distribution Centre in the north of the airport precinct, close to Roe Hwy, is remote from the nearest residences and is provided further separation by virtue of the freight line and Roe Hwy. Also access to this is focussed from the Karel Avenue access point, meaning trucks do not need to pass the residences along Berrigan Drive in the Resource zone. In contrast, the Kmart location does not have any of these advantages in terms of physical separation from residences. It would

appear more appropriate that by centrally locating uses which are intended to operate 24 hrs per day, lower intensity uses could then buffer those higher intensity uses as the transition occurs out to the boundary of the airport site.

Given the Kmart Distribution Centre is proposed to operate 24 hours per day, and will comprise significant freight handling activities, it is arguable that its location on the periphery of the site nearby residences is inappropriate.

### Conclusion

Following analysis of the Preliminary Draft MDP, it is recommended that Council refuse supporting to proposal on the basis that:

1. It does not comply with the 2009 Master Plan;
2. The 2009 Master Plan has non-compliances associated with the lack of infrastructure investment identified in the Master Plan;
3. The proposal will exceed the traffic carrying capacity of the road network;
4. The proposal will exacerbate the already unsatisfactory traffic management levels in the area;
5. The proposal is inappropriately located, being on the periphery of the site and therefore having impacts by way of noise on the amenity of nearby residences; and
6. The proposal orientates itself from a temporary access, which is proposed to be closed in the future by the City.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- A safe and efficient transport system.
- A defined freight transport network.

### **Budget/Financial Implications**

There are no specific financial implications associated with this report.

## Legal Implications

*Airports Act 1996*

## Community Consultation

There is no specific community consultation being undertaken by the City in this respect. As part of requirements of the Act, the Proposed MDP is being advertised for public comment until 31 October 2014.

## Attachment(s)

1. Location Plan
2. Site Plan
3. Proposed Development Plans

## Advice to Proponent(s)/Submissioners

N/A. This is a request for comment from the City of Cockburn.

## Implications of Section 3.18(3) Local Government Act, 1995

Nil.

### 14.5 (OCM 9/10/2014) - PURCHASE OF LOT 702 (NO. 7) BELLIER PLACE HAMILTON HILL - OWNER DEPARTMENT OF HOUSING - APPLICANT CITY OF COCKBURN (2201061) (LGATT) (ATTACH)

#### RECOMMENDATION

That Council:

- (1) consent to the purchase of Lot 702 (No. 7) Bellier Place Hamilton Hill for the purchase price of \$800,000.00 ex-GST; and
- (2) amend the 2014/15 adopted municipal budget by adding capital expenditure of \$800,000 (ex-GST) for the purchase of the land against a new CW project – Purchase of Lot 702 Bellier Place, Hamilton Hill and funding this via transfer from the Land Development and Investment Fund Reserve.

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

#### COUNCIL DECISION

## **Background**

As part of the Phoenix Rise Master Plan, Lot 65 Erpingham Road is identified for development. In order to maximise the development potential of this land, it is proposed to purchase Lot 702 (No. 7) Bellier Place, Hamilton Hill from the Department of Housing. This purchase provides a direct frontage to Bellier Place, to enable the entire redevelopment project to be realised for sale in accordance with the City's Land Management Strategy.

## **Submission**

### **Report**

The City's officers have been endeavouring to progress the development of Lot 65 Erpingham Road, Hamilton Hill for some time. Lot 65 is currently land locked and the City has approached the Department of Housing (DOH) and BP Refinery Australia (BP) to facilitate the creation of access to the land.

For some time the City has been negotiating with BP for a road access across the BP pipeline to provide access/egress for the City's rubbish trucks and future residents of the development of Lot 65.

BP has recently consented to the construction of the road access across their pipeline and the necessary documents are being prepared to facilitate the future construction of the access.

This proposal now seeks to provide legal access to Lot 65, by purchasing the house and land located at Lot 702 (No. 7) Bellier Place Hamilton Hill and amalgamating it with the City's Lot 65. Lot 702 is owned by the DOH and they have now relocated their tenant to another more suitable property and Lot 702 is available for the City to purchase.

The purchase of Lot 702 will provide legal street access to Lot 65 along with the benefit of all services being accessible from this Lot 702. The connection of the services through Lot 702 will be straight forward and cost effective for the development. The previous plan involving the PAW access required the City to provide all services to the lot via crossing the BP Pipeline. This would have involved more costs to the City and risk to the BP Refinery Pipeline.

The City has received a written valuation from a Licensed Valuer which states the subject property would command a market value range from \$875,000 to \$925,000 with the sum of \$900,000 considered fair and reasonable. (A copy of the valuation is attached to the Agenda). The City also sought advice from a local real estate

agent (First National Real Estate Davies) to which he replied 'based on our findings we suggest a realistic market estimate of \$800,000 as a 5 unit site with an R40 zoning. We also confirm the sale of 6 Helena Place in March this year for the sum of \$505,000 set on a 332 sq.m. lot'. The DOH have provided in principal agreement that they will accept an offer from the City for \$800,000.00 ex-GST.

This report seeks a Council resolution to delegate authority to the CEO to negotiate the purchase the DOH house and land for \$800,000.00 ex-GST.

## **Strategic Plan/Policy Implications**

### **Growing City**

- Development that is soundly balanced between new and existing areas.
- Diversity of housing to respond to changing needs and expectations.

### **Leading & Listening**

- Manage our financial and infrastructure assets to provide a sustainable future.

## **Budget/Financial Implications**

Amend the 2014/15 adopted municipal budget by adding capital expenditure of \$800,000 (ex GST) for the purchase of the land against a new CW project – Purchase of Lot 702 Bellier Pl, Hamilton Hill and funding this via transfer from the Land Development & Investment Fund Reserve.

## **Legal Implications**

N/A

## **Community Consultation**

Past consultation has already been undertaken with the adjoining owners regarding the proposed changes to Lot 65 Erpingham Road, which have now been formally gazetted. The City has also previously sent a letter dated 31 August 2012 to surrounding landowners advising them of the timeline and planning of the redevelopment of Lot 65 Erpingham Road.

**Attachment(s)**

1. Valuation letter from WBP Property Group (WA)
2. Location Sketch

**Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**14.6 (OCM 9/10/2014) - SALE OF LAND - LOT 432 (NO. 16) RODD PLACE, HAMILTON HILL (2201061) (L GATT) (ATTACH)**

**RECOMMENDATION**

That Council

- (1) accepts the offer from PRDnationwide Project Marketing (WA) for a consideration of \$2,400,000 (ex GST) subject to the statutory advertising required by Section 3.58 of the *Local Government Act 1995*; and
- (2) amend the 2014/15 adopted municipal budget by adding capital income of \$2,400,000 (ex-GST) from the sale proceeds against a new CW project – Sale Lot 432 Rodd Place, Hamilton Hill and transferring these into the Land Development and Investment Fund Reserve.

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

**COUNCIL DECISION**

**Background**

The City has owned the subject land in freehold since 1981. Lot 432 has a split zoning, with 3131sqm reserved for Parks and Recreation and 4504sqm zoned Residential R30.

## Submission

The City has received an offer to purchase Lot 432 Rodd Place, Hamilton Hill. The offer of \$2,400,000.00 (ex-GST) is in line with the City's valuation and has a proposed settlement date of 28 February 2015. A copy of the valuation can be seen at Attachment 1.

## Report

The sale of this land is in line with the Land Management Strategy which identified this property as being land available for sale, once it had been value added to achieve its highest potential. This occurred by way of the Phoenix Rise Master Plan, which identified a rationalised zoning to enable residential development to occur overlooking a redeveloped piece of open space. This would occur as part of the overall redevelopment of the land.

The Master Plan incorporates a number of primary elements that focus on achieving the key objectives of increasing safety and revitalisation of the area overall. The Phoenix Rise Master Plan was prepared and advertised in 2005, and adopted in 2006. A copy of the Master Plan sketch and summary is attached in Attachment 2.

The proposed purchaser will be required to purchase the entire lot 432 and subsequently cede the portion of the land reserved for recreation and open space back to the City free of cost. The proposed purchaser will also be required to upgrade the reserved land and redesign and develop the stormwater sump to the satisfaction of the City. The intent of this is to ensure a coordinated redevelopment occurs, with a positive relationship between the residential component and future public open space component.

PRDnationwide intend to construct a two storey apartment development on Lot 432.

The proposed development will take advantage of the outlook to public open space north of the development and the views south west of the development. The apartments will particularly accommodate those seeking to downsize whilst remaining part of the community they have always lived in.

The proposed sale price of \$2,400,000 (ex GST) is in line with the Licensed Valuation that has been provided. It is recommended that the sale of the land proceed in accordance with the requirements of the Act.

## **Strategic Plan/Policy Implications**

### **Demographic Planning**

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.

### **Governance Excellence**

- To conduct Council business in open public forums and to manage Council affairs by employing publicly accountable practices.

### **Budget/Financial Implications**

Amend the 2014/15 adopted municipal budget by adding capital income of \$2,400,000 (ex-GST) from the sale proceeds against a new CW project – Sale Lot 432 Rodd Place, Hamilton Hill and transferring these into the Land Development & Investment Fund Reserve.

### **Legal Implications**

Provisions of Section 3.58 of the *Local Government Act 1995* apply.

### **Community Consultation**

As required by Section 3.58 of the *Local Government Act 1995*. Details of the proposed disposal have been advertised in the newspaper for State-wide publication, for a period of two weeks commencing on 2 September 2014.

No objections to the sale were received by the closing date of the advertising being 17 September 2014.

### **Attachment(s)**

- 1: Valuation
- 2: Location Plan

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) have been advised that this matter is to be considered at the 9 October 2014 Ordinary Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**14.7 (OCM 9/10/2014) - PROPOSED STRUCTURE PLAN - LOCATION: LOTS 12 & 13 LYON ROAD, AUBIN GROVE - OWNER: VARIOUS - APPLICANT: RPS (110/100) (C HOSSEN) (ATTACH)**

**RECOMMENDATION**

That Council:

- (1) pursuant to Clause 6.2.9.1 of City of Cockburn Town Planning Scheme No. 3 ("Scheme"), adopts the Proposed Structure Plan for Lots 12 and 13 Lyon Road, Aubin Grove subject to the following:
  1. The Local Water Management Strategy being updated to reflect the comments of the Department of Water and incorporated into the Structure Plan documentation.
- (2) in pursuance of Clause 6.2.10.1 of the Scheme, send the Structure Plan once modified to the Western Australian Planning Commission for endorsement;
- (3) endorse the schedule of submissions prepared in respect of the Structure Plan;
- (4) advise the proponent and those persons who made a submission of the Council's decision; and
- (5) advise the proponent that the site is subject to Development Contribution Area No. 13.

**COUNCIL DECISION**

**Background**

The purpose of this report is to consider for adoption the Proposed Structure Plan for Lots 12 and 13 Lyon Road, Aubin Grove ("subject land"). The Proposed Structure Plan seeks to provide the development framework for the subject land incorporating a medium density urban outcome and accommodating an area of public open space and associated road network.

The Proposed Structure Plan has been advertised for public comment and also referred to authorities for comment. This report now seeks to

specifically consider the Proposed Structure Plan for adoption, in light of the advertising process and assessment by officers.

### **Submission**

The Structure Plan was lodged by RPS on behalf of the owners of Lot 12 and 13 Lyon Road, Aubin Grove.

### **Report**

#### Planning Background

The subject land is 8.01 ha in size and is situated between existing residential development to the north and south, the Kwinana Freeway to the west and Lyon Road to the east. The subject land is the last significant development site within the Aubin Grove locality. A location plan is shown in Attachment 1.

The subject area is zoned 'Urban' under the Metropolitan Region Scheme ('MRS'). The subject area is zoned 'Development' under the City of Cockburn Town Planning Scheme No. 3 ('Scheme'). The subject land is also located within Development Area 11 (DA11) and is subject to the Development Contribution Area 13 (DCA13).

Pursuant to Clause 6.2.4 and Schedule 11 of the Scheme, a Structure Plan is required to be prepared and adopted prior to any subdivision and development of land within a Development Area.

#### Proposed Structure Plan

The Proposed Structure Plan as shown on Attachment 2 provides for a medium density residential development, with one area of Public Open Space ('POS') and an associated road network. Based on the design, approximately 191 dwellings (comprising single and grouped dwellings) will eventuate. The Proposed Structure Plan is expected to achieve a gross subdivisional density of approximately 26 dwellings a hectare.

The subject area is located in close proximity to the future Aubin Grove Train Station and Harvest Lakes Neighbourhood Centre; each being approximately 500m from the subject area. Therefore the site is provided with a high degree of strategic merit in assisting in achieving the performance expectations of the Harvest Lakes Activity Centre under the City's Local Commercial and Activity Centre Strategy.

The Proposed Structure Plan satisfies density objectives, POS requirements, provides a suitable road network and offers respectful integration with the existing residential areas. The proposed street layout and residential density further supports walking, cycling and

public transport trips to the Proposed Aubin Grove Train Station and the Harvest Lakes Neighbourhood Centre.

### Public Open Space

The Proposed Structure Plan provides for .8646 ha of Public Open Space; one large park being located in centre subject area, a small extension of Observatory Park south is also proposed. The proposed Structure Plan meets the requirement of 10% of the gross subdivisional area as per Element 4 of Liveable Neighbourhoods. The location of the POS should allow for the retention of some remnant bushland and will also have a drainage function.

The park is provided with a high degree of visible permeability through direct lot frontage and direct street frontage.

### Community Consultation

The Proposed Structure Plan was advertised in the Cockburn Gazette for public comment for a period of 21 days from 26 August 2014 to 16 September 2014. The Proposed Structure Plan was advertised to nearby and affected landowners and also referred to relevant government authorities.

In total twelve submissions were received for the proposed structure plan, including:

- One (1) from adjoining landowners
- Six (6) from government agencies
- Five (5) late submissions from adjoining landowners

The City received three (3) late submissions from residents of Twilight Mews, Aubin Grove. The submissions primarily went to concerns relating to the proposed height of retaining walls on their southern boundary of their properties. The finished lot levels and height of retaining is primarily a matter dealt with at subdivision stage of development; however, the City is cognisant of the residents' concerns and has communicated this to the applicant. The applicant has written to the City and confirmed that they will work constructively with all parties to address the matter.

Two (2) additional late submissions were received from adjoining landowners offering support for the proposal while raising some concerns. These have been directly addressed in the schedule of submissions.

All of the submissions that were received are set out and addressed in the Schedule of Submissions (Attachment 3).

No objections were received from either adjoining landowners or government authorities.

Comment was received from the Department of Water recommending a minor technical modification to the Local Water Management Strategy; this forms part of the Council recommendation.

As no matters of substance were raised by submitters there is no a need to directly address them in this report. Please see the Schedule of Submissions (Attachment 3) for detail and formal responses to each submission.

### Conclusion

It is recommended that the Council adopt the Structure Plan for Lots 12 and 13, Lyon Road, Aubin Grove, subject to modification and once the modifications are satisfactorily completed; pursuant to Clause 6.2.10 of the Scheme refer it to the Western Australian Planning Commission for their endorsement.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.
- Diversity of housing to respond to changing needs and expectations.

#### **Infrastructure**

N/A

#### **Community & Lifestyle**

- Communities that are connected, inclusive and promote intergenerational opportunities.

#### **Leading & Listening**

#### **Budget/Financial Implications**

The required fee was calculated on receipt of the Proposed Structure Plan and has been paid by the proponent. The site is subject to

Development Contribution Areas No 13. There aren't any other direct financial implications associated with the Proposed Structure Plan.

### **Legal Implications**

Clause 6.2.9.1 of the Scheme requires Council to make a decision on the application within 60 days from the end of the advertising period of such longer period as may be agreed by the applicant. The advertising period concluded on 16 September 2014.

### **Community Consultation**

In accordance with Clause 6.2.8 of the City's Scheme, the Proposed Structure Plan was advertised from 26 August 2014 to 16 September 2014. This included a notice in the Cockburn Gazette, letters to landowners within the Structure Plan area, adjoining landowners and State Government agencies.

Analysis of the submissions has been undertaken within the Schedule of Submissions (Attachment 3) as not matters of substance were raised.

### **Attachment(s)**

1. Location Plan
2. Structure Plan
3. Schedule of Submissions

### **Advice to Proponent(s)/Submitters**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **14.8 (OCM 9/10/2014) - PROPOSED VARIATION TO ROBB JETTY LOCAL STRUCTURE PLAN: VARIOUS LOTS, NORTH COOGEE APPLICANT: LANDCORP (110/063) (C CATHERWOOD) (ATTACH)**

### **RECOMMENDATION**

That Council:

- (1) pursuant to Section 6.2.8 of City of Cockburn Town Planning Scheme No. 3 ("Scheme"), advertise the proposed variation to the Robb Jetty Local Structure Plan subject to the following

modifications being undertaken:

1. Remove the reservation of 'Public Open Space' from the area of contamination on Lot 2108 Bennett Avenue and replace with the reservation 'Public Purpose (Civic)'.
  2. Public Open Space schedule and all mapping within Local Structure Plan documentation to be updated as per (1) 1 above.
- (2) advise the proponent that in advertising the proposed variation, Council is not willing to accept at this stage the future Management Order for the land indicated as 'Public Purpose (Civic)' on the proposed plan; and
- (3) advise the proponent that it may be willing to reconsider its position under (2) subject to the following:
1. A Site Management Plan being submitted which details the maintenance responsibilities which would be associated with this site, including a breakdown of costs each year for a minimum time horizon of 30 years.
  2. An independent analysis to determine whether these maintenance responsibilities exceed (by way of cost) the normal responsibilities associated with an area of public open space which is not contaminated, for a time horizon of 30 years.
  3. Agreement from the proponent to meet the additional cost gap, if one such exists, for the 30 year time horizon.
  4. Advice on the type of geotextile membrane proposed to be used, its likely lifespan, and the practicalities of replacement.
  5. A Landscape Management Plan for the site.

## COUNCIL DECISION

### Background

The original Robb Jetty Local Structure Plan was adopted by Council on 8 May 2013 and endorsed by the WAPC on 16 July 2014. Since then, more detailed subdivisional design and contaminated sites investigation has led the proponent (Landcorp) to seek variations to the endorsed plan.

Detail of the variations is set out in the Report section of this item. In brief, these variations involve:

- Changing two areas of land currently zoned for 'Residential' and reserved as 'Road' to land reserved as 'Public Purpose (Civic)'. This land is proposed to retain contaminants (primarily lead) on site.
- Changing two areas currently zoned for 'Residential' to reserves for 'Lakes and Drainage'. This land is currently used for drainage purposes and is now proposed to be retained for this purpose. The associated Local Water Management Strategy has also been updated and forms an attachment to the Local Structure Plan.

City officers have noted in the draft management documentation, Landcorp's intention is to leave contaminants in the area currently shown for Public Open Space. This is not acceptable given Council's policy position of not accepting contaminated land as public open space. There is also the additional issue of development contributions being used to pay for, and embellish public open space land. City officers are not prepared to accept land with such an encumbrance as part of the 10% public open space. Therefore, an additional modification is recommended which seeks to have all the contaminated area shown as 'Public Purpose (Civic)' so it is clear it does not form part of the 10% POS calculation (and DCP14 contribution for other landowners). There is a small surplus of public open space in Robb Jetty which should be able to accommodate this.

### **Submission**

The proponent submits the proposed variations are requested to manage contaminants (enabling retention and management in situ rather than removal off site). Retention of the contaminants also impacts the proposed drainage strategy as basins can no longer be located on the affected land without compromising the management plan.

The variation to the Robb Jetty Local Structure Plan has been submitted by HASSELL on behalf of Landcorp.

### **Report**

The purpose of this report is for Council to consider whether it is prepared to advertise the variation to the Robb Jetty Local Structure Plan.

### Delegation

Ordinarily, delegation is used to advertise a local structure plan, or a proposed variation thereof. In this case, delegation set out in APD55 Structure Plans, Rezoning Applications and MRS Amendments cannot be utilised as the proposed variation presents a conflict with adopted Council policy.

The source of conflict is the notion of contaminants being contained on land which would be vested with the City i.e. become the City's responsibility. Council quite appropriately does not normally accept land burdened in this manner.

### Planning Background

The Robb Jetty Local Structure Plan applies to the Cockburn Coast project area bounded by Rollinson, Cockburn and McTaggart Cove Roads and the foreshore reserve and freight railway line as shown in the Precinct Plan (Attachment 1).

The subject land is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") and 'Development' under City of Cockburn Town Planning Scheme No. 3 ("Scheme"). The subject land is also located within Development Area 33 ("DA 33"), Development Contribution Area No. 13 ("DCA 13") and proposed Development Contribution Area No. 14 ("DCA 14").

### Proposed Variation to Local Structure Plan

#### *Proposed retention of drainage reserves*

An updated Local Water Management Strategy ("LWMS") has been prepared to accompany the proposed variation to the Robb Jetty Local Structure Plan. Since the original LWMS was endorsed, further site investigations and stakeholder discussions have been undertaken. This resulted in the notification that additional portions of the development area are subject to contaminated material including part of the area where the proposed basin (N2) was to be located.

As a result of this contaminated lands site investigation, it was advised that the area previously identified for stormwater infiltration is no longer suitable for retaining and infiltrating stormwater due to the underlying contamination and as a consequence its potential for movement and the associated impact on surrounding areas. As such, the stormwater strategy originally proposed for Robb Jetty requires amendment to reflect the revised site conditions.

The change in the LWMS is reflected in the proposed variation to the Local Structure Plan map by reserving dedicated drainage lots as 'Lakes and Drainage'. There are associated textual and minor mapping updates within the structure plan text to reflect this change.

#### *Proposed Public Purpose (Civic) reserve*

A contaminated site investigation of the former marshalling yards site has indicated contaminants (primarily lead) present on the property. The suggested management plan is to retain these on site and manage in situ, covering in geotextile and fill. This approach is discussed further below under the subheading 'Land Management Issues'.

This land would be in addition to the minimum 10% public open space already provided for, and is situated directly adjacent to one of these areas. The land has been proposed to be removed from the Residential zone and reserved as 'Public Purposes (Civic)'. Selection of this reserve under the Scheme is important as land reserved for Public Open Space forms part of the proposed DCP14 (and DCP13 in the case of the oval site). Other landowners should not be expected to fund the management solution for this contaminated land. Reservation as 'Public Purpose (Civic)' assists in reinforcing that position.

The proposed variation to the Local Structure Plan map is shown by reserving the affected land as 'Public Purpose (Civic)'. There are associated textual and minor mapping updates within the structure plan text to reflect this change.

#### *Additional Modifications required*

City officers have noted in the draft management documentation, Landcorp's intention is to leave contaminants in the area currently shown for Public Open Space. This is not acceptable given Council's policy position of not accepting contaminated land as public open space. There is also the additional issue of development contributions being used to pay for, and embellish public open space land. City officers are not prepared to accept land with such an encumbrance as part of the 10% public open space. Therefore, an additional modification is recommended which seeks to have all the contaminated area shown as 'Public Purpose (Civic)' so it is clear it does not form part of the 10% POS calculation (and DCP14 contribution for other landowners). There is a small surplus of public open space in Robb Jetty which should be able to accommodate this.

#### Land Management Issues

The practice of containing contaminated soil on site does not occur regularly. City staff have discussed the concept with the City's

Contaminated Sites Auditor who has confirmed it is acceptable in similar circumstances, both nationally and internationally. The concept has also been discussed with the Manager of Health at the City of Fremantle (cognisant this land may transfer to Fremantle under local government reform). They have indicated they are also comfortable with the proposal.

The alternative option to management on site is to excavate, transfer to trucks, and transport to a landfill for burial. This raises concerns with potential for dust emissions.

The contaminated material is largely lead which is not especially toxic and doesn't leach to ground water. It would be buried at a depth that removes any likelihood of disturbance by excavation in the future. It would be covered by a strong geotextile sheet plus a visible warning layer to alert any future excavator.

The geotextile material would be guaranteed to last for a period of time that would be similar to the design life of a typical building and would be commonly accepted by jurisdictions around the world. Advice has been sought from Landcorp on the lifespan of the proposed geotextile. A Landscape Management Plan (also requested) would be needed to show the presence of any tall trees which may have deep roots and any structures needing deep footings and these areas will need special treatment to ensure they don't impact the geotextile layer. This would be included in the various Management Plans and any Management Order should not be accepted until complete satisfaction is achieved with these plans. There would be an ongoing need for the managing local government to ensure that they have records systems that alert their operations workforce and planners about the presence of the material. It is noted the City of Fremantle have a number of contaminated sites with similar restrictions including in North Fremantle and around the old South Fremantle Landfill and quite possibly already manage the necessary alert systems.

This suggests that it may be viable for the City to accept future management of this contaminated land. In saying this, there are still uncertainties pertaining to financial risks. To address such uncertainties, it is recommended that Council appropriate caveat its position by not agreeing to accept management of the site, and that further information is needed to reconsider this position. This information being:

- A Site Management Plan, which details the maintenance responsibilities which would be associated with this site, including a breakdown of costs each year for a minimum time horizon of 30 years.
- An independent analysis to determine whether these maintenance responsibilities exceed (by way of cost) the normal responsibilities

associated with an area of public open space which is not contaminated, for a time horizon of 30 years.

- Agreement from the proponent to meet the additional cost gap, if one such exists, for the 30 year time horizon.
- Advice on the type of geotextile membrane proposed to be used; its likely lifespan and the practicalities of replacement.
- A Landscape Management Plan for the site.

This information once received would assist Council in determining whether it is or is not willing to accept the ultimate management of this contaminated site. Note that in the absence of Council accepting such, it would fall to the State Government's Department of Land to manage.

In summary, it is appropriate for Council to initiate advertising of the structure plan variation, but to clearly advise the proponent that at this time, it is not in a position to accept future management of the 'Public Purpose (Civic)' land shown on the proposed plan. Council should also advise that it may be willing to reconsider this position, by submission of additional information as outlined above.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.

#### **Leading & Listening**

- A culture of risk management and compliance with relevant legislation, policy and guidelines.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.
- Identification and minimisation of impacts to human health risk.

### **Budget/Financial Implications**

#### Proposed Public Purpose (Civic) reserve

Should this land be vested with the City (or any entity) there would be an ongoing need for maintenance and renewal attributed to the site. The proposed management of contaminants would see a geotextile layer secured over the contaminants and a design response for the

embellishment of the land planned and installed accordingly by Landcorp.

Maintenance and renewal costs are likely to be akin to management of the adjacent public open space. This land will be in addition to the minimum 10% public open space requirement. Should the City look to take on management of this land, a contribution should be made by Landcorp to offset the maintenance and renewal costs incurred in managing this site. A thirty year period should be considered appropriate. This is reflected in the officer recommendation.

#### Proposed retention of drainage reserves

Previously these basins were proposed to be 'designed out'. With their retention there may come an expectation they be visually improved by surrounding landowners. A requirement has been included that the adjoining developers, should they choose to drain into these drainage reserves, will need to contribute (proportionally) to their upgrading. This is not likely to be sufficient to cover the entire costs of upgrading the basins for aesthetic purposes. Although, the Local Water Management Strategy outlines the City has no intention to carry out further upgrades on these basins, there may be a requirement to complete the upgrade works and undertake ongoing maintenance.

#### **Legal Implications**

Nil

#### **Community Consultation**

Should Council consent to advertise this variation, it would be advertised as per Section 6.2.8 of the Scheme (not less than 21 days).

#### **Attachment(s)**

1. Cockburn Coast Precinct Plan.
2. Proposed variation to Robb Jetty Local Structure Plan with areas of variations marked up.

#### **Advice to Proponent(s)/Submissioners**

N/A

#### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**14.9 (OCM 9/10/2014) - FOUR (4) MULTIPLE DWELLINGS - LOCATION: 21 (LOT 26) LUPIN WAY COOGEE - OWNER: ATKINS PROPERTY DEVELOPMENT PTY LTD - APPLICANT: MOMENTUM WEALTH PROJECTS (3317437) (R TRINH) (ATTACH)**

**RECOMMENDATION**

That Council:

- (1) approve the application for Four (4) Multiple Dwellings at No. 21 (Lot 26) Lupin Way, Coogee, in accordance with the attached plans and subject to the following conditions and advice notes:

Conditions

1. Prior to the lodgement of a Building Permit, arrangements being made to the satisfaction of the Chief Executive Officer for the pro-rata subdivider contributions towards those items listed in the City of Cockburn Town Planning Scheme No. 3 for Development Contribution Area 13 – Community Infrastructure.
2. No activities causing and/or inconvenience to neighbours being carried out after 7.00pm or before 7.00am, Monday to Saturday, and not at all on Sunday or Public Holidays.
3. Walls, fences and landscape areas are to be truncated within 1.5 metres of where they adjoin vehicle access points where a driveway and/or parking bay meets a public street or limited in height to 0.75 metres.
4. All stormwater being contained and disposed of on-site to the satisfaction of the City.
5. The development site must be connected to the reticulated sewerage system of the Water Corporation before commencement of any use.
6. A schedule of the materials, finishes and colours shall be submitted to and approved by the City prior to the lodgement of a Building Permit Application. The schedule shall include details of the type of materials proposed to be used, including their colour and texture. The development shall thereafter be maintained in accordance with the approved materials schedule.
7. All services and service related hardware, including antennae, satellite dishes and air conditioning units, being suitably located away from public view and/or

screened to the satisfaction of the City.

8. The visitor parking bays shall be permanently marked, maintained and accessible at all times for use exclusively by visitors to the property, be clearly visible or suitably sign posted from the street or communal driveway and be located, together with the reversing area, in front of any security gates or barrier for the development unless otherwise approved by the City of Cockburn.
9. All vehicle parking, access ways, footpaths and external lighting shall be constructed and maintained in accordance with the Australian Standards AS2890 in the form and layout depicted on the approved plans to the satisfaction of the City.
10. Crossovers and the Common Property access way are to be constructed and drained in accordance with the City's specifications and standards.
11. Clothes drying shall not occur on open balconies at any time unless behind purpose built screening approved as part of the development. Prior to the lodgement of a Building Permit, revised plans are to be submitted to the satisfaction of the City showing the location of the clothes drying area for Units 3 and 4 and ensuring it is screened from Lupin Way.
12. The dining room window of unit 3 shown in red on the approved plans being either: fixed obscure glazing; or with a minimum sill height of 1.6metres above finished floor level; or an obscure awning type window; or any other alternative that complies with Clause 6.8.1 (A1) of the Residential Design Codes of Western Australia to the satisfaction of the City. Details shall be provided with the Building Permit Application.
13. A minimum of 2 bicycle stands/racks that conform to Australian Standard 2890.3 shall be provided in close proximity to the entrance of the building prior to occupation of the building. Details of the bicycle parking shall be provided prior to the lodgement of a Building Permit Application.
14. Landscaping including verge planting shall be installed, reticulated and/or irrigated in accordance with the/an approved plan and maintained thereafter to the satisfaction of the City. The landscaping shall be

implemented during the first available planting season post completion of development and any species which fail to establish within a period of 12 months from planting shall be replaced to the satisfaction of the City.

#### Advice Notes

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, or with any requirements of the City of Cockburn Town Planning Scheme No. 3 or with the requirements of any external agency.
2. Where the obligation for payment of developer contributions has been met by a previous approval, such as subdivision, Condition 1 will be deemed to have been complied with.
3. With regard to Condition 4, the onsite storage capacity for stormwater is to be designed to contain a 1 in 20 year storm of a 5 minute duration. This is based on the requirements to contain surface water by the Building Code of Australia.
4. Retaining wall(s) being constructed in accordance with a suitably qualified Structural Engineer's design and a building permit being obtained prior to construction. Retaining walls are required for any cut and/or fill greater than 150mm in height. In this regard, any fill above or below natural ground level at the lot boundaries is to be suitably retained or have a compliant stabilised embankment.
5. With regards to street numbering of this proposal, you are advised to contact the City's Strategic Planning team on 9411 3444 or email [streetnumbers@cockburn.wa.gov.au](mailto:streetnumbers@cockburn.wa.gov.au) to ensure that any street numbers used comply with the City's requirements. This should be done prior to any sales contracts being drawn up.
6. The development is to comply with the noise pollution provisions of the Environmental Protection Act 1986, and more particularly with the requirements of the Environmental Protection (Noise) Regulations 1997. The installation of equipment within the development including air-conditioners, spas, pools and similar equipment must not result in noise emissions to neighbouring properties

exceeding imposed by the Environmental Protection (Noise) Regulations 1997 (as amended).

7. All toilets, ensuites and kitchen facilities in the development are to be provided with mechanical ventilation flued to the outside air, in accordance with the requirements of the Building Code of Australia, the Sewerage (Lighting, Ventilation and Construction) Regulations 1971, Australian Standard S1668.2-1991 "The use of mechanical ventilation for acceptable indoor air quality" and the City of Cockburn Health Local Laws 2000. The City's Health Service further recommends that laundries without external windows and doors should also be ventilated to external air.
8. All bathrooms, laundry facilities and sanitary conveniences in the development are to be provided with an adequate lining of impervious material in accordance with the requirements of the Sewerage (Lighting, Ventilation and Construction) Regulations 1971, the Health Act (Laundries and Bathrooms) Regulations 1971 and the City of Cockburn Health Local Laws 2000.

- (2) advise the applicant and those who made a submission of Council's decision.

## COUNCIL DECISION

### Background

The subject site is 731m<sup>2</sup> in area and is located on the southern side of Lupin Way, Coogee. The site naturally slopes upwards towards the rear of the lot by approximately 5m. The site is currently vacant and surrounded by predominantly existing single houses and grouped dwellings.

The subject site is zoned 'Residential-R30' under the City of Cockburn's Town Planning Scheme No.3 (TPS 3) and is located within Development Area 1 (Packham) and Developer Contribution Area 13.

The proposal is for four multiple dwellings, which is a 'D' use under TPS 3. 'D' means that the use is not permitted unless the local government has exercised its discretion by granting planning approval.

The proposed development is also subject to comply with the Residential Design Codes (R-Codes) and is seeking approval under the design principles with regard to the rear setback, visual privacy setback to the dining room window and the plot ratio of the development. The variations were seen to potentially adversely affect neighbouring properties and therefore adjoining land owners were notified and given an opportunity to provide comments regarding the development.

As per APD53, City staff has delegation to determine development applications but due to the responses received from adjoining land owners and issues raised by some elected members of the City of Cockburn Council, this application has been referred to Council for determination.

### **Submission**

The applicant is seeking planning approval for four multiple dwellings with a plot ratio of 0.52 (382.62m<sup>2</sup>). Each unit consists of three bedrooms, two bathrooms, a living area and a courtyard/balcony. Eight car parking spaces are proposed for residents at the rear of the development, which follows the natural ground level of the lot and one visitor car parking bay is proposed at the front of the development.

The variations proposed for this development are for a plot ratio 0.02 (17.12m<sup>2</sup>) larger than the maximum plot ratio permitted for a R30 development, a rear setback of 0.9m in lieu of 1.5m and a side setback of 5m in lieu of 6m of a dining room window that overlooks to the western side of the lot.

### **Consultation**

Three (3) land owners were notified of the variations that were seen to adversely affect their property and were given 14 days to provide comments. A total of three (3) responses were received, all being objections to the development.

The objections were in regards to:

- the size of the units that exceed the plot ratio for an R30 development;
- 9 vehicle parking spaces that would generate increased noise and traffic movements;
- the proposed development not be in keeping with surrounding development;
- potentially 16-20 people that would increase impact, opposed to a standard home;

- excessive number of units proposed; and
- impact on property values.

None of the comments received were based on relevant planning consideration having regard to the design principles of the R-Codes.

## **Report**

### Statutory Framework

#### *Metropolitan Region Scheme (MRS)*

The subject site is zoned 'Urban' under the MRS and the proposal is consistent with this zone.

#### *Town Planning Scheme No.3 (TPS 3)*

The subject site is zoned 'Residential-R30' under the City of Cockburn's Town Planning Scheme No.3 (TPS 3) and is located within Development Area 1 (Packham) and Developer Contribution Area 13. The proposed multiple dwellings generally comply with the provisions of TPS 3 and is a 'D' use under TPS 3. 'D' means that the use is not permitted unless the local government has exercised its discretion by granting planning approval.

### Discussion

#### *Rear setback*

The rear setback of the retaining wall is proposed at 0.9m in lieu of 1.5m. Eight (8) vehicle car parking spaces are proposed at the rear of the lot with a retaining wall and car parking shelter setback 0.9m from the rear boundary. Landscaping is proposed between the retaining wall and the rear boundary within the 0.9m setback area. The proposed car parking bays will be 3.7m lower than the fence of the property at the rear and will be sheltered and not viewable from the property behind the development. This variation met the design principles of the R-Codes as the retaining wall results in land that can be effectively used for the benefit of residents and do not detrimentally affect adjoining properties and are designed, engineered and landscaped having due regard to site works and visual privacy. Therefore it was not seen to adversely affect adjoining land owners.

#### *Visual privacy*

The proposed setback from the dining room window of unit 3 is 5m in lieu of 6m. Although the setback does not meet the deemed to comply provisions of the R-Codes, the window overlooks a vehicle access way along the side of the house, which is not considered an active habitable

space or outdoor living area. This variation was advertised to the affected land owner.

### *Building Size*

R30 lots are permitted a maximum plot ratio of 0.5 of the lot, however the building bulk and scale of the proposed development is consistent with the existing and future desired built form of the locality. The minor variation of 0.02 (17.62m<sup>2</sup>) above the maximum permitted plot ratio would not affect adjoining land owners, however given that the surrounding area is predominantly single houses and grouped dwellings, the proposal was advertised to adjoining land owners.

The proposed development generally complies with the requirements of TPS 3 and the Residential Design Codes (R-Codes) with variations to the rear setback, visual privacy setback to the dining room window and the plot ratio of the development.

In keeping with the building bulk and scale of development in the surrounding area of the lot, the proposed development maintains the aesthetics of a two-storey dwelling with access to the rear for parking, similar to other houses in the street. Although the plot ratio is above the maximum size permitted under the R-Codes, the total floor area of the development is comparable to the permitted floor area of a single house.

### Conclusion

The proposed development will not appear unbecoming to the area, given that the proposed development will be visually consistent with the surrounding area and is permissible under TPS 3. The development provides an alternative residential dwelling and creates a variety of residential choices in the surrounding area. The plot ratio is considered to meet the relevant design principle of the R-Codes.

As detailed in this report, the proposed car parking spaces at the rear of the development will be sufficiently screened from view with landscaping and well below the fence line of the adjoining property.

To satisfy the visual privacy concerns, should Council approve the proposal, a planning condition can be imposed to modify the plans submitted with the building permit to demonstrate the dining room window complying with the visual privacy requirements of the R-Codes by being either fixed obscure glazed, with a minimum sill height of 1.6 metres above finished floor level, an obscure awning type window or any other alternative that complies.

Given that the development is permissible under TPS 3 and generally complies with the requirements of the R-Codes, which will not adversely impact on the amenity of the surrounding area, it is therefore recommended that Council approve the application, subject to the conditions listed in this report.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.
- Diversity of housing to respond to changing needs and expectations.

#### **A Prosperous City**

- Sustainable development that ensures Cockburn Central becomes a Strategic Regional Centre.
- Investment in the local economy to achieve a broad base of services and activities.

### **Budget/Financial Implications**

Nil.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

1. Location Plan
2. Street elevation
3. Feature and contour survey
4. Site plan
5. Ground floor plan
6. First floor plan
7. North and west elevation
8. East and south elevation

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **15. FINANCE AND CORPORATE SERVICES DIVISION ISSUES**

### **15.1 (OCM 9/10/2014) - LIST OF CREDITORS PAID - AUGUST 2014 (076/001) (N MAURICIO) (ATTACH)**

#### **RECOMMENDATION**

That Council adopt the List of Creditors Paid for August 2014, as attached to the Agenda.

#### **COUNCIL DECISION**

### **Background**

It is a requirement of the Local Government (Financial Management) Regulations 1996, that a List of Creditors be compiled each month and provided to Council.

### **Submission**

N/A

### **Report**

The List of Accounts for August 2014 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

### **Strategic Plan/Policy Implications**

#### **Leading & Listening**

- Effective and constructive dialogue with all City stakeholders.
- A responsive, accountable and sustainable organisation.

**Budget/Financial Implications**

N/A

**Legal Implications**

N/A

**Community Consultation**

N/A

**Attachment(s)**

List of Creditors Paid – August 2014.

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**15.2 (OCM 9/10/2014) - STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - AUGUST 2014 (071/001) (N MAURICIO) (ATTACH)**

**RECOMMENDATION**

That Council

- (1) adopt the Statement of Financial Activity and associated reports for August 2014, as attached to the Agenda; and
- (2) amend the 2014/15 Municipal Budget by adjusting the net Department of Fire and Emergency Services (D.F.E.S.) funding for emergency and bushfire brigade services in accordance with advised funding levels (net income increase of \$4,498).

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

**COUNCIL DECISION**

## Background

Regulations 1996 prescribes that a local government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:–

- (a) details of the composition of the closing net current assets (less restricted and committed assets);
- (b) explanation for each material variance identified between YTD budgets and actuals; and
- (c) any other supporting information considered relevant by the local government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within 2 months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature and type, statutory program or business unit. The City chooses to report the information according to its organisational business structure, as well as by nature and type.

Local Government (Financial Management) Regulations - Regulation 34 (5) states:

- (5) *Each financial year, a local government is to adopt a percentage or value, calculated in accordance with the AAS, to be used in statements of financial activity for reporting material variances.*

This Regulation requires Council to annually set a materiality threshold for the purpose of disclosing budget variance details. Council adopted a materiality threshold of \$200,000 for the 2014/15 financial year at its August meeting.

## Submission

N/A

## **Report**

### Opening Funds

The current figure of \$13.2M (unaudited) represents the closing municipal position for 2013/14 and covers the \$3M surplus forecast in the adopted budget, \$8.6M of municipal funding attached to carried forward works & projects and a residual balance of uncommitted funds to be further applied in accordance with Council's budget policy. This matter has been addressed in a separate item to Council this month.

### Closing Funds

The City's closing funds of \$93.9M are \$7.9M higher than the YTD budget target. This comprises net favourable cash flow variances across the operating and capital programs as detailed later in this report and the impact of the opening funds variance described earlier.

The revised budget shows end of year closing funds of \$0.1M. This change has predominantly resulted from a small increase from the notional Financial Assistance Grants (FAGS) advised to the City for 2014/15.

The budgeted closing funds fluctuate throughout the year, due to the impact of Council decisions and budget recognition of additional revenue. Details on the composition of the budgeted closing funds are outlined in Note 3 to the financial summaries attached to this report

### Operating Revenue

Consolidated operating revenue of \$91.5M was slightly below the YTD budget forecast by \$0.1M. Significant variances in this result were:

- Revenue under Governance was \$0.27M under YTD budget due to lower interest earnings on investments (down \$0.12M) and charges raised for the underground power schemes (down \$0.11M).

Further details of budget variances are disclosed in the Agenda attachment.

### Operating Expenditure

Reported operating expenditure (including asset depreciation) of \$16.7M was under the YTD budget by \$2.3M and comprised the following significant items:

- Material and Contracts were \$2.3M under YTD budget with most business units contributing. Software support expenses were \$0.4M under the YTD budget and the Engineering directorate contributed \$1.2M of the variance.
- Other Expenses were \$0.3M under YTD budget primarily due to lower charges for the landfill levy.

A more detailed explanation of the variances within each business unit is included in the attached financial report.

The following table shows the operating expenditure budget performance at the consolidated nature and type level:

<b>Nature or Type Classification</b>	<b>Actual Expenses \$M</b>	<b>YTD Revised Budget \$M</b>	<b>Variance to YTD Budget \$</b>	<b>FY Revised Budget \$M</b>
Employee Costs - Direct	6.65	6.52	(0.13)	42.69
Employee Costs - Indirect	0.09	0.09	0.01	0.90
Materials and Contracts	3.64	5.96	2.32	35.95
Utilities	0.89	0.75	(0.13)	4.52
Interest Expenses	0.00	0.00	0.00	0.12
Insurances	1.14	1.12	(0.02)	2.34
Other Expenses	0.84	1.14	0.30	7.58
Depreciation (non-cash)	3.98	3.96	(0.02)	23.76

### Capital Expenditure

The City's actual capital spend for the month of August was \$4.1M, representing an under spend of \$4.7M on the YTD budget of \$8.8M.

The following table shows the budget variance analysis by asset class:

<b>Asset Class</b>	<b>YTD Actuals \$M</b>	<b>YTD Budget \$M</b>	<b>YTD Variance \$M</b>	<b>Annual Budget \$M</b>	<b>Commit Orders \$M</b>
Roads Infrastructure	0.84	2.25	1.42	16.42	0.94
Drainage	0.05	0.05	0.00	1.60	0.02
Footpaths	0.20	0.23	0.03	1.29	0.09
Parks Hard Infrastructure	0.18	0.61	0.43	8.21	1.40
Parks Soft Infrastructure	0.12	0.09	(0.03)	0.92	0.12
Landfill Infrastructure	0.06	0.08	0.02	1.49	0.04
Freehold Land	0.02	0.22	0.20	1.38	0.00
Buildings	1.94	3.10	1.16	31.72	2.31
Furniture & Equipment	0.00	0.01	0.01	0.01	0.00
Computers	0.19	0.52	0.33	1.19	0.21

<b>Asset Class</b>	<b>YTD Actuals \$M</b>	<b>YTD Budget \$M</b>	<b>YTD Variance \$M</b>	<b>Annual Budget \$M</b>	<b>Commit Orders \$M</b>
Plant & Machinery	0.54	1.68	1.14	5.59	1.56
<b>Total</b>	<b>4.15</b>	<b>8.85</b>	<b>4.70</b>	<b>69.83</b>	<b>6.68</b>

The major variances occur in the roads, buildings and plant & machinery asset classes. Further details on the significant spending variances by project are disclosed in the attached CW Variance analysis report.

### Capital Funding

Capital funding sources are generally highly correlated to capital spending, the sale of assets and the rate of development within the City (developer contributions received).

Significant variances for August include:

- Transfers from financial reserves were \$1.8M behind budget, consistent with the capital under spend.
- Developer contributions received under the Community Infrastructure plan are \$0.8M ahead of the YTD budget.
- Developer contributions totalling \$0.4M received for Success North and Munster DCP areas.
- POS cash in lieu contributions of \$0.2M received. These are held in restricted funds and are not budgeted due to inability to estimate.
- Proceeds from the sale of land and plant assets were collectively \$0.9M behind YTD budget settings.

### Cash & Investments

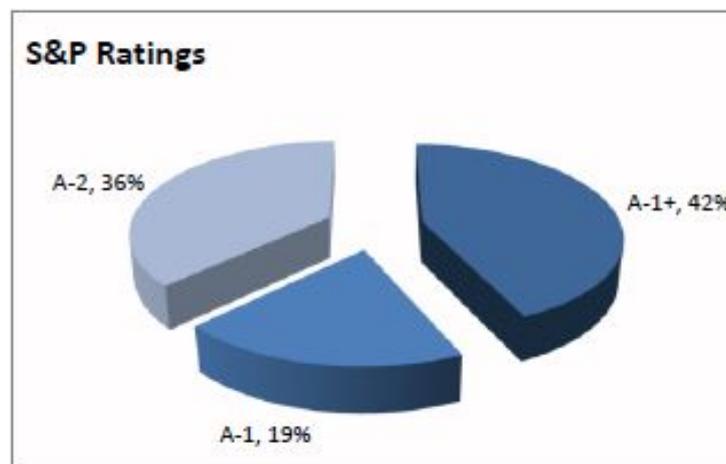
Council's cash and financial investments holding at August month end totalled \$130.9M, up from \$105.5M the previous month as rates payments flowed in. \$82.2M represented the balance held in the City's cash backed financial reserves. Another \$3.8M represented funds held for other restricted purposes such as deposit and bond liabilities. The remaining \$44.9M represented the cash and financial investment component of the City's working capital, available to fund current operations, capital projects, financial liabilities and other financial commitments.

The City's investment portfolio made a weighted annualised return of 3.73% in August, down from 3.92% from the previous month. Whilst this compares favourably against the BBSW 6 month annualised rate of 2.69%, the return is trending downwards due to the low official

Australian cash rate of 2.50% and a general tightening of margins within the term deposit market.

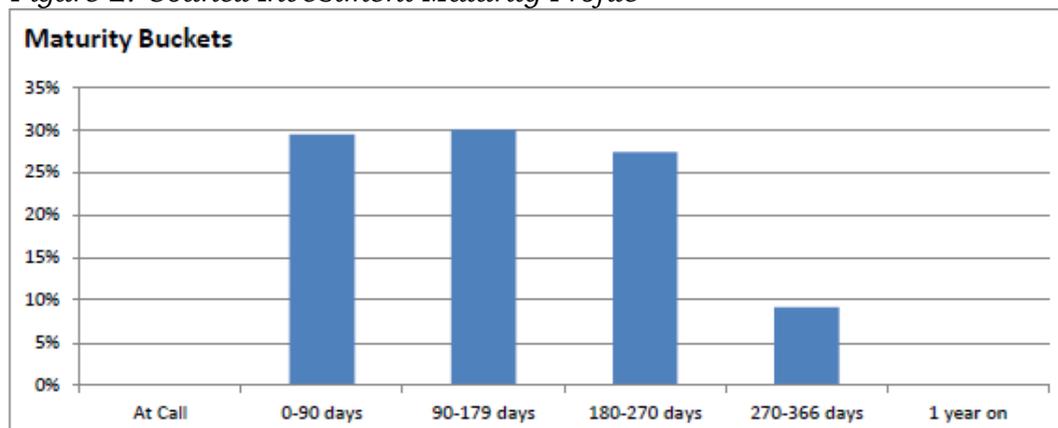
The majority of investments are held in term deposit (TD) products placed with highly rated APRA (Australian Prudential Regulation Authority) regulated Australian banks. These are invested for terms ranging between three and twelve months in order to lock in the most beneficial rate and meet the City's cash flow requirements. Factors considered when investing include maximising the value offered within the current interest rate yield curve and mitigating cash flow liquidity risks. All TD investments comply with the Council's Investment Policy and fall within the following risk rating categories:

*Figure 1: Council Investment Ratings Mix*



Given we are now at the bottom of the current interest rate cutting cycle (consensus view of the market) the investment strategy now aims to shorten the average duration for the investment portfolio. TD investments offering value over short to medium terms (3 to 6 months) are preferred, subject to cash flow planning requirements. This will reduce risks associated with a potential increase in interest rates over the medium term. The City's investment portfolio currently has an average duration of 148 days, graphically depicted below:

Figure 2: Council Investment Maturity Profile



### Budget Revisions

A minor adjustment is necessary to the Department of Fire & Emergency Services (DFES) funded budgets in order to reconcile to the approved funding level. This will impact the City's budgeted closing funds by an increase from \$103,114 to \$107,612, a net increase of \$4,498 against GL activities 161,162 & 175.

### Description of Graphs and Charts

There is a bar graph tracking Business Unit operating expenditure against budget. This provides a very quick view of how the different units are tracking and the comparative size of their budgets.

The Capital Expenditure graph tracks the YTD capital spends against the budget. It also includes an additional trend line for the total of YTD actual expenditure and committed orders. This gives a better indication of how the capital budget is being exhausted, rather than just purely actual cost alone.

A liquidity graph shows the level of Council's net current position (adjusted for restricted assets) and trends this against previous years. This gives a good indication of Council's capacity to meet its financial commitments over the course of the year. Council's overall cash and investments position is provided in a line graph with a comparison against the YTD budget and the previous year's position at the same time.

Pie charts included show the break-up of actual operating income and expenditure by nature and type and the make-up of Council's current assets and liabilities (comprising the net current position)

## **Strategic Plan/Policy Implications**

### **Leading & Listening**

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

### **Budget/Financial Implications**

Budget amendment included in the recommendation. Increase the City's Municipal Budget position by \$4,498 to \$107,612.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

Statement of Financial Activity and associated reports – August 2014.

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **15.3 (OCM 9/10/2014) - 2013/14 CARRIED FORWARD WORKS & PROJECTS & CLOSING MUNICIPAL FUNDS (071/002) (N MAURICIO) (ATTACH)**

### **RECOMMENDATION**

That Council

- (1) amend the 2014/15 budget by adding the Carried Forward Works and Projects as set out in the schedule attached to the Agenda and summarised in the following table:

Capital Expenditure	\$23,655,202
Operating Expenditure	\$481,788
Transfers to Reserves (from land sales)	\$4,022,727
Capital Income – Sale of Asset Proceeds	(\$4,258,727)
Grants yet to be received	(1,005,833)
Transfers from Reserves (Funding)	(\$11,882,349)
Restricted Grants and Contributions unspent	(\$2,122,133)
Municipal Funding required for carried forwards*	(\$8,890,675)

- (2) amends the 2014/15 budget by bringing in the 2013/14 closing municipal funds and allocating these as follows:

Closing funds as per June 2014 Statement of Financial Activity (budget surplus)	\$13,281,518
LESS: Closing funds b/fwd in 2014/15 adopted budget	(\$3,000,000)
LESS: Municipal funding required for carried forwards*	(\$8,890,675)
LESS: t/f to Community Surveillance Levy Reserve	(\$140,758)
LESS: t/f to CCW Development Fund Reserve	(\$1,250,085)
Net impact on closing budget position	Nil.

**TO BE CARRIED BY ABSOLUTE MAJORITY OF COUNCIL**

## COUNCIL DECISION

### Background

When Council adopted its Budget for the 2014/15 financial year (FY) at the June meeting, detailed carried forward works and projects were not included as these were unknown at that time. A \$3M estimated closing municipal position was included in the adopted budget for 2014/15.

Post 30 June 2014 end of FY year processing has now been completed, allowing for the closing municipal position and the value of carried forward works and projects to be declared (subject to external audit adjustment). Should the audit determine a change is required to the closing municipal position, this will be dealt with in the mid-year budget review.

### Submission

N/A

## Report

The interim statement of financial activity presented to the August Council meeting showed closing municipal funds of \$14,098,265. Now that the end of financial year processing has been completed and the final figures calculated, the closing funds position has reduced to \$13,281,518 (a net decrease of \$816,747). An updated statement of financial activity as at 30 June 2014 is attached to the Agenda.

The closing funds include the municipal funding requirement for carried forward works and projects. A schedule of the carried forwards is attached to the Agenda, showing a net municipal funding requirement of \$8,890,675. There is also \$140,758 of unspent funds as at 30 June 2014 from the community surveillance security levy that needs to be directed to the associated financial reserve. In the 2014/15 adopted budget, the City included a brought forward municipal position of \$3,000,000.

Allowing for the abovementioned items still leaves \$1,250,085 of the final closing position uncommitted. As per Council Policy SC34 'Budget Management', surplus closing municipal funds identified at the end of each financial year are to be transferred to financial reserves or other financial contingencies with the objective of attaining the target values set for them. Accordingly, it is proposed that the \$1,250,085 be transferred into the CCW Development Fund Reserve to partially offset the borrowing requirement for this significant community infrastructure project.

Council is required to formally adopt the carried forward works and projects each year and this is being achieved through amendment to the 2014/15 budget. The carried forward works and projects include capital and operating expenditure totalling \$24,136,990. These are funded from financial reserves, grants and contributions, in addition to the municipal funding previously stated.

Whilst there are 169 projects carried forward, only 33 of these are for more than \$0.1M each and totalling \$20.7M. These account for 86% of the total carried forward expenditure. The top ten value projects comprising significant road and building infrastructure projects account for \$15.7M or 65% of the total value. The balance remaining on the Cockburn Integrated Health and Community Facility project was \$4.7M and \$3.4M was carried forward for the North Lake Rd (Hammond to Kentucky) project.

Also carried forward are outstanding land sales totalling \$4.0M, which once realised, will be transferred into the Land Development and Investment Fund Reserve as per Council's Land Development Strategy.

## **Strategic Plan/Policy Implications**

### **Leading & Listening**

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

### **Budget/Financial Implications**

The 2014/15 adopted Budget will be amended to show \$13,281,518 of opening funds brought forward from the 2013/14 FY and to include the carried forward works and projects expenditure totalling \$24,136,990 and land sales income of \$4,022,727.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

1. 2013/14 Schedule of Carried Forward Works & Projects.
2. Statement of Financial Activity – June 2014 (Final).

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## 16. ENGINEERING AND WORKS DIVISION ISSUES

### 16.1 (OCM 9/10/2014) - MATTERS TO BE NOTED FOR INVESTIGATION - POLICY FOR NEGOTIATION OF DEVELOPER CONTRIBUTIONS (163/006) (M LITTLETON)

#### RECOMMENDATION

That Council note the contents of the report.

#### COUNCIL DECISION

#### Background

At the Ordinary Council Meeting 12 June 2014 under Matters to be Noted for Investigation Without Debate, Cllr Portelli requested the following:

*That a policy be formulated to allow Council to arbitrate on negotiations between the City and any developer that proposes a development that will impact on the community by way of adding to traffic congestion. The aim is to ensure substantial developments are supported by appropriate infrastructure so that road and path upgrades are done in a timely manner to support the extra traffic created.*

#### Submission

N/A

#### Report

Development is controlled by the City's Town Planning Scheme, relevant planning and technical policies and standards as well as other relevant legislation. The City is not the final arbiter and it is important to acknowledge that developers have the right of appeal to SAT. Invariably the City and the developer must work collaboratively to arrive at a fair and reasonable compromise with regards to infrastructure upgrade contributions.

The City, represented by its officers, seeks to get the best outcome on behalf of the community. The City is represented by technical and professional staff with specific skills in engineering, planning, health, environment, building and community services. These Staff have the

best knowledge of the network constraints, the infrastructure, the legislative requirements and any precedent established through other development. They are employed by the City, they work in the best interests of all, and are in the best position to negotiate an agreement which delivers the best outcome.

From an engineering perspective, development impacts on our civil infrastructure in a range of different ways:

- Roads
  - Traffic generation (internal and external)
  - Traffic flow
  - Future networks / growth
  - noise
- Paths – footpaths and cycleways
- Drainage
- Services – street lighting etc.
- Public open space areas including landscaping
- Environmental assessments
- Ground water sources including licence provisions
- facilities

There is much to consider. When considering any development, the City receives countless studies, documents and plans which details the various impacts that the development will have on the existing infrastructure. The City receives traffic impact assessments, noise assessments, drainage detail, service plans, POS landscape plans, marina and waterway plans, environmental assessments and the list goes on. Invariably these assessments support a particular position which is usually favourable to the developer (eg. traffic modelling is not an exact science and relies on a range of assumptions).

Officers use these assessments, their own assessments, peer reviews and their knowledge of the local network and environment to determine what is fair, reasonable and justifiable by way of infrastructure upgrades. This forms the basis of the negotiations. Our officers work hard to ensure that the outcomes of any negotiations are reasonable and consistent with their understanding of the network and they ensure that their position can be substantiated. The City has been very successful in negotiating suitable agreements which have resulted in significant investment in the network as a result.

Often these agreements take years to resolve. For example the City's officers worked with the Perron group for at least 18 months to secure a reasonable agreement on road network upgrades for its recent stage 3 of the Cockburn Gateways Development. The City was clear in its objective and was ultimately able to substantiate its position through the SAT process. Having reached agreement, the WAPC and the PTA further delayed the approval process and the delivery of the works for

very little additional gain (yet the City's infrastructure upgrades were agreed and committed). It is a complex process and the City's staff is its best advocate when negotiating these agreements.

To date, Council has trusted its staff to deliver the best outcomes knowing that it has been well informed through briefings and council reports and understand the general principle of the negotiations taking place through that process. Council is provided clear feedback on transport related issues through Council reports. Periodically, officers provide briefings to Council to provide an update on the progress of the development and often these relate to traffic and network upgrades because more often than not, they are a point of contention. It is also not uncommon for the developer themselves to brief council directly.

You could draw the inference from Councillor Portelli's matter to be noted for investigation that the City's current method of achieving developer contributions has failed. The City of Cockburn deals with significant developments and it has managed to secure significant investment in its infrastructure as a result. The City uses this and other funds as leverage for increased external and government funding to further improve the road network. Council must take a strategic and holistic approach to delivering infrastructure upgrades.

The process of negotiation should be founded on being fair and reasonable and the same is expected from the developer. It is acknowledged that the developer is there to make a return but it should not do so at the expense of the existing community. For larger developments (Port Coogee, Banjup, Cockburn Central, Gateways etc), it is reasonable for infrastructure upgrades to keep pace with the rate of development. The agreement reached over the Banjup development takes these things into consideration.

The terms of that agreement require the intersection with Armadale Road to be completed before the end of this calendar year. Upgrades of Jandakot Road (to a full dual carriageway urban standard road) and Solomon Road have been deferred until the 900<sup>th</sup> lot is developed or 2017 whichever is the sooner. That agreement is structured around the fact that initially the development will gain its sole access from Armadale Road. It is not until the first stage of the development is completed that they will require access onto the City's roads at Jandakot and Solomon. Upgrade of these roads is therefore timed to coincide with the access requirement and the rate of development. The development will not contribute to the broader traffic flow on these roads until a later stage. If Jandakot and Solomon Roads are currently experiencing problems, that is not the result of the development itself.

It is difficult to understand the basis for a policy. Taking into consideration the complexity of the issues and the amount of work that

goes into negotiating and reaching a consensus position with the developer, on what basis would Council then chose to ignore the advice of staff and vary the agreement? If Council is not likely to vary the agreement, why introduce further administrative steps into the process? In any commercial negotiations you would have your expert people, those with the intimate knowledge of both parties position, reach agreement.

It is not recommended to introduce more bureaucracy into a complex process which already takes considerable time to complete. The current process already delivers infrastructure in a timely manner and supports traffic generated by the development which was a key consideration of Cr Portelli. Nothing has been presented which supports a contrary view. A policy is not necessary.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- Development that is soundly balanced between new and existing areas.

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- A safe and efficient transport system.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

N/A

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

N/A

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**16.2 (OCM 9/10/2014) - MATTERS TO BE NOTED FOR INVESTIGATION - TRAFFIC CALMING BARRINGTON STREET WEST (J KIURSKI) (1030029) (ATTACH)****RECOMMENDATION**

That Council:

- (1) notes the outcome of Traffic Management Warrant System (TMWS) to manage the behaviour of vehicle traffic in Barrington Street; and
- (2) develop project proposal, design concept and cost for appropriate traffic treatments on Barrington Street between Stock Road and Rockingham Road for consideration during the 2015/16 budget deliberations.

**COUNCIL DECISION****Background**

At the Ordinary Council Meeting 14 August 2014, Deputy Mayor Carol Reeve-Fowkes requested that a report be prepared under Matters to be noted for Investigation the following:

*The potential traffic calming for Barrington Street (west) to identify opportunities to improve safety for residents, wildlife and motorists.*

*There have been several accidents along the road in last few years and the traffic on Barrington Street has increased as motorists seek to avoid congestion at Rockingham Road/Mayor Road/Stock Road intersections. A traffic count and review for potential safety improvements with the report to be brought back to Council for the OCM October 2014.*

## **Submission**

N/A

## **Report**

The section of Barrington Street between Stock Road and Rockingham Road is classified as a Local Distributor road under the road hierarchy classification of roads within the City of Cockburn. The function of these roads is to collect and distribute traffic from access streets, linking to the major collector roads within the neighbourhood. They can also provide secondary connections direct to the external arterial road network within the residential, industrial and commercial areas.

A preliminary assessment of the current traffic environment has been completed which includes a traffic survey, a review of traffic count data and a review of traffic crash history over the last 5 years particularly on intersections and the section between Stock Road and Rockingham Road. The current Council Policy SEW3 'Local Area Traffic Management' and the "Warrant Criteria and Weightings" have been used to evaluate the request for traffic management devices to manage the behaviour of vehicle traffic in Barrington Street.

Because of significant deference in traffic volume generated through the intersection of Barrington Street and Marvell Avenue, the "Warrant Criteria and Weightings" has been applied for two separated sections; Barrington Street between Rockingham Road and Marvell Avenue and the section between Marvell Avenue and Stock Road.

The traffic count survey completed in the last week of May 2014 shows that the average weekday traffic for the section of Barrington Road between Rockingham Road and Marvell Avenue was 5,261v/d and for the section between Marvell Avenue and Stock Road was 7,281v/d. The traffic speed (expressed as the 85th percentile speed) of both sections is also higher than the 50km/hour posted speed environment (average traffic speed between Marvell Avenue and Stock Road is 58km/h and between Rockingham Road and Marvell Avenue is 64 km/hr). Officers also reviewed the MRWA crash statistics for this section which shows that there were 7 accidents registered for the last 5 years (3 within the section between Marvell Avenue and Stock Road).

Officers subsequently followed the procedure for evaluating the need for the installation of traffic management measures and have assessed other traffic parameters such as: road design and topography, vulnerable road users, major bicycle or pedestrian crossing points, activity generators (school, retail) and amenity factors to confirm an intervention warrant. Both sites scored more than 50 points which

highlights the need for further technical assessment to determine appropriate solutions or traffic calming measures.

The City's officer will need to conduct further investigations to determine the best solutions to calm traffic and reduce a number of accidents within the section of road. These investigations will be completed prior to March 2015 to enable a design concept and budget submission to be prepared for consideration during the 2015/16 budget deliberations. During that process this project will be assessed against other budget requests, requirements and priorities having regard to budget funding limitations.

## **Strategic Plan/Policy Implications**

### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

The cost of the project development and completion of design for proposed solution has been accommodated within the current 2014/15 adopted budget, construction cost will be considered during the 2015/16 financial year budget deliberation.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

1. Barrington Street [Rockingham Rd - Marvell Av] Outcome of TMWS
2. Barrington Street [Marvell Av - Stock Rd] Outcome of TMWS

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 October 2014 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

### **16.3 (OCM 9/10/2014) - JANDAKOT AIRPORT ACCESS ARRANGEMENTS (J MCDONALD / M LITTLETON) (110/001) (ATTACH)**

#### **RECOMMENDATION**

That Council:

- (1) by 31 October 2014, complete negotiations with Jandakot Airport Holding Pty Ltd to fund and deliver an agreed scope of works generally in accordance with the Deed of Agreement attached;
- (2) authorise the CEO to execute an appropriate Deed of Agreement between the City of Cockburn and Jandakot Airport Holdings Pty Ltd to fund and deliver the agreed scope of works as detailed in the Agreement appended; and
- (3) commence actions to undertake the following if an agreement is not reached by the 31 October 2014:
  1. Withdraw approval of the temporary traffic management at the Karel Avenue/Berrigan Drive intersection.
  2. Close the temporary access of Spartan Street at Berrigan Drive.
  3. Advertise the temporary closure of Berrigan Drive at Jandakot Road / Dean Street roundabout and present this matter to future meeting of Council for further consideration.
  4. Lodge an objection on the 2014 Masterplan on the basis that JAH have not provided necessary and promised road network upgrades to facilitate wide scale commercial, industrial and retail development on the site.

**COUNCIL DECISION****Background**

Jandakot Airport is one of the busiest airfields in Australia and a substantial area, 156 hectares, of previously vacant land within the airport boundary is now being developed for non-aviation commercial purposes. Whilst the City is generally supportive of that development that is conditional on the airport adequately managing the associated amenity, environmental and transport related impacts that development will generate (*Refer Minute No. 4064 – OCM 8/10/2009*).

The transport related impacts of the airport are a particular concern for the City and road users because of the increasing volume of traffic that is being generated by the airport and the fact that the airport still only has a single primary access point via Karel Avenue. A secondary left-in/left out only connection of Spartan Street to Berrigan Drive has been granted as a temporary access until Orion Road connects to the Berrigan Drive/Jandakot Road intersection. The City has also acceded to allowing a signalman to provide traffic control during the PM peak period at the intersection Karel /Berrigan (north) to improve accessibility for JAH.

This report considers the status of road improvements planned to improve access to/from the airport and seeks to reaffirm Council's position of the access arrangements to help guide the airport's planning and decision making processes.

**Submission**

Nil.

**Report****Jandakot Airport Master Plan 2009**

In March 2010, the Federal Minister for Infrastructure, Transport, Regional Development and Local Government approved the Jandakot Airport Master Plan 2009 (the Master Plan) developed by Jandakot Airport Holdings (JAH). In Section 9.0 Road Access System of the Master plan a suite of road improvements to address the impacts of the traffic generated by the airport developments onto the surrounding road network were identified.

Those road improvements included:

1. Berrigan Drive (Kwinana Freeway to Jandakot Road)
  - Widen existing two-lane divided road to four-lanes divided
2. Berrigan Drive North (Karel Avenue to Jandakot Road)
  - Upgrade Berrigan Drive/Karel Avenue intersection from a roundabout to traffic signal control.
  - Erect *Local* Traffic Only signs at either end of this road section.
  - If *necessary*, supplement the above with traffic calming treatments.
3. East Link (Airport to Ranford Road)
  - Construct a new 2-lane arterial road with traffic signal control at Ranford Road.
4. Karel Avenue (Farrington Road to Roe Highway)
  - Upgrade of approach and departure lanes to Roe Highway from two-lane divided road to four-lanes divided.
5. Karel Avenue (Roe Highway to Berrigan Drive)
  - Widen existing two-lane divided road to four-lanes divided.
6. South Link (Extension of Orion Road to Berrigan Drive/Dean Road/Jandakot Road intersection)
  - Construct a new arterial road by extending Orion Road to Berrigan Drive/Jandakot Road and installing suitable traffic control (multi-lane roundabout or traffic signals).

This includes realigning a section of Berrigan Drive just north of Jandakot Road so that a T-intersection is formed, with the South Link being the continuing road and Berrigan Drive the terminating road.

These access arrangements are discussed in Section 9.0 Road Access System of the Master Plan, which is included as Attachment 1.

#### Council position on 2009 Master Plan

It is worth revisiting the council report detail from the Ordinary Council Meeting of 8 October 2009 in relation to traffic and transport as follows:

Proposed access to the airport includes the following connections:

- Access from Berrigan Drive or Karel Avenue via the Karel Avenue extension (existing);

- South Link - this road is proposed to connect from Karel Avenue to Jandakot Road and Berrigan Drive at the existing Jandakot Road/Berrigan Drive/Dean Road intersection;
- Proposed East Link - The connection of this road outside of the airport boundary will be determined in discussions with the State Government and local governments.

In respect of the proposed access and development, there are a number of issues in terms of the traffic, transport and road system which need to be addressed. These include:

1. Karel Avenue needs to be a full four lanes all the way between Farrington Road and the airport to accommodate the increased traffic and to seamlessly tie in with the existing four lanes north of Farrington Road. The railway and Roe Highway bridges will need to be widened to accommodate this and a pedestrian/cycling path link retained.
2. Berrigan Drive South will need to be widened to four lanes between the South Link Road and Kwinana Freeway to accommodate the additional traffic generated by the airport development.
3. The airport roads are considered public roads, and they should integrate with and complement the existing surrounding road system. In this respect, the new South Link Road through the airport connecting Karel Avenue and Berrigan Drive South should replace Berrigan Drive North as a District Distributor A in the City's road hierarchy. Consequently, it should be a four lane road through the airport with limited and controlled property access, and encouraged as the priority route.
4. Through traffic needs to be discouraged from using Berrigan Drive North, and its connections to Karel Avenue and the South Link Road need to take this into account.
5. The connections at South Link Road/Dean Road/Jandakot Road/Berrigan Drive will require detailed consideration, particularly as a large number of roads converge at this interchange. Private land acquisition may be required for provision and construction of South Link Road connection (outside of the airport). Should the feasibility of land acquisition become an issue, alternative options for this road connection will need to be identified as part of this Master Plan.
6. Fraser Road is an unmade road. It is shown as carrying traffic under the airport's development plan. If this road is required to

facilitate the airport's road access system, then it needs to be constructed as part of the airport's road development program.

7. As the airport may be subject to emergency situations, ready, separate and controlled full turn emergency access should be provided to and from the adjacent Roe Highway away from the regular interchanges. This would be appropriate with the new Fiona Stanley Hospital being located off Roe Highway. This access point could also be utilised by other vehicles in order to reduce traffic impacts on the regular interchanges.
8. A separate study is required to model the traffic, access requirements and upgrades to accommodate the traffic generation and transport impacts on the road feeder system and the freeway/highway system. The upgrade options for Karel Avenue, Berrigan Drive, Jandakot Road, freeway accesses etc (including treatments to encourage Karel Avenue traffic to drive through the airport rather than using Berrigan Drive North) can be modelled for various scenarios to identify the most appropriate treatments. A traffic model of the area was set up by the Roe7 Alliance and this model, if available, could be readily calibrated and modified to reflect the current traffic flow characteristics and the scenarios required for the airport development.
9. The airport roads function as public roads and consequently should be designed and constructed to a minimum standard, particularly for public safety. They should be designed and constructed to the minimum requirements and procedures set by the new Local Government Guidelines for Subdivisional Development, published by the Institute of Public Works Engineering Australia (WA Division). Such should also be consistent with the City's Guidelines and Standards for the Design, Construction and Handover of Subdivision within the Municipality.
10. The subdivision process and civil works will need to be guided and monitored to ensure compliance with drawings, standards and the guidelines, and appropriate records and conformance declarations kept for any subsequent legal requirements, on-going asset management or should the care and management of the infrastructure be handed over into the future;
11. The cost of all required civil upgrade works to external infrastructure as a result of the airport's expansion is considered the responsibility of JAH to fund.

Council considered a report (*Minute No. 4064 refers*) about the Master Plan on 8 October 2009. The key recommendations from that report that relate to the airport access arrangements are:

- “2. *All specific requirements in relation to traffic and transport infrastructure as discussed under the ‘Traffic and Transport Issues’ section of the officer’s report being undertaken by JAH;*
3. *JAH being responsible for the constructing/upgrading of all the existing infrastructure outside of the airport site identified as being required to facilitate the Master Plan;*
9. *A formal protocol being established by way of Memorandum of Understanding between the City and JAH which requires involvement by the City in terms of the preparation and processing of structure plans, the processing of subdivision and development applications including parks and landscaping design and construction standards for infrastructure”*

#### What has changed?

The initial Masterplan was premised on a land-use which would have seen the development generate approximately 37,000 vehicles per day. Since the Master Plan was developed JAH have reviewed and revised the forecast development gross floor area and workforce population figures so that the total traffic generation of the airport has been reduced to approximately 24,000 vehicles per day. The suite of road network upgrades contemplated in 2009 was necessary as a result of the higher traffic forecast.

In 2009 the City believed that the further development of Jandakot Airport would be the most substantial development in this area and as a result, a significant contributor to network demands. Since 2009 the Banjup development has been approved which will also contribute to network demand in this area. On that basis it is reasonable to consider and more proportionate basis for contributions to some of road network upgrades identified.

The suite of road network upgrades contemplated in 2009 is expansive and expensive and it is reasonable for council to give some further consideration to how they will be delivered and by when. Under normal circumstances Council agrees to have road network upgrades completed that generally reflect the rate of development. This essentially ensures that there is sufficient capacity within the network to cater for traffic generated by the development.

The internal roads within the Jandakot Airport precinct are not public roads and the City has no effective control over them. That means that how these internal roads operate and their ultimate function are entirely within the control of the management of JAH. Whilst the development of the land-use will to a large degree dictate the ultimate road form, they will be unlikely to consider issues such as design standards and access control which would ultimately improve the broader road network functionality. In fact JAH have been strident in their view that their internal network should not serve a broader regional purpose despite the fact that they have actively sought to link the network to major roads through their east and south link proposals.

Whilst the City's recommendation in October 2009 clearly stipulated that JAH will be responsible to construct and fund the infrastructure upgrades identified, JAH dispute the fact that they should be responsible for funding all upgrades. As we have little in the way of planning control to bring this matter to a conclusion, we have had to enter into lengthy negotiations to reach agreement.

Works have recently commenced on the widening of the Kwinana Freeway between Farrington Road and Armadale Road. Officers believe that the traffic volumes in Berrigan Drive (north) are being distorted by a tendency for vehicles to rat run from Roe Highway to Kwinana Freeway (using Karel Avenue and Berrigan Drive north) due to the lack of capacity at the merge point of Roe Highway and Kwinana Freeway. Once this works is completed (in 2015), officers believe that the traffic volumes will substantially reduce (by about 6,000vpd) in Berrigan Drive north.

The 2009 Masterplan and more specifically the road network modelling and associated upgrades were predicated on development at Jandakot Airport being completed within a 5 year period. This development timeframe was ambitious.

It would appear that the only level of influence that the City can exert on JAH is on how it connects with the external road network as these roads are under the care, control and management of the City.

#### Berrigan Drive (north)

Over the years the City has been presented with varying traffic modelling data and many different design options for intersections. Throughout this process it has been common for the City to be presented with options that would maintain Berrigan Drive, from Jandakot Road to Karel Avenue, as a major route carrying approximately 15,000vpd which contradicts Council's view of that road as expressed in the 2009 report.

Given the lack of planning control over the leased land and our inability to dictate the manner in which Orion Road (or any other road for that matter) is designed and operated, it is not considered appropriate to seek to downgrade Berrigan Drive at this point in time. Maintaining accessibility with Berrigan Drive will improve permeability in this precinct and will give the city greater flexibility to manage traffic flow in future. The City's challenge however will be to ensure that traffic growth on Berrigan Drive is limited. Given the likely reduction in traffic volumes once the freeway widening works are completed and the recommended road network layout proposed in this report, officers believe that that will be achieved.

### Status of planned road improvements

Since 2009 the City has been working with JAH to progress the implementation of the suite of road works. The City has been prepared to consider the staging of the road network upgrades. Over the years it has been presented with varying traffic modelling data and road design options for intersections prepared by a number of consultants. No significant progress has been made on any of the planned road improvements outside the airport boundary. However, development of land for non-aviation commercial use has progressed as can be seen by comparing the aerial photographs from January 2009 and February 2014 that are included as Attachment 2.

After considerable and protracted negotiations the City and JAH have agreed on a road network layout and a priority scope of works as shown as Option 1 and is included as Attachment 3 (it may-be of some interest to some that this network is in generally the same form that was endorsed in 2009). In May, the City provided JAH with agreement in-principle (AIP) to support their application to MRWA for the installation of traffic signals at 3 intersections on the surrounding road network to facilitate this scope (refer Attachment 4) including:

1. Berrigan Drive/Dean Road/Jandakot Road/Orion Road (South Link).
2. Berrigan Drive/Orion Road T intersection.
3. Berrigan Drive/Karel Avenue intersection.

MRWA have subsequently responded to this AIP and have supported 2 of the signalised intersections with a further review of the other once the freeway widening works are completed (refer Attachment 5).

The proposed scope of works therefore is as follows:

1. The extension of Orion Road from the intersection of Berrigan Drive/Jandakot Road/Dean Road to the constructed portion of Orion Road within the Jandakot Airport boundary via the

designated road reserve (Orion Road Extension), such works to include but not be limited to:

- i. constructing the Orion Road Extension as a single carriageway road and connecting the Orion Road Extension with the external road network via a signalised intersection; and
  - ii. purchasing sufficient land to construct the Orion Road Extension as a dual carriageway and ceding that land to the Crown as road land.
2. Signalising and constructing the intersection of Jandakot Road/Berrigan Drive (south)/Dean Road/Orion Road generally in accordance with the design concept endorsed by the City and MRWA.
  3. Constructing the intersection of Berrigan Drive (north)/Orion Road generally in accordance with the design concept endorsed by the City and MRWA and providing appropriate signage control to the intersection in accordance with MRWA's approval.
  4. Signalising and constructing the intersection of Berrigan Drive (north)/Karel Avenue generally in accordance with the design concept endorsed by the City and MRWA.

This scope of works has been formulated into a Deed of Agreement which is attached at Attachment 6. The Deed specifies the timeframes for completion of the works (generally 12 months from execution of the Deed) and a range of performance conditions to be achieved. It is recommended that officers proceed to finalise and execute this agreement without further delays.

#### Further Network Upgrades

This Deed outlines the priority works required to accommodate the current development of the non-aviation land within the federal government lease and should not be considered final. Development of the airport land continues and the City will need to ensure that road network upgrades continue to keep pace with development demand. That may be harder to achieve with the reform outcome however we will need to continue to maintain a professional relationship with JAH.

Progress has been made on three projects which will further support the JAH precinct as follows:

- The City has prioritised the widening of the section of Berrigan Drive, from the Kwinana Freeway to Jandakot Road and has proposed to commence work this financial year. There is no current commitment for JAH to contribute to this road upgrade beyond the intersection works contained in the Deed of Agreement.

- Planning for the East Link has been well progressed by the Department of Planning (DoP), who have completed a study identifying a preferred road alignment. DoP are progressing with an amendment to the Metropolitan Region Scheme to formalise the road reservation needed for that road alignment. There is currently no commitment to fund this road.
- JAH extended Orion Road from Marriott Road to the airport's southern boundary (half-way to the Berrigan Dve/Jandakot Rd intersection) in the third quarter of 2012. The extension to the external road network is included in the Deed of Agreement.

#### Spartan Street Future, K-Mart Distribution Centre & 2014 Masterplan

Whilst the City has provided support for the temporary left in / left out access at Spartan Street, it is clear that JAH intend to pursue retention of this link in perpetuity. The 2014 Masterplan identifies this link as forming part of the broader road network supporting the development. It is important that we be clear, this link is approved as a temporary link only until the south link road is completed. That approval however can be revisited at any time and can also be withdrawn at Councils discretion.

From a technical viewpoint, on the basis that all other roads contained within the Deed of Agreement are constructed, there is no major objection to retaining Spartan Street in its current form (ie. left in / left out).

Recently, the City has been asked to comment on an MDP for a K-Mart distribution centre which proposes to derive an access point from Spartan Street (this matter is further considered in this OCM agenda at Item 14.4). Until the south link road and other associated works contained in the Deed are completed, the City should not support this development as the road network cannot support major industrial, commercial and retail land use which generates significant consumer, employment and freight traffic demand. It will also be the City's intention to withdraw support for the Spartan Street access from Berrigan Drive meaning that this distribution centre (should it be approved) will need to gain access from the internal road network provided by JAH. That outcome will clearly create additional pressure on the internal network.

JAH have also advertised their 2014 Masterplan for comment. This matter will be presented to council for consideration at the November OCM. It is important however to state upfront that without the necessary infrastructure being provided, we cannot support further development of this site.

### No Agreement

The City has spent considerable time and resources dealing with JAH on the access arrangements for the airport, including officer reviews of submitted reports and drawings as well as commissioning occasional peer reviews by external consultants.

The City has been very patient in its dealings with JAH and extended a generous level of goodwill whilst waiting on the implementation of the planned road improvements, by permitting them to:

- operate temporary (since August 2011) traffic management at the Karel Avenue/Berrigan Drive intersection, in weekday PM peak hours to minimise the delays to exiting airport traffic, and
- construct a temporary left-in/left-out access to Berrigan Drive (at Spartan Road). This access was to be available until the south link road was provided and its future ultimately was to be considered as part of the 2014 Masterplan.

Development of the airport land has been progressing and the need for the road improvements is now overdue and it is imperative for JAH to commit to completing them in a timely manner.

If JAH refuse to commit to an acceptable arrangement that results in the planned road improvements being completed in a timely manner, then it is reasonable that the City withdraw its goodwill by cancelling the approval for the 'temporary' traffic management at the Karel Avenue/Berrigan Drive intersection and close the connection of Spartan Street to Berrigan Drive. Officers have also considered more substantive amendments to the network – i.e. the closure of Berrigan Drive at Jandakot Road roundabout – in an effort to prioritise our residents and seek to buffer them from the Jandakot Airport operations as well as the rat running traffic created by the ineffective primary network (Roe Highway and Kwinana Freeway).

### **Strategic Plan/Policy Implications**

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- A safe and efficient transport system.
- A defined freight transport network.

#### **Budget/Financial Implications**

As stated in the Master Plan.

### **Legal Implications**

Jandakot Airport is a Federal Government property that is leased to Jandakot Airport Holdings Pty Ltd. This means that neither the City or State Government's recommendations about the Master Plan are binding or enforceable, unless that is approved by the Federal Government.

To address this issue, a draft legal agreement has been prepared by the City's lawyers that will require JAH to commit to the planned road improvements within a specific time frame.

### **Community Consultation**

N/A

### **Attachment(s)**

1. Section 9.0 Road Access System - Jandakot Airport Master Plan 2009.
2. Aerial photographs of Jandakot Airport from January 2009 and February 2014.
3. Option 1 – Agreed Road Network layout.
4. City of Cockburn AIP
5. MRWA response
6. Deed of Agreement

### **Advice to Proponent(s)/Submissioners**

The Proponents have been advised that this matter is to be considered at the 9 October 2014 Ordinary Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.4 (OCM 9/10/2014) - CITY OF COCKBURN 2014-2017 TRAVELSMART PLAN (142/007) (K BOSCHETTI) (ATTACH)**

### **RECOMMENDATION**

That Council:

- (1) adopt the 2014-2017 TravelSmart Plan;
- (2) proceed to implement actions recommended by the TravelSmart Action Plan.

**COUNCIL DECISION**

**Background**

The City of Cockburn has previously implemented two TravelSmart Plans – one focusing on the City employees and a broader plan targeting residents. These plans encouraged use of active travel modes for commuter trips over single vehicle car trips for work day commuting.

In order to consolidate and build on the early successes of these two Plans, the new TravelSmart Plan 2014-17 outlines actions that targets the City of Cockburn as a whole, for local workplaces and schools. This plan proactively tackles the hot community issues of traffic congestion in our City and is a supporting document for the new Integrated Transport Plan and the City's Strategic Community Plan 2012-22.

Active travel and trip chaining (trips that incorporate different active modes) builds in much needed physical activity minutes into each day for those who opt for these modes. In this way active travel contributes to the reduction of obesity and overweight issues for our community – which is currently measured at 74% of adults of the City's population.

Other additional benefits of inspiring more active travel in our City include reduction in the emissions our vehicles produce, positive impacts on the family budget, a sense of community connectedness and place making and an enthusiasm around sustainability themes for many school communities who are engaged in the TravelSmart to School program.

**Submission**

NA

**Report**

The City's existing TravelSmart Program has a reviewed focus and has aims to see:

- A reduction in the number of private vehicle trips made in the City by 10% by:
  - \* increasing the number of walking, riding and public transport trips made, and

- Improve the recall of TravelSmart messages among our community by 20% from 2013 to 2017.

This Plan looks to build and foster relationships with local schools as community hubs, workplaces, young people and Seniors and also offer a suite of initiatives for City of Cockburn staff.

With a mix of behaviour change programs, skills based programs, provision of new infrastructure and an extensive events calendar, the TravelSmart Program will look to normalise riding, walking and public transport instead of driving for the work day commute and travel for short trips.

The new Plan will capitalise on the successful engagement of 10,000 households who took part in the Your Move program over the last twelve months using resources that have been handed over such as the participant database and printed publications.

The TravelSmart Program will be run by the TravelSmart officer for the duration of the Plan.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- Reduction in energy dependency and greenhouse gas emissions within our City.

#### **Community & Lifestyle**

- Promotion of active and healthy communities.

#### **Environment & Sustainability**

- A community that uses resources in a sustainable manner.

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- Facilitate and promote healthy transport opportunities.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

The financial implications of the TravelSmart Plan do not involve significant additional expenditure. The TravelSmart Officer position represents a cost of about \$100,000 per year and reflects the need for the City to focus on traffic and congestion issues from a proactive point of view, promoting the benefits of TravelSmart initiatives. The TravelSmart Officers position is currently budgeted until June 2015 and would be extended to June 2017 to coincide with the duration of this Plan.

The current financial year program budget for the TravelSmart Program is about \$60,000 and this would look to be continued for the duration of the plan.

### **Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

City of Cockburn 2014-2017 TravelSmart Plan

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

- 16.5 (OCM 9/10/2014) - LOT 6 JUNCTION BOULEVARD, COCKBURN CENTRAL - TEMPORARY CLOSURE OF PORTION OF SLEEPER LANE, SIX PARKING BAYS ON LINKAGE AVENUE, TWO PARKING ON JUNCTION BOULEVARD AND TWO PARKING BAYS ON SIGNAL TERRACE (ES/R/002) (J KIURSKI) (ATTACH)**

#### **RECOMMENDATION**

That Council in accordance with Section 3.50 of the Local Government Act 1995 institutes a temporary closure of portion of Sleeper Lane, six parking bays on Linkage Avenue, two parking bays on Junction Boulevard and two parking bays on Signal Terrace, Cockburn Central for a period of 18 months commencing September 2014 to April 2016 subject to:

1. There being no substantial objection received as a result of advertising in a local newspaper.
2. There being no substantial objection from service authorities, emergency services or adjoining owners.
3. The developer engaging and appropriately accredited traffic management contractor to submit a certified traffic management plan to monitor and control traffic movement due to the closure.
4. The developer will provided the parking bays for its contractors on the Lot 5 site, which will stopped any trades people from using the existing on road parking bays.
5. The developer will install temporary perimeter fencing to the rear of car parking bays to Linkage Avenue, Junction Boulevard and Signal Terrace as detailed on the site fencing plan. The fence be positioned and of a height and form of construction that does not create a traffic hazard for motorists, cyclists and pedestrians, including mobility impaired persons, and including not blocking currently available lines of sight at intersections.
6. All works on existing City infrastructure (roads, footpaths, drainage, parks or verges) completed and reinstated in accordance with the "Public Utilities Code of Practice 2000", "Restoration and Reinstatement Specification for Local Government 2002" and the City of Cockburn "Excavation Reinstatement Standards 2002" as a minimum.
7. The developer to pay an amount of \$120,000 to any damage to the City's infrastructure prior the closure of any parking bays and the pedestrian path along Linkage Avenue, Junction Boulevard and Signal Terrace and portion of Sleeper Lane.

**COUNCIL DECISION**

## **Background**

Lot 6 Junction Boulevard Cockburn Central development was granted planning approval and building licence for 106 multiple dwellings and 6 commercial units and construction work has commenced. The development is being undertaken by Australand Holdings Ltd.

The proposed development on Lot 6 Junction Boulevard Cockburn Central is surrounded by Slipper Lane, Linkage Avenue, Junction Boulevard and Signal Terrace.

## **Submission**

Australand Holdings Ltd has requested Council to implements procedures to temporarily closure of portion of Sleeper Lane, six parking bays on Linkage Avenue, two parking bays on Junction Boulevard and two parking bays on Signal Terrace, for a period of 18 months during the construction of the 106 multiple dwellings & 6 commercial units on Lot 6 Junction Boulevard, Cockburn Central.

## **Report**

During the construction activities of (Lot 6) Signal Tce, Cockburn Central the footpath and parking bay closure can be supported for the below reasons:

The footpath and parking bays surrounding Lot 6 Signal Terrace, abutting the worksite will remain closed until the completion of works and appropriate signage will be installed to direct pedestrians to the other side of Junction Boulevard, Linkage Avenue and Signal Terrace.

Australand will maintain the footpath and parking bays area and will make good any damage caused by construction vehicles on completion of the project. The footpath and parking bays closure will have minimum impact on pedestrian movements as pedestrians will be able to use the existing footpath on northern side of Junction Boulevard, eastern side of Linkage Avenue and southern side of Signal Terrace.

Australand will install temporary perimeter fencing to the road side of the car parking bays as detailed on the site fencing plan, the temporary fence will be mesh panel fencing system and the remainder of the site will be surrounded by a combination of solid and mesh fencing system.

Australand have provided an additional 111 bays for its contractors on the Lot 5 site. This will stop any trades people using the existing car bays. Cockburn Central will therefore lose 10 car parking bays due to the development construction.

Australand has appointed a certified traffic management contractor (Altus Traffic) to monitor the impact of the footpath closure and access arrangement for the site. Altus Traffic has already submitted a traffic management plan, which is in line with Australian Standards and Main Roads field guidelines.

The proposal is for eighteen (18) months period and with appropriate traffic management controls in place, including road barriers, signage and protective surfaces covering public footpath and parking area, the closure will not create any undue congestion and impact on surrounding land uses. Advance warning signs will also be installed and an advice of the proposed closure will be placed in both the local newspaper and West Australian newspaper prior to the closure.

All works on existing City infrastructure (roads, footpaths, drainage, parks or verges) completed and reinstated in accordance with the "Public Utilities Code of Practice 2000", "Restoration and Reinstatement Specification for Local Government 2002" and the City of Cockburn "Excavation Reinstatement Standards 2002" as a minimum.

Australand agreed to pay an amount of \$120,000 for any damage to the City's infrastructure prior the closure of any parking bays and the pedestrian path along Linkage Avenue, Junction Boulevard and Signal Terrace and portion of Sleeper Lane.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

All costs to the closure will be covered by the Australand Holdings Ltd.

### **Legal Implications**

Section 3.50 of the Local Government Act.

### **Community Consultation**

To be advertised in a local newspaper and service authorities, emergency services and adjoining owners advised.

### **Attachment(s)**

Site Fencing Plan

### **Advice to Proponent(s)/Submissioners**

Australand Holdings Ltd have been advised that the matter will be considered by Council at the 9 October 2014 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.6 (OCM 9/10/2014) - CITY OF COCKBURN 2014-2020 ROAD SAFETY STRATEGY (163/007) (J KIURSKI) (ATTACH)**

### **RECOMMENDATION**

That Council

- (1) adopt the City of Cockburn 2014-2020 Road Safety Strategy (RSS); and
- (2) proceed to implement the short-term actions and plan for the implementation of the medium actions recommended by the RSS Action Plan.

### **COUNCIL DECISION**

### **Background**

The City's Strategic Community Plan defines the vision for the City's movement network as "a robust, safe and integrated transport network that meets people and industry needs while minimising environmental impacts." In 2013 the City revised the existing District Traffic Study, and developed a new plan to provide the foundation for the future road network planning of the City. The DTS highlighted the need for a more sustainable approach to reduce congestion within road networks.

To increase the efficiency and effectiveness of the City's transport network and to move people and goods via all modes of transport within and through the municipality, in 2014 the City developed the Integrated Transport Plan. The ITP advanced the number of actions to reduce the number of vehicles on roads, raised the community awareness of other transport alternatives and considered the alternatives to improve safety within the road network.

The City is aware of the number of crashes that occur within the City and recognizes the negative and significant impact of road trauma on families and the broader community. There were 8,208 crashes on Cockburn Roads during the 2009 – 2013 period; 21 fatal, 427 hospital, 1,200 medical and 6,500 property damage crashes.

To define how to bring on changes to our road safety practices and culture, and eliminates death and serious injury on our roads the City's engineering service completed a review of existing crash histories and road safety issues across City, identified gaps and recommended the safety initiatives.

The engineering service completed the Road Safety Strategy which is aligned with the National Road Safety Strategy and Western Australia's Towards Zero – Road Safety Strategy, based on the "Safe System Approach" and is now presented to Council for formal adoption, and is included in the attachment.

### **Submission**

N/A

### **Report**

In line with the Strategic Community Plan, Council is aimed at bringing on changes to our road safety practices and culture, and eliminates death and serious injury on our roads. In doing so it will:

- Develop a safe road network system.
- Embed the 'Safe System Approach' into the Council and community
- Provide assurance to the government and the community that we are achieving results.

The City of Cockburn plans to use the 'Safe System Approach' to reduce road trauma in the municipality. The 'Safe System Approach', is based on approaches used in Sweden's 'Vision Zero' and the Netherlands 'Sustainable Safety', uses three main themes to promote a reduction in road crashes and the incidence and severity of associated road trauma:

- Safer Roads and Roadsides
- Safer Vehicles

- Safer Road Users.

The City of Cockburn Road Safety Strategy will draw on these elements to identify gaps and areas of improvement to create a safe system for all road users, with the City taking a lead role in coordinating and providing a 'Safe System Approach' for everyone who lives, works, learns, spends leisure time or travels within or through City.

The reduction in road crashes will be achieved through the following approaches:

- Engineering - keeping road and roadside infrastructure and roadside environment at a safe standard.
- Education - informing road users of road safety and awareness of risk.
- Encouragement - encouraging the use and ownership of safe vehicles and to be a responsible road user.

#### Road Safety Issues of Today

A review of the Crash Data 2009-2013 composed by WA Police and MRWA, and the outcome of the Collaborative Map public survey held in August 2013, identified the number of traffic and road safety issues within the City. The main issues identified are:

- Speeding in residential streets and through school zones
- Aggressive driving 'hoon' driving
- Road network congestion
- 'Black Spot' areas
- Safety for walking and cycling
- Pedestrian and school crossing
- Drivers driving under the influence of alcohol and other drugs
- Provision of pedestrian and cycling paths to key destinations
- Road crossing facility
- Parking and walking around school area.
- Promote and encourage safe cycling practices
- Visibility of motorcycles
- Provision of accessible paths for vision impaired and/or physically impaired people
- Large number of crashes

The road crashes analyses within this document aim to identify patterns and emerging trends in road trauma across the City of Cockburn, in order to target issues of concern and relevant short-term and medium-term solutions to help increase safety on our roads, for all road-users.

There were 21 fatal crashes and 427 serious injuries on Cockburn's roads during the 2009 - 2013 period. During this period the main road

users group involved in serious and fatal crashes were drivers (90%) and pedestrians (3.5%) and most of the crashes took place in 60km/h and 70km/h speed zones.

Although the number of pedestrians (3.87%) and bicyclist (1.46%) is lower in comparison with other road users in the five year period 2009 – 2013, there were 6 fatalities involving pedestrians and 1 fatality was a cyclist. The highest risk group is the 25 to 39 year olds; they accounted for 29.6% of crashes in Cockburn.

The crashes by road type have been analysed and an outcome of this is that there were 1,938 road related crashes on Primary and Regional Distributor roads, 3,969 road crashes on District and Local Distributor and 1,853 crashes on Local Access roads during the period of 2009 – 2013.

In comparison with other regional Councils such as Melville, Fremantle, Kwinana and Rockingham, the data shows that the City of Melville has a slightly larger number of crashes than the City of Cockburn, but Cockburn has a higher number of fatalities than other LG areas.

The comparison based on crashes per 10,000 population shows that a large number of crashes in the category of hospital, medical and property damage are in the Fremantle area, but the fatality crashes per 10,000 population is larger in the City of Rockingham. The City of Cockburn has a similar road trauma profile to the City of Melville.

#### Safe System Approach

The 'Safe System' views the road transport system holistically by seeking to manage the interaction between road users, roads and roadsides, travel speeds and vehicles. The 'Safe System' recognizes it is probably not possible to prevent all crashes but aims to prevent those that result in death and serious injury.

The Western Australia Road Safety Strategy 'Towards Zero', sets an ambitious target of 11,000 fewer deaths and serious injuries by 2020.

The City of Cockburn Strategy is about creating the environment, procedures, systems and tools to make the strategic objective a reality. It complements the State Government's Road Safety Strategy with initiatives and actions that will enhance and build capability to deliver the Towards Zero outcomes and targets.

The City's Safe System comprises three key principles: Safer Roads and Safe Roadsides, Safer Vehicles and Safer Road Users.

- Safer Roads and Safe Roadsides**
  - Implementing appropriate infrastructure treatments, maintaining existing infrastructure to the required standards and introducing new initiatives.
  - Supporting programs (such as installation of red light and speed cameras, enforcing of school speed limits, enforcing of speed limits in built-up areas).
  
- Safer Vehicles**
  - Provide targeted information about safer vehicles to a range of groups in the community.
  - Provide and promote enhanced and up-to date vehicle safety features, via information on vehicle safety features and associated benefits, and information relevant to learner motorcyclists.
  
- Safe Road Users**
  - Engage all road users through targeted communication and tailored education and engineering programs.

By developing the RSS, Council will support safety programs delivered by State and Federal Government and place an action plan to improve the road safety of residents of the City of Cockburn.

### **Strategic Plan/Policy Implications**

#### **Demographic Planning**

- To ensure the planning of the City is based on an approach that has the potential to achieve high levels of convenience and prosperity for its citizens.
- To ensure development will enhance the levels of amenity currently enjoyed by the community.

#### **Transport Optimisation**

- To ensure the City develops a transport network that provides maximum utility for its users, while minimizing environmental and social impacts.
- To construct and maintain roads which are convenient and safe for vehicles, cyclists and pedestrians.

### **Budget/Financial Implications**

The cost of the RSS development has been accommodated within the current 2014/15 adopted budget.

Any actions arising will be considered as part of the Budget deliberations.

**Legal Implications**

N/A

**Community Consultation**

N/A

**Attachment(s)**

City of Cockburn 2014-2020 Road Safety Strategy

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**16.7 (OCM 9/10/2014) - 2014-2024 PUBLIC OPEN SPACE STRATEGY (A LEES) (146/004) (ATTACH)****RECOMMENDATION**

That Council adopt the 2014-2024 Public Open Space Strategy.

**COUNCIL DECISION****Background**

The City manages approximately 577 hectares of public open space (POS), including streetscapes. POS is the most visible asset the City is responsible for providing, managing and maintaining, and provides a key opportunity for communicating the City's vision and values. POS provides a number of different services, including opportunities for social and recreational pursuits, community development, improved amenity, green spaces and ecological improvement.

The City has developed a Public Open Space Strategy in order to strategically plan the City's current and future POS management requirements.

The POS Strategy complements the City's Natural Area Management Strategy, and the two documents contain the information previously held within the City's Greening Plan 1999, which has now been superseded by these documents.

### **Submission**

N/A

### **Report**

Public Open Space (POS) is the most visible asset the City is responsible for providing, managing and maintaining and provides a key opportunity for communicating the City's vision and values. POS provides a number of different services, including opportunities for social and recreational pursuits, community development, improved amenity, green spaces and ecological functions.

The City manages over 300 parks and 36,173 street trees along 878km of roads and this incredible responsibility necessitates a strategic approach. For the purposes of this strategy, POS is not confined to a traditional understanding, but includes roadside or streetscape areas, regional parks and foreshore reserves

The City has developed this strategy in order to strategically manage these assets for the community now and into the future. This Strategy sits within the City's broader framework of corporate strategic planning, ensuring alignment with the organisation's vision and values and is cognisant of state and local planning frameworks and regulations, which will guide the sustainable management of POS.

POS contains a number of different values, which must be understood to ensure maximum benefit for the community. The City combines these values with an understanding of trends – demographic, health, population, recreation and other – to support the creation and maintenance of community assets, which are accessible, sustainable, functional and well utilised. Because of this, POS management must be flexible in order to respond to the many factors which affect it.

The Action Plan contained within this document, enables a tangible approach to achieving the Strategy's vision, values and objectives. They are classed according to time frames and costs, which will guide future planning.

As well as outlining a classification of the City's streetscapes, this Strategy highlights the network of roads and streetscapes, which are under active management. This Strategy outlines the level of service

per annum for key activities as well as a comprehensive inventory of the City's POS.

### **Strategic Plan/Policy Implications**

#### **Community & Lifestyle**

- Communities that are connected, inclusive and promote intergenerational opportunities.

#### **A Prosperous City**

- Creation and promotion of opportunities for destination based leisure and tourism facilities.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

### **Budget/Financial Implications**

Any actions arising will be considered as part of the Budget deliberations.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

2014–2024 Public Open Space Strategy.

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

N/A.

**16.8 (OCM 9/10/2014) - COOGEE BEACH ECO SHARK BARRIER  
(064/030) (D VICKERY) (ATTACH)**

**RECOMMENDATION**

That Council:

- (1) finalises negotiations with Eco Shark Barriers Pty Ltd for the continuation of the trial for a 3 year period from November 2014 to November 2017 for a sum of \$85,000 per annum funded on a pro-rata basis, quarterly in advance, and on the basis that Eco Shark Barrier P/L will:
  - Retain responsibility for installation, management, insurance, cleaning and monitoring of the barrier for the entire period of the trial.
  - Provide appropriate certification for the product and any modification to the barrier during the course of the trial.
  - Retain public liability insurance to the value of \$20,000,000 for the duration of the trial.
  - Provide an bi-annual report (in March and September each year) detailing the impact of coastal processes on the beach environment.
  - Remove the barrier, anchor piles, anchor chains and any other associated product at the end of the trial period if no alternative arrangements have been made with the City.
- (2) applies for and retains necessary approvals and licenses in the name of the City from the Department of Lands and the Department of Transport for the duration of the trial; and
- (3) allocate funding of:
  1. an additional sum of \$45,000 via the 2014/15 FY mid-year budget review; and
  2. \$85,000 per annum for the 2015/16 and 2016/17 FY budgets.

for this project

**TO BE CARRIED BY AN ABSOLUTE MAJORITY OF COUNCIL**

**COUNCIL DECISION**

## Background

Following an increased incidence of fatal shark attacks along the West Australian coastline, the State Government committed funds to research and trials of various shark hazard mitigation treatments. The State Government's Department of Commerce (which houses the office of the Chief Scientist) had sought Expressions of Interest from Local Governments for grant funding of up to \$150,000 to trial a beach enclosure to protect swimmers from risk of shark encounters. The City submitted an EOI and was shortlisted, however was unsuccessful in securing the funds on account of the form of barrier the City proposed (the Eco Shark Barrier) not being consistent with the product that the State Government wanted to trial. The City of Busselton was subsequently successful in securing a grant to trial a net at Dunsborough.

As a means of testing their product, the proponents of the Eco Shark Barrier sought support from the City of Cockburn to trial their barrier at Coogee Beach over the summer months at no cost to Council. The matter was presented to Council at its 11<sup>th</sup> July 2013 Ordinary Council Meeting and the recommendation was adopted to approve the trial of the Eco Shark Barrier at Coogee Beach from September 2013 until March 2014 subject to a number of conditions including placement, engineering certification, approvals and insurances

Form Designs Pty Ltd (the designers) and Eco Shark Barriers (the inventor, funder and installer) were successful in gaining the necessary approvals and the trial of the eco shark barrier proceeded with it being installed in December 2013 and removed (with the exception of the anchor pylons and seabed components) on the 26<sup>th</sup> April 2014. The trial was successful including proving popular with beachgoers and having no marine entrapment issues.

Following the conclusion of the successful trial a report was presented to 8 May 2014 Ordinary Council Meeting and the following recommendation was adopted:

- (1) *commence negotiations with Eco Shark Barriers Pty Ltd and the state government to continue the trial for a 3 year period from September 2014 to September 2017;*
- (2) *informs Eco Shark Barriers Pty Ltd that the trial will enable the Eco shark barrier to remain in place during both summer and winter months and will provide a more robust trial of the barriers ability to withstand wave action and storm events;*
- (3) *negotiate on the basis that Eco Shark Barrier P/L will:*

- *retain responsibility for installation, management, insurance, cleaning and monitoring of the barrier for the entire period of the trial;*
  - *provide appropriate certification for the product;*
  - *retain public liability insurance to the value of \$20,000,000 for the duration of the trial;*
  - *provide an annual report (in September of each year) detailing the impact of coastal processes on the beach environment;*
  - *remove the barrier, anchor piles, anchor chains and any other associated product at the end of the trial period if no alternative arrangements have been made with the City.*
- (4) *seek approval from the Department of Lands to lease the area bounded by the Eco shark barrier for a 3 year period during the trial;*
- (5) *seek the necessary approvals from the Department of Planning and the Department of Transport to re-install the eco shark barrier for a 3 year period from September 2014 to September 2017; and*
- (6) *match the state government contributions, on a dollar for dollar basis, up to a maximum value of \$75,000 per annum.*

Most of the conditions have been met to varying degrees and it is now proposed to proceed with the further trial of the eco shark barrier under somewhat modified parameters as detailed in the report below.

### **Submission**

N/A

### **Report**

Following from 8 May 2014 OCM resolution, officers from the City have carried out discussions and negotiations with Eco Shark Barriers Pty Ltd, sought approvals and licenses from various agencies, and sought funding contribution from the State Government, with view to reinstalling the eco shark barrier in October/November 2014 for a further trial period up to September 2017. Details in respect to these negotiations and inquiries are as follows.

#### Eco Shark Barriers Pty Ltd

Eco Shark Barriers Pty Ltd (the proprietors being Craig and Leanne Moss) are keen to work with the City on a further extended trial of the eco shark barrier out to September 2017, including leaving it in place

over the winter periods to test the barrier's resilience to wind and wave conditions.

Since April 2014 the company has worked on an improved design of the barrier segments to make assembly and disassemble easier and lessen their cross section width to lessen the drag in the water, whilst ensuring the strength, structural integrity and non-entrapment characteristics of the barrier are not compromised. It is the barrier incorporating this improved design that the company want to install at Coogee Beach for the forthcoming summer of 2014/15, at no different cost to lease as compared to what would have applied if reinstalling the barrier used in the 2013/14 trial period.

The proprietors of Eco Shark Barriers Pty Ltd have provided a number of pricing alternatives on the basis, as requested by the City, of their continued ownership, maintenance and overall responsibility for the barrier over the extended trial period out to September 2017. A copy of their price proposal is attached at Appendix 1.

#### State Government Agency Licenses and Approvals

Relevant State Government departments or agencies with a statutory interest in the Eco Shark Barrier installation at Coogee Beach have been approached with view to the City being the applicant or license holder over this further trial period. A summary of their responses follows:

- a) The Department of Transport – Coastal Infrastructure Branch issue an annual license for the structure (as they would for a jetty) and they have indicated that the license issued to Eco Shark Barriers can be transferred over to the City.
- b) The Department of Planning Department of Planning have advised that further approval is not required from the WAPC in respect to the re-installation of the eco shark barrier at Coogee Beach.
- c) The Department of Lands have referred to their Legal Services team the preparation of a Section 91 licence under the Land Administration Act 1997, in the name of the City of Cockburn. It is understood this will also cover any Native Title matters. The advice to the officer at the Department dealing with the City's application is that the City is seeking for the license to be issued in time for a reinstallation of the barrier by the end of October 2014.

#### State Government Funding Contribution

The Mayor and the City's Director Engineering and Works made representations to the State Government in early June seeking

contributory funding toward a three year extension of the eco shark barrier trial at Coogee Beach. Copies of the letters received back from the Premier Colin Barnett and the Director of the Office of Science Wendy Attenborough dated 24 and 19 June respectively are attached at Appendix 2. The responses advised that the Government is evaluating the performance of the shark barrier trial they sponsored at Dunsborough over last summer.

Further liaison with the Office of Science since these dates has received the same advice, that the Government will consider future funding options when the evaluation is finalised.

The Governments response to media inquiries around the EPA not supporting the reinstallation of the shark drum lines off the West Australian coast appears to indicate that the Government is receptive to the installation of shark barriers at Perth metropolitan beaches. However there is no certainty that the Government would support a barrier at Coogee Beach in preference to one of the other Perth beaches that could also suit such a measure.

Separate to the City's representations to the Government, the proprietors of Eco Shark Barriers have made their own submissions to both the Office of Science and to the Minister for Fisheries, seeking support from the Government toward the City's proposed reinstallation of the eco shark barrier at Coogee Beach, and also to be aware of the potential for the barrier to be installed at other Western Australian beach locations. The responses they have received have been similar to that received by the City, that the Government is not in a position to financially support the installation of a eco shark barrier (or we take it, any other form of barrier) at this point of time.

#### Timing and Cost for Reinstallation of the Eco Shark Barrier

There is a lead time for the company to proceed with procurement of the new components, assembly and reinstallation of the barrier, such that if given the go ahead following 9 October Ordinary Council Meeting the barrier could conceivably be installed around the end of October.

It is also by the end of October that the Section 91 license is expected to be issued by the Department of Lands.

Eco Shark Barrier Pty Ltd have provided three pricing options for installation of the improved shark barrier as detailed in Section 5 of their presentation document (appended at Appendix 1), these being:

- a) Cost for the City to purchase the Barrier: \$255,000, plus estimated maintenance cost \$10,000 per annum

- b) Cost for the City to lease the barrier over a 3 year period: \$100,000 per annum plus estimated maintenance cost \$10,000 per annum
- c) Cost for the City to lease the barrier for a period of 7 years including maintenance: \$65,000 per annum.

With options (b) or (c) Eco Shark Barriers would have the continuing ownership responsibilities for the barrier including any reinstatement should it be damaged by storm events or vandalism.

It is not envisaged that the City would incur any additional significant cost with any of the options over the 3 to 7 year time frame. There may be some additional minor officer time and cost in administration and periodic inspections carried out by City officers, and for the license fees.

The costs are reflective of the pylons, anchor chains, navigation markers and management plans having already been installed and available for the re-installation.

#### Additional Considerations and Recommendation

In its previous item, the City made the point that the eco shark barrier attracted users from the metropolitan area at large and on that basis, some contribution should be expected by the State Government to continue to proof up the product for more wide scale use. The City committed a sum of \$75,000 to match any contribution the state would provide. As the City has been unable to secure any contributions, it must now decide if it wishes to proceed with the trial and fund the annual costs.

In view of the eco shark barrier design essentially being a prototype, with potential for further development and improvement in the future, plus it being untested in winter conditions, it would appear prudent that the City not opt for outright purchase or an overly long lease period.

Eco Shark Barrier Pty Ltd has provided the City with a range of funding options to consider. It is also open to the City to make a counter offer to Eco Shark Barrier given it is still relatively untested in surf or swell conditions and the continued trial was contemplated as a means of enabling the company to further proof the technology. Officers believe that that is the most appropriate position to take with further negotiations with the company. It is recommended that the City offer to enter into a 3 year lease with Eco Shark Barrier as previously contemplated in its May recommendation for the sum of \$85,000 per annum (paid on a pro-rata basis and quarterly in advance). This sum will include the installation and maintenance costs for the duration of the lease. The sum is based on the capital cost of the product (\$255K)

and the annual maintenance (option 1) proportioned across the lease period. All other terms shall remain the same.

### **Strategic Plan/Policy Implications**

#### **Infrastructure**

- Community facilities that meet the diverse needs of the community now and into the future.

#### **A Prosperous City**

- Creation and promotion of opportunities for destination based leisure and tourism facilities.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

### **Budget/Financial Implications**

There is a current 2014/15 budget provision of \$42,386 carried forward from 2013/14 FY. If the recommendation is successful an additional \$45,000 allocation will be required via the 2014/15 mid-year budget review.

### **Legal Implications**

N/A

### **Community Consultation**

A website survey was undertaken over the period of the trial.

### **Attachment(s)**

1. Eco Shark Barrier Presentation to City of Cockburn 21 July 2014.
2. Correspondence from the Premier dated 24 June 2014 and the Director of the Office of Science dated 19 June 2014.

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## 16.9 (OCM 9/10/2014) - MATTERS TO BE NOTED FOR INVESTIGATION - SHARED WASTE BINS (167/002) (M LITTLETON)

### RECOMMENDATION

That Council note the contents of the report.

### COUNCIL DECISION

### Background

At the September 2014 Ordinary Council Meeting Councillor Portelli requested under Matters to be noted for Investigation the following:

*That a report be prepared to investigate the feasibility for residents to share rubbish bins in specific areas and under varying conditions. For example to be provided at discounted rates.*

1. *How much could be proposed?*
2. *What sort of discounts could be provided if they are sharing bins? - This proposal is designed to recognise that some residents have minimum rubbish requirements and could be allowed to share bins and therefore reduce their respective costs.*
3. *What discounts would be feasible and what savings are possible?*
4. *Can it be done in zones or controlled so that rubbish collection cannot be uneconomical in some areas?*
5. *What issues can be seen?*

### Submission

N/A

### Report

Waste is an essential service. All properties generate waste and will require access to the service. Whilst it would be nice to be able to tailor the service to meet individual properties, it is difficult to see how this can be achieved (at least in the short term). We have identified a number of issues which would need to be addressed:

1. We currently provide a specific service for aged or mobility impaired residents. Whilst these residents pay the same levy, the City physically collects the bins.
2. sharing bins is currently permitted in multi-storey residential units only. Residents however pay the same levy as the City services

- these properties through basement level facilities with a specific low profile vehicle (requires 2 employees).
3. Sharing bins between individual freehold titles cannot be sustained as there is no way of identifying the proportion of waste being generated by either party.
  4. Allowing people to share bins does not remove the need for the City to travel every street or reduce the amount of waste collected.
  5. The overall costs of disposal will not change (i.e. we will still generate the same amount of MSW regardless).
  6. Put simply, it removes a small number of lifts yet requires the City to collect the same tonnage and travel the same km's.
  7. We cannot currently control who is putting waste in a bin (once a bin has been presented, it is open for any resident to place waste in it).
  8. If you do allow residents to share, who presents the bin, who stores it, who cleans it etc?
  9. Are there trespass issues?
  10. It relies on neighbours to be neighbourly. What happens if the relationship sours or if one resident moves away?
  11. How do we levy a fee for service?
  12. What is the service? Do people opt in and opt out?

It is important to acknowledge that the current service is highly regarded by our community as is evidenced by the response to our community perceptions survey (97% satisfaction). We are also currently in discussions with the City of Kwinana to determine the best outcome for service provision in that district should reform occur.

### The Current Service

The Waste Collection budget is based on a "cost neutral" principle in that it spreads the cost of the entire service over all users. The \$435 waste charge levied on all improved properties allows the delivery of the following suite of services (proportional cost included as a percentage):

- A weekly 240lt Municipal Solid Waste (MSW) or green top bin (60%),
- A weekly 240lt recycled or yellow top bin (20%),
- 4 bulk verge collections (10%)
- 6 trailer passes (8%)
- A bin delivery/ maintenance service (1%) and
- A park and litter bin service (1%)

Residents are also charged an additional \$27/year for 4 years to cover the cost of the supply of a recycle and a general waste (MSW) bin.

### Service Charge Methodology

The current waste service requires the following:

- Green Bin - 2.17 M lifts per annum (44,000 weekly services @ 95%)
- 28,000 tonne of putrescible waste
- We travel every street every week.
- Yellow bin - 1.83 M lifts per annum (44,000 weekly services @ 80%)
- 14,000 tonne of recyclables
- We travel every street every week.
- 11 serviced areas \* 4 Verge collections per annum (servicing every domestic property in the district)
- 2,500 tonne of green waste and 1,700 tonne of junk.
- 60,000 trailer passes redeemed (of the 240,000 passes issued)

### The SMRC

Through its service charge the City is also funding the governance and administration of the SMRC as well as the significant loan liability for the RRRC. Our waste disposal costs are also reflective of the increased costs required to process our MSW through the SMRC.

### Growth

Currently the waste collection service grows at 1,200 lifts per annum. Every 5 years we must add an additional MSW and Recycle truck to the fleet to manage the growth.

Our current waste levy of \$435 per annum covers this service in its entirety and each residential property has access to the same service. Our budget is a 'closed loop' with all costs being incorporated.

### User Pays Methodology

Developing a "user pays" principle could be considered; however, is very complex to deliver. Our officers always look for opportunities to reducing waste to landfill. It is well know that those initiatives that link financial incentives to sustainability outcomes are the simplest to implement and have greatest chance of success.

As waste is an essential service, the City requires some degree of certainty to enable it to offer the range of services currently provided. Waste collection is a logistics business providing a 5 day per week operation (with only Christmas and New Year's Day off). We manage 14 waste trucks and 17 staff. Each truck lifts in excess of 1,200 bins each day and we need to know where those services are.

User Pays becomes complicated because of the following:

- Our collection method is largely fixed.
- You cannot control who puts waste in a bin once it is on the street.
- Whilst we see some advantage in providing 140ltr and even 360ltr bins, the costs to service them are exactly the same.
- The additional costs to administer a user pays service increases.

Whilst these issues maybe overcome, I am not sure that the benefits outweigh the demand.

From a City of Cockburn service view point, there will be many residents who don't use the suite of services provided by the City e.g. libraries, recreation centres, park bins, CoSafe, etc yet are required to contribute regardless.

#### Cost plus Methodology

With a cost plus methodology the City would have to factor in the broad capital costs of delivering a base level of service and apply that cost as a levy. We could then offer additional services at a fixed fee and enable users to pick and choose based on their specific need. In taking this approach however the City would need to apply the full cost of the additional service. As an example, we could reduce the waste levy by \$30 if we were to remove all tip passes. Residents that required tip passes could purchase them from the City however we would apply the full cost of \$270 per 6 trailer passes.

The City currently enables residents to purchase additional MSW or recycling bin collections on a cost recovery basis (\$311 for a second MSW bin and \$100 per second recycle bin).

The basis of this methodology is not inconsistent with the manner in which the Water Corporation levies costs for water and sewerage. Whilst there is a broad consumption charge, every resident must pay water and sewerage rate.

#### Legislation

The Waste and Resource Recovery Act 2007 (WARR Act) Section 67 gives the Local Government the power to impose a receptacle charge on the owner of a premise provided with a waste service. The City is obliged to provide this service to all improved properties in its local government area. Whether occupants choose to use the service by presenting or not presenting bins has no bearing on the amount charged.

The interpretation and application of this charge by the City has been sanctioned by the Department of Local Government as being consistent with the Act and the practices of other local governments.

### Waste Management & Education Strategy

In our efforts to reduce waste to landfill, officers see the merit of reducing the receptacle size. This however needs to be considered holistically and strategically. The Waste Management and Education Strategy 2013-2023 has listed an action for the 16-17 FY in respect to this matter as follows:

*“Prepare a business case for the introduction of financial incentives to reduce domestic MSW bins to 140lt.”*

Given the local government reform matters currently on the agenda, the 2016/17 timeframe is considered reasonable. This initiative fits well around the “Recycle Right” principles.

### Conclusion

Under the current fee structure there is no opportunity to reduce the costs for those users who produce less waste. The principle applied by our current waste levy effectively spreads the service costs across many users, and enables the City to maintain the service charge at the current level. If Council sought to move to a more user pays methodology officers could not predict the resource allocations required to deliver the service from 1 year to the next which would add additional costs and risk to the service. Whilst it may also marginally reduce costs to a few residents, it would not reduce the program costs thus costs to other residents would need to increase as a result.

### **Strategic Plan/Policy Implications**

#### **Environment & Sustainability**

- A community that uses resources in a sustainable manner.
- Community and businesses that are supported to reduce resource consumption, recycle and manage waste.

#### **Budget/Financial Implications**

N/A

#### **Legal Implications**

N/A

**Community Consultation**

N/A

**Attachment(s)**

N/A

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**17. COMMUNITY SERVICES DIVISION ISSUES**

Nil

**18. EXECUTIVE DIVISION ISSUES**

Nil

**19. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**

Nil

**20. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING**

**21. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY COUNCILLORS OR OFFICERS**

**22. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE**

**23. CONFIDENTIAL BUSINESS**

**24 (OCM 9/10/2014) - RESOLUTION OF COMPLIANCE (SECTION 3.18(3), LOCAL GOVERNMENT ACT 1995)**

**RECOMMENDATION**

That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

**COUNCIL DECISION**

**25. CLOSURE OF MEETING**



To: Chief Executive Officers

From: Tony Brown  
Executive Manager  
Governance & Strategy

Organisation: All Local Governments

Date: 25 September 2014

Reference: 05-034-01-0018

Priority: High

Subject: WALGA's Poll Provisions Advocacy Position

**IN BRIEF:**

Operational Area:	<b>Governance</b>
Key Issues:	<ul style="list-style-type: none"> <li>• Feedback is sought by <b>31 October</b> from all Local Governments to refine WALGA's advocacy position relating to the poll provisions contained in Schedule 2.1 of the <i>Local Government Act 1995</i>;</li> <li>• Options are provided: that all boundary change proposals could be eligible for a poll or, that a percentage variation – 10, 25 or 50 percent – in population, rateable properties, or revenue could define 'significantly affected' that would enable the community to request a poll. General feedback or alternative suggestions are also welcome.</li> </ul>
Action Required:	Provide a response to <a href="mailto:tlane@walga.asn.au">tlane@walga.asn.au</a> by Friday, <b>31 October</b> .

**Background**

Input is sought to define WALGA's position regarding advocacy for amendments to the poll provisions contained in Schedule 2.1 of the *Local Government Act 1995* to enable electors of a Local Government that will be abolished or significantly affected by a boundary change proposal to demand a poll.

At the **2 July** State Council meeting, State Council resolved to adopt, and advocate for, a policy position that the poll provisions should be amended so that electors of a Local Government where one or more Local Governments will be abolished or significantly affected by a boundary change proposal are able to demand a poll on the proposal, with 'significantly affected' being specifically defined as causing a fifty percent variation in:

- i. Population; or,
- ii. Rateable properties; or,
- iii. Revenue.

At WALGA's Annual General Meeting, held on **6 August**, the meeting resolved:

*That this Annual General Meeting, recognising the current approach by the State Government to the manipulation of the principles of the 'Dadour' poll provisions:*

- a) *endorse WALGA's position of providing community access to the poll provisions where 1 or more districts are to be abolished rather than the 2 or more districts as currently provided for in the Local Government Act 1995;*
- b) *endorse WALGA's proposed extension of the poll provisions to significant boundary adjustments subject to any associated criteria and any percentages being agreed to by a majority of all local governments in Western Australia, and*
- c) *reaffirm as policy, that WALGA is opposed to the removal or dilution of the 'Dadour' poll provisions including the temporary dilution or removal of those provisions.*

State Council, at their **3 September** meeting, endorsed parts (a) and (c) of the AGM resolution above and resolved the following in relation to part (b):

# INFOPAGE

*4.7B Part (b) – endorse WALGA's proposed extension of the poll provisions to include significant boundary adjustments subject to further research and sector consultation being carried out on any associated criteria and for a report to be presented through the next Zone/State Council Meetings.*



This InfoPage represents sector consultation on the definition and criteria associated with a Local Government being significantly affected by a boundary change proposal. Local Governments are invited to provide feedback prior to Friday **31 October** to inform an agenda item to be prepared for the **3 December** State Council meeting.

## Comment

Defining the criteria for whether a boundary change significantly affects a Local Government is difficult and there are divergent views in the Local Government sector.

There is a general view that a minor boundary change, perhaps to fix an anomaly, should not be the subject of a potential poll of electors. There is also a general view that, where one or more Local Governments will be abolished or a Local Government's viability could be affected by a boundary change proposal, electors should have the right to demand a poll.

Criteria defining whether a Local Government would be 'significantly affected' could be defined in the Local Government Act. This was State Council's original approach where it was resolved that a 50 percent variation in population, or rateable properties or revenue would be the trigger for the community to have the option to call a poll. In addressing this matter it needs to be determined whether these are the appropriate criteria or whether there should be an alternative method to determine whether a Local Government would be 'significantly affected' by a boundary change proposal.

Options to address this issue are presented:

1. All boundary change proposals could be the subject of a poll. While there is a general view that minor boundary changes should not be subject to a poll of the community, it could be argued that a minor boundary change that only affects a small number of properties would be unlikely to attract enough interest from the community for a poll to be called or to ultimately be successful in overturning the proposal. This would remove the need for criteria to be established to define 'significantly affected'.
2. Criteria defining whether a Local Government would be 'significantly affected' could be defined in the Local Government Act. It is suggested that a percentage variation in population, or rateable properties, or revenue could be defined as the appropriate criteria to trigger the community's right to call a poll. Three percentages are presented as options to define these criteria in the Local Government Act:
  - a. 10 percent.
  - b. 25 percent
  - c. 50 percent

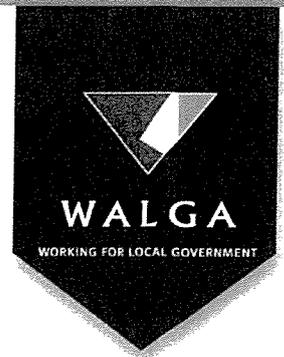
A Faxback is attached to facilitate feedback on the options presented above as well as general feedback to refine WALGA's advocacy position, including alternative criteria to define when a Local Government would be 'significantly affected' by a poll.

Feedback will guide a State Council agenda item to be considered by Zones in late November and then State Council at their **3 December** meeting.

*Please submit feedback to Manager Strategy and Reform, Tim Lane, on [tlane@walga.asn.au](mailto:tlane@walga.asn.au) by Friday **31 October** 2014.*

ONE70  
LV1, 170 Railway Parade, West Leederville, WA 6007  
PO Box 1544, West Perth, WA 6872  
T: (08) 9213 2000 F: (08) 9213 2077 [info@walga.asn.au](mailto:info@walga.asn.au)  
[www.walga.asn.au](http://www.walga.asn.au)

# FAX BACK



To: Chief Executive Officers                      From: Tony Brown  
Organisation: All Local Governments                      Executive Manager  
Reference: 05-034-01-0018                      Governance & Strategy  
Priority: High  
Subject: WALGA's Poll Provisions Advocacy Position

FAX BACK TO WALGA ON 9213 2077 or email [tlane@walga.asn.au](mailto:tlane@walga.asn.au)

## IN BRIEF:

Operational Area:	<b>Governance</b>
Key Issues:	<ul style="list-style-type: none"><li>• Feedback is sought by <b>31 October</b> from all Local Governments to refine WALGA's advocacy position relating to the poll provisions contained in Schedule 2.1 of the <i>Local Government Act 1995</i>;</li><li>• Options are provided: that all boundary change proposals could be eligible for a poll or, that a percentage variation – 10, 25 or 50 percent – in population, rateable properties, or revenue could define 'significantly affected' that would enable the community to request a poll. General feedback or alternative suggestions are also welcome.</li></ul>
Action Required:	Provide a response to <a href="mailto:tlane@walga.asn.au">tlane@walga.asn.au</a> by Friday, <b>31 October</b> .

Please advise whether your Council supports the *Local Government Act 1995* being amended so that the community of a Local Government could demand a poll with which of the following conditions being met:

1. Under any boundary change proposal YES / NO
- OR
2. With a significant variation in population, or rateable properties or revenue by:
- a. 10 percent
  - b. 25 percent
  - c. 50 percent

*(Please indicate preferences – i.e. 1 for most preferred, then 2 then 3)*

General feedback to refine WALGA's advocacy position would be welcomed including alternative criteria to define when a Local Government would be 'significantly affected' by a poll.

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Feedback will guide a State Council agenda item to be considered by Zones in late November and then State Council at their **3 December** meeting.

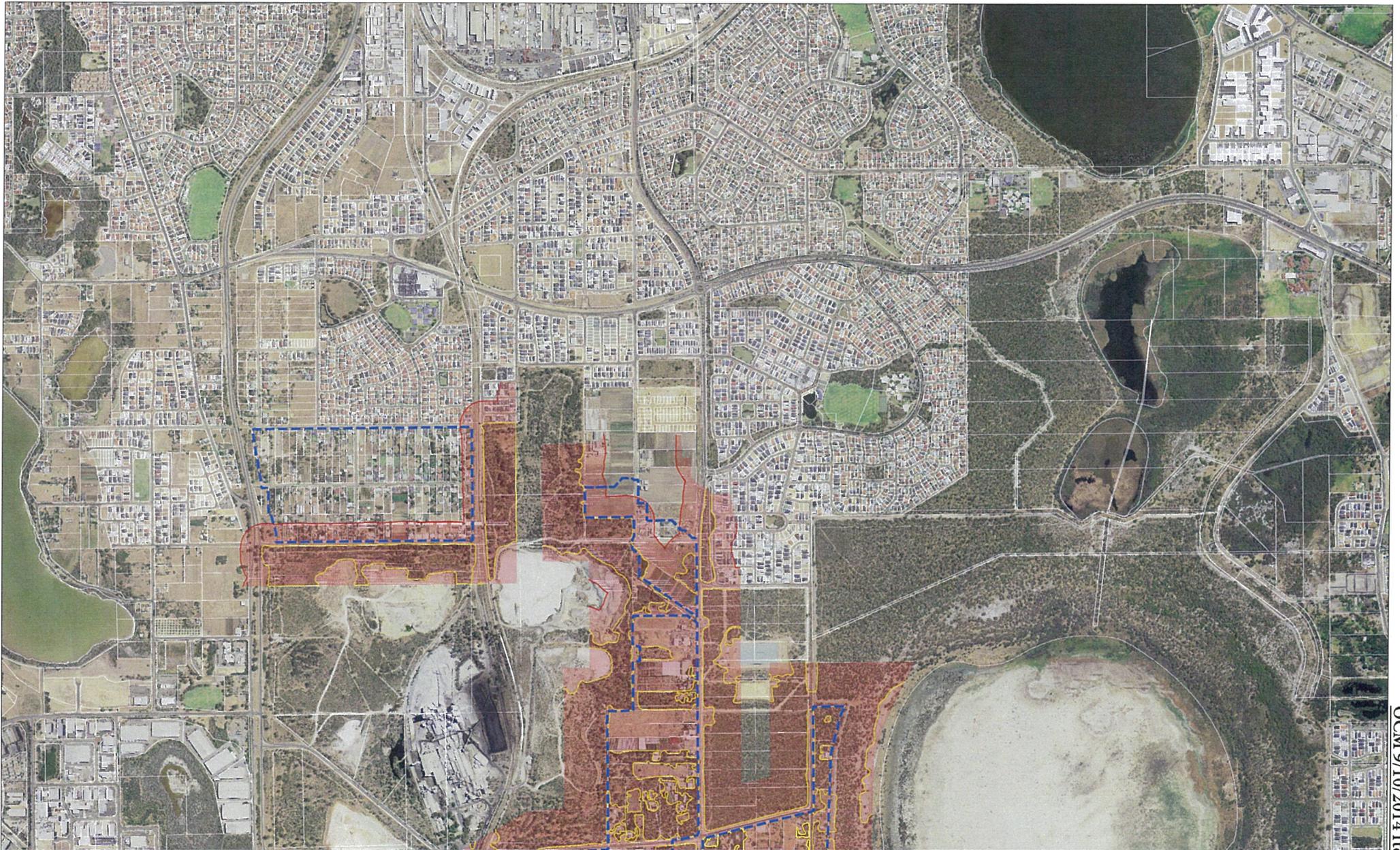
Please submit feedback to Manager Strategy and Reform, Tim Lane, on [tlane@walga.asn.au](mailto:tlane@walga.asn.au) by **Friday 31 October 2014**.

**Thank you for your assistance.**

**For further information please contact:**

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City of  
**Cockburn**

wetlands to waves

GNA 9/2014

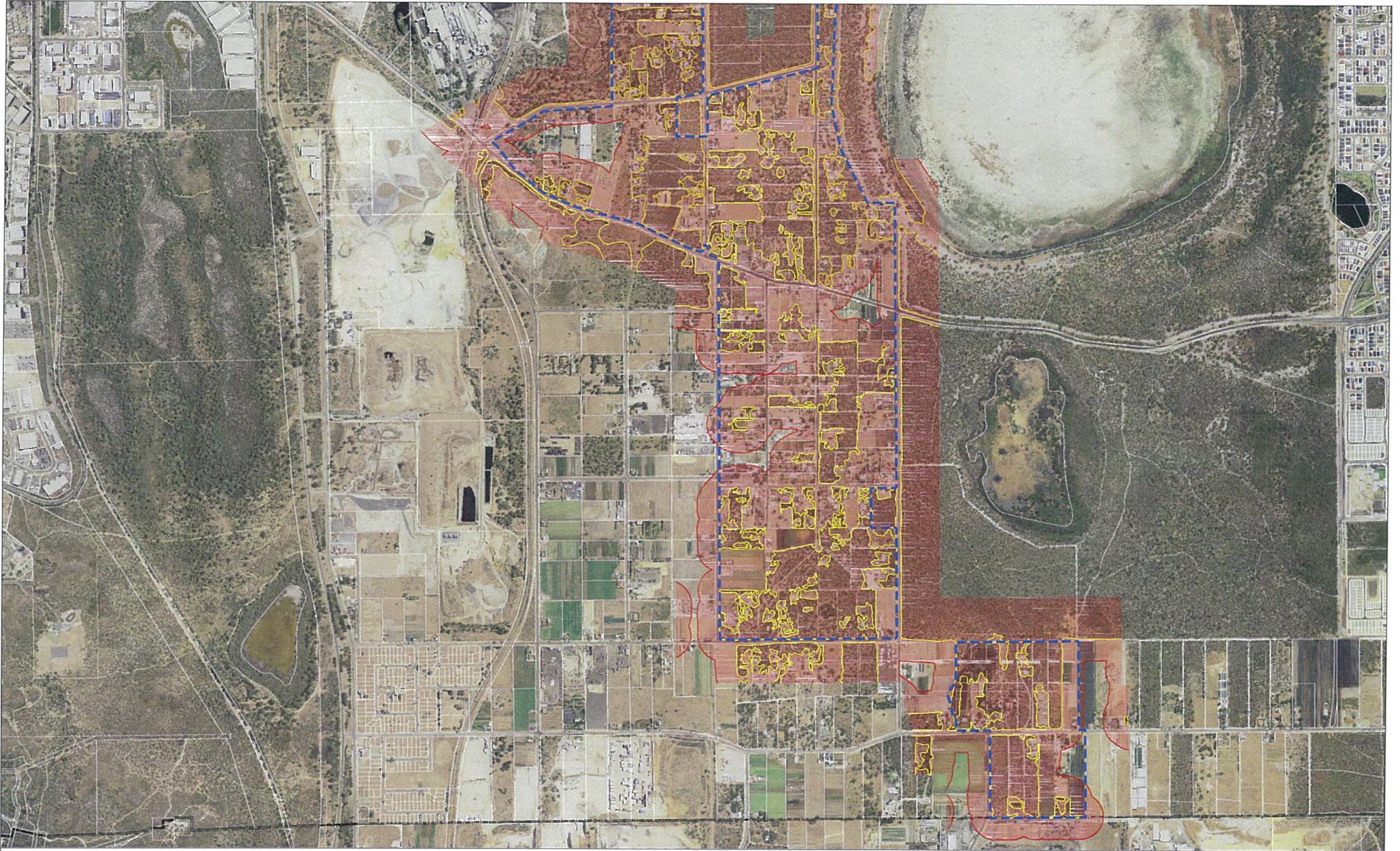
0 25m  
Scale

-  Bushfire Prone Area boundary
-  Bushland vegetation areas
-  100m buffer

## BUSHFIRE HAZARD ASSESSMENT MAP

Map 1

OCM 9/10/2014 Item 14.1 Attach 1



-  Bushfire Prone Area boundary
-  Bushland vegetation areas
-  100m buffer

## BUSHFIRE HAZARD ASSESSMENT MAP

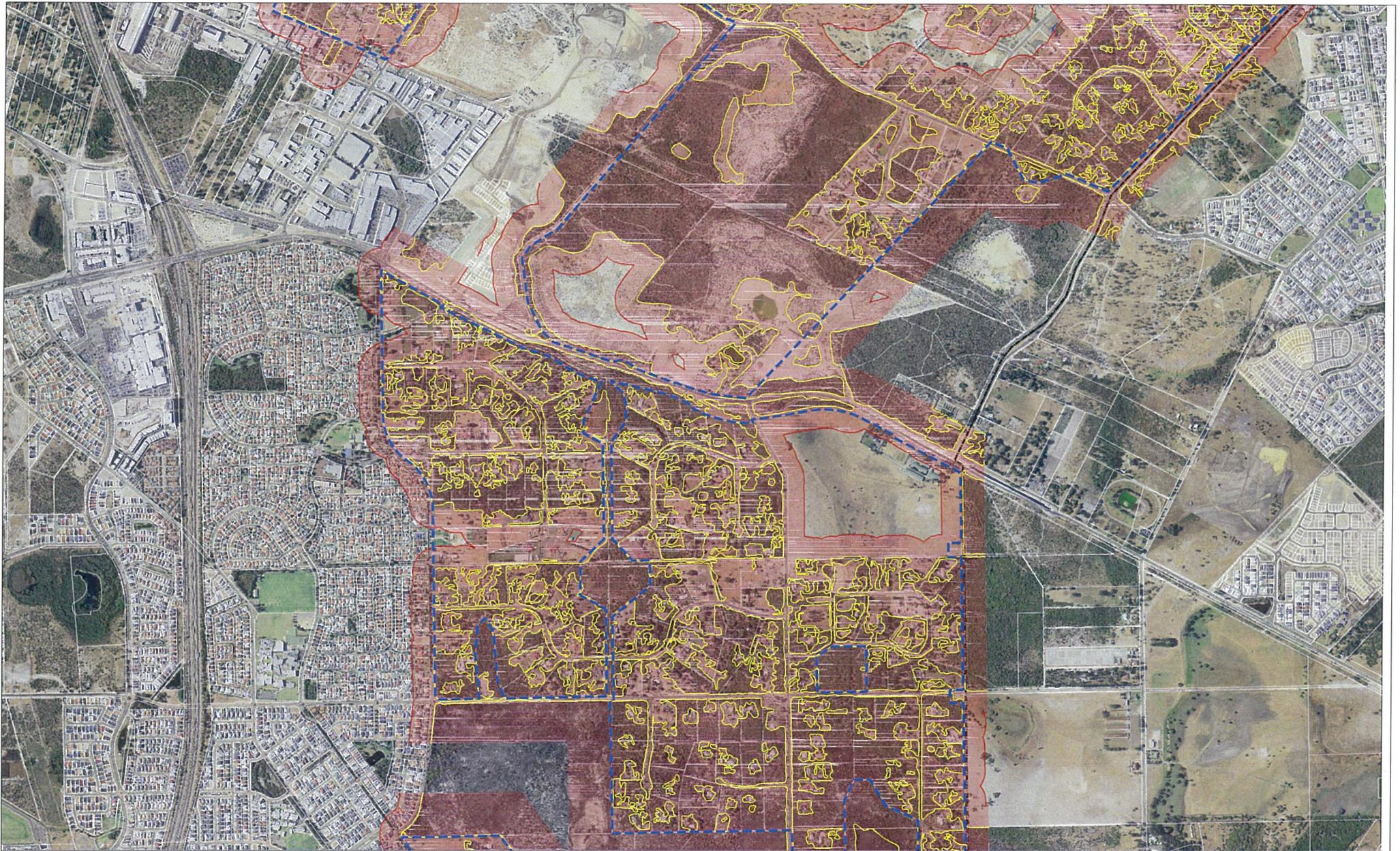


0 250m  
Scale

-  Bushfire Prone Area boundary
-  Bushland vegetation areas
-  100m buffer

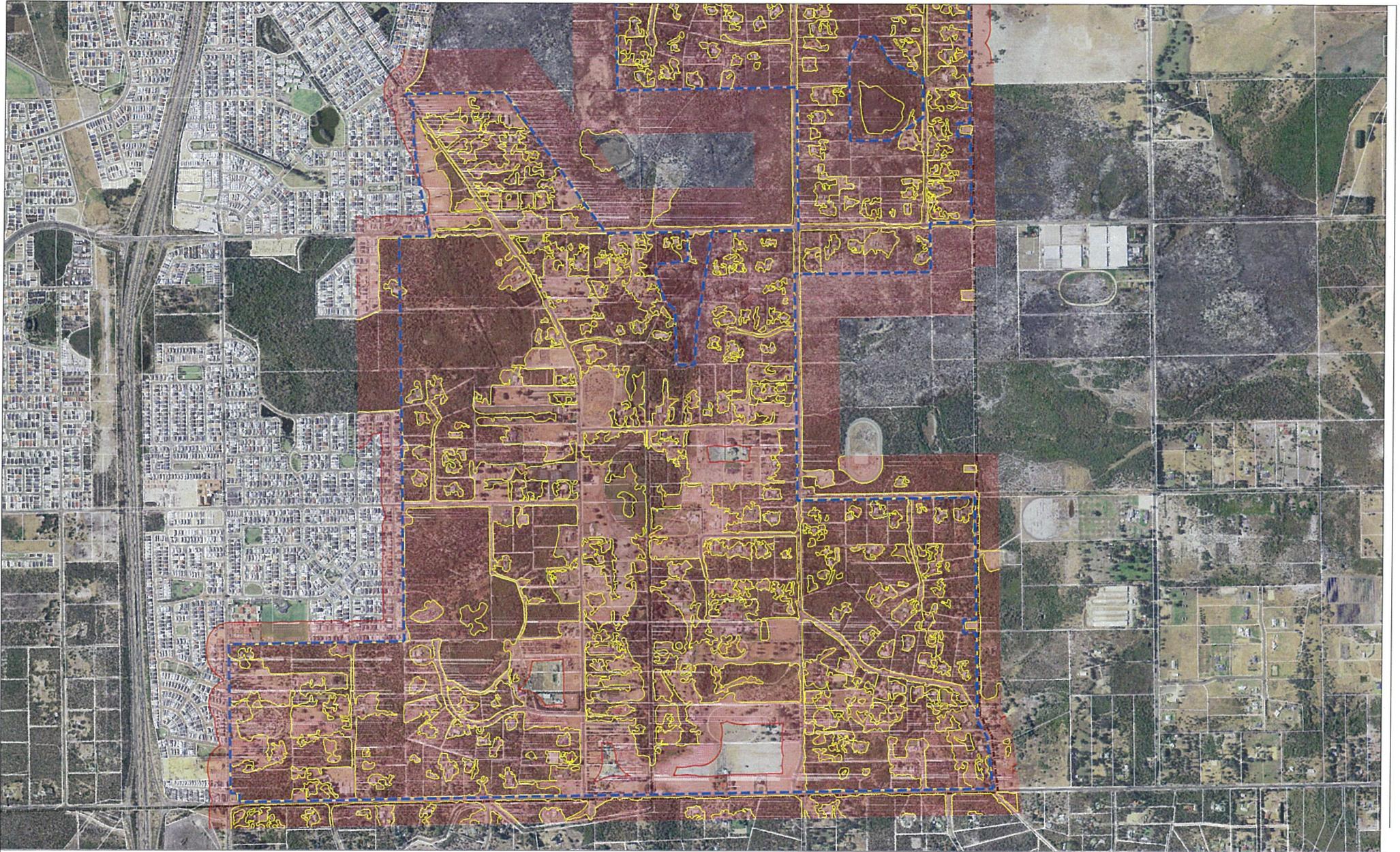
## BUSHFIRE HAZARD ASSESSMENT MAP

Map 3



-  Bushfire Prone Area boundary
-  Bushland vegetation areas
-  100m buffer

## BUSHFIRE HAZARD ASSESSMENT MAP



0 250m  
Scale

-  Bushfire Prone Area boundary
-  Bushland vegetation areas
-  100m buffer

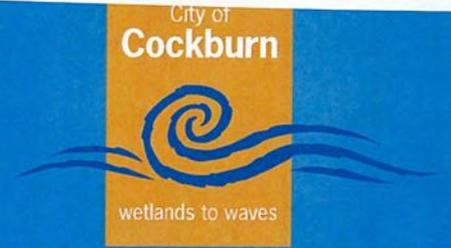
## BUSHFIRE HAZARD ASSESSMENT MAP

Map 5

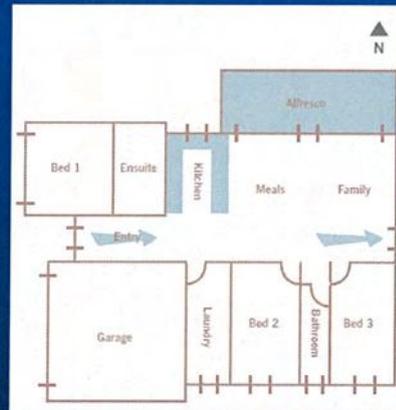
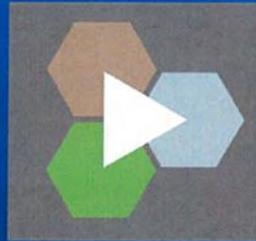
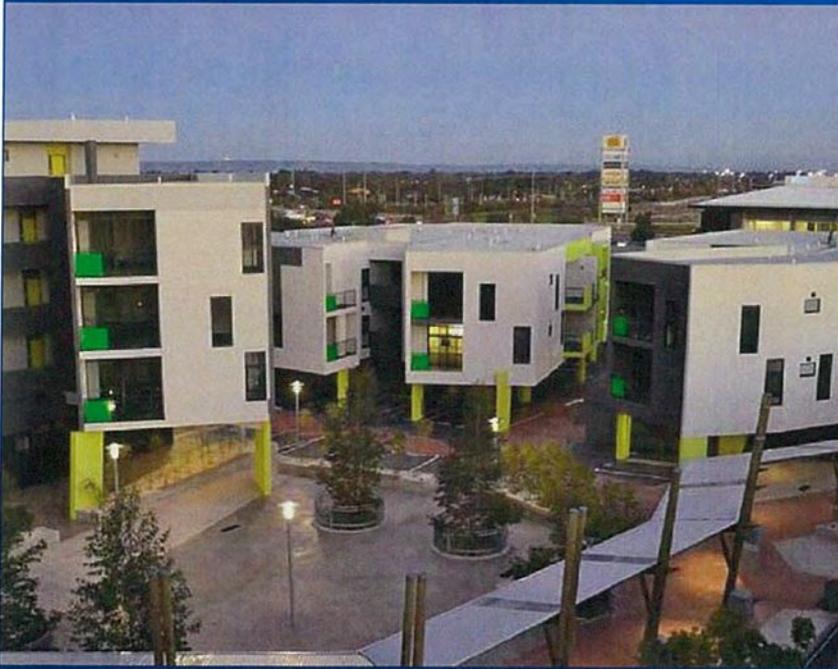
**SCHEDULE OF SUBMISSIONS  
PROPOSED BUSHFIRE HAZARD ASSESSMENT MAP**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	City of Cockburn Landowner	<p>My husband and I live at 281 Henderson Road in Munster. We have done so for 9 years and have not had any bushfires come close to our property in that time. We have positioned our house close to Henderson Road and keep the firebreaks and areas around the house cleared.</p> <p>I sincerely hope that our land has not been identified as being bushfire prone. If it has, I'd like to know on what basis? What evidence is there to support that this area is bushfire prone?</p> <p>My only reason for not wanting my land identified as bushfire prone, other than the fact that it is not, is that I do not want any additional building and planning requirements to be imposed in relation to new proposed development on the land. You already cannot do anything. At the moment, the area is zoned rural so you cannot subdivide your lot, even in two. There is strong resistance to any second dwelling being erected and stringent requirements in relation to wanting to build a granny flat. The very last thing we need, is further red tape, prohibiting and limiting our freedom to do what we want on our land.</p>	<p><b>Objection Noted. Not Supported</b></p> <p>The absence of a bushfire is not necessarily an indication of the level of risk and vulnerability a particular piece of land is faced with.</p> <p>The City has utilised the Office of Bushfire Risk Managements State Bushfire Prone Area Map mapping standard to determine what areas within certain zones are likely to be subject to bushfire risk.</p> <p>This approach to addressing risk is consistent with the intent of Scheme Amendment No, 92, the recommendations of the Keelty Report and the recently released Draft State Planning Policy 3.7.</p>
2	City of Cockburn Landowner	<p>Objection</p> <p>In reference to the above Proposal, I wish to voice my opinion regarding the prevention of bush fires in the areas identified as hazardous especially on Map 3.</p> <p>Most of the areas identified as being in a bush fire prone area are in close proximity to quite densely populated urban areas. I am of the opinion that when a bush fire takes hold especially during the hot summer months in the year, they are especially difficult to control and do spread to the urban areas surrounding the bush fire prone areas with devastating results. I personally believe, it would be far more sensible to reduce the bush in these zones which would considerably</p>	<p><b>Objection Noted. Not Supported</b></p> <p>The City also wishes to avoid repeats of significant bushfire events such as those experienced on Black Saturday and the Roleystone Fires. The City in enacting Bushfire Prone Areas is actioning a recommendation of the Keelty Report into the Perth Hills Bushfires.</p> <p>By ensuring that all new development correctly addresses the bushfire risk the City hopes to lower the vulnerability of property and lives to bushfire.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>reduce the chance of fires breaking out in identified areas. This will reduce the damage a potential fire could cause and stop it from spreading into neighbouring built up areas causing loss of lives and homes. I'm sure no one would like to see a repeat of the Black Saturday bush fires in Perth. Also, we are already paying additional rates to cover FESA's costs in the event of a bush fire.</p>	<p>Broad scale clearing of Local and State reserves and private land within the Resource Zone would be inconsistent with both the City's Scheme and various State Planning Policies. By enacting bushfire prone areas on land where people live the City is attempting to strike a balance between the preservation of environmental assets and the protection of property and lives.</p>



# City of Cockburn Housing Affordability and Diversity Strategy DRAFT



# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

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# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## EXECUTIVE SUMMARY

The City of Cockburn, like much of Western Australia, is facing a significant challenge in housing affordability. Direct housing costs have increased at a much faster rate than household incomes resulting in many individuals and families being priced out of the market for the foreseeable future. The rising cost of housing, which represents around 30-40 per cent of household expenditure, is the single largest driver of financial hardship for low income households in Western Australia<sup>1</sup>.

Without affordable housing individuals and families are more likely to suffer increased levels of financial and personal stress. Households struggling to pay housing costs are faced with a series of issues including unmanageable levels of debt, working and travelling long hours to cover housing costs, living in overcrowded or substandard housing and sacrificing essentials such as heating, food, medication and education. This has a negative impact on the whole community.

Housing is considered affordable when households, particularly low and moderate income households, renting or purchasing, are able to pay their housing costs and still have sufficient income to meet other basic needs. Households with incomes below 120 per cent of the gross median income of all households are considered to be experiencing housing stress when their housing costs exceed 30 per cent of gross household income.

The growing trend is a decrease in household sizes, and by 2031 'Couples without dependents' and 'Lone person households' will account for 53.6 per cent of all households, yet 53 per cent of dwellings in the City of Cockburn are detached dwellings with four or more bedrooms. Dwellings with more than three bedrooms represent 81 per cent of the housing stock in the City of Cockburn. The household projections and housing stock assessment indicate a mismatch of housing throughout the City that is likely to become more pronounced into the future. This is a trend across the Perth Metropolitan area, and in Australia generally.

It is therefore considered that the City's projected housing stock will not match the needs of future households, and will not provide an ideal range of housing choices for future households. This will be more pronounced in certain areas. For example residents wishing to 'downsize' in suburbs such as Aubin Grove and Hammond Park will have limited opportunities to do so in their local area. The shortage of availability of smaller dwellings throughout the City of Cockburn may result in residents living in housing that does not suit their needs, or residents being forced to buy or rent dwellings that are larger and more costly than they require.

## KEY FINDINGS:

### **Housing stock mismatch**

The City's current housing stock does not match the projected smaller households, and will not provide an adequate range of housing choices for future households. A greater number of smaller dwellings will be required to meet the needs of smaller households.

### **Need for a compact urban form**

The Perth Metropolitan Region is still characterised by predominately low density residential codings that have resulted in a housing stock of large detached dwellings, and many dwellings that in general do not have high levels of accessibility. The City should continue with great earnest its programs of revitalisation strategies, which have been very successful in delivering higher residential densities within established communities like Spearwood and Hamilton Hill. These identify opportunities for higher density living particularly within easy

<sup>1</sup> Western Australian Council of Social Services Inc (2012) *Cost of Living Report 2012*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

access to public transport, as a way to assist lower income households who may not have access to a car. Individuals on lower incomes also indicate a stronger preference for easy access to public transport than other income groups. Providing dwellings with good access to services and public transport is particularly important for young people and people with disabilities, who are high users of public transport.

## **Declining housing affordability**

The number of Australian households in housing stress has increased dramatically since 2003, and this is a trend that is likely to continue into the future. For 'very low', 'low' and 'moderate' income earners this means that owning a property is likely to be out of reach. Households susceptible to housing stress are low income renters, low income households with children, older people renting, and people with disabilities.

## **Cost of living impacts for low income households**

All households are impacted on by increasing costs of living, however low income households are the most affected. In particular it is single parent families and lone person households that are most susceptible to living cost increases. Housing affordability is a particular issue for family households who have a variety of living expenses that make them more susceptible to financial hardship, in addition to having high housing costs through the requirement in many cases for larger dwellings that have higher rental costs.

## **Need for adaptable housing**

In the City of Cockburn there is an ageing population, and 18 per cent of people have a disability. For many of these people their home may not have a level of accessibility to suit their needs either now or in the future, because the number of private and public dwellings that have been built to incorporate universal design elements is very low. Inaccessible housing leads to social disadvantage and has negative effects for social integration and participation. Modifications to dwellings to improve accessibility, such as installation of ramps, are often expensive and unsatisfactory. These costs place increased financial pressure on such households, and moving to find a better house design suited to their specific needs is often not a viable option due to the high 'sunk costs' in the current accommodation<sup>2</sup>.

## **Demand for aged care facilities**

The ageing population, particularly the increase in people over 70 years of age, will see an increased demand for aged care facilities for those whose care needs can no longer be met within their own homes.

The demand for low and high care facilities, in addition to respite care will continue to increase across the Perth metropolitan area. In particular there will be a demand for affordable aged care.

Traditionally such facilities have been located on sites of 6-8 ha, however it is becoming increasingly difficult to find such sites, and a more flexible approach is required. The lack of suitable locations raises concerns regarding the shortfall of accommodation in the near future. It will be crucial that the City of Cockburn actively target supported accommodation development in any future redevelopment in order to meet the identified needs.

## **Shortage of crisis accommodation**

There is a trend of increased homelessness particularly for vulnerable households such as people with disabilities. Compounding the problems associated with greater incidence of homelessness in the community

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<sup>2</sup> Tully, Beer (2009) The housing careers of people with a disability and carers of people with a disability AHURI Southern Research centre.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

has been the breakdown of the affordable housing system. There is also an identified shortage of crisis accommodation in the City of Cockburn

There has been increased pressure on crisis accommodation because in addition to more people seeking crisis accommodation, the average length of stay has increased because of longer waiting times for social housing.

Addressing the lack of affordable housing will go some way to reducing vulnerability to homelessness for some households. However, provision of crisis accommodation is still important to ensure that there is adequate accommodation for people waiting for social housing.

### SUMMARY OF ACTIONS

The following actions are proposed to address the outcomes of the Strategy:

PLANNING MECHANISMS	
PLANNING MECHANISMS	a) Encourage other housing types, including dwellings in mixed-use environments, such as 'shop-top' housing to increase the number and diversity of smaller dwellings in the City, particularly in areas with good accessibility to services and public transport.
	b) Investigate opportunities to encourage dwellings in mixed use developments, including: <ul style="list-style-type: none"> <li>adopting guidelines for 'Mixed Use' development to provide guidance to developers and Council in achieving appropriate mixed uses. This may take the form of a Local Planning Policy and/or guidance notes or 'best practice' notes.</li> <li>reviewing the objectives of the commercial zones in the Town Planning Scheme to reference provision of dwellings to encourage mixed use development where appropriate.</li> </ul>
	c) Encourage development of ancillary dwellings by making them exempt from planning approval.
	d) Investigate the potential use of planning incentives to encourage affordable and diverse housing in targeted areas in the City of Cockburn, similar to that introduced for the Cockburn Coast area.
	e) Ensure Urban Revitalisation Strategies identify measures to address the findings of this Strategy.
	f) Ensure wherever possible Structure Plans do not seek to transfer higher building costs on to landowners. This is primarily to endeavour that structure planning better responds to the inherent site characteristics of a land parcel, such as to avoid development on land which is subject to noise or bushfire risk and which requires a more expensive dwelling to be built.
	g) Ensure all Local Structure Plans respond specifically to the outcomes of this Strategy, and address the future housing needs of the community.
	h) Undertake a review of clause 5.8.3 of the Scheme (or equivalent - residential codings for commercial zones) to consider whether a higher residential coding may be appropriate in the commercial zones, in all or some targeted areas.
	i) Modify Local Planning Policy APD12 'Aged and Dependent Persons' Dwellings' to allow less than five dwellings to be developed in any single development.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

	j) Modify Local Planning Policy APD12 'Aged and Dependent Persons Dwellings' to allow flexibility for Section 70A Notifications not to be included on the certificate of title that require at least one occupant to be a disabled or physically dependent person or aged person (where it does not exceed a plot ratio of 100m <sup>2</sup> ).
	k) Modify Local Planning Policy APD56 'Single Bedroom Dwellings' to allow a second multi-purpose room that could be used as a bedroom where the plot ratio of the dwelling does not exceed 70m <sup>2</sup> .
	l) Prepare a new Local Planning Policy for Ancillary Dwellings that sets out criteria for consideration under the 'Design Principles' of the Residential Design Codes (including plot ratio greater than 70m <sup>2</sup> ) to ensure ancillary dwellings are able to meet the needs of people with disabilities.
	m) Continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning.
	n) Lobby the state government to remove the requirement in the Residential Design Codes for Section 70A Notifications to be included on the certificate of title requiring at least one occupant to be a disabled or physically dependent person or aged person.
	o) Lobby the state government to empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity, for example requiring two storey development and mandating minimum floor areas.

PARTNERSHIPS	
<b>PARTNERSHIPS</b>	a) Continue to examine the City's freehold land assets with the view to maximising the provision of new land for residential development within established suburbs that have been the subject of revitalisation strategies.
	b) Work with the private sector to identify landholdings across the City which would be prime opportunities for affordable housing projects, and advocate for these landholdings to pursue affordable housing through partnerships and design based approaches.
	c) Ensure the feasibility of aged care accommodation is investigated as part of any Master Plan/Structure Plan for the Council's administration building site identified in the Phoenix Central Revitalisation Strategy.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

LEADERSHIP, ADVOCACY AND COMMUNICATION	
LEADERSHIP, ADVOCACY AND COMMUNICATION	a) Provide information to the community on the issue of housing affordability and diversity, and promote its benefits.
	b) Investigate innovative tools to convey housing and neighbourhood design issues in the City of Cockburn, and to explain the way these issues are being addressed, including: <ul style="list-style-type: none"> <li>• Integration of the City's existing sustainability initiatives with affordable housing information to create an 'Affordable Living' portal on the City's website that also provides links to useful information and tools.</li> <li>• The development of an interactive diagram setting out the principles of affordable living (housing diversity, walkable neighbourhoods, compact urban form etc.) to assist with communicating these concepts visually.</li> <li>• Preparation of Affordable Living Fact Sheets to help communicate to the community what Council is seeking to achieve with its initiatives.</li> </ul>
	c) Continue to explore new opportunities for sustainability initiatives that assist with reducing the cost of living for households, including affordable transport.
	d) Identify measures to improve public perceptions of higher density development, including the opportunities for positive media portrayal at a local level.
	e) Promote the Adaptable Housing (Universal Housing Design Principle) and the Livable Homes Design Guidelines.
	f) Prepare a 'Housing Options' information kit setting out information and options for special purpose dwellings.
	g) Lobby the state government to: <ul style="list-style-type: none"> <li>• Undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing;</li> <li>• Empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity.</li> </ul>

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 1.0 INTRODUCTION

The City of Cockburn, like much of Western Australia, is facing a significant challenge in housing affordability.

The cost of land and housing grew dramatically in Western Australia between the December quarters of 2003 and 2007, and the median price for housing in Perth doubled<sup>3</sup>. This had a major impact on housing affordability in Western Australia, the effects of which are still being felt today. The rising cost of housing, which represents around 30-40 per cent of household expenditure, is the single largest driver of financial hardship for low income households in Western Australia<sup>4</sup>.

There has been a growing focus on the requirement for action and cooperation across all levels of government to address housing affordability issues, particularly evidenced by the COAG National Affordable Housing Agreement (NAHA) 2009 which aims to ensure that all Australians have access to affordable, safe and sustainable housing.

Without affordable housing individuals and families are more likely to suffer increased levels of financial and personal stress. Secure accommodation is foundational to so many facets of life, including education, employment and health.

Households struggling to pay housing costs are faced with a series of issues including unmanageable levels of debt, working and travelling long hours to cover housing costs, living in overcrowded or substandard housing and sacrificing essentials such as heating, food, medication and education. Coping strategies, such as frequent moving, can contribute to a lack of social cohesion, and impact the whole community<sup>5</sup>. A lack of affordable housing can negatively impact on the local economy by making it difficult for businesses to attract and retain employees.

Local Government has an important role to play in facilitating affordable and diverse housing, and is well

placed to identify local need and identify specific responses to housing issues within the community.

The City of Cockburn recognises the importance of affordable and diverse housing to respond to changing needs and expectations as a key objective.



### 1.1 WHAT IS AFFORDABLE HOUSING?

This Strategy uses the definition of affordable housing set out in the Department of Housing Affordable Housing Strategy; Opening Doors 2010 – 2020:

*Housing is affordable when households, particularly low and moderate income households, which are renting or purchasing, are able to pay their housing costs and still have sufficient income to meet other basic needs such as food, clothing, transport, medical care and education. Households with incomes below 120 per cent of the gross median income of all households are considered to be experiencing housing stress when their housing costs exceed 30 per cent of gross household income.*

<sup>3</sup> Real Estate Institute of Western Australia (REIWA) (2003). *REIWA Market Update December 2003 Quarter*. REIWA.

Real Estate Institute of Western Australia (REIWA) (2008) Op. Cit.

<sup>4</sup> Western Australian Council of Social Services Inc (2012) *Cost of Living Report 2012*

<sup>5</sup> AHURI (2007) *Housing Affordability, a 21st Century Problem*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 1.2 CURRENT CITY OF COCKBURN INITIATIVES

The City of Cockburn recognises that access to secure, appropriate and affordable housing is a fundamental requirement and an essential component of an inclusive and sustainable city.

The City of Cockburn has been proactive in identifying the importance of diverse and affordable housing in a number of strategic plans, and has worked to address the issue through a number of initiatives, including the following:

- Preparation of urban revitalisation strategies that seek to encourage a variety of dwelling types, and promote walkable neighbourhoods.
- Introduction of affordable housing incentives for the Cockburn Coast area.
- Online 'Affordable Housing' Toolkit.
- Ensuring lot and dwelling diversity through the local structure planning process.
- Sustainable and Affordable Living brochures, including 'Building a Sustainable Home'; and 'Sustainable Renters Guide'.
- Subdivision of Council owned freehold land in Hamilton Hill to create affordable residential lots.
- Leasing of Council land to the MS Society for the purposes of a Respite Facility.
- Leasing of a reserve to the MS Society for a Care Facility.
- Leasing of land for affordable aged care facility in Coolbellup.

This Strategy aims to build on the success of these initiatives.



Figure 1. Apartments in Cockburn Central – A transit oriented development.

## 1.3 CITY OF COCKBURN HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

The 2009 Social Housing Taskforce report 'More than a Roof and Four Walls' identified a number of initiatives to deliver an additional 20,000 social and affordable housing units across Western Australia by 2020. One of these key initiatives is that Local Governments are to develop local housing strategies that identify the future affordable housing needs of their communities.

An Affordable Housing Strategy is often recommended as a supplement for a local housing strategy; focusing on resourcing and directing planning actions, including policy formulation, to address the community's need for affordable housing.

Local housing strategies generally comprise an analysis of local housing supply and demand, future oriented

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

demographic and market trends, as well as policy statements and recommendations for planning processes, town planning schemes, and development controls.

The City's approach has been to develop urban revitalisation strategies which serve the function of a Local Housing Strategy. This approach has been successful in the City, and in accordance with the City's Strategic Community Plan it is proposed to continue, as follows:

*1.4.1 Ensure our strategic land use planning in the form of: the Local Planning Strategy, Town Planning Scheme, revitalisation strategies and structure plans, achieves a robust planning framework delivering adequate supply and diversity in housing choice.*

In addition to the preparation of urban revitalisation strategies, it is considered that the issue of housing affordability and diversity needs to be examined in detail across the whole of the City. A Housing Affordability and Diversity Strategy provides the opportunity to do this.

### 1.4 MORE THAN SIMPLY SUPPLY

A final point of reflection before embarking on the Strategy is to help frame the focus of the Strategy. What is acknowledged in embarking on this Strategy is the need to look beyond simple supply side causes of housing affordability and diversity issues.

To explain this, discussion about housing affordability has often focused on so called planning issues,

particularly at the local government level. These are alluded to:

- contribute to a shortage of residential zoned land;
- which causes a shortage of new residential lots;
- thereby causing an ultimate shortage of new (family sized) residential dwellings.

Particularly from a Perth perspective, what isn't acknowledged in respect of these supply side discussions is the nature of underlying demand. The focus on merely increasing the supply of more residential zoned land to create more lots to develop more dwellings on, feeds an unsustainable 'expectation' of this being the only market of housing within Perth and Australia. This is very clearly illustrated by the sprawling environments of all Australian cities, and the costs (both direct and indirect) which this mode of housing often has associated where it is located in fringe areas of cities. An alternative perspective to the supply side focus then, is to consider how we need to also go about lifting the profile and demand for 'alternative' forms of related housing - being through urban consolidation and infill. This becomes an important element of consideration for this Strategy, and a very pertinent issue to the context of Cockburn.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 1.4 KEY OBJECTIVES

*Key objectives of the Housing Affordability and Diversity Strategy are:*

- a) To provide households with access to housing that is appropriate to their needs in terms of size, physical attributes and location.*
- b) To provide housing that is affordable to households of varying financial capacity.*
- c) To provide a variety of housing types in locations that have good accessibility to public transport, and essential services.*
- d) To promote affordable living, taking into consideration the total cost of living in a dwelling, including energy and water consumption, the price of transport to access employment and essential services, and other daily needs impacted by location.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 2.0 POLICY CONTEXT

### MORE THAN A ROOF AND FOUR WALLS (SOCIAL HOUSING TASKFORCE, 2009)

The Social Housing Taskforce believes that an additional 20,000 social and affordable housing units can be created across Western Australia by 2020. To achieve this significant increase in housing units the following initiatives must be implemented:

1. Implementation of a whole of government approach to the provision of social and affordable housing through development of a State Affordable Housing Strategy. This Strategy will clearly outline the roles, responsibilities, key initiatives, funding and performance measures that will underpin the development and delivery of affordable housing across Western Australia over the next 10 years.
2. Local Governments to develop local housing strategies that identify the future affordable housing needs of their communities.

### AFFORDABLE HOUSING STRATEGY 2010-20 OPENING DOORS TO AFFORDABLE HOUSING OPENING DOORS (DEPARTMENT OF HOUSING)

The Affordable Housing Strategy; Opening Doors 2010 – 2020 is the Department of Housing's mandate to lead vital changes in the housing system.

It identifies a number of strategies for improving housing supply, including the following:

#### 1. Implement Planning Reforms

*Greater emphasis will be given to encouraging more diversity in the size, nature and choice of dwellings offered in the marketplace. Large homes in sprawling suburbs are not always affordable and often unsuited to the growing number of single-person households and childless couples. By providing more choice, better aligned to the needs of smaller households, the number of affordable entry points (and affordable living opportunities) can be increased for those on lower*

*incomes. The government will therefore work with industry and local government to facilitate demonstration trials and offer more choice in local housing solutions and alternatives.*

*Housing strategies will be developed by local government authorities and regional bodies to identify current and future housing needs. This will require Local Planning Schemes to include provisions that facilitate the development of affordable housing to meet the current and future needs of communities. Local governments will be encouraged to incorporate requirements and voluntary incentives for developers to include wider affordable housing components in developments above a specified size, offset by appropriate concessions and/or plot ratio bonuses. Formal inclusionary zoning will not be supported.*

### NATIONAL AFFORDABLE HOUSING AGREEMENT

The National Affordable Housing Agreement (NAHA) aims to ensure that all Australians have access to affordable, safe and sustainable housing that contributes to social and economic participation. The NAHA is an agreement by the Council of Australian Governments (COAG) that commenced on 1 January 2009, initiating a whole-of-government approach in tackling the problem of housing affordability.

Under the terms of the NAHA, all parties to the Agreement (including local government) are accountable to the community for their performance against agreed objectives and outcomes in respect of their allocated roles and responsibilities.

### COUNT ME IN: DISABILITY FUTURE DIRECTIONS STRATEGY

This document was launched by the Disability Services Commission in 2009. The strategy outlines the framework that is required to achieve the goal of a genuinely inclusive community experience for people with all forms of disability.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

The strategy is relevant for local government as it outlines a long-term process of wide ranging changes to areas such as housing, transport, community attitudes, education, employment and technology, and service delivery.

The Disability Future Directions Strategy identifies what are termed "pathways". These "pathways" have wide ranging relevance for the development of Disability Action and Inclusion Plans (DAIP) strategies and actions, as it articulates aspirations such as:

- Developing well-planned communities linked to streamlined transport to create welcoming and stimulating places to live which are accessible and enable people to move around easily and safely – in and outside their homes, on footpaths, in cafes, getting to schools, parks and other amenities.
- Town planning which incorporates comprehensive universal design principles at all levels, including public open spaces and building, businesses and business districts, zoning and public housing.

## CORPORATE STRATEGIC PLANS

### CITY OF COCKBURN STRATEGIC COMMUNITY PLAN 2012-2022

One of the key objectives of the City's Strategic Community Plan is:

*Diversity of housing to respond to changing needs and expectations (1.1.4).*

*Ensure our strategic land use planning in the form of: the Local Planning Strategy, Town Planning Scheme, revitalisation strategies and structure plans, achieves a robust planning framework delivering adequate supply and diversity in housing choice.*

### CITY OF COCKBURN AGE FRIENDLY CITY STRATEGIC PLAN

A key outcome of the City's Age friendly City Strategic Plan is that the ageing population in the City of

Cockburn has access to affordable suitable housing options that allow them to age safely and be socially supported within the community to which they belong.

### CITY OF COCKBURN YOUTH SERVICES STRATEGIC PLAN

The City's Youth Services Strategic Plan identifies that there is insufficient crisis and transitional housing options for young people in Cockburn with Anglicare operating the only service.

### CITY OF COCKBURN DISABILITY ACCESS AND INCLUSION PLAN 2012- 2017

The intention of the development of a Disability Access and Inclusion Plan (DAIP) is to provide a framework by which to ensure that people with disabilities have equitable access to a Public Authorities buildings & environment, services and information, to facilitate each individual's independence, inclusion & opportunities within the community.

With regards to planning and development, which are of particular relevance to this Strategy, the following principles are set out, to be reflected in all planning and development processes and activities:

- a) Local area community needs and priorities are reflected.
- b) The diversity of community requirements are reflected in planning for social, physical and economic infrastructure.
- c) Services are developed and provided which counters racist, ageist, sexist and other discriminatory attitudes.
- d) Resources are allocated to reflect specific needs of community members
- e) A diversity of resident representation is reflected in planning processes.

## STATE PLANNING CONTEXT

### STATE PLANNING STRATEGY (2012)

One of the objectives of the State Planning Strategy is:

*'Affordable living through housing diversity and compact settlements'.*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

*The State's communities will provide diverse housing opportunities suited to different income levels, lifestyle choices and household types. Compact settlement structures will be mixed use and transit orientated providing access to employment, services and amenities.*

*Development will be designed in a way to use natural resources efficiently and sustainably.*

The State Planning Strategy acknowledges that affordable living is an important contributor to social wellbeing and economic growth. It includes not only the cost of housing but also the basic household running costs of utilities such as water and energy as well as the transport costs associated with travelling to work, education, shopping and community facilities. Affordable living also includes the financial cost of living such as the price of food, transport, shelter and the level of rental and mortgage stress.

The following have been identified as being of particular importance:

- the mix, type and location of available and planned housing
- the efficiency of the built form such as consumption rates of utilities including energy and water
- neighbourhood design such as lot layout, orientation, density, transit orientated development options
- proximity to employment opportunities, essential facilities, services and social activities

The State Planning Strategy notes the following key facts that relate to housing affordability in Western Australia:

- Over 60 per cent of households are now one or two people, however over 70 per cent of housing stock was developed as a family home.
- In 1976, housing loan repayments consumed a quarter of average full-time income. Recent figures show that housing loan repayments consume about one third of the median household income

It sets out the following strategic approach to planning for affordable living:

- Communities provide diverse housing opportunities for different income levels, lifestyle choices and household types
- Compact and diverse settlement structures
- Development conserves consumption of natural resources

### DIRECTIONS 2031 AND BEYOND: METROPOLITAN PLANNING BEYOND THE HORIZON

*Directions 2031* is the latest spatial planning framework for Perth and Peel and outlines the planning vision and direction which will guide the planning of the City to 2031 and beyond.

*Directions 2031* identifies the connected city model as the preferred medium-density future growth scenario for the metropolitan Perth and Peel region.

A connected city pattern of urban growth is characterised by:

- planning for an adequate supply of housing and land in response to population growth and changing community needs;
- facilitating increased housing diversity, adaptability, affordability and choice;
- planning and developing key public transport corridors, urban corridors and transit oriented developments to accommodate increased housing needs and encourage reduced vehicle use.

To achieve a connected city pattern of growth, *Directions 2031* has set the following targets as medium to long-term aspirations and to ensure growth of the city can be sustained beyond 2031:

*50 per cent improvement on current infill residential development trends of 30 and 35 per cent; and, has set a target of 47 per cent or 154,000 of the required 328,000 dwellings as infill development.*

*50 per cent increase in the current average residential density 10 dwellings per gross urban zoned hectare; and, has set a target of*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

*15 dwellings per gross urban zoned hectare of land in new development areas*

## STATE PLANNING POLICY NO. 4.2 ACTIVITY CENTRES POLICY

State Planning Policy No. 4.2 Activity Centres Policy (SPP No. 4.2) is a state planning policy for the planning and development of activity centres throughout Perth and Peel. The main purpose of SPP 4.2 is to specify broad planning requirements for the planning and development of new activity centres and the redevelopment and renewal of existing centres in Perth and Peel.

SPP 4.2 focuses on optimising higher density residential development within walkable catchments of activity centres, as follows:

### *5.2.2 Residential density*

*(1) Commercial and residential growth should be optimised through appropriately-scaled buildings and higher-density development in walkable catchments of centres.*

*(2) Higher-density housing should be incorporated within and immediately adjacent to activity centres to establish a sense of community and increase activity outside normal business hours. Performance targets for residential density are in Table 3.*

### *6.2.3 Housing density targets*

*(1) Activity centres should be coded under the Residential Design Codes, applying activity centre and built form-based controls to enable housing development that complements the desired scale and intensity of other development in the centre.*

*(2) Local planning strategies and schemes and activity centre structure plans should optimise housing potential in walkable catchments and meet density targets (Table 3).*

## STATE PLANNING POLICY 3.1 RESIDENTIAL DESIGN CODES

The R-Codes provide a comprehensive basis for the control of residential development throughout Western Australia. Housing diversity is addressed to some extent in the R-Codes, through the following key provisions.

The introduction of the multi-unit housing code in late 2010 sought to encourage development of multiple dwellings, particularly on land zoned R30-R60. In this way it seeks to encourage housing diversity and affordability by expanding the permissible range of housing in areas zoned R30-R60.

The R-Codes offers incentives for the development of smaller dwellings or aged and dependent persons' dwellings. It sets out that the minimum site area for these dwellings is one third lower than would otherwise be applied. This provision therefore allows up to 50 per cent more dwellings to be provided on the site if they are single bedroom or aged and dependent person's dwellings.

In August 2013 there were key changes to the R-Codes that are relevant to housing diversity as follows:

- Increased the maximum floor area of single bedroom dwellings from 60 square metres to 70 square metres, to provide greater flexibility and improved amenity for such dwellings.
- Removed restrictions on ancillary dwellings so that they are no longer required to be occupied by a family member of the occupiers of the main dwelling; and increased the maximum floor area from 60 square metres to 70 square metres.

## STATE PLANNING POLICY 3 URBAN GROWTH AND SETTLEMENTS

State Planning Policy 3 Urban Growth and Settlements (SPP No. 3) sets out the principles and considerations which apply to planning for sustainable urban growth and settlements patterns in Western Australia.

In regard to affordable housing, it includes the following reference:

*Affordability of housing is a key issue. There is a need to maintain a supply of affordable land for housing and affordable housing products for all in the community including those with special needs. The majority of new*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

*affordable housing land is in the outlying suburbs, reinforcing the need for medium and higher density housing in inner and middle suburbs for low to middle income households as well as in the growth corridors.*

## LIVEABLE NEIGHBOURHOODS

Liveable Neighbourhoods is a WAPC operational policy for the design and assessment of structure plans and subdivision for new urban areas in the metropolitan area and country centres.

The fundamental principle of Liveable Neighbourhoods is walkable mixed-use neighbourhoods which reduce car dependence; facilitate safe, efficient and pleasant walking, cycling and driving; and foster a sense of community and strong local identity in neighbourhoods.

## DEVELOPMENT CONTROL POLICY 1.6 PLANNING TO SUPPORT TRANSIT USE AND TRANSIT ORIENTED DEVELOPMENT

Development Control Policy 1.6 Planning to Support Transit Use and Transit Oriented Development (DC 1.6) encourages the integration of land use and transport planning, through the promotion of higher residential densities and mixed use developments within the walkable catchments of transit facilities.

Transport orientated development provides an alternative to car-based suburban and urban fringe development. The purpose of this type of development is to reduce car dependence; to increase accessibility for those without access to private cars; to reduce congestion on the road network and the demand for new road space; to reduce fuel consumption and air pollution; and to provide quality diverse and affordable forms of housing and development.

## LOCAL PLANNING CONTEXT

### CITY OF COCKBURN LOCAL PLANNING STRATEGY

The City's Local Planning Strategy was adopted in 1999 and sets out actions for the Strategies and Actions included in the State Planning Strategy. It sets out the long-term planning directions for the municipality and provides the rationale for the zones

and other provisions of the TPS3. The LPS sets out the City's general aims and intentions for future long-term growth and change.

The following strategies from the LPS directly relate to the formulation of the LCS:

*Strategy (k) Respond to the changing needs of the population.*

*Actions - (2) Ensuring that there is an appropriate housing and density mix to fulfil existing and potential demand from aged people, Aboriginal people, people with disabilities, non-traditional families and different ethnic groups.*

*Strategy (m) - Provide a range of housing opportunities.*

*Actions - (1) Encourage the provision of a range of lots and housing types in large comprehensively planned development projects or smaller redevelopments to reflect the diverse needs of the community.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 3.0 HOUSING NEEDS ASSESSMENT

To assess the City's future housing needs, a demographic projection analysis has been undertaken, with a focus on household composition and size, and age structure. This is followed by an assessment of the current and projected housing stock to determine the appropriateness of this housing for current and future households.

To address housing affordability 'low and moderate income' households have been defined to assess and plan for the housing needs of these households. A market assessment has enabled household incomes to be assessed against housing prices and rental costs to ascertain housing affordability. Census data has been supplemented with real estate data and other research that has been undertaken to analyse trends on housing prices and rents.

## 3.1 FORECAST DEMOGRAPHIC PROFILE

### POPULATION PROJECTIONS

The total population of the City of Cockburn is expected to increase by over 63,491 by 2031, taking it from a 2014 population of 101,973 persons to 165,465 persons in 2031. This is a growth change of 62.3%, at an average annual growth rate of 3.7%. This is based on an increase of over 25,174 dwellings during the period, with the average number of persons per household falling slightly from 2.70 to 2.66 by 2031.

Table 1 and Figure 2 below show where the additional population is likely to be accommodated. This is based on land availability and the current planning framework, indicating that this population increase will be seen in Coogee-North Coogee (primarily the Cockburn Coast area), South Lake-Cockburn Central (primarily Cockburn Central), Hammond Park-Wattleup-Henderson, Success, Aubin Grove-Banjup and Beeliar. These areas have some substantial portions of land identified for residential development (primarily zoned 'Development' zone and requiring future Structure Plans).

### Population and age structure map - 0 to 85

City of Cockburn, 2014 to 2031 percent change

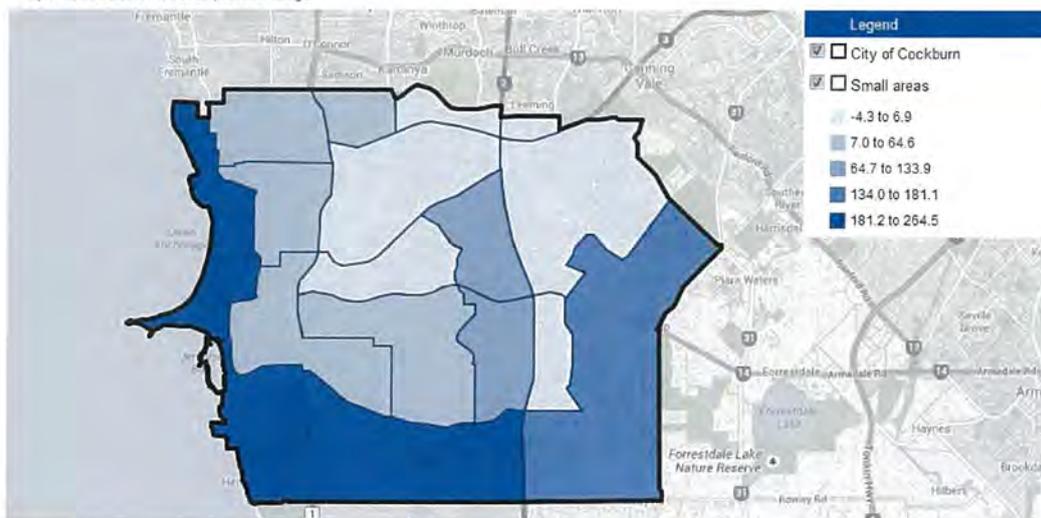


Figure 2. City of Cockburn Population Forecast - Percentage Increase from 2014-2031

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

**TABLE 1: CITY OF COCKBURN POPULATION FORECAST BY SUBURB (2014-2031)**

City of Cockburn	2014		2031		Change between 2014 and 2031		
	Area	Number	%	Number	%	Number	%
City of Cockburn		101,973	99.6	165,465	99.5	+63,491	+62.3
Atwell		9,220	99.6	8,875	99.5	-346	-3.7
Aubin Grove - Banjup		6,970	99.6	16,314	99.6	+9,343	+134.0
Beeliar		7,150	99.6	9,404	99.5	+2,254	+31.5
Bibra Lake		6,188	99.7	6,215	99.6	+27	+0.4
Coogee - North Coogee		6,259	99.6	22,756	99.6	+16,498	+263.6
Coolbellup		5,276	99.6	7,465	99.6	+2,189	+41.5
Hamilton Hill		10,514	99.6	15,610	99.6	+5,097	+48.5
Hammond Park - Wattleup - Henderson		4,471	99.6	12,572	99.5	+8,101	+181.2
Jandakot		2,859	99.6	2,736	99.6	-123	-4.3
Leeming		2,300	99.6	2,299	99.6	-1	0
Munster		4,520	99.6	5,844	99.6	+1,324	+29.3
North Lake		1,313	99.5	1,327	99.5	+13	+1.0
South Lake - Cockburn Central		7,766	99.6	18,125	99.5	+10,359	+133.4
Spearwood		9,743	99.6	12,067	99.6	+2,323	+23.8
Success		9,400	99.6	15,486	99.6	+6,086	+64.7
Yangebup		7,907	99.6	8,246	99.6	+339	+4.3
Rottne Island		116	99.6	124	99.6	+8	+7.0

### AGE STRUCTURE

The overall population of the City of Cockburn is ageing, a trend which is seen across Australia. In 2014 27.4 per cent of the population of the City of Cockburn were over 50 years, and by 2031 this will have risen to 29.5 per cent. Of particular note the number of residents over the age of 70 will more than double from 7,189 in 2014, to 14,869 in 2031.

The ageing population has significant implications for housing, including the need for aged care facilities, smaller dwellings for smaller households, and

adaptable housing to allow people to remain in their own homes. It is important to note that the care needs of older Australians vary, and it is therefore important that housing and care options are flexible.

While full ownership is the dominant tenure type for older Australians, the proportion of those aged over 65 living in mortgaged dwellings and in private rentals has increased in recent years.

In the City of Cockburn there are a significant number of older people that own their home, however they also comprise a large number of households in public housing and rental accommodation. The number of

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

older people in low income, rental households in Western Australia is projected to almost double by 2026<sup>6</sup>.

In the City of Cockburn there is a steady projected increase in young people aged between 15 and 24 years. In general young people are characterised by shared rental tenure, and low home-ownership rates. This group can be vulnerable to housing stress as they transition from the family home to independent living.

They can also face a number of barriers entering the home ownership market, such as difficulty with obtaining appropriate deposits; lower incomes; higher levels of part-time work; and student debts.

Young people in the City of Cockburn and Perth Metropolitan area generally are also higher users of public transport, with lower rates of car ownership. Therefore housing located within close proximity to public transport is particularly important for this age group.

### HOUSEHOLD COMPOSITION AND SIZE

In 2014 the dominant household type in the City of Cockburn was 'Couple families with dependents', which accounted for 36.8 per cent of all households. 'Couple families with dependents' were the dominant household type in all suburbs, with the exception of Hamilton Hill and Coolbellup.

Across most suburbs within the City of Cockburn there is a projected steady increase in 'Couple families with dependants', amounting to an additional 7,956 households across the whole City by 2031.

However, in a number of suburbs, including Bibra Lake and Atwell, there is a forecasted decrease in the actual number of 'Couple families with dependents', and a substantial increase in the number of one and two person households.

Across the City there is a significant projected increase in 'Lone person households', which will increase by 6,176 households and account for 22.5 per cent of all households by 2031.

There are various situations in which a person will live by themselves; people not forming live-in partnerships, as a result of a relationship breakdown, or as a result of becoming widowed. As older people are more likely to live alone (most often because of widowhood), an ageing population has the effect of increasing the number of lone person households<sup>7</sup>.

Across Australia almost two-thirds of the increase in lone person households between 2006 and 2031, is projected to be among people aged 60 years and over.



*22.5% of all households will be lone person households by 2031*



*Two and one person households will be the dominant household type by 2031*

<sup>6</sup> AHURI (2008) *Rental Housing For Lower-Income Older Australians*, Issue 96

<sup>7</sup>ABS (2013) 3222.0 - Population Projections, Australia, 2012 (base) to 2101

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## Forecast household types map - Lone person households

City of Cockburn, 2014 to 2031

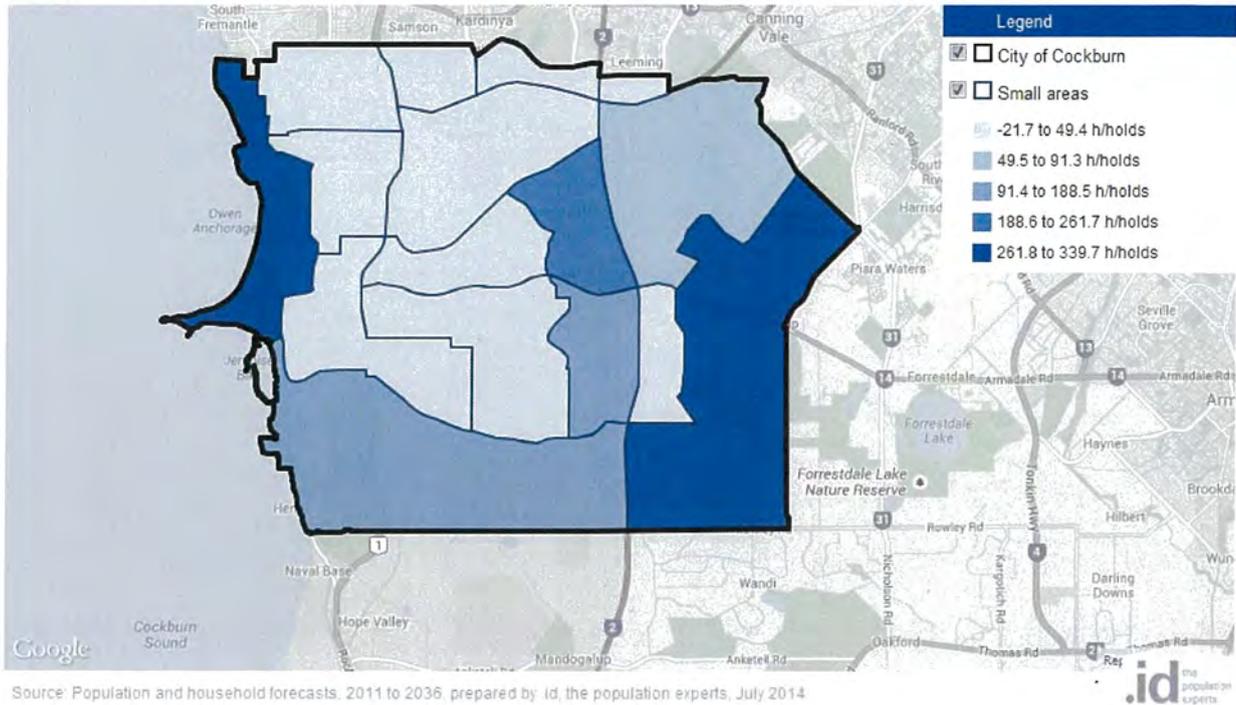


Figure 3. City of Cockburn Household Types Map – Change in Lone Person Households (2011-2031)

## Forecast household types map - Couple families with dependents

City of Cockburn, 2014 to 2031

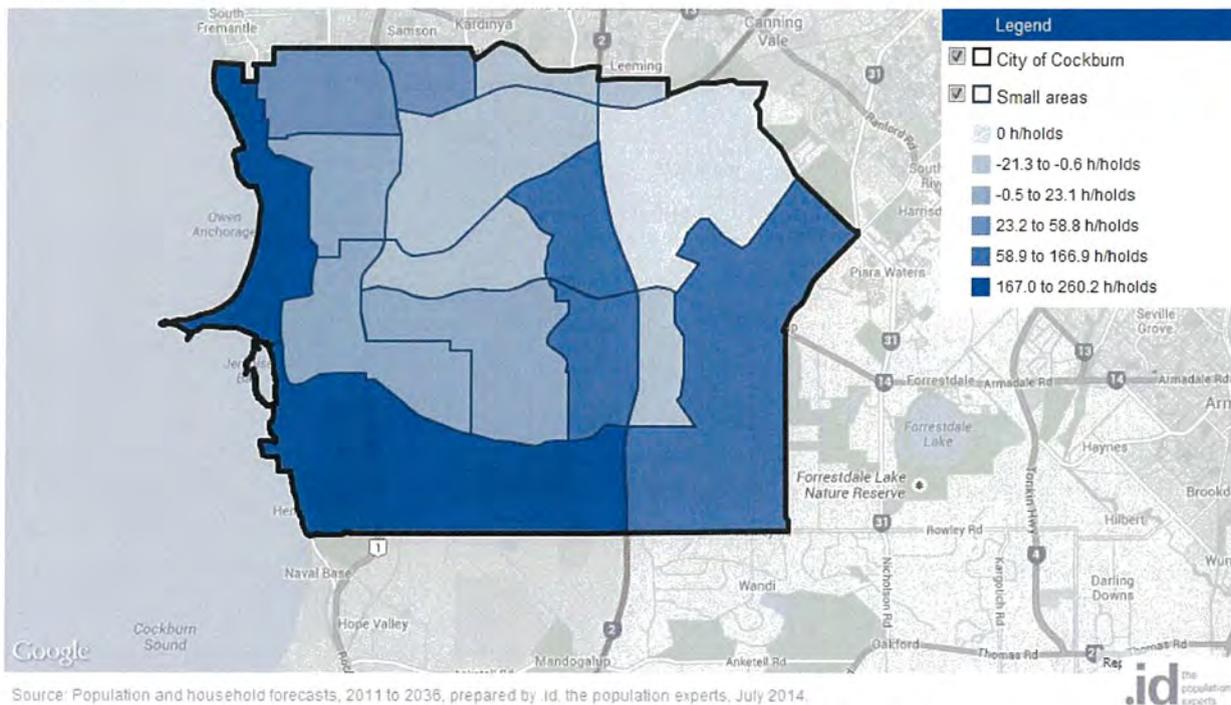


Figure 4. City of Cockburn Household Types Map – Change in Couple Families with Dependents (2011-2031)

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

'Couple households without dependents' are forecast to increase by the greatest number, with an additional 6,826 households forecast by 2031, to represent 27.2 per cent of households. The addition of lone person and couple only households will be the dominant household type in the City of Cockburn by 2031.

The growth in couples without children is in part due to the increasing tendency for couples to not have children, but is largely due to the ageing of the population<sup>8</sup>.

This trend is already apparent in Hamilton Hill and Coolbellup, whereby 'Lone person households' account for around 32 per cent of all households. These changing household occupancy trends are seen across the Perth metropolitan area, and in fact most of Australia. In Western Australia 52 per cent of the forecast population growth to 2021 will be lone people or couples over 65; and a further 19 per cent will be singles or couples under 60 years of age<sup>9</sup>. This indicates that there will be significant demand for smaller dwellings into the future.

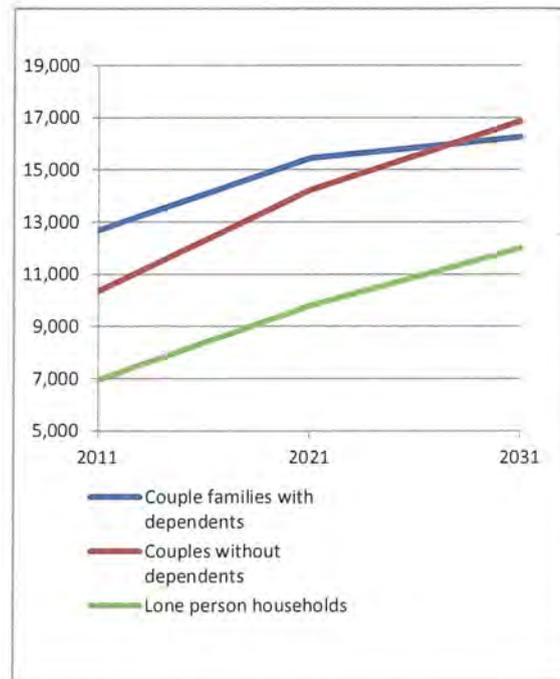


Figure 5. Selected Households Types Total No. 2011-2031

City of Cockburn Type	2014		2031	
	Number	%	Number	%
Couple families with dependents	13,780	36.8	21,736	35.3
Couples without dependents	9,949	26.5	16,775	27.2
Group households	1,260	3.4	1,861	3.0
Lone person households	7,667	20.5	13,843	22.5
One parent family	3,884	10.4	5,920	9.6
Other families	941	2.5	1,503	2.4

<sup>8</sup> ABS (2009) *Future population growth and Ageing, Australian Social Trends*

<sup>9</sup> Department of Housing (2009) *More than a Roof and Four Walls, Social Housing Taskforce Final Report*

Table 3: Forecast Household Types

HOUSEHOLD TYPE	2014	2031	
COUPLES WITHOUT CHILDREN 	26%	27%	↑
COUPLES WITH CHILDREN 	36%	35%	↓
LONE PERSON HOUSEHOLDS 	20%	22%	↑
SINGLE PARENT HOUSEHOLDS 	10%	9%	↓
GROUP HOUSEHOLDS 	3%	3%	

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## PEOPLE WITH DISABILITIES

In the City of Cockburn there are approximately 13,420 people with a disability, representing 18 per cent of the population. Appendix A shows a breakdown of the severity of residents' disability, and shows the number of people with a disability by age group.

The largest age group of people with disabilities are between 25-64 years of age (7,818) persons, and there are 3,335 persons over 65 years of age with a disability. The number of people needing daily assistance increases significantly over the ages of 75, with 42 per cent of residents requiring daily assistance.

Many individuals with disabilities rely on the disability support pension, others find it challenging to get and keep rewarding and well-paid jobs. Family and carers work less hours than others or do not work at all. People with disabilities are more susceptible to financial hardship because they can face extra costs relating to housing (including modifications to existing houses and lack of options in the housing market), transport, equipment and additional and ongoing health care costs.

Under these circumstances there are reduced opportunities to make investments and build wealth. One in four Australians with a disability live below the poverty line, using the internationally accepted poverty line of less than 50 per cent of median equivalised disposable income<sup>10</sup>. This rate is twice as high as the general population.

Modifications to housing by people with a disability can be expensive and place pressure on limited financial resources. Moving house to find a better house design suited to their specific needs is often not a viable option due to the high 'sunk costs' in the current accommodation.

People with disabilities are more likely to be renting than owning their home and are often unable to obtain or afford homes in the areas that are close to work, transport, family, friends and activities. Access to

transport is a major factor in determining where people with a disability would like to live<sup>11</sup>.

Entry into the private rental market poses challenges in securing appropriate accommodation and then in trying to gain permission to carry out modifications.

People with disabilities have been found to have longer wait periods for public housing to suit their needs.

People with a disability are vulnerable to homelessness because they may have lower incomes and are more likely to be unemployed and have limited housing options<sup>12</sup>. Therefore for people with disabilities affordable, accessible housing that conforms to the principles of universal housing design is important to reducing rates of poverty and increasing opportunities for economic and social inclusion.



**1 in 4 Australians with a disability live below the poverty line**

## HOMELESSNESS

Homelessness is not just a housing problem. It is a complex issue with diverse social, economic and personal factors that relate to homelessness and the risk of becoming homeless<sup>13</sup>.

Homelessness does not just include people who are sleeping rough; it also refers to people staying in temporary, unstable or substandard accommodation. Across Australia since 2001, the rate of homelessness has remained relatively constant, but there are increasing numbers of children, families and older people experiencing homelessness. Children under the

<sup>11</sup> AHUR (2007) The housing careers of persons with a disability and family members with care responsibilities for persons with a disability National Research Venture 2: 21st century housing careers and Australia's housing future, authored by Michael Kroehn, Keith Hutson, Debbie Faulkner and Andrew Beer for the Australian Housing

<sup>12</sup> University of Adelaide (2001) Addressing homelessness amongst persons with a disability: Identifying and enacting best practice

<sup>13</sup> Commonwealth of Australia (2008) The Road Home: A National Approach to Reducing Homelessness

<sup>10</sup> ACOSS (2013) Poverty and Disability Report

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

age of 18 make up 27 per cent of people experiencing homelessness in Australia<sup>14</sup>.

In the South West Perth Metropolitan area (encompassing the Cities of Rockingham, Kwinana, Melville, Cockburn and Fremantle) there are estimated to be a total of 1,035 homeless people, and a further 637 people living in overcrowded dwellings, improvised dwellings and in marginal housing such as caravan parks.<sup>15</sup> (See Appendix C for more data).

There are a number of personal circumstances that can increase a person's risk of becoming or remaining homeless, including poor physical health; intellectual disability; drug/alcohol abuse, and family breakdown. There are also a number of structural elements that can contribute to the problem of homelessness, including poverty, unemployment, and lack of affordable housing. Research suggests that persons with a disability have a greater exposure to the risk of homelessness than the general population<sup>16</sup>.

Of the homeless people in Western Australia, 28 per cent are homeless because of financial difficulties, housing stress, and/or unemployment. A further 19 per cent are homeless due to accommodation issues, housing crisis, inadequate or inappropriate dwellings.

Addressing the shortage of affordable housing is identified as one of the ways to tackle the structural drivers of homelessness and to reduce the risk of homelessness.

It is also important to target groups that are at risk of homelessness such as older people in housing stress, people with disabilities, and very low income households with children.

While homelessness may not be thought of as a significant issue for the City of Cockburn, declining housing affordability and increasing living costs are trends that could increase homelessness for vulnerable individuals and households; particularly the number of people living in marginal or inappropriate housing, which can be a hidden problem.



*There are over 1,000 homeless people in the South West Perth Metropolitan area and 600 people living in sub-standard accommodation*

<sup>14</sup> ABS (2012) *Census of Population and Housing: Estimating Homelessness 2011*

<sup>15</sup> ABS (2012) *Op. cit.*

<sup>16</sup> University of Adelaide (2001) *Addressing homelessness amongst persons with a disability: Identifying and enacting best practice*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

### POPULATION FORECAST KEY FINDINGS:

- a) *Couples without children (two person households) and lone person households are forecast to increase significantly across all suburbs in the City of Cockburn.*
- b) *'Couple households without dependents' are forecast to increase by the greatest number across the City of Cockburn.*
- c) *There is a projected steady increase in family households with children.*
- d) *There is an ageing population, with a significant projected increase in people over the age of 70 by 2031.*
- e) *18 per cent of the population in the City of Cockburn have a disability.*
- f) *Homelessness, which includes people living in marginal or inappropriate housing, is an increasing problem, with a trend towards families with children and older people being vulnerable to homelessness.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 3.2 ASSESSMENT OF HOUSING STOCK AND URBAN FORM

### DWELLING TYPES AND SIZES

Across the City of Cockburn 86.3 per cent of dwellings are single detached houses and 53 per cent have four or more bedrooms. Dwellings with more than three bedrooms represent 81 per cent of the housing stock in the City of Cockburn.

Suburbs such as Atwell, Aubin Grove, Coogee, Bibra Lake, Leeming, Munster, North Lake, South Lake, Yangebup and Banjup are strongly characterised by separate housing which represent over 90 per cent of the housing stock, and the large majority of these dwellings have four or more bedrooms (over 70 per cent).

The charts below show the types of housing in each suburb, and the number of bedrooms. This depicts the strong dominance of detached houses with three or more bedrooms across the City.

Suburbs such as Cockburn Central, Coolbellup, Hamilton Hill and North Coogee have a greater diversity

of dwelling types. For Cockburn Central this trend is projected to continue, with development ongoing in the Cockburn Central town centre, and development commencing in the Muriel Court Structure Plan area which identifies a diversity of residential codings, with a focus on higher codings. In Cockburn Central 40.5 per cent of dwellings have one or two dwellings, and the majority of these dwellings are new apartments.

The Cockburn Coast area (North Coogee) is proposed to have a diversity of housing types, primarily with a mix of higher residential codings.

The chart below shows the predominance of dwellings with three or more bedrooms across most suburbs. When this housing stock is compared with the projected household structures for 2031, a mismatch can be seen.

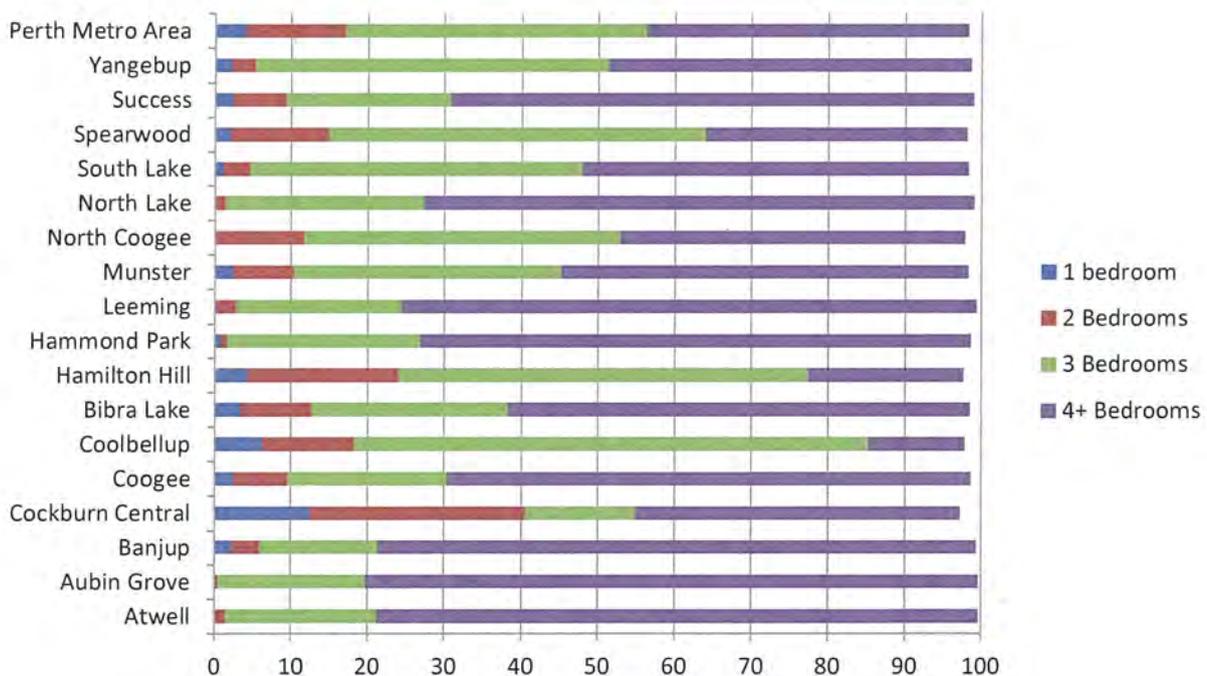


Figure 6. City of Cockburn Number of Bedrooms by Suburb (2011)

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

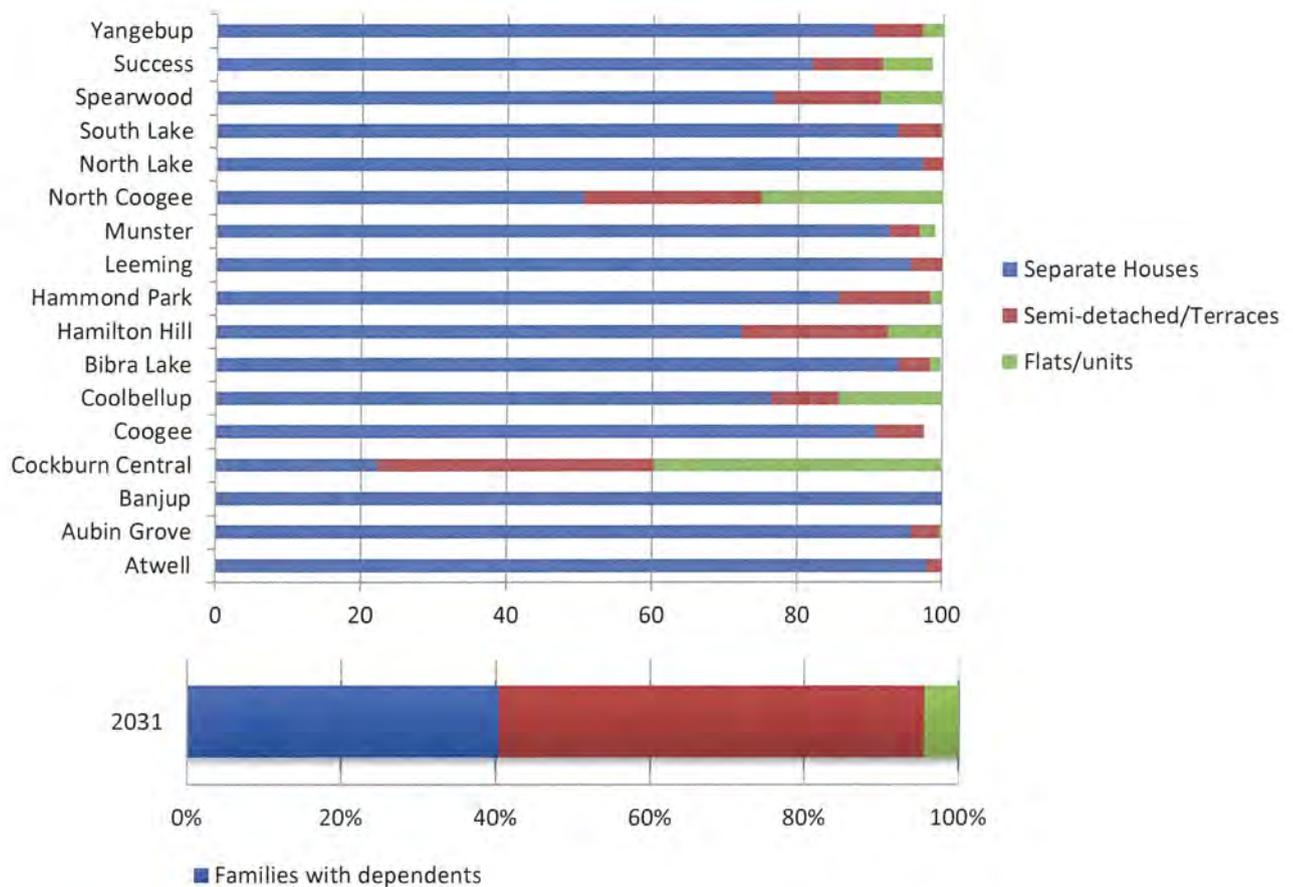


Figure 7. City of Cockburn housing types compared against projected household types

Across Australia there has been a strong trend towards the construction of medium density housing types, and in all states but Western Australia the proportion of medium density dwellings being constructed is greater than stand-alone dwellings. In Australia in the 12 months leading to October 2013 the proportion of new homes approved which are medium density homes – units, townhouses and semi-detached – has increased to 43.4 per cent in the 12 months leading to October 2013, this is up from 39.8 per cent in 2012, and is the highest level on record<sup>17</sup>.

In Western Australia while there has been an increase in the number of medium density dwellings being constructed, this has been less than half of the growth in standalone dwellings; and only 20 per cent of home approvals were for medium density dwellings over the

past year. This is considerably less than the national average of 43 per cent.

However, in the City of Cockburn over a two year period to October 2012, 33 per cent of approvals were for medium density dwellings, much higher than the state average of 20 per cent. This is largely due to development in North Coogee and Cockburn Central.

In more recent times the City of Cockburn has seen an increasing number of proposals for dwellings in mixed use developments, such as residential apartments above commercial and/or retail uses (see Figure 6). While currently there are very few of these housing types in the City of Cockburn, it is anticipated that in the next few years a number of these dwellings will be developed, contributing to an improved range of smaller dwelling types.

<sup>17</sup> Bankwest (2014) *Housing Density Report February 2013* Bankwest Financial Indicator Series

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY



Figure 8. Examples of apartments in Hammond Park and Success



In the City of Cockburn between 2006 and 2011 there was an increase in the percentage of households living in flats/units/apartments from 0.1 per cent to 2.6 per cent. It is 'Couples without children' and 'Lone person households' that are increasingly living in apartments and units, whereas 'Couple families with children' remain predominately in detached houses.

The household projections and housing stock assessment indicate a mismatch of housing throughout the City of Cockburn that is likely to become more pronounced into the future. It is evident that the City's projected housing stock will not match the needs of future households, and will not provide an adequate range housing choices for future households. This will be more pronounced in certain areas. For example residents wishing to 'downsize' in suburbs such as Aubin Grove and Hammond Park will have limited opportunities to do so in their local area. The shortage of availability of smaller dwellings throughout the City of Cockburn may result in residents living in housing that does not suit their needs, or residents being forced to buy or rent dwellings that are larger and more costly than they require.

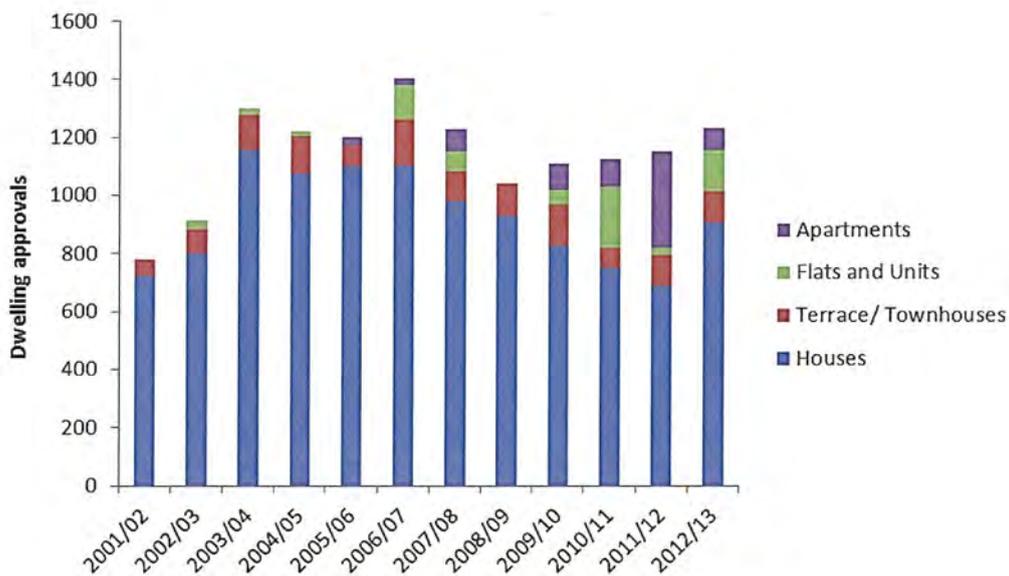


Figure 9. City of Cockburn Proportion of Dwelling Types by Year

## URBAN FORM

Over the past 50 years Perth's urban expansion has been characterised by low-density suburban development on the urban fringe. This residential development has traditionally comprised low-density housing on larger lots, and suburbs have been designed with an emphasis on car travel. This type of urban development has resulted in significant expansion of the Perth Metropolitan area to accommodate an increasing population, resulting in what is often referred to as 'urban sprawl'. This has created many residential areas with poor levels of accessibility - isolated from services and employment, and reliant on private vehicles for transport.

The unsustainable nature of urban sprawl has seen the State government adopt planning policies to encourage greater urban consolidation as a means of achieving a number of environmental, social and economic objectives, including reduced competition for land, lower resource use, particularly energy, reduced greenhouse gas emissions from transport and improved health outcomes through an increase in active transport. *Directions 2031* places greater emphasis on creating a diverse and compact city through urban consolidation to cater for the projected demographic profile population growth.

Neighbourhood design concepts now focus on reducing dependency on private vehicles, and achieving more energy and land efficient development. This type of neighbourhood design is often referred to as 'transit orientated development'. This is reflected in Liveable Neighbourhoods, the WAPC operational policy for the design and assessment of structure plans and subdivision for new urban areas. The fundamental principle of Liveable Neighbourhoods is walkable mixed-use neighbourhoods which reduce car dependency; facilitate safe, efficient and pleasant walking, cycling and driving; and foster a sense of community and strong local identity in neighbourhoods.

Across the City of Cockburn there are a range of residential codings designated under Town Planning Scheme No. 3, however a coding of 'R20' predominates. This is reflected in the current and

projected housing stock. It is important to note that while lots have been getting smaller, houses have been getting larger, and are almost double the size of homes in the 1950s and double the floor space per occupant of a new house in the 1970s.

Cockburn Town Centre has been designed as a 'Transit Orientated Development' with a vibrant mix of residential, retail and commercial properties. Residential medium to high density apartment living will cater for the diverse community of the future. Shops, food and beverage establishments and offices are connected to the surrounding area through a public transport interchange next to the town square.

Recent planning for Cockburn Central, and Cockburn Coast has focused on higher densities which has contributed significantly to the diversity of housing stock in the City of Cockburn. However, as discussed in the previous section the housing diversity is not found across all suburbs. Much of the City's current residential densities are mismatched with objectives of sustainability, and the City should continue to aim for a more compact urban form.

The City assesses all Structure Plans and subdivision against the policies of Liveable Neighbourhoods, and seeks to ensure all Structure Plans incorporate a range of residential densities.

The City has developed urban revitalisation strategies, and there have been three strategies adopted – for Spearwood (Phoenix Central), Hamilton Hill and Coolbellup. These strategies include increases to residential codings from R20 generally to R30, and to R60 and R40 within 400m of centres, and where there are high levels of accessibility to public transport.

In the Phoenix Central Revitalisation Strategy and Hamilton Hill Revitalisation Strategy these zoning changes have been implemented, and have seen the development of smaller dwellings consistent with the higher codings. This has increased the housing diversity in these areas, providing increased housing options for residents. This has included National Rental

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

Affordability Scheme ("NRAS") dwellings, further contributing to affordable rental properties.

A program for future urban revitalisation strategies has been adopted by Council as follows:

- Stage 1 – North Lake and Bibra Lake (2014/2015)
- Stage 2 – South Lake (2015/2016)
- Stage 3 – Yangebup (2016/2017)
- Stage 4 – Southern portion of Spearwood and Munster (2018/2019)

The future urban revitalisation strategies present the ideal opportunity to address housing issues identified through this Strategy, and to ensure the housing stock matches the need of future households.

## TENURE

### RENTAL DWELLINGS

In the City of Cockburn 24 per cent of residential properties are being rented. Figure 6 provides a breakdown of how the rental market is spread across different dwelling types, and indicates that the large majority of rental dwellings in the City of Cockburn are separate houses (71 per cent), although the rental housing stock is more diverse than the housing stock in general where 86.3 per cent of housing is single detached.

In terms of the housing owned by the Department of Housing, 37 per cent of their dwellings are semi-detached/terrace housing, 15 per cent are

City of Cockburn Rental Dwelling Types

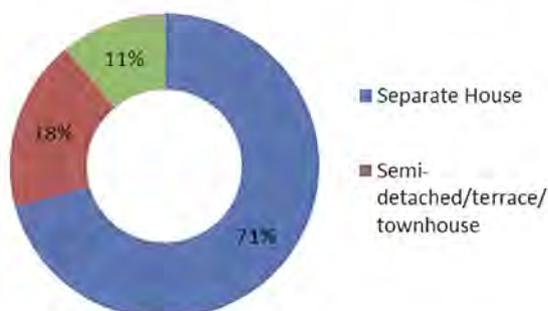


Figure 10. City of Cockburn Rental Dwelling Types

flat/unit/apartment, and 48 per cent single dwellings.

Across Western Australia the public housing waiting lists are greatest for two bedroom dwellings, indicating the need generally for smaller dwellings in the public housing system<sup>18</sup>.

## AGED ACCOMMODATION

The City of Cockburn Community Care provides Home and Community Care (HACC) and Community Aged Care Package (CACP) funded support services for seniors and younger people with disabilities who live in Cockburn. The services are funded by the Australian Government and the West Australian Government to assist people to remain living in their own home, as follows:

- Home Support Services (HACC) - providing extensive and important help with cleaning, shopping, bill paying, social support to engage in the community, respite, personal care, transport and basic home and garden maintenance. Cockburn Community Care has Aboriginal staff who can provide culturally appropriate support to Aboriginal and Torres Strait Islander clients if required.
- Community Aged Care Packages (CACP) - Designed to meet the needs of seniors with more extensive and complex needs than the HACC funded services. The package may include any or all of the other services as well as medication prompting.

For those whose care needs can no longer be met within their own homes there are two types of residential aged care within an accommodation setting. Aged care facilities are classified as 'low level' or 'high level'. Low-level residential care provides a supported environment for residents who are still able to move about but need extra help with everyday tasks like cleaning, laundry and meals. High-level residential care includes assistance for most day-to-day living activities, as well as care from either registered nurses, or from carers under their supervision, 24 hours a day.

<sup>18</sup> Department of Housing (2013) 'Housing Authority – Annual Report 2012-13'

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

Name	Location	High Care Bed	Low Care Beds	Respite	Ageing in Place
Amberley Aged Care	SPEARWOOD	114 total		Info not provided	Info not provided
Villa Dalmacia Aged Care Facility	SPEARWOOD	50	20	Yes	Yes
Bethanie Illawong	HAMILTON HILL	No	39	Yes	No
Carrington Aged Care Facility	HAMILTON HILL	60	43	Yes	Yes
Hale Hostel (Amana Living - Hale Hostel)	COOLBELLUP	No	40	Yes	Info not provided
Brightwater - South Lake Care Facility	SOUTH LAKE	No	30	Yes	No
Frank Prendergast House	SUCCESS	73	No	No	Yes
Regents Garden Aubin Grove	AUBIN GROVE	80	18	Yes	Yes
<b>TOTAL APPROX BEDS</b>	<b>567</b>	<b>263</b>	<b>190</b>		

Source: My Aged Care (<http://www.myagedcare.gov.au/>)

Short stays (respite care) in a residential aged care facility (either low or high) may be a step along the way to permanent care. Respite care offers temporary or casual residential care to support both older people and their carers.

There are currently eight aged care facilities in the City of Cockburn, with approximately 567 total aged care beds, as shown in Table 4. These beds have not been divided into low and high care beds as there is some flexibility in how services allocate beds according to the need and funding at the time.

A key mechanism used by the Australian Government in planning residential aged care service provision is the 'planning target' for levels of provision relative to population. In 2011, this target was a total of 115.3 places per 1,000 persons aged 70 and over, with 79.5

of these dedicated as residential places, and the remaining as combined places and packages. Table 5 demonstrates that by 2031 there should be 1498 aged care beds in the City of Cockburn, and currently there are only 365 beds available.

The ageing population is likely to increase demand for low and high care services and respite care over the next 20 years across the Perth metropolitan area. In particular there will be a demand for affordable aged care. However supply from the private sector is declining due to rising costs associated with construction, difficulties in securing suitable parcels of land, and funding constraints<sup>19</sup>. This is likely to result in an increased shortage of aged care beds in appropriate places, lower quality of service and greater pressure on Government to provide more of these services through hospitals and other care systems, at far greater cost.

	2006	2011 (6,388 persons over 70)	2021 (9,651 persons over 70)	2031 (12,485 persons over 70)
Targeted beds for supported accommodation	449	766	1,158	1,498
Identified beds	365	567		

Source: My Aged Care (<http://www.myagedcare.gov.au/>)

<sup>19</sup> Department of Housing (2009) *More than a Roof and Four Walls Social Housing Taskforce final report*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

### ADAPTABLE HOUSING (UNIVERSAL HOUSING DESIGN)

Traditionally most homes have not been designed or built in a way that can easily accommodate the changing needs of households over their lifetime.

In the City of Cockburn, with the exception of purpose built aged and dependent care accommodation, very few dwellings have been built to incorporate universal design features.

A large proportion of the public housing stock is physically inappropriate for people with disabilities, and there is only an extremely small amount of public housing stock that has already modified. People with disabilities have therefore been found to spend longer periods on the public housing waiting list than people without a disability<sup>20</sup>.

Adaptable housing is designed and built to meet the changing needs of occupants across their lifetime. They are designed to be:

- easy to enter
- easy to navigate in and around
- capable of easy and cost-effective adaptation
- responsive to the changing needs of home occupants.

The Livable Housing Design Guidelines and the National Dialogue on Universal Housing Design Strategic Plan were launched by the Australian Government in 2010. These documents were developed jointly by all levels of government and the disability, aged, community, building and construction sectors. The Liveable Homes initiative was developed to increase the number of private and public homes in Western Australia that are built with universal access. It is a resource that was developed for people designing new homes or renovating existing homes. The initiative is intended to benefit people with disability and ageing Australians by designing Australian homes to meet the changing needs of home occupants across their lifetime. This will occur through the inclusion of key

<sup>20</sup>Physical Disability Australia Ltd (2011) A home of my own: The need for accessible public housing for people with physical disability in Australia.

easy living features that aim to make homes more accessible.

In the City of Cockburn 18 per cent of the population have a disability, and with an ageing population it is considered important to increase the adaptable housing stock to provide more housing options, and flexibility for in-house care.



Figure 11. Livable Housing Design - Outdoor Living Area (Werribee Apartments) ([www.livablehousingaustralia.org.au](http://www.livablehousingaustralia.org.au))



Figure 12. Livable Housing Design - Kitchen Design (Mallorca Circuit Burdell, Qld) ([www.livablehousingaustralia.org.au](http://www.livablehousingaustralia.org.au))



Figure 13. Livable Housing Design - Accessible Bathroom (Isabella Gardens, ACT) ([www.livablehousingaustralia.org.au](http://www.livablehousingaustralia.org.au))

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

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## CRISIS ACCOMMODATION

Crisis accommodation is short term accommodation for people experiencing homelessness. Its main purpose is to help people resolve a crisis situation and assist in obtaining appropriate long-term housing when the crisis is resolved. Residents can stay at a crisis accommodation facility for a short time, usually up to 12 weeks. Facilities usually include support services, including assistance to move to longer term accommodation.

Generally facilities are offered specifically for young people (up to 25 years old); single women; women with children; and single men.

The City's Youth Services Strategy (2011-2016) identifies that there is a shortage of accessible accommodation services for youth, including crisis, transitional and longer term supported accommodation options. There are a number of services in the Fremantle area, however within the City of Cockburn Anglicare currently provide the only crisis accommodation, located in Spearwood. The 'Y-Shac Spearwood' assists 15-20 year old young people who are homeless or at imminent risk of homelessness. This crisis accommodation site is a three bed facility capable of providing short stays and quick admissions.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## HOUSING STOCK AND URBAN FORM KEY FINDINGS:

- a) *The City of Cockburn housing stock is characterised by single detached residential dwellings with three or more bedrooms (81 per cent of all dwellings).*
- b) *Newer suburbs in the City of Cockburn are strongly characterised by detached houses with four or more bedrooms, with limited smaller housing options.*
- c) *There has been a shift towards a larger number of units and apartments being constructed, and an increase in new one and two bedroom dwellings, however the proportion of larger detached dwellings being constructed is still much greater than smaller dwellings types such as units, apartments and town houses.*
- d) *The majority of new smaller dwellings in the City of Cockburn are found in Cockburn Central; and older smaller dwellings, such as semi-detached duplexes, are found in Spearwood, Hamilton Hill and Coolbellup.*
- e) *There is a lack of private and public housing 'adaptable housing' that have been designed with universal principles in the City of Cockburn, and across the Perth Metropolitan area generally.*
- f) *There is an identified shortage of crisis accommodation in the City of Cockburn.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 3.3 HOUSING STRESS AND HOUSING AFFORDABILITY

Over the past 30 years, Western Australia has experienced a growing decline in housing affordability. In the last decade the unaffordability of housing has reached unprecedented levels with Western Australia's rapidly growing economy lead to a massive increase in the cost of housing<sup>21</sup>. Upward pressure on rents and house prices has led to a decline in affordable housing supply and the social housing system has been unable to cope with increased demand.

Even during the high economic growth of the last decade, property prices have consistently increased a greater rate than incomes. The past decade has also seen the cheaper entry-level housing that has been relied on by low-income households effectively removed from the market, and there are fewer opportunities at the lower price points. This is as a result of high demand and increased spending on home renovations nationally, together with the closing gap between the prices of low cost Perth apartments versus houses<sup>22</sup>.

Affordable housing has now declined to the point where average home loan repayments are now 40 per cent less affordable (in trend terms) than they were in 1980.

The housing market is influenced by a myriad of international, national and local issues that impact on costs and affordability. Further information regarding the factors influencing housing supply, demand and affordability can be found at Appendix D. These issues are acknowledged however they are beyond the scope of this Strategy. The analysis undertaken in this Strategy focuses on matters that can be directly influenced at a local government level, with a particular focus on matters that can be influenced through land use planning.

This section will define housing affordability and then identify the groups that are in housing stress.

### DEFINING HOUSING AFFORDABILITY

The definition of affordable housing set out in the *Affordable Housing Strategy 2010-2020: Opening Doors to Affordable Housing* is:

*Dwellings which households on low-to-moderate incomes can afford, while meeting other essential living costs. It includes public housing, not-for-profit housing, other subsidised housing under the National Rental Affordability Scheme together with private rental and home ownership options for those immediately outside the subsidised social housing system.*

This definition makes it necessary to define 'low and moderate income' to identify these households and assess and plan for their housing needs. 'Low and moderate-income households' have incomes below 120 per cent of the gross median income of all households. This category can be further separated as follows:

**'Very low-income'** - incomes below 50 per cent of the median

**'Low-income'** - between 50 to 79 per cent of the median

**'Moderate-income'** - between 80 and 119 per cent of the median

A widely used indicator for calculating the affordability of housing costs is that mortgage or rental payments should be less than 30 per cent of gross household income<sup>23</sup>. It is acknowledged that there are a number of limitations of using this indicator. Significantly it does not capture the opportunity costs of living in one dwelling over another. For example, a house may be cheaper to rent or buy in the outer suburbs; however accessibility to services and employment may be less resulting in higher transport costs and reduced opportunities for employment. Conversely higher housing costs in accessible locations, such as near a train station may result in significantly reduced transportation costs, particularly if a household can

<sup>21</sup> AHURI (2007) *Housing Affordability, a 21<sup>st</sup> Century Problem*

<sup>22</sup> Department of Housing (2010) *Affordable Housing Strategy 2010-2020, Opening Doors to Affordable Housing*

<sup>23</sup> Gurran, N. (2008) *Affordable Housing National Leading Practice Guide and Toolkit*, Sydney: National Commitment 2 Working Group.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

manage without a vehicle, or with only one vehicle rather than two which results in substantial savings. This ratio method also omits those who are experiencing homelessness, or living in marginal housing.

Nonetheless, it is widely considered to be the best indicator to provide an overview of housing affordability, and is therefore the measure used in this Strategy. From this gross rent and mortgage affordability has been calculated against gross household income and outlined in Table 6 below. The median incomes have been calculated for the Perth metropolitan region to

ensure that a diverse housing stock can be incorporated.

Table 6 below shows the breakdown of weekly income by household types, classified into very low, low and moderate. In the City of Cockburn there are 11.5 per cent of family households are classified as low income earners, with a significant proportion of one person families having low incomes (21 per cent) and very low incomes (38 per cent).

**TABLE 6: AFFORDABLE HOUSING RENTAL AND PURCHASE BENCHMARKS**

	Weekly Income	Affordable Rental Benchmark	Affordable Purchase Benchmark <i>22.2% of income</i>
<b>Moderate</b>	\$1169 - \$1760	\$350 - \$528	\$273,820 – \$412,252
<b>Low</b>	\$740 - \$1168	\$220 - \$350	\$173,333 – \$273,585
<b>Very Low</b>	< \$739	<\$222	\$173,099

**TABLE 7: WEEKLY INCOME FOR SELECTED HOUSEHOLDS IN CITY OF COCKBURN (2011)**

	Weekly Income (approximately)	Number and Percentage of all family households	Number and Percentage of lone person households	One parent families	Couples with children	Couples without children
<b>Moderate</b>	\$1169 - \$1760	1,617 (6.7%)	975 (15%)	642 (17.3%)	1,535 (17.7%)	2,686 (22.4%)
<b>Low</b>	\$740 - \$1168	2,787 (11.5%)	1180 (18.3%)	772 (21%)	1,064 (12.3%)	1,023 (8.5%)
<b>Very Low</b>	< \$739	3,542 (14.6%)	3628 (56%)	1408 (38%)	1,948 (22.5%)	501 (4.2%)

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## ASSESSMENT OF HOUSING COSTS AND INCOME

### RENTAL PRICES

In 2001 the average cost of rent in the City of Cockburn represented 16 per cent of the average household income, and by 2011 this had rose to 21 per cent<sup>24</sup>.

Between 2006 and 2011 household rental payments in the City of Cockburn increased by 45 per cent, whereas household and personal income increased by only around 28 per cent<sup>25</sup>.

In 2011 there were 1084 families with 'very low' household incomes (below \$799 a week) renting in the City of Cockburn, and 660 of these were in housing stress (61 per cent).

Low income households are more likely to be renting (29 per cent) compared with 17 per cent of moderate and high income families, and fewer low income households are purchasing a home. Young people and people with disabilities are also more likely to be renting their home.

	Low Income Earners	Moderate and High Income Earners
Owned outright	42%	20%
Owned with mortgage	26%	62%
Being Purchased under rent/buy scheme	1%	1%
Rented	29%	17%
Occupied Rent free	1%	1%

For low and moderate income households home ownership is more difficult than it has ever been, and entering home ownership means facing greater financial risks than households with equivalent incomes 30 years ago<sup>26</sup>. Consequently home purchase rates for low-moderate income households appear to be

declining, particularly for first time homebuyers and young families. In addition, evidence suggests that low-moderate income purchasers are more likely to be still paying off houses after retirement age<sup>27</sup>. This will see a continuing trend of low and moderate income households in rental accommodation.

Generally the rental vacancy rate can be a good measure of the state of private rentals. A rule of thumb is that a three per cent vacancy rate represents a balance between supply and demand for housing, and less than three per cent indicates a shortage and will lead to an upward pressure on rents. Perth's median rent increased because of a low vacancy rate of 1.9 per cent as of March 2012<sup>28</sup>. In September 2013 the vacancy rate in Perth was 3.1 per cent, however high rental prices are still being seen throughout the Perth Metropolitan area, including the City of Cockburn.

An overview of available properties in the City of Cockburn over a 6 month period identified that there were no properties available for private rent for very low income earners that were within the affordable rental benchmark range. However, it is noted that a significant proportion of these very low income earners are likely to be accommodated in public housing.

For low income earners, there were a small number of private rental properties available within the affordable rental benchmark range, being smaller one or two bedroom dwellings primarily in Hamilton Hill and Spearwood. There were no dwellings with more than three bedrooms that were affordable for low income earners.

For moderate income earners at the higher end of the income bracket there were a number of affordable rental properties available across the City, including larger dwellings, at any given time. However at the lower end of this income bracket there were only a

<sup>24</sup> ABS (2011) Census of Population and Housing (City of Cockburn)

<sup>25</sup> ABS (2011) Census of Population and Housing - City of Cockburn

<sup>26</sup> AHURI (2012) *What are the benefits and risks of home ownership for low-moderate income households?*, Issue 149

<sup>27</sup> AHURI (2012) Op Cit.

<sup>28</sup> Housing Industry Forecasting Group (2012) *Forecast Dwelling Commencements in Western Australia*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

small number of properties in certain suburbs that were affordable at \$350 a week.

In 2013 the median price charged to rent a 4 bedroom dwelling in the City of Cockburn was \$579, and a 3 bedroom dwelling was \$478<sup>29</sup>.

An overview of relevant rental costs for dwellings of various sizes for the City of Cockburn and the Perth Metropolitan area are provided in Table 9. This demonstrates the significant difference in rental prices dependent on the number of bedrooms, with smaller dwellings being more affordable.

**TABLE 9: AVERAGE RENT (PRIVATE DWELLINGS) BY NUMBER OF BEDROOMS FOR CITY OF COCKBURN AND PERTH METROPOLITAN AREA (2013)**

No. of Bedrooms	Average Rent (Private Dwellings)	
	City of Cockburn	Perth Metropolitan Area
1	\$325	\$400
2	\$411	\$450
3	\$478	\$470
4+	\$579	\$550

Source: REIWA Perth Suburbs Rental Data 2013

### National Rental Affordability Scheme (NRAS)

The National Rental Affordability Scheme ('NRAS') commenced in 2008 and seeks to address the shortage of affordable rental housing by offering financial incentives for the construction of dwellings to be rented to low and moderate income households at a rate that is at least 20 per cent below the market value rent. To be eligible to rent an NRAS dwelling individuals and households must be low-moderate income earners, as defined by the income eligibility limits set each year for various household compositions, dependant on the number of adults and children.

NRAS aims to increase the supply of new affordable rental housing; reduce rental costs for low and moderate income households; and encourage large-

scale investment and innovative delivery of affordable housing.

NRAS has the potential to play an important role in the supply affordable rental housing for the low- and moderate-income households as it offers a strong incentive for small scale investors to increase the affordable rental stocks. It has been found to perform best where the land cost and property values are relatively low, and across Australia most of the incentives were allocated to the areas located at long distance from CBD since the costs of these investments are relatively lower that can ensure positive returns on investments.

There have been a total of 5,470 incentives (allocated and reserved) in Western Australia, representing 14 per cent of the total national incentives<sup>30</sup>. In Western Australia not-for-profit housing providers have utilised 584 incentives (active and proposed), representing 10.7 per cent of the total incentives. In the City of Cockburn there were 62 active NRAS dwellings at the end of 2013, with another 173 proposed. These are distributed across the City, as outlined in the table below.

**TABLE 10. CITY OF COCKBURN NRAS INCENTIVES**

Incentives by Suburb	Active Dwellings	Proposed Dwellings	Total Incentives
<b>WA TOTAL</b>	<b>1,426</b>	<b>4,044</b>	<b>5,470</b>
Aubin Grove	21	2	23
Coolbellup	22	45	67
Hamilton Hill	-	6	6
Spearwood	-	20	20
Success	19	100	119
<b>CITY OF COCKBURN TOTAL</b>	<b>62</b>	<b>173</b>	<b>235</b>

Source: Australian Government Department of Social Services (2013) National Rental Affordability Scheme Monthly Performance Report 30 June 2013

<sup>29</sup> REIWA (2013) Perth Suburb Rental Data

<sup>30</sup> Australian Government Department of Social Services (2013) National Rental Affordability Scheme Monthly Performance Report 30 June 2013

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY



*Almost 3 out of 5 low-income family households renting in the City of Cockburn are in housing stress*

In Spearwood new NRAS 2 bedroom apartments were available for rent at \$312 a week, and in Aubin Grove they were available for \$360 a week which represents 20 per cent below market rent. This has made these properties affordable for low income earners, when they would otherwise have been unaffordable at full market rent. This demonstrates the success of NRAS if it could become more widespread.

An assessment of 2013 housing rental availability and prices in the City of Cockburn identifies the following key points:

- The most affordable rental dwellings are smaller one and two bedroom flats in older style apartment buildings, or one and two bedroom older style duplex dwellings in Spearwood and Hamilton Hill, but this rent is still around \$350-\$390 a week, with the cheapest being \$310 a week.
- The most affordable dwellings were ancillary dwellings ('granny flats') with rental prices around \$290-\$310 a week, found predominately in Spearwood and Hamilton Hill, with only a small number available (one or two at any given time).
- There are new one and two bedroom apartments found in Spearwood, Hamilton Hill resulting from increased codings from the Phoenix Revitalisation Strategy are generally not affordable for low income earners.
- Options for shared rental accommodation are limited, although there is availability in nearby areas outside of the City of Cockburn (such as Murdoch), driven by demand for student accommodation.
- Rental properties available under the NRAS Scheme are within the affordable benchmark

for low income households when they otherwise would not have been.

### HOUSING PRICES

In 2006 in the City of Cockburn the average mortgage repayments were 24 per cent of the average household income, and by 2011 this had risen to 30 per cent of the average household income.

Local house prices provide an estimate of the amount of available housing supply in relation to demand. Prices reflect the amount of 'active' stock available for renting or buying. House price data can provide a good basis for measuring affordability in a particular area.

Table 11 below outlines the median house prices for detached houses in the City of Cockburn, and indicates that even smaller one and two bedroom detached dwellings have a median price of \$443,000. Given that detached houses represent 86.3 per cent of the housing stock in the City of Cockburn this highlights the need for other housing types.

This analysis indicates that the smaller dwellings being developed are still unaffordable for low and very low income earners.



*Apartments developed under the NRAS Scheme in the City of Cockburn are affordable for low income households*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

**TABLE 11. MEDIAN DWELLING PRICE BY NO. OF BEDROOMS (2013)**

SUBURB	1-2 Bedrooms	3 Bedrooms	4 Bedrooms
Atwell	\$490,000	\$529,000	\$560,000
Aubin Grove	\$418,000	\$479,000	\$581,000
Coolbellup	\$430,000	-	\$510,000
Bibra Lake	\$385,000	\$483,000	\$580,000
Hamilton Hill	\$510,000	\$506,000	\$565,000
Munster	-	\$471,00	\$600,000
South Lake	\$415,000	\$411,000	\$459,000
Spearwood	\$402,000	\$494,000	\$590,000
Success	\$460,000	\$478,000	\$550,000
Yangebup	\$480,000	\$443,000	\$480,000
<b>AVERAGE</b>	<b>\$443,000</b>	<b>\$477,000</b>	<b>\$547,000</b>

*Source: REIWA Perth Suburbs Price Data*

**TABLE 12: AFFORDABLE PURCHASE BENCHMARKS BY WEEKLY INCOME**

	Weekly Income	Affordable Purchase Benchmark <i>22.2% of income</i>
Moderate	\$1169 - \$1760	\$273,820 – \$412,252
Low <sup>+</sup>	\$740 - \$1168	\$173,333 – \$273,585
Very Low	< \$739	\$173,099

*Source: Derived from Gurrán, N. (2008) Affordable Housing National Leading Practice Guide and Toolkit, Sydney: National Commitment 2 Working Group.*

An assessment of housing costs across the City of Cockburn in 2013 indicates that there were no properties available for sale that were affordable for very low income earners. There were only a limited number available for low-income earners, with the cheapest being around \$240,000 for one bedroom apartments in Spearwood, Hamilton Hill or Coolbellup.

For moderate income earners there are a few more options available, particularly at the higher end of this income bracket. This demonstrates that the City of Cockburn does still offer affordable options for moderate income earners.

The Department of Housing offer a range of options to assist low and moderate income earners buy their own home, including shared ownership options. These allow low and moderate income earners to purchase a home from the Department of Housing, with a SharedStart loan through Keystart, the Government's lending agent. Some advantages of Shared Home Ownership are a low deposit; no savings history required; and no lenders mortgage insurance. First home buyers can use the First Home Owners Grant towards the deposit and apply for a grant from the Home Buyers Assistance Account.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## COST OF LIVING

Affordable living is used to describe the factors that influence household expenditure in addition to rent or mortgage payments. In particular utility and transport costs are an important consideration when determining the total cost of living in a home. Factors such as a thermal efficiency and proximity to employment, public transport and essential services can affect household living expenditure and thus housing affordability. For example, a low rent dwelling with high living expenses may be less affordable than a dwelling with higher rent and low utility and transport costs.

The rising costs of living impact on all households to some extent, but it is low income households, particularly those reliant on income support payments, who are most affected by increases in the costs of essential goods and services. The reasons for this include the fact that their income rarely increases at the same rate as middle and higher income households; and a greater proportion of their income is spent on essential items (such as housing, utilities, transport and food) than for someone on a higher income.

In Western Australia the Cost of Living Report 2013 (WACOSS) found the following:

**Working couple family households** had income sufficient to maintain a basic standard of living, however with little capacity to save money or cope within unexpected costs.

**Single parent families** were in a worse position with their financial circumstances being highly insecure and lacking resilience, as they have little or no capacity to save and are at significant risk of going into unsustainable debt.

**Unemployed single persons** had inadequate income to maintain a basic standard of living, sufficiently low to present a barrier to their ability to find work, rather than an incentive.

Housing costs are the single biggest item of expenditure for most households, and the cost of housing is the biggest contributor to financial hardship in low income households.

Utility costs only represent a small fraction of the overall cost of living for most households — between 3.5 and 4.5 per cent<sup>31</sup>. The rate of utility hardship might be better thought of as a symptom of general levels of financial stress resulting from transferred housing stress. Utilities hardship may act as an early indicator of financial stress because utility bills are intermittent and unpredictable.

While the contribution of utilities to the overall cost of living is relatively small, it should be noted that it makes up a significantly larger proportion of expenditure for most low income households than it does for most of those on median and higher incomes. This means that increases in the cost of utilities impact disproportionately on disadvantaged households.

There is considerable variation in power usage across different kinds of low income households, depending on both household make-up and the quality and efficiency of their housing and appliances. Larger low income households, particularly those with children, consume more electricity, water and gas – especially when they are in old housing stock of poor quality and design.

The City has been proactive in promoting affordable living and has an extensive range of sustainability initiatives that seek to help households reduce living costs. This includes a number of guides for households, such as the following:

### *Affordable Living Initiatives*

- Building a sustainable house in the City of Cockburn
- Is solar energy right for you?
- Landowner Biodiversity Conservation Grant Program Brochure
- Sustainable Living Home Online
- Sustainable Renters' Guide
- Waterwise Brochures - Planting a local native garden;
- Protect and maintain our water supplies;
- Top 10 tips for saving water in the home and garden

The City also offers free home energy and water audits, which are in-home consultations offered to home owners and renters in Cockburn during autumn each year, helping them to reduce energy and water usage.

<sup>31</sup> WACOSS (2013) Western Australia the Cost of Living Report 2013

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## GROUPS VULNERABLE TO HOUSING STRESS

Across Australia the greatest numbers in housing stress are private renters, working households and households with children (both couple and sole parent households)<sup>32</sup>. The incidence of housing stress is highest for lower-income private renters; single-person households aged less than 65 years; and lower income home purchasers. Almost half of lower-income households in stress are working households, and over one-third of lower-income working households are in housing stress. For many (69 per cent) of these households, home ownership is not something they see as likely<sup>33</sup>.

The anticipated increase in the incidence of housing stress is particularly significant for lower-income households in the private rental market as a result of the decline in home ownership rates among younger households, and a reduction in the availability of public housing.

Young people can be vulnerable to housing stress as they transition from the family home to independent living. In general young people between aged between 15 and 24 years reside in their family home, or their housing is characterised by shared rental tenure. The high rate of rental tenure by this age group is linked to housing purchase affordability. Some of the barriers confronting young people entering the home ownership market are:

- Difficulty with obtaining appropriate deposits
- Lower levels of employment
- Lower incomes from entry level employment positions
- Higher levels of part-time work / lower levels of full-time work
- Student debts
- Decline in stock of affordable housing

People with disabilities are more susceptible to housing stress and financial hardship because they face extra costs relating to housing, transport, aids and equipment, in addition to income restriction.



*Low income households are most affected by rising costs of living*

Ongoing medical costs and housing modifications deplete financial reserves, causing uncertainty<sup>34</sup>.

Many individuals with disabilities rely on the disability support pension which remains significantly below the poverty line, and others find it challenging to get and keep rewarding and well-paid jobs. Family and carers work less hours than others, or do not work at all. Under these circumstances there are reduced opportunities to make investments and build wealth.

People with disabilities are more likely to be renting than owning their home and are often unable to obtain or afford homes in the areas that are close to work, transport, family, friends and activities. Entry into the private rental market poses challenges in securing appropriate accommodation and then in trying to gain permission to carry out modifications.

Lower income older Australians are also at risk of housing stress, particularly lone households, and evidence suggests that their housing needs will not be met by the social housing system alone<sup>35</sup>. Current trends indicate that Australia is on the threshold of a steady and sustained increase in the number of low-income, older renters. The greatest projected change is in the 85 and over age range where across Australia the number of low-income renters is estimated to increase by 194 per cent from 17,300 to 51,000<sup>36</sup>.

<sup>34</sup> AHURI (2007) *The housing careers of persons with a disability* National Research Venture 2: 21<sup>st</sup> century housing careers and Australia's housing future

<sup>35</sup> AHURI (2008) *Rental Housing for Lower Income Older Australians – Research & Policy Bulletin* Issue 96 January 2008

<sup>36</sup> AHURI (2008) Op.Cit

<sup>32</sup> AHURI (2012) *What are the benefits and risks of home ownership for low-moderate income households?*, Issue 149

<sup>33</sup> AHURI (2012) Op.Cit.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

### HOUSING AFFORDABILITY KEY FINDINGS:

- a) *Home ownership is more difficult than it has ever been for low and moderate income households, and entering home ownership means facing greater financial risks than households with equivalent incomes 30 years ago.*
- b) *An assessment of housing costs in the City of Cockburn indicates no properties available for sale that were affordable for very low income earners, and only a limited number available for low-income earners, with the cheapest being around \$240,000 for one bedroom apartments in Spearwood, Hamilton Hill or Coolbellup.*
- c) *For moderate income earners there are a few options available, particularly at the higher end of this income bracket, demonstrating that the City of Cockburn still offers affordable options for moderate income earners.*
- d) *The most affordable rental dwellings are smaller one and two bedroom flats in older style apartment buildings, or one and two bedroom older style duplex dwellings in Spearwood and Hamilton Hill, with rent around \$310-\$390 a week.*
- e) *Ancillary dwellings ('granny flats') were the most affordable dwellings, with rental prices around \$300 a week, found predominately in Spearwood and Hamilton Hill, but there was only a small number available.*
- f) *New one and two bedroom apartments found in Spearwood and Hamilton Hill resulting from increased codings from the Phoenix Revitalisation Strategy are generally not affordable for purchase by low income earners.*
- g) *For low income earners there were a small number of private rental properties available within the affordable rental benchmark range, being smaller one or two bedroom dwellings primarily in Hamilton Hill and Spearwood.*
- h) *For moderate income earners at the higher end of the income bracket there were a number of affordable rental properties available across the City, however at the lower end of this income bracket there were only a small number of properties in certain suburbs that were affordable at \$350 a week.*
- i) *Rental properties available under the National Rental Affordability Scheme ("NRAS") are within the affordable benchmark for low income households when they otherwise would not have been.*
- j) *Young people, low income families with children, people with disabilities and older renters are vulnerable to housing stress.*
- k) *Trends indicate that Australia is on the threshold of a steady and sustained increase in the number of low-income, older renters and evidence suggests that their housing needs will not be met by the social housing system alone.*
- l) *The rising costs of living costs impact on all household, but low income households, particularly those reliant on income support payments, are most affected by increases in the costs of essential goods and services.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 4.0 KEY FINDINGS AND IMPLICATIONS FOR HOUSING

The assessment of the City's housing stock and household and population forecasts has identified the following key issues that require addressing:

### HOUSING STOCK MISMATCH

The City's current housing stock does not match the projected smaller households, and will not provide an adequate range of housing choices for future households. A greater number of smaller dwellings will be required to meet the needs of smaller households.

It is acknowledged that over time there will be some adaptation to high housing costs and larger dwellings, and dwellings will be utilised differently. For example, there is likely to be more shared housing arrangements, and more than one household per dwelling. However, such arrangements will not suit all households, and given the significance of the projected mismatch, it is considered appropriate that an increase in the number of smaller dwellings types should be encouraged across the City. This can be achieved through increases to residential densities in appropriate locations, and ensuring adequate higher densities in new areas.

Facilitating the provision of smaller dwellings will assist in providing options for residents seeking to downsize or move to more appropriate housing while staying in the local area while they are still able to live independently. It is acknowledged that many older Australians living in large homes consider that the dwelling meets their needs<sup>37</sup>. However, studies indicate that a proportion, particularly those with disabilities, will want to downsize or move to a more appropriately designed dwelling<sup>38</sup>. Most of these people will want to remain in their local area, and this Strategy aims to facilitate the housing options for those who wish to downsize, or whose homes no longer suit their needs. An assessment of the housing stock in the City of Cockburn indicates that these options are limited in many areas, and this issue needs to be addressed.

<sup>37</sup> AHURI (2010) *How well do older Australians utilise their homes?* Issue 126 May 2010

<sup>38</sup> AHURI (2010) *Op. Cit*

### NEED FOR A COMPACT URBAN FORM

In recent years the City has moved towards more compact and sustainable urban development, reflected in the Cockburn Town Centre (a transit-oriented development), the planning for Cockburn Coast, urban revitalisation strategies (for Håmilton Hill and Spearwood), and various local structure plans that seek to achieve a range of residential densities.

However, like much of the Perth Metropolitan area, the City is still characterised by predominately low density residential codings that have resulted in a housing stock of large detached dwellings, and many dwellings that in general do not have high levels accessibility. Continuing to identify opportunities for higher densities, particularly in areas with high levels of accessibilities will provide opportunities for smaller dwellings and a greater number of dwellings with good access to services and public transport.

Living within easy access to public transport is particularly important for lower income households who may not have access to a car. Individuals on lower incomes indicate a stronger preference for easy access to public transport than other income groups. Providing dwellings with good access to services and public transport is particularly important for young people and people with disabilities, who are high users of public transport.

This City's program of urban revitalisation strategies provide the opportunity to achieve a more compact and sustainable urban form.

### DECLINING HOUSING AFFORDABILITY

The number of Australian households in housing stress has increased dramatically since 2003, and this is a trend that is likely to continue into the future. For 'very low', 'low' and 'moderate' income earners this means that owning a property is likely to be out of reach. Households susceptible to housing stress are low income renters, low income households with children, older people renting, and people with disabilities.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

Therefore it is considered that provision of a greater number of smaller dwelling types, including ancillary dwellings which have been found to be the most affordable rental accommodation, across the City will assist in providing more affordable housing.

## COST OF LIVING IMPACTS FOR LOW INCOME HOUSEHOLDS

While all households are impacted on by increasing costs of living, low income households are the most affected. In particular it is single parent families and lone person households that are most susceptible to living cost increases. Housing affordability is a particular issue for family households who have a variety of living expenses that make them more susceptible to financial hardship, in addition to having high housing costs through the requirement in many cases for larger dwellings that have higher rental costs.

Increasing the stock of smaller dwellings, and providing more dwellings within close proximity to public transport and services will assist greatly in providing housing options that have lower transport costs. Encouraging more sustainable dwellings, and providing information regarding sustainable living options will also assist households.

## NEED FOR ADAPTABLE HOUSING

In the City of Cockburn there is an ageing population, and 18 per cent of people have a disability. The number of private and public dwellings that have been built to incorporate universal design elements is very low, therefore for many of these people their home may not have a level of accessibility to suit their needs either now or in the future.

Inaccessible housing leads to social disadvantage and has negative effects for social integration and participation. Modifications to dwellings to improve accessibility, such as installation of ramps, are often expensive and unsatisfactory. These costs place increased financial pressure on such households, and moving house to find a better house design suited to

their specific needs is not a viable option due to the high 'sunk costs' in the current accommodation<sup>39</sup>.

A cost-benefit study comparing the types of care for ageing Australians found that providing home-based care is less costly on average than residential care. The annual average cost of residential care is reportedly \$48,710 per person whereas in-home formal care costs on average \$7,520 per annum. It has been estimated that if 20 per cent of new homes included universal housing design, the cost savings to the Australian health system would range from \$37 million to \$54.5 million per annum<sup>40</sup>.

A large proportion of the public housing stock is physically inappropriate for people with disabilities, and there is only an extremely small amount of public housing stock that has already modified<sup>41</sup>. People with disabilities have been found to spend longer periods on the public housing waiting list than people without a disability, due to requiring accessible housing<sup>42</sup>.

Livable Homes are easy to move around in and easy to use. They are open-plan and designed to maximise space in key areas of the home. Some of the design features include flat level thresholds, larger rooms and passages for wheelchair access, low kitchen benches etc. Livable Homes ensure people of all ages and abilities can live in or visit with comfort.

Improving the supply of affordable housing that is accessible and conforms to the principles of universal housing design would be a significant positive step towards reducing rates of poverty and providing increased opportunities for economic and social inclusion for people with disabilities.

<sup>39</sup> Tully, Beer (2009) The housing careers of people with a disability and carers of people with a disability AHURI Southern Research Centre.

<sup>40</sup> Australian Housing and Urban Research Institute (2010) Dwelling, Land and Neighbourhood Use by Older Home Owners, pp. 188-189.

<sup>41</sup> Physical Disability Australia Ltd (2011) A home of my own: The need for accessible public housing for people with physical disability in Australia.

<sup>42</sup> Physical Disability Australia Ltd (2011) Op. Cit.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## DEMAND FOR AGED CARE FACILITIES

The ageing population, particularly the increase in people over 70 years of age, will see an increased demand for aged care facilities for those whose care needs can no longer be met within their own homes.

The demand for low and high care facilities, in addition to respite care, will continue to increase across the Perth metropolitan area. In particular there will be a demand for affordable aged care.

Traditionally such facilities have been located on sites of 6-8 ha, however it is becoming increasingly difficult to find such sites, and a more flexible approach will be required. The lack of suitable locations raises concerns regarding the shortfall of accommodation in the near future. It will be crucial that the City of Cockburn actively target supported accommodation development in any future redevelopment in order to meet the identified needs.

## SHORTAGE OF CRISIS ACCOMMODATION

There is a trend of increased homelessness, particularly for vulnerable households such as people with disabilities. Compounding the problems associated with greater incidence of homelessness in the community has been the breakdown of the affordable housing system.

There has been increasing pressure on crisis accommodation. This is because in addition to more people seeking crisis accommodation, the average length of stay has increased due to longer waiting times for social housing<sup>43</sup>.

Addressing the lack of affordable housing will go some way to reducing vulnerability to homelessness for some households. However, provision of crisis accommodation is still important to ensure that there is adequate accommodation for people waiting for social housing.

<sup>43</sup> Department of Housing (2009) *More than a Roof and Four Walls Social Housing Taskforce Final Report*

## 5.0 STRATEGIES FOR ACTION

This section will discuss the various mechanisms that are available to address the key housing need issues that have been identified:

- *Housing stock mismatch*
- *Need for a compact urban form*
- *Declining affordability*
- *Demand for aged care facilities*
- *Cost of living impacts for low income households*
- *Shortage of crisis accommodation*
- *Need for adaptable housing*

## 5.1 PLANNING MECHANISMS

There are a number of different planning mechanisms potentially available to assist in the provision of affordable and diverse housing, and to address the key housing issues, in particular the following key issues:

- *Housing stock mismatch*
- *Need for a compact urban form*
- *Declining affordability*
- *Cost of living impacts*
- *Need for adaptable housing*

This section will provide an overview of the available mechanisms, and their appropriateness within the City of Cockburn to address the areas of concern. It should be noted that none of these mechanisms alone are considered sufficient to address the issue.

### BARRIER REDUCTION STRATEGIES

In some circumstances there may be elements of the planning framework which either individually or used in combination may discourage or constrain the provision of affordable housing in new developments.

Barriers can include:

- Restrictive planning controls which prevent diverse or low-cost housing forms (such as ancillary dwellings, grouped/multiple dwellings, boarding houses)
- Excessive building costs by mandating inclusions of expensive finishes/materials, design features.

- Restrictive covenants such as provisions limiting the use of property or prohibiting certain uses, such as boarding houses, or covenants that mandate high-cost finishes which add to the cost of development.

The City of Cockburn Town Planning Scheme No. 3 ("the Scheme") has flexibility to vary requirements in a variety of circumstances, including parking standards. Land use permissibilities under each zone are also considered to be flexible, providing Council with discretion to consider a wide range of uses in each zone.

Notably the Scheme exempts two grouped dwellings from planning approval, which eliminates application fees and reduces the time required for approvals where proposals comply with the R-Codes.

The following Local Planning Policies (adopted pursuant to the Scheme) have particular relevance to the provision of affordable and diverse housing:

- APD12 – Aged or Dependent Persons Dwellings
- APD56 -Single Bedroom Dwellings
- APD58 - Residential Design Guidelines
- APD67 – Lodging House – Design Guidelines

It is considered that in general their scope and content are appropriate in achieving diverse housing types, however it is considered there a number of areas where more could be done to encourage affordable and diverse housing types, and to reduce additional building

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

costs that arise from land use planning outcomes. These are discussed below:

## ENCOURAGING SPECIAL PURPOSE DWELLINGS

The R-Codes provides for three types of Special Purpose Dwellings:

- Ancillary Dwellings
- Single Bedroom Dwellings
- Aged and Dependent Dwellings

The objectives of these dwellings are to:

- *Ensure that residential development is provided to accommodate people with or without special needs.*
- *To provide ancillary accommodation which is independent or semi-independent to residents of the single house.*
- *To ensure that dwellings for the aged and people with special needs can be provided within residential areas.*
- *To provide opportunities for affordable housing.*

## ANCILLARY DWELLINGS

The R-Codes provides for the development of ancillary dwellings for people who live either independently or semi-independently of the residents of a single house, sharing some site facilities and services.

As for all development under the R-Codes, approval for an ancillary dwelling can be obtained in two ways:

1. Compliance with the 'deemed to comply' provisions; or
2. Demonstrated consistency with the 'design principles'.

The 'deemed to comply' requirements require:

- the lot is greater than 450m<sup>2</sup>;
- maximum plot ratio of 70m<sup>2</sup> (amongst other things).

In circumstances where the occupant of an ancillary dwelling has a disability, particularly a physical disability, a plot ratio of 70m<sup>2</sup> may not be adequate to meet their needs. Additional space requirements may arise to provide for:

- comfortable wheelchair access around the dwelling

- accommodating equipment, such as hoists and aids
- a larger bathroom for wheelchair accessibility and use of aids
- small additional separate bathroom facilities (shower, toilet, basin) for a carer to use
- 'breakout' space for a carer who needs to be present in the dwelling but at times needs their own space

Applicants have the option to meet the 'design principles', which do not include a maximum floor area. This provides the option to seek consideration under the 'design principles', providing justification for a larger ancillary dwelling.

Where development does not satisfy the 'deemed to comply' provisions, it must be demonstrated that the following 'design principle' is achieved:

*P1 Ancillary dwelling for people who live either independently or semi-dependently to the residents of the single house, sharing some site facilities and services and without compromising the amenity of surrounding properties.*

Such proposals are assessed on case by case basis. However, it is considered that providing further guidance, in the form of a local planning policy, would assist proponents in seeking consideration under the 'design principles'. Such a policy would also assist Council in determining the appropriateness of proposals.

The policy should stipulate the considerations and criteria for considering ancillary dwellings larger than 70m<sup>2</sup>, and specify the information that is required to be submitted to Council.

In this regard consideration should be given to allowing increases to the plot ratio of an ancillary dwelling (generally up to around 100m<sup>2</sup>) where:

- The ancillary dwelling is built to the 'deemed-to-comply' standards set out in the R-Codes for 'Aged and Dependent' dwellings (ie. universally accessible).

Requiring any ancillary dwelling that is greater than 70m<sup>2</sup> to be universally accessible will ensure that it is

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

ideally suited to the needs of any occupants with a disability. Importantly it will contribute to the stock of universally accessible dwellings which have been identified as being in very short supply. Such dwellings have the potential to provide affordable accessible dwellings to meet the needs of people with (or without) disabilities.

It is considered very important that ancillary dwellings remain 'ancillary' to the main dwelling, and that they remain as smaller dwellings for the following key reasons:

- So that they remain affordable - Ancillary dwellings have been found to provide the most affordable rental accommodation in the City of Cockburn, and this is largely due to their modest scale. In line with the objectives of this Strategy it is considered imperative that ancillary dwellings continue to remain affordable.
- To ensure they contribute to the stock of smaller dwellings – Given that the housing needs analysis demonstrates the growing number of one and two person households, and the predominance of dwellings with three or more bedrooms it is important to strongly encourage smaller dwellings.
- To ensure they generally accommodate only one or two people – this will ensure that the vehicle movement and activity associated with the dwelling does not compromise the amenity of the surrounding properties.

To summarise, the local planning policy should seek to:

- Ensure that ancillary dwellings are able to meet the needs of people with disabilities, whilst still being ancillary to the main dwelling.
- Strongly encourage the development of universally accessible ancillary dwellings.
- Ensure ancillary dwellings do not compromise the amenity of the surrounding properties and the streetscape.
- Provide further guidance on the 'design principles' set out in the R-Codes for ancillary dwellings, particularly in relation to plot ratio.

The local planning policy will explore these matters in further detail, and will be subject to community consultation, and discussion with Disability Services Commission.

### AGED AND DEPENDENT DWELLINGS

The R-Codes provides for the development of aged and dependent dwellings which are built specifically for a person who is either over 55 years of age or has a disability.

The R-Codes set out the design requirements for these dwellings (ie. minimum doorway and corridor widths; level entry ways; bathroom requirements; accessible paths from car parking area and the street etc.).

For aged and dependent dwellings (and single bedroom dwellings) the R-Codes allow a variation to the minimum and average lot sizes for subdivision and development, reducing the required site area by one third. This means that there is the potential for more dwellings to be developed than would ordinarily be permitted, proving an incentive for universally accessible dwellings to be built.

The 'deemed to comply' requirements of the R-codes state that 'Aged and Dependent Persons Dwellings' should be a minimum of five dwellings within any single development. However the R-Codes identify the option for local governments to adopt a local planning policy that varies this requirement to allow the incentive to be taken up for smaller developments.

It is therefore proposed that the City's Local Planning Policy APD12 'Aged and Dependent Persons Dwelling' be amended to include removal of the minimum number of aged and dependent dwellings required in any single development.

The 'deemed to comply' requirements of the R-codes require that aged and dependent dwellings are occupied by at least one person who is aged and/or dependent. They require that a memorial be placed on the title of these dwellings (Section 70A notification), stipulating this requirement, which then applies in perpetuity to all subsequent occupiers.

It is considered that this requirement for a memorial to be placed on the title is a significant disincentive for

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

developers to build aged and dependent dwellings, as it potentially reduces the value of the dwelling. Even people who are aged and/or dependent may be reluctant to build or purchase such dwellings because they are aware that the restriction on occupancy may be a problem for resale or future rental potential. Consequently, with the exception of aged care providers, few aged and dependent dwellings are being built in the City of Cockburn.

It is considered that if the memorial restricting the occupancy of aged and dependent dwelling was not required this would make this incentive significantly more attractive, and would assist in increasing the stock of universally accessible dwellings. With this modification it is accepted that the dwelling could be used by anyone, however importantly the stock of universally accessible dwellings is being increased.

The memorial is not a requirement of the 'design principles'. It is therefore proposed that Local Planning Policy APD12 'Aged and Dependent Persons Dwelling' be amended to include further guidance on the application of the 'design principles' that does not require a memorial to be included on the title. It is considered important that if this were to be accepted that the dwelling size is restricted to a maximum of 100m<sup>2</sup> to ensure the dwelling is contributing both to the stock of smaller, affordable dwellings, whilst also contributing to the stock of universally accessible dwellings.

For many landowners these changes will provide another option for the addition of a special purpose dwelling by potentially allowing an aged and dependent dwelling to be built behind their house (because of the allowance under the R-Codes to vary the site area), when they otherwise may not have a lot that is large enough to build a second dwelling.

It is considered that removing the minimum number of dwellings for aged and dependent dwellings in conjunction with the removal of the requirement for a memorial on the title in the R-Codes would:

- See a significant increase in the uptake of the incentive for aged and dependent dwellings;
- Provide another housing option for many families (that can also be subdivided and placed on a

separate title, which is not possible with ancillary dwellings);

- Increase the stock of universally accessible dwellings;
- Increase the stock of smaller, affordable dwellings.

### SINGLE BEDROOM DWELLINGS

The R-Codes provide for development of single bedroom dwellings to provide alternative and affordable housing options for singles or couples.

The 'deemed to comply' requirement limits the floor area of a single bedroom dwelling to 70m<sup>2</sup>. This is considered important, however the restriction on number of rooms capable of use as a bedroom is considered restrictive in today's housing market.

Given that the 'design principles' provides for housing suitable for one or two persons, the limitation of only one room capable for use as a bedroom is considered to prejudice the use of the dwelling for two people other than a couple. There may be many instances where a parent and child, two siblings, two friends/flatmates or other non-couples wish to reside together in a small dwelling without being restricted to one bedroom. It is envisaged that in the majority of instances however, the second room will typically be used as an ancillary or utility type space such as a study, a spare room, an activity room or a guest bedroom. This extra space is consistent with modern expectations and standards in contemporary housing and provides for greater flexibility generally regarding occupancy and use.

This flexibility is unlikely to cause any impact on the amenity of an area or adjoining neighbours; it simply provides a more flexible floor plan that will suit a greater range of smaller household, and it will make the incentive more attractive.

Such proposed changes to the policy were considered by Council in 2011 (Ordinary Meeting 12 May 2011). This included insertion of a new clause providing acceptance of an additional multi-purpose room capable of use as a second bedroom if required where the dwelling complied with the maximum floor area set out in the R-Codes, and where it provided limited accommodation suitable for one or two persons.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

At that time the R-Codes specified a maximum floor area of 60m<sup>2</sup> for single bedroom dwellings, and Council considered that this floor area was not sufficient to adequately accommodate a second bedroom.

The R-Codes have now increased the allowable floor area to 70m<sup>2</sup>, and it is considered that this additional 10m<sup>2</sup> provides adequate space to potentially accommodate a second additional room. It is therefore appropriate to consider modifications to APD56 'Single Bedroom Dwellings' to allow consideration of an additional room capable of use as a bedroom provided that the floor area does not exceed 70m<sup>2</sup>.

### MODIFICATION TO RESIDENTIAL CODING APPLICABLE IN COMMERCIAL ZONES

Currently under clause 5.8.3 of the Scheme a residential coding of R60 is applicable to dwellings developed in the commercial zones where residential uses are permitted and another coding has not been specifically applied. This applies to the 'Local Centre', 'Mixed Business' and 'District Centre' zone. Consideration should be given to whether a higher coding may be appropriate in all or some locations, to serve as an incentive for mixed use development, and provide the opportunity for greater dwelling numbers in areas close to services.

### REFERENCING AFFORDABLE HOUSING IN THE SCHEME

Currently the Scheme does not specifically refer to affordable housing; rather it only references the concept of housing diversity.

The next review of the Scheme should include consideration of affordable housing as a key objective and consideration.

The Scheme objectives for the commercial zones do not currently include any references to mixed use and housing diversity, and consideration should also be given to including references to dwellings in these objectives to ensure mixed use development is encouraged where appropriate.

### MINIMISING ADDITIONAL BUILDING COSTS

In certain circumstances additional building costs arise from requirements identified at the local structure planning stage of development. This includes building within proximity to bushfire prone areas where additional building costs are incurred by future purchasers of land. In many circumstances the need to build to a higher Building Attack Level (BAL) pursuant to Australian Standard AS3959 (Building in Bushfire Prone Areas) can be 'designed out' at the structure planning stage, through the provision of adequate separation distances. The same principle applies to requirements for 'quiet-house-design' – often the need for these higher building standards could be avoided through better design.

Council should ensure that proposed structure plans and subdivisions are designed to minimise the imposition of additional dwelling construction costs by minimising the requirement for dwellings built to a BAL; and 'quiet-house-design' wherever possible.

### EXEMPTING ANCILLARY DWELLINGS FROM PLANNING APPROVAL

Ancillary dwellings that comply with the R-Codes could be made exempt from development approval under the Scheme. This would eliminate the planning application fee and make approval times quicker, serving to encourage development of ancillary dwellings which have been found to provide the most affordable rental accommodation in the City.

### POWER TO EXTINGUISH COVENANTS

Currently clause 5.5 of the Scheme allows Council to extinguish covenants that restrict the number of residential units that can be developed, inconsistent with the R-Codes. This power could be further broadened to give Council the power to extinguish covenants that mandate minimum dwellings sizes, which have been known to be imposed in certain circumstances by developers, forcing purchasers to build homes larger than they may require.

### PROTECTIVE AFFORDABLE HOUSING POLICIES

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

Protective measures seek to keep affordable, low-cost housing in an area, or seek to mitigate loss of such housing. Policies can include:

- Requiring Social Impact Assessments to identify any loss of low-cost housing;
- Impact Mitigation.

The City does not have any identified low-cost housing that would be appropriate to identify and either protect or mitigate its potential loss.

## MANDATORY INCLUSIONARY ZONING/DEVELOPER CONTRIBUTIONS

Mandatory inclusionary zoning is the requirement for developers to contribute to affordable housing as a condition of development or subdivision approval. This may be a monetary contribution, provision of affordable housing itself, or land for the development of affordable housing.

Mandatory inclusionary zoning has not been used in Western Australia, and is not supported by the Department of Housing's Affordable Housing Strategy: 'Opening Doors' (2010). Developer contributions for affordable housing fall outside the scope of State Planning Policy 3.6 Development Contributions for Infrastructure. It is therefore considered that without direction from the State Government through the development of a State Planning Policy, or modification to SPP 3.6, it is not possible for the City to consider mandatory inclusionary zoning under the current framework.

Given the extent of the housing affordability problem, it is considered warranted that mandatory inclusionary zoning be investigated, and that the City of Cockburn lobby the state government to consider such mechanisms through a comprehensive review of the planning system in relation to affordable housing.

## PLANNING INCENTIVES

Planning incentives can be used to encourage developers to provide affordable or more diverse housing. They can directly and indirectly reduce costs and increase profits for developers, and also lead to favourable outcomes for housing. Planning incentives

can include graduated planning standards, bonus systems and planning process incentives.

## GRADUATED PLANNING STANDARDS

Relaxed controls on development performance criteria, such as open space requirements, car parking requirements (such as less parking for dwellings in centres with services and high quality transport) and use of plot ratio scales for development.

Currently the Scheme has flexibility to vary car parking standards, and the introduction of formal graduated planning standards is not considered necessary. If affordable housing is included as an objective in the Scheme this will give it greater weight in the exercise of discretion.

## BONUS SYSTEMS

Bonus systems enable a potential for greater development densities. Bonus systems operate through negotiation and assessment. Examples of incentives for a specific development include: reduced setback and density restrictions; reduced private and public open space requirements; reduced car parking requirements (justified in terms of future need).

The City of Cockburn has introduced planning incentives for affordable housing in the Cockburn Coast area.

Within Cockburn Coast the City of Cockburn may grant a floorspace bonus in the event that a development application includes the provision of affordable housing at the following ratio:

1. *Affordable yield 10% = 30% floorspace bonus*
2. *Affordable yield 20% = 40% floorspace bonus*
3. *Affordable yield 25% = 45% floorspace bonus*

It is considered that introduction of incentives in other areas of the City should be investigated.

The Cockburn Coast Affordable Housing Strategy has introduced floorspace bonuses for provision of affordable housing, and it is considered that further investigation should be made into broadening their application to other areas of the City.

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## VOLUNTARY NEGOTIATED AGREEMENTS

Voluntary negotiated agreements for affordable housing are made between a developer and local planning authority on a case by case basis.

Even though they are not required for planning approval, a developer may seek to negotiate affordable housing goals for a concession or subsidy. They work like incentives, but on a case by case basis. In particular they can be successful in large developments which generally involve much negotiation which can provide the opportunity to fit an affordable housing contribution into the process.

Incentives can include the following:

- Reduction of other infrastructure payments.
- Concessions in other development application fees.
- Pre-purchase commitments (for example, the local government can purchase housing to be managed by a social housing provider).
- Assistance with financing costs.
- Promotional or marketing assistance.

It is considered that further investigation should be made into broadening their application to other areas of the City.

## FACILITATING HOUSING DIVERSITY

The planning framework in Western Australia has focused on encouraging housing diversity as an indirect means of achieving housing affordability. In Western Australia, and in the City of Cockburn this has seen an increase in housing diversity, reflected in the change in the housing stock.

However, there is still a current mismatch between the housing stock and households, and projections indicate

that the trend for smaller households will increase at a greater rate than construction of smaller dwellings.

Increases to residential codings can facilitate smaller dwellings, and this has been seen in the areas of Spearwood and Hamilton Hill as a result of residential coding changes through the Phoenix Central Revitalisation Strategy. The continuation of urban revitalisation strategies across the City of Cockburn that examine the potential for residential zoning changes will provide an opportunity to encourage small dwelling types, particularly in areas with high levels of accessibility.

It is considered that more could be done to encourage diverse housing types in the City, not just smaller housing types. For example, provision of dwellings in mixed use developments can contribute to housing diversity by providing smaller dwelling units in areas that have good levels of accessibility by reason of their proximity to centres.

The City has seen more of these occurring in recent years, however there is an opportunity to encourage more such forms of housing.

## CHANGES TO THE PLANNING SYSTEM

The planning framework in Western Australia has focused on encouraging housing diversity as an indirect means of achieving housing affordability. Given the extent of the issue, and the growing trend of declining housing affordability, it is considered that the planning system will need to do more than just promote housing diversity. As noted above, to address this issue it is recommended that Council continue to lobby the State Government to undertake a comprehensive review of the planning system.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

### PLANNING FRAMEWORK ACTIONS:

- a) *Encourage other housing types, including dwellings in mixed-use environments, such as 'shop-top' housing to increase the number and diversity of smaller dwellings in the City, particularly in areas with good accessibility to services and public transport.*
- b) *Investigate opportunities to encourage development of dwellings in mixed use development, including:*
  - *adopting guidelines for 'Mixed Use' development to provide guidance to developers and Council in achieving appropriate mixed uses. This may take the form of a Local Planning Policy and/or guidance notes or 'best practice' notes.*
  - *reviewing the objectives of the commercial zones in the Town Planning Scheme to reference provision of dwellings to encourage mixed use development where appropriate.*
- c) *Encourage development of ancillary dwellings by making them exempt from planning approval.*
- d) *Investigate the potential use of planning incentives to encourage affordable and diverse housing in targeted areas in the City of Cockburn, similar to that introduced for the Cockburn Coast area.*
- e) *Ensure Urban Revitalisation Strategies identify measures to address the findings of this Strategy.*
- f) *Ensure wherever possible Structure Plans do not seek to transfer higher building costs on to landowners. This is primarily to endeavour that structure planning better responds to the inherent site characteristics of a land parcel, such as to avoid development on land which is subject to noise or bushfire risk and which requires a more expensive dwelling to be built.*
- g) *Ensure all Local Structure Plans respond specifically to the outcomes of this Strategy, and address the future housing needs of the community.*
- h) *Undertake a review of clause 5.8.3 of the Scheme (residential codings for commercial zones) to consider whether a higher residential coding may be appropriate in the commercial zones, in all or some targeted areas.*
- i) *Modify Local Planning Policy APD12 'Aged and Dependent Persons' Dwellings' to allow less than five dwellings to be developed in any single development.*
- j) *Modify Local Planning Policy APD12 'Aged and Dependent Persons Dwellings' to allow flexibility for Section 70A Notifications not to be included on the certificate of title that require at least one occupant to be a disabled or physically dependent person or aged person (where it does not exceed a plot ratio of 100m<sup>2</sup>).*
- k) *Modify Local Planning Policy APD56 'Single Bedroom Dwellings' to allow a second multi-purpose room that could be used as a bedroom where the plot ratio of the dwelling does not exceed 70m<sup>2</sup>.*
- l) *Prepare a new Local Planning Policy for Ancillary Dwellings that sets out criteria for consideration under the 'Design Principles' of the Residential Design Codes (including plot ratio greater than 70m<sup>2</sup>) to ensure ancillary dwellings are able to meet the needs of people with disabilities.*
- m) *Continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning.*
- n) *Lobby the state government to empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity, for example requiring two storey development and mandating minimum floor areas.*

## 5.2 PARTNERSHIPS

An effective affordable housing strategy needs to foster partnerships and create resourcing mechanisms that will enable and facilitate private and community provision<sup>44</sup>.

Partnerships in respect to affordable housing can involve different combinations of local community associations, private institutions, social service and housing support agencies as well as housing providers. These partnerships can remain as informal cooperative working arrangements, or be developed more formally through memoranda of understanding and may evolve into contractual relationships to deliver locally defined affordable housing outcomes.

In a partnership for affordable housing, local government generally uses its leadership and coordination to assist community partners to deliver support services, and to develop, construct, and manage affordable housing.

### STRATEGIES FOR COUNCIL OWNED LAND

The City of Cockburn owns various land assets within the district in freehold, some of which has the potential to be sold to enable the land asset to be realised. This has been a long standing practice of the City, whereby it seeks to utilise its land assets in an optimal way to realise the best outcome for the City. Approaching and planning this in a strategic way is a core purpose of the Land Management Strategy.

The Land Management Strategy also seeks to set out where land should be held by the City, or where consideration may be made to acquire land considered to be of strategic significance. An important aspect of the Land Management Strategy is that it establishes the strategic principles to underpin decisions in relation to the disposal, holding or acquisition of land.

The aim of the Land Management Strategy is to establish an effective framework to manage the City's land portfolio, in such a way as to maximise financial returns and support the financial sustainability of the City. This in turn supports the City undertaking further

strategic capital investment, as well as expanding the range and types of services and facilities it is expected to deliver to the community.

The key objectives include the following:

1. To facilitate the effective management of the City's land portfolio;
2. To identify City owned land that has the potential to be sold in the short to medium term (rolling five year timeframe), in conjunction with the demand for such funds;
3. To identify development constraints and methodologies in order to establish land disposal priorities;
4. To identify City owned land that has value of a 'strategic' nature, to ensure development proposals optimise long-term financial benefits for the City.

The Land Management Strategy identifies in detail:

- Land Asset Disposal Principles;
- Land Asset Purchase Principles;
- Joint Venture Considerations.

These form the basis to decisions being made in respect of the City's land portfolio.

The next review of the Land Management Strategy will continue to examine the City's freehold land assets, but with a heightened view to maximising the provision of new land for residential development within established suburbs that have been the subject of a revitalisation strategy. This will help to focus Council resources on unlocking the urban potential of Council owned land in such areas, so as to be able to provide new opportunities for residential development in addition to the redevelopment that private landowners will undertake as a result of higher residential densities which eventuate from revitalisation strategy processes.

<sup>44</sup> Shelter WA (2012) Local Government Guide to Developing an Affordable Housing Strategy

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## PARTNERSHIP OPPORTUNITIES

The City has been involved in a number of partnerships to deliver affordable and diverse housing/land, as outlined below:

### *MS Society Care Facility*

The City purchased a former reserve on Redmond Road, Hamilton Hill and leased the site to the MS Society for the purposes of a care facility for people living with MS and other neurological conditions.



Figure 14. MS Society Care Facility and respite home, Hamilton Hill

### *MS Society Respite Care Centre*

Adjacent to the MS Society care facility Council leased Reserve 27691 Redmond Road, Hamilton Hill to the MS Society of WA (Inc) for the development of a respite home. This Reserve had no identified function for current or future public open space and was surplus to the recreational needs of the surrounding area, and therefore its purpose was changed from 'Park and Recreation' to 'Respite Care'. Council entered an agreement with the MS Society of WA (Inc) to lease Reserve 27691 Redmond Road, Hamilton Hill for a period of 21 years for a peppercorn rent annually and with the payment of all outgoings and charges being the responsibility of the MS Society.

The MS Society have constructed a building on the site at their cost, suitable for a Respite Care Centre. The respite centre is a home away from home for people living with MS, and other neurological conditions. Staff

at these facilities provide 24 hour care, and are fully trained to care for people with neurological conditions, to provide a break for families and carers. Eligible Cockburn residents are given priority access to the respite facility where possible

### *Former Korilla School Site – Affordable Aged Care Facility*

The City is currently progressing a plan for affordable aged care at the former Korilla School site in Coolbellup. The City is seeking to purchase the site and lease it to Bethanie for the purposes of affordable aged care, both low and high care. Wherever possible Coolbellup residents will be given priority to access the facility.

These partnerships are examples where Council has been able to purchase reserve land no longer required for its original purpose, and enter into a partnership to deliver much needed housing facilities that are lacking in the City of Cockburn.

### *Subdivision of Council Land for Affordable Lots*

Council subdivided and created 29 lots in Grandpre Crescent, Hamilton Hill. These lots were all priced between \$150,000 and \$260,000, resulting in total house and land prices under \$400,000 which is affordable for moderate income earners. Many of the lots were purchased by first homebuyers.

### *Future Opportunities*

Council should continue to explore the possibility of such projects and partnerships.

The Phoenix Central Revitalisation Strategy identify the possibility of incorporating an aged care facility on the Council's administration site as part of any redevelopment of the site. Should a Structure Plan/Master Plan be proposed for the Council's administration site this should include investigation into the feasibility of an aged care facility on the site.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

### **PARTNERSHIP ACTIONS:**

- a) *Continue to examine the City's freehold land assets with the view to maximising the provision of new land for residential development within established suburbs that have been the subject of revitalisation strategy.*
- b) *Work with the private sector to identify landholdings across the City which would be prime opportunities for affordable housing projects, and advocate for these landholdings to pursue affordable housing through partnerships and design based approaches.*
- c) *Ensure the feasibility of aged care accommodation is investigated as part of any Master Plan/Structure Plan for the Council's administration building site identified in the Phoenix Central Revitalisation Strategy.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 5.3 ADVOCACY AND COMMUNICATION

### PROMOTING THE BENEFITS OF HOUSING DIVERSITY AND COMPACT URBAN FORM

The stigmatisation of affordable housing (particularly 'social housing') is a policy problem in its own right. In Australia this stigmatisation can be traced to under-investment in social housing, which has contributed to poor maintenance, and the allocation of housing to the most disadvantaged and marginalised tenants<sup>45</sup>.

Stigmatisation of affordable and social housing has negative impacts on wellbeing outcomes for residents and on neighbourhood amenity. It can cause anxiety in the wider community and hamper efforts to improve social diversity, and therefore needs to be addressed.

It is noted that negative depictions of social housing are over represented in the media. While influencing media portrayal will not have a direct impact on the underlying problems associated with inequality, it can challenge negative stereotypes that fuel discrimination and prejudice. Some of the most successful innovations to tackle stigma have sought to influence the media reporting of social housing, in particular encouraging media outlets to develop an understanding of the issues that confront organisations managing social housing<sup>46</sup>.

Strategies that are specifically tailored to the perceptions of small groups of influential business representatives (real estate professionals, local businesses, property developers), welfare professionals (teachers, medical staff) and local residents (prospective first time buyers, parents with school age children) can have a positive impact.

While housing affordability has been declining over the past 20 years, the problem has significantly worsened in more recent years, and this impacts the whole community. The issue of the mismatch between the housing stock and household types is also a growing trend that will become more pronounced into the future; however it is not necessarily an issue that the wider community are aware of. For many residents the extent and nature of the problem is not likely to be well understood. This means there is an opportunity to raise greater awareness of local housing issues, and to promote the benefits of affordable and diverse housing types in the City of Cockburn.

In order for this Strategy to be successful it needs to build a cohesive and integrated community that understands the role of diversity in creating successful places.



Figure 15. Illustration used in the Hamilton Hill Revitalisation Strategy to demonstrate how a proposed coding of R60 would look in the local area.

<sup>45</sup> AHURI Research and Policy Bulletin(2012) 'Addressing the stigmatisation of social housing', Issue 151

<sup>46</sup> AHURI Research and Policy Bulletin (2012) Op. cit.

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

Addressing the mismatch of housing stock and household projections, and facilitating more affordable and diverse housing in the City of Cockburn will include consideration of increases in residential densities. There are often negative perceptions of medium and high density development. Traditionally Perth suburban areas have been characterised by low-density residential development, and the numerous poor examples of medium and high density residential development from the legacy of older state housing flats through Perth's suburbs have contributed to negative perceptions of higher density development.

It is considered there is an opportunity to improve public perception of medium/high density living.

Building medium to high density housing, including affordable housing, in Australia's low density suburbs can lead to a high degree of anxiety for local homeowners who perceive it as devaluing the amenity of their area and the value of their home<sup>47</sup>. The key concerns generally expressed by the community in relation to high/medium density housing are:

- Increased traffic
- Increased on-street parking
- Loss of neighbourhood character
- Loss of privacy
- Loss of property value
- Loss of trees/open space

The opportunity exists to clearly communicate to the community the findings of this Strategy, and to explain the concepts of affordable and diverse housing, and to address the above concerns. It is also important for Council to clearly communicate its objectives in relation to affordable and diverse housing and transit-orientated design to the community.

The City should develop an information and community education program to foster a better understanding of the role of social and affordable housing in creating successful, dynamic, healthy, sustainable and diverse places.

This could include the development of an interactive diagram setting out the principles of affordable living

could assist with communicating these concepts. Studies have shown that when shown an illustration explaining the concept of transit orientated development the majority of people say they would consider living in such a precinct<sup>48</sup>.

It is considered that development of an 'Affordable Living' portal on the City's website could provide a mechanism to integrate all of the City's relevant initiatives, where customers can be directed to different tools and sources of information.

### PROMOTING THE BENEFITS OF ADAPTABLE HOUSING

The benefits of adaptable housing design are wide ranging, and there is an opportunity to increase awareness of the benefits arising from Universal Housing Design for both the residential building and property industry, for existing home owners and new home buyers.

The City should develop an awareness campaign to promote the Livable Housing Design Guidelines, and to inform, educate and enthuse home buyers about the benefit of selecting a universally designed home.

Houses designed with these features benefit a wide range of households, including:

- families with young children who need to get prams into their homes, and want safer homes;
- people who sustain temporary or permanent injuries which limit their mobility who would potentially require less time in hospital if they could safely move around their home while recovering;
- ageing baby boomers who are looking to renovate their existing homes to better accommodate their future needs;
- older people who are particularly vulnerable to slip, trip and fall injuries in their homes;

<sup>47</sup> AHURI (2013) Evidence Review 021 Neighbours oppose higher density and affordable housing, Wed 12 Jun 2013

<sup>48</sup> Housing and Urban Research Institute WA and Curtin University Planning and Transport Research Centre (2007) *Housing in Railway Station Precincts: Some Empirical Evidence of Consumer Demand for Transit Oriented Housing in Perth Western Australia*

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

- people with disability and their families who are looking for a home that will accommodate their current and future needs;
- people with disability who wish to visit the homes of friends and relatives; and
- home care workers and family and friends who provide in-home care and support.

The new 'Affordable Living' portal should include links to the Livable Homes resource website, which was developed for people designing new homes or renovating existing homes. It is currently available online with other resources which are free to use and includes guidelines, information and checklists, 'how to' guides and case studies.



### PROMOTING SMALLER HOUSING OPTIONS

While the concept of a 'granny flat' (ancillary dwelling) is well-known, the R-Codes provide for other 'Special Purpose Dwellings' that fewer people are familiar with:

- Single bedroom dwellings
- Aged and dependent dwellings

The R-Codes provide an incentive for the development of single bedroom dwellings and aged and dependent dwellings. This means that

For many landowners this will provide another option for the addition of a special purpose dwelling by potentially allowing an aged and dependent dwelling to be built behind their house (because of the allowance under the R-Codes to vary the site area), when they otherwise may not have a lot that is large enough to build a second dwelling.

It is proposed that the City's Local Planning Policy APD12 'Aged and Dependent Persons Dwelling' be amended to include removal of the minimum number of aged and dependent dwellings required in any single development.. This will make this option potentially

available to a much larger number of landowners, and should be promoted.

A new local planning policy for ancillary dwellings is proposed that provides further guidance on varying the 70m<sup>2</sup> maximum plot ratio where it can be demonstrated that the additional space is required for a person with a disability, and/or it is built to the standards for aged and dependent dwellings set out in the R-Codes (ie. with universal access). This will provide further guidance for people considering building a special purpose dwelling.

Navigating information regarding the requirements for ancillary dwellings can be difficult, and it is considered that preparation of a 'Housing Options' information kit would assist people in this regard. This could provide information on the potential options available to landowners looking to build an additional dwelling on their property, and explain the differences between the options.

This information could be provided to Disability Services Commission Local Area Coordinator(s) to distribute to families looking at their housing options.

### PROMOTING AFFORDABLE AND DIVERSE HOUSING TO DEVELOPERS

The City's Affordable Housing Online Toolkit contains an extensive range of information for developers. The Online Toolkit could be further promoted to encourage private developers to investigate affordable dwellings, including NRAS dwellings which have been found to contribute to affordable rental accommodation in the City of Cockburn.

### PROMOTING AFFORDABLE LIVING

The City of Cockburn currently has an extensive range of sustainability initiatives that address affordable living, as discussed in the previous section. This includes a number of information brochures that assist with reducing and minimising living costs.

It is considered that these initiatives could be expanded to include information on transport costs, comparing the total costs of different modes of transport, and vehicle types, to assist households make decisions about

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

vehicle selection, mode of transport, and location of dwellings.

The cost of owning and running a vehicle are substantial, and contribute to the cost of living. A fuel-efficient car can save \$1,000 a year in fuel costs and reduce greenhouse gas emissions by 1.5 tonnes a year<sup>49</sup>. That is around half the cost and emissions than those of a less efficient car. The Green Vehicle Guide Fuel Costs and CO2 Emissions Calculator allows users to calculate the annual fuel cost and CO2 contribution for different vehicle makes and models.

There are also numerous government websites that provide useful information to reduce living costs, including utility costs and transport costs, as follows:

- [www.energymadeeasy.gov.au](http://www.energymadeeasy.gov.au)
- [www.livinggreener.gov.au/](http://www.livinggreener.gov.au/)
- [www.energyrating.gov.au/](http://www.energyrating.gov.au/)
- [www.yourhome.gov.au](http://www.yourhome.gov.au)
- [www.greenvehicleguide.gov.au](http://www.greenvehicleguide.gov.au)

Finding and navigating all of this information can be difficult, and it is recommended that these links or summarised information be included on the new 'Affordable Living' Portal.

### 5.4 DEMAND FOR AGED CARE FACILITIES

Traditionally aged care facilities have been located on sites of 6-8 ha, however it is becoming increasingly difficult to find such sites, and a more flexible approach is required.

The City should continue to identify suitable opportunities to develop supported accommodation as an integral component of all future redevelopments. shortfall of accommodation in the near future.

It will be crucial that the City of Cockburn actively target supported accommodation development in any future redevelopment in order to meet the identified needs.

### 5.5 ADDRESSING THE LACK OF CRISIS ACCOMMODATION

Homelessness is a growing problem in Western Australia, and there is an identified shortfall of crisis housing in the City of Cockburn and surrounding area.

Homelessness in young people is a complex problem that can benefit from early intervention strategies that target young people and/or their families before the young person has left home or within one month of the young person leaving home.

Council should continue to identify opportunities for crisis accommodation in the City of Cockburn as a priority.

<sup>49</sup> Living Greener <http://www.livinggreener.gov.au/>, accessed 17 December 2013

### ACTIONS - ADVOCACY AND COMMUNICATION:

- a) *Provide information to the community on the issue of housing affordability and diversity, and promote its benefits.*
- b) *Investigate innovative tools to convey housing and neighbourhood design issues in the City of Cockburn, and to explain the way these issues are being addressed, including:*
  - *Integration of the City's existing sustainability initiatives with affordable housing information to create an 'Affordable Living' portal on the City's website that also provides links to useful information and tools.*
  - *The development of an interactive diagram setting out the principles of affordable living (housing diversity, walkable neighbourhoods, compact urban form etc.) to assist with communicating these concepts visually.*
  - *Preparation of Affordable Living Fact Sheets to help communicate to the community what Council is seeking to achieve with its initiatives.*
- c) *Continue to explore new opportunities for sustainability initiatives that assist with reducing the cost of living for households, including affordable transport.*
- d) *Identify measures to improve public perceptions of higher density development, including the opportunities for positive media portrayal at a local level.*
- e) *Promote the Adaptable Housing (Universal Housing Design Principle) and the Livable Homes Design Guidelines.*
- f) *Prepare a 'Housing Options' information kit setting out information and options for special purpose dwellings.*
- g) *Lobby the state government to:*
  - *Undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing;*
  - *Empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity.*

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 5.6 ACTION PLAN

PLANNING MECHANISMS	ACTION	Responsibility	Timeframe
	a) Encourage other housing types, including dwellings in mixed-use environments, such as 'shop-top' housing to increase the number and diversity of smaller dwellings in the City, particularly in areas with good accessibility to services and public transport.	Strategic/Statutory Planning	Ongoing
	b) Investigate opportunities to encourage dwellings in mixed use developments, including: <ul style="list-style-type: none"> <li>adopting guidelines for 'Mixed Use' development to provide guidance to developers and Council in achieving appropriate mixed uses. This may take the form of a Local Planning Policy and/or guidance notes or 'best practice' notes.</li> <li>reviewing the objectives of the commercial zones in the Town Planning Scheme to reference provision of dwellings to encourage mixed use development where appropriate.</li> </ul>	Strategic Planning/Statutory Planning	Ongoing
	c) Encourage development of ancillary dwellings by making them exempt from planning approval.	Strategic Planning/Statutory Planning	Short term
	d) Investigate the potential use of planning incentives to encourage affordable and diverse housing in targeted areas in the City of Cockburn, similar to that introduced for the Cockburn Coast area.	Strategic Planning	Short term
	e) Ensure Urban Revitalisation Strategies identify measures to address the findings of this Strategy.	Strategic Planning	Ongoing
	f) Ensure wherever possible Structure Plans do not seek to transfer higher building costs on to landowners. This is primarily to endeavour that structure planning better responds to the inherent site characteristics of a land parcel, such as to avoid development on land which is subject to noise or bushfire risk and which requires a more expensive dwelling to be built.	Strategic Planning	Ongoing
	g) Ensure all Local Structure Plans respond specifically to the outcomes of this Strategy, and address the future housing needs of the community.	Strategic Planning	Ongoing
	h) Undertake a review of clause 5.8.3 of the Scheme (residential codings for commercial zones) to consider whether a higher residential coding may be appropriate in the commercial zones, in all or some targeted areas.	Strategic Planning	Short term

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

i)	Modify Local Planning Policy APD12 'Aged and Dependent Persons' Dwellings' to allow less than five dwellings to be developed in any single development.	Strategic Planning/Statutory Planning	Short term
j)	Modify Local Planning Policy APD12 'Aged and Dependent Persons Dwellings' to allow flexibility for Section 70A Notifications not to be included on the certificate of title that require at least one occupant to be a disabled or physically dependent person or aged person (where it does not exceed a plot ratio of 100m <sup>2</sup> ).	Strategic Planning/Statutory Planning	Short term
k)	Modify Local Planning Policy APD56 'Single Bedroom Dwellings' to allow a second multi-purpose room that could be used as a bedroom where the plot ratio of the dwelling does not exceed 70m <sup>2</sup> .	Strategic Planning/Statutory Planning	Short term
l)	Prepare a new Local Planning Policy for Ancillary Dwellings that sets out criteria for consideration under the 'Design Principles' of the Residential Design Codes (including plot ratio greater than 70m <sup>2</sup> ) to ensure ancillary dwellings are able to meet the needs of people with disabilities.	Strategic Planning/Statutory Planning	Short-term
m)	Continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning.	Strategic Planning	Ongoing
n)	Lobby the state government to remove the requirement in the Residential Design Codes for Section 70A Notifications to be included on the certificate of title requiring at least one occupant to be a disabled or physically dependent person or aged person.	Strategic Planning	Ongoing
o)	Lobby the state government to empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity, for example requiring two storey development and mandating minimum floor areas.	Strategic Planning	Ongoing

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

PARTNERSHIPS	ACTION	Responsibility	Timeframe
	a) Continue to examine the City's freehold land assets with the view to maximising the provision of new land for residential development within established suburbs that have been the subject of revitalisation strategy.	Strategic Planning	Ongoing
	b) Work with the private sector to identify landholdings across the City which would be prime opportunities for affordable housing projects, and advocate for these landholdings to pursue affordable housing through partnerships and design based approaches.	Strategic Planning	Ongoing
	c) Ensure the feasibility of aged care accommodation is investigated as part of any Master Plan/Structure Plan for the Council's administration building site identified in the Phoenix Central Revitalisation Strategy.	Strategic Planning	Short-medium term
	d) Continue to identify opportunities for crisis accommodation in the City of Cockburn.	Strategic Planning	Ongoing

LEADERSHIP, ADVOCACY AND COMMUNICATION	ACTION	Responsibility	Timeframe
	a) Provide information to the community on the issue of housing affordability and diversity, and promote its benefits.	Strategic Planning	Short term
	b) Investigate innovative tools to convey housing and neighbourhood design issues in the City of Cockburn, and to explain the way these issues are being addressed, including: <ul style="list-style-type: none"> <li>• Integration of the City's existing sustainability initiatives with affordable housing information to create an 'Affordable Living' portal on the City's website that also provides links to useful information and tools.</li> <li>• The development of an interactive diagram setting out the principles of affordable living (housing diversity, walkable neighbourhoods, compact urban form etc.) to assist with communicating these concepts visually.</li> <li>• Preparation of Affordable Living Fact Sheets to help communicate to the community what Council is seeking to achieve with its initiatives.</li> </ul>	Strategic Planning	Short term

## HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

	<p>c) Continue to explore new opportunities for sustainability initiatives that assist with reducing the cost of living for households, including affordable transport.</p>	<p>Strategic Planning and Environment Services</p>	<p>Ongoing</p>
	<p>d) Identify measures to improve public perceptions of higher density development, including the opportunities for positive media portrayal at a local level.</p>	<p>Strategic Planning</p>	<p>Ongoing</p>
	<p>e) Promote the Adaptable Housing (Universal Housing Design Principle) and the Livable Homes Design Guidelines.</p>	<p>Strategic Planning</p>	<p>Short term</p>
	<p>f) Prepare a 'Housing Options' information kit setting out information and options for special purpose dwellings.</p>	<p>Strategic Planning</p>	<p>Short term</p>
	<p>g) Lobby the state government to:</p> <ul style="list-style-type: none"> <li>• Undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing;</li> <li>• Empower all local governments to be able to extinguish restrictive covenants that actively work to reduce housing affordability and diversity.</li> </ul>	<p>Strategic Planning</p>	<p>Ongoing</p>

# HOUSING AFFORDABILITY AND DIVERSITY STRATEGY

## 5.7 MEASURES OF SUCCESS

The progress of each of the actions in this Strategy will be reported on annually via the following channels:

- Annual reports to Directors and Elected Members
- Annual update of this Strategy
- Annual review of relevant Local Planning Policies

To measure the success of the Strategy in achieving more diverse and affordable housing it is proposed that a number of performance indicators be set out.

To summarise, the key actions of the Strategy focus on encouraging:

- Multiple dwellings which are in general smaller and more affordable than detached dwellings.
- Dwellings in mixed use development scenarios to increase the stock of smaller, affordable dwellings in accessible locations.
- Ancillary dwellings that have been found to be the most affordable rental accommodation in the City.
- Smaller dwellings including single bedroom dwellings that are in general more affordable dwellings for rent or purchase, and better match the needs of future households.
- Adaptable dwellings (including aged and dependent dwellings) to provide improved housing options for people with disabilities.

Therefore it is considered appropriate that the following key performance indicators be used to measure the success of the actions set out in this Strategy:

### KEY PERFORMANCE INDICATORS

- 1) *Increase in the percentage of multiple dwellings approved*
- 2) *Increase in the percentage of aged and dependent dwellings approved*
- 3) *Increase in the percentage of single bedroom dwellings approved*
- 4) *Increase in the percentage of ancillary dwellings approved*
- 5) *Increase in the percentage of adaptable dwellings approved*

## APPENDIX A:

### RESIDENTS WITH A DISABILITY IN THE CITY OF COCKBURN & PERTH METROPOLITAN AREA (2003)

TABLE 13: RESIDENTS WITH A DISABILITY IN THE CITY OF COCKBURN & PERTH METROPOLITAN AREA (2003)		
By severity of disability	City of Cockburn	Perth Metropolitan Area
<i>Profound disability</i>	3401	4.78
<i>Moderate Disability</i>	2652	3.72
<i>Mild Disability</i>	3788	5.32
<b>By age group</b>		
<i>0-14 Years</i>	1325	1.8
<i>15-24 Years</i>	942 <sup>i</sup>	1.32
<i>25-64 Years</i>	7818	10.9
<i>65+ Years</i>	3335	4.6
Requiring personal care assistance: all ages	6186	8.6
<b>Total persons with a disability</b>	<b>13 420</b>	<b>18.8</b>
<i>Source: DSC website March 2012 citing ABS, 2003 data</i>		

## APPENDIX B: HOMELESSNESS IN THE PERTH METROPOLITAN AREA

TABLE 14: HOMELESSNESS STATISTICS PERTH METROPOLITAN AREA (ABS 2011)

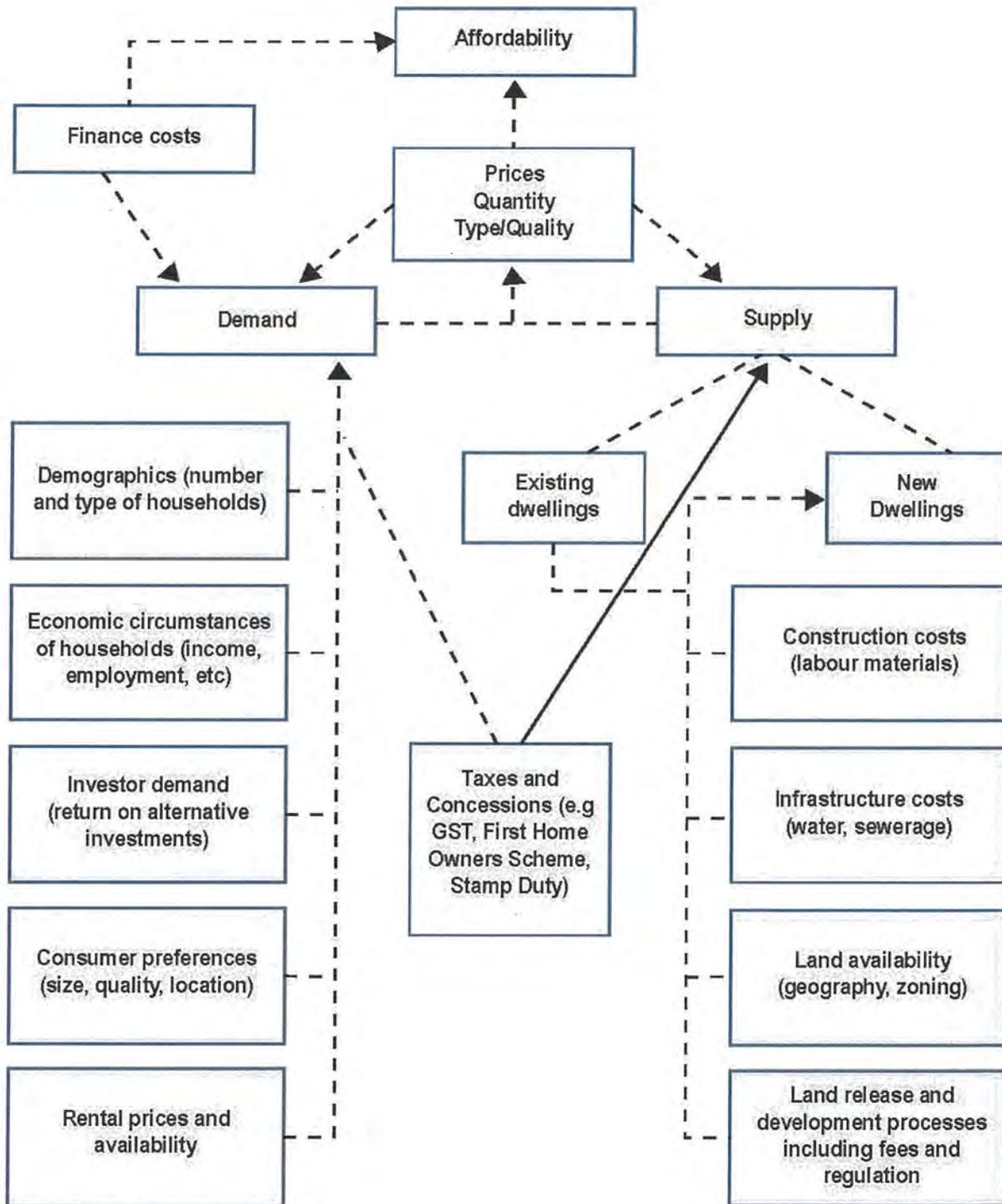
Area within Greater Perth	HOMELESS OPERATIONAL GROUPS							PERSONS IN MARGINAL ACCOMMODATION		
	Staying in improvised dwellings, tents or sleeping out	Persons in supported accommodation for the homeless	Persons staying temporarily with other households	Persons staying in boarding houses	Persons in other temporary lodging	Persons living in 'severely' crowded dwellings	All homeless persons	Persons living in other crowded dwellings	Persons in other improvised dwellings	Persons who are marginally housed in caravan parks
Perth - Inner	189	166	113	419	19	80	986	206	0	0
Perth - North East	17		177	23		248	515	312	26	73
Perth - North West	19	75	329	24	11	421	879	877	0	99
Perth - South East	19	286	338	121	14	508	1,286	986	53	88
Perth - South West	93		342	261		178	1,035	505	32	100

## APPENDIX C:

### HOMELESSNESS STATISTICS PERTH METROPOLITAN AREA (ABS 2011)

TABLE 15: HOMELESSNESS STATISTICS PERTH METROPOLITAN AREA (ABS 2011)										
Area within Greater Perth	HOMELESS OPERATIONAL GROUPS							PERSONS IN MARGINAL ACCOMMODATION		
	Staying in improvised dwellings, tents or sleeping out	Persons in supported accommodation for the homeless	Persons staying temporarily with other households	Persons staying in boarding houses	Persons in other temporary lodging	Persons living in 'severely' crowded dwellings	All homeless persons	Persons living in other crowded dwellings	Persons in other improvised dwellings	Persons who are marginally housed in caravan parks
Perth - Inner	189	166	113	419	19	80	986	206	0	0
Perth - North East	17		177	23		248	515	312	26	73
Perth - North West	19	75	329	24	11	421	879	877	0	99
<b>Perth - South East</b>	<b>19</b>	<b>286</b>	<b>338</b>	<b>121</b>	<b>14</b>	<b>508</b>	<b>1,286</b>	<b>986</b>	<b>53</b>	<b>88</b>
Perth - South West	93		342	261		178	1,035	505	32	100

## APPENDIX D: AFFORDABILITY



Source: National Housing Supply Council (2010) 2<sup>nd</sup> State of Supply Report

## APPENDIX E: NRAS HOUSEHOLD INCOME ELIGIBILITY LIMITS

The 2013-14 household income eligibility limits are:

2013-14 Year	\$
1st Adult	45,956
Each Additional Adult	17,579
First Sole Parent	48,336
Each Child	15,243

2013-14 income eligibility limits for various household compositions:

Household composition	Initial household income limit (\$)	Existing tenant income limit (\$)*
One adult	45,956	57,445
Two adults	63,535	79,419
Three adults	81,114	101,393
Four adults	98,693	123,366
Sole parent with one child	63,579	79,474
Sole parent with two children	78,822	98,528
Sole parent with three children	94,065	117,581
Couple with one child	78,778	98,473
Couple with two children	94,021	117,526
Couple with three children	109,264	136,580

\*If the household income of an existing tenant exceeds the indicated limit (25 per cent greater than the initial income limit) in two consecutive NRAS years, the tenant will cease to be an eligible tenant. This column indicates figures which are 25 per cent higher than the household income limits for ease of reference for this purpose.

## APPENDIX F: NRAS INCENTIVES DATA

Incentive Status by State/Territory						
State	Incentives Allocated	National per cent	Incentives Reserved	National per cent	Total Incentives	National per cent
WA	1,426	9.8	4,044	16.9	5,470	14.2
<b>National Total</b>	<b>14,575</b>	<b>100.0</b>	<b>23,884</b>	<b>100.0</b>	<b>38,459</b>	<b>100</b>

Type of Homes by State/Territory											
State	Apartment		House		Studio		Townhouse		Subsidiary Dwelling		Grand Total
	Allocated	Reserved	Allocated	Reserved	Allocated	Reserved	Allocated	Reserved	Allocated	Reserved	
WA	403	704	539	567	381	626	103	2,147	-	-	5,470
<b>Total</b>	<b>5,325</b>	<b>10,077</b>	<b>4,017</b>	<b>3,973</b>	<b>2,407</b>	<b>4,125</b>	<b>2,821</b>	<b>5,709</b>	<b>5</b>	<b>-</b>	<b>38,459</b>

Size of Homes by State/Territory							
State	Studio	1 Bedroom	2 Bedrooms	3 Bedrooms	4 Bedrooms	5 or more Bedrooms	Total Allocated Incentives
ACT	1,469	383	328	330	9	31	2,550
NSW	1,179	1,666	2,861	641	160	5	6,512
NT	89	475	312	184	-	-	1,060
QLD	326	1,732	2,646	4,487	1,705	-	10,896
SA	150	508	1,161	1,668	247	7	3,741
TAS	770	73	375	232	13	-	1,463
VIC	1,546	2,150	2,195	762	109	5	6,767
WA	1,008	1,533	1,985	759	183	2	5,470
<b>Total</b>	<b>6,537</b>	<b>8,520</b>	<b>11,863</b>	<b>9,063</b>	<b>2,426</b>	<b>50</b>	<b>38,459</b>

**APPENDIX G:  
NRAS PARTICIPANTS IN WESTERN AUSTRALIA (2013)**

NRAS Participants in Western Australia	Active Dwellings	Proposed Dwellings	Total Incentives
WA	1,426	4,044	5,470
Access Housing Australia Ltd	28	278	306
Affordable Management Corporation Pty Ltd ATF Affordable Housing Management Fund	19	-	19
Amana Living Incorporated	22	-	22
Campus Living Villages At ECU Pty Limited	70	72	142
Community Housing Ltd	8	165	173
Ethan Affordable Housing Ltd	3	40	43
Florin Pty Ltd	39	-	39
Foundation Housing Ltd	36	47	83
Goldmaster Enterprises Pty Ltd	-	100	100
Heyspring Land Pty Ltd	-	30	30
Midwest NRAS Group Pty Ltd	-	48	48
National Housing Group Pty Limited	11	34	45
Quantum Housing Group Pty Ltd as Trustee for The Quantum Affordable Housing Unit	414	239	653
Questus Funds Management Limited as Responsible Entity for the Questus Residential	171	2,063	2,234
Realty Capital Pty Ltd	-	47	47
UWA Accommodation Services Pty Ltd	523	477	1,000
Yaran Residential Investments Pty Ltd as Trustee for the Yaran Residential Inves	82	404	486
<b>National Total Dwellings</b>	<b>14,575</b>	<b>23,884</b>	<b>38,459</b>

**SCHEDULE OF SUBMISSIONS**  
**DRAFT HOUSING AFFORDABILITY AND DIVERSITY STRATEGY**

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
1	City of Cockburn landowner	<p>Support</p> <p>I am in favour of greater housing diversity and density in my suburb, Bibra Lake. I encourage the Council to consider rezoning Bibra Lake to R30 or higher to allow for smaller lot sizes and a larger range of housing types. This will help to make Bibra Lake more affordable and diverse by providing a better range of housing options.</p>	<p>Noted. Bibra Lake is identified as Stage 1 (2014/2015) in the urban revitalisation strategy staging plan that has been adopted by Council.</p>
2	City of Cockburn landowner	<p>Support</p> <p>I support greater housing diversity and density particularly in my suburb, Bibra Lake. Currently the majority of dwellings are large separate houses on large (600m2+) blocks. I support and encourage the rezoning of Bibra Lake to R30 or higher to allow for higher density, smaller lot sizes and a larger range of housing types.</p>	<p>Noted. Bibra Lake is identified as Stage 1 (2014/2015) in the urban revitalisation strategy staging plan that has been adopted by Council.</p>
3	<p>Disability Services Commission PO Box 441 West Perth WA 6872</p>	<p>Support</p> <p>Thank you for your letter of 1 July 2014 advising the Disability Services Commission (the Commission) of the development of a draft housing strategy for affordability and diversity. While the Commission has noted the draft strategy, it will not make a formal submission, but would like to make the general comments below.</p> <p>The Commission welcomes the draft strategy and notes with interest in particular, the sections that cover the housing needs for people with disability. The proposal to increase the range of housing options through ancillary housing, by removing the need for planning approval has merit, resulting in affordable and timely construction of ancillary homes.</p> <p>Ancillary homes can very adequately meet the needs of some people with disability and can be the preferred option for the individual and their family. The 2013 changes which now allow for a larger dwelling in the ancillary category benefit people with disability. Often the need is for a building that is</p>	<p>Noted. Further discussions have also been undertaken with Disability Services Commission to examine the issue of special purpose dwellings for people with disabilities, and additional information and actions have been recommended and discussed in the report.</p>

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<p>large enough to meet the accessibility needs and storage for the individual's required equipment and aides.</p> <p>The Commission also welcomes the draft strategy's comments on the value of the Count Me In and Liveable Homes initiatives, and would encourage wide adoption of these principles in all accommodation to allow for greater access for people with disability.</p> <p>Thank you for the opportunity to comment of the draft strategy. The Commission values partnerships with local governments and other agencies to ensure the needs and wishes of people with disability are met.</p>	
4	Chantal Roberts, Shelter WA 1st Floor/33 Moore Street East Perth WA 6004	<p>About Shelter WA:</p> <p>Shelter WA is the peak body for affordable housing in Western Australia, funded to provide policy advice, stakeholder engagement and influencing services.</p> <p>It was founded in 1979 as an independent community based peak body committed to accessible, affordable and secure housing for Western Australians. Shelter WA provides a link between government and the community through consultation, research, systemic advocacy, and policy advice and development. Our role is to provide an independent voice on housing rights and options in the state along the housing continuum, based on stakeholder consultation and research of housing market and homelessness trends. Our work focuses on promoting the development of appropriate affordable housing options for low to moderate income earners, those who are otherwise disadvantaged in the housing market and people experiencing homelessness.</p> <p>Shelter WA promotes a diverse range of affordable housing options including social housing, housing within the private rental market and affordable home ownership. Shelter WA also provides community education, and advice to government on housing policy issues. Our development of policy recommendations is based on sound research and consultation with housing consumers and organisations working on housing and related issues.</p> <p>Shelter WA's Response:</p> <p>Shelter WA welcomes the opportunity to provide this response to the City of Cockburn Draft Housing Affordability and Diversity Strategy. Shelter WA</p>	<ol style="list-style-type: none"> <li>1. Noted.</li> <li>2. Noted.</li> <li>3. These opportunities will be identified through the Land Management Strategy, rather than being specific in the Strategy itself. The inclusion of performance indicators is agreed, and have now been included in the draft at Attachment 1.</li> <li>4. Noted. The Strategy is proposed to be reviewed annually.</li> <li>5. Noted.</li> </ol>

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<p>recognises that land use planning and decision making plays a central role in the supply and availability of affordable housing. Shelter WA believes that clear planning mechanisms are needed to ensure increased housing supply is affordable and accessible to low and moderate income households. Without specific provisions to ensure additional dwellings are targeted to these households, there is a risk of developing housing in new and existing areas without increasing the social and economic mix needed for the health and vitality of Western Australian communities.</p> <p>Local governments are in a strong position to influence the provision of affordable housing, as they have the ability to integrate strategic policy objectives with urban planning responsibilities that lead to tangible outcomes. The Local Government Act, 1995, suggests that each local government has a responsibility to plan for the current and future needs of its community and Shelter WA considers this to include the need to plan for affordable housing (State of Western Australia, 1995). Shelter WA commends the City of Cockburn on the key objectives of its Housing Affordability and Diversity Strategy. These are:</p> <ul style="list-style-type: none"> <li>a) To provide households with access to housing that is appropriate to their needs in terms of size, physical attributes and location;</li> <li>b) To provide housing that is affordable to households of varying financial capacity;</li> <li>c) To provide a variety of housing types in locations that have good accessibility to public transport and essential services;</li> <li>d) To promote affordable living, taking into consideration the total cost of living in a dwelling, including energy and water consumption, the price of transport to access employment and essential services, and other daily needs impacted by location.</li> </ul> <p>1. The City of Cockburn, through the Draft Housing Affordability and Diversity Strategy, has acknowledged its responsibility to effectively plan for the future housing needs of its community. It is also evident that the City of Cockburn is committed to the present and future housing challenges within the region and in Western Australia. This can be seen through proposed amendments to their Town Planning Scheme, which will now include affordable housing as an objective and consideration. It can also be seen through the commitment to lobby State Government to extinguish restrictive covenants that work to reduce affordability and diversity, for example requiring particular built form and mandating</p>	

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<p>minimum floor areas. It is also evident through the City of Cockburn's commitment to lobby for a State Government review of planning mechanisms which can deliver more affordable housing. This includes a review of mandatory inclusionary zoning, which has the potential to deliver a significant amount of affordable housing across Western Australia.</p> <p>2. Shelter WA supports this mechanism in our Submission to Department of Planning: Planning Provisions for Affordable Housing. Shelter WA is encouraged by other elements of the Draft Housing Affordability and Diversity Strategy. These are:</p> <ul style="list-style-type: none"> <li>• An acknowledgement that a more diverse range of housing types must be delivered at significantly lower costs to meet the needs of low to moderate income earners;</li> <li>• A comprehensive demographical analysis, including the assessment of the prevalence of housing stress within the City of Cockburn;</li> <li>• A broad strategy to promote the creation of residential dwellings within mixed-use commercial zones, including the assessment of whether higher density should be encouraged;</li> <li>• A provision which encourages development of ancillary accommodation through making it exempt from planning approval;</li> <li>• Specified developer incentives, including graduated planning standards, which have already been utilised in the Cockburn Coast Affordable Housing Strategy;</li> <li>• A wide-ranging communications strategy to inform the community of the need for housing affordability, and to improve negative perceptions of diverse and higher density developments;</li> <li>• The acknowledgement that Structure Plans need to better respond to site characteristics. The Strategy explains that residential development will be avoided on land which is subject to noise and/or bushfire risk. Buildings located on these areas are required to conform to increased design requirements, which often reduces affordability;</li> <li>• That the City of Cockburn have utilised the Shelter WA Local Government Guide to Developing an Affordable Housing Strategy. This document was created to assist local governments' formulation of housing strategies, especially to determine appropriate ways to facilitate affordable housing in a defined local</li> </ul>	

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<p>housing market.</p> <p>3. Shelter WA considers it is imperative that all levels of government, and broader society, recognise that affordable housing and homelessness are intangibly connected. The continued increase in homelessness within Western Australia is greatly impacted by the current shortage of affordable housing. As a part of the Draft Housing Affordability and Diversity Strategy, the City of Cockburn has identified a shortage of crisis accommodation in its locality. This is considered especially important for youth and those with a disability in the area (City of Cockburn, 2014). The Strategy recommends that the council should continue to identify opportunities for crisis accommodation in the City of Cockburn as a priority (City of Cockburn, 2014, p. 55). The City of Cockburn also states that one of its strategies for council owned land is to maximise the provision of new land for residential development within established suburbs, through a review of its Land Management Strategy (City of Cockburn, 2014, p. 50). Shelter WA believes that these two objectives could be managed simultaneously. Shelter WA recommends the Draft Housing Affordability and Diversity Strategy detail a commitment by the City of Cockburn to partner with affordable housing providers, including those providing crisis accommodation. This could assist the City of Cockburn in realising the potential of undeveloped and underutilised land, and also provide added social benefits for the community. The City of Cockburn could pursue this commitment in a number of ways. These include:</p> <ul style="list-style-type: none"> <li>• Crown land transfers for social housing development;</li> <li>• Partnering in joint venture housing initiatives and management for long term affordability requirements;</li> <li>• Transferring existing management opportunities of existing LGA affordable housing dwellings to social housing providers, enabling asset leveraging opportunities for social housing providers;</li> <li>• Tendering the sale of local government land with affordable housing outcomes forming part of the tender evaluation process. Shelter WA believes there are also other opportunities for the City of Cockburn to support and deliver affordable housing through the Draft Housing Affordability and Diversity Strategy. Shelter WA would recommend that the City of Cockburn should explore:</li> <li>• A best use analysis of whether to sell, transfer or lease Council-owned land to support affordable housing developments;</li> </ul>	

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<ul style="list-style-type: none"> <li>• Innovative approaches to deliver affordable housing, including community land trusts and housing co-operatives, which are promoted in the State Planning Strategy (Department of Planning, 2014) (Department of Planning, 2014);</li> <li>• More detailed identification of preferred locations for affordable housing within the local government area through the use of its Land Management Strategy;</li> <li>• Provision of an affordable housing percentage target, based on a demographical needs analysis;</li> <li>• Liaison with the Department of Housing to ensure public housing stock is adequately provided for the future residents of the City of Cockburn;</li> <li>• Development of performance indicators to assess whether objectives contained in this document are being achieved, with reporting responsibility assigned to local government employees;</li> <li>• The use of working groups to further the issues covered in this document, and to ensure housing affordability and affordable housing is kept on the local agenda;</li> <li>• The opportunity to organise, attend and support events for local government housing and community organisations and support services.</li> </ul> <p>4. Conclusion: Shelter WA research suggests that strategies, such as City of Cockburn's Draft Housing Affordability and Diversity Strategy, can be effective in bringing about positive housing outcomes at a local government level. Shelter WA proposes these types of strategies are utilised appropriately and reviewed frequently in order to be effective, and would encourage the City of Cockburn to continue to refer to and update this strategy on an ongoing basis.</p> <p>5. Shelter WA commends the City of Cockburn for encouraging, facilitating and emphasising affordable housing in its Draft Housing Affordability and Diversity Strategy. The strategy provides a clear message to developers, the general public, internally to its council and staff, and to other government bodies, that the need for housing, including affordable housing, is necessary to ensure the health and vitality of future communities. The access to safe, secure, and affordable housing should be a right of every community member. Shelter WA believes this strategy will provide the guidance necessary to ensure diverse and affordable housing is a key feature in the future planning of the City of Cockburn.</p>	

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
		<p>Shelter WA looks forward to the City of Cockburn working with key stakeholders, including Shelter WA, to ensure the strategy is implemented effectively. Shelter WA would be happy to provide support, wherever possible, to ensure this and other projects within the City of Cockburn bring about an increase in the supply of affordable housing and also lead to a reduction in the homeless population in Western Australia.</p>	
5	<p>Housing Industry Association Ltd 22 Parkland Road Osborne Park WA 6017</p>	<p>This document provides HIA's comments and response to the "City of Cockburn Draft Housing Affordability and Diversity Strategy".</p> <p>Housing affordability and diversity is a very complex issue. There are many factors that affect the delivery of housing and the cost at which it can be built. Whilst all levels of government should be looking at the roles they need to be playing to address the shortage of housing stock in most Australian cities, it is a balance of encouraging and enabling policies together with market forces that are required to ensure that different types of housing stock can be developed to meet the needs of a growing population and changing demographic.</p> <ol style="list-style-type: none"> <li>Whilst it is encouraging that the City of Cockburn is investigating matters of housing affordability and diversity of housing stock, the mandatory interventions discussed in the draft paper are not supported by HIA. These measures, including mandatory affordability quotas and the installation of mandatory adaptable housing features, in most circumstances can be counter-productive to achieving improved affordability.</li> </ol> <p>Mandatory affordability quotas can lead to higher prices for the sections/units within the development which are not included or sold as "affordable housing". Houses sold at the "affordable housing" rate can be on-sold at market rates. The lack of appropriate management arrangements for these types of housing solutions mean that ultimately they do not, in HIA's opinion, work to solve housing affordability issues in a meaningful way.</p> <p>Similarly, mandatory adaptable housing features can be costly to implement where they are not needed by all occupants, and as a minimum measure may not meet the actual requirements of those residents who may require tailored solutions to meet their individual</p>	<ol style="list-style-type: none"> <li>This is incorrect. There are no proposed 'mandatory affordability' interventions proposed. There are no proposed mandatory adaptable housing features, rather the Strategy is seeking to 'promote' the Liveable Design Guidelines' which are an excellent resource for anyone considering building a dwelling that is universally accessible. Dwellings that are universally accessible are suitable for a wide range of households, including people with children, people with injuries etc.</li> <li>The City is aware that there are a number of other issues that affect housing affordability, as demonstrated in Appendix D of the Strategy. The Strategy focuses on matters that are within the control of the local government.</li> <li>It is not considered that the Strategy demonstrates a lack of understanding of this issue. The Strategy has sought to identify the housing needs of the community and to assist in the delivery of dwellings to better meet these needs. The Strategy is specifically seeking to address the needs of the community by including further flexibility to the requirements</li> </ol>

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		<p>housing needs. In this way they can be counter-productive to achieving better housing diversity as adaptations are still required.</p> <p>2. There are a number of taxation, planning, building and land supply issues that can affect housing affordability. It is these additional costs that form an enormous part of the final cost of housing and therefore affect affordability. Yet in reality, they are mostly outside the jurisdiction of local governments.</p> <p>3. Also affecting affordability is the economic reality that housing form must take into consideration the needs and preferences of the market. Housing must also be built for a cost that the market will bear for that particular product. Builders do not build product that consumers and investors will not or cannot purchase. This is often forgotten in affordability arguments.</p> <p>4. However, the City of Cockburn is clearly looking to address the issues it can change to make inroads into its growing housing affordability issue and an anticipated mismatch between residents needs and housing type into the future. This is a positive step in itself and HIA supports many of the actions proposed in the draft paper – particularly in relation to planning reform measures. Council's key assumption is that the population of City of Cockburn will not be housing a mobile population - that those who live there will want to continue to do so. This appears to be the underlying assumption for the study.</p> <p>In HIA's view, Council's best course of action is to provide an enabling planning and taxation environment around housing to ensure the market is well placed to deliver a wide range of housing that is demand driven by those who chose to reside in the City of Cockburn, both now and into the future. HIA has provided some comment on the broader factors that affect affordability and also provided further comment on the City of Cockburn's findings and proposed actions.</p> <p><b>5. Factors Affecting the Affordability of Housing</b></p> <p>There are a broad range of matters that contribute to the unaffordability of housing including government policies and the taxation treatment of housing (state stamp duties and other taxes - including development contributions)</p>	<p>for ancillary dwellings, single bedroom dwellings and aged and dependent dwellings to help people obtain dwellings to suit their needs and budgets.</p> <p>4. The Strategy is not based on the assumption that the population will be immobile. The City's forecast.id system identifies a variety of migration assumptions, and net migration by age is an important way of understanding housing markets. Migration patterns differ for each area across City of Cockburn depending on the housing markets and stage in the suburb life cycle. These have been factored into all of the population and household projections included in the Strategy. It is based on these projections that the Strategy is seeking to ensure there are adequate housing options for future households.</p> <p>5. These issues have been acknowledged in Appendix D, however they have not been discussed and addressed in detail because they fall outside of the control of the City of Cockburn.</p> <p>6. Matters relating to stamp duty, GST and land tax fall outside the jurisdiction of local government.</p> <p>The City does not support the exemption of Council rates during the construction or development phase, as this would result in increases to</p>

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		<p>council rates and lengthy planning delays which contribute to large land holding costs. Many of these are not addressed in the paper (apart from planning mechanisms) possibly as they are in the jurisdiction of other areas of government.</p> <p>Some of these factors that will affect housing affordability are summarised below:</p> <p><b>6. Taxation and Housing Affordability</b></p> <p>All tiers of government rely heavily on taxes and charges on residential properties for their revenue which has both a direct and indirect impact on housing affordability, and hence places pressure on low to moderate income households to have access to more affordable housing. The types of taxes which affect housing include:</p> <ul style="list-style-type: none"> <li>o GST</li> <li>o Stamp duty</li> <li>o Land tax</li> <li>o Council rates</li> <li>o Development contributions</li> </ul> <ul style="list-style-type: none"> <li>• <b>GST</b> - In terms of GST alone, the residential building sector accounts for 13 per cent of all GST revenue raised by the Commonwealth Government.<sup>1</sup> Whilst not able to be controlled or altered by the State Government, it still needs to be a factor in the consideration of governments as a contributor towards the unaffordability of housing.</li> <li>• <b>Stamp Duty</b> - Levied when a property is sold, stamp duty can be paid multiple times on a new property. The current process for levying stamp duty can involve a 'triple taxation' meaning is a 'tax on a tax on a tax'. This is obviously reflected in new house prices with each transaction in the construction process adding further cost to the end consumer. At each stage in housing production stamp duty is collected on both the contract sale price and levied on items such as GST, development charges and stamp duty applied in previous transactions. Stamp duty is generally imposed at three stages in the construction of a new house:</li> <li>• Sale of land to developer;</li> </ul>	<p>existing rates.</p> <p>The City's Developer Contribution are not considered to be ambiguous, as each clearly set out the items covered by the contribution.</p> <p>Council's Development Contribution Plan No. 13 is consistent with the Western Australian Planning Commission State Planning Policy which recognises the strain new developments place on local government community infrastructure, often to the detriment of their existing ratepayers. Transferring these costs onto ratepayers would increase living costs and is contrary to the intent of the Strategy. This is therefore not considered to be a solution to the issue of affordability.</p> <p>7. Noted.</p> <p>8. Noted.</p> <p>9. The assumption that the Strategy is based on an immobile population is incorrect. The Strategy is based on population and household projections set out in forecast.id which includes an assessment of net migration based on current population and the life cycle of the suburb. Therefore the statistics and discussion in the Strategy regarding future households are not based on an extrapolation of the current population ageing as suggested. The comment in the</p>

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		<ul style="list-style-type: none"> <li>• Sale of land from developer to builder; and</li> <li>• Sale of house and land package to purchaser.</li> </ul> <p>Stamp duty levied on previous taxes and charges as well as stamp duty levied on earlier transactions in respect to land and other development processes necessary to produce new house and land package, all adds cost to new house and land packages. In other industries, for example the used car industry, the commodity is regarded as holding stock and does not attract stamp duty until the sale to the ultimate consumer.</p> <p><b>Land Tax</b> – Land taxes paid by the developer must be passed to the consumer as part of the end price of a home.</p> <p><b>Council Rates</b> - Council rates during the development phase of housing also add to the holding costs of developers and, like all taxes, add to the final cost of a home and land package or an apartment. The costs of rates paid whilst approvals are sought can be significant and should be addressed by either a rate cut or exemption during the development phase.</p> <p><b>Development Contributions</b> – These contributions are considered to be an ambiguous tax and are imposed by both state governments and local councils for the provision of infrastructure. High infrastructure charges contribute greatly towards the unaffordability of housing as many authorities charge for items of infrastructure which are considered by HIA to be unreasonable. Development contributions should not apply in infill areas where the amenity and existing facilities are largely in place and available for the use of current and future residents. Council rates should be used for the maintenance and upgrade of these facilities over time as opposed to development contributions paid for new homed being built.</p> <p><b>7. Planning Delays and Housing Affordability</b></p> <p>Delays and uncertainty in the planning system add substantial cost to housing. An increase in the number of proposals requiring planning approval, a lack of experienced planning staff, high turnover rates in councils, lengthy referral processes that often add little value to built outcomes and the capacity for “objections” all serve to stifle timely decision-making and add to the cost of building a home.</p> <p>When poorly administered, local councils planning systems negatively affect</p>	<p>submission that “<i>Many people do prefer to down size in the area in which they have previously lived, but may be prevented from doing so as a result of inadequate housing stock to meet their needs or that home prices are too high</i>” is one of the key reasons we are trying to ensure that those appropriate housing options are available so that people have the opportunity to stay in their local area if that is their preference.</p> <p>10. The City, through its urban revitalisation strategies and structure planning is always seeking to achieve an appropriate range of residential densities. This Strategy will provide further justification for encouragement of higher densities and smaller dwellings in appropriate locations.</p> <p>11. The statistics relating to housing costs are noted.</p> <p>12. While cost of living issues are multi-faceted, it is still considered that planning does play an important role, particularly in creating compact, walkable neighbourhoods that will reduce car vehicle journeys; seeking to achieve lots with appropriate orientation and proportions through the structure planning and subdivision process; and encouraging the development of more sustainable dwellings.</p> <p>The Strategy is seeking to promote</p>

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		<p>the cost of housing. Companies and land owners have compounding holding costs whilst awaiting approvals. Rather than increasing the amounts of affordable housing quotas, councils would be better placed to assist housing affordability by ensuring planning permits are dealt with in a timely fashion. Land holding costs as a result of planning delays increase the cost of housing provision.</p> <p>HIA is supportive of many of the key planning actions proposed in the draft paper to improve planning processes within the municipality.</p> <p><b>8. Land prices and Housing Affordability</b></p> <p>High land prices are an obvious contributor to housing affordability issues and the cost of supplying housing. Sydney and Perth were the two most expensive land markets during the March 2014 quarter, both in terms of median lot size and in per square metre terms. In Sydney, the median lot price was \$299,500 (\$563 per square metre), largely unchanged since the previous quarter. Perth's median lot price was \$260,000 (\$619 per square metre) during the same period, a strong increase of 4.8 per cent over the previous quarter. On a per square metre basis, therefore, Perth is by far the most expensive land market in Australia.</p> <p>The high level of prices in Sydney and Perth reflects strong demand and incomes in both markets, as well as strongly growing population. However, land costs are a major factor behind poor affordability outcomes in both cities.</p> <p>Institutional factors also explain the high cost of land in both cities. The proportion of dwelling price accounted for by taxation is very high in both cities. The release of shovel ready land and the pace of planning and zoning in both cities has not kept pace with economic and population growth in either city.</p> <p><b>HIA Comments on the Key Findings in the Draft Paper</b></p> <p><b>9. Housing stock mismatch</b></p> <p><i>The City's current housing stock does not match the projected smaller households, and will not provide an adequate range of housing choices for future households. A greater number of smaller dwellings will be required to meet the needs of smaller households.</i></p>	<p>the benefits of building a sustainable dwelling. There are no proposals to mandate built form outcomes to address cost of living impacts.</p> <p>13. The Strategy is clear that the City is only seeking to promote the Liveable Homes Design Guidelines. There is no proposal to mandate these guidelines, nor to implement the City's own separate guidelines.</p> <p>14. There is generally not a separate zoning required for aged care, which is a permissible use in most zones.</p> <p>15. There is no proposal to mandate crisis accommodation. The Strategy seeks only to ensure that Council continues to identify opportunities for crisis accommodation in the City of Cockburn as a priority.</p> <p>16. The Strategy does not specifically recommend any 'mandatory measures' as such. The only reference to a mandatory measure is that the City continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning. It is considered appropriate that all options to improve the provision of affordable housing be explored given the extent of the issue. It is clear that the market is not delivering affordable outcomes, regardless of developers and builders</p>

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		<p>The likely mobility of the population is a key issue and the study assumes that the current residents of the municipality will remain rather than the city experiencing a change in demographics. Many people do prefer to down size in the area in which they have previously lived, but may be prevented from doing so as a result of inadequate housing stock to meet their needs or that home prices are too high.</p> <p>Local Government's role, however, must be to create the right environment to allow housing development to occur in a range of forms. Housing affordability is a function of adequate supply. The types of mandatory interventions, such as affordable housing quotas discussed in the paper, are not supported and can be counter-productive to the overall supply of all housing types, which ultimately limits supply, rather than increases it.</p> <p>Whilst industry builds a product that complies with relevant planning and building regulation, it must also take into consideration the needs and preferences of the market and ultimately build it at a cost that the market will bear for that particular product. Housing will respond to the demographics and needs of a population if an enabling regulatory environment is in place. Builders do not build product that consumers and investors will not or cannot purchase.</p> <p><b>10. Urban form mismatch</b></p> <p><i>The Perth Metropolitan Region is still characterised by predominately low density residential codings that have resulted in a housing stock of large detached dwellings, and many dwellings that in general do not have high levels of accessibility. The City should continue with great earnest its programs of revitalisation strategies, which have been very successful in delivering higher residential densities within established communities like Spearwood and Hamilton Hill. These identify opportunities for higher density living particularly within easy access to public transport, as a way to assist lower income households who may not have access to a car.</i></p> <p><i>Individuals on lower incomes also indicate a stronger preference for easy access to public transport than other income groups. Providing dwellings with good access to services and public transport is particularly important for young people and people with disabilities, who are high users of public transport.</i></p>	<p>providing housing at the 'lower end of the market' – the problem is that this is still not affordable for many low to moderate income earners.</p>

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		<p>There is a real role for the City of Cockburn to ensure that land is appropriately coded to allow for the development of housing it envisages will be needed by current and future residents. A wide range of residential codes should be encouraged to support a wide range of housing to be developed. Multiple dwellings provide greater choice and diversity particularly at the more affordable end of the market and for single person and small households.</p> <p>Housing diversity should be left to a combination of the planning system and market forces. This is not contrary to sound and orderly planning principles, but rather wholly consistent with the intent of the planning system to provide general guidance (for example through the R Codes) on where development should occur and let the market decide on whether demand exists.</p> <p><b>11. Declining Housing Affordability</b></p> <p><i>The number of Australian households in housing stress has increased dramatically since 2003, and this is a trend that is likely to continue to increase into the future. For 'very low', 'low' and 'moderate' income earners this means that owning a property is likely to be out of reach. Households susceptible to housing stress are low income renters, low income households with children, older people renting, and people with disabilities.</i></p> <p>Declining housing affordability has been an issue in Perth. In the twelve months to June 2014, the prices saw the following developments:</p> <ul style="list-style-type: none"> <li>• House values – annual growth of 5.0 per cent. Average annual growth over the past decade of 8.7 per cent.</li> <li>• Unit values – annual growth of 8.9 per cent. Average annual growth over the past decade of 8.0 per cent.</li> </ul> <p>In June 2014, Perth's median house price was \$532,000, while the median unit price was \$440,000. Following a price downturn in the eighteen-month period to October 2011, dwelling values in Perth have been recovering and have since increased by 15.2 per cent.</p> <p>After reaching its most favourable level in five years during the September 2013 quarter, housing affordability across Western Australia has deteriorated slightly due to slower earnings growth in the state and continued dwelling price increases. This is despite slightly lower mortgage interest rates over the</p>	

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		<p>same period.</p> <p><b>12. Cost of living impacts for low income households</b></p> <p><i>All households are impacted on by increasing costs of living, however low income households are the most affected. In particular it is single parent families and lone person households that are most susceptible to living cost increases. Housing affordability is a particular issue for family households who have a variety of living expenses that make them more susceptible to financial hardship, in addition to having high housing costs through the requirement in many cases for larger dwellings that have higher rental costs.</i></p> <p>This is a far greater issue that must be dealt with by other measures not by controlling the built form. The planning system has no effective role in the management of cost of living expenses in the broadest sense.</p> <p><b>13. Need for Adaptable Housing</b></p> <p><i>In the City of Cockburn there is an ageing population, and 18 per cent of people have a disability. For many of these people their home may not have a level of accessibility to suit their needs either now or in the future, because the number of private and public dwellings that have been built to incorporate universal design elements is very low. Inaccessible housing leads to social disadvantage and has negative effects for social integration and participation. Modifications to dwellings to improve accessibility, such as installation of ramps, are often expensive and unsatisfactory. These costs place increased financial pressure on such households, and moving house to find a better house design suited to their specific needs is not a viable option due to the high 'sunk costs' in the current accommodation.</i></p> <p>HIA has supported the voluntary development of national guidelines for universal (liveable) housing. However the case for mandating these standards in all new housing is yet to be made. Whilst reference is made to 18 per cent of people having a disability, this does not equate to 18 per cent of people having a physical disability that limits their ability to access and freely move in and around their homes.</p> <p>People with disabilities that limit their ability to access and move around their homes are supported by both Federal and State government programs that provide for tailored adaptation of their homes to meet their unique needs.</p>	

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		<p>HIA participated in the National Dialogue on Universal Housing Design, a joint building industry, disability and community sector working group established by the Federal Government in 2009. The group agreed to a 10 year strategic plan for voluntary action to increase awareness and uptake of universal housing design principles. The Strategic Plan has already included aspirational targets for industry adoption of a set of national design guidelines for all new residential buildings, being:</p> <ul style="list-style-type: none"> <li>• 25 per cent to silver level by 2013</li> <li>• 50 per cent to silver level by 2015</li> <li>• 75 per cent to silver level by 2018</li> <li>• 100 per cent to silver level by 2020</li> </ul> <p>Liveable Housing Australia (LHA) has been established and HIA continues to work with that group to promote voluntary adoption of the guidelines - including through programs run through individual organisations - such as HIA's own GreenSmart program.</p> <p>The HIA in Western Australia has been working closely with the State Governments Disability Services Commission (DSC) for several years to promote their Liveable Homes brand and "design essentials" to the residential housing sector in a non-regulatory/non-mandatory way, which has seen a positive approach to universal housing design by the industry. The HIA, as well as other government departments sit on the Liveable Homes Working Group chaired by the Director General of DSC Ron Chalmers to develop "non-regulatory" strategies to promote Liveable Homes.</p> <p>HIA has supported these initiatives and it would be undesirable for dilution of these programs through individual local governments running their own programs or establishing specific Local Government Authority requirements, however the promotion of the DSC Liveable Homes is supported by the HIA.</p> <p><b>14. Demand for Aged Care Facilities</b></p> <p><i>The ageing population, particularly the increase in people over 70 years of age, will see an increased demand for aged care facilities for those whose care needs can no longer be met within their own homes.</i></p> <p><i>The demand for low and high care facilities, in addition to respite care will</i></p>	

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		<p><i>continue to increase across the Perth metropolitan area. In particular there will be a demand for affordable aged care. Traditionally such facilities have been located on sites of 6-8 ha, however it is becoming increasingly difficult to find such sites, and a more flexible approach is required. The lack of suitable locations raises concerns regarding the shortfall of accommodation in the near future. It will be crucial that the City of Cockburn actively target supported accommodation development in any future redevelopment in order to meet the identified needs.</i></p> <p>The City of Cockburn should consider its zoning of land to allow for this type of development to occur as required.</p> <p><b>15. Shortage of Crisis Accommodation</b></p> <p><i>There is a trend of increased homelessness particularly for vulnerable households such as people with disabilities. Compounding the problems associated with greater incidence of homelessness in the community has been the breakdown of the affordable housing system. There is an identified shortage of crisis accommodation in the City of Cockburn There has been increased pressure on crisis accommodation because in addition to more people seeking crisis accommodation, the average length of stay has increased because of longer waiting times for social housing. Addressing the lack of affordable housing will go some way to reducing vulnerability to homelessness for some households. However, provision of crisis accommodation is still important to ensure that there is adequate accommodation for people waiting for social housing.</i></p> <p>Instead of mandating crisis accommodation together with affordable housing quotas, the supply of affordable housing and housing affordability should be addressed by improved planning and building measures. This allows industry to provide a service or type of housing to meet community need. Again, this should be able to be dealt with via the zoning of land.</p> <p>Crisis accommodation will always be a matter requiring government support in some form, whether through the provision of subsidies to assist community housing and charitable organisations provide accommodation or through the direct management of social housing to meet this need. Again this falls outside the scope of the planning system.</p> <p><b>(Table included in submission)</b></p>	

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		<p><b>16. Concluding Comments</b></p> <p>Housing affordability is affected by a complex array of issues – land supply, planning and building requirements and taxes and charges.</p> <p>Whilst the City of Cockburn is proactively looking at ways to match housing stock to the future population needs, intervention with mandatory provisions is not the answer.</p> <p>Mandatory measures outside of the technical requirements of the National Construction Code are not supported as they often contribute towards the unaffordability of housing – particularly if a cost benefit for the product, policy or requirement has not been accurately calculated.</p> <p>Inclusionary zoning policies and affordable housing quotas cause serious concern to an already over-taxed and over-regulated housing industry. Even where some nexus can be demonstrated, the application of these concepts is inequitable, particularly for the purchasers of new dwellings.</p> <p>Housing is largely a private sector activity and, as such, is driven by market forces. Many developments and builders already contribute towards providing affordable housing, based on their price positioning at the lower end of the market. If social housing is required then it is the responsibility of governments to provide such infrastructure from general revenue.</p> <p>Many of the planning initiatives outlined in this document are within the jurisdiction of the City of Cockburn and where considered appropriate, these have been supported by HIA. The Council is clearly looking towards how it can facilitate diverse housing development and particularly housing that its citizens can look to the future and afford to purchase as their needs change.</p>	
6	Property Council of Australia Ground Floor Parmelia House 191 St Georges Terrace Perth WA 6000	<p>The Property Council of Australia welcomes the opportunity to submit comments on the City of Cockburn's 'Housing Diversity and Affordability Strategy'.</p> <p>The Property Council of Australia is the leading advocate for Australia's property industry. It counts the bulk of the nation's major investors, property owners and developers - as well as the industry's professional service and trade providers - amongst its members.</p>	<p>1. Agreed. The City has been proactive by preparing urban revitalisation strategies to identify opportunities for infill development where appropriate. This has resulted in increases to residential codings in Hamilton Hill and Spearwood, with further zoning changes identified in Coolbellup. In addition, the City seeks to achieve a</p>

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		<p>Housing affordability is a concept denoting the dynamic relationship between household income and expenditure on accommodation. The Property Council believes that the concept of affordable living provides an appropriate framework for understanding the role that local government and the property industry can encourage housing diversity and affordable outcomes. As defined by in the Department of Planning's Planning for Affordable Housing Discussion Paper, affordable living recognises that direct rental or mortgage payments are not the only expenses that households incur. Affordable living recognises indirect costs of dwelling such as utility payments, access and cost of transport and proximity to employment. In some cases, the additional cost of living in low quality or poorly located dwelling can outweigh the benefits of lower priced accommodation.</p> <p>The Property Council therefore extends the following comments to encourage affordable living outcomes in the City of Cockburn.</p> <p><b>1. Infill and Greenfield Development</b></p> <p>The Western Australian property market presents significant opportunities to encourage diversity through infill and greenfield development. The Property Council (2013) in conjunction with Australian Urban Design Research Centre and The Greens investigated the infill housing potential along seven growth corridors. Planning provisions that focus on diversity to deliver affordable living opportunities should encourage an increase in housing supply through both infill and greenfield development. The Transforming Perth report concluded that medium to high density developments could yield more than 157,000 new dwelling if infill development was supported by public policy. Directions 2031 and Beyond as a strategic plan for Perth and Peel provides the opportunity for planning provisions to contribute to affordable living outcomes through a joint focus on both infill and greenfield development.</p> <p>The Property Council's support for dwelling diversity is supported by property industry data and echoed by other industry groups. The Housing Industry Association with the Commonwealth Bank (2013) noted an increasing demand for diversity through size particularly around transport nodes. The report revealed that there is a significant market for relatively smaller lots within new estates located in growth corridors. The Property Council strongly supports planning provisions that increase housing supply while promoting housing diversity. The Property Council maintains that infill development of</p>	<p>diversity of lot sizes and dwelling types in new Structure Plan areas.</p> <ol style="list-style-type: none"> <li>2. Noted. The range of incentives for provision of affordable housing is noted. All of these options will be examined as part of the proposed Planning Mechanism action: "Investigate the potential use of planning incentives to encourage affordable and diverse housing in targeted areas in the City of Cockburn, similar to that introduced for the Cockburn Coast area."</li> <li>3. The City has been implementing higher residential codings where appropriate through urban revitalisation strategies and structure planning. This enables a range of densities to be identified as deemed appropriate, taking into consideration the local circumstances. The R-Codes does not set out 'minimum lot sizes' for multiple dwellings, and they offer significant flexibility. It is therefore not considered necessary or appropriate for the City to ignore minimum lot sizes.</li> <li>4. Disagree. The proposed action is for the City to continue to lobby the state government to undertake a comprehensive state wide review of planning mechanisms to deliver affordable housing, including the option of mandatory inclusionary zoning. Given the extent of the housing affordability problem it is considered appropriate to examine</li> </ol>

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		<p>Perth's growth corridors presents the opportunity to achieve this goal. Property industry members believe that encouraging diversity in the context of the affordable living framework would be the most effective, long term market based strategy to deliver more affordable living outcomes.</p> <p><b>2. Incentives</b></p> <p>Voluntary planning incentives could take multiple forms to increase the supply of affordable living options including fast-tracked approvals and assessments, or development bonuses such as plot ratio bonuses, density bonuses or height bonuses as well as development concessions to increase the rate at which affordable living options are provided to the market. However, the Property Council suggests that local councils that employ voluntary incentives should be guided by a toolbox of approved measures.</p> <p>Fast-tacked assessment systems with guaranteed approval timeframes also provide an incentive to deliver affordable living. The Property Council believes that efficient housing supply requires a disciplined assessment and approvals system so that the housing market is operating at its fullest potential. Prioritised approvals will be an attractive incentive.</p> <p>Property Council of Australia members suggested that height, density and plot ratio bonuses make sense as a low cost planning incentive but only if they are structured to increase the project's feasibility. The affordable housing incentive structure should not impact the commercial viability of the development and preferably improve the developer's return. Although this may be the obvious function of a planning incentive structure, Property Council members have noted that some local governments in the Perth metropolitan region have designed incentive structures that impact the commercial return. For example the assumption that more floor space through height bonuses would be equated to a lower cost per square metre is incorrect. The additional space through height does not necessarily offset the cost of providing affordable housing and if anything may trigger more costs due to subsequent building codes. Ultimately any incentives employed to encourage affordable living dwellings need to be tailored to suit the development.</p> <p>Voluntary incentives could also include car bay concessions. The minimum number of car bays per new dwelling is a development cost that can be eliminated in areas of good public transport. Car parking adds significant cost</p>	<p>this as an option. Any review of the planning framework should include consideration of all options to determine whether they are appropriate. It is therefore recommended that this action remain in the Strategy.</p>

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		<p>particularly when densities make it impossible to accommodate surface parking. Additionally, sustainability concessions in the built environment for property developers to encourage utility efficiency could support affordable living outcomes as they reduce the overall cost of living.</p> <p>Fairly low cost incentives such as this can be implemented by integrating planning provisions with public transport plans.</p> <p>The Property Council believes that providing voluntary incentives to support the affordable living outcome is an appropriate role to be taken through the planning provisions. Feedback from Property Council of Australia members would caution that the incentives need to be tailored to the development so that the commercial return is not threatened.</p> <p><b>3. R-Codes</b></p> <p>The City of Cockburn would be able to easily create the environment under which greater affordable housing options are possible by choosing not to enforce the minimum size restrictions under the R-Codes. In this way, apartment size can be market dependent, with smaller apartments created to cater to those with smaller budgets. Pursuing housing affordability outcomes through the R-codes would provide dwelling opportunities to the demographics identified by the City of Cockburn as being most vulnerable to housing stress such as the elderly and young people.</p> <p>By the City of Cockburn choosing not to apply the R-Codes, they will be creating an environment in which it is possible to have diversity of stock that is also affordable.</p> <p><b>4. Mandatory Inclusionary Zoning</b></p> <p>The Property Council does not support the City's decision to lobby the State government to consider Mandatory Inclusionary zoning as mechanism to encourage housing affordability. International evidence shows that mandatory provisions fail to provide a significant boost to supply and in fact can discourage property development. Mandated provisions establish an incognito, inefficient tax levied that is passed onto non-mandated dwellings make the bulk of supply more expensive.</p> <p>The Property Council strongly supports market based solutions, opposed to mandates, to deliver affordability outcomes.</p>	

NO.	NAME/ADDRESS	SUBMISSION	CITY'S RECOMMENDATION
7.	Building Commission Department of Commerce Locked Bag 12 West Perth, Western Australia 6872	<p><i>Thank you for the opportunity to provide comments on the Draft Housing Affordability and Diversity Strategy.</i></p> <p>The Building Commission supports initiatives that encourage affordable and diverse housing without additional red tape and regulatory burden. In this regard, it is recommended that the City ensure that the adoption of such initiatives does not lead to an increase in construction requirements more stringent than the applicable building standards within the Building Regulations 2012 (i.e primarily those contained in the National Construction Code (Building Code of Australia).</p> <p>I wish the City every success with these initiatives in the future.</p>	<p>Noted.</p> <p>The Strategy is seeking to make development of smaller, affordable dwellings easier by removing the requirement for planning approval for ancillary dwellings, and providing more flexibility for single bedroom dwellings and aged and dependent dwellings.</p> <p>There are not proposals to increase construction requirements. The actions of the Strategy are simply seeking to better inform the community of the Liveable Design Guidelines and sustainable principles of design to assist them in their selection and design of new dwellings that are appropriate to their needs.</p>

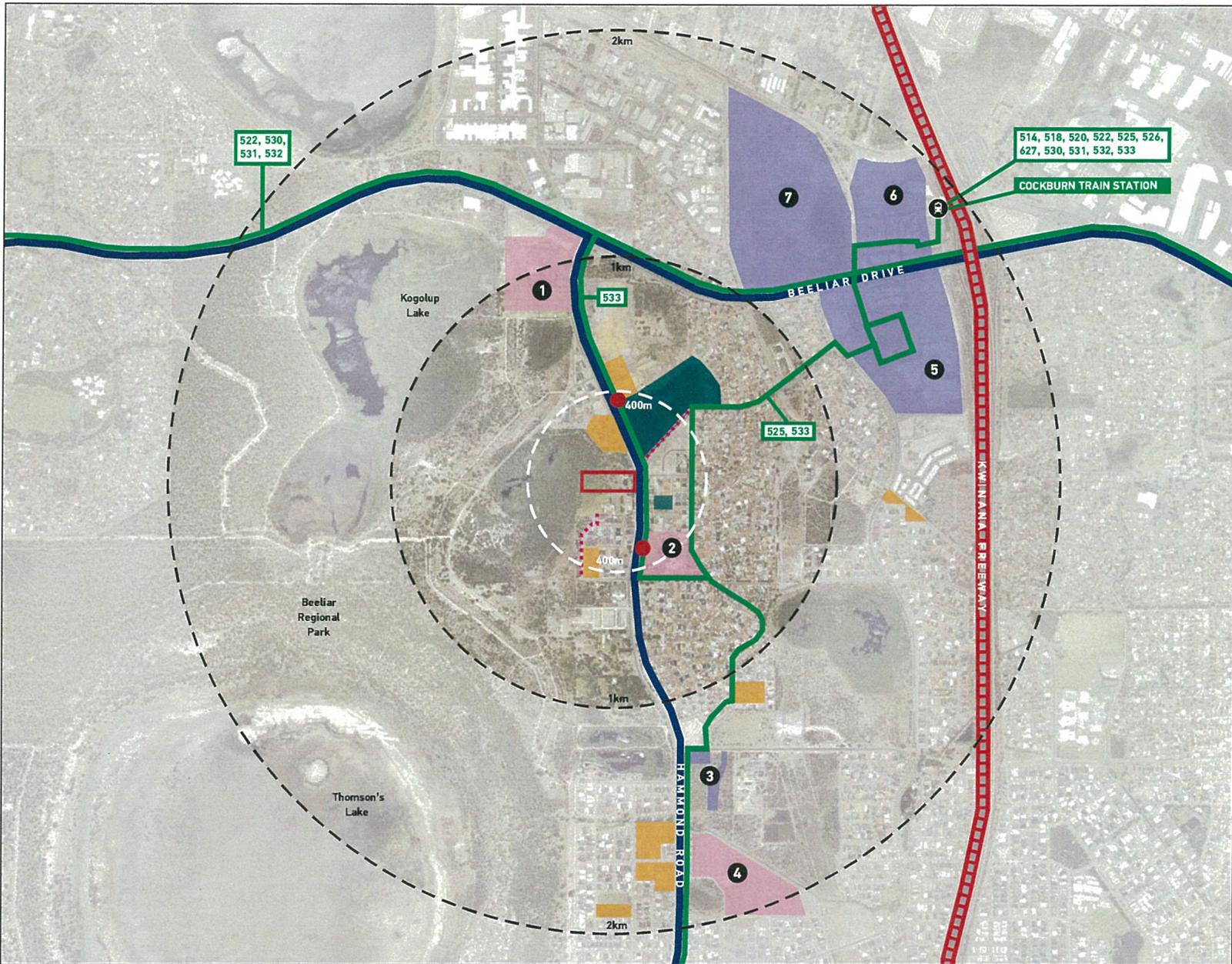
# CONTEXT PLAN

Lot 6 (210) Hammond Road, Success

Date: 22 Jul 2014  
 Scale: NTS @ A3  
 Project Manager: DR  
 Checked: AMH  
 Drawn: GW  
 Drawing No: 714-320 PL contextual



- source: neamaps
- Subject Site
  - Freeway
  - Key Roads
  - Rail Line
  - Bus Routes
  - 1km radius
  - 400m walkable catchment
  - Cockburn Train Station
  - Bus Stops in proximity to site
  - Commercial / Retail / Mixed Use
  - Community Facilities
  - Pockets of higher density
  - Public Open Space
  - Walk Trail
- 1** Emmanuel Catholic College
  - 2** Jandakot Primary School & Educational Support
  - 3** Future Local Centre & Mixed Business
  - 4** Sports & Recreation Centre
  - 5** Cockburn Central Shopping Centre
  - 6** Cockburn Central Mixed Use
  - 7** Cockburn Central West Development Area



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TOWN PLANNING  
 URBAN DESIGN AND HERITAGE

Figure 2 – Site, Aerial and Conservation Assets Plan



Lot 6 (210) Hammond Road, Success - Local Structure Plan 7

This concept has been prepared for the purpose of meeting client specifications. The drawing does not constitute an invitation, agreement or contract (or any part thereof) of any kind whatsoever.  
 Although care has been taken in the compilation of this drawing by The Planning Group Pty Ltd, all parties associated with the proposed project's development disclaim all responsibility for any errors or omissions. The right is reserved to change the plan at any time.  
 Liability is expressly disclaimed by The Planning Group Pty Ltd for any loss or damage which may be sustained by any person acting on any visual impression gained from this drawing.  
 All areas and dimensions are approximate and are subject to survey.

- Legend**
-  Subject Site
  -  Conservation Category Wetland
  -  50m Offset from Conservation Category Wetland
  -  MRS Road Reserve (Other Regional Roads)

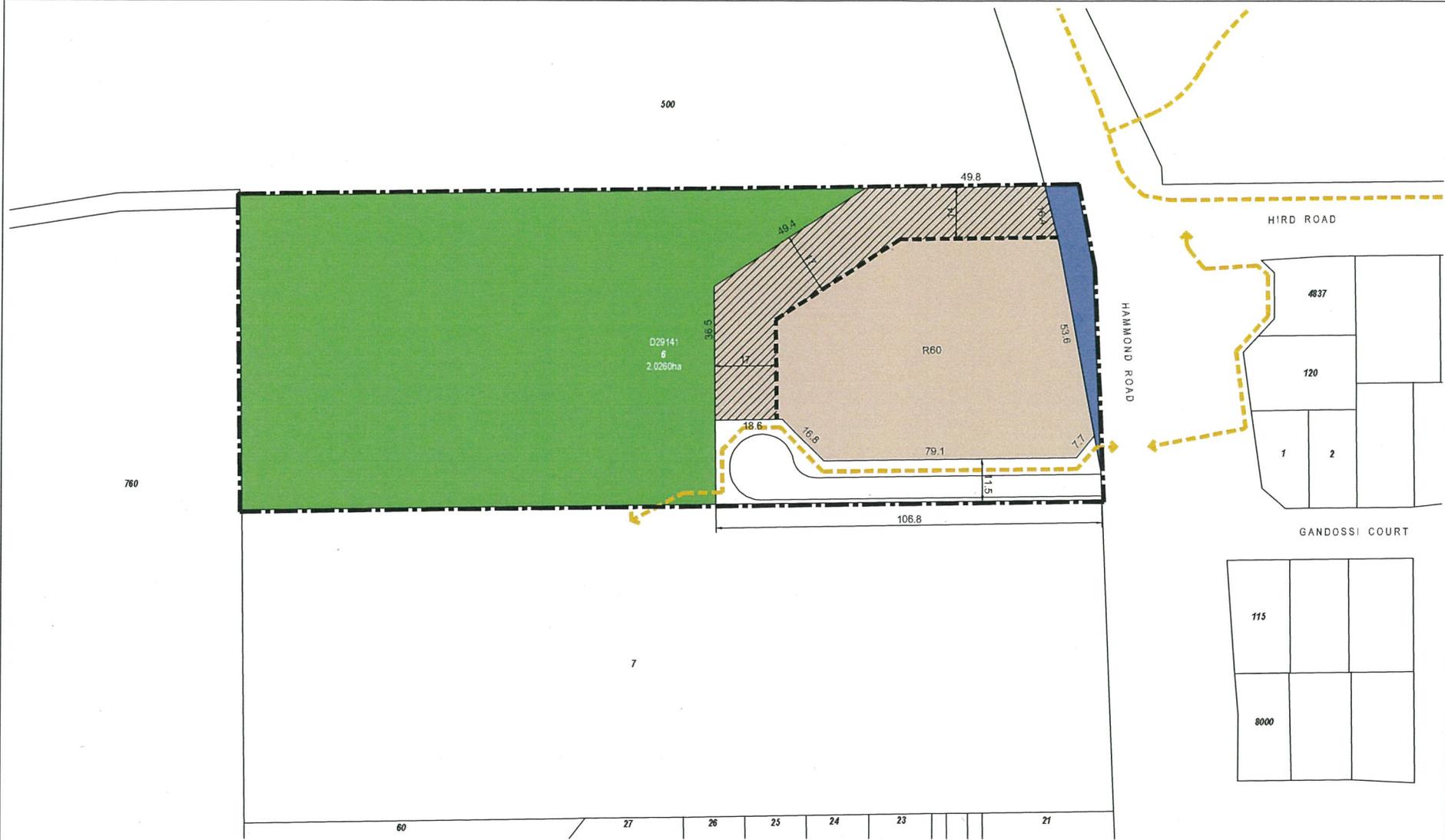
**SITE PLAN**  
 Lot 6 (210) Hammond Road, Success

Date: 22 Jul 2014      Designer: DR  
 Scale: 1:1,000 @ A3      Drafter: GW  
 Drawing No: 714-320 PL base.dwg

**TOWN PLANNING  
 AND URBAN DESIGN**



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**LEGEND**

**MRS RESERVES**

- Parks and Recreation
- Other Regional Road

**LOCAL PLANNING SCHEME ZONES**

- Residential (R60)
- Local Road

**OTHER**

- Structure Plan Area
- Indicative Location for Shared Path
- No Building Zone (see Local Structure Plan text)

**LOCAL STRUCTURE PLAN MAP**  
Lot 6 (210) Hammond Road, Success

**btg**

**TOWN PLANNING AND URBAN DESIGN**

Date: 15 Sept 2014      Designer: DR  
 Scale: 1:1,000 @ A3      Drawn: GW  
 Drawing No: 714-320 PL base.dwg

Level 7, 102 St George's Road, Park Station Auckland 1000      PO Box 728, Clarendon Square, Park Station Auckland 1000      Telephone: 09 428 6236      Fax: 09 428 6235      The Planning Group (NZ) Pty Ltd, Suite 20, 273 222

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**SCHEDULE OF SUBMISSIONS**  
**PROPOSED LOCAL STRUCTURE PLAN FOR LOT 6 (NO. 210) HAMMOND ROAD, SUCCESS**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Department of Planning – Policy Development and Review	<p><b>SUPPORT</b></p> <p>Within the Development Concept Plan (DCP) attached to this LSP, a 'low fuel zone' is proposed with Bush Forever area 391. The Bush Fire Management Plan attached to this proposal does not clarify what impacts the 'low fuel zone' will have on the Bush Forever area and without this clarification, Policy Development and Review suspects this entails clearing of bush land within a Bush Forever area and this is not supported.</p> <p>The DCP also shows housing is proposed to back onto Bush Forever area 391. The bushland/development interface should be designed to; minimise edge effects on the bushland, such as, problems resulting from nearby weeds and disease, uncontrolled access, surface water runoff and rubbish dumping; assist in managing bush fire risk to property and; controlling social issues such as vandalism through visual surveillance. Whilst Policy Development and Review have no objections to the LSP, the DCP is not supported. As such, Policy Development and Review recommends the LSP is to incorporate, and the DCP be amended to:</p> <ul style="list-style-type: none"> <li>demonstrate all bushfire risk management measures are within the development area,</li> </ul>	<p>Noted. The applicant has since revised the Local Structure Plan report (September 2014) inclusive of a revised Bushfire Management Plan (BMP). The revised version no longer requires clearing within Bush Forever area 391. Clearing/ fuel load reduction will be limited to Lot 6.</p> <p>The indicative development has since been amended to be located further from the Bush Forever area within the revised LSP report. Furthermore a 'no Building zone' has been incorporated into the Local Structure Plan Map which ensures appropriate setbacks from the Bush Forever area.</p> <p>Section 3.5 Part 2 of the revised September 2014 LSP report ensures no direct drainage of waste and/or storm water enters the adjacent Bush Forever area. The detailed drainage details will be addressed further within the Urban Water Management Plan at subdivision stage.</p> <p>Section 5.2 Part 1 of the revised September 2014 LSP report mandates the requirement for a Detailed Area Plan for lots with direct boundary frontage (primary or secondary) to an area of Parks and Recreation. The future DAP(s) will mandate appropriate fencing requirements at subdivision stage.</p> <p>Appendix 4 –Bushfire Management Plan has been included as part of the LSP report which reflects draft State</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Lot 210 Hammond Road, not within the adjacent Bush Forever area. Clearing of bush land within Bush Forever is contrary to the intent of State Planning Policy 2.8 - Bushland Policy for the Perth Metropolitan Region which supports a general presumption against clearing regionally significant bush land within a Bush Forever area. The draft Planning for Bushfire Risk Management Guidelines also supports the acknowledgment and integration of conservation objectives into bushfire risk management;</p> <ul style="list-style-type: none"> <li>• ensure no direct drainage of waste and/or storm water enters the adjacent Bush Forever area;</li> <li>• provide for a hard edge, preferable a road; lot design facilitating development fronting the bushland; and open style fencing; between the four dwellings in the north east corner and the Bush Forever area; and</li> <li>• provide for appropriate fencing to facilitate bush land management between the development area and the wetland and Bush Forever area.</li> </ul> <p>There is a Conservation Category Wetland (CCW) on the north-western portion of Lot 6, which is proposed to be transferred to the Crown free of cost. As part of this negotiation, the standard 50 metre</p>	<p>Planning Policy 3.7 and the previous 2010 Bushfire guidelines.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>CCW buffer is proposed to be reduced. There is currently a dwelling on proposed Lot 1, which is currently in the 50 metre setback area to the CCW. This dwelling will be retained. The wetland will be separated from the development by a new access road for Lot 1, providing a hard edge and low fuel zone. However, it is noted the hard edge does not appear to continue around all of the current dwelling within proposed lot 1 on the DCP. As such Policy Development and Review recommends:</p> <ul style="list-style-type: none"> <li>• advice is sought from the Department of Parks and Wildlife (DPaW) regarding the proposed reduction of the wetland buffer; and</li> <li>• consideration is given to providing a hard edge, such as a road or a path, which extends around the entire dwelling within lot 1 to further assist in the management of the adjacent wetland and Bush Forever area and provide for visual surveillance.</li> </ul> <p>Please note that this is Policy Development &amp; Review's response with regard to Bush Forever only on the proposed Local Structure Plan and does not reflect comments of other branches of the Department of Planning or a formal position of the Western Australian Planning Commission (WAPC), which may need to be consulted on this proposal.</p> <p>It should also be noted that draft State Planning Policy 3.7 - Planning for Bush Fire Risk Management and draft Planning for Bushfire Risk</p>	

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		<p>Management Guidelines have been produced and the WAPC have endorsed the documents to be 'seriously entertained' when preparing planning proposals and making planning decisions. The LSP and Bush Fire Management Plan should acknowledge these documents be incorporated into the LSP proposal.</p>	
2	Department of Parks and Wildlife	<p><b>SUPPORT</b></p> <p>The subject land (Lot 6) is located adjacent to an area of Beeliar Regional Park, which comprises Branch Circus Wetland and forms part of Bush Forever Site No. 391. Branch Circus Wetland extends into the western and northern portions of Lot 6.</p> <p><b>Wetland management and buffers</b></p> <p>The subject land comprises part of a Conservation category wetland (CCW) - known as Branch Circus Wetland - as identified in the Department's Geomorphic Wetlands Swan Coastal Plain dataset. CCWs are wetlands that support a high level of ecological attributes and functions, and are the highest priority for protection.</p> <p>In accordance with Environmental Protection Authority's (EPA's) <i>Guidance Statement No. 33 - Environmental Guidance for Planning and Development (EPA 2008)</i>, the Department's position is that a minimum 50m buffer is required to protect the ecological functionality of CCWs and to protect wetlands from proposed land use change and</p>	<p><b>Wetland management and buffers</b></p> <p>Noted. Part 2 section 3.5.1 specifies "<i>environmental consideration such as weed management, nutrient filtering, reducing spread of rubbish and disturbance by human activity, among others, will be addressed at the detailed design stage of the proposal, with the requirement for a wetland rehabilitation and landscaping strategy to be prepared and implemented prior to development</i>". The buffer treatment details will therefore be addressed at subdivision stage and or development stage. As requested by DPaW - Part 1 section 6.2 (1) (a) was included as part of the revised LSP report dated September 2014 to include the need for a '<i>Wetland rehabilitation and landscape strategy</i>' at as a condition of subdivision or development approval (whichever is sooner).</p> <p><b>Drainage Management</b></p> <p>Part 2 Section 5.6 specifies that the post development site levels are expected to accommodate and treat smaller storm events on-site before discharging into the wetland. The associated Engineering Services Report clarifies that all infiltration swales will incorporate nutrient-stripping</p>

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		<p>impacts such as weed invasion. The Department notes from the LSP that a partial buffer (0.3654ha area) is proposed for protection and shown to contribute towards the public open space (POS) allocation for the LSP area.</p> <p>The Department considers that the proposed treatment of the wetland buffer is not sufficiently clear in the LSP Report. Where a wetland buffer is being considered as part of the subdivision POS, its treatment should be appropriate and contribute towards the maintenance of ecological functioning within the wetland that is the buffer should be revegetated with appropriate native vegetation species of local provenance. Vegetation of natural structure including groundcovers, mid-storey and over-storey around wetlands is critical for filtering and absorption of nutrients and pollutants, provision of fauna habitat, abating nuisance insect issues and restricting the spread of rubbish into the wetlands. Therefore, while some passive recreation may be acceptable in defined areas of the wetland buffer, the placement of lawns, playgrounds and other active recreation areas are generally not considered appropriate.</p> <p>The Department recommends that the LSP include a statement(s) to the effect that a Wetland Management Plan is likely to be required as a condition of any future subdivision. Specifically, this Plan will need to address such issues as protection of remnant vegetation, and access provision and control (e.g. fencing and gates), and also include</p>	<p>plants to help maintain long-term water quality. Drainage will be addressed in further detail at subdivision stage via the Urban Water Management Plan.</p> <p><b>Flora Management</b></p> <p>Noted.</p> <p><b>Boundary Interface treatment</b></p> <p>Section 5.2 Part 1 of the revised September 2014 LSP report mandates the requirement for a Detailed Area Plan, at subdivision stage, for lots with direct boundary frontage (primary or secondary) to an area of Parks and Recreation. This DAP will mandate appropriate fencing requirements at subdivision and development stage.</p> <p><b>Fire Management</b></p> <p>The applicant has since revised the Local Structure Plan report (September 2014) inclusive of a revised Bushfire Management Plan (BMP). The revised version no longer requires clearing within Bush Forever area 391. Clearing will be limited to Lot 6.</p> <p>The indicative development is now further setback from the Bush Forever area within the revised LSP report. Furthermore a 'no Building zone' has been incorporated into the Local Structure Plan Map which ensures appropriate setbacks from the Bush Forever area. The applicant did not decide to peruse the 'fire wall' between the subject site the wetland/ Bush Forever site (which was discussed at the DPaW, City and applicant meeting in early</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>strategies to control weeds over set period of time and to revegetate the buffer with local endemic flora species. Note that the wetland management plan should be to the satisfaction of the City of Cockburn, on the advice of the Department.</p> <p>The Department recommends that the applicant:</p> <ul style="list-style-type: none"> <li>• provide for an appropriate buffer to CCW;</li> <li>• preparation and implementation of Wetland Management Plan for the retained wetland and buffer; and</li> <li>• ensure that residential dwellings are setback from the CCW to provide for adequate fire protection without the need for fuel reduction within the CCW or its agreed buffer.</li> </ul> <p><b>Drainage management</b></p> <p>It should be noted that no development (including stormwater infrastructure) should be located within the CCW or the wetland buffer, nor should any stormwater drainage (including road drainage) discharge directly into a CCW or wetland buffer.</p> <p><b>Flora management</b></p> <p>Aerial imagery indicates limited native vegetation within the proposed development area (i.e. land outside of the wetland and wetland buffer). The Department recommends that the proponent avoid clearing native vegetation outside of the wetland and buffer area, if possible.</p>	<p>September 2014).</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p><b>Boundary interface treatment</b></p> <p>The Department supports having a hard edge (e.g. perimeter road) between residential development and the regional park, and between residential development and the proposed conservation area (e.g. public open space/wetland buffer), for reasons of public safety, protection of bush land and fire safety for residents.</p> <p>To ensure an adequate interface between the regional park and residential development, the proponent should ensure there is appropriate fencing (being to the specifications and satisfaction of the Department) installed along the boundary of the regional park and the boundary of the public open space.</p> <p><b>Fire management</b></p> <p>All necessary fire management requirements should be provided for within the subject land, in accordance with the (Interim) Planning for Bushfire Protection Guidelines, Edition 2 (Western Australian Planning Commission and Fire and Emergency Services Authority 2010) and any other relevant policies. The Department recommends the provision of a hard edge (e.g. perimeter road) to provide adequate separation between conservation areas (e.g. public open space, regional park) and urban development. Such a hard edge helps mitigate fire risk and provides improved access for fire suppression.</p>	

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>It is noted that the 20m Building Protection Zone encroaches into the wetland and wetland buffer, which contravenes the guidance in the EPA Guidance Statement No. 33 which outlines that CCWs support a high level of ecological attributes and functions, and are the highest priority for protection, and considers a minimum of a 50m buffer zone is required to maintain the values, functions and attributes of protected wetland areas. If the applicant decides on a wall at the regional park – subject land interface as a fire protection measure, then the applicant is to ensure the wall is located completely within the boundary of Lot 6 Hammond Road. Additionally, it is to be noted that the wall may become susceptible to vandalism or graffiti, for which the Department accepts no responsibility.</p>	
3	Water Corporation	<p><b>SUPPORT</b></p> <p><b>Water</b></p> <p>Reticulated water is currently available to the subject area. All water main extensions, if required for the development site, must be laid within the existing and proposed road reserves, on the correct alignment and in accordance with the Utility Providers Code of Practice.</p> <p><b>Wastewater</b></p> <p>Reticulated sewerage is currently available to the subject area. All sewer main extensions, if required</p>	Noted.

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>for the development site, should be laid within any existing and proposed road reserves, on the correct alignment and in accordance with the Utility Providers Code of Practice.</p> <p>It should be noted that existing sewerage mains are located alongside the subject land near the southern boundary and on the correct alignment of the existing Hammond Road. The sewer in Hammond Road may need relocating if Hammond Road is widened. The sewer on the southern boundary should be located on the correct alignment of the proposed road reserve. The developer is required to fund the full cost of protecting or modifying any of the existing infrastructure which may be affected by the proposed development.</p> <p><b>Drainage</b></p> <p>The subject area falls within the Southern Lakes Drainage Catchment.</p> <p><b>General Comments</b></p> <p>The principle followed by the Water Corporation for the funding of subdivision or development is one of user pays. The developer is expected to provide all water and sewerage reticulation if required. A contribution for Water, Sewerage and Drainage headworks may also be required. In addition the developer may be required to fund new works or the upgrading of existing works and protection of all works. Any temporary works needed are required to</p>	

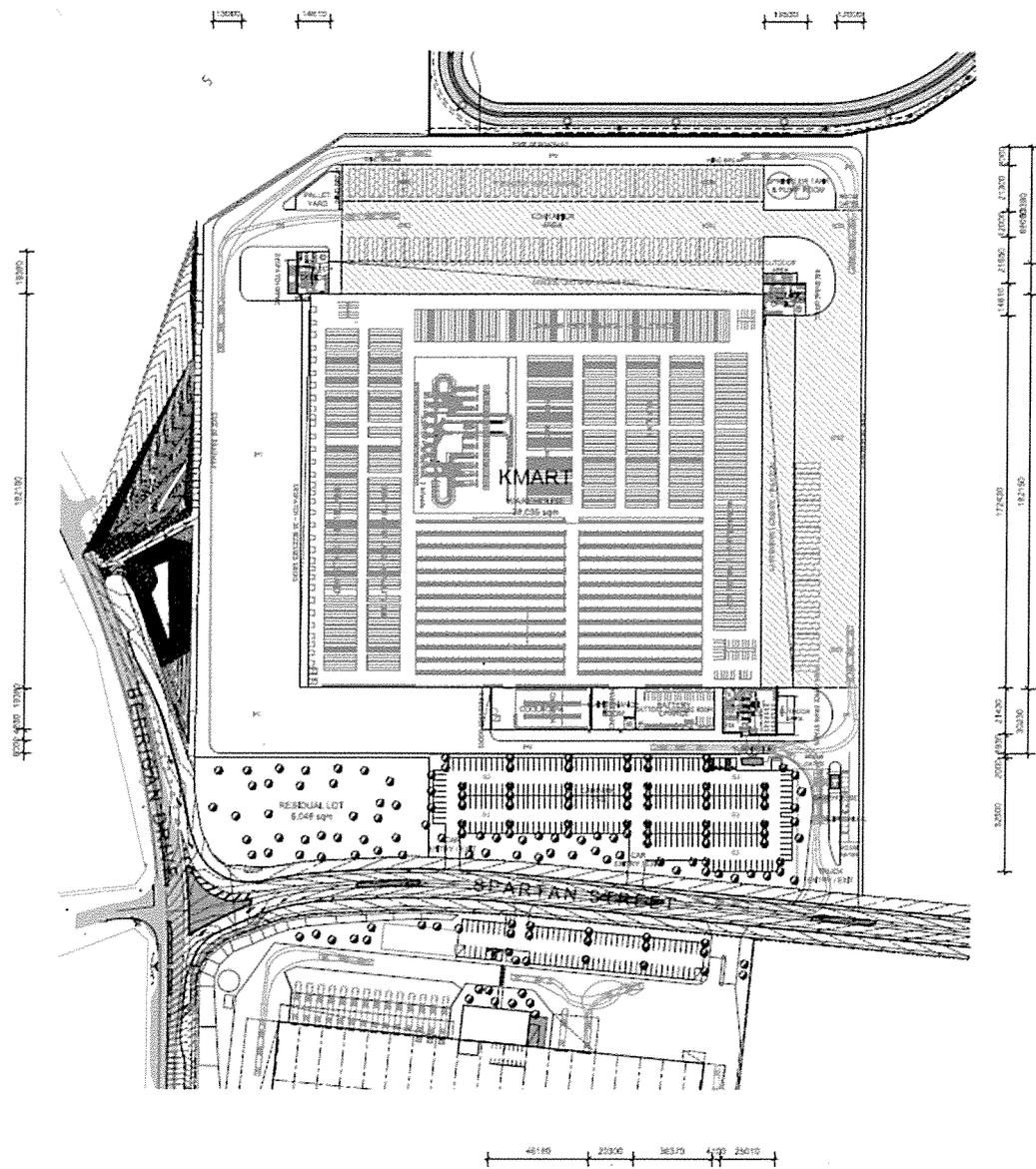
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>be fully funded by the developer. The Corporation may also require land being ceded free of cost for works.</p>	
4	Department of Aboriginal Affairs	<p><b>SUPPORT</b></p> <p>The Department of Aboriginal Affairs (OM) has reviewed the Aboriginal Heritage Register and can inform you that there are no known Aboriginal Heritage Sites or Other Heritage Places, located on the land.</p>	Noted.
5	Department of Planning – Strategy, Policy and Projects	<p><b>SUPPORT</b></p> <p><b>Land Requirements</b></p> <p>Lot 6 abuts Hammond Road which is reserved as an Other Regional Road (ORR) in the Metropolitan Region Scheme (MRS) and is reserved as Category 2 as per Plan Number SP 694/3. The subject land is affected by the ORR reservation for Hammond Road, as per the attached Western Australian Planning Commission (WAPC) Land Requirement Plan No. 1.2918/1 and this requirement has been acknowledged on the submitted plans.</p> <p><b>Transport Impact Assessment</b></p> <p>The Transport Impact Assessment prepared by KCTT dated July 2014, states that the development will generate a total of 231 daily vehicular trips with approximately 34 trips in the peak hour. A left in / left out access arrangement is proposed via a new intersection which will be created as a result of this proposal. Hammond Road will be upgraded to dual</p>	<p><b>Land Requirements</b></p> <p>Noted.</p> <p><b>Transport Impact Assessment</b></p> <p>Noted.</p> <p><b>WAPC SPP 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning</b></p> <p>The current and expected future vehicle movements per day are under the prescribed amount (20 000vpd) within section 5.2.2 of State Planning Policy 5.4. Accordingly the LSP cannot mandate the need for a notification on title. Notwithstanding this may be something that the Western Australian Planning Commission addresses at subdivision stage or alternatively something that the future Development Applications addresses later in the project.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>carriageway with median by 2017/18 which will exclude right turning movements from Lot 6 in the longer term.</p> <p>The crossover to the existing dwelling is to be relocated to the cul-de-sac head of the new road. The submitted layout plan states that 'all existing crossovers (shall be) closed to Hammond Road'. This is in accordance with the Commission's Regional Roads (Vehicular Access) Policy D.C. 5.1, which seeks to minimise the number of new crossovers onto regional roads and rationalise existing access arrangements.</p> <p>Section 7.0 'Development Contribution Items and Arrangements' states that 'Hammond Road is to be upgraded to a dual carriageway standard' and 'road widening is to be transferred to the Crown free of cost to the State'. It also states that 'land identified for the creation of a 15m wide new road shall be transferred free of cost to the crown'.</p> <p><b>WAPC SPP 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning</b></p> <p>WAPC SPP 5.4 seeks to minimise the adverse impact of transport noise on developments. Main Roads WA ROM mapping for 2031 shows approximately 14,700 vpd in the subject location by 2031. Due regard should be given to Policy SPP 5.4 which advises that if noise management and mitigation measures cannot reduce noise levels to</p>	

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>acceptable levels, then a notification should be placed on Title for affected lots. An example of a suitable notice is as follows:</p> <p><i>Notice: This lot is situated in the vicinity of a transport corridor and is currently affected, or may in the future be affected, by transport noise. Further information is available on request from the relevant local government offices.</i></p> <p>The Department of Planning has no objection to the proposal on regional transport planning grounds.</p>	
6	Department of Water	<p><b>Urban Water Management</b></p> <p>Consistent with Better Urban Water Management (BUWM) (WAPC, 2008) and policy measures outlined in State Planning Policy 2.9, Water Resources, Local Structure Plans should be supported by a Local Water Management Strategy (LWMS).</p> <p>The LWMS should demonstrate how the subject area will address water use and management. It should contain a level of information that demonstrates the site constraints and the level of risk to the water resources.</p> <p>However, previous DoW advice stated that a full LWMS would not be required and that water management issues shall be addressed within the Engineering Report. Outstanding issues that are yet to be addressed include:</p>	<p><b>Urban Water Management</b></p> <p>The applicant has been advised of the Department of Water (DoW) comments. The applicant is in the process of updating the Engineering report, included as part of the LSP appendices, in accordance with the details requested by the DoW. The Council report includes an appropriate condition to this effect. The modified engineering report will be provided to the City prior to referral of the LSP to the Western Australian Planning Commission. The details of the updated engineering report will be to the satisfaction of the City of Cockburn in consultation with the Department of Water.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<ul style="list-style-type: none"> <li>• Demonstrate the management of runoff from the site for the 1 year 1 hour, 5 year and 100 year ARI storm event to ensure the proposed "drainage area" is adequately sized in the Local Structure Plan area;</li> <li>• Design concept of the drainage area including batters, inverts and depth to groundwater (basin invert is to be a minimum 0.3m from maximum groundwater level);</li> <li>• 1 year 1 hour AR events to drain into bioretention areas that are to be included in the parking areas, and/or drainage area;</li> <li>• Confirmation that finish floor levels are to be 0.3m above 100 year flood levels;</li> <li>• Groundwater levels for the site.</li> </ul> <p>The above-mentioned points should also be demonstrated within figures and clearly identified on plans.</p> <p>Therefore, in accordance with Better Urban Water Management, the proposed amended local structure plan should not be endorsed in the absence of the relevant water management information to the satisfaction of the City and DoW.</p>	







SOURCE: KMART



11<sup>th</sup> April 2014

Sheena Malcolm  
Level 3/169 Hay Street  
East Perth WA 6004

Dear Sheena,

**RE: Kerbside Valuation of 7 Beelier Place, Hamilton Hill WA 6163**

We refer to your letter of instruction dated 9<sup>th</sup> April 2014, in relation to the above mentioned property.

Further to your instructions, we confirm having conducted a kerbside inspection of the subject property on the 11<sup>th</sup> April 2014 and have also undertaken the necessary market investigations to complete our assessment of value.

In providing our revised assessment of value for the property, we have investigated and analysed sales evidence having occurred throughout the locality from which we have drawn our conclusions. In addition to the sales evidence examined, our investigations have included enquiries with real estate agents active within the locality and in particular, those agents who have been involved in the sale of such properties.

Having regard for all of the above, we conclude that the subject property would command a market value range from **\$875,000 to \$925,000 with the sum of \$900,000** considered fair and reasonable.

We trust the above is sufficient for your needs however, should you have any further queries or require further elaboration, please do not hesitate to contact the undersigned.

**WBP PROPERTY GROUP**

A handwritten signature in black ink, appearing to read 'Justin Honey', is written over a light blue horizontal line.

**JUSTIN HONEY- AAPI, B. Com (Prop and Fin)**  
AAPI CPV 65226 Lic 44372

VIC | NSW | QLD | SA | WA | TAS

Suburban Valuations Pty Ltd trading as WBP Property Group (WA), 56 Palmerston Street Perth WA 6000  
Postal Address: PO Box 154 Northbridge WA 6865

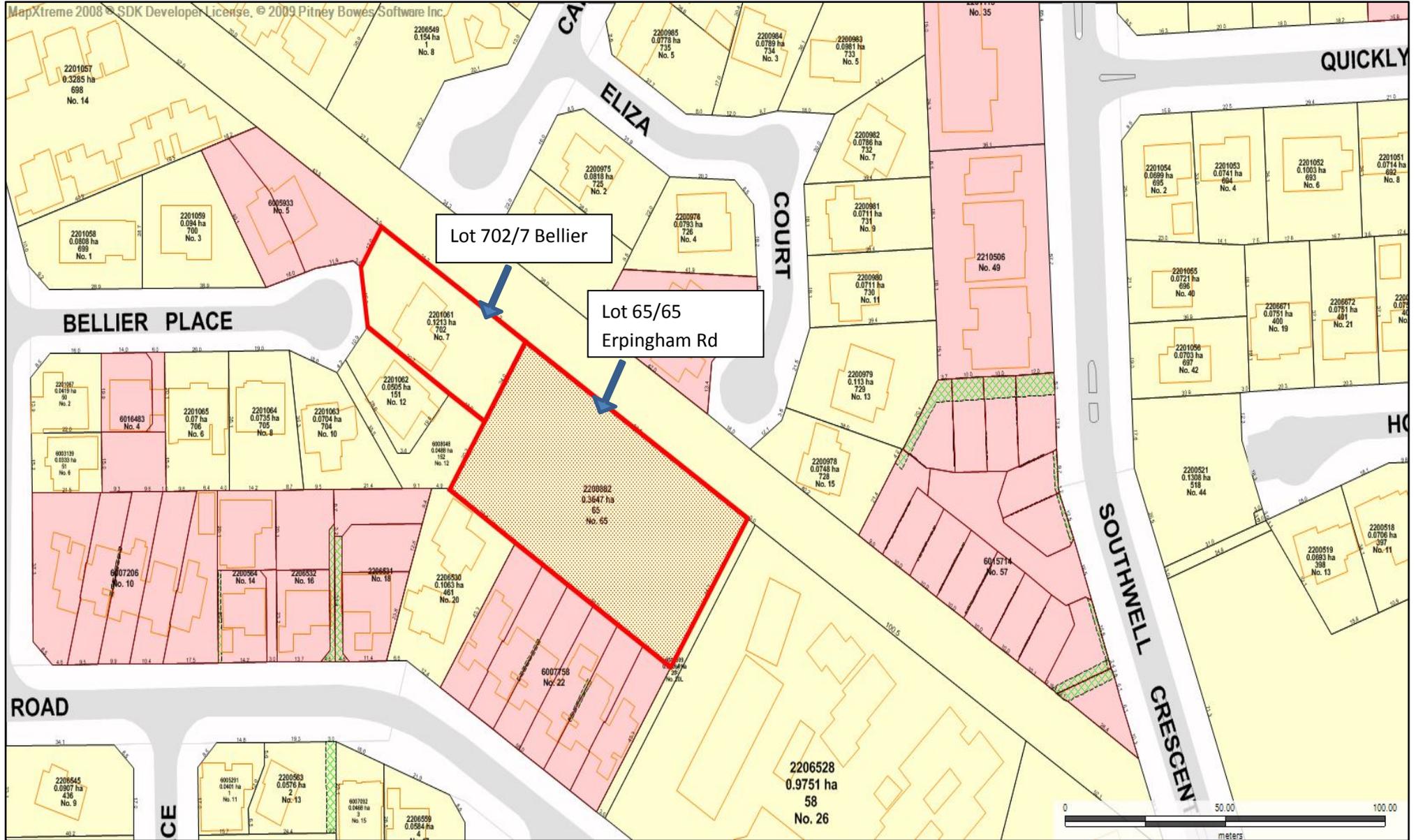
T. (08) 9328 6242 F. (08) 9328 6492 E. admin@wbppropertywa.com W. www.wbpproperty.com



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## Executive Summary

<b>Property Address:</b>	No. 16 (Lot 432) Rodd Place, Hamilton Hill
<b>General Description:</b>	The property comprises a 7,639m <sup>2</sup> irregular shaped lot that is zoned "Parks and Recreation" and "Residential R30" in accordance with the City of Cockburn's Town Planning Scheme No. 3. Our valuation has assumed that 4,504m <sup>2</sup> of the site will be zoned "Residential R30" whilst the remainder of the site will need to be ceded free of cost as "Parks and Recreation".
<b>Purpose of Valuation:</b>	To assess the fair current market value of the subject lot for disposal purposes.
<b>Valuation:</b>	\$2,625,000
	This valuation amount is inclusive of a Goods and Services Tax and assumes that 4,504m <sup>2</sup> of the site is zoned "Residential R30" whilst the remainder of the subject lot will be ceded free of cost for "Parks and Recreation".
<b>Date of Inspection:</b>	25 August 2014.
<b>Date of Valuation:</b>	25 August 2014.
<b>Senior Valuer:</b>	<b>Wayne Srhoy</b> AAPI, Masters (Property) Certified Practising Valuer Licensed Valuer No. 44175 Western Australia

**This Executive Summary is a brief synopsis of the property and our assessment of market value.**

**It is designed to provide a brief overview and must not be read in isolation, separate from our formal valuation report.**

### Definition of "Market Value":

*The International Valuation Standards Council (and as adopted by the Australian Property Institute) defines market value as:*

*"The estimated amount for which an asset or liability should exchange on the date of valuation between a willing buyer and a willing seller in an arm's length transaction after proper marketing wherein the parties had each acted knowledgeably, prudently and without compulsion."*

### Assumptions, Conditions and Limitations:

- o The planning and cadastral details obtained from the Western Australian Planning Commission, Main Roads Western Australia, Landgate and Local Authority websites are current and correct.
- o Adjoining land owners or community groups do not impede or restrain development as foreseen.

o We are not aware of any notices currently issued against the property and we have made no enquiries in this regard.

o A visual site inspection has not revealed any obvious asbestos contamination. Nevertheless, we are not experts in the detection or quantification of asbestos problems and accordingly, have not carried out a detailed investigation. Therefore, this valuation is made on the assumption that there are no actual or potential asbestos contamination issues affecting the subject property.

Should a subsequent investigation undertaken by a suitably qualified expert show that the site is contaminated, we reserve the right to amend our valuation.

o The value and utility of land can be adversely affected by the presence of Aboriginal sacred sites and/or sites of aboriginal heritage significance. We have made no investigations in this regard, as Aboriginal requirements can only be determined by the appointment of an appropriate expert.

Under these circumstances, we cannot warrant that there are no such sites on the land and if it is subsequently determined that the realty is so affected, we reserve the right to review this valuation.

o The land comprises topsoils, which appear to be relatively free draining however, as no geotechnical investigations have been either undertaken or commissioned, we are unable to report on the underlying nature of the site.

o This valuation assumes there is no environmental contamination of the property.

o This valuation assumes there is no encroachment of adjoining buildings onto the subject land.

o This valuation assumes an unencumbered fee simple title to the property.

o If there are any encumbrances, encroachments, restrictions, leases or covenants which are not noted in this report, they may affect the assessment of value. If any such matters are known or discovered, we should be advised and asked as to whether they affect our assessment of value.

o We have assumed that all information supplied in conducting this valuation consists of a full and accurate disclosure of all information that is relevant.

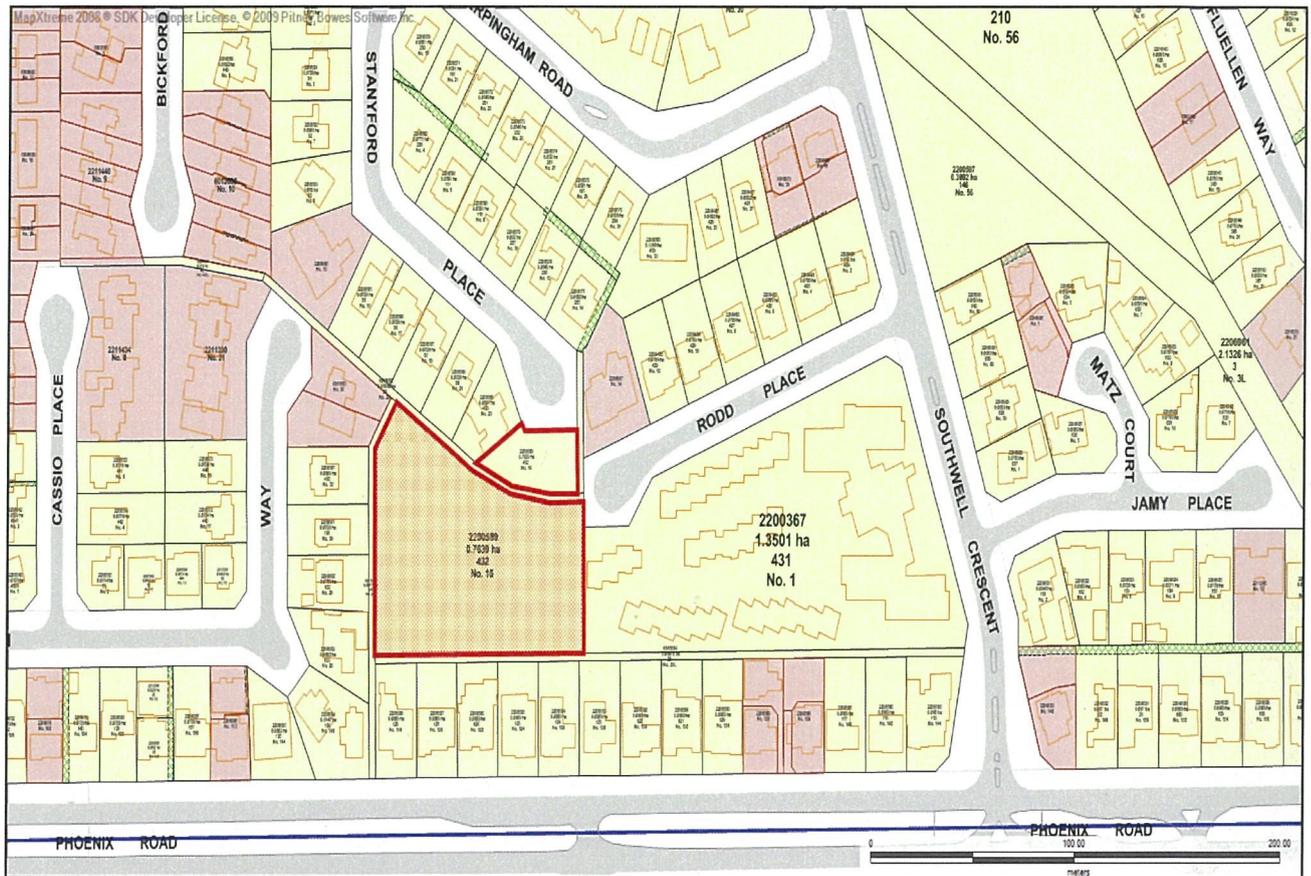
o It is assumed that no significant event occurs between the date of inspection and the date of valuation that would impact on the value of the subject property.

o We have not obtained a Property Interest Report in providing our advice. A property-specific report will provide detailed information of property interests not listed on the Certificate of Title that may affect the use and enjoyment of the land.

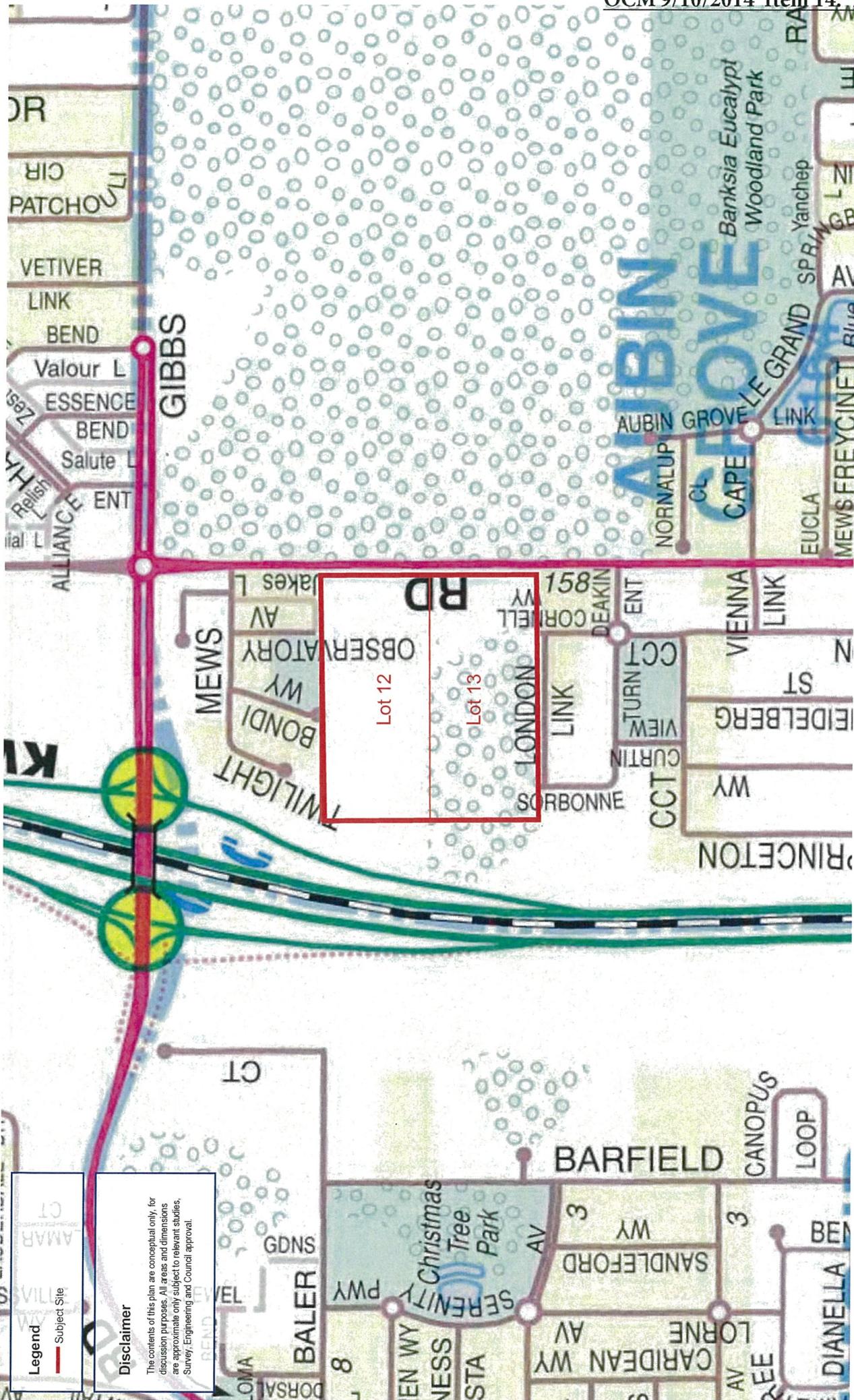
A report can be obtained from Landgate for a charge of \$54.95 (inc GST). If a subsequent Property Interest Report reveals any aspects of the property that may impact on its value, we reserve the right to review our assessment.

If there is any variance/contradiction in any of the above assumptions, then we reserve the right to review this valuation accordingly.

\*\*\*\*\*



**Lot 432 Rodd Place, Hamilton Hill.**



**Legend**  
— Subject Site

**Disclaimer**  
 The contents of this plan are conceptual only, for discussion purposes. All areas and dimensions are approximate only subject to relevant studies, Survey, Engineering and Council approval.

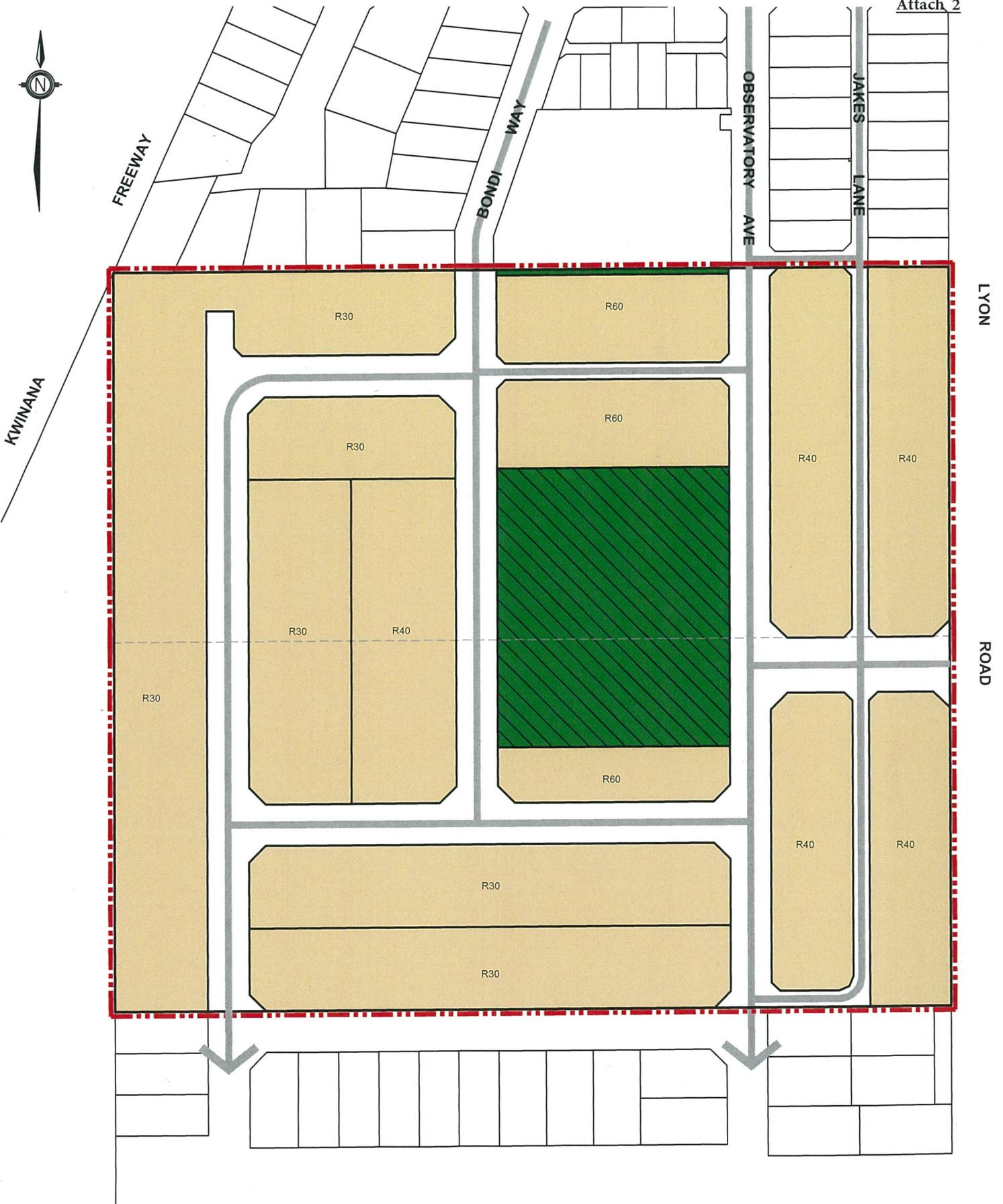
**Lot 12 & 13 Lyon Road, Aubin Grove**  
 Figure 1 - Location Plan

**RPS**  
 RPS Australia East Pty. Ltd  
 ABN 44 140 202 762  
 PO Box 405 Subiaco WA 6004  
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PRELIMINARY FOR DISCUSSION PURPOSES ONLY

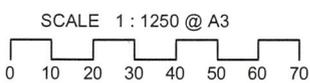
0 200 400 600

Scale 1:6000@A4 | Date January, 2014 | Project No 116057\_b\_001a



### Legend

-  Local Structure Plan Boundary
-  PARKS AND RECREATION
-  RESIDENTIAL



DATE : 26 June 2014 DWG NAME : 116057-2-002G Structure

## Local Structure Plan

### Lots 12 & 13 Lyon Road, Aubin Grove

File No. 110/100

**SCHEDULE OF SUBMISSIONS  
STRUCTURE PLAN PROPOSAL – LOTS 12 & 13 LYON ROAD, AUBIN GROVE**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1.	Department of Fire and Emergency Services 20 Stockton Bend Cockburn Central WA 6164	<p><b>No Objection</b></p> <p>On behalf of DFES, I have "No Comment" on the proposed local structure plans for,</p> <ul style="list-style-type: none"> <li>• Lot 6 Hammond Rd, Success</li> <li>• Lots 12 &amp; 13 Lyon Rd, Aubin Grove</li> <li>• Cockburn Central Activity Centre Plan</li> </ul>	Noted.
2.	Department of Parks and Wildlife Locked Bag 104 Bentley Delivery Centre WA 6983	<p><b>No Objection</b></p> <p>Parks and Wildlife has no comments on this referral.</p>	Noted.
3.	Main Roads Western Australia PO Box 6202 EAST PERTH WA 6892	<p><b>Support</b></p> <p>Thank you for your letter dated the 22nd of August, 2014 requesting Main Roads comment on the above proposed Local Structure Plan.</p> <p>Main Roads supports the proposed LSP and provides the following comments:</p> <p>1. The recommendations as set out in the Conclusion of the Acoustic Assessment undertaken by Lloyd George Acoustics (ref: 14052791-01) are supported by Main Roads, assuming they are implemented in full by the proponent and reflected on any future Detailed Area Plan.</p>	Noted. The City supports the implementation of the recommendations of the Acoustic Report at time of subdivision and Detailed Area Plan stages.
4.	The Department of Education 151 Royal Street East Perth WA 6004	<p><b>No Objection</b></p> <p>Thank you for your letter dated 22 August 2014 regarding the Proposed Local Structure Plan - Lots 12 &amp; 13 Lyon Road, Aubin Grove.</p> <p>The Department of Education has reviewed the proposal and has no objection to this proposed Local Structure Plan.</p>	Noted.

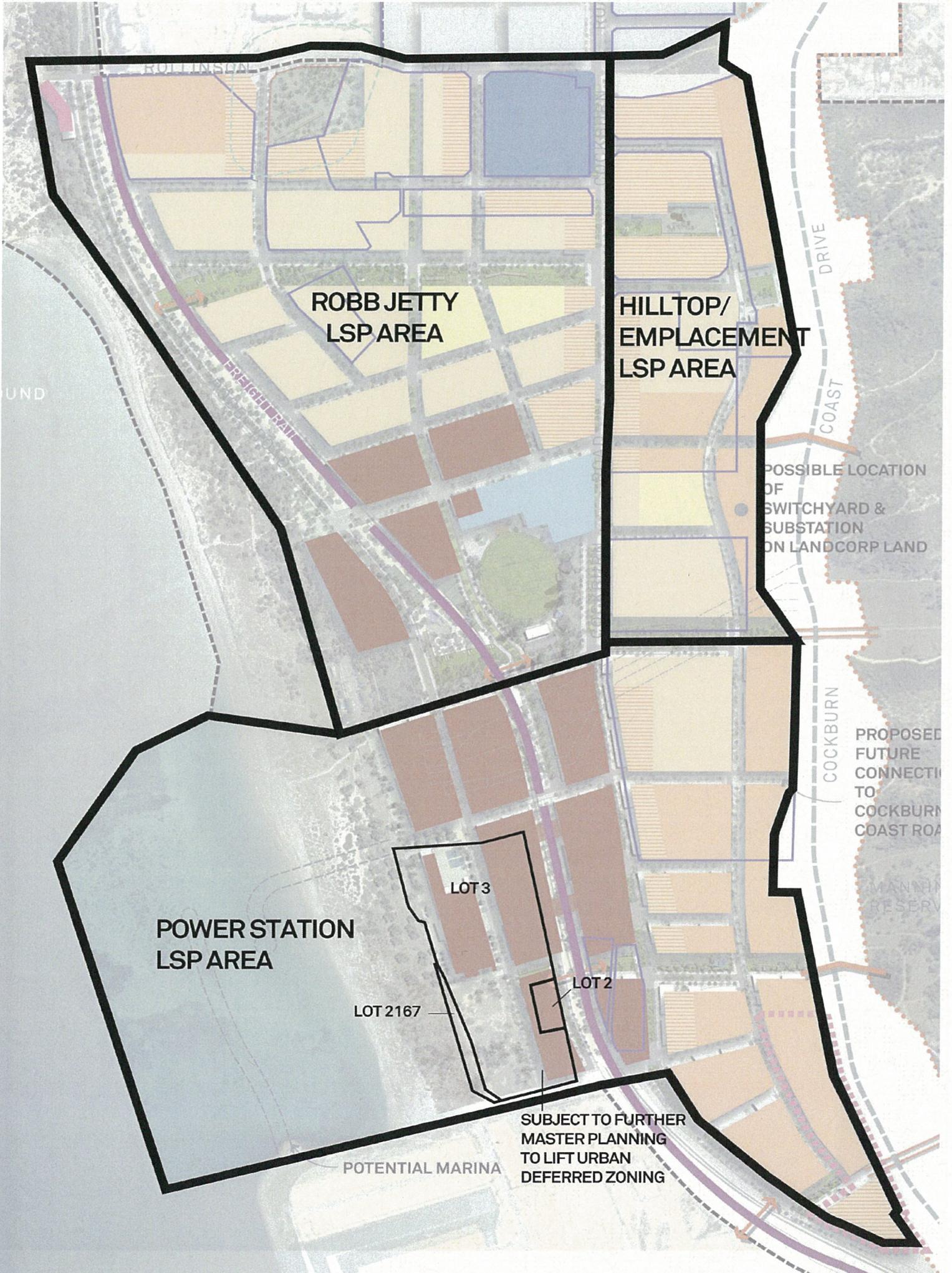
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
5.	The Department of Water PO Box 332 Mandurah WA 6210	<p>Thank you for the Lots 12 &amp; 13 Lyon Road, Aubin Grove - Local Water Management Strategy (LWMS) Revision 2, August 2014 received on 29 August 2014. The Department of Water (DoW) has reviewed the LWMS and provides the following comment:</p> <p>Section 5.2: Stormwater Management</p> <ul style="list-style-type: none"> <li>Figure 9: Drainage Plan and Retention Basin Details, indicates that stormwater runoff may be discharged off-site. Section should indicate the volume of stormwater discharged to off-site catchments from ARI events, confirm that the receiving drainage infrastructure is designed to convey this volume and indicate City of Cockburn support for this design.</li> </ul> <p>It is recommended that the LWMS be revised incorporating the above points and any others recommended by the City of Cockburn. In the revised LWMS please identify where and how comments have been addressed.</p>	<p>Noted and Supported.</p> <p>The recommended technical change to the LWMS forms part of the Council resolution.</p>
6.	Department of Aboriginal Affairs (DAA) PO Box 315 East Perth WA 6892	<p><b>No Objections</b></p> <p>Thank you for the opportunity to provide comment on the above proposed structure plan. The Department of Aboriginal Affairs (DAA) has reviewed the Aboriginal Heritage Sites Register and can inform you that there are no known Aboriginal Heritage Sites or Other Heritage Places, located on the land. As such, there are no known Aboriginal heritage reasons why the proposed works cannot go ahead.</p> <p>DAA recommends that the planning approval include reference to the Cultural Heritage Due Diligence Guidelines (the Guidelines) so that the prospective developers are aware of their obligations with regards to Aboriginal heritage. The Guidelines can be found electronically at: <a href="http://www.daa.wa.gov.au/Documents/Reports/Publications/DDG.pdf">www.daa.wa.gov.au/Documents/Reports/Publications/DDG.pdf</a></p>	<p>Noted.</p>
7.	City of Cockburn Landowner	<p><b>Support</b></p> <p>I am supportive of the proposed structure plan, however; I think that the location of the proposed Parks and Recreation should be modified so that more of the good to very good condition remnant vegetation can be kept.</p>	<p>Noted.</p> <p>The City and the proponent contemplated a number of design locations for the Public Open Space. When considering the various factors</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>(bushfire, drainage, level of retaining, respect for existing residences etc.) at play in the development of the land it was determined that the proposed location was the most preferable.</p> <p>It is expected that some remanent vegetation will be retained within the central area of Public Open Space</p>
8	City of Cockburn Landowner	<p>My name is Pat and I live at 46 Twilight Mews Aubin Grove. I understand the land behind us is being developed as we have received a structure plan of lots 12 and 13 Lyon road Aubin Grove.</p> <p>I have a few concerns about the development. I read that my back fence height will finish at a height above 3.5 metres. I feel that this is too high and will be an eye saw at the back of my property. When I bought this property one of the contributing factors was the beautiful bushland that backed onto my fence. It is so wonderful to hear and see the birds each day.</p> <p>To hear that this is all going to be destroyed breaks my heart, but to hear that the fence is going to be so high makes me worried that aesthetically my backyard is going to feel like a box. This was not what I had in mind when I bought the property. If there is any way to reduce the fence height to under 3 metres (including the retaining wall) it would be much appreciated. Could I also ask that the fence colour matches the current colour of my boundary fence?</p>	<p><b>LATE SUBMISSION</b></p> <p><b>Comments and Concerns Noted</b></p> <p>The City is cognisant of the concerns raised by Twilight Mews residents to the proposed height of the retaining walls along their rear boundaries.</p> <p>Although the finished lot levels and retaining wall heights are not traditionally matters addressed at Structure Plan stage the City has communicated the concerns to the applicant.</p> <p>The applicant has written to the City and confirmed their intention to work with all stakeholders to lessen the any amenity impact on existing properties.</p>
9	City of Cockburn Landowner	<p>Our names are John and Malissa Kelly and we live at 19 Twilight Me-w5 Aubin Grove. We have been at this address for 6 years now. We were the very first house to move into the Gracelands Estate.</p> <p>We received a letter explaining that the land behind us is being developed. We received the structure plan for lots 12 and 13 Lyon road Aubin Grove. We are disappointed after all this time of having the beautiful bushland behind us, to hear that it is all going to be developed. Therefore we hope that you take our concerns to the next council meeting:</p> <p>Firstly we are very concerned about the height proposed for the finished</p>	<p><b>LATE SUBMISSION</b></p> <p><b>Comments and Concerns Noted</b></p> <p>The City is cognisant of the concerns raised by Twilight Mews residents to the proposed height of the retaining walls along their rear boundaries.</p> <p>Although the finished lot levels and retaining wall heights are not traditionally matters</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>fence that will run along our back boundary. To hear that part of our fence it is going to be approximately 3.4 meters high and then drop down to 3 metres high is worrying us.</p> <p>This was not in any future plans when we bought the land and developed it. We have just completed the landscaping and pool in our backyard and to now think that our backyard will consist of such a high fence is disappointing. It is going to be terrible to have to look at each day! Could we please propose that the final height of the fence, including the retaining wall, be no greater than 2.2 meters. We also want the colour of the fence to match the existing colour, which also matches our neighbour's fence colour.</p> <p>Secondly, as council regulations state, we shared all the costs of our fence with our neighbours. So we would like to know who is going to reimburse us for our half of the back fence that we originally installed?</p> <p>Thirdly, we have a pool installed in the top left corner of our property. We are very concerned about the effects that the earthworks is going to have on our pool and the paving. We are worried that the compaction of the new development will cause structural damage to our pool and cause cracks in our paving. We would like to know if the developers would pay for an independent contractor to inspect our premises, including the pool and anything structural during the time of construction.</p> <p>We also have a limestone garden bed which includes a stone water feature that is connected to our reticulation and as this is close to the boundary fence, we would like the developer to ensure that there will be no damage to the property or to the plants.</p> <p>We would like it in writing that if they damage anything, they are responsible to replace it or fix it.</p> <p>Finally, we have a very large dog (Alaskan Malamute). We both work full time and therefore when the fence is removed, we would like to know how they are going to ensure security of our property as well as keep our dog inside our property.</p> <p>We want to be notified well in advance what day the fence will be taken down and if needed, for the developers to pay kennel costs. We are also</p>	<p>addressed at Structure Plan stage the City has communicated the concerns to the applicant.</p> <p>The applicant has written to the City and confirmed their intention to work with all stakeholders to lessen the any amenity impact on existing properties.</p> <p>It is expected that the developer will contact the relevant landowners prior to any works that may impact upon their land or property. The City cannot comment on arrangement relating to registered dogs on the premises during any future construction of the Structure Plan area.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
10	City of Cockburn Landowner	<p>worried about security to our property as once the fence is taken down, our garage can be accessed from the inside. We would like to know if the temporary fencing is going to be covered.</p> <p>Considering that we are the longest standing household in the estate, we hope you will consider our concerns and proposals.</p> <p>My name is Nathan Hodder and I live at 21 twilight mews Aubin Grove. I understand the land behind us is being developed as we have received a structure plan for lots 12 and 13 Lyon road Aubin Grove.</p> <p>I have a few concerns about this and would like to express these as follows.</p> <ol style="list-style-type: none"> <li>1. I am not happy about the height of the fence retaining wall as there will be a 1.6m retaining wall built where the existing fence is and then a 1.8m colour bond fence on top of that which will give a combined height of 3.4m high which will feel very enclosed and boxed in. We will walk out and stare at that every time we go outside. Could I please propose that the height be reduced on the retaining wall to a manageable height of somewhere near 600 high which will give us a combined height of 2.6.</li> </ol> <p>We have had the beautiful bushland behind us for so long now as an open space and it is disappointing that it is all going to be demolished.</p> <ol style="list-style-type: none"> <li>2. I would like to make sure the colour of the fence matches the existing fence that surrounds our property. I also want to know who is going to reimburse us for half of our existing back fence, as stated in council regulations. I would like the developers to reimburse me for half of the existing fence.</li> <li>3. What will happen with security when the fence gets built? I am concerned that a temporary fence would allow people to see into my yard which would not be private. I also have two young children to consider. Please can you ask for any temporary fencing to be covered securely?</li> </ol> <p>Hi I would like to my details kept confidential. I have spoken to Christopher Hossen on the phone regarding this proposal. I would like to add due to the close location to our estate that any earth works have a well-developed dust management plan in place. The prevailing winds would blow and dust and sand straight over into our estate.</p>	<p><b>LATE SUBMISSION</b></p> <p><b>Comments and Concerns Noted</b></p> <p>The City is cognisant of the concerns raised by Twilight Mews residents to the proposed height of the retaining walls along their rear boundaries.</p> <p>Although the finished lot levels and retaining wall heights are not traditionally matters addressed at Structure Plan stage the City has communicated the concerns to the applicant.</p> <p>The applicant has written to the City and confirmed their intention to work with all stakeholders to lessen the any amenity impact on existing properties.</p>
11	City of Cockburn Landowner		<p><b>LATE SUBMISSION</b></p> <p><b>Support</b></p> <p>At time of clearance of subdivision any proposal for bulk earthworks will be required to be</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
12	City of Cockburn Landowner	<p>I wish to make the following submission as a property owner that abuts the proposed development.</p> <p>In principle I support the Structure plan for the subdivision and development of Lots 12 &amp; 13 Lyon Road with the following concerns which I hope can be address in the final approval.</p> <ol style="list-style-type: none"> <li>1. As a large amount of fill is required along my boundary I require that any damage to the fence or my house that is cause by the placement and compaction of the fill is repaired to my satisfaction and dust and debris is cleaned and removed from my property.</li> <li>2. I am also concerned at the amount of additional traffic that will be generated in Jakes Lane which is proposed to be lengthened by approximately 2.8 times its current length. SHAWMAG's estimated traffic volumes support my concerns when you consider that Jakes Lane does not have footpaths and road pavement is much narrower in comparison to Observatory Avenue.</li> </ol> <p>I would like the Structure Plan to be reviewed to achieve shorter sections of Jakes Lane accessing Lyon Road and Observatory Avenue, this would result in a safer residential area.</p>	<p>accompanied by a dust management plan and approved by the City.</p> <p><b>LATE SUBMISSION</b></p> <p><b>Support with concern</b></p> <ol style="list-style-type: none"> <li>1. At time of clearance of subdivision any proposal for bulk earthworks will be required to be accompanied by a dust management plan and approved by the City.</li> <li>2. The proposed traffic volumes proposed on the extended Jakes Lane are within the normal expected numbers for a laneway. This matter has been assessed by the City's traffic engineer and deemed appropriate. The lengthening of Jakes Lane will however require traffic calming, such as speed bumps, to be installed. These will be required at time of construction drawing approval.</li> </ol>



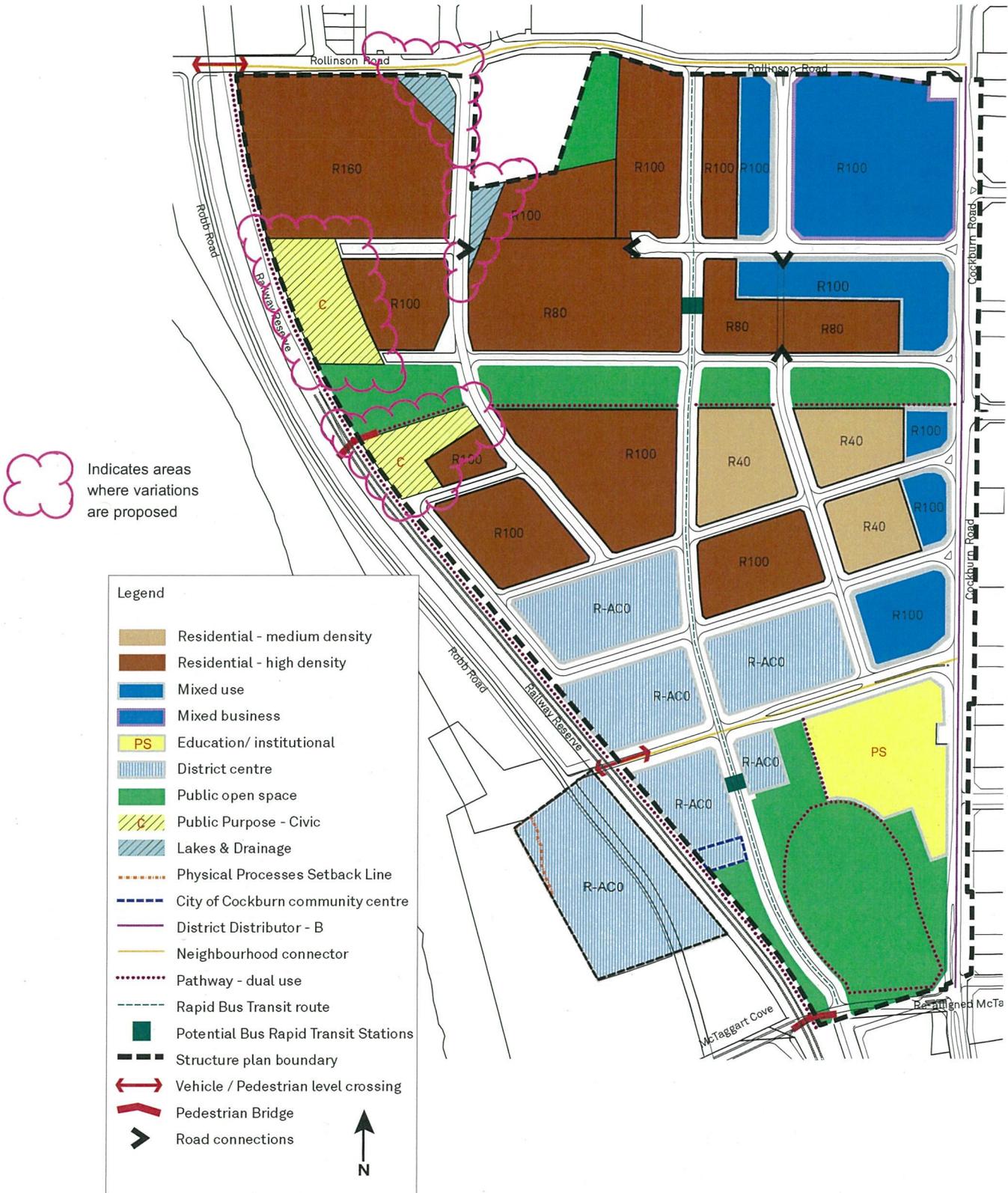


Figure 01\_Local Structure Plan Map




**City of Cockburn**  
G.I.S Services Department

PRINTED ON:  
Wednesday, 24 September 2014

SCALE = 1:1000

DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.

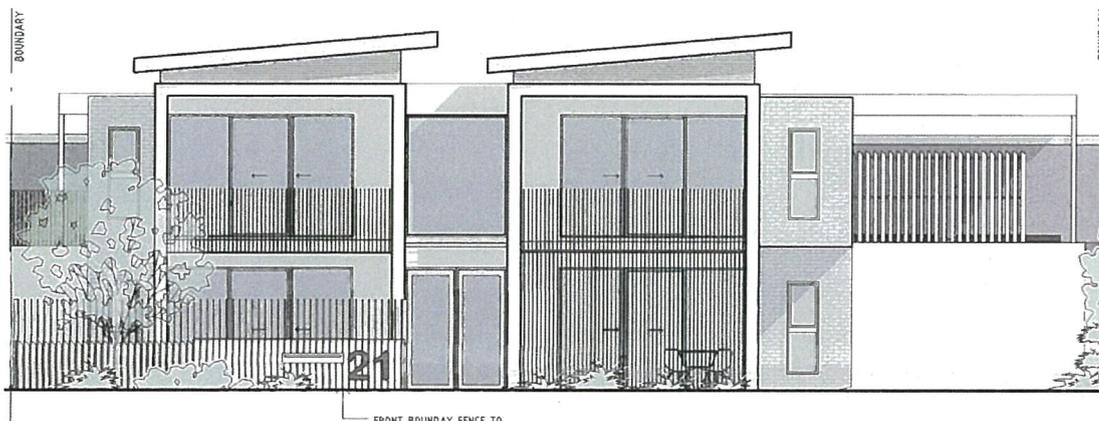


# COOGEE RESIDENTIAL DEVELOPEMENT 21 LUPIN WAY, COOGEE

PLANNING APPROVAL DRAWINGS			
SHEET No.	SHEET NAME	DATE	REVISION
A0.0	CONTENT PAGE	15.08.14	G
A1.0	SURVEY PLAN	15.08.14	G
A1.1	SITE PLAN	15.08.14	G
A2.0	GROUND FLOOR PLAN	15.08.14	G
A2.1	FIRST FLOOR PLAN	15.08.14	G
A3.0	ELEVATIONS	15.08.14	G
A3.1	ELEVATIONS	15.08.14	G



R30 SITE AREA: 731.00 m <sup>2</sup>	1	2	3	4	5	6		7		
	DESCRIPTION	MAXIMUM PLOT RATIO	MINIMUM OPEN SPACE (% OF SITE)	MINIMUM PRIMARY STREET BOUNDARY SETBACK	SECONDARY STREET SETBACK	MAXIMUM HEIGHT (m)			MAXIMUM HEIGHT OF WALLS BUILT UP TO BOUNDARY (m-)	
						TOP OF EXTERNAL WALL	TOP OF EXTERNAL WALL (CONCEALED ROOF)	TOP OF PITCHED ROOF	MAXIMUM HEIGHT	AVERAGE
	R-CODES REQUIREMENTS	0.5	45%	4.0 m	15 m	6.0 m	7.0 m	9.0 m	3.5 m	3.0 m
SITE POTENTIAL	365.50 m <sup>2</sup>	328.95 m <sup>2</sup>	-	-	-	-	-	-	-	
CURRENT DESIGN	400.33 m <sup>2</sup>	504.00 m <sup>2</sup>	3.0 m	-	4.9m	4.6m	7.0m	-	-	
AREA (m <sup>2</sup> )	FLOOR	UNIT NO.	UNIT AREA (INC. WALLS)	NLA	BALCONY	COURTYARD	STORE	TOTAL		
	GROUND	UNIT 1	101.73 m <sup>2</sup>	90.77 m <sup>2</sup>		14.09 m <sup>2</sup>	4.16 m <sup>2</sup>	119.98 m <sup>2</sup>		
		UNIT 2	99.11 m <sup>2</sup>	88.26 m <sup>2</sup>		17.28 m <sup>2</sup>	4.87 m <sup>2</sup>	121.27 m <sup>2</sup>		
		UNIT 3	99.38 m <sup>2</sup>	79.99 m <sup>2</sup>	14.09 m <sup>2</sup>		4.39 m <sup>2</sup>	117.86 m <sup>2</sup>		
		UNIT 4	100.10 m <sup>2</sup>	86.45 m <sup>2</sup>	17.86 m <sup>2</sup>		4.28 m <sup>2</sup>	122.24 m <sup>2</sup>		



FRONT BOUNDARY FENCE TO COMPLY WITH LOCAL AUTHORITIES REQUIREMENTS

**1** STREET ELEVATION  
A1.1 SCALE 1 : 100

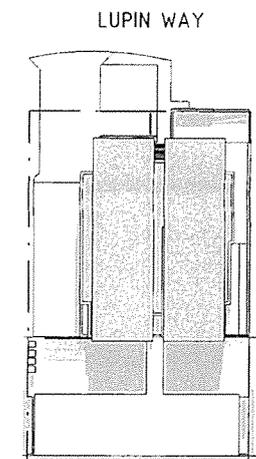
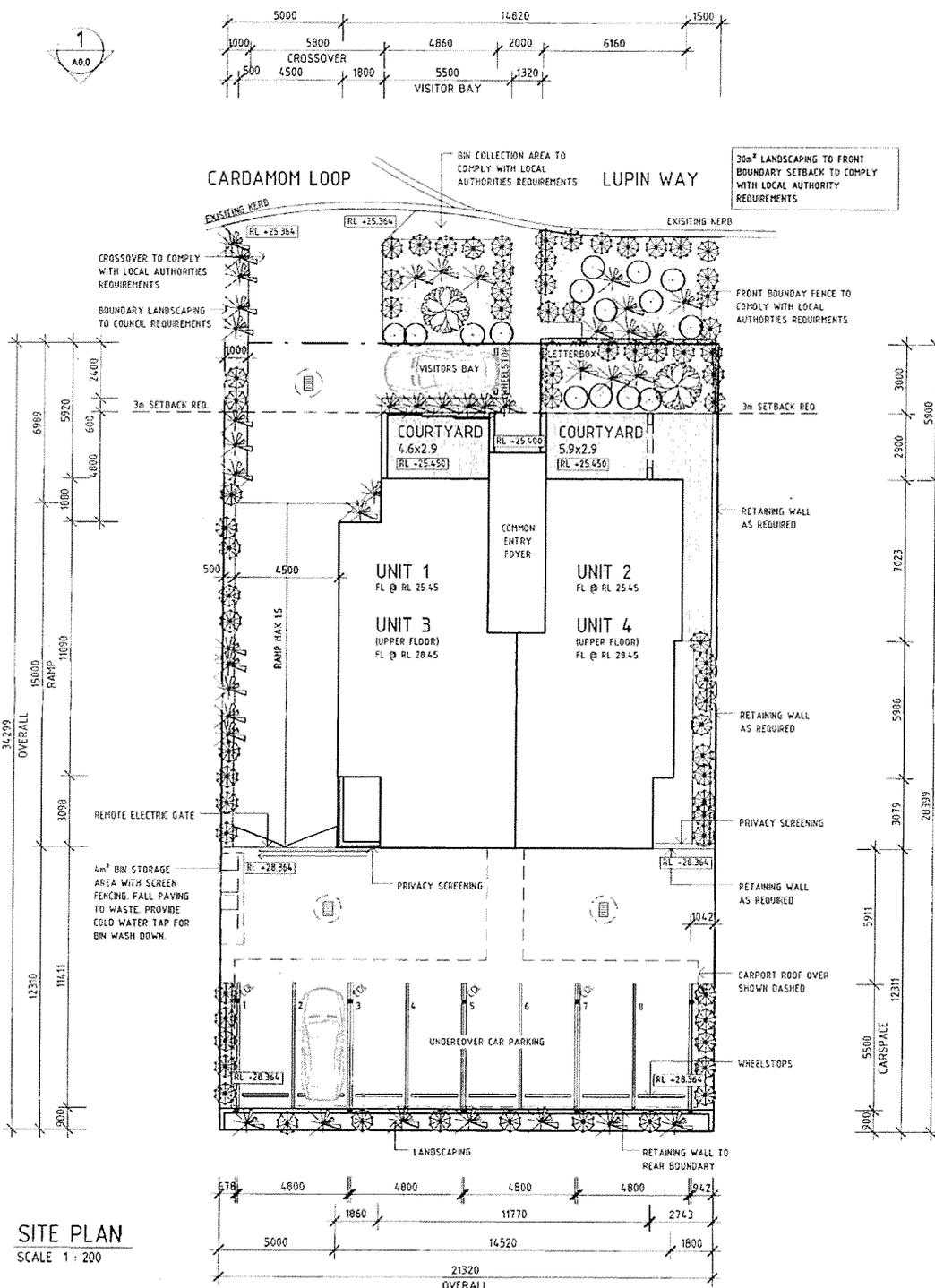
G	BIN STORAGE AREA	15.08.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ.	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.07.14
C	OPTION 2 - CLIENT COMMENT	03.07.14
B	CLIENT COMMENT OPTION 2	01.07.14
A	ISSUED FOR PRE-PLANNING APPROVAL	18.06.14

Checked	J JOSEPHS	CONTENT PAGE	1 : 100
Drawing Coordinated	J MARTIN		
Drawing Director Approval	J JOSEPHS	<b>PLANNING APPROVAL DWG</b>	
Project Description	COOGEE RESIDENTIAL DEVELOPEMENT 21 LUPIN WAY, COOGEE		
Project Number	106742	Drawing Number	A0.0
		Issue	G



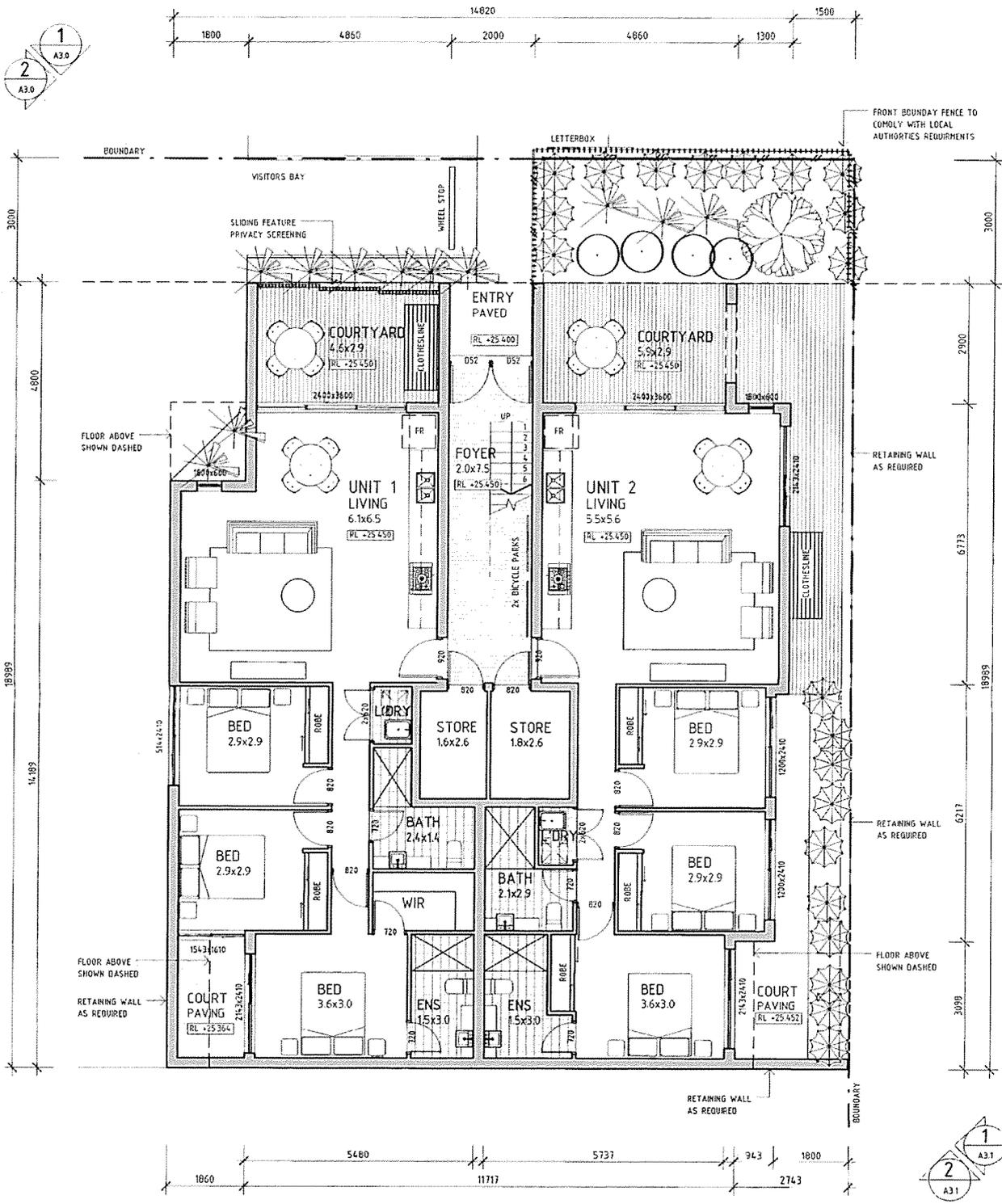
**LEGEND**

- PROPOSED SOIL AND PLANTING AREAS - 100mm MULCH TOP COVER
- PROPOSED PAVING - TYPE 1
- PROPOSED PAVING - TYPE 2
- PROPOSED TIMBER DECKING
- PROPOSED GRASS
- PROPOSED GRAVEL
- WA RED FLOWERING GUM - 30t
- ANGOZANTHOS AUTUMN BLAZE - 10t
- WESTRINGIA BULLI COASTIER CREEPER - 5t
- LOMANDRA SIRE'S WINGARRA - 5t



G	BIN STORAGE AREA	15.08.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.07.14
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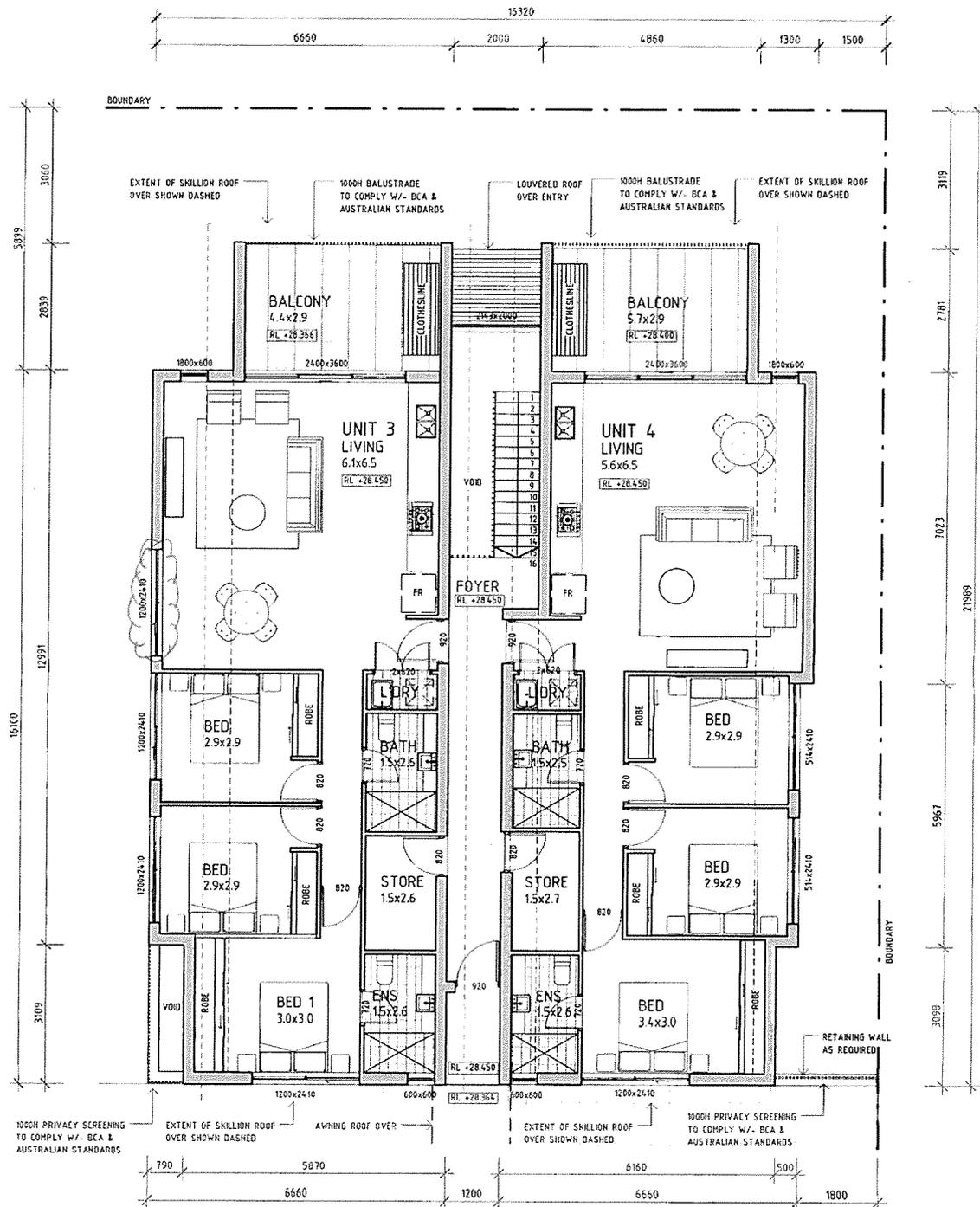
Checked	JJOSEPHS	SITE PLAN	As indicated
Drawing Coordinated	JMARTIN		
Drawing Director Approval	JJOSEPHS		
Project Description		<b>PLANNING APPROVAL DWG</b>	
COOGEE RESIDENTIAL DEVELOPMENT		Drawing Number	Issue
21 LUPIN WAY, COOGEE		A1.1	G
Project Number 106742			



**UNIT 1 & 2 - GROUND FLOOR PLAN**  
 SCALE 1:100

G	BIN STORAGE AREA	15.08.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.07.14
C	OPTION 2 - CLIENT COMMENT	03.07.14
B	CLIENT COMMENT OPTION 2	01.07.14
A	ISSUED FOR PRE-PLANNING APPROVAL	18.06.14

Checked	JJOSEPHS	GROUND FLOOR PLAN	1:100
Drawing Coordinated	JMARTIN		
Drawing Director Approval	JJOSEPHS		
Project Description		<b>PLANNING APPROVAL DWG</b>	
COOGEE RESIDENTIAL DEVELOPMENT		Drawing Number	ISSUE
21 LUPIN WAY, COOGEE		A2.0	G
Project Number		106742	

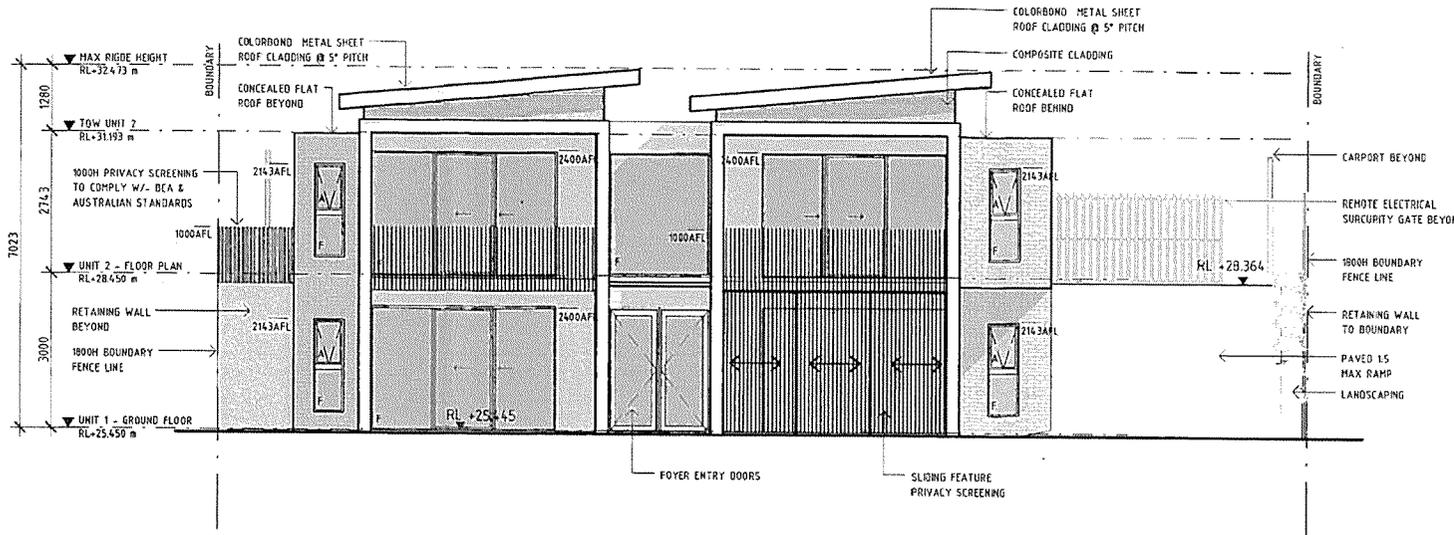


**UNIT 3 & 4 - FIRST FLOOR PLAN**  
 SCALE 1 : 100



G	BIN STORAGE AREA	15.08.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.07.14
C	OPTION 2 - CLIENT COMMENT	03.07.14
B	CLIENT COMMENT OPTION 2	01.07.14
A	ISSUED FOR PRE-PLANNING APPROVAL	18.06.14

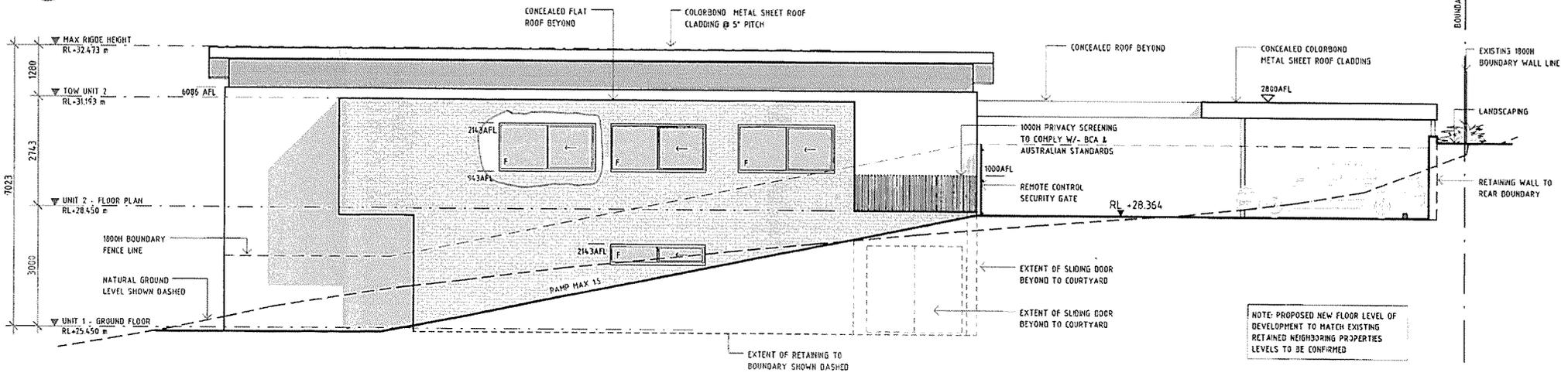
Checked	J JOSEPHS	FIRST FLOOR PLAN	1 : 100
Drawing Coordinated	J MARTIN		
Drawing Director Approval	J JOSEPHS		
Project Description		<b>PLANNING APPROVAL DWG</b>	Issue
COOGEE RESIDENTIAL DEVELOPMENT			
21 LUPIN WAY, COOGEE			
Project Number	106742	Drawing Number	A2.1
		Issue	G



**LEGEND**

-  DULUX ACRATEX TEXTURED RENDER  
COAT COLOUR DULUX VIVID WHITE
-  COMPOSITE CLADDING
-  AUSTRAL ELEMENTS GRAPHITE OR  
SIMILAR FACE BRICK
-  GLAZING CLEAR
-  GLAZING OPAQUE

**1 NORTH ELEVATION**  
SCALE 1 : 100



NOTE: PROPOSED NEW FLOOR LEVEL OF DEVELOPMENT TO MATCH EXISTING RETAINED NEIGHBORING PROPERTIES LEVELS TO BE CONFIRMED

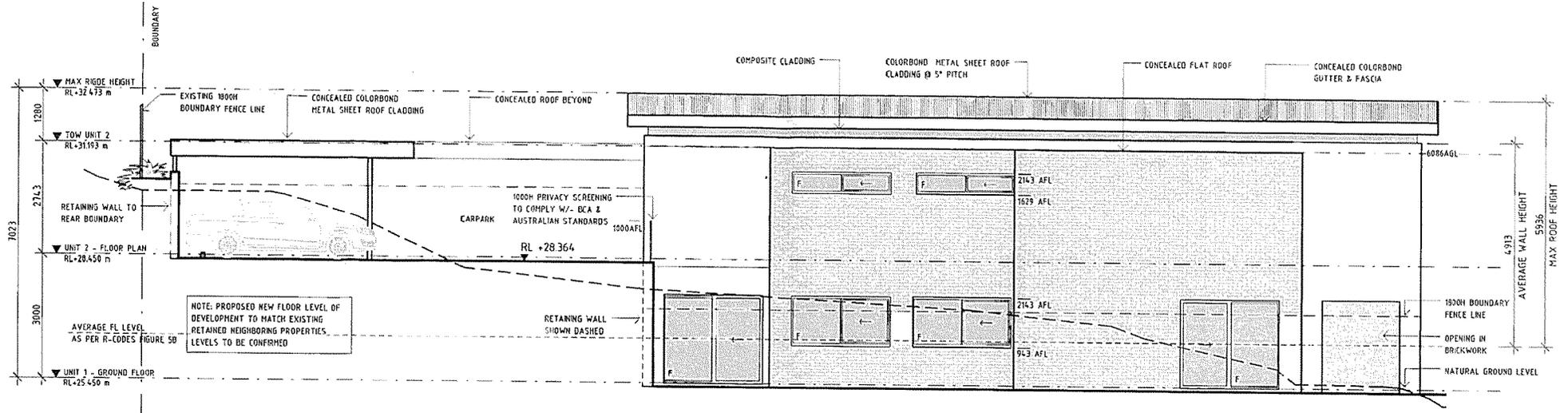
**2 WEST ELEVATION**  
SCALE 1 : 100

G	BIN STORAGE AREA	15.09.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.07.14
C	OPTION 2 - CLIENT COMMENT	03.07.14
B	CLIENT COMMENT OPTION 2	01.07.14
A	ISSUED FOR PRE-PLANNING APPROVAL	18.06.14
Issue	Revision Description	Date

Checked	J.JOSEPHS	ELEVATIONS	1 : 100
Drawing Coordinated	J.MARTIN	<b>PLANNING APPROVAL DWG</b>	
Drawing Director Approval	J.JOSEPHS		
Project Description		Drawing Number	Issue
COOGEE RESIDENTIAL DEVELOPEMENT		A3.0	G
21 LUPIN WAY, COOGEE			
Project Number 106742			

Attach 7

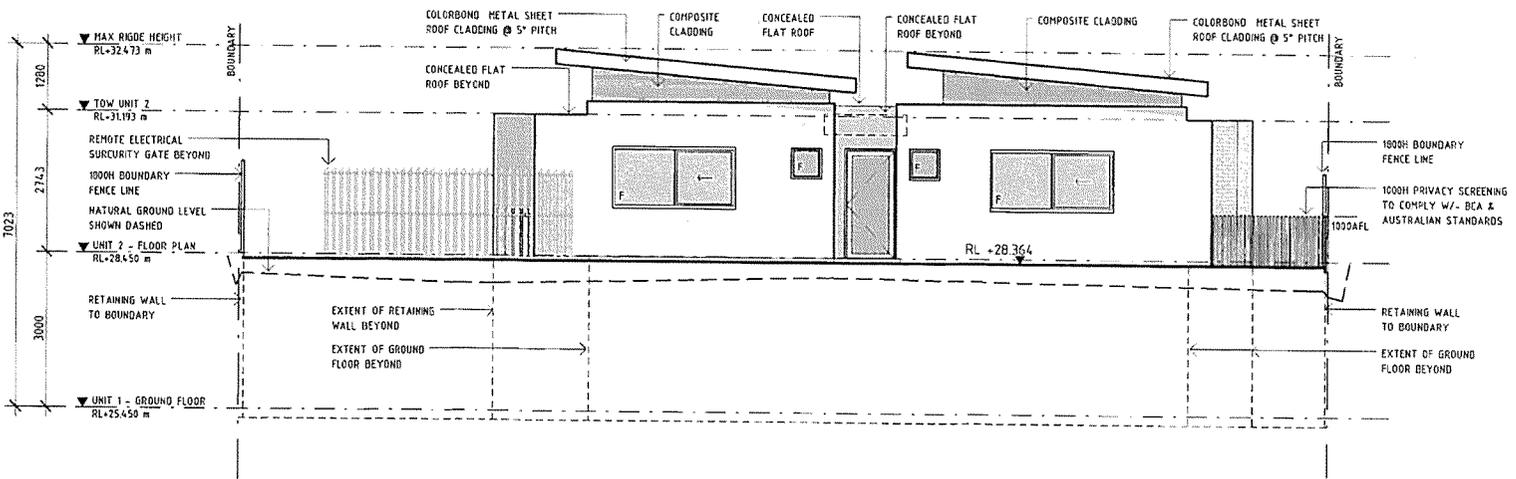
PITCHED ROOF 9m MAX



**1 EAST ELEVATION**  
 A2.0 SCALE 1 : 100

**LEGEND**

- DULUX ACRATEX TEXTURED RENDER  
COAT COLOUR DULUX VIVID WHITE
- COMPOSITE CLADDING
- AUSTRAL ELEMENT'S GRAPHITE OR  
SIMILAR FACE BRICK
- GLAZING CLEAR
- GLAZING OPAQUE



**2 SOUTH ELEVATION**  
 A2.0 SCALE 1 : 100

G	BIN STORAGE AREA	15.08.14
F	PLANNING APPROVAL	15.07.14
E	ELEVATION OPTION 2 - CLIENT REQ	14.07.14
D	CLIENT COMMENT - ELEVATIONS	10.01.14
C	OPTION 2 - CLIENT COMMENT	03.07.14
B	CLIENT COMMENT OPTION 2	01.07.14
A	ISSUED FOR PRE-PLANNING APPROVAL	18.06.14
Issue	Revision Description	Date

Checked	J.JOSEPHS	ELEVATIONS	1 : 100
Drawing Coordinated	J.MARTIN	<b>PLANNING APPROVAL DWG</b>	
Drawing Director Approval	J.JOSEPHS		
Project Description		Drawing Number	Issue
COOGEE RESIDENTIAL DEVELOPMENT		A3.1	G
21 LUPIN WAY, COOGEE			
Project Number 106742			

Attach 8

## MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF080002	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	4/08/2014	302,771.00
EF080003	10305	<b>CHILD SUPPORT AGENCY</b> PAYROLL DEDUCTIONS	4/08/2014	11,386.33
EF080004	10405	<b>COOLBELLUP COMMUNITY ASSOCIATION</b> COUNCIL CONTRIBUTIONS / GRANTS	4/08/2014	214.50
EF080005	10888	<b>LJ CATERERS</b> CATERING SERVICES	4/08/2014	4,502.85
EF080006	10938	<b>MAXWELL ROBINSON &amp; PHELPS</b> PEST & WEED MANAGEMENT	4/08/2014	18,537.00
EF080007	12575	<b>ST JEROMES SENI</b> BUS HIRE SUBSIDY	4/08/2014	60.00
EF080008	15402	<b>ZURICH AUSTRALIA</b> MOTOR VEHICLE INSURANCE CLAIM	4/08/2014	1,000.00
EF080009	16058	<b>SHOP-A-DOCKET PTY LTD</b> ADVERTISING SERVICES	4/08/2014	1,628.00
EF080010	18173	<b>COOGEE BEACH CARAVAN RESORT SOCIAL CLUB</b> BUS HIRE SUBSIDY	4/08/2014	100.00
EF080011	18553	<b>SELECTUS PTY LTD</b> PAYROLL DEDUCTIONS	4/08/2014	10,947.52
EF080012	21403	<b>ROBERTA BUNCE</b> COMMUNITY CARE VOLUNTEER REIMBURSEMENTS	4/08/2014	91.35
EF080013	25474	<b>ROBERT MARTIN</b> VOLUNTEER REIMBURSEMENT	4/08/2014	27.00
EF080014	25987	<b>TOYOTA FLEET MANAGEMENT</b> PAYROLL DEDUCTIONS - NOVATED LEASE	4/08/2014	567.62
EF080015	26104	<b>MG &amp; GJ WHITE</b> BIRD BATH REBATE	4/08/2014	50.00
EF080016	10152	<b>AUST SERVICES UNION</b> PAYROLL DEDUCTIONS	8/08/2014	5,106.80
EF080017	10733	<b>HOSPITAL BENEFIT FUND</b> PAYROLL DEDUCTIONS	8/08/2014	2,652.35
EF080018	11001	<b>MUNICIPAL EMPLOYEES UNION</b> PAYROLL DEDUCTIONS	8/08/2014	950.60
EF080019	11856	<b>WA LOCAL GOVERNMENT SUPER PLAN</b> PAYROLL DEDUCTIONS	8/08/2014	509,616.47
EF080020	11857	<b>CHAMPAGNE SOCIAL CLUB</b> PAYROLL DEDUCTIONS	8/08/2014	1,673.20
EF080021	11859	<b>STAFF SOCIAL CLUB</b> PAYROLL DEDUCTIONS	8/08/2014	78.10
EF080022	11860	<b>45S CLUB</b> PAYROLL DEDUCTIONS	8/08/2014	70.00
EF080023	18005	<b>COLONIAL FIRST STATE</b> PAYROLL DEDUCTIONS	8/08/2014	596.91
EF080024	18247	<b>ELLIOTT SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	367.76
EF080025	18432	<b>HESTA SUPER FUND</b> PAYROLL DEDUCTIONS	8/08/2014	6,613.03
EF080026	18718	<b>FIRST STATE SUPER</b> PAYROLL DEDUCTIONS	8/08/2014	1,596.18
EF080027	19010	<b>SUMMIT PERSONAL SUPER PLAN PASQUALE CARRELLO</b> PAYROLL DEDUCTIONS	8/08/2014	608.16
EF080028	19193	<b>REST SUPERANNUATION</b> PAYROLL DEDUCTIONS	8/08/2014	71.01

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080029	19726	<b>HEALTH INSURANCE FUND OF WA</b> PAYROLL DEDUCTIONS	8/08/2014	4,759.80
EF080030	19727	<b>MTAA SUPER FUND</b> PAYROLL DEDUCTIONS	8/08/2014	593.53
EF080031	19997	<b>AUSTRALIANSUPER</b> PAYROLL DEDUCTIONS	8/08/2014	22,876.29
EF080032	20056	<b>CBUS</b> PAYROLL DEDUCTIONS	8/08/2014	2,802.09
EF080033	20217	<b>DOWNING SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	4,128.15
EF080034	20300	<b>CATHOLIC SUPER &amp; RETIREMENT FUND</b> PAYROLL DEDUCTIONS	8/08/2014	1,052.25
EF080035	20406	<b>HOSTPLUS SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	842.95
EF080036	21299	<b>DUFFIELD SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	131.84
EF080037	21365	<b>ING LIFE - ONEANSWER PERSONAL SUPER</b> PAYROLL DEDUCTIONS	8/08/2014	168.76
EF080038	21921	<b>MAURICIO FAMILY</b> PAYROLL DEDUCTIONS	8/08/2014	2,808.36
EF080039	21996	<b>ANZ ONEANSWER PERSONAL SUPER</b> PAYROLL DEDUCTIONS	8/08/2014	409.80
EF080040	22067	<b>STEPHENS SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	435.11
EF080041	22901	<b>FONTANA SUPER PLAN</b> PAYROLL DEDUCTIONS	8/08/2014	1,894.02
EF080042	23695	<b>NETWEALTH INVESTMENT &amp; SUPERANNUATION</b> PAYROLL DEDUCTIONS	8/08/2014	1,397.07
EF080043	23993	<b>ONEPATH LIFE LIMITED</b> PAYROLL DEDUCTIONS	8/08/2014	747.30
EF080044	24620	<b>E &amp; B PINTO SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	8/08/2014	1,680.78
EF080045	24642	<b>TWUSUPER</b> PAYROLL DEDUCTIONS	8/08/2014	1,329.88
EF080046	24813	<b>KINETIC SUPER</b> PAYROLL DEDUCTIONS	8/08/2014	931.76
EF080047	25043	<b>COLONIAL FIRST STATE - KERRY MARGARET ROBERTS</b> PAYROLL DEDUCTIONS	8/08/2014	303.66
EF080048	25051	<b>ANZ SMART CHOICE SUPER (ONEPATH MASTERFUND) ROAN BARRETT</b> PAYROLL DEDUCTIONS	8/08/2014	1,791.66
EF080049	25394	<b>CONCEPT ONE THE</b> PAYROLL DEDUCTIONS	8/08/2014	57.27
EF080050	25495	<b>ONEPATH CUSTODI</b> PAYROLL DEDUCTIONS	8/08/2014	689.88
EF080051	25538	<b>NORTH PERSONAL SUPERANNUATION PLAN</b> PAYROLL DEDUCTIONS	8/08/2014	217.62
EF080052	25590	<b>FIRST CHOICE WHOLESALE PERSONAL SUPER - MATHEW SAPSWORTH</b> PAYROLL DEDUCTIONS	8/08/2014	1,075.74
EF080053	25649	<b>COMMONWEALTH BANK GROUP SUPER</b> PAYROLL DEDUCTIONS	8/08/2014	634.86
EF080054	25950	<b>BT LIFETIME SUPER - EMPLOYER PLAN</b> PAYROLL DEDUCTIONS	8/08/2014	841.53
EF080055	25956	<b>COLONIAL FIRST STATE - KHRISTINE ANNE RYAN</b> PAYROLL DEDUCTIONS	8/08/2014	15.59

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF080056	25963	<b>ONEPATH SUPER - RACHEL PLEASANT</b> PAYROLL DEDUCTIONS	8/08/2014	881.73
EF080057	26070	<b>ZUVELA ENDPLAN SUPERANNUATION FUND</b> SUPERANNUATION FUND	8/08/2014	732.73
EF080058	26071	<b>NORTH PERSONAL SUPERANNUATION PLAN</b> SUPERANNUATION FUND	8/08/2014	108.09
EF080059	26089	<b>ESSENTIAL SUPPERANNUATION FUND</b> SUPPERANNUATION	8/08/2014	996.27
EF080060	11425	<b>SOUTHERN METROPOLITAN REGIONAL COUNCIL</b> WASTE DISPOSAL GATE FEES	15/08/2014	267,975.64
EF080061	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	18/08/2014	302,921.00
EF080062	10351	<b>COCKBURN BMX STADIUM</b> SPORTING EQUIPT GRANT/REGISTRATION FEES	18/08/2014	3,008.73
EF080063	10944	<b>MCLEODS</b> LEGAL SERVICES	18/08/2014	1,009.65
EF080064	12740	<b>MAYOR LOGAN HOWLETT</b> WALGA SITTING FEEES	18/08/2014	1,550.00
EF080065	13910	<b>ATO - DEPUTY COMMISSIONER OF TAXATION</b> FBT PAYMENT	18/08/2014	582.91
EF080066	15862	<b>FREMANTLE MILK DISTRIBUTORS</b> MILK DELIVERY	18/08/2014	931.85
EF080067	15883	<b>TONY ROMANO - COUNCILLOR</b> WALGA SITTING FEES	18/08/2014	1,291.67
EF080068	16058	<b>SHOP-A-DOCKET PTY LTD</b> ADVERTISING SERVICES	18/08/2014	1,628.00
EF080069	18553	<b>SELECTUS PTY LTD</b> PAYROLL DEDUCTIONS	18/08/2014	12,956.02
EF080070	23250	<b>DEPARTMENT OF PLANNING</b> DAP APPLICATIONS & DAP FEES	18/08/2014	5,409.00
EF080071	23351	<b>COCKBURN GP SUPER CLINIC PTY LTD</b> OPERATING FUNDS	18/08/2014	55,000.00
EF080072	24447	<b>LORRAINE COLE</b> RATES REFUND	18/08/2014	3,980.00
EF080073	25109	<b>MICHELLE CHAMPION - MURDOCH UNIVERSITY</b> POSTGRADUATE CERTIFICATE IN BUSINESS AD.	18/08/2014	2,625.00
EF080074	25335	<b>JAXON PTY LTD</b> BUILDING CONSTRUCTION SERVICES	18/08/2014	458,497.99
EF080075	25635	<b>MW URBAN</b> CONSULTANCY SERVICES	18/08/2014	7,791.52
EF080076	25960	<b>FITMONKY</b> PERSONAL TRAINING SERVICES	18/08/2014	1,200.00
EF080077	25987	<b>TOYOTA FLEET MANAGEMENT</b> PAYROLL DEDUCTIONS - NOVATED LEASE	18/08/2014	567.62
EF080078	26080	<b>JANESSA RUNDLE</b> CROSSOVER CONTRIBUTION	18/08/2014	300.00
EF080079	26115	<b>GEOFFREY HEALY</b> HIGH RISK LICENCE RENEWAL	18/08/2014	70.00
EF080080	10071	<b>AUSTRALASIAN PERFORMING RIGHT ASSOC. LTD</b> LICENCE - PERFORMING RIGHTS	29/08/2014	134.64
EF080081	10082	<b>ARMANDOS SPORTS</b> SPORTING GOODS	29/08/2014	346.25
EF080082	10086	<b>ARTEIL WA PTY LTD</b> ERGONOMIC CHAIRS	29/08/2014	1,694.00

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080083	10118	<b>AUSTRALIA POST</b> POSTAGE CHARGES	29/08/2014	55,649.83
EF080084	10135	<b>ENVIRONMENTAL HEALTH AUSTRALIA</b> TRAINING SERVICES - HEALTH	29/08/2014	2,675.00
EF080085	10160	<b>DORMA AUSTRALIA PTY LTD</b> AUTOMATIC DOOR SERVICES	29/08/2014	7,073.00
EF080086	10184	<b>BENARA NURSERIES</b> PLANTS	29/08/2014	6,438.31
EF080087	10201	<b>BIG W DISCOUNT STORES</b> VARIOUS SUPPLIES	29/08/2014	213.60
EF080088	10212	<b>BOSS BOLLARDS</b> SECURITY PRODUCTS	29/08/2014	59.40
EF080089	10219	<b>BOUSFIELDS MENSWEAR</b> CLOTHING SUPPLIES	29/08/2014	400.00
EF080090	10220	<b>BOYA EQUIPMENT</b> EQUIPMENT SUPPLIES	29/08/2014	194.45
EF080091	10221	<b>BP AUSTRALIA LIMITED</b> DIESEL/PETROL SUPPLIES	29/08/2014	10,884.07
EF080092	10226	<b>BRIDGESTONE AUSTRALIA LTD</b> TYRE SERVICES	29/08/2014	62,632.57
EF080093	10246	<b>BUNNINGS BUILDING SUPPLIES PTY LTD</b> HARDWARE SUPPLIES	29/08/2014	1,579.55
EF080094	10247	<b>BUNZL AUSTRALIA LTD</b> PAPER/PLASTIC/CLEANING SUPPLIES	29/08/2014	349.67
EF080095	10255	<b>CABCHARGE AUSTRALIA PTY LTD</b> CABCHARGES	29/08/2014	276.18
EF080096	10256	<b>CABLE LOCATES &amp; CONSULTING</b> LOCATING SERVICES	29/08/2014	1,080.20
EF080097	10279	<b>CASTROL AUSTRALIA PTY LTD</b> GREASE/LUBRICANTS	29/08/2014	4,717.99
EF080098	10292	<b>CHADSON ENGINEERING PTY LTD</b> MEDICAL SUPPLIES	29/08/2014	605.00
EF080099	10328	<b>CITY OF MELVILLE</b> SECURITY SERVICES	29/08/2014	2,000.00
EF080100	10335	<b>CLASSIC HIRE</b> EQUIPMENT HIRING SERVICES	29/08/2014	572.00
EF080101	10346	<b>COATES HIRE OPERATIONS PTY LTD</b> EQUIPMENT HIRING SERVICES	29/08/2014	1,396.75
EF080102	10348	<b>COCA COLA AMATIL</b> SOFT DRINK SUPPLIES	29/08/2014	2,564.17
EF080103	10349	<b>COCKBURN BASKETBALL ASSOC INC</b> ELECTRICITY REIMBURSEMENTS	29/08/2014	600.00
EF080104	10351	<b>COCKBURN BMX STADIUM</b> SPORTING EQUIPT GRANT/REGISTRATION FEES	29/08/2014	600.00
EF080105	10360	<b>COCKBURN PARTY</b> HIRE OF PARTY EQUIPMENT	29/08/2014	4,146.15
EF080106	10365	<b>COC VOLUNTARY SES</b> EXPENSE REIMBURSEMENTS	29/08/2014	7,219.98
EF080107	10368	<b>COCKBURN WETLANDS EDUCATION CENTRE</b> COMMUNITY GRANT	29/08/2014	250.00
EF080108	10384	<b>PROGILITY PTY LTD COMMUNICATIONS AUSTRALIA PTY LTD</b> COMMUNICATION SERVICES	29/08/2014	17,960.47
EF080109	10386	<b>COMMUNITY NEWSPAPER GROUP</b> ADVERTISING SERVICES	29/08/2014	15,604.54

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080110	10394	<b>CD'S CONFECTIONERY WHOLESALERS</b> CONFECTIONERY	29/08/2014	1,522.45
EF080111	10456	<b>DATANET PTY LTD</b> SOFTWARE MODIFICATIONS	29/08/2014	20,887.29
EF080112	10459	<b>DAVID GRAY &amp; CO</b> MOBILE GARBAGE BINS	29/08/2014	329.34
EF080113	10483	<b>LANDGATE</b> MAPPING/LAND TITLE SEARCHES	29/08/2014	7,566.54
EF080114	10485	<b>DEPARTMENT OF SPORT AND RECREATION</b> ACCOMMODATION DEPOSIT	29/08/2014	29,050.70
EF080115	10498	<b>DIGITAL MAPPING SOLUTIONS</b> COMPUTER SOFTWARE	29/08/2014	550.00
EF080116	10512	<b>DOMUS NURSERY</b> VARIOUS PLANTS	29/08/2014	473.38
EF080117	10526	<b>E &amp; MJ ROSHER PTY LTD</b> MOWER PARTS	29/08/2014	17,908.25
EF080118	10535	<b>WORKPOWER INCORPORATED T/AS ECOSYSTEM MANAGEMENT SERVICE</b> PLANTS	29/08/2014	21,142.99
EF080119	10573	<b>FAIRBRIDGE WESTERN AUSTRALIA INC</b> OUTDOOR RECREATION SERVICES	29/08/2014	870.00
EF080120	10580	<b>FC COURIERS</b> COURIER SERVICES	29/08/2014	3,040.44
EF080121	10588	<b>FINANCIAL COUNSELLORS ASSOC OF WA INC</b> MEMBERSHIP RENEWAL	29/08/2014	555.00
EF080122	10590	<b>DEPARTMENT OF FIRE AND EMERGENCY SERVICES</b> COST SHARING - COMMUNITY FIRE MANAGER	29/08/2014	3,373.08
EF080123	10603	<b>FLOORING SOLUTIONS</b> FLOOR COVERINGS	29/08/2014	1,969.00
EF080124	10609	<b>FORESTVALE TREES P/L</b> PLANTS - TREES/SHRUBS	29/08/2014	4,268.00
EF080125	10626	<b>FREMANTLE PRISON TOURS DEPARTMENT OF FINANCE</b> FREMANTLE PRISON DAY TOURS	29/08/2014	1,081.00
EF080126	10664	<b>GOLDER ASSOCIATES</b> CONSULTANCY SERVICES	29/08/2014	36,242.72
EF080127	10666	<b>GOLDNET SECURITY</b> SECURITY SERVICES/PRODUCTS	29/08/2014	100.00
EF080128	10679	<b>GRASSTREES AUSTRALIA</b> PLANTS & PLANTING SERVICES	29/08/2014	1,551.00
EF080129	10692	<b>AECOM AUSTRALIA PTY LTD</b> CONSULTANCY SERVICES	29/08/2014	32,361.73
EF080130	10732	<b>HORIZONS WEST BUS &amp; COACHLINES</b> TRANSPORTATION SERVICES	29/08/2014	330.00
EF080131	10737	<b>RAIN SCAPE WATERWISE SOLUTIONS</b> RETICULATION/IRRIGATION SUPPLIES	29/08/2014	712.83
EF080132	10743	<b>ICON-SEPTECH PTY LTD</b> DRAINAGE PRODUCTS	29/08/2014	3,437.39
EF080133	10762	<b>INDUSTRIAL PROTECTIVE PRODUCTS WA</b> PROTECTIVE SUPPLIES	29/08/2014	260.48
EF080134	10779	<b>J F COVICH &amp; CO PTY LTD</b> ELECTRICAL SERVICES	29/08/2014	34,092.98
EF080135	10783	<b>JANDAKOT METAL INDUSTRIES</b> METAL SUPPLIES	29/08/2014	1,443.20
EF080136	10787	<b>JANDAKOT ACCIDENT REPAIR CENTRE</b> PANEL BEATING SERVICES	29/08/2014	633.60

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080137	10803	<b>GECKO CONTRACT</b> MOWING/LANDSCAPING SERVICES	29/08/2014	70,298.70
EF080138	10814	<b>JR &amp; A HERSEY PTY LTD</b> SAFETY CLOTHING SUPPLIES	29/08/2014	1,075.80
EF080139	10816	<b>JUNGLE GYM</b> ENTRY FEES	29/08/2014	280.00
EF080140	10864	<b>LAMP REPLACEMENTS</b> LIGHTING SUPPLIES	29/08/2014	361.35
EF080141	10879	<b>LES MILLS AEROBICS</b> INSTRUCTION/TRAINING SERVICES	29/08/2014	2,174.10
EF080142	10884	<b>WSP BUILDINGS PTY LTD</b> CONSULTANCY SERVICES	29/08/2014	7,700.00
EF080143	10888	<b>LJ CATERERS</b> CATERING SERVICES	29/08/2014	6,781.65
EF080144	10892	<b>LOCAL GOVT MANAGERS AUSTRALIA</b> SUBSCRIPTION	29/08/2014	2,200.00
EF080145	10893	<b>LOCAL GOVT SUPERVISORS ASSOC OF WA INC</b> CONFERENCE/SEMINARS	29/08/2014	77.00
EF080146	10896	<b>LOCAL HEALTH AUTHORITIES ANALYTICAL COMMITTEE</b> ANALYTICAL SERVICES	29/08/2014	22,973.61
EF080147	10913	<b>MACDONALD JOHNSTON ENGINEERING CORP</b> REPAIR SERVICES	29/08/2014	17,237.49
EF080148	10918	<b>MAIN ROADS WA</b> REPAIRS/MAINTENANCE SERVICES	29/08/2014	47,894.00
EF080149	10923	<b>MAJOR MOTORS PTY LTD</b> REPAIRS/MAINTENANCE SERVICES	29/08/2014	2,776.05
EF080150	10938	<b>MAXWELL ROBINSON &amp; PHELPS</b> PEST & WEED MANAGEMENT	29/08/2014	71,113.44
EF080151	10939	<b>LINFOX ARMAGUARD</b> BANKING SECURITY SERVICES	29/08/2014	1,402.64
EF080152	10944	<b>MCLEODS</b> LEGAL SERVICES	29/08/2014	29,978.34
EF080153	10973	<b>MIRCO BROS PTY LTD</b> FERTILISER SUPPLIES	29/08/2014	261.00
EF080154	10981	<b>MOBILE MASTERS</b> COMMUNICATIONS EQUIPMENT/SERVICES	29/08/2014	2,371.05
EF080155	10991	<b>BEACON EQUIPMENT</b> MOWING EQUIPMENT	29/08/2014	1,214.00
EF080156	10997	<b>WILSON PARKING AUSTRALIA</b> SECURITY SERVICES	29/08/2014	159,505.60
EF080157	11004	<b>MURDOCH UNIVERSITY OFFICE OF FINANCE, PLANNING &amp; REPORTING</b> ANALYSING SERVICES	29/08/2014	741.10
EF080158	11022	<b>NATIVE ARC</b> DONATION	29/08/2014	350.18
EF080159	11026	<b>NESTLE FOOD SERVICES</b> CATERING SUPPLIES	29/08/2014	378.00
EF080160	11036	<b>NORTHLAKE ELECTRICAL</b> ELECTRICAL SERVICES	29/08/2014	15,635.17
EF080161	11039	<b>NOVUS AUTO GLASS</b> WINDSCREEN REPAIR SERVICES	29/08/2014	250.00
EF080162	11077	<b>P &amp; G BODY BUILDERS PTY LTD</b> PLANT BODY BUILDING SERVICES	29/08/2014	9,036.50
EF080163	11152	<b>FULTON HOGAN INDUSTRIES PTY LTD</b> ROAD MAINTENANCE	29/08/2014	4,106.30

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EF080164	11205	<b>QUALITY TRAFFIC MANAGEMENT PTY LTD</b> TRAFFIC CONTROL SERVICES	29/08/2014	5,362.52
EF080165	11208	<b>QUICK CORPORATE AUSTRALIA PTY LTD</b> STATIONERY/CONSUMABLES	29/08/2014	6,920.81
EF080166	11214	<b>RAECO INTERNATIONAL PTY LTD</b> STATIONERY SUPPLIES	29/08/2014	707.88
EF080167	11235	<b>REINFORCED CONCRETE PIPES PTY LTD</b> CONCRETE PIPE SUPPLIES	29/08/2014	83,118.87
EF080168	11240	<b>INITIAL HYGIENE RENTOKIL INITIAL PRT LTD</b> SANITARY SERVICES	29/08/2014	1,033.06
EF080169	11264	<b>ROCLA PIPELINE PRODUCTS</b> CONCRETE LINER SUPPLIES	29/08/2014	6,058.16
EF080170	11294	<b>SAFEMAN (WA) PTY LTD</b> PROTECTIVE CLOTHING/EQUIPMENT	29/08/2014	1,035.47
EF080171	11304	<b>SANAX MEDICAL &amp; FIRST AID SUPPLIES</b> MEDICAL SUPPLIES	29/08/2014	373.89
EF080172	11307	<b>SATELLITE SECURITY SERVICES PTY LTD</b> SECURITY SERVICES	29/08/2014	6,689.35
EF080173	11308	<b>SBA SUPPLIES</b> HARDWARE SUPPLIES	29/08/2014	5,982.70
EF080174	11311	<b>SCITECH DISCOVERY CENTRE</b> ENTERTAINMENT SERVICES	29/08/2014	305.00
EF080175	11337	<b>SHERIDANS FOR BADGES</b> NAME BADGES & ENGRAVING	29/08/2014	173.36
EF080176	11361	<b>SIGMA CHEMICALS PTY LTD</b> CHEMICAL SUPPLIES	29/08/2014	1,150.00
EF080177	11380	<b>SNAP PRINTING FREMANTLE</b> PRINTING SERVICES	29/08/2014	3,400.50
EF080178	11382	<b>SNAP PRINTING SUBIACO</b> PRINTING SERVICES	29/08/2014	668.00
EF080179	11399	<b>SOUTH COOGEE VOLUNTEER BUSHFIRE BRIGADE</b> EXPENSE REIMBURSEMENTS	29/08/2014	6,218.54
EF080180	11425	<b>SOUTHERN METROPOLITAN REGIONAL COUNCIL</b> WASTE DISPOSAL GATE FEES	29/08/2014	700,543.81
EF080181	11453	<b>SPEARWOOD NEWSROUND</b> NEWSPAPER SUPPLIES	29/08/2014	575.91
EF080182	11459	<b>SPEARWOOD VETERINARY HOSPITAL</b> VETERINARY SERVICES	29/08/2014	1,426.00
EF080183	11469	<b>SPORTS TURF TECHNOLOGY</b> TURF CONSULTANCY SERVICES	29/08/2014	1,584.00
EF080184	11470	<b>SPORTSWORLD OF WA</b> SPORT SUPPLIES	29/08/2014	551.10
EF080185	11505	<b>STATE LIBRARY OF WESTERN AUSTRALIA</b> BOOK SUPPLIES	29/08/2014	2,013.00
EF080186	11531	<b>SUNNY INDUSTRIAL BRUSHWARE PTY LTD</b> BRUSH/ROAD BROOM SUPPLIES	29/08/2014	3,223.00
EF080187	11533	<b>SUPERBOWL MELVILLE</b> ENTERTAINMENT SERVICES	29/08/2014	76.00
EF080188	11546	<b>T FAULKNER &amp; CO</b> INSTALLATIONS/SUPPLY OF HAND RAILS	29/08/2014	23,604.90
EF080189	11613	<b>TIMEZONE FREMANTLE</b> ENTERTAINMENT SERVICES	29/08/2014	448.00
EF080190	11625	<b>TOTAL EDEN PTY LTD</b> RETICULATION SUPPLIES	29/08/2014	62,368.88

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EF080191	11651	<b>TREE WATERING SERVICES</b> TREE WATERING SERVICES	29/08/2014	560.00
EF080192	11652	<b>TRENCHBUSTERS</b> HIRING SERVICES	29/08/2014	277.20
EF080193	11657	<b>TRUCKLINE PARTS CENTRES</b> AUTOMOTIVE SPARE PARTS	29/08/2014	3,909.19
EF080194	11663	<b>TUDOR HOUSE</b> BANNERS/FLAGS	29/08/2014	928.00
EF080195	11665	<b>TUNNEL VISION</b> PLUMBING SERVICES	29/08/2014	121.00
EF080196	11697	<b>VAT MAN-FAT FILTERING SYSTEMS</b> FILTER CLEANING SERVICES	29/08/2014	657.90
EF080197	11699	<b>VERNON DESIGN GROUP</b> ARCHITECTURAL SERVICES	29/08/2014	29,205.00
EF080198	11708	<b>VITAL PACKAGING PTY LTD</b> PACKAGING SUPPLIES	29/08/2014	4,427.50
EF080199	11715	<b>WA BLUEMETAL</b> ROADBASE SUPPLIES	29/08/2014	13,667.29
EF080200	11722	<b>WA HINO SALES &amp; SERVICE</b> REPAIRS/MAINTENANCE SERVICES	29/08/2014	72,456.13
EF080201	11726	<b>WA LIMESTONE</b> LIMESTONE SUPPLIES	29/08/2014	7,574.37
EF080202	11773	<b>WESFARMERS LANDMARK LIMITED</b> CHEMICAL SUPPLIES	29/08/2014	2,875.60
EF080203	11786	<b>WESTCARE INDUSTRIES</b> STATIONERY/SAFETY VESTS	29/08/2014	774.40
EF080204	11789	<b>WALGA</b> ADVERTISING/TRAINING SERVICES	29/08/2014	6,655.00
EF080205	11793	<b>WESTERN IRRIGATION PTY LTD</b> IRRIGATION SERVICES/SUPPLIES	29/08/2014	10,458.69
EF080206	11795	<b>WESTERN POWER</b> ELECTRICAL SERVICES	29/08/2014	42,417.00
EF080207	11806	<b>WESTRAC PTY LTD</b> REPAIRS/MTNCE - EARTHMOVING EQUIPMENT	29/08/2014	1,054.09
EF080208	11828	<b>WORLDWIDE ONLINE PRINTING - O'CONNOR</b> PRINTING SERVICES	29/08/2014	1,848.00
EF080209	11841	<b>YANGEBUP FAMILY CENTRE INC</b> DONATION / GRANT	29/08/2014	200.00
EF080210	11854	<b>ZIPFORM</b> PRINTING SERVICES	29/08/2014	28,623.01
EF080211	11873	<b>WATTLEUP TRACTORS</b> HARDWARE SUPPLIES	29/08/2014	571.55
EF080212	11987	<b>SAFETY ZONE AUSTRALIA PTY LTD</b> SAFETY EQUIPMENT	29/08/2014	1,699.30
EF080213	11990	<b>EARTHCARE (AUSTRALIA) P/L</b> LANDSCAPING SERVICES	29/08/2014	1,267.20
EF080214	11999	<b>SOUTHERN SCENE PTY LTD</b> BOOK SUPPLIES	29/08/2014	35.09
EF080215	12007	<b>SHANE MCMASTER SURVEYS</b> SURVEYING SERVICES	29/08/2014	21,230.00
EF080216	12014	<b>TUTT BRYANT EQUIPMENT BT EQUIPMENT PTY LTD T/AS</b> EXCAVATING/EARTHMOVING EQUIPMENT	29/08/2014	1,331.00
EF080217	12018	<b>O'CONNOR LAWNMOWER &amp; CHAINSAW CENTRE</b> MOWING EQUIPMENT/PARTS/SERVICES	29/08/2014	141.50

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EF080218	12065	<b>ROMERI MOTOR TRIMMERS</b> AUTOMOTIVE UPHOLSTERY SERVICES	29/08/2014	400.00
EF080219	12101	<b>THE GREAT ESCAPE</b> ENTRY FEES	29/08/2014	649.20
EF080220	12173	<b>CHALLENGE CHEMICALS AUSTRALIA</b> CHEMICAL SUPPLIES	29/08/2014	194.04
EF080221	12207	<b>CIVICA PTY LTD</b> SOFTWARE SUPPORT/LICENCE FEES	29/08/2014	12,558.04
EF080222	12219	<b>PARKS AND LEISURE AUSTRALIA</b> SUBSCRIPTION RENEWAL	29/08/2014	1,270.50
EF080223	12539	<b>COCKBURN JUNIOR FOOTBALL CLUB INC.</b> SPORTS SERVICES	29/08/2014	400.00
EF080224	12542	<b>SEALIN GARLETT</b> CEREMONIAL SERVICES	29/08/2014	800.00
EF080225	12589	<b>AUSTRALIAN INSTITUTE OF MANAGEMENT</b> TRAINING SERVICES	29/08/2014	515.00
EF080226	12694	<b>SPECIALISED LIFTING SERVICE</b> LIFTING EQUIPMENT & SERVICES	29/08/2014	1,583.36
EF080227	12712	<b>MISS MAUD</b> CATERING SERVICES	29/08/2014	167.30
EF080228	12780	<b>PALASSIS ARCHITECTS</b> ARCHITECTURAL SERVICES	29/08/2014	2,136.75
EF080229	12791	<b>ALCHEMY TECHNOLOGY</b> COMPUTER SOFTWARE SERVICES	29/08/2014	7,273.20
EF080230	12882	<b>ALLFLOW INDUSTRIAL</b> WASTE DISPOSAL SERVICES	29/08/2014	430.05
EF080231	12883	<b>CONSERVATION VOLUNTEERS AUSTRALIA</b> ENVIRONMENTAL SERVICES	29/08/2014	1,650.00
EF080232	13000	<b>BORAL ASPHALT WA</b> SUPPLY OF ASPHALT	29/08/2014	497.31
EF080233	13044	<b>METTLER TOLEDO LTD</b> REPAIR/MAINTENANCE SERVICES	29/08/2014	38,280.00
EF080234	13341	<b>GYMBUS</b> ENTERTAINMENT SERVICES	29/08/2014	580.80
EF080235	13364	<b>NEW TOWN TOYOTA</b> VEHICLE PURCHASE	29/08/2014	28,750.02
EF080236	13373	<b>THE HIRE GUYS</b> HIRING SERVICES	29/08/2014	3,177.00
EF080237	13409	<b>KLEENIT</b> CLEANING SERVICES	29/08/2014	24,165.00
EF080238	13462	<b>ATI-MIRAGE PTY LTD</b> TRAINING SERVICES	29/08/2014	642.98
EF080239	13563	<b>GREEN SKILLS INC. ECOJOBS ENVIRONMENTAL PERSONNEL</b> EMPLOYMENT SERVICES	29/08/2014	12,311.76
EF080240	13671	<b>STAPLES AUSTRALIA PTY LTD</b> OFFICE/STATIONERY SUPPLIES	29/08/2014	14,130.49
EF080241	13779	<b>PORTER CONSULTING ENGINEERS</b> ENGINEERING CONSULTANCY SERVICES	29/08/2014	7,287.50
EF080242	13825	<b>JACKSON MCDONALD</b> LEGAL SERVICES	29/08/2014	8,733.87
EF080243	13832	<b>INSIGHT CALL CENTRE SERVICES</b> COMMUNICATION SERVICES	29/08/2014	7,766.98
EF080244	14034	<b>ADECCO PLEASE REFER TO SUPPLIER NO 25955</b> EMPLOYMENT SERVICES	29/08/2014	6,935.23

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EF080245	14350	<b>BAILEYS FERTILISERS</b> FERTILISER SUPPLIES	29/08/2014	444.40
EF080246	14435	<b>LAKES JUNIOR FOOTBALL CLUB</b> YOUTH ACTIVE PROGRAM REGISTRATION FEES	29/08/2014	490.00
EF080247	14459	<b>BIDVEST (WA)</b> FOOD/CATERING SUPPLIES	29/08/2014	675.30
EF080248	14593	<b>AUSTREND INTERNATIONAL PTY LTD</b> ALUMINIUM SUPPLIES	29/08/2014	2,919.95
EF080249	14630	<b>NATALE SECURITY SERVICES</b> SECURITY SERVICES	29/08/2014	15,611.75
EF080250	14632	<b>UHY HAINES NORTON</b> ACCOUNTANCY SERVICES/PRODUCTS	29/08/2014	11,016.50
EF080251	14777	<b>LGIS INSURANCE BROKING</b> INSURANCE PREMIUMS	29/08/2014	318,411.23
EF080252	15162	<b>PERTH MANAGEMENT SERVICES</b> PROPERTY MANAGERS	29/08/2014	254.50
EF080253	15267	<b>CHEMSEARCH AUSTRALIA</b> CHEMICAL SUPPLIES	29/08/2014	16,230.95
EF080254	15274	<b>CHARITY LINK</b> MEMBERSHIP FEE	29/08/2014	88.00
EF080255	15363	<b>JONES LANG LASALLE (WA) PTY LTD</b> SHOP RENT - GATEWAY SHOPPING CENTRE	29/08/2014	24,203.03
EF080256	15393	<b>GREENWAY ENTERPRISES</b> HARDWARE SUPPLIES	29/08/2014	485.63
EF080257	15850	<b>ECOSCAPE</b> ENVIRONMENTAL CONSULTANCY	29/08/2014	2,158.75
EF080258	15862	<b>FREMANTLE MILK DISTRIBUTORS</b> MILK DELIVERY	29/08/2014	1,293.40
EF080259	16107	<b>WREN OIL</b> WASTE DISPOSAL SERVICES	29/08/2014	413.05
EF080260	16175	<b>PUBLIC LIBRARIES AUSTRALIA LTD</b> MEMBERSHIP FEES	29/08/2014	385.00
EF080261	16396	<b>MAYDAY EARTHMOVING</b> GRADER HIRE	29/08/2014	91,020.14
EF080262	16403	<b>ROBINSON BUILDTECH</b> BUILDING SERVICES - ALTERATIONS	29/08/2014	13,988.60
EF080263	16510	<b>LLOYD GEORGE ACOUSTICS PTY LTD</b> CONSULTANCY SERVICES - ACOUSTIC	29/08/2014	5,225.00
EF080264	16675	<b>FREMANTLE PLUMBING SERVICE PTY LTD</b> PLUMBING SERVICES	29/08/2014	242.00
EF080265	16894	<b>TREBLEX INDUSTRIAL PTY LTD</b> CHEMICALS - AUTOMOTIVE	29/08/2014	633.60
EF080266	16926	<b>CURTIN UNIVERSI</b> EDUCATION SERVICES	29/08/2014	5,500.00
EF080267	16985	<b>WA PREMIX</b> CONCRETE SUPPLIES	29/08/2014	5,262.18
EF080268	16997	<b>AUS SECURE</b> SECURITY SERVICES/PRODUCTS	29/08/2014	580.00
EF080269	17097	<b>VALUE TISSUE</b> PAPER PRODUCTS	29/08/2014	678.48
EF080270	17213	<b>COCKBURN CITY SOCCER CLUB INC</b> SPORT EQUIPMENT GRANT	29/08/2014	800.00
EF080271	17272	<b>SOUTH COOGEE JUNIOR FOOTBALL CLUB</b> REGISTRATION FEES	29/08/2014	10,400.00

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EF080272	17345	<b>KENNARDS HIRE - MYAREE</b> EQUIPMENT HIRE	29/08/2014	13,019.66
EF080273	17362	<b>JOHN EARLEY</b> TRAINING	29/08/2014	490.00
EF080274	17471	<b>PIRTEK (FREMANTLE) PTY LTD</b> HOSES & FITTINGS	29/08/2014	4,007.91
EF080275	17604	<b>WRITING WA</b> MEMBERSHIP	29/08/2014	135.00
EF080276	17661	<b>THE COCKBURN BU</b> ADVERTISING SERVICES	29/08/2014	3,000.00
EF080277	17798	<b>WESTERN DIAGNOSTIC PATHOLOGY</b> ANALYTICAL SERVICES	29/08/2014	342.54
EF080278	17942	<b>MRS MAC'S</b> FOOD SUPPLIES	29/08/2014	413.55
EF080279	18084	<b>VIZCOM TECHNOLOGIES PTY LTD</b> AUDIO VISUAL EQUIPMENT	29/08/2014	873.73
EF080280	18100	<b>DAVIS LANGDON AUSTRALIA</b> COST MANAGEMENT SERVICES	29/08/2014	6,429.50
EF080281	18114	<b>BOLLIG DESIGN GROUP P/L</b> ARCHITECTURAL SERVICES	29/08/2014	5,720.00
EF080282	18272	<b>AUSTRACLEAR LIMITED</b> INVESTMENT SERVICES	29/08/2014	134.78
EF080283	18304	<b>JANDAKOT JETS JUNIOR FOOTBALL CLUB</b> SPORTING EQUIPMENT GRANT	29/08/2014	5,200.00
EF080284	18398	<b>JOE CRISAFIO KIA</b> VEHICLE PURCHASE	29/08/2014	35,228.00
EF080285	18425	<b>SUCCESS STRIKER</b> REGISTRATION FEES	29/08/2014	5,400.00
EF080286	18494	<b>DEPARTMENT OF PARKS &amp; WILDLIFE</b> LICENCE RENEWAL	29/08/2014	1,032.00
EF080287	18508	<b>JOHN TURNER</b> BRICK LAYING SERVICES	29/08/2014	7,040.00
EF080288	18593	<b>TOTAL ALARMS &amp; SECURITY PRODUCTS</b> SECURITY SERVICES	29/08/2014	200.00
EF080289	18613	<b>ECO-HIRE</b> EQUIPMENT HIRE	29/08/2014	2,409.00
EF080290	18695	<b>MYAREE CRANE HIRE</b> CRANE HIRE	29/08/2014	1,243.00
EF080291	18731	<b>OCCMEDIC</b> MEDICAL SERVICES	29/08/2014	90.00
EF080292	18734	<b>P &amp; R EDWARDS</b> ENTERTAINMENT SERVICES	29/08/2014	600.00
EF080293	18952	<b>BRENNAN &amp; ASSOCIATES</b> LEGAL SERVICES	29/08/2014	1,390.40
EF080294	18962	<b>SEALANES (1985) P/L</b> CATERING SUPPLIES	29/08/2014	4,746.88
EF080295	19038	<b>DOWSING CONCRETE</b> CONCRETING SERVICES	29/08/2014	3,927.00
EF080296	19288	<b>ROTARY CLUB OF</b> DONATION	29/08/2014	5,000.00
EF080297	19293	<b>SPRAYLINE SPRAYING EQUIPMENT</b> SPRAYING EQUIPMENT	29/08/2014	1,901.80
EF080298	19436	<b>WHITCHURCH REFRIGERATION &amp; AIRCONDITIONING</b> AIR CONDITIONING SERVICES	29/08/2014	850.85

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EF080299	19446	<b>ENVISIONWARE PTY LTD</b> SOFTWARE	29/08/2014	1,119.69
EF080300	19494	<b>THE ARCHERY CENTRE</b> ARCHERY	29/08/2014	750.00
EF080301	19505	<b>ADVANCED WINDOW SHUTTERS</b> WINDOW SHUTTERS	29/08/2014	2,700.00
EF080302	19533	<b>WOOLWORTHS LTD</b> GROCERIES	29/08/2014	228.63
EF080303	19546	<b>THE BIG PICTURE FACTORY</b> PRINTING SERVICES	29/08/2014	919.60
EF080304	19558	<b>COMPLETE FIRE DESIGN</b> FIRE CONSULTANCY SERVICES	29/08/2014	8,486.70
EF080305	19628	<b>PAPERBARK TECHNOLOGIES</b> ARBORICULTURAL CONSULTANCY SERVICES	29/08/2014	730.00
EF080306	19718	<b>SIFTING SANDS</b> CLEANING SERVICES - SAND	29/08/2014	18,490.00
EF080307	19731	<b>IPDAT COMMUNICATIONS CONSULTATION</b> CONSULTANCY SERVICES - COMMUNICATIONS	29/08/2014	9,922.00
EF080308	19755	<b>EMBROIDME MYAREE</b> EMBROIDERY	29/08/2014	3,630.49
EF080309	19885	<b>SAFEGUARD INDUSTRIES</b> SECURITY SCREENS/DOORS	29/08/2014	680.00
EF080310	20000	<b>AUST WEST AUTO ELECTRICAL P/L</b> AUTO ELECTRICAL SERVICES	29/08/2014	25,642.11
EF080311	20068	<b>CLARITY COMMUNICATIONS</b> PUBLIC RELATIONS CONSULTANCY SERVICES	29/08/2014	1,100.00
EF080312	20146	<b>DATA#3 LIMITED</b> CONTRACT IT PERSONNEL & SOFTWARE	29/08/2014	64,914.04
EF080313	20215	<b>POWERVAC</b> CLEANING EQUIPMENT	29/08/2014	436.40
EF080314	20549	<b>A1 CARPET, TILE &amp; GROUT CLEANING</b> CLEANING SERVICES - TILES/CARPET	29/08/2014	2,865.50
EF080315	20556	<b>DVG MOUNTWAY MELVILLE</b> FLEET VEHICLES	29/08/2014	23,598.85
EF080316	20697	<b>ENCYCLE CONSULTING PTY LTD</b> ENVIRONMENTAL CONSULTANCY	29/08/2014	7,920.00
EF080317	20882	<b>BELL-VISTA FRUIT &amp; VEGETABLE</b> FRUIT & VEGETABLE	29/08/2014	2,423.38
EF080318	20933	<b>ACTROL PARTS PTY LTD</b> REFRIGERATION	29/08/2014	1,210.00
EF080319	21005	<b>BRAIN TEASERS OZ PTY LTD</b> EDUCATIONAL PRODUCTS	29/08/2014	66.00
EF080320	21120	<b>SHOREWATER MARINE PTY LTD</b> MARINE CONSTRUCTION SERVICES	29/08/2014	1,072.50
EF080321	21127	<b>JOANNA AYCKBOURN</b> INSTRUCTION - SINGING	29/08/2014	750.00
EF080322	21131	<b>STATE WIDE TURF SERVICES</b> TURF RENOVATION	29/08/2014	3,746.60
EF080323	21287	<b>T.J.DEPIAZZI &amp; SONS</b> SOIL & MULCH SUPPLIES	29/08/2014	2,783.00
EF080324	21291	<b>CHITTERING VALLEY WORM FARM</b> ENVIRONMENTAL EDUCATION	29/08/2014	1,969.00
EF080325	21363	<b>TENDERLINK.COM PTY LTD</b> COMPUTER SOFTWARE	29/08/2014	550.00

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080326	21364	<b>OFFICINO OFFICE FURNITURE</b> OFFICE FURNITURE	29/08/2014	1,939.30
EF080327	21371	<b>LD TOTAL SANPOINT PTY LTD</b> KERBING SERVICES	29/08/2014	31,124.06
EF080328	21469	<b>JOHN HUGHES VOLKSWAGON</b> PURCHASE OF NEW VEHICLE	29/08/2014	27,076.94
EF080329	21471	<b>WA MACHINERY GLASS</b> GLAZING SERVICES	29/08/2014	1,903.00
EF080330	21527	<b>TOUCHWOOD NURSERY</b> PLANT SUPPLIES	29/08/2014	1,347.33
EF080331	21627	<b>MANHEIM PTY LTD</b> IMPOUNDED VEHICLES	29/08/2014	1,707.20
EF080332	21664	<b>ACT INDUSTRIAL PTY LTD</b> SKIP BINS - MANUFACTURE	29/08/2014	9,680.00
EF080333	21739	<b>EXCEL TRAFFIC D</b> TRAFFIC SURVEYING SERVICES	29/08/2014	2,475.00
EF080334	21778	<b>HILTON SQUASH AND FITNESS</b> SPORTING SERVICES	29/08/2014	200.00
EF080335	21791	<b>THE LEISURE INSTITUTE OF WA (AQUATICS) INC.</b> PROFESSIONAL ORGANISATION	29/08/2014	1,470.00
EF080336	21796	<b>GREEN LEAF GARDENS</b> LANDSCAPING SERVICES	29/08/2014	7,080.00
EF080337	21946	<b>RYAN'S QUALITY MEATS</b> MEAT SUPPLIES	29/08/2014	514.78
EF080338	21986	<b>RECLINK AUSTRALIA INC</b> CHARITABLE ORGANISATION	29/08/2014	130.00
EF080339	21990	<b>MEDIBANK HEALTH SOLUTIONS PTY LTD</b> MEDICAL SERVICES	29/08/2014	1,641.75
EF080340	22012	<b>ELEGANT GLOVES EVENTS AND SERVICES</b> CATERING SERVICES	29/08/2014	795.00
EF080341	22133	<b>AIR-BORN AMUSEM</b> AMUSEMENT SERVICES	29/08/2014	1,890.50
EF080342	22182	<b>KALAMUNDA FENCING &amp; GATEMAKERS</b> FENCING SERVICES	29/08/2014	33,608.30
EF080343	22192	<b>VANESSA PAGET B</b> EDUCATION/ENTERTAINMENT	29/08/2014	269.50
EF080344	22242	<b>ASPHALT SURFACES PTY LTD</b> ASPHALTING SERVICES	29/08/2014	3,189.25
EF080345	22342	<b>MIDWAY DRYCLEANERS</b> DRYCLEANING SERVICES	29/08/2014	300.00
EF080346	22388	<b>CARRINGTON'S TRAFFIC SERVICES</b> TRAFFIC MANAGEMENT SERVICES	29/08/2014	5,486.26
EF080347	22404	<b>CLEVERPATCH PTY LTD</b> ARTS/CRAFT SUPPLIES	29/08/2014	456.63
EF080348	22448	<b>CAKES WEST PTY LTD</b> CATERING	29/08/2014	104.75
EF080349	22553	<b>BROWNES FOOD OPERATIONS</b> CATERING SUPPLIES	29/08/2014	1,508.33
EF080350	22569	<b>SONIC HEALTH PLUS PTY LTD</b> MEDICAL SERVICES	29/08/2014	2,843.50
EF080351	22600	<b>CUBIC PROMOTIONS PTY LTD</b> PROMOTIONAL PRODUCTS	29/08/2014	1,452.00
EF080352	22613	<b>VICKI ROYANS</b> ARTISTIC SERVICES	29/08/2014	400.00

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF080353	22619	<b>KSC TRAINING</b> TRAINING SERVICES	29/08/2014	2,636.00
EF080354	22624	<b>AUSSIE EARTHWORKS PTY LTD</b> EARTHWORKS	29/08/2014	118,877.00
EF080355	22639	<b>SHATISH CHAUHAN</b> TRAINING SERVICES - YOGA	29/08/2014	1,400.00
EF080356	22653	<b>PCYC FREMANTLE</b> SPONSORSHIP	29/08/2014	200.00
EF080357	22680	<b>LEONARD THORN</b> CULTURAL PRESENTATION SERVICES	29/08/2014	500.00
EF080358	22682	<b>BEAVER TREE SERVICES PTY LTD</b> TREE PRUNING SERVICES	29/08/2014	146,908.71
EF080359	22716	<b>PHOENIX LACROSSE CLUB</b> SPONSORSHIP / SPORTS EQUIPMENT GRANT	29/08/2014	1,400.00
EF080360	22720	<b>DIAMONDS NETBALL CLUB WA INC.</b> SPORTS - NETBALL	29/08/2014	400.00
EF080361	22737	<b>CJS LIMESTONE CONTRACTORS PTY LTD</b> LIMESTONE WORKS	29/08/2014	44,330.00
EF080362	22798	<b>PUMPS AUSTRALIA PTY LTD</b> PUMP EQUIPMENT	29/08/2014	907.50
EF080363	22805	<b>COVS PARTS PTY LTD</b> MOTOR PARTS	29/08/2014	4,332.06
EF080364	22806	<b>AUSTRALIAN FUEL DISTRIBUTORS PTY LTD</b> FUEL SUPPLIES	29/08/2014	131,879.61
EF080365	22854	<b>LGISWA</b> INSURANCE PREMIUMS	29/08/2014	937,808.39
EF080366	22859	<b>TOP OF THE LADDER GUTTER CLEANING</b> GUTTER CLEANING SERVICES	29/08/2014	393.96
EF080367	22879	<b>REMIDA PERTH INC</b> ARTISTIC SERVICES	29/08/2014	410.00
EF080368	22970	<b>WASHPOD CONSOLIDATED PTY LTD</b> CLEANING - EQUIPMENT	29/08/2014	291.50
EF080369	23213	<b>SPOTLESS FACILITY SERVICES PTY LTD (LAUNDRY)</b> LAUNDRY SERVICES	29/08/2014	1,668.74
EF080370	23253	<b>KOTT GUNNING</b> LEGAL SERVICES	29/08/2014	6,057.47
EF080371	23309	<b>FUN IN TRAINING AUSTRALIA PTY LTD</b> FITNESS CLASSES-INSTRUCTIONS	29/08/2014	1,254.00
EF080372	23348	<b>ZUMBA WITH HONEY</b> FITNESS CLASSES	29/08/2014	1,056.00
EF080373	23442	<b>PRICewaterhouseCOOPERS LEGAL</b> PROFESSIONAL/LEGAL SERVICES	29/08/2014	5,280.00
EF080374	23507	<b>LOCAL GEOTECHNICS</b> GEOTECHNICAL/ANALYTICAL SERVICES	29/08/2014	9,278.50
EF080375	23669	<b>WA IVECO</b> PURCHASE OF NEW TRUCKS	29/08/2014	376,798.72
EF080376	23670	<b>LIEBHERR AUSTRALIA PTY LTD</b> SPARE PARTS	29/08/2014	6,227.75
EF080377	23765	<b>BOOKERY EDUCATION</b> BOOK SUPPLIES	29/08/2014	1,050.00
EF080378	23810	<b>SABI ART &amp; DEISGN</b> ARTISTIC SERVICES	29/08/2014	11,000.00
EF080379	23815	<b>SOUTH FREMANTLE WOMENS FOOTBALL CLUB</b> REGISTRATION FEES - KIDSPORT	29/08/2014	400.00

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<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080380	23818	<b>AM &amp; IE MUTCH ENGINEERING CONSULTANTS</b> CONSULTANCY SERVICES	29/08/2014	2,046.00
EF080381	23849	<b>JCB CONSTRUCTION EQUIPMENT AUSTRALIA</b> PLANT/MACHINERY	29/08/2014	1,110.65
EF080382	23858	<b>SPECIALISED SECURITY SHREDDING</b> DOCUMENT DESTRUCTION SERVICES	29/08/2014	301.46
EF080383	23863	<b>HAMMOND PARK JUNIOR FOOTBALL CLUB (INC)</b> REGISTRATION FEES	29/08/2014	1,685.00
EF080384	23971	<b>FIND WISE LOCATION SERVICES</b> LOCATING SERVICES - UNDERGROUND	29/08/2014	12,329.90
EF080385	24035	<b>NEXT POWER</b> RENEWABLE ENERGY	29/08/2014	440.00
EF080386	24041	<b>ALLEN AIR &amp; REFRIDGERATION</b> AIR CONDITIONING SERVICES	29/08/2014	432.67
EF080387	24130	<b>WEST AUSTRALIAN</b> ENTERTAINMENT	29/08/2014	420.00
EF080388	24171	<b>KARDINYA NETBALL CLUB</b> REGISTRATION FEES	29/08/2014	400.00
EF080389	24180	<b>KARDINYA JUNIOR FOOTBALL CLUB</b> REGISTRATION FEES	29/08/2014	800.00
EF080390	24185	<b>HIPPY BELLY DANCE</b> TRAINING SERVICES - DANCE CLASSES	29/08/2014	325.00
EF080391	24189	<b>COMMUNITY TRANSFORMATIONS INC</b> TRAINING SERVICES	29/08/2014	652.50
EF080392	24192	<b>THE ECO FAERIES</b> ENTERTAINMENT SERVICES	29/08/2014	250.00
EF080393	24424	<b>DATAKOM SYSTEMS (WA) PTY LTD</b> COMPUTER HARDWARE/SOFTWARE	29/08/2014	19,635.00
EF080394	24444	<b>ROSEMARY ALLAN</b> WORKSHOPS	29/08/2014	450.00
EF080395	24506	<b>AMARANTI'S PERSONAL TRAINING</b> PERSONAL TRAINING SERVICES	29/08/2014	600.00
EF080396	24524	<b>CALO HEALTH</b> HEARTMOVE CLASSES	29/08/2014	2,605.00
EF080397	24557	<b>AVELING</b> CONSULTANCY SERVICES	29/08/2014	2,805.00
EF080398	24558	<b>MACQUARIE BANK LIMITED</b> LEASE REPAYMENT	29/08/2014	9,083.27
EF080399	24594	<b>THE GREEN ROOM CREATIVE PTY LTD</b> GRAPHIC DESIGN SERVICES	29/08/2014	1,122.00
EF080400	24643	<b>BIBLIOTHECA RFID LIBRARY SYSTEMS AUSTRALIA PTY LTD</b> PURCHASE OF LIBRARY TAGS	29/08/2014	208,960.00
EF080401	24655	<b>AUTOMASTERS SPEARWOOD</b> VEHICLE SERVICING	29/08/2014	6,345.00
EF080402	24724	<b>QUALITY MARINE COATING SYSTEMS P/L</b> CLEANING SERVICES - ROAD SURFACES	29/08/2014	2,860.00
EF080403	24736	<b>ZENIEN</b> CCTV CAMERA LICENCES	29/08/2014	277.75
EF080404	24805	<b>KAREN WOOLHEAD</b> DANCING CLASSES	29/08/2014	1,120.00
EF080405	24862	<b>BEARDS SECURITY</b> SECURITY SERVICES	29/08/2014	400.00
EF080406	24945	<b>NS PROJECTS PTY LTD</b> PROJECT MANAGEMENT SERVICES	29/08/2014	16,500.00

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Cheque/ EFT	Account No.	Account/Payee	Date	Value
EF080407	24946	<b>WT PARTNERSHIP</b> QUANTITY SURVEYING SERVICES	29/08/2014	5,500.00
EF080408	24954	<b>FREMANTLE FURY NETBALL CLUB</b> REGISTRATION FEES	29/08/2014	400.00
EF080409	24962	<b>EVERGLAZE</b> PAINTING SERVICES	29/08/2014	1,430.44
EF080410	24973	<b>BLUESTONE RECRUITMENT</b> TEMPORARY PERSONNEL SERVICES	29/08/2014	32,417.83
EF080411	24974	<b>SCOTT PRINT</b> PRINTING SERVICES	29/08/2014	13,563.00
EF080412	24976	<b>SNAP PRINTING - COCKBURN CENTRAL</b> PRINTING SERVICES	29/08/2014	986.60
EF080413	24980	<b>WILLAGEE RUGBY LEAGUE FOOTBALL CLUB</b> REGISTRATION FEES	29/08/2014	800.00
EF080414	25063	<b>SUPERIOR PAK PTY LTD</b> VEHICLE MAINTENANCE	29/08/2014	498.75
EF080415	25072	<b>SANPRO CONSTRUCTION PTY LTD</b> CONSTRUCTION SERVICES	29/08/2014	63,025.61
EF080416	25074	<b>BMT OCEANICA PTY LTD</b> CONSULTANCY SERVICES - ENVIRONMENTAL	29/08/2014	8,133.95
EF080417	25102	<b>FREMANTLE MOBILE WELDING</b> WELDING SERVICES	29/08/2014	5,049.00
EF080418	25115	<b>FIG</b> INVESTMENT MANAGEMENT SERVICES	29/08/2014	2,750.00
EF080419	25121	<b>IMAGESOURCE DIGITAL SOLUTIONS</b> BILLBOARDS	29/08/2014	3,218.05
EF080420	25158	<b>MPIRE SECURITY</b> SECURITY SERVICES	29/08/2014	6,272.05
EF080421	25190	<b>GARBOLOGIE</b> MATTRESS RECYCLING	29/08/2014	15,970.90
EF080422	25262	<b>SANDOVER PINDER ARCHITECTS</b> ARCHITECTURAL SERVICES	29/08/2014	262,704.44
EF080423	25263	<b>SYSTEM MAINTENANCE</b> SEWERAGE PUMP MAINTENANCE	29/08/2014	3,932.76
EF080424	25335	<b>JAXON PTY LTD</b> BUILDING CONSTRUCTION SERVICES	29/08/2014	577,092.20
EF080425	25385	<b>CREATIVE PATHWAYS</b> STORYTELLING WORKSHOPS	29/08/2014	825.00
EF080426	25415	<b>JANDAKOT STOCK &amp; PET SUPPLIES</b> PET SUPPLIES	29/08/2014	276.50
EF080427	25418	<b>CS LEGAL</b> LEGAL SERVICES	29/08/2014	13,485.28
EF080428	25494	<b>STEVE'S ALL-TRADE'S SOLUTIONS</b> PAINTING AND ROOF SUPPLIES	29/08/2014	1,210.00
EF080429	25539	<b>BROWN CONSULTING (VIC) PTY LTD</b> CONSULTANCY SERVICES	29/08/2014	17,600.00
EF080430	25540	<b>JOHN MASSEY GROUP PTY LTD</b> BUILDING SURVEYING SERVICES	29/08/2014	5,500.00
EF080431	25586	<b>ENVIROVAP</b> HIRE OF LEACHATE UNITS	29/08/2014	11,275.00
EF080432	25588	<b>CIVCO MINING SERVICES PTY LTD</b> PLANT / EQUIPMENT HIRE	29/08/2014	10,797.60
EF080433	25636	<b>CENTRE FOR PAVEMENT ENGINEERING EDUCATION</b> TRAINING SERVICES	29/08/2014	2,970.00

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<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF080434	25644	<b>DYMOCKS GARDEN CITY</b> PURCHASE OF BOOKS	29/08/2014	3,040.42
EF080435	25657	<b>LOCK JOINT AUSTRALIA</b> LOCKSMITH SERVICES	29/08/2014	4,708.00
EF080436	25676	<b>STEPPING TOWARD SUCCESS</b>	29/08/2014	250.00
EF080437	25713	<b>DISCUS ON DEMAND</b> PRINTING SERVICES	29/08/2014	605.00
EF080438	25720	<b>SOUTH WESTERN SYDNEY MEDICARE LOCAL</b> PURCHASE OF MANUALS	29/08/2014	374.00
EF080439	25733	<b>MIRACLE RECREATION EQUIPMENT</b> PLAYGROUND REPAIRS	29/08/2014	6,556.00
EF080440	25736	<b>EMERGE ASSOCIATES</b> CONSULTANCY SERVICES	29/08/2014	32,697.50
EF080441	25747	<b>JONATHAN LAKE ARCHITECTS</b> ARCHITECT	29/08/2014	8,750.00
EF080442	25796	<b>TABLE &amp; CHAIR COMPANY</b> FURNITURE SUPPLIES	29/08/2014	121,334.55
EF080443	25800	<b>OPTUM HEALTH &amp; TECHNOLOGY (AUST) PTY LTD</b> CONSULTANCY SERVICES	29/08/2014	18,040.00
EF080444	25940	<b>LEAF BEAN MACHINE</b> COFFEE BEAN SUPPLY	29/08/2014	800.00
EF080445	25944	<b>ABBIN ENGINEERING PTY LTD</b> OFFICE FURNITURE	29/08/2014	9,699.80
EF080446	25945	<b>CH2</b> OFFICE FURNITURE	29/08/2014	655.49
EF080447	25946	<b>K CARE HEALTHCARE EQUIPMENT</b> HEALTHCARE EQUIPMENT	29/08/2014	13,915.00
EF080448	25949	<b>BAPTIST CHURCHES OF WA</b> REGISTRATION FEES	29/08/2014	200.00
EF080449	25955	<b>ADECCO</b> EMPLOYEMENT SERVICES	29/08/2014	5,550.13
EF080450	25972	<b>CASTLEDEX PTY LTD</b> OFFICE FURNITURE	29/08/2014	9,077.20
EF080451	25981	<b>EAST FREMANTLE LACROSSE CLUB INC</b> BUILDING LEVY	29/08/2014	275.00
EF080452	26025	<b>NORMA MACDONALD</b> STORY TELLING SERVICES	29/08/2014	660.00
EF080453	26046	<b>KNOWLEDGE</b> ONLINE SURVEY SERVICES	29/08/2014	5,918.22
EF080454	26062	<b>BP BEELIAR</b> BP BEELIAR	29/08/2014	9,900.00
EF080455	26090	<b>FREMANTLE MILK DISTRIBUTORS</b> MILK DISTRIBUTORS	29/08/2014	629.90
EF080456	26100	<b>LINDA'S BOOKS ROLEYSTONE COURIER</b> ENTERTAINMENT SERVICES	29/08/2014	400.00
EF080457	26102	<b>VIXENS NETBALL CLUB</b> NETBALL REGISTRATION FEES	29/08/2014	800.00
EF080458	26107	<b>JOSIE BOYLE</b> STORYTELLING & CULTURAL ACTIVITIES	29/08/2014	250.00
EF080459	26111	<b>ASSETVAL PTY LTD</b> LAND VALUATION SERVICES	29/08/2014	24,525.01
EF080460	11794	<b>SYNERGY</b> ELECTRICITY USAGE/SUPPLIES	29/08/2014	321,567.13

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EF080461	12025	<b>TELSTRA CORPORATION</b> COMMUNICATIONS SERVICES	29/08/2014	42,596.55
EF080462	14241	<b>PAUL STANNERS &amp; CASEY STANNERS</b> OVERPAYMENT	29/08/2014	590.00
EF080463	22831	<b>JAMES SHARPE</b> BIRD BATH REBATE	29/08/2014	37.50
EF080464	26103	<b>SHEREE BOYD</b> DOG REGISTRATION REFUND	29/08/2014	114.00
EF080465	26124	<b>TARA PEARCE</b> BIRD BATH REBATE	29/08/2014	50.00
EF080466	26125	<b>JACINTA VASILE</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
EF080467	26126	<b>DB &amp; LA WHITTAKER</b> RATES REFUND	29/08/2014	255.00
EF080468	26136	<b>BETHEL HUTCHINSON</b> BIRD BATH REBATE	29/08/2014	30.10
EF080469	26137	<b>JOYCE DIX</b> BIRD BATH REBATE	29/08/2014	37.50
EF080470	26138	<b>PATRICIA BYRNE</b> BIRD BATH REBATE	29/08/2014	37.50
EF080471	26139	<b>SIMON WEE</b> BIRD BATH REBATE	29/08/2014	34.50
EF080472	26141	<b>ANNA HAEBICH</b> OVERPAYMENT OF RATES	29/08/2014	12,000.00
EF080473	26142	<b>JOSIP SKAJKO</b> DOUBLE PAYMENT BY BPAY	29/08/2014	1,176.79
EF080474	26143	<b>SERENA PARKER</b> DOG REGISTRATION REFUND	29/08/2014	38.75
EF080475	26149	<b>MICHAEL YAP</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
EF080476	26150	<b>VIDOJE NERIC</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
EF080477	26151	<b>JAMJ PTY LTD</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
EF080478	26152	<b>RISHI GIRISH BAROT</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
EF080479	26153	<b>HEBE THOMAS</b> COUNCIL CROSSOVER CONTRIBUTION	29/08/2014	300.00
026057	13932	<b>ARMAGUARD</b> BANKING SERVICES	7/08/2014	2,441.30
026058	13932	<b>ARMAGUARD</b> BANKING SERVICES	14/08/2014	2,886.75
026059	13932	<b>ARMAGUARD</b> BANKING SERVICES	21/08/2014	2,939.70
026060	13932	<b>ARMAGUARD</b> BANKING SERVICES	27/08/2014	2,368.25
026061	10589	<b>FINES ENFORCEMENT REGISTRY</b> FINES ENFORCEMENT FEES	27/08/2014	1,258.20
026062	10747	<b>IINET LIMITED</b> INTERNET SERVICES	29/08/2014	499.65
026063	11760	<b>WATER CORPORATION</b> SEWER EASEMENT	29/08/2014	2,734.00
026064	12351	<b>CITY OF NEDLANDS</b> LIBRARY SUPPLIES	29/08/2014	13.20

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026065	20110	<b>TOWN OF CAMBRIDGE</b> LIBRARY BOOKS	29/08/2014	10.10
026066	14396	<b>A &amp; M CARDOSO</b> RATES REFUND	29/08/2014	60.00
026067	20679	<b>OFFICE OF STATE REVENUE</b> RATES REFUND	29/08/2014	32.54
026068	26127	<b>MARGARET KING</b> RATES REFUND	29/08/2014	444.15
026069	26128	<b>IRENE WADDELL</b> RATES REFUND	29/08/2014	218.54
026070	26129	<b>N &amp; P PADOVAN</b> RATES REFUND	29/08/2014	458.25
026071	26130	<b>DOROTHY ANDERSON</b> RATES REFUND	29/08/2014	421.58
026072	26131	<b>MJ &amp; E HUGHES</b> RATES REFUND	29/08/2014	662.70
026073	26132	<b>MARGARET TINNEY</b> RATES REFUND	29/08/2014	354.04
026074	26133	<b>JONNEEN COMPASSI</b> RATES REFUND	29/08/2014	620.39
026075	26134	<b>MM &amp; NJ VIDAL</b> RATES REFUND	29/08/2014	356.47
026076	26135	<b>LN CAMPBELL &amp; C CAMPBELL</b> RATES REFUND	29/08/2014	330.64
026077	26140	<b>COLIN &amp; MAXINE TATE</b> PENSION REBATE	29/08/2014	460.30
026078	10047	<b>ALINTA ENERGY</b> GAS SUPPLIES	29/08/2014	1,979.50
026079	11758	<b>WATER CORP</b> WATER USAGE SUPPLIES <b>ADD RETENTION HELD</b> NIL <b>LESS PRIOR PERIOD CANCELLED CHEQUES/EFTS</b>	29/08/2014	24,453.53
026054	25960	FITMONKY		-1,200.00
EF079469	26080	JANESSA RUNDLE		-300.00
EF079728	15862	FREMANTLE MILK DISTRIBUTORS		-931.85
EF080009	16058	SHOP-A-DOCKET PTY LTD		-1,628.00
		<b>PAYMENT LIST TOTAL</b>		#####
		<b>TOTAL AS PER AP SOURCE 14GLACT9991000</b>		#####
		<b>TOTAL AS PER TR SOURCE 14GLACT9991000</b>		#####
		<b>ADDITIONAL DIRECT PAYMENTS</b>		
		<b>BANK FEES</b>		
		MERCHANT FEES COC		21,529.27
		MERCHANT FEES SLLC		4,143.10
		MERCHANT FEES VARIOUS OUT CENTRES		226.70
		NATIONAL BPAY CHARGE		6,680.38
		RTGS/ACLR FEE		-
		NAB TRANSACT FEE		684.13
				<b>33,263.58</b>

CITY OF COCKBURN  
MUNICIPAL BANK ACCOUNT

Cheque/ EFT	Account No.	Account/Payee	Date	Value
		<b>FAMILY DAY CARE AND IN HOME CARE PAYMENTS</b>		
		FDC PAYMENTS		61,633.27
		IHC PAYMENTS		97,135.46
				#####
		<b>PAYROLL TRANSACTIONS</b>		
		COC 01/08/14 CITY OF COCKBURN 042958		15026.25
		COC 07/08/14 CITY OF COCKBURN 042958		4580.26
		COC 12/08/14 CITY OF COCKBURN 042958		11714.6
		COC 12/08/14 CITY OF COCKBURN 042958		1005063.4
		COC 13/08/14 CITY OF COCKBURN 042958		2496.88
		COC 15/08/14 CITY OF COCKBURN 042958		7531.75
		COC 20/08/14 CITY OF COCKBURN 042958		10217.14
		COC 26/08/14 CITY OF COCKBURN 042958		6256.58
		COC 26/08/14 CITY OF COCKBURN 042958		1097584.92
		COC 12/08/14 CITY OF COCKBURN 042958 HARRIS Resend		1878.69
				#####
		<b>CREDIT CARD PAYMENTS</b>		
		CBA CREDIT CARD PAYMENT		55,330.67
				<b>55,330.67</b>
				#####
		<b>TOTAL PAYMENTS FOR AUGUST</b>		#####

**PAYMENT SUMMARY**

**CHEQUE PAYMENTS**

026057 - -026079

**CANCELLED PAYMENTS**

Nil.

**ELECTRONIC FUNDS TRANSFER PAYMENT**

EF080002 – EF080479

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 August 2014

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Operating Revenue</b>						
Governance	64,772,260	65,045,376	0%	(273,116) X	74,108,927	74,022,182
Financial Services	141,036	109,508	29%	31,527	657,050	657,050
Information Services	90	375	-76%	(285)	1,500	1,500
Human Resource Management	27,790	14,333	94%	13,457	86,000	86,000
Library Services	7,871	8,891	-11%	(1,020)	53,346	53,346
Community Services	3,363,789	3,364,840	0%	(1,051)	7,345,732	7,338,204
Human Services	1,403,025	1,278,425	10%	124,600	6,459,383	6,459,383
Corporate Communications	29	7,400	-100%	(7,371)	12,500	12,500
Development Services	937,533	880,903	6%	56,630	3,374,215	3,374,215
Planning Services	639,145	669,026	-4%	(29,881)	1,471,943	1,471,943
Waste Services	20,135,844	20,215,731	0%	(79,887)	29,153,124	29,143,124
Parks & Environmental Services	9,903	317	3027%	9,586	1,900	1,900
Engineering Services	82,668	48,894	69%	33,774	303,363	293,363
Infrastructure Services	1,602	550	192%	1,052	3,297	3,297
	91,522,583	91,644,568	0%	(121,985)	123,032,280	122,918,007
<b>Total Operating Revenue</b>	<b>91,522,583</b>	<b>91,644,568</b>	<b>0%</b>	<b>(121,985)</b>	<b>123,032,280</b>	<b>122,918,007</b>
<b>Operating Expenditure</b>						
Governance	(554,390)	(739,232)	-25%	184,841	(4,688,532)	(4,633,859)
Financial Services	(1,662,061)	(1,600,552)	4%	(61,508)	(5,504,284)	(5,464,284)
Information Services	(648,371)	(1,010,869)	-36%	362,497 ✓	(4,407,974)	(4,385,908)
Human Resource Management	(481,468)	(371,722)	30%	(109,746)	(2,312,028)	(2,302,028)
Library Services	(443,687)	(515,481)	-14%	71,793	(3,185,897)	(3,168,305)
Community Services	(1,102,404)	(1,351,114)	-18%	248,711 ✓	(9,511,031)	(9,490,807)
Human Services	(1,110,683)	(1,257,027)	-12%	146,344	(7,824,295)	(7,729,708)
Corporate Communications	(316,170)	(313,614)	1%	(2,556)	(2,706,953)	(2,682,290)
Development Services	(736,718)	(722,856)	2%	(13,862)	(4,661,122)	(4,624,505)
Planning Services	(198,027)	(219,802)	-10%	21,775	(1,587,702)	(1,566,420)
Waste Services	(2,321,733)	(3,027,105)	-23%	705,373 ✓	(20,076,655)	(20,076,655)
Parks & Environmental Services	(1,376,254)	(1,808,064)	-24%	431,809 ✓	(11,365,867)	(11,330,867)
Engineering Services	(1,321,051)	(1,287,350)	3%	(33,701)	(7,997,243)	(7,985,243)
Infrastructure Services	(971,993)	(1,366,684)	-29%	394,690 ✓	(8,278,904)	(8,195,699)
	(13,245,011)	(15,591,472)	-15%	2,346,461	(94,108,486)	(93,636,577)

OCM 9/10/2014 - Item 15.2

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 August 2014

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Less:</b> Net Internal Recharging	539,910	533,410	1%	6,499	3,243,783	3,243,783
<b>Add:</b> Depreciation on Non-Current Assets						
Computer & Electronic Equip	(34,871)	(15,740)	122%	(19,131)	(94,440)	(94,440)
Furniture & Equipment	(28,125)	(26,069)	8%	(2,056)	(156,413)	(156,413)
Plant & Machinery	(498,230)	(494,878)	1%	(3,352)	(2,969,268)	(2,969,268)
Buildings	(710,276)	(710,276)	0%	-	(4,261,656)	(4,261,656)
Roads	(1,577,992)	(1,577,992)	0%	-	(9,467,952)	(9,467,952)
Drainage	(381,678)	(381,678)	0%	-	(2,290,068)	(2,290,068)
Footpaths	(193,558)	(193,558)	0%	-	(1,161,348)	(1,161,348)
Parks Equipment	(559,002)	(559,002)	0%	-	(3,354,012)	(3,354,012)
	<b>(3,983,732)</b>	<b>(3,959,193)</b>	<b>1%</b>	<b>(24,539)</b>	<b>(23,755,157)</b>	<b>(23,755,157)</b>
<b>Total Operating Expenditure</b>	<b>(16,688,833)</b>	<b>(19,017,254)</b>	<b>-12%</b>	<b>2,328,422</b>	<b>(114,619,860)</b>	<b>(114,147,951)</b>
<b>Change in Net Assets Resulting from Operations</b>	<b>74,833,751</b>	<b>72,627,314</b>	<b>3%</b>	<b>2,206,437</b>	<b>8,412,420</b>	<b>8,770,056</b>
<b>Non-Operating Activities</b>						
<b>Profit/(Loss) on Assets Disposal</b>						
Plant & Machinery	32,727	85,074	-62%	(52,347)	(346,947)	(582,947)
Freehold Land	-	550,454	-100%	(550,454) X	5,172,727	1,150,000
Furniture & Office Equipment	-	-	0%	-	-	-
Buildings	-	-	0%	-	-	-
	<b>32,727</b>	<b>635,528</b>	<b>-95%</b>	<b>(602,801)</b>	<b>4,825,780</b>	<b>567,053</b>
<b>Less:</b> Underground Power Infrastructure Contribution	-	(574)	-100%	574	(574)	-
<b>Asset Acquisitions</b>						
Land and Buildings	(1,964,940)	(3,318,661)	-41%	1,353,721 ✓	(33,104,756)	(24,387,000)
Infrastructure Assets	(1,447,027)	(3,326,023)	-56%	1,878,996 ✓	(29,934,206)	(17,116,028)
Plant and Machinery	(539,601)	(1,676,500)	-68%	1,136,899 ✓	(5,591,500)	(4,007,500)
Furniture and Equipment	(4,797)	(11,000)	-56%	6,203	(11,000)	(206,000)
Computer Equipment	(192,382)	(521,500)	-63%	329,118 ✓	(1,191,595)	(434,000)
	<b>(4,148,746)</b>	<b>(8,853,683)</b>	<b>-53%</b>	<b>4,704,937</b>	<b>(69,833,057)</b>	<b>(46,150,528)</b>
<b>Note 1.</b>						
<b>Add:</b> Transfer to Reserves	(2,094,145)	(2,608,113)	-20%	513,967 ✓	(39,556,836)	(35,534,109)
	<b>68,623,586</b>	<b>61,800,472</b>	<b>11%</b>	<b>6,823,114</b>	<b>(96,152,268)</b>	<b>(72,347,529)</b>

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 August 2014

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Add Funding from</b>						
Grants & Contributions - Asset Development	2,676,554	832,500	222%	1,844,054 ✓	7,722,142	6,726,309
Less: held in restricted funds from prior years	-	-	0%	-	-	-
Proceeds on Sale of Assets	32,727	899,579	-96%	(866,852) ✗	6,783,852	2,525,125
Reserves	5,442,092	7,247,424	-25%	(1,805,333) ✗	32,307,190	18,281,347
Loan Funds Raised	-	-	0%	-	20,000,000	20,000,000
	<b>76,774,959</b>	<b>70,779,975</b>	<b>8%</b>	<b>5,994,984</b>	<b>(29,339,085)</b>	<b>(24,814,748)</b>
<b>Non-Cash/Non-Current Item Adjustments</b>						
Depreciation on Assets	3,983,732	3,959,193	1%	24,539	23,755,157	23,755,157
Profit/(Loss) on Assets Disposal	(32,727)	(635,528)	-95%	602,801 ✓	(4,825,780)	(567,053)
Loan Repayments	-	-	0%	-	(1,373,356)	(1,373,356)
Non-Current Leave Provisions	31,253	-	0%	31,253	-	-
	<b>80,757,217</b>	<b>74,103,639</b>	<b>9%</b>	<b>6,653,577</b>	<b>(11,783,063)</b>	<b>(3,000,000)</b>
Opening Funds	13,192,331	11,890,675	11%	1,301,656	11,890,675	3,000,000
<b>Closing Funds</b>	<b>93,949,548</b>	<b>85,994,314</b>	<b>9%</b>	<b>7,955,233</b>	<b>107,612</b>	<b>-</b>
	-	-		-	-	-

Note 2, 3.

## Notes to Statement of Financial Activity

### Note 1.

Additional information on the capital works program including committed orders at end of month:

	Actuals	Commitments at Month End	Commitments & Actuals YTD	YTD Revised Budget	Full Year Revised Budget	Uncommitted at Month End
	\$	\$			\$	\$
<b>Assets Classification</b>						
Land and Buildings	(1,964,940)	(2,224,862)	(4,189,801)	(3,318,661)	(33,104,756)	28,914,955
Infrastructure Assets	(1,447,027)	(2,629,962)	(4,076,989)	(3,326,023)	(29,934,206)	25,857,217
Plant and Machinery	(539,601)	(985,619)	(1,525,220)	(1,676,500)	(5,591,500)	4,066,280
Furniture and Equipment	(4,797)	-	(4,797)	(11,000)	(11,000)	6,203
Computer Equipment	(192,382)	(225,535)	(417,917)	(521,500)	(1,191,595)	773,678
	(4,148,746)	(6,065,978)	(10,214,724)	(8,853,683)	(69,833,057)	59,618,333

### Note 2.

Closing Funds in the Financial Activity Statement are represented by:

	Actuals	YTD Revised Budget	Full Year Revised Budget	Adopted Budget
	\$	\$	\$	\$
<b>Current Assets</b>				
Cash & Investments	126,554,154	166,049,042	93,895,460	86,462,923
Rates Outstanding	47,159,376	900,000	900,000	900,000
Rubbish Charges Outstanding	9,710,466	300,000	300,000	300,000
Sundry Debtors	11,237,004	2,700,000	2,700,000	2,700,000
GST Receivable	836,715	-	-	-
Prepayments	552,255	350,000	350,000	350,000
Accrued Debtors	308,074	-	-	-
Stock on Hand	723	20,000	20,000	20,000
	196,358,767	170,319,042	98,165,460	90,732,923
<b>Current Liabilities</b>				
Creditors	(15,985,705)	(3,641,948)	(5,000,496)	(5,000,496)
Income Received in Advance	-	52,856	52,856	52,856
GST Payable	(328,696)	-	-	-
Withholding Tax Payable	-	-	-	-
Provision for Annual Leave	(2,631,520)	(2,000,000)	(2,000,000)	(2,000,000)
Provision for Long Service Leave	(1,854,079)	(2,110,367)	(2,595,980)	(2,595,980)
	(20,800,000)	(7,699,459)	(9,543,620)	(9,543,620)
<b>Net Current Assets</b>	175,558,767	162,619,584	88,621,840	81,189,303
<b>Add: Non Current Investments</b>	4,384,209	4,339,420	4,339,420	4,339,420
	179,942,976	166,959,004	92,961,260	85,528,723
<b>Less: Restricted/Committed Assets</b>				
Cash Backed Reserves #	(82,240,617)	(80,964,690)	(92,853,648)	(85,528,723)
Deposits & Bonds Liability *	(3,752,811)	(2,789,342)	(2,789,342)	(2,789,342)
Grants & Contributions Unspent *	-	2,789,343	2,789,343	2,789,343
	93,949,548	85,994,314	107,612	-
<b>Closing Funds (as per Financial Activity Statement)</b>	<b>93,949,548</b>	<b>85,994,314</b>	<b>107,612</b>	<b>-</b>

# See attached Reserve Fund Statement

\* See attached Restricted Funds Analysis

Note 3.

Amendments to original budget since budget adoption. Surplus/(Deficit)

Ledger	Project/ Activity	Description	Council Resolution	Classification	Non Change (Non Cash Items) Adjust.	Increase in Available Cash	Decrease in Available Cash	Amended budget Running Balance
					\$	\$	\$	\$
		<b>Budget Adoption</b>		<b>Closing Funds Surplus(Deficit)</b>				<b>0</b>
GL	830	Increase conference budget		Operating Expenditure			2,000	(2,000)
GL	594	Salary reduction due to system error		Operating Expenditure		18,369		16,369
GL	105	Increase in FAGS grant		Operating Income		86,745		103,114
GL	161, 162,							
GL	175	Reallocating FESA grants and expenditure		Operating Income		4,498		107,612
				<b>Closing Funds Surplus (Deficit)</b>	<b>0</b>	<b>109,612</b>	<b>2,000</b>	<b>107,612</b>

# Statement of Comprehensive Income *by Nature and Type*

for the period ended 31 August 2014

	Actual	Amended YTD Budget	\$ Variance to YTD Budget	Forecast	Amended Budget	Adopted Budget
	\$	\$	\$	\$	\$	\$
<b>OPERATING REVENUE</b>						
01 Rates	61,735,780	61,746,667	(10,886)	62,869,114	62,880,000	62,880,000
02 Specified Area Rates	248,806	270,000	(21,194)	248,806	270,000	270,000
05 Fees and Charges	22,622,715	22,587,514	35,200	39,744,111	39,708,911	39,708,911
06 Service Charges	3,832,067	3,939,752	(107,686)	3,832,067	4,000,000	4,000,000
10 Grants and Subsidies	2,152,697	2,071,161	81,537	9,511,575	9,430,038	9,325,765
15 Contributions, Donations and Reimbursements	69,869	71,782	(1,914)	364,926	366,840	356,840
20 Interest Earnings	858,719	956,508	(97,790)	6,271,601	6,369,391	6,369,391
25 Other revenue and Income	1,931	1,183	747	7,847	7,100	7,100
<b>Total Operating Revenue</b>	<b>91,522,583</b>	<b>91,644,568</b>	<b>(121,985)</b>	<b>122,850,048</b>	<b>123,032,280</b>	<b>122,918,007</b>
<b>OPERATING EXPENDITURE</b>						
50 Employee Costs - Salaries & Direct Oncosts	(6,645,569)	(6,515,838)	(129,730)	(42,819,523)	(42,689,793)	(42,697,487)
51 Employee Costs - Indirect Oncosts	(86,117)	(94,944)	8,827	(895,640)	(904,466)	(898,966)
55 Materials and Contracts	(3,640,863)	(5,959,535)	2,318,672	(33,633,236)	(35,951,908)	(35,536,617)
65 Utilities	(887,218)	(754,127)	(133,091)	(4,650,235)	(4,517,144)	(4,513,005)
70 Interest Expenses	-	-	-	(123,300)	(123,300)	(123,300)
75 Insurances	(1,142,854)	(1,122,551)	(20,303)	(2,360,803)	(2,340,500)	(2,340,500)
80 Other Expenses	(842,390)	(1,144,476)	302,086	(7,279,289)	(7,581,375)	(7,526,702)
85 Depreciation on Non Current Assets	(3,983,732)	(3,959,193)	(24,539)	(23,779,696)	(23,755,157)	(23,755,157)
<b>Add Back: Indirect Costs Allocated to Capital Works</b>	<b>539,910</b>	<b>533,410</b>	<b>6,499</b>	<b>3,250,282</b>	<b>3,243,783</b>	<b>3,243,783</b>
<b>Total Operating Expenditure</b>	<b>(16,688,833)</b>	<b>(19,017,254)</b>	<b>2,328,422</b>	<b>(112,291,439)</b>	<b>(114,619,860)</b>	<b>(114,147,951)</b>
<b>CHANGE IN NET ASSETS RESULTING FROM OPERATING ACTIVITIES</b>						
	<b>74,833,751</b>	<b>72,627,314</b>	<b>2,206,437</b>	<b>10,558,609</b>	<b>8,412,420</b>	<b>8,770,056</b>
<b>NON-OPERATING ACTIVITIES</b>						
11 Capital Grants & Subsidies	352,500	332,500	20,000	3,377,220	3,357,220	2,361,387
16 Contributions - Asset Development	2,324,054	500,000	1,824,054	6,188,976	4,364,922	4,364,922
95 Profit/(Loss) on Sale of Assets	32,727	635,528	(602,801)	4,222,978	4,825,780	567,053
57 Acquisition of Crown Land for Roads	-	-	-	-	-	-
58 Underground Power Scheme	-	(574)	574	-	(574)	-
<b>Total Non-Operating Activities</b>	<b>2,709,282</b>	<b>1,467,454</b>	<b>1,241,827</b>	<b>13,789,175</b>	<b>12,547,348</b>	<b>7,293,362</b>
<b>NET RESULT</b>	<b>77,543,032</b>	<b>74,094,768</b>	<b>3,448,264</b>	<b>24,347,783</b>	<b>20,959,767</b>	<b>16,063,417</b>

## Notes to Statement of Comprehensive Income

### Note 1.

Additional information on main sources of revenue in fees & charges.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
<b><u>Community Services:</u></b>				
Recreational Services	103,881	89,053	534,320	534,320
South Lake Leisure Centre	346,556	370,998	2,988,286	2,988,286
Law and Public Safety	103,543	64,749	388,496	388,496
	553,980	524,801	3,911,102	3,911,102
<b><u>Waste Services:</u></b>				
Waste Collection Services	18,522,466	18,471,060	18,695,101	18,695,101
Waste Disposal Services	1,612,191	1,741,337	10,448,023	10,448,023
	20,134,657	20,212,397	29,143,124	29,143,124
	20,688,637	20,737,198	33,054,226	33,054,226

### Note 2.

Additional information on Salaries and Direct On-Costs by each Division.

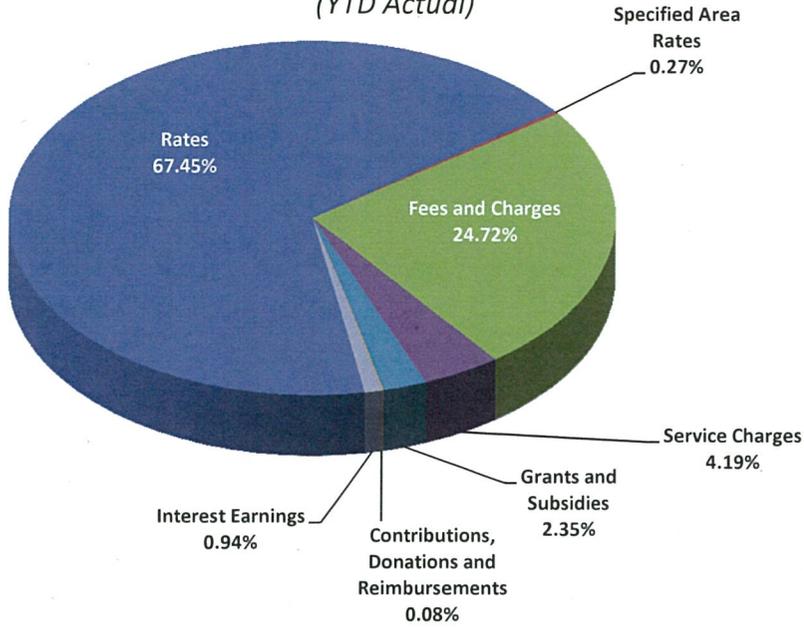
	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(283,529)	(294,403)	(1,919,506)	(1,919,506)
Finance & Corporate Services Division	(1,120,102)	(932,955)	(6,145,420)	(6,145,420)
Community Services Division	(1,978,282)	(1,980,667)	(13,189,123)	(13,196,817)
Planning & Development Division	(807,110)	(774,924)	(5,024,097)	(5,024,097)
Engineering & Works Division	(2,456,545)	(2,532,889)	(16,411,647)	(16,411,647)
	(6,645,569)	(6,515,838)	(42,689,793)	(42,697,487)

### Note 3

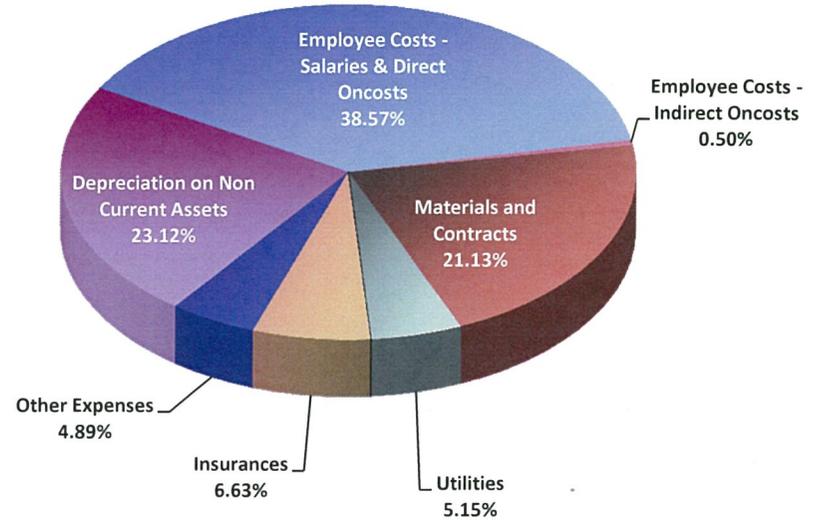
Additional information on Materials and Contracts by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(158,031)	(340,142)	(2,087,788)	(2,087,788)
Finance & Corporate Services Division	(386,089)	(871,368)	(3,341,779)	(3,269,713)
Community Services Division	(784,578)	(1,232,717)	(7,841,255)	(7,649,134)
Planning & Development Division	(125,193)	(160,639)	(1,123,794)	(1,065,895)
Engineering & Works Division	(2,186,972)	(3,354,668)	(21,557,292)	(21,464,087)
Not Applicable	0	0	0	0
	(3,640,863)	(5,959,535)	(35,951,908)	(35,536,617)

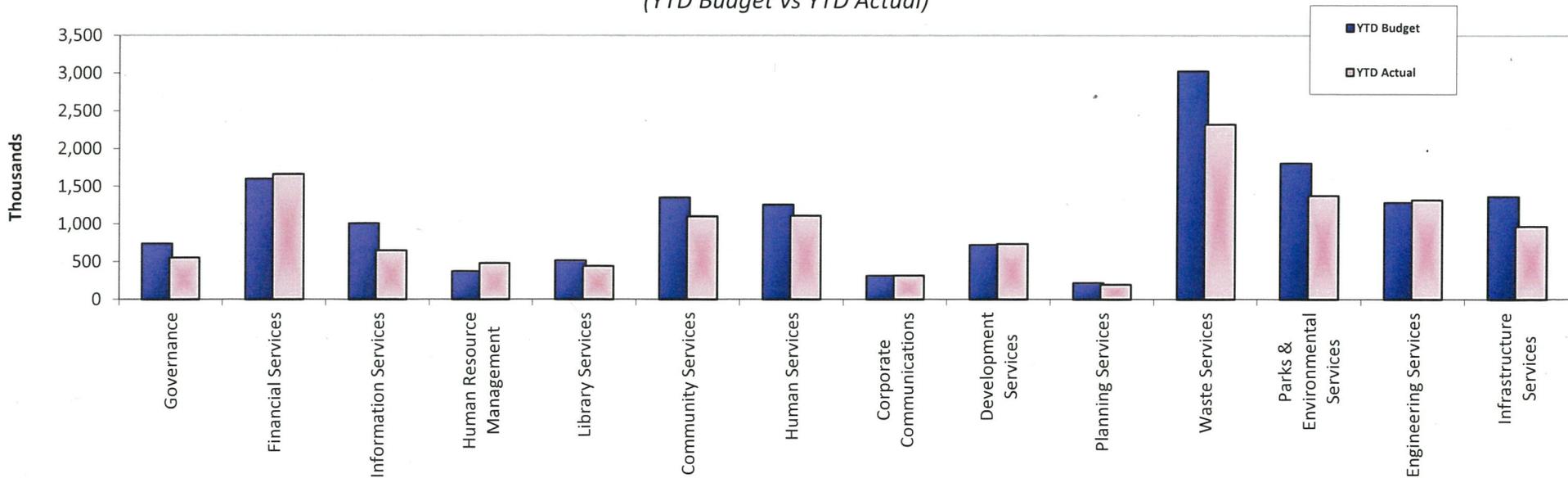
**Operating Income by Nature and Type**  
(YTD Actual)



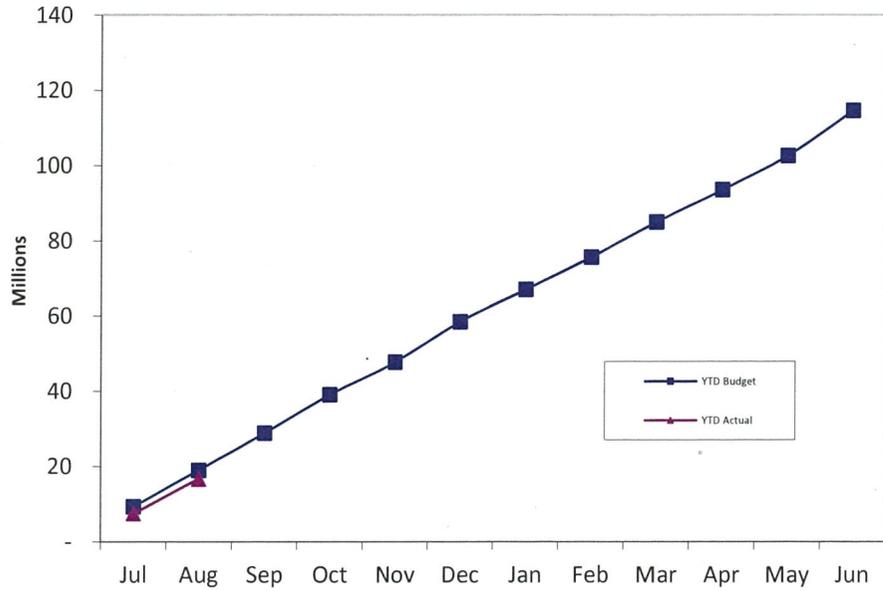
**Operating Expenditure by Nature and Type**  
(YTD Actual)



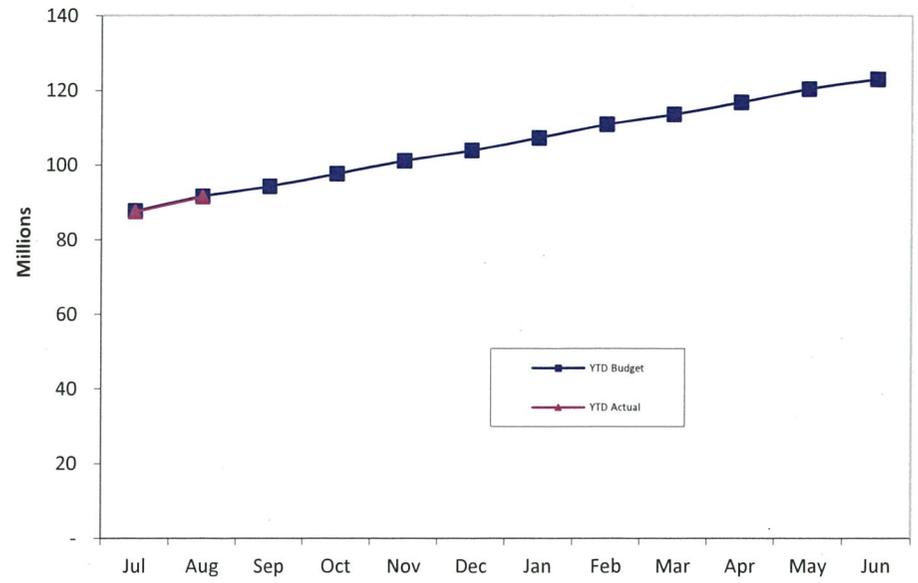
**Operating Expenditure by Business Unit**  
(YTD Budget vs YTD Actual)



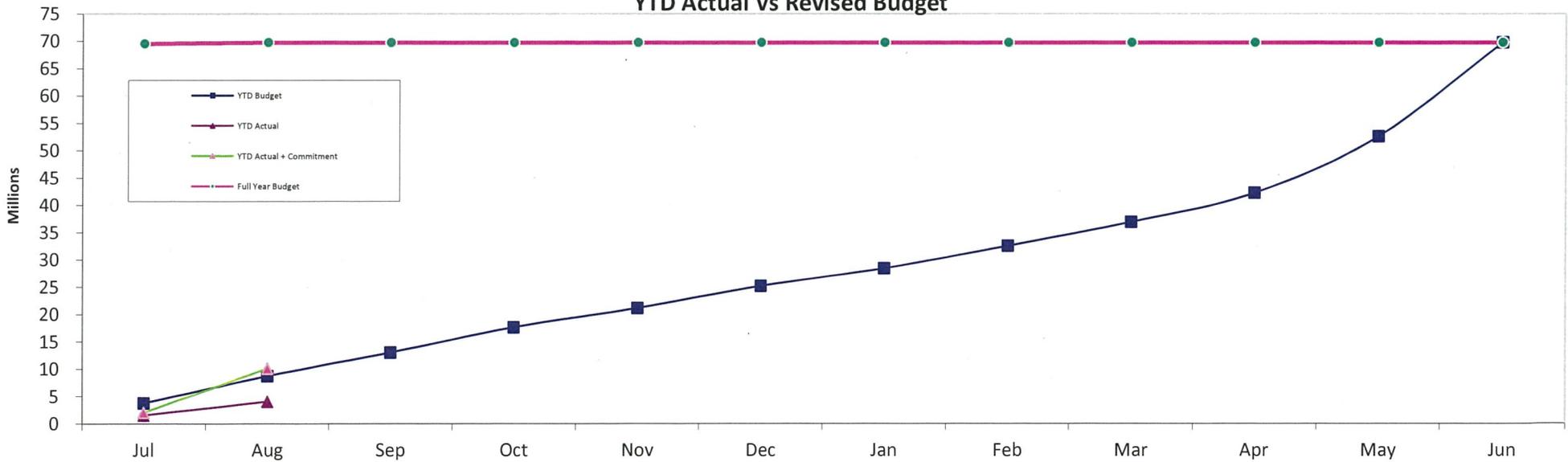
**YTD Operating Expenditure Vs YTD Revised Budget**



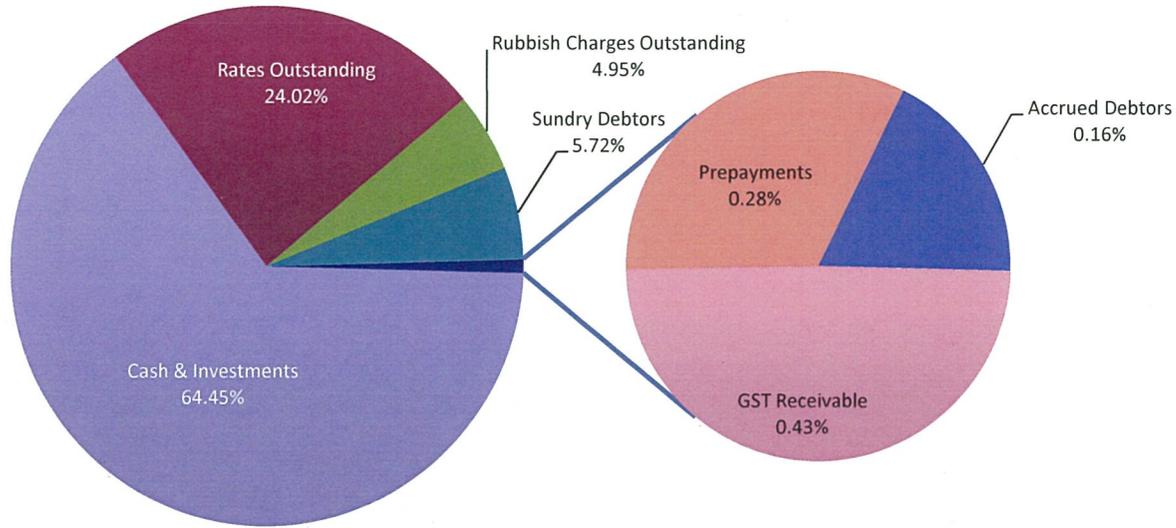
**YTD Operating Income Vs YTD Revised Budget**



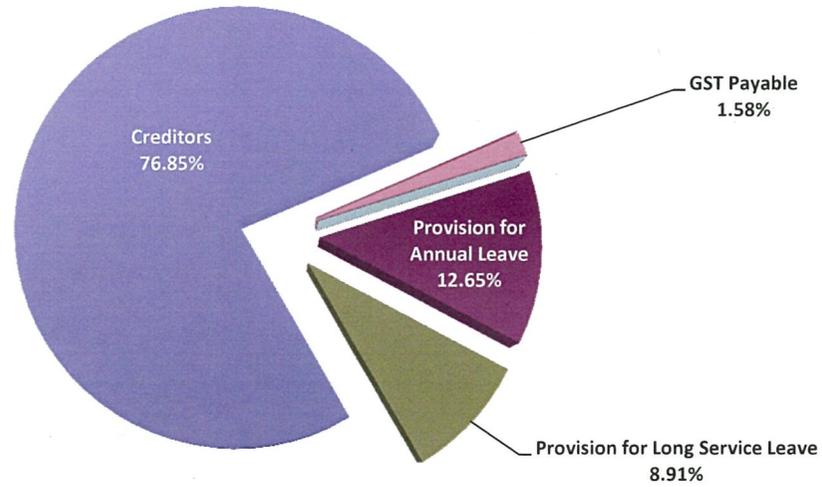
**Capital Expenditure  
YTD Actual Vs Revised Budget**



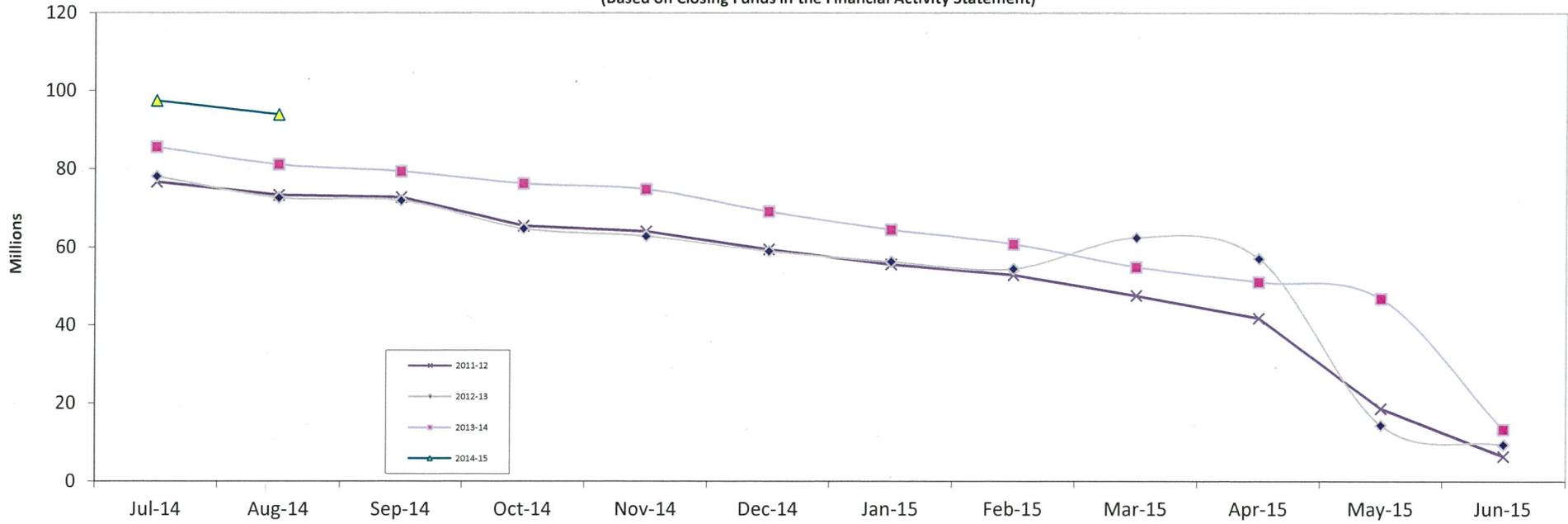
### Current Assets (YTD Actual)



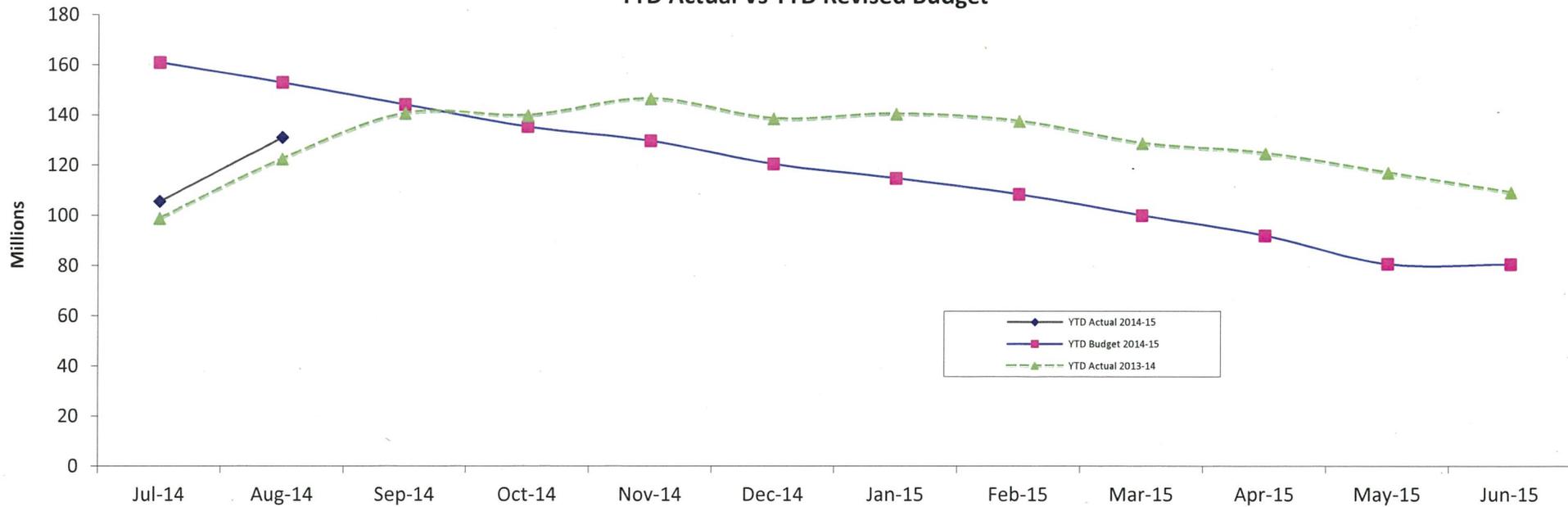
### Current Liabilities (YTD Actual)



**Municipal Liquidity Over the Year**  
 (Based on Closing Funds in the Financial Activity Statement)



**Cash & Investments Positions**  
 YTD Actual Vs YTD Revised Budget



## City of Cockburn - Reserve Funds

### Financial Statement for Period Ending 31 August 2014

Account Details	Opening Balance		Interest Received		t/f's from Municipal		t/f's to Municipal		Closing Balance	
	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual
<b>Council Funded</b>										
Bibra Lake Management Plan Reserve	847,819	847,819	(9,120)	3,565	-	-	(120,000)	-	718,699	851,384
Bibra Lake Nutrient Managment	313,447	313,447	12,230	1,318	-	-	-	-	325,677	314,765
Community Infrastructure	9,830,572	9,830,572	200,590	41,921	4,508,068	-	(8,105,850)	(17,150)	6,433,380	9,855,343
Community Surveillance Levy Reserve	653,841	653,841	29,690	2,550	503,160	-	(227,898)	(100,412)	958,793	555,979
Contaminated Sites	2,518,882	2,518,882	51,520	10,520	-	-	(100,000)	(35,028)	2,470,402	2,494,375
DCD Redundancies Reserve	2,991	2,991	-	13	-	-	-	-	2,991	3,004
Environmental Offset Reserve	277,367	277,368	(3,110)	1,166	-	-	-	-	274,257	278,534
Green House Emissions Reductions	652,516	652,516	13,880	2,743	200,000	-	(762,000)	-	104,396	655,259
Information Technology	261,600	261,600	37,220	1,037	100,000	-	(167,361)	(48,675)	231,459	213,962
Land Development & Investment Fund Reserve	4,719,455	4,719,455	283,600	19,426	5,552,417	-	(4,308,477)	(1,347,501)	6,246,995	3,391,380
Major Buildings Refurbishment	5,439,366	5,439,366	25,930	22,869	-	-	-	-	5,465,296	5,462,235
Mobile Rubbish Bins	63,279	63,279	24,440	265	-	-	(170,000)	(322)	(82,281)	63,223
Municipal Elections	49,722	49,721	13,270	209	-	-	-	-	62,992	49,930
Naval Base Shacks	766,920	766,921	16,420	3,224	151,413	-	(759,428)	-	175,325	770,145
Plant & Vehicle Replacement	5,930,546	5,930,546	76,610	24,254	3,450,000	-	(3,655,375)	(410,000)	5,801,781	5,544,801
Port Coogee Special Maintenance Reserve	1,005,467	1,005,468	23,060	4,226	270,000	-	(117,925)	(35,991)	1,180,602	973,702
Roads & Drainage Infrastructure	2,026,150	2,026,150	101,580	8,011	1,250,000	-	(3,214,532)	(600,114)	163,198	1,434,047
Staff Payments & Entitlements	2,271,100	2,271,100	157,540	9,546	110,000	-	(180,000)	(89,500)	2,358,640	2,191,145
Waste & Recycling	18,659,246	18,659,246	626,270	78,447	3,518,824	-	(2,500,495)	(52,542)	20,303,845	18,685,151
Waste Collection Levy	264,697	264,697	1,540	1,700	190,955	-	(200,000)	-	257,192	266,397
Workers Compensation	379,495	379,495	15,480	1,596	-	-	-	-	394,975	381,091
POS Cash in Lieu (Restricted Funds)	4,240,467	4,240,466	132,710	17,801	-	-	(888,000)	(33,885)	3,485,177	4,224,382
	<b>61,174,947</b>	<b>61,174,947</b>	<b>1,831,350</b>	<b>256,408</b>	<b>19,804,837</b>	<b>-</b>	<b>(25,477,341)</b>	<b>(2,771,120)</b>	<b>57,333,793</b>	<b>58,660,235</b>
<b>Grant Funded</b>										
Aged & Disabled Vehicle Expenses	322,162	322,162	9,855	1,323	62,625	-	(178,000)	(25,869)	216,643	297,616
CCW Development Fund	-	-	-	-	11,241,207	-	-	-	11,241,207	-
CIHF Building Maintenance Resrv	-	-	-	-	600,000	-	-	-	600,000	-
Cockburn Super Clinic Reserve	1,936,374	1,936,374	169,220	7,552	-	-	(1,985,154)	(39,924)	120,440	1,904,002
Family Day Care Accumulation Fund	22,384	22,383	3,000	184	-	-	(30,000)	(24,314)	(4,616)	(1,746)
Naval Base Shack Removal Reserve	333,944	333,945	4,270	1,404	54,693	-	-	-	392,907	335,349
Restricted Grants & Contributions Resrv	5,923,657	5,923,657	-	20,128	-	-	(2,767,575)	(2,580,864)	3,156,082	3,362,921
UNDERGROUND POWER	754,224	754,224	(11,570)	3,171	1,200,000	-	(1,372,637)	-	570,016	757,395
Welfare Projects Employee Entitlements	444,423	444,422	12,452	1,915	-	-	(11,060)	-	445,815	446,337
	<b>9,737,168</b>	<b>9,737,168</b>	<b>187,227</b>	<b>35,677</b>	<b>13,158,526</b>	<b>-</b>	<b>(6,344,426)</b>	<b>(2,670,971)</b>	<b>16,738,494</b>	<b>7,101,873</b>
<b>Development Cont. Plans</b>										
Aubin Grove DCA	170,698	170,698	4,705	718	-	-	(887)	-	174,516	171,416
Community Infrastructure DCA 13	10,029,345	10,029,345	140,180	43,360	3,000,000	1,318,537	(359,999)	-	12,809,526	11,391,242
Gaebler Rd Development Cont. Plans	984,238	984,238	18,924	4,138	-	-	(3,474)	-	999,687	988,376
Hammond Park DCA	(14,180)	(14,180)	9,354	(60)	396,000	-	(13,595)	-	377,579	(14,240)
Munster Development	432,526	432,526	18,147	2,037	443,798	109,278	(17,871)	-	876,600	543,841
Muriel Court Development Contribution	(48,104)	(48,104)	(206,000)	(202)	-	-	(19,092)	-	(273,196)	(48,307)
Packham North - DCA 12	(105,792)	(105,792)	10,529	(445)	434,388	-	(19,192)	-	319,933	(106,237)
Solomon Road DCA	360,190	360,190	8,493	1,514	120,000	-	(7,721)	-	480,962	361,704
Success Lakes Development	887,990	887,991	3,817	3,734	-	-	(3,474)	-	888,333	891,724
Success Nth Development Cont. Plans	1,185,551	1,185,550	15,311	4,985	11,700	311,129	(5,611)	-	1,206,951	1,501,664
Thomas St Development Cont. Plans	12,079	12,079	294	51	-	-	-	-	12,373	12,130
Wattleup DCA 10	(9,363)	(9,363)	-	(39)	-	-	(12,695)	-	(22,058)	(9,402)
Yangebup East Development Cont. Plans	436,865	436,865	6,026	1,837	130,036	-	(3,748)	-	569,179	438,702
Yangebup West Development Cont. Plans	354,406	354,406	9,195	1,490	-	-	(2,624)	-	360,977	355,896
	<b>14,676,448</b>	<b>14,676,448</b>	<b>38,975</b>	<b>63,117</b>	<b>4,535,922</b>	<b>1,738,944</b>	<b>(469,984)</b>	<b>-</b>	<b>18,781,361</b>	<b>16,478,510</b>
<b>Total Reserves</b>	<b>85,588,562</b>	<b>85,588,563</b>	<b>2,057,552</b>	<b>355,201</b>	<b>37,499,284</b>	<b>1,738,944</b>	<b>(32,291,751)</b>	<b>(5,442,092)</b>	<b>92,853,648</b>	<b>82,240,617</b>

# Capital Expenditure

#REF!

	Actuals	YTD Revised Budget	Full Year Revised Budget	\$ Variance to YTD Budget	v = Favourable X = Unfavourable	Explanation
	\$	\$	\$	\$		
<b>SUMMARY</b>						
Purchase of Land and Buildings	1,964,940	3,318,661	44,042,673	1,353,721	√	
Acquisition & Development of Infrastructure Assets	1,447,027	3,326,023	23,558,540	1,878,996	√	
Purchase of Plant and Machinery	539,601	1,676,500	5,543,561	1,136,899	√	
Purchase of Furniture and Equipment	0	11,000	11,736	11,000	√	
Purchase of Computer Equipment	192,382	521,500	2,974,879	329,118	√	
	<b>4,143,949</b>	<b>8,853,683</b>	<b>76,131,390</b>	<b>4,709,734</b>		
<b>Material Variances Identified:</b>						
<b><u>Works in Progress - Roads Infrastructure</u></b>						
3544 - North Lake Road (Hammond to Kentucky)	603,333	840,968	0	237,635	√	Progressing and project on track, delay in payments. Committed order of \$278k.
2471 - Beelias Drive (Wentworth Pde to Kwinana Fwy)	39,808	278,532	0	238,724	√	Limited resources. CW3545 given higher priority. Expected to start major works October.
2442 - Frankland Avenue construction Single carrieway Roper Bouleva	102,686	470,344	1,432,000	367,658	√	Ongoing, expected to finish project September.
3545 - Beelias Drive Hammond Road North and South	22,944	554,478	0	531,534	√	Western power is holding up the project as a pillar needs to be removed from the area. Expected to finish the project in October.
Sub Total	<b>768,772</b>	<b>2,144,322</b>	<b>1,432,000</b>	<b>1,375,550</b>		
<b><u>Works in Progress - Parks Hard Infrastruc</u></b>						
5443 - Formalise path to lookout from Manning Carpark. (Wooden Step	0	100,000	119,603	100,000	√	Grant successful and tender being developed. Approx 5% complete with a committed order of \$6k.
Sub Total	<b>0</b>	<b>100,000</b>	<b>119,603</b>	<b>100,000</b>		
<b><u>Works in Progress - Buildings</u></b>						
026 - Cockburn Central Aquatic Recreation Ctr	414,152	305,552	0	(108,600)	X	Committed order of \$1m. \$238k architectural expense for CW4517 with the budget being \$65k for that project.
005 - Cockburn Integrated Health Facilities	1,336,766	2,333,606	29,269,466	996,840	√	Committed order of \$1.68m. Initial delay due to change of construction methodology. Library systems charge from Bibliotheca of \$179k.
Sub Total	<b>1,750,918</b>	<b>2,639,158</b>	<b>29,269,466</b>	<b>888,240</b>		
<b><u>Computers</u></b>						
1403 - OCR AP Automation Processing	0	100,000	0	100,000	√	Other options being explored. Kwinana users to be trained in Tech One before final go ahead can happen.
Sub Total	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>100,000</b>		
<b><u>Plant &amp; Machinery</u></b>						
7756 - New Waste Collection Truck PL756-1	0	330,000	0	330,000	√	Committed order of \$325k. Delivered August, invoice to be paid
7755 - Heavy Fleet-Waste Iveco F2350G/260 Rubbish Truck PL7551	0	350,000	0	350,000	√	Committed order of \$343k. Delivered August, invoice to be paid.
Sub Total	<b>0</b>	<b>680,000</b>	<b>0</b>	<b>680,000</b>		

Variance Analysis						
Municipal Financial Activity Statement for the period ended 31 August 2014						
	YTD Actuals	YTD Revised Budget	Full Year Revised Budget	YTD Variance	√ = Favourable X = Unfavourable	Aug-14
	\$	\$	\$	\$		
<b>OPERATING REVENUE</b>						
Governance	64,772,260	65,045,376	74,108,927	(273,116)	X	Income received from Underground Power Service Charges are \$111k under ytd budget.
<b>OPERATING EXPENDITURE</b>						
Information Services	648,371	1,010,869	4,407,974	362,497	√	Software Support Expenses are under ytd. budget by \$389k.
Community Services	1,102,404	1,351,114	9,511,031	248,711	√	Material & Contracts Expenses are under ytd budget by \$181k.
Waste Services	2,321,733	3,027,105	20,076,655	705,373	√	Expenses in landfill levy are under ytd budget by \$272k. Expenditures in Entry fees-Rrrc and Entry fees for Recyclables are under ytd budget by \$104k and \$108k respectively.
Parks & Environmental Services	1,376,254	1,808,064	11,365,867	431,809	√	Operating Expenditure of Environmental Works Operating Projects and Parks & Playgrounds Operating Projects are under ytd budget by \$101k and \$142k respectively.
Infrastructure Services	971,993	1,366,684	8,278,904	394,690	√	Materials & Contracts expenditures are underspent by \$368k.
<b>ADDITIONAL FUNDING RECEIVED</b>						
Grants & Contributions - Asset Development	2,676,554	832,500	7,722,142	1,844,054	√	Owner Contribution received for DCA1,DCA6 and DCA13 are ahead of its ytd budget by \$311k, \$109k, \$823k. Contribution received from POS Cash in Lieu is ahead of its ytd budget by \$250k. Developer Contribution Received for Asset Development are ahead of its ytd budget by \$115k. Developer Contributions Received for New Cockburn Central Aquatic & Recreation Centre are ahead of its ytd budget by \$207k.
Proceeds on Sale of Assets	32,727	899,579	6,783,852	(866,852)	X	Lot40 Cervantes Loop, Lot23 Russell Road and Subdivision Lot 702 Bellier Pl & Lot 65 Erpingham Rd have not been sold, resulting in unfavourable variance of \$150k, \$167k and \$234k respectively.

## 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/ TF to Reserves	Income Source			
					Natural	External		External	Reserves	Restricted Grants & Contributions	Municipal
					Account	Account					
113 - Other Governance	CW	CW4449 - New Cockburn Central Aquatic & Recreation	Y	4449	6200		933,311		(639,520.00)	(293,791.00)	0.00
113 - Other Governance	CW	CW4512 - Cockburn Integrated Health & Community Facility – Post Gavin	Y	4512	6200		2,404,231		(2,404,231.00)		0.00
113 - Other Governance	CW	CW4513 - Cockburn Central Super Clinic - Feds Funded- Post Gavin	Y	4513	6200	5111	2,262,981	(332,500.00)	(1,930,481.00)		0.00
113 - Other Governance	CW	CW4515 - Project Manager - NS Projects	Y	4515	6200						0.00
113 - Other Governance	CW	CW4516 - QS - WT Partnership Cockburn Central Integrated Facility	Y	4516	6200						0.00
113 - Other Governance	CW	CW4517 - Architect - Sandover Pinder/DWP Suters	Y	4517	6200						0.00
113 - Other Governance	CW	CW4518 - Professional Service 1: civil, geo, facade, wind	Y	4518	6200						0.00
113 - Other Governance	CW	CW4519 - Professional Service 2: elect, IT, telcom, vertical, security	Y	4519	6200						0.00
113 - Other Governance	CW	CW4520 - Professional Service 3: hydraulic, fire	Y	4520	6200						0.00
113 - Other Governance	CW	CW4521 - Professional Service 4: mechanical	Y	4521	6200						0.00
113 - Other Governance	CW	CW4522 - Professional Service 5: BCA	Y	4522	6200						0.00
113 - Other Governance	CW	CW4523 - Professional Service 6: Pool	Y	4523	6200						0.00
113 - Other Governance	CW	CW4524 - Professional Service 7: ESD, enviro	Y	4524	6200						0.00
113 - Other Governance	OP	OP9117 - GP Super Clinic Board – Grant Funded	Y	9117	6813		39,924		(39,924.00)		0.00
113 - Other Governance	OP	OP9186 - GP Super Clinic Board – Interest Funded	Y	9186	6813		14,749		(14,749.00)		0.00
113 - Other Governance	CW	CW4393 - Coogee Beach Integrated Community Facility - Stage 2	Y	4393	6200		141,532				(141,532.00)
211 - Accounting Services	OP	OP9155 - Business Intelligence Module	Y	9155	6200		40,000				(40,000.00)
221 - Information Technology	CW	CW1088 - Fibre Link between Youth Centre and Super Clinic	Y	1088	6200		20,000		(20,000.00)		0.00
221 - Information Technology	CW	CW1357 - Audio Visual equipment for Boardroom and Chambers	Y	1357	6200		20,000				(20,000.00)
221 - Information Technology	CW	CW1359 - Fibre Infrastructure	Y	1359	6200		93,000		(93,000.00)		0.00
221 - Information Technology	CW	CW1395 - Disaster Recovery Project	Y	1395	6200		32,295		(32,295.00)		0.00
221 - Information Technology	OP	OP8250 - Network Configuration & Improvement	Y	8250	6200		22,066		(22,066.00)		0.00
224 - Business Systems Services	CW	CW1089 - Facilities Booking System for Youth Centre, Seniors and Rec	Y	1089	6200		44,209				(44,209.00)
224 - Business Systems Services	CW	CW1366 - EDMS	Y	1366	6200		204,951				(204,951.00)

OCM 9/10/2014 - Item 15.3 Attach 1

## 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
224 - Business Systems Services	CW	CW1367 - GIS Integration with T1	Y	1367	6200		7,240				(7,240.00)
224 - Business Systems Services	CW	CW1368 - Mobility	Y	1368	6200		4,840				(4,840.00)
224 - Business Systems Services	CW	CW1369 - Recreation and Leisure Centre Software	Y	1369	6200		7,750				(7,750.00)
224 - Business Systems Services	CW	CW1377 - Contract Management System	Y	1377	6200		19,150				(19,150.00)
224 - Business Systems Services	CW	CW1385 - Content Management and Web Design	Y	1385	6200		61,740				(61,740.00)
231 - Human Resources	OP	OP9159 - Technology One modules	Y	9159	6200		10,000				(10,000.00)
311 - Management Libraries	CW	CW1373 - Print Management Solution	Y	1373	6200		1,576				(1,576.00)
311 - Management Libraries	CW	CW4361 - RFID Project Implementation - Success Library	Y	4361	6200		17,350		(17,350.00)		0.00
311 - Management Libraries	CW	CW4481 - Spearwood workroom regeneration	Y	4481	6200		11,210				(11,210.00)
311 - Management Libraries	OP	OP8171 - Cockburn Libraries' off-site materials storage	Y	8171	6200		2,760				(2,760.00)
311 - Management Libraries	OP	OP9292 - National Year of Reading 2012 (NYR)	Y	9292	6200		2,588				(2,588.00)
311 - Management Libraries	OP	OP9295 - Your Tutor Online Delivery	Y	9295	6200		9,500				(9,500.00)
311 - Management Libraries	OP	OP9995 - Library Grants Minor	Y	9995	6200	5110	2,744			(2,744.00)	0.00
328 - Law and Public Safety	CW	CW1386 - CCTV Expansion Program	Y	1386	6200		146,055		(137,898.00)		(8,157.00)
328 - Law and Public Safety	CW	CW1387 - Electronic Infringement System and Software	Y	1387	6200		27,928				(27,928.00)
328 - Law and Public Safety	CW	CW1388 - CCTV Connecting and Protecting the Cockburn Coast	Y	1388	6200		5,723			(5,723.00)	0.00
328 - Law and Public Safety	CW	CW1391 - Dog Playground Park	Y	1391	6200		57,257				(57,257.00)
328 - Law and Public Safety	OP	OP8177 - Bushfire Management Plan	Y	8177	6200		26,574				(26,574.00)
329 - Community Development	OP	OP8322 - Connecting South Lake	Y	8322	6200		33,128				(33,128.00)

## 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/ TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
329 - Community Development	OP	OP8580 - Alcoa Cockburn Community Development	Y	8580	6200	5750	1,214			(1,214.00)	0.00
329 - Community Development	OP	OP9652 - Community Projects – Fremantle Ports Sponsorship	Y	9652	6200	5110	12,429			(12,429.00)	0.00
330 - Human Services Unit Management	CW	CW4514 - Western Suburbs Skate Parks	Y	4514	6200		18,500		(18,500.00)		0.00
330 - Human Services Unit Management	OP	OP9144 - Lease Cost Recovery	Y	9144	6200	5901	1,318			(1,318.00)	0.00
331 - Family Services	OP	OP8255 - Raising Solid Families	Y	8255	6200	5110	20,060			(20,060.00)	0.00
331 - Family Services	OP	OP9182 - Reconciliation Action Plan review and update	Y	9182	6200		1,721				(1,721.00)
331 - Family Services	OP	OP9562 - Coolbellup Financial Counselling Projects	Y	9562	6200		11,060		(11,060.00)		0.00
331 - Family Services	OP	OP9570 - Family Support Admin Officer - Joint Funded	Y	9570	6200	5151	5,104			(5,104.00)	0.00
331 - Family Services	OP	OP9729 - MY Time Group	Y	9729	6200	5110	1,909			(1,909.00)	0.00
332 - Youth Services	OP	OP8266 - Blissco at the Hub Project	Y	8266	6000	5101	7,928			(7,928.00)	0.00
332 - Youth Services	OP	OP8266 - Blissco at the Hub Project	Y	8266	6000	5151	2,747			(2,747.00)	0.00
333 - Aged and Disabled Services	OP	OP8324 - Private Fee Income	Y	8324	6202	5540	10,262			(10,262.00)	0.00
334 - Child Care Services	OP	OP9208 - Active After-school Communities	Y	9208	6200		1,406			(1,406.00)	0.00
335 - Seniors Services	OP	OP8247 - Fundraising - Senior Centre	Y	8247	6200	5901	10,638			(10,638.00)	0.00
335 - Seniors Services	OP	OP9184 - Strengthening Relationships - Senior Centre	Y	9184	6200		2,381				(2,381.00)
341 - Corporate Communications	CW	CW5317 - Public artworks	Y	5317	6200		40,000				(40,000.00)
341 - Corporate Communications	OP	OP8184 - Develop Social Media Policy Training	Y	8184	6200		10,000				(10,000.00)
341 - Corporate Communications	OP	OP8187 - Public Art Strategy review	Y	8187	6200		14,663				(14,663.00)
413 - Health Services	OP	OP8188 - Travel Smart/Active Smart	Y	8188	6200	5110	31,229			(31,229.00)	0.00
413 - Health Services	OP	OP8231 - RAC Community Partnership Grant	Y	8231	6200		5,388			(5,388.00)	0.00
423 - Land Administration	CW	CW1530 - Lot 1 Berrigan Dr, South Lake Rezoning & Subdivision	Y	1530	6200		4,994		(4,994.00)		0.00
423 - Land Administration	CW	CW1539 - Subdivision Lot 702 Bellier Pl & Lot 65 Erpingham Rd	Y	1539	6200		584,340		(584,340.00)		0.00
423 - Land Administration	CW	CW1539 - Subdivision Lot 702 Bellier Pl & Lot 65 Erpingham Rd	Y	1539	7152	9900	1,402,727	(1,402,727.00)			0.00
423 - Land Administration	CW	CW1545 - Lot 133 Arthur Street - fill and compact former drainage sum	Y	1545	6200		5,000		(5,000.00)		0.00
423 - Land Administration	CW	CW1548 - Lot 40 Cervantes Loop - surveying and construction of access	Y	1548	6200		25,000		(25,000.00)		0.00
423 - Land Administration	CW	CW1548 - Lot 40 Cervantes Loop - surveying and construction of access	Y	1548	7152	9900	900,000	(900,000.00)			0.00
423 - Land Administration	CW	CW1553 - Subdivision and development of Lot 915 and Reserve 38537	Y	1553	6200		377,135		(377,135.00)		0.00

## 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
423 - Land Administration	CW	CW1554 - Purchase of portion of Koorilla School Site aged persons land	Y	1554	6200		150,000		(150,000.00)		0.00
423 - Land Administration	CW	CW1584 - Subdivision and development of Lot 1, 4218 and 4219 Quarimor	Y	1584	7152	9900	720,000	(720,000.00)			0.00
423 - Land Administration	CW	CW1585 - Lot 23 Russell Road, Hammond Park	Y	1585	6200		184,827		(184,827.00)		0.00
423 - Land Administration	CW	CW1585 - Lot 23 Russell Road, Hammond Park	Y	1585	7152	9900	1,000,000	(1,000,000.00)			0.00
423 - Land Administration	OP	OP9082 - Hamilton Hill Revitalisation Strategy 2011	Y	9082	6200		1,399				(1,399.00)
423 - Land Administration	OP	OP9166 - Scheme amendment, master planning and design guidelines for	Y	9166	6200		19,883				(19,883.00)
512 - Waste Disposal Services	CW	CW1929 - Henderson Waste Recovery Park Commercial Transfer Station	Y	1929	6200		180,990		(180,990.00)		0.00
512 - Waste_Disposal_Services	CW	CW1935 - Remediation & landfill leachate management	Y	1935	6200		104,130		(104,130.00)		0.00
512 - Waste_Disposal_Services	CW	CW1954 - Household Hazardous Waste Shed	Y	1954	6200		129,936		(129,936.00)		0.00
512 - Waste_Disposal_Services	CW	CW1959 - Security Upgrade HWRP	Y	1959	6200	5790	12,414	(10,000.00)	(2,414.00)		0.00
512 - Waste Disposal Services	CW	CW1962 - Upgrade to Weighbridge Software	Y	1962	6200		25,000		(25,000.00)		0.00
512 - Waste Disposal Services	CW	CW1964 - Renew Waste Gas Contract Document- Legal expenses	Y	1964	6200		10,000		(10,000.00)		0.00
521 - Parks Construction and Maintenance	CW	CW5261 - Bibra Lake MP Adventure Playground	Y	5261	6200		2,244,748				(2,244,748.00)
521 - Parks Construction and Maintenance	CW	CW5459 - Playground Renewal - Bibra Lake Res 4	Y	5459	6200		4,189				(4,189.00)
521 - Parks Construction and Maintenance	CW	CW5461 - Playground Renewal - Bunker Garden	Y	5461	6200		6,250				(6,250.00)
521 - Parks Construction and Maintenance	CW	CW5463 - Playground Renewal - Hargreaves Park 1	Y	5463	6200		6,613				(6,613.00)
521 - Parks Construction and Maintenance	CW	CW5470 - Playground Renewal - Poole Res	Y	5470	6200		13,587				(13,587.00)
521 - Parks Construction and Maintenance	CW	CW5471 - Playground Renewal - Sebastian Cres	Y	5471	6200		9,381				(9,381.00)
521 - Parks Construction and Maintenance	CW	CW5474 - Market Garden Swamp Seating	Y	5474	6200		3,372				(3,372.00)
521 - Parks Construction and Maintenance	CW	CW5477 - Shade Sails - Beale Oval	Y	5477	6200		6,235				(6,235.00)
521 - Parks Construction and Maintenance	CW	CW5478 - Spearwood Av - Friendship Way - Peace Section	Y	5478	6200		70,853				(70,853.00)
521 - Parks Construction and Maintenance	CW	CW5479 - Spearwood Ave - Friendship Way - Indigenous Section	Y	5479	6200		91,514				(91,514.00)
521 - Parks Construction and Maintenance	CW	CW5480 - Greening Plan-Bibra dr	Y	5480	6200		20,185				(20,185.00)
521 - Parks Construction and Maintenance	CW	CW5481 - Greening Plan - Elderberry Dr	Y	5481	6200		14,674				(14,674.00)
521 - Parks Construction and Maintenance	CW	CW5483 - Greening Plan - Parkway Rd	Y	5483	6200		24,750				(24,750.00)
521 - Parks Construction and Maintenance	CW	CW5487 - Irrigation Renewal - Michigan Park	Y	5487	6200		99,743				(99,743.00)

### 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
521 - Parks Construction and Maintenance	CW	CW5489 - Park Development – Enright Res Stage 1	Y	5489	6200		83,125				(83,125.00)
521 - Parks Construction and Maintenance	CW	CW5495 - Greening Plan - Banning/Alabaster	Y	5495	6200		19,626				(19,626.00)
522 - Environmental Management	CW	CW5443 - Formalise path to lookout from Manning Car park. (Wooden Step	Y	5443	6200		100,239				(100,239.00)
522 - Environmental Management	CW	CW5496 - Repair 2 Lookouts on Davilak Trail	Y	5496	6200		60,000				(60,000.00)
522 - Environmental Management	CW	CW5631 - Yangebup Lake - Solar Power Nutrient Removal Trial.	Y	5631	6200		50,000				(50,000.00)
522 - Environmental Management	CW	CW5642 - Revegetation Lot 1 Mayor Road	Y	5642	6200	5756	9,541			(9,541.00)	0.00
522 - Environmental Management	OP	OP9177 - Count Me in Inclusion Grant	Y	9177	6200		12,131			(12,131.00)	0.00
531 - Road Construction and Maintenance	CW	CW2356 - BEELIAR DR [Hammond/Dunraven] - Construct 2nd cwy Stage 1	Y	2356	6200		84,343			(84,343.00)	0.00
531 - Road Construction and Maintenance	CW	CW2358 - WINTERFOLD RD/LOCKETT ST/LETCHFORD ST - Contribution to Frem	Y	2358	6200		49,313				(49,313.00)
531 - Road Construction and Maintenance	CW	CW2375 - TRAFFIC SAFETY MANAGEMENT - Traffic calming & minor works	Y	2375	6200	5131	115,967				(115,967.00)
531 - Road Construction and Maintenance	CW	CW2442 - Frankland Avenue construction Single carriaway Roper Boulevard	Y	2442	6200		470,344		(4,518.00)		(465,826.00)
531 - Road Construction and Maintenance	CW	CW2448 - Banjup - Footpath	Y	2448	6200		264,803				(264,803.00)
531 - Road Construction and Maintenance	CW	CW2450 - Millrun Drive (William - Millrun)	Y	2450	6200		56,000				(56,000.00)
531 - Road Construction and Maintenance	CW	CW2451 - Huxley Place (Barrett - Bishop Park)	Y	2451	6200		9,000				(9,000.00)
531 - Road Construction and Maintenance	CW	CW2452 - Blondell Drive (Turfan - Fenimore)	Y	2452	6200		22,269				(22,269.00)
531 - Road Construction and Maintenance	CW	CW2453 - Kipling Street (Milton - cul de sac)	Y	2453	6200		29,000				(29,000.00)
531 - Road Construction and Maintenance	CW	CW2454 - Cary Street (Marvell - Kipling)	Y	2454	6200		7,000				(7,000.00)
531 - Road Construction and Maintenance	CW	CW2455 - Milton Place (Marvell - Kipling)	Y	2455	6200		7,000				(7,000.00)
531 - Road Construction and Maintenance	CW	CW2471 - Beeliar Drive [Wentworth Pde to Kwinana Fwy]	Y	2471	6200	5132	696,330	(440,000.00)		(256,330.00)	0.00
531 - Road Construction and Maintenance	CW	CW2492 - Parking Signage & Line Marking	Y	2492	6200		40,331				(40,331.00)
531 - Road Construction and Maintenance	CW	CW2791 - Branch Circus (Coojong to Gadd)- Resurfacing	Y	2791	6200		68,088				(68,088.00)
531 - Road Construction and Maintenance	CW	CW2797 - Gaebler Road (Murrumbidgee to Frankland)- Resurfacing	Y	2797	6200		18,563				(18,563.00)
531 - Road Construction and Maintenance	CW	CW2858 - Mell Road (Rigby to D/End)- Resurfacing	Y	2858	6200		32,322				(32,322.00)
531 - Road Construction and Maintenance	CW	CW2862 - Gadd Street (Branch to End)- Resurfacing	Y	2862	6200		12,854				(12,854.00)
531 - Road Construction and Maintenance	CW	CW2863 - Caratti Road (Moylan to End) Resurfacing	Y	2863	6200		10,738				(10,738.00)
531 - Road Construction and Maintenance	CW	CW2877 - Solace Gardens, Atwell - Drainage upgrade	Y	2877	6200		40,772				(40,772.00)
531 - Road Construction and Maintenance	CW	CW2880 - Hurford Street (Corner Starling Street)	Y	2880	6200		43,010				(43,010.00)

### 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
531 - Road Construction and Maintenance	CW	CW2888 - Beale Park (SP 023)	Y	2888	6200		34,798				(34,798.00)
531 - Road Construction and Maintenance	CW	CW2893 - Drainage upgrade Frederick Road HAMILTON HILL 6163	Y	2893	6200		39,085				(39,085.00)
531 - Road Construction and Maintenance	CW	CW2932 - 14 Elderberry Dr - Drainage Upgrade	Y	2932	6200		76,757				(76,757.00)
531 - Road Construction and Maintenance	CW	CW2939 - Mala Wy/Sutton Dr - Drainage Upgrade	Y	2939	6200		156,022				(156,022.00)
531 - Road Construction and Maintenance	CW	CW2960 - Hammond road (Cooper - End)	Y	2960	6200		40,144				(40,144.00)
531 - Road Construction and Maintenance	CW	CW2961 - Hammond road (Beeliar - Hammond)	Y	2961	6200		9,610				(9,610.00)
531 - Road Construction and Maintenance	CW	CW2962 - North Lake Rd (Hammond - NA)	Y	2962	6200		25,171				(25,171.00)
531 - Road Construction and Maintenance	CW	CW2970 - Children's Crossings	Y	2970	6200		38,365				(38,365.00)
531 - Road Construction and Maintenance	CW	CW2989 - Berrigan Dr - [Kwinana Fwy to Jandakot] Construct 2nd cwy	Y	2989	6200	5132	1,063,967	(213,333.00)		(850,634.00)	0.00
531 - Road Construction and Maintenance	CW	CW3461 - Bus Stop Facilities	Y	3461	6200	5114	25,910			(20,000.00)	(5,910.00)
531 - Road Construction and Maintenance	CW	CW3507 - Turfan Way (Aden to Jessica)	Y	3507	6200		3,419				(3,419.00)
531 - Road Construction and Maintenance	CW	CW3510 - March Street (Lancaster to Orleans)	Y	3510	6200		23,017				(23,017.00)
531 - Road Construction and Maintenance	CW	CW3511 - Rockingham Road (Coleville to Rockingham)	Y	3511	6200		23,000				(23,000.00)
531 - Road Construction and Maintenance	CW	CW3516 - Realign footpath at Roe Highway on ramp	Y	3516	6200		15,419				(15,419.00)
531 - Road Construction and Maintenance	CW	CW3520 - Realign footpath at Roe Highway on ramp	Y	3520	6200	5139	10,000	(10,000.00)			0.00
531 - Road Construction and Maintenance	CW	CW3531 - Russell Road (NA)	Y	3531	6200		19,500				(19,500.00)
531 - Road Construction and Maintenance	CW	CW3532 - Tindal Avenue (Tindal avn - McLaren avn)	Y	3532	6200		6,900				(6,900.00)
531 - Road Construction and Maintenance	CW	CW3543 - North Lake Road (Discovery to Masefield)	Y	3543	6200	5139	43,503			(43,503.00)	0.00
531 - Road Construction and Maintenance	CW	CW3544 - North Lake Road (Hammond to Kentucky)	Y	3544	6200	5132	3,363,875	(1,640,014.00)		(357,208.00)	(1,366,653.00)
531 - Road Construction and Maintenance	CW	CW3545 - Beeliar Drive Hammond Road North and South	Y	3545	6200		1,663,434	(938,025.00)			(725,409.00)
531 - Road Construction and Maintenance	CW	CW3546 - Howson Way New Street lighting	Y	3546	6200		44,800				(44,800.00)
531 - Road Construction and Maintenance	CW	CW3547 - Sparks and Russell Road West Intersection improvements	Y	3547	6200		67,974				(67,974.00)
531 - Road Construction and Maintenance	CW	CW3548 - Russell Power and Henderson Intersection	Y	3548	6200		161,762				(161,762.00)
531 - Road Construction and Maintenance	CW	CW3551 - Yangebup Road (next 93)	Y	3551	6200		30,000				(30,000.00)
531 - Road Construction and Maintenance	CW	CW3611 - Bus Shelter Replacement Program	Y	3611	6200		11,380				(11,380.00)
531 - Road Construction and Maintenance	OP	OP8574 - Underground Power	Y	8574	6690		574		(574.00)		0.00

## 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
542 - Asset Services	CW	CW1392 - Works & Assets Systems Development	Y	1392	6200		38,788				(38,788.00)
543 - Facilities Mtce and Management	CW	CW4238 - Phoenix Central Revitalisation Plan	Y	4238	6200		20,000				(20,000.00)
543 - Facilities Mtce and Management	CW	CW4241 - Cockburn Bowling Club - Design & Doc & Construction	Y	4241	6200		4,200				(4,200.00)
543 - Facilities Mtce and Management	CW	CW4377 - Building & Facility - Asbestos Removal Works	Y	4377	6200		3,408				(3,408.00)
543 - Facilities Mtce and Management	CW	CW4385 - Operations Centre - New Op Cntr Building Design & Construction	Y	4385	6200		46,057				(46,057.00)
543 - Facilities Mtce and Management	CW	CW4396 - Admin Building Office Modifications	Y	4396	6200		12,832				(12,832.00)
543 - Facilities Mtce and Management	CW	CW4451 - Cockburn Toy Library	Y	4451	6200		5,227				(5,227.00)
543 - Facilities Mtce and Management	CW	CW4464 - Coogee Toilet Upgrade sewerage from domestic system	Y	4464	6200		199,380		(148,060.00)		(51,320.00)
543 - Facilities Mtce and Management	CW	CW4471 - Civic Bldg Energy Reduction Initiative	Y	4471	6200		262,000		(262,000.00)		0.00
543 - Facilities Mtce and Management	CW	CW4472 - Naval Base Reserve Improvements (drainage, lighting, beach a	Y	4472	6200		209,428		(209,428.00)		0.00
543 - Facilities Mtce and Management	CW	CW4473 - Coogee Holiday Park Electrical Supply Upgrade	Y	4473	6200		387,957		(199,890.00)		(188,067.00)
543 - Facilities Mtce and Management	CW	CW4485 - Atwell Clubrooms & Playing Surfaces - Upgrades	Y	4485	6200		41,510				(41,510.00)
543 - Facilities Mtce and Management	CW	CW4491 - Coleville Cres Staff/Visitor Car park Mod's inc. EV Charge St	Y	4491	6200		50,000				(50,000.00)
543 - Facilities Mtce and Management	CW	CW4492 - Community Men's Shed - Buckley Street, Cockburn Central West	Y	4492	6200		3,374				(3,374.00)
543 - Facilities Mtce and Management	CW	CW4493 - Coogee Beach Enclosure Trial	Y	4493	6200		42,386				(42,386.00)
543 - Facilities Mtce and Management	CW	CW4504 - Manning Southern Toilet Block Replacement	Y	4504	6200		272,896				(272,896.00)
543 - Facilities Mtce and Management	CW	CW4527 - Replace the AC units servicing the Admin IT Ser	Y	4527	6200		25,240				(25,240.00)
543 - Facilities Mtce and Management	CW	CW4529 - Replace the AC units Spearwood library	Y	4529	6200		4,000				(4,000.00)
543 - Facilities Mtce and Management	CW	CW4530 - Replace the AC Seniors Ctr activity rooms	Y	4530	6200		43,300				(43,300.00)
543 - Facilities Mtce and Management	CW	CW5400 - Malabar Park (BMX) landscaping design works	Y	5400	6200		8,140				(8,140.00)

### 2013/14 Carried Forwards Information

Service Unit	Ledger OP/CW	Project Description	Approved	Project #	Expenditure		Expenditure/TF to Reserves	Income Source			
					Natural Account	External		External	Reserves	Restricted Grants & Contributions	Municipal
543 - Facilities Mtce and Management	OP	OP6248 - Standby Generator	Y	6248	6200		11,000				(11,000.00)
543 - Facilities Mtce and Management	OP	OP8230 - Cockburn Urban Art Projects	Y	8230	6200		2,348			(2,348.00)	0.00
543 - Facilities Mtce and Management	OP	OP9068 - Sports Walk of Fame -OP Executive Support Services –	Y	9068	6200		6,758				(6,758.00)
543 - Facilities Mtce and Management	OP	OP9176 - Coastal Vulnerability & Adaption Study	Y	9176	6200	5110	72,205			(72,205.00)	0.00
544 - Plant Maintenance	CW	CW7209 - Light Fleet PL 209 Manager Engineering Works M.Lugod	Y	7209	6200		33,000		(33,000.00)		0.00
544 - Plant Maintenance	CW	CW7282 - New Roads Graffiti Truck PL282	Y	7282	6200	9900	65,000	(13,000.00)	(52,000.00)		0.00
544 - Plant Maintenance	CW	CW7285 - Light Fleet Ranger Svs Extracab 4x4 Ute PL285	Y	7285	6200	9900	40,000	(14,000.00)	(26,000.00)		0.00
544 - Plant Maintenance	CW	CW7551 - Light Fleet PL551 Building Coordinator	Y	7551	6200		33,000				(33,000.00)
544 - Plant Maintenance	CW	CW7554 - New Car Client Liaison/Carer Support Officer - HACC	Y	7554	6200		33,000		(33,000.00)		0.00
544 - Plant Maintenance	CW	CW7716 - Truck Under 5T - Roads Services	Y	7716	6200	9900	80,000	(25,000.00)	(55,000.00)		0.00
544 - Plant Maintenance	CW	CW7755 - Heavy Fleet-Waste Iveco F2350G/260 Rubbish Truck PL7551	Y	7755	6200	9900	350,000	(62,000.00)	(288,000.00)		0.00
544 - Plant Maintenance	CW	CW7756 - New Waste Collection Truck PL756-1	Y	7756	6200	9900	330,000	(62,000.00)	(268,000.00)		0.00
544 - Plant Maintenance	CW	CW7757 - Isuzu FVZ 1400 Water Truck - Roads	Y	7757	6200	9900	280,000	(60,000.00)	(220,000.00)		0.00
544 - Plant Maintenance	CW	CW7783 - Heavy Fleet Side Loader Recycling Waste Truck	Y	7783	6200		340,000		(340,000.00)		0.00
							<b>28,159,717</b>	<b>(5,264,560)</b>	<b>(11,882,349)</b>	<b>(2,122,133)</b>	<b>(8,890,675)</b>

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2014 (Final)

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Operating Revenue</b>						
Governance	67,952,303	66,374,953	2%	1,577,350 ✓	66,374,953	67,587,336
Financial Services	928,838	908,954	2%	19,884	908,954	684,954
Information Services	9,200	3,706	148%	5,494	3,706	3,706
Human Resource Management	328,005	133,789	145%	194,216 ✓	133,789	133,789
Library Services	48,696	49,532	-2%	(836)	49,532	49,532
Community Services	7,293,629	7,067,161	3%	226,469 ✓	7,067,161	6,898,253
Human Services	6,717,632	6,550,099	3%	167,533 ✓	6,550,099	6,414,360
Corporate Communications	39,512	32,736	21%	6,776	32,736	12,736
Development Services	3,907,161	3,607,880	8%	299,281 ✓	3,607,880	3,310,270
Planning Services	1,482,239	1,329,514	11%	152,725 ✓	1,329,514	1,279,514
Waste Services	29,903,875	31,710,988	-6%	(1,807,113) ✗	31,710,988	36,102,121
Parks & Environmental Services	95,303	27,963	241%	67,340	27,963	6,963
Engineering Services	387,256	208,988	85%	178,268 ✓	208,988	208,988
Infrastructure Services	199,341	158,304	26%	41,037	158,304	8,304
	119,292,989	118,164,566	1%	1,128,422	118,164,566	122,700,826
Less: Restricted Grants & Contributions b/fwd	-	-	0%	-	-	-
<b>Total Operating Revenue</b>	<b>119,292,989</b>	<b>118,164,566</b>	<b>1%</b>	<b>1,128,422</b>	<b>118,164,566</b>	<b>122,700,826</b>
<b>Operating Expenditure</b>						
Governance	(4,344,929)	(4,920,221)	-12%	575,291 ✓	(4,920,221)	(4,942,112)
Financial Services	(5,752,240)	(5,810,194)	-1%	57,954	(5,810,194)	(5,287,789)
Information Services	(4,221,944)	(4,538,217)	-7%	316,274 ✓	(4,538,217)	(4,126,942)
Human Resource Management	(2,243,313)	(2,253,541)	0%	10,228	(2,253,541)	(2,221,344)
Library Services	(2,707,800)	(2,794,948)	-3%	87,147	(2,794,948)	(2,778,074)
Community Services	(8,858,816)	(9,499,789)	-7%	640,973 ✓	(9,499,789)	(9,087,564)
Human Services	(7,911,815)	(8,166,646)	-3%	254,831 ✓	(8,166,646)	(7,582,097)
Corporate Communications	(2,548,962)	(2,642,893)	-4%	93,931	(2,642,893)	(2,592,517)
Development Services	(4,583,373)	(4,928,718)	-7%	345,345 ✓	(4,928,718)	(4,681,677)
Planning Services	(2,354,511)	(1,758,929)	34%	(595,582) ✗	(1,758,929)	(1,454,445)
Waste Services	(17,628,252)	(17,834,246)	-1%	205,994 ✓	(17,834,246)	(19,937,008)
Parks & Environmental Services	(11,314,018)	(10,768,912)	5%	(545,107) ✗	(10,768,912)	(10,482,547)
Engineering Services	(7,349,778)	(7,578,222)	-3%	228,445 ✓	(7,578,222)	(7,578,222)
Infrastructure Services	(8,385,454)	(7,909,357)	6%	(476,097) ✗	(7,909,357)	(7,681,404)
	(90,205,204)	(91,404,831)	-1%	1,199,628	(91,404,831)	(90,433,743)

Attach 2

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2014 (Final)

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Less: Net Internal Recharging	3,038,200	3,090,592	-2%	(52,392)	3,090,592	3,085,871
Add: Reverse Impairment Charge - Investments	-	-	0%	-	-	-
Add: Depreciation on Non-Current Assets						
Computer & Electronic Equip	(154,245)	(140,256)	10%	(13,989)	(140,256)	(140,256)
Furniture & Equipment	(163,828)	(163,668)	0%	(160)	(163,668)	(163,668)
Plant & Machinery	(2,964,495)	(3,236,760)	-8%	272,265	(3,236,760)	(3,236,760)
Buildings	(3,290,811)	(3,943,239)	-17%	652,428	(3,943,239)	(3,943,239)
Roads	(9,018,452)	(9,283,992)	-3%	265,540	(9,283,992)	(9,283,992)
Drainage	(2,181,134)	(2,253,252)	-3%	72,118	(2,253,252)	(2,253,252)
Footpaths	(1,106,165)	(1,118,532)	-1%	12,367	(1,118,532)	(1,118,532)
Parks Equipment	(3,067,741)	(2,066,748)	48%	(1,000,993)	(2,066,748)	(2,066,748)
	<b>(21,946,870)</b>	<b>(22,206,447)</b>	<b>-1%</b>	<b>259,577</b>	<b>(22,206,447)</b>	<b>(22,206,447)</b>
<b>Total Operating Expenditure</b>	<b>(109,113,874)</b>	<b>(110,520,687)</b>	<b>-1%</b>	<b>1,406,813</b>	<b>(110,520,687)</b>	<b>(109,554,318)</b>
<b>Change in Net Assets Resulting from Operations</b>	<b>10,179,115</b>	<b>7,643,880</b>	<b>33%</b>	<b>2,535,235</b>	<b>7,643,880</b>	<b>13,146,507</b>
<b>Non-Operating Activities</b>						
<b>Profit/(Loss) on Assets Disposal</b>						
Plant & Machinery	288,036	(416,641)	-169%	704,677	(416,641)	(627,141)
Freehold Land	(833,841)	5,146,427	-116%	(5,980,268)	5,146,427	2,783,700
Furniture & Office Equipment	(6,165)	-	0%	(6,165)	-	-
Buildings	(44,759)	-	0%	(44,759)	-	-
	<b>(596,729)</b>	<b>4,729,786</b>	<b>-113%</b>	<b>(5,326,515)</b>	<b>4,729,786</b>	<b>2,156,559</b>
Less: Movement in Joint Venture	-	-			-	-
Less: Underground Power Infrastructure Contribution	(594,426)	(595,000)	0%	574	(595,000)	(1,040,000)
<b>Asset Acquisitions</b>						
Land and Buildings	(27,348,239)	(36,381,148)	-25%	9,032,908	(36,381,148)	(25,506,000)
Infrastructure Assets	(17,965,745)	(26,769,134)	-33%	8,803,390	(26,769,134)	(17,598,224)
Plant and Machinery	(2,543,977)	(4,393,413)	-42%	1,849,436	(4,393,413)	(3,899,500)
Furniture and Equipment	(19,695)	(20,000)	-2%	305	(20,000)	(139,000)
Computer Equipment	(750,716)	(1,528,088)	-51%	777,372	(1,528,088)	(540,000)
Note 1.	<b>(48,628,372)</b>	<b>(69,091,783)</b>	<b>-30%</b>	<b>20,463,411</b>	<b>(69,091,783)</b>	<b>(47,682,724)</b>
Add: Transfer to Reserves	(48,931,524)	(45,840,580)	7%	(3,090,944)	(45,840,580)	(33,226,292)
	<b>(88,571,936)</b>	<b>(103,153,698)</b>	<b>-14%</b>	<b>14,581,762</b>	<b>(103,153,698)</b>	<b>(66,645,950)</b>

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 30 June 2014 (Final)

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Add Funding from</b>						
Grants & Contributions - Asset Development	14,465,768	9,262,872	56%	5,202,896 ✓	9,262,872	5,629,495
Less: held in restricted funds from prior years	-	-	0%	-	-	-
Proceeds on Sale of Assets	4,403,544	8,580,727	-49%	(4,177,183) X	8,580,727	6,007,500
Reserves	38,732,421	52,426,202	-26%	(13,693,780) X	52,426,202	36,284,216
Loan Funds Raised	-	-	0%	-	-	-
Contributed Developer Assets	5,260,583	-	0%	5,260,583 ✓	-	-
	<b>(25,709,619)</b>	<b>(32,883,897)</b>	<b>-22%</b>	<b>7,174,277</b>	<b>(32,883,897)</b>	<b>(18,724,739)</b>
<b>Less: Transfer from Reserves - Impaired Investments</b>	-	-	0%	-	-	-
<b>Non-Cash/Non-Current Item Adjustments</b>						
Depreciation on Assets	21,946,870	22,206,447	-1%	(259,577) X	22,206,447	22,206,447
Profit/(Loss) on Assets Disposal	596,729	(4,729,786)	-113%	5,326,515 ✓	(4,729,786)	(2,156,559)
Loan Repayments	(1,313,314)	(1,325,149)	-1%	11,835	(1,325,149)	(1,325,149)
Joint Venture Investment	-	-	0%	-	-	-
Non-Current Accrued Debtors	-	-	0%	-	-	-
Non-Current Leave Provisions	232,226	-	0%	232,226 ✓	-	-
Net Change in Restricted/Committed Cash	6,281,369	6,281,369	0%	-	6,281,369	-
Deferred Pensioners Adjustment	-	-	0%	-	-	-
	<b>2,034,260</b>	<b>(10,451,016)</b>	<b>-119%</b>	<b>12,485,276</b>	<b>(10,451,016)</b>	<b>0</b>
Opening Funds	11,247,256	11,247,256	0%	(0)	11,247,256	-
<b>Closing Funds</b>	<b>13,281,518</b>	<b>796,241</b>	<b>1568%</b>	<b>12,485,278</b>	<b>796,241</b>	-
	-	-		<b>(2)</b>	-	-

Note 2, 3.

## Notes to Statement of Financial Activity

### Note 1.

Additional information on the capital works program including committed orders at end of month:

Assets Classification	Actuals	Commitments at Month End	Commitments & Actuals YTD	YTD Revised Budget	Full Year Revised Budget	Uncommitted at Month End
	\$	\$			\$	\$
Land and Buildings	(27,348,239)	(5,290,992)	(32,639,231)	(36,381,148)	(36,381,148)	3,741,917
Infrastructure Assets	(17,965,745)	(1,820,738)	(19,786,482)	(26,769,134)	(26,769,134)	6,982,652
Plant and Machinery	(2,543,977)	(1,259,880)	(3,803,857)	(4,393,413)	(4,393,413)	589,557
Furniture and Equipment	(19,695)	-	(19,695)	(20,000)	(20,000)	305
Computer Equipment	(750,716)	(260,231)	(1,010,947)	(1,528,088)	(1,528,088)	517,141
	<b>(48,628,372)</b>	<b>(8,631,840)</b>	<b>(57,260,212)</b>	<b>(69,091,783)</b>	<b>(69,091,783)</b>	<b>11,831,571</b>

### Note 2.

Closing Funds in the Financial Activity Statement are represented by:

	Actuals	YTD Revised Budget	Full Year Revised Budget	Adopted Budget
	\$	\$	\$	\$
<b>Current Assets</b>				
Cash & Investments	104,834,013	69,606,145	69,606,145	65,409,779
Rates Outstanding	186,655	-	-	-
Rubbish Charges Outstanding	246,371	-	-	-
Sundry Debtors	3,075,643	-	-	-
GST Receivable	1,149,214	-	-	-
Prepayments	552,401	-	-	-
Accrued Debtors	311,508	-	-	-
Stock on Hand	39,421	-	-	-
	<b>110,395,225</b>	<b>69,606,145</b>	<b>69,606,145</b>	<b>65,409,779</b>
<b>Current Liabilities</b>				
Creditors	(8,374,009)	-	-	-
Income Received in Advance	-	-	-	-
GST Payable	(249,936)	-	-	-
Withholding Tax Payable	-	-	-	-
Provision for Annual Leave	(2,601,865)	-	-	-
Provision for Long Service Leave	(1,854,079)	-	-	-
	<b>(13,079,889)</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Net Current Assets</b>	<b>97,315,336</b>	<b>69,606,145</b>	<b>69,606,145</b>	<b>65,409,779</b>
<b>Add: Non Current Investments</b>	<b>4,357,037</b>	<b>-</b>	<b>-</b>	<b>-</b>
	<b>101,672,373</b>	<b>69,606,145</b>	<b>69,606,145</b>	<b>65,409,779</b>
<b>Less: Restricted/Committed Assets</b>				
Cash Backed Reserves #	(85,588,563)	(68,809,904)	(68,809,904)	(62,309,778)
Deposits & Bonds Liability *	(2,802,292)	-	-	-
Grants & Contributions Unspent *	-	-	-	(3,100,000)
	<b>13,281,518</b>	<b>796,241</b>	<b>796,241</b>	<b>0</b>
<b>Closing Funds (as per Financial Activity Statement)</b>	<b>13,281,518</b>	<b>796,241</b>	<b>796,241</b>	<b>0</b>

# See attached Reserve Fund Statement

\* See attached Restricted Funds Analysis

Note 3.

Amendments to original budget since budget adoption. Surplus/(Deficit)

Ledger	Project/ Activity	Description	Council Resolution	Classification	Non Change - (Non Cash Items) Adjust.	Increase in Available Cash	Decrease in Available Cash	Amended budget Running Balance
					\$	\$	\$	\$
		<b>Budget Adoption</b>		<b>Closing Funds Surplus(Deficit)</b>				<b>0</b>
	590 to							
GL	595	Adjust SLLC salaries including fixing error in salary level		Operating Expenditure		75,762		75,762
GL	241	Extra income from activity for the first six months		Operating Income		649		76,411
	161, 162,							
GL	175	Balancing FESA budget according to its funding		Operating Expenditure		2,568		78,979
OP	628	Adjusting Summer of Fun events according to OCM	OCM July13 17.3	Operating Expenditure			5,175	73,804
OP	9170	Correcting funding for Offset Surf Life Saving Club		Operating Income		23,000		96,804
CW	2075	Adding owners contribution to Crossover Construction project		Operating Income		30,000		126,804
OP	6818	New commercial lease at Orsino Boulevard, North Coogee		Operating Income		5,000		131,804
GL	105	Extra Financial Assistance Grant received		Operating Income		167,547		299,351
GL	165	New income from Cats Legislation		Operating Income		10,000		309,351
		Adjusting carry forward budget by increasing Council admin charge and						
GL	202	workers compensation insurance		Operating Expenditure		6,841		316,192
GL	323	Increase in hire income - Youth Axis Room		Operating Income		2,250		318,442
OP	9470	Grant for Regional Concert		Operating Income		20,000		338,442
				Operating Expenditure &				
Various		Mid-year budget review	OCM 13 Feb 14	Income		165,114		503,556
GL	105	Signage correction for mid-year budget review adjustment		Operating Income		329,130		832,686
			OCM May14					
OP	Various	Grants and donations budget adjustments	13.2	Operating Expenditure			36,445	796,241
				<b>Closing Funds Surplus (Deficit)</b>	<b>0</b>	<b>837,861</b>	<b>41,620</b>	<b>796,241</b>

# Statement of Comprehensive Income *by Nature and Type*

for the period ended 30 June 2014

	Actual	Amended YTD Budget	\$ Variance to YTD Budget	Forecast	Amended Budget	Adopted Budget
	\$	\$	\$	\$	\$	\$
<b>OPERATING REVENUE</b>						
01 Rates	58,521,022	57,916,814	604,209	58,521,022	57,916,814	57,916,814
02 Specified Area Rates	271,464	235,000	36,464	271,464	235,000	235,000
05 Fees and Charges	Note 1 40,700,398	42,291,054	(1,590,655)	40,700,398	42,291,054	46,273,232
06 Service Charges	3,842,864	3,700,000	142,864	3,842,864	3,700,000	3,700,000
10 Grants and Subsidies	8,545,383	7,956,788	588,594	8,545,383	7,956,788	9,046,274
15 Contributions, Donations and Reimbursements	1,232,856	810,619	422,237	1,232,856	810,619	474,614
20 Interest Earnings	6,159,822	5,244,826	914,996	6,159,822	5,244,826	5,044,826
25 Other revenue and Income	19,179	9,466	9,713	19,179	9,466	10,066
<b>Total Operating Revenue</b>	<b>119,292,989</b>	<b>118,164,566</b>	<b>1,128,422</b>	<b>119,292,989</b>	<b>118,164,566</b>	<b>122,700,826</b>
<b>OPERATING EXPENDITURE</b>						
50 Employee Costs - Salaries & Direct Oncosts	Note 2 (40,946,739)	(40,562,720)	(384,019)	(40,946,739)	(40,562,720)	(40,783,674)
51 Employee Costs - Indirect Oncosts	(1,177,444)	(955,938)	(221,507)	(1,177,444)	(955,938)	(929,483)
55 Materials and Contracts	Note 3 (35,102,733)	(35,908,891)	806,158	(35,102,733)	(35,908,891)	(33,543,022)
65 Utilities	(4,212,014)	(4,404,779)	192,765	(4,212,014)	(4,404,779)	(4,315,599)
70 Interest Expenses	(183,342)	(171,505)	(11,837)	(183,342)	(171,505)	(171,505)
75 Insurances	(2,245,574)	(2,235,067)	(10,507)	(2,245,574)	(2,235,067)	(2,005,067)
80 Other Expenses	(6,337,358)	(7,165,932)	828,574	(6,337,358)	(7,165,932)	(8,685,393)
85 Depreciation on Non Current Assets	(21,946,870)	(22,206,447)	259,577	(21,946,870)	(22,206,447)	(22,206,447)
<b>Add Back: Indirect Costs Allocated to Capital Works</b>	3,038,200	3,090,592	(52,392)	3,038,200	3,090,592	3,085,871
<b>Total Operating Expenditure</b>	<b>(109,113,874)</b>	<b>(110,520,687)</b>	<b>1,406,813</b>	<b>(109,113,874)</b>	<b>(110,520,687)</b>	<b>(109,554,318)</b>
<b>CHANGE IN NET ASSETS RESULTING FROM OPERATING</b>						
<b>ACTIVITIES</b>	<b>10,179,115</b>	<b>7,643,880</b>	<b>2,535,235</b>	<b>10,179,115</b>	<b>7,643,880</b>	<b>13,146,507</b>
<b>NON-OPERATING ACTIVITIES</b>						
11 Capital Grants & Subsidies	4,402,061	5,276,828	(874,767)	4,402,061	5,276,828	2,081,658
16 Contributions - Asset Development	10,063,707	3,986,044	6,077,663	10,063,707	3,986,044	3,547,837
95 Profit/(Loss) on Sale of Assets	(596,729)	4,729,786	(5,326,515)	(596,729)	4,729,786	2,156,559
57 Acquisition of Crown Land for Roads	-	-	-	-	-	-
58 Underground Power Scheme	(594,426)	(595,000)	574	(594,426)	(595,000)	(1,040,000)
<b>Total Non-Operating Activities</b>	<b>13,274,613</b>	<b>13,397,658</b>	<b>(123,045)</b>	<b>13,274,613</b>	<b>13,397,658</b>	<b>6,746,054</b>
<b>NET RESULT</b>	<b>23,453,728</b>	<b>21,041,538</b>	<b>2,412,190</b>	<b>23,453,728</b>	<b>21,041,538</b>	<b>19,892,561</b>

## Notes to Statement of Comprehensive Income

### Note 1.

Additional information on main sources of revenue in fees & charges.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
<b><u>Community Services:</u></b>				
Recreational Services	611,115	524,136	524,136	524,136
South Lake Leisure Centre	2,807,131	2,992,450	2,992,450	2,941,890
Law and Public Safety	558,324	399,737	399,737	361,346
	<u>3,976,570</u>	<u>3,916,323</u>	<u>3,916,323</u>	<u>3,827,372</u>
<b><u>Waste Services:</u></b>				
Waste Collection Services	17,186,691	17,240,000	17,240,000	16,807,430
Waste Disposal Services	12,699,117	14,451,995	14,451,995	19,256,811
	<u>29,885,808</u>	<u>31,691,995</u>	<u>31,691,995</u>	<u>36,064,241</u>
	<u>33,862,378</u>	<u>35,608,318</u>	<u>35,608,318</u>	<u>39,891,613</u>

### Note 2.

Additional information on Salaries and Direct On-Costs by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(1,896,577)	(1,940,610)	(1,940,610)	(2,242,610)
Finance & Corporate Services Division	(6,248,914)	(6,204,163)	(6,204,163)	(6,150,515)
Community Services Division	(12,406,959)	(12,355,981)	(12,355,981)	(12,187,347)
Planning & Development Division	(4,778,994)	(4,602,322)	(4,602,322)	(4,743,558)
Engineering & Works Division	(15,615,295)	(15,459,644)	(15,459,644)	(15,459,644)
	<u>(40,946,739)</u>	<u>(40,562,720)</u>	<u>(40,562,720)</u>	<u>(40,783,674)</u>

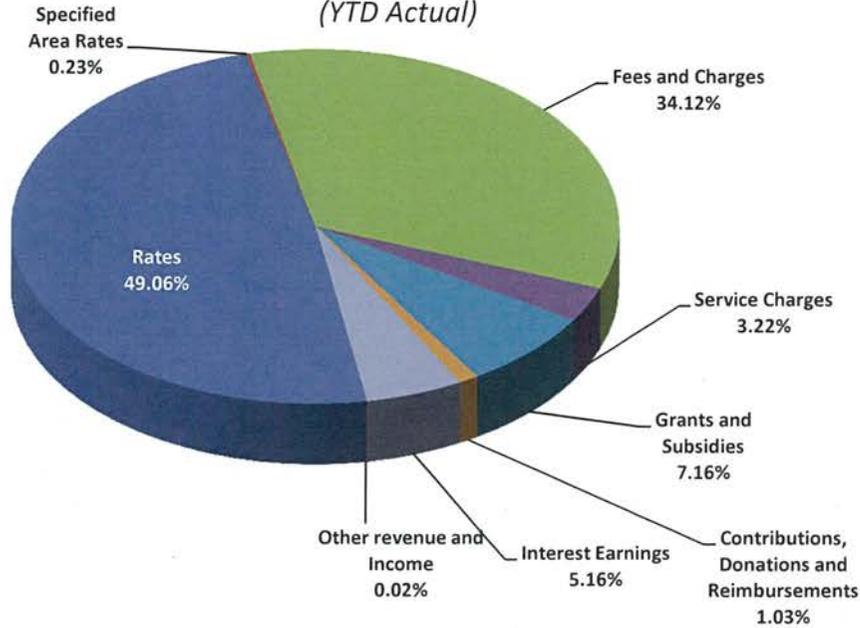
### Note 3

Additional information on Materials and Contracts by each Division.

	Actual	Amended YTD Budget	Amended Budget	Adopted Budget
	\$	\$	\$	\$
Executive Division	(1,654,397)	(2,014,625)	(2,014,625)	(1,839,190)
Finance & Corporate Services Division	(2,975,814)	(3,741,486)	(3,741,486)	(3,089,257)
Community Services Division	(7,628,152)	(8,515,179)	(8,515,179)	(7,652,734)
Planning & Development Division	(1,978,573)	(1,974,133)	(1,974,133)	(1,285,508)
Engineering & Works Division	(20,865,798)	(19,663,468)	(19,663,468)	(19,676,332)
Not Applicable	0	0	0	0
	<u>(35,102,733)</u>	<u>(35,908,891)</u>	<u>(35,908,891)</u>	<u>(33,543,022)</u>

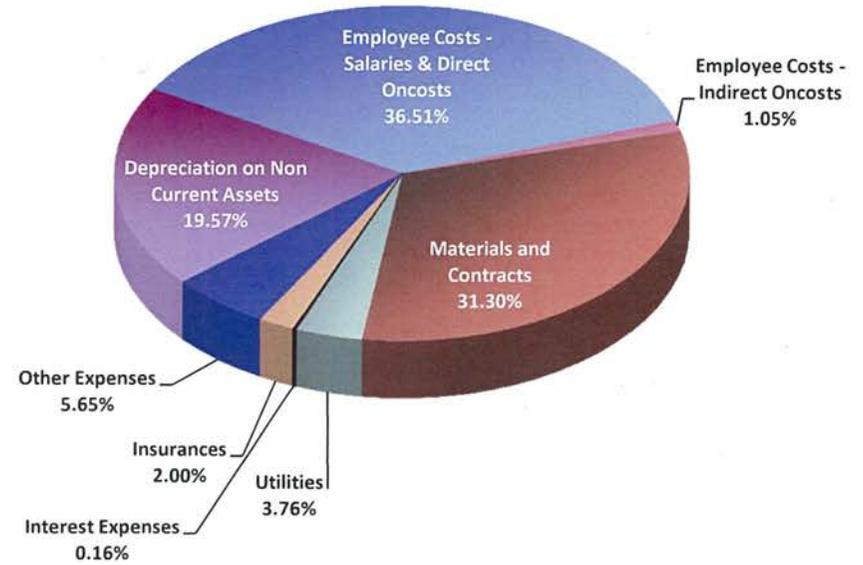
### Operating Income by Nature and Type

(YTD Actual)



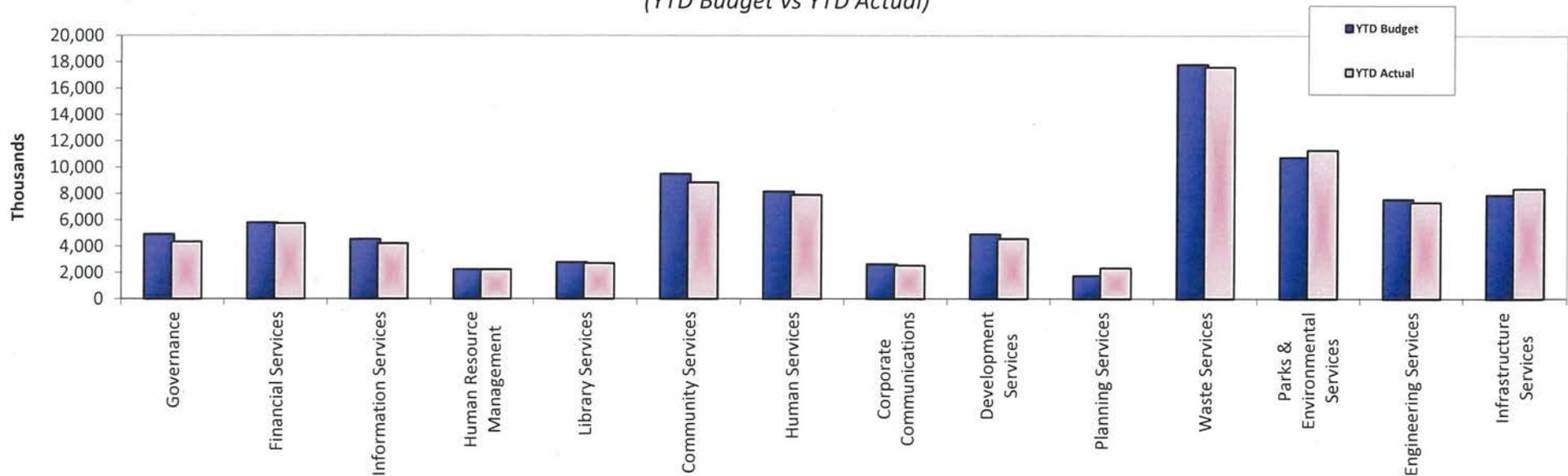
### Operating Expenditure by Nature and Type

(YTD Actual)

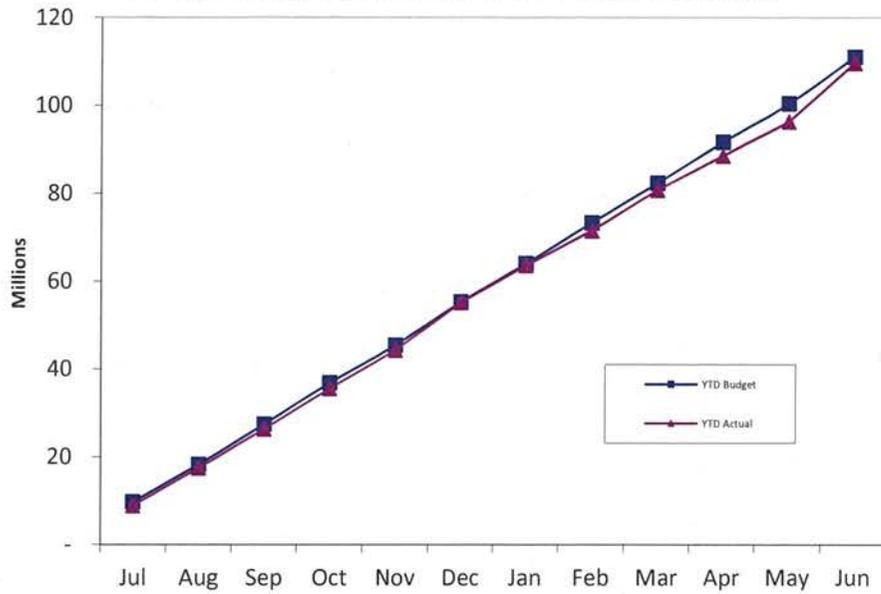


### Operating Expenditure by Business Unit

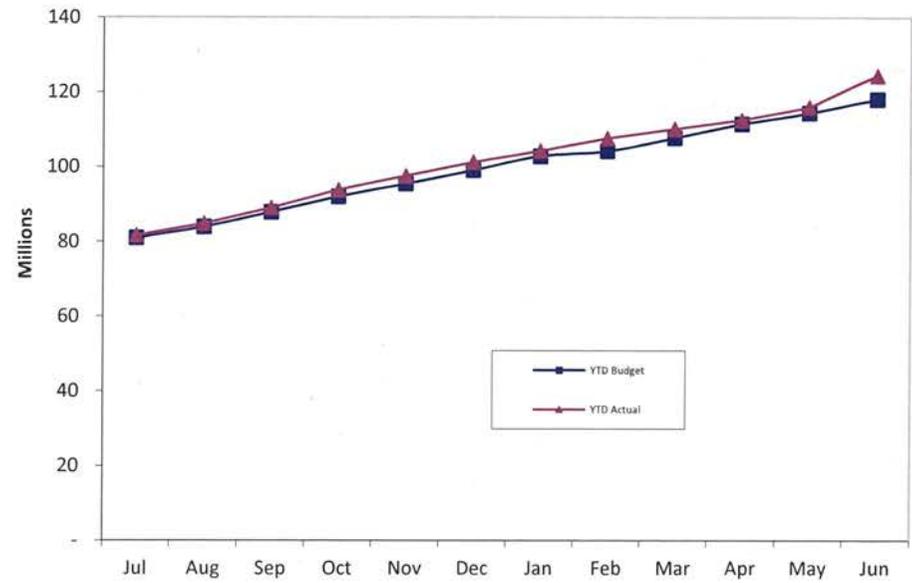
(YTD Budget vs YTD Actual)



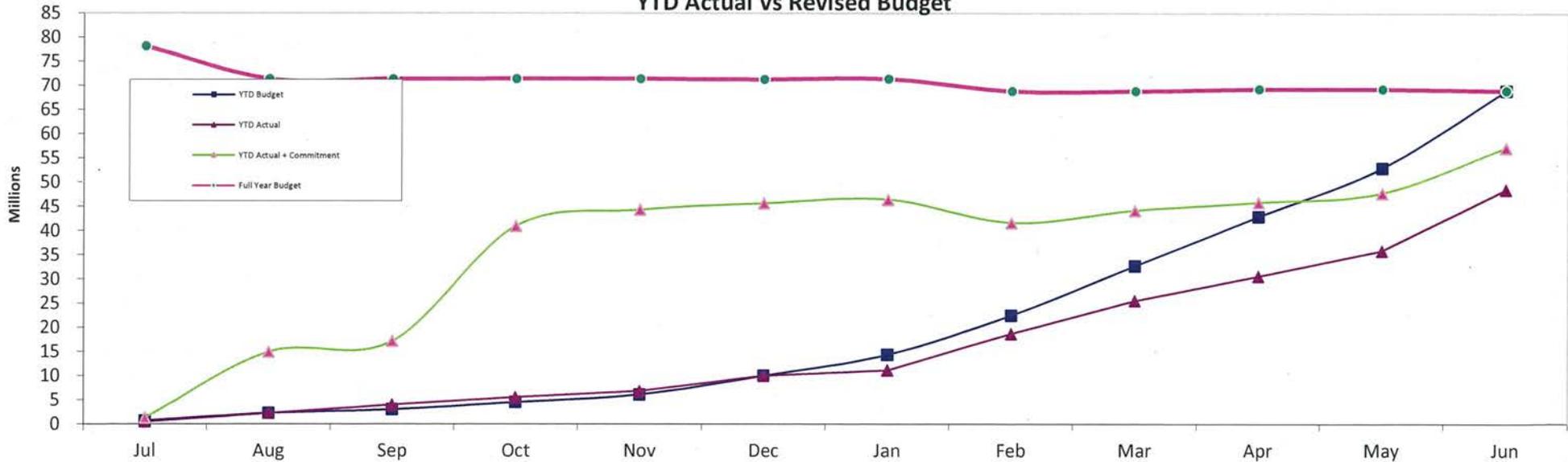
**YTD Operating Expenditure Vs YTD Revised Budget**



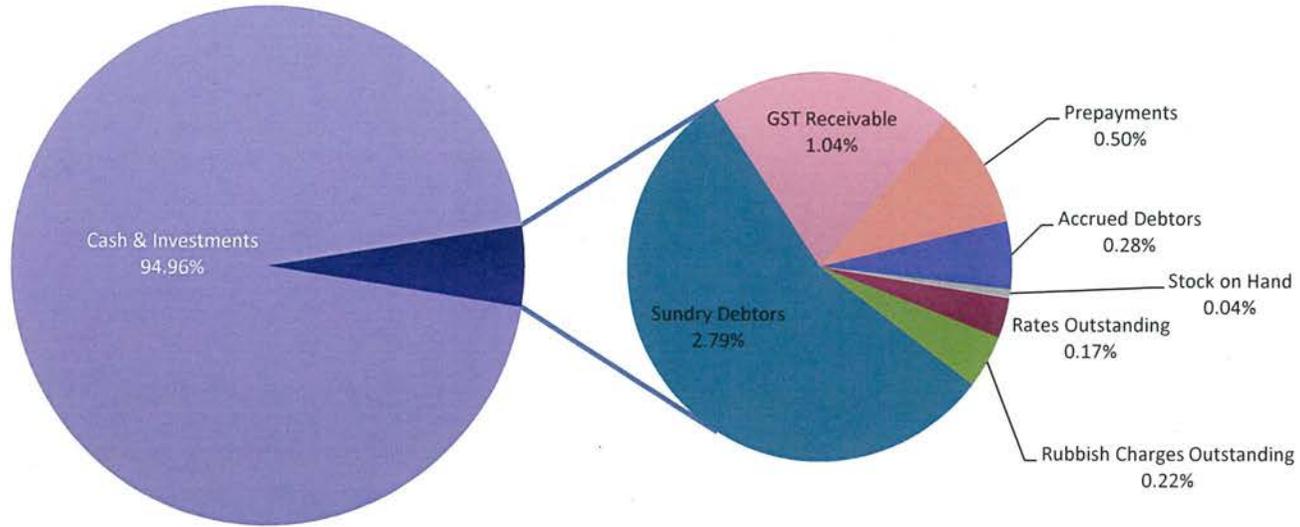
**YTD Operating Income Vs YTD Revised Budget**



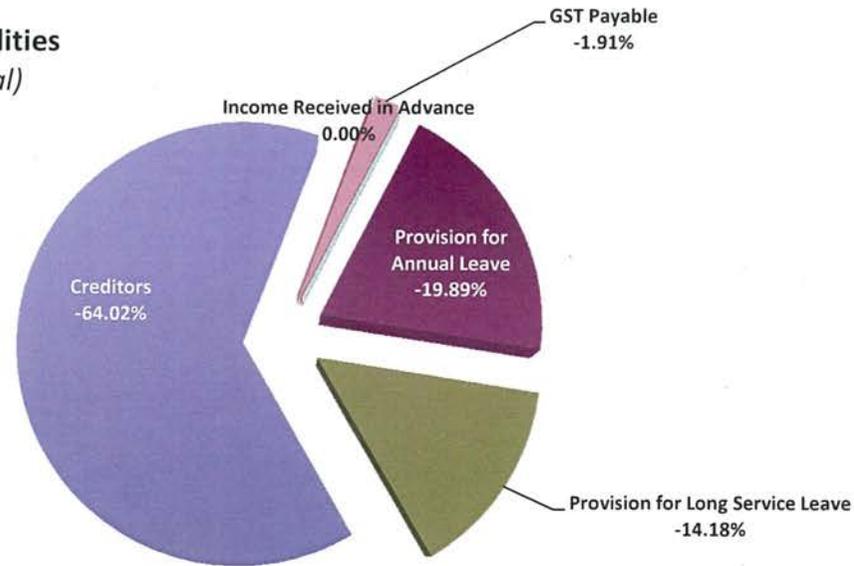
**Capital Expenditure  
YTD Actual Vs Revised Budget**



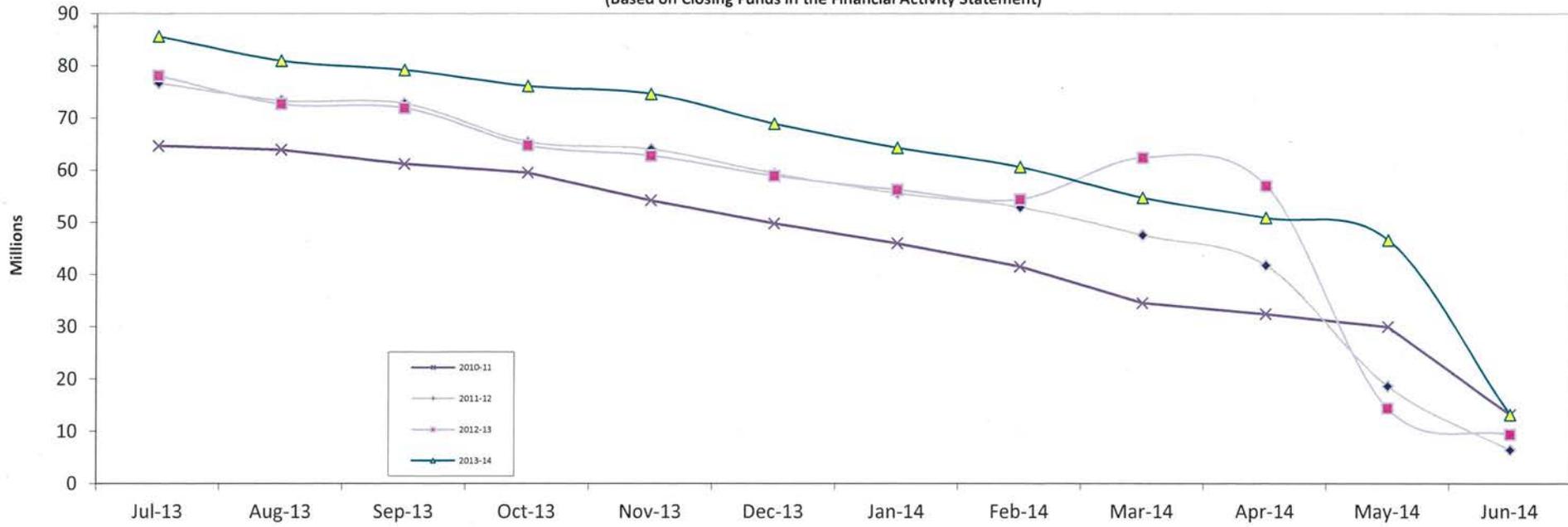
**Current Assets**  
(YTD Actual)



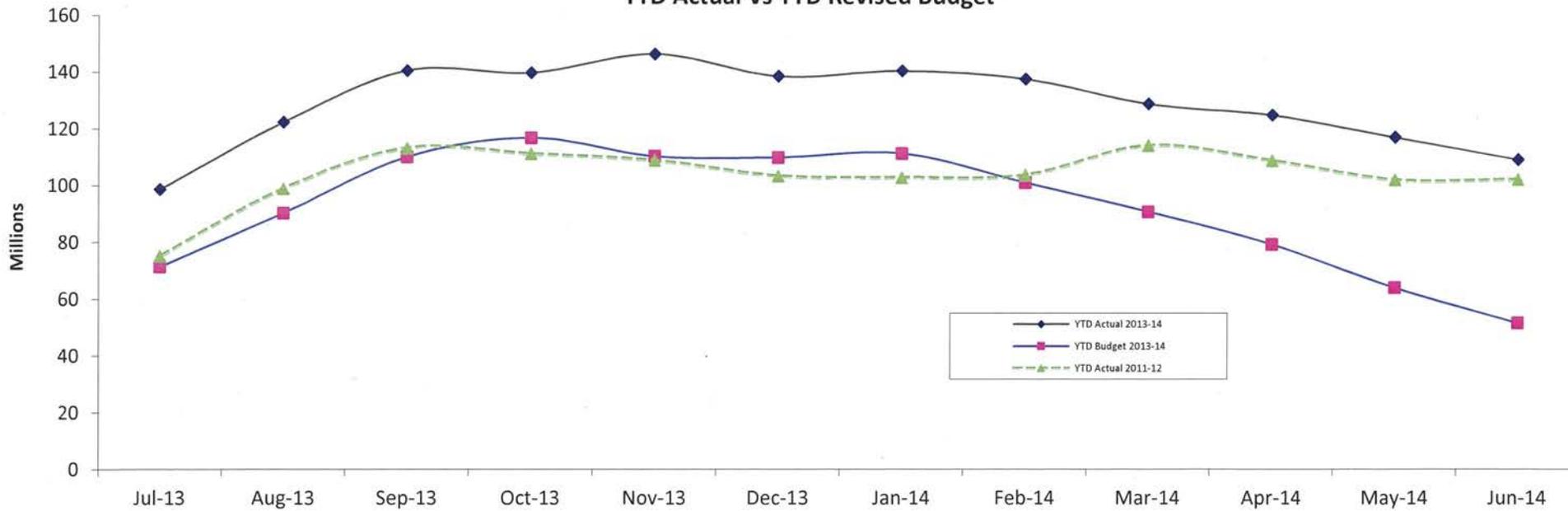
**Current Liabilities**  
(YTD Actual)



### Municipal Liquidity Over the Year (Based on Closing Funds in the Financial Activity Statement)



### Cash & Investments Positions YTD Actual Vs YTD Revised Budget



## City of Cockburn - Reserve Funds

### Financial Statement for Period Ending 30 June 2014

Account Details	Opening Balance		Interest Received		t/f's from Municipal		t/f's to Municipal		Closing Balance	
	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget	Actual
<b>Council Funded</b>										
Bibra Lake Management Plan Reserve	1,014,207	1,014,207	-	25,171	-	-	(191,559)	(191,559)	822,648	847,819
Bibra Lake Nutrient Management	305,625	305,625	10,395	7,823	-	-	-	-	316,020	313,447
Carbon Pollution Reduct Scheme Res CPRS	-	-	-	-	-	-	-	-	-	-
Community Infrastructure	10,890,947	10,890,947	172,410	212,151	8,733,787	8,733,787	(10,684,218)	(10,006,312)	9,112,925	9,830,572
Community Surveillance Levy Reserve	498,556	498,556	25,230	12,136	193,294	259,509	(254,258)	(116,360)	462,822	653,841
Contaminated Sites	1,999,849	1,999,849	43,790	50,759	500,000	500,000	(200,000)	(31,726)	2,343,639	2,518,882
DCD Redundancies Reserve	2,916	2,916	-	75	-	-	-	-	2,916	2,991
Environmental Offset Reserve	357,376	357,376	-	8,486	-	-	(138,591)	(88,494)	218,785	277,368
Green House Emissions Reductions	579,053	579,053	11,792	14,195	200,000	200,000	(507,000)	(140,733)	283,845	652,516
Information Technology	428,166	428,166	31,635	8,953	124,671	124,671	(487,550)	(300,189)	96,922	261,600
Land Development & Investment Fund Reserve	13,933,953	13,933,953	241,058	328,534	7,683,727	3,670,520	(17,506,991)	(13,213,552)	4,351,747	4,719,455
Major Buildings Refurbishment	2,409,325	2,409,325	22,038	61,539	2,998,545	2,998,545	(30,043)	(30,043)	5,399,865	5,439,366
Mobile Rubbish Bins	209,552	209,552	20,773	3,081	-	-	(170,000)	(149,353)	60,325	63,279
Municipal Elections	493,285	493,285	11,274	8,635	-	-	(490,000)	(452,198)	14,559	49,721
Naval Base Shacks	596,438	596,438	13,956	15,573	158,854	158,854	(220,228)	(3,945)	549,020	766,921
Plant & Vehicle Replacement	3,731,633	3,731,633	65,118	84,821	3,469,500	3,377,920	(2,896,600)	(1,263,828)	4,369,651	5,930,546
Port Coogee Special Maintenance Reserve	809,083	809,083	19,595	21,827	235,000	271,464	(96,907)	(96,907)	966,771	1,005,468
Roads & Drainage Infrastructure	2,087,403	2,087,403	86,338	33,059	1,250,000	1,250,000	(2,984,325)	(1,344,311)	439,415	2,026,150
Staff Payments & Entitlements	2,261,717	2,261,717	133,904	54,883	105,000	105,000	(186,000)	(150,500)	2,314,621	2,271,100
Waste & Recycling	13,772,203	13,772,203	532,326	346,920	6,737,224	5,143,048	(2,963,818)	(602,925)	18,077,935	18,659,246
Waste Collection Levy	132,072	132,072	1,306	3,380	310,732	129,244	-	-	444,110	264,697
Workers Compensation	399,501	399,501	13,154	9,994	-	-	(30,000)	(30,000)	382,655	379,495
POS Cash in Lieu (Restricted Funds)	4,031,593	4,031,593	112,890	98,237	-	547,000	(436,364)	(436,364)	3,708,119	4,240,466
	<b>60,944,452</b>	<b>60,944,452</b>	<b>1,568,982</b>	<b>1,410,234</b>	<b>32,700,334</b>	<b>27,469,562</b>	<b>(40,474,452)</b>	<b>(28,649,300)</b>	<b>54,739,316</b>	<b>61,174,947</b>
<b>Grant Funded</b>										
Aged & Disabled Vehicle Expenses	424,948	424,948	9,170	10,200	68,496	68,496	(220,830)	(181,482)	281,784	322,162
Cockburn Super Clinic Reserve	4,242,180	4,242,180	121,679	80,055	-	-	(4,371,016)	(2,385,861)	(7,156)	1,936,374
Family Day Care Accumulation Fund	64,233	64,233	2,500	1,644	-	-	(2,800)	(43,493)	63,933	22,383
Naval Base Shack Removal Reserve	272,408	272,408	3,624	7,536	54,000	54,000	-	-	330,032	333,945
Restricted Grants & Contributions Reserv	-	-	-	12,145	6,287,369	8,888,133	(3,398,671)	(2,976,621)	2,888,699	5,923,657
UNDERGROUND POWER	1,301,740	1,301,740	-	36,691	1,200,000	1,328,699	(1,967,063)	(1,912,905)	534,677	754,224
Welfare Projects Employee Entitlements	452,182	452,182	16,911	10,711	106,568	494,929	(109,110)	(513,399)	466,551	444,422
	<b>6,757,691</b>	<b>6,757,691</b>	<b>153,884</b>	<b>158,982</b>	<b>7,716,433</b>	<b>10,834,256</b>	<b>(10,069,489)</b>	<b>(8,013,761)</b>	<b>4,558,519</b>	<b>9,737,168</b>
<b>Development Cont. Plans</b>										
Aubin Grove DCA	167,325	167,325	21,604	4,280	-	-	(730)	(907)	188,199	170,698
Community Infrastructure DCA 13	3,361,786	3,361,786	105,715	159,648	2,000,000	6,587,912	(129,496)	(80,001)	5,338,005	10,029,345
Gaebler Rd Development Cont. Plans	760,607	760,607	4,944	19,465	-	204,472	(8,610)	(306)	756,941	984,238
Hammond Park DCA	(9,371)	(9,371)	-	(240)	383,540	-	-	(4,569)	374,169	(14,180)
Munster Development	724,330	724,330	8,498	14,405	8,753	176,710	(15,700)	(482,920)	725,881	432,526
Muriel Court Development Contribution	(43,595)	(43,595)	-	(3,746)	206,000	139,499	(162,472)	(140,262)	(67)	(48,104)
Packham North - DCA 12	(18,720)	(18,720)	-	(1,758)	515,000	-	(75,131)	(85,314)	421,149	(105,792)
Solomon Road DCA	97,272	97,272	-	5,583	257,500	264,547	(15,060)	(7,212)	339,712	360,190
Success Lakes Development	1,582,041	1,582,041	-	40,490	-	438,437	(1,429,357)	(1,172,977)	152,684	887,991
Success Nth Development Cont. Plans	601,206	601,206	10,661	17,229	10,981	567,421	(10,410)	(306)	612,438	1,185,550
Thomas St Development Cont. Plans	11,778	11,778	-	301	-	-	-	-	11,778	12,079
Wattleup DCA 10	(4,674)	(4,674)	-	(120)	-	-	(13,010)	(4,569)	(17,684)	(9,363)
Yangebup East Development Cont. Plans	188,928	188,928	3,986	5,364	57,150	299,869	(9,010)	(57,296)	241,054	436,865
Yangebup West Development Cont. Plans	268,405	268,405	10,712	9,191	95,903	109,533	(7,210)	(32,723)	367,810	354,406
	<b>7,687,318</b>	<b>7,687,318</b>	<b>166,120</b>	<b>270,091</b>	<b>3,534,827</b>	<b>8,788,399</b>	<b>(1,876,196)</b>	<b>(2,069,360)</b>	<b>9,512,069</b>	<b>14,676,448</b>
	<b>75,389,461</b>	<b>75,389,461</b>	<b>1,888,986</b>	<b>1,839,307</b>	<b>43,951,594</b>	<b>47,092,217</b>	<b>(52,420,137)</b>	<b>(38,732,421)</b>	<b>68,809,904</b>	<b>85,588,563</b>

Attach 1 -

[Rockingham Rd - Marvell Av] Outcome of TMWS

**City of Cockburn Traffic Management Warrant System**

Road name: **Barrington Street (1030029)**  
 Suburb: **Munster/Spearwood**  
 Location detail: **Between Rockingham Rd and Marvell Ave (SLK 0.02 - 0.57)**  
 Road classification: **Local Distributor Road**  
 Reason for analysis: **Concerns about traffic**

Analysis officer: John McDonald Date: 5/09/2014

*TABLE 1 – Warrant criteria and weightings**Note: Maximum road length for each analysis = 500 metres*

<b><u>PARAMETER</u></b>		<b><u>VALUE</u></b>	<b><u>SCORE</u></b>
Traffic speed		64	15
Traffic volume		5,251	12
Reported crash data (5-year period)	Fatalities	0	0
	Injuries	0	0
	Non-injuries	4	8
Road design and topography	Restricted sight crest curve		0
	Restricted sight horizontal curve	No	0
	Bends with unrestricted sight	No	0
	Steep hill	No	0
Vulnerable road users	Major bicycle or ped. crossing point	No	0
	Important bicycle route		0
Activity generators	College	No	0
	School	No	0
	Retail	Yes	8
Amenity factors	Trucks	5.00%	10
	Peak hour volume	10.00%	0
<b>Total:</b>			<b><u>53</u></b>

*Table 2: Intervention warrant*

A technical problem site - identify solutions for consideration for funding and implementation

Comments:

## MetroCount Traffic Executive Weekly Vehicle Counts (Virtual Week)

### VirtWeeklyVehicle-480 -- English (ENA)

#### Datasets:

**Site:** [Barrington-03] Barrington Street - Between Chaucer Close and Dryden Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:06 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-03 0 2014-05-29 1258.EC0 (Plus )  
**Identifier:** FR62BYDT MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

**Filter time:** 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** North, East, South, West (bound), P = East  
**Separation:** All - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 48965 / 49007 (99.91%)

Weekly Vehicle Counts (Virtual Week)

## VirtWeeklyVehicle-480

Site: Barrington-03.0.0EW  
 Description: Barrington Street - Between Chaucer Close and Dryden Street<50>  
 Filter time: 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
 Scheme: Vehicle classification (AustRoads94)  
 Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>0)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages		
								1 - 5	1 - 7	
0000-0100	12.0	11.0	10.5	18.0	21.0	34.0	33.0	14.0	17.9	
0100-0200	7.0	6.0	9.5	15.0	8.0	18.0	32.0	9.5	12.6	
0200-0300	2.0	7.0	9.5	8.0	11.0	16.0	20.0	7.8	9.8	
0300-0400	12.0	9.0	7.0	11.5	13.0	12.0	9.0	10.0	10.1	
0400-0500	16.0	19.0	20.5	22.0	15.0	13.0	10.0	19.3	17.7	
0500-0600	101.0	96.5	108.5	93.5	91.0	49.0	20.0	98.6	85.8	
0600-0700	157.0	160.0	148.0	168.0	154.0	48.0	40.0	157.9	135.1	
0700-0800	271.0	267.5	280.5	281.5	264.0	105.0	71.0	274.3	237.0	
0800-0900	456.0	475.5	462.0	456.5	441.0	238.0	99.0	460.6	402.2	
0900-1000	323.0	331.0	310.0	292.0	336.0	304.0	180.0	315.6	300.9	
1000-1100	309.0	298.0	287.0	314.5	306.0	344.0	249.0	301.8	300.7	
1100-1200	303.0	315.5	314.0	337.5	316.0	373.0	348.0	319.1	327.4	
1200-1300	316.0	314.5	334.5	319.0	328.0	358.0	280.0	322.5	321.8	
1300-1400	291.0	308.5	290.0	295.0	348.0	323.0	262.0	304.4	301.8	
1400-1500	338.0	380.0	375.0	383.0	365.0	292.0	288.0	366.8	351.4	
1500-1600	528.5	552.0	540.0	538.0	587.0	311.0	265.0	545.8	494.2	
1600-1700	496.0	493.0	524.0	530.0	536.0	279.0	258.0	511.5	462.9	
1700-1800	456.0	490.0	530.5	493.0	448.0	305.0	256.0	486.8	445.5	
1800-1900	255.5	278.5	319.0	315.0	310.0	214.0	167.0	291.4	271.2	
1900-2000	148.5	158.0	182.5	212.0	160.0	124.0	119.0	168.8	159.3	
2000-2100	87.5	106.5	120.5	134.0	138.0	107.0	83.0	112.6	109.1	
2100-2200	63.0	79.5	88.0	86.0	74.0	90.0	54.0	77.6	76.5	
2200-2300	34.0	54.0	50.0	48.0	78.0	63.0	33.0	50.3	49.8	
2300-2400	20.0	15.5	27.0	25.0	44.0	50.0	16.0	24.3	26.0	
<b>Totals</b>										
0700-1900	4343.0	4504.0	4566.5	4555.0	4585.0	3446.0	2723.0	4500.4	4217.0	
0600-2200	4799.0	5008.0	5105.5	5155.0	5111.0	3815.0	3019.0	5017.3	4697.0	
0600-0000	4853.0	5077.5	5182.5	5228.0	5233.0	3928.0	3068.0	5091.8	4772.8	
0000-0000	5003.0	5226.0	5348.0	5396.0	5392.0	4070.0	3192.0	5250.9	4926.7	
<b>AM Peak</b>	0800	0800	0800	0800	0800	1100	1100			
	456.0	475.5	462.0	456.5	441.0	373.0	348.0			
<b>PM Peak</b>	1500	1500	1500	1500	1500	1200	1400			
	528.5	552.0	540.0	538.0	587.0	358.0	288.0			

\* - No data.

## MetroCount Traffic Executive Speed Histogram

### SpeedHist-481 -- English (ENA)

#### Datasets:

**Site:** [Barrington-03] Barrington Street - Between Chaucer Close and Dryden Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:06 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-03 0 2014-05-29 1258.EC0 (Plus )  
**Identifier:** FR62BYDT MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

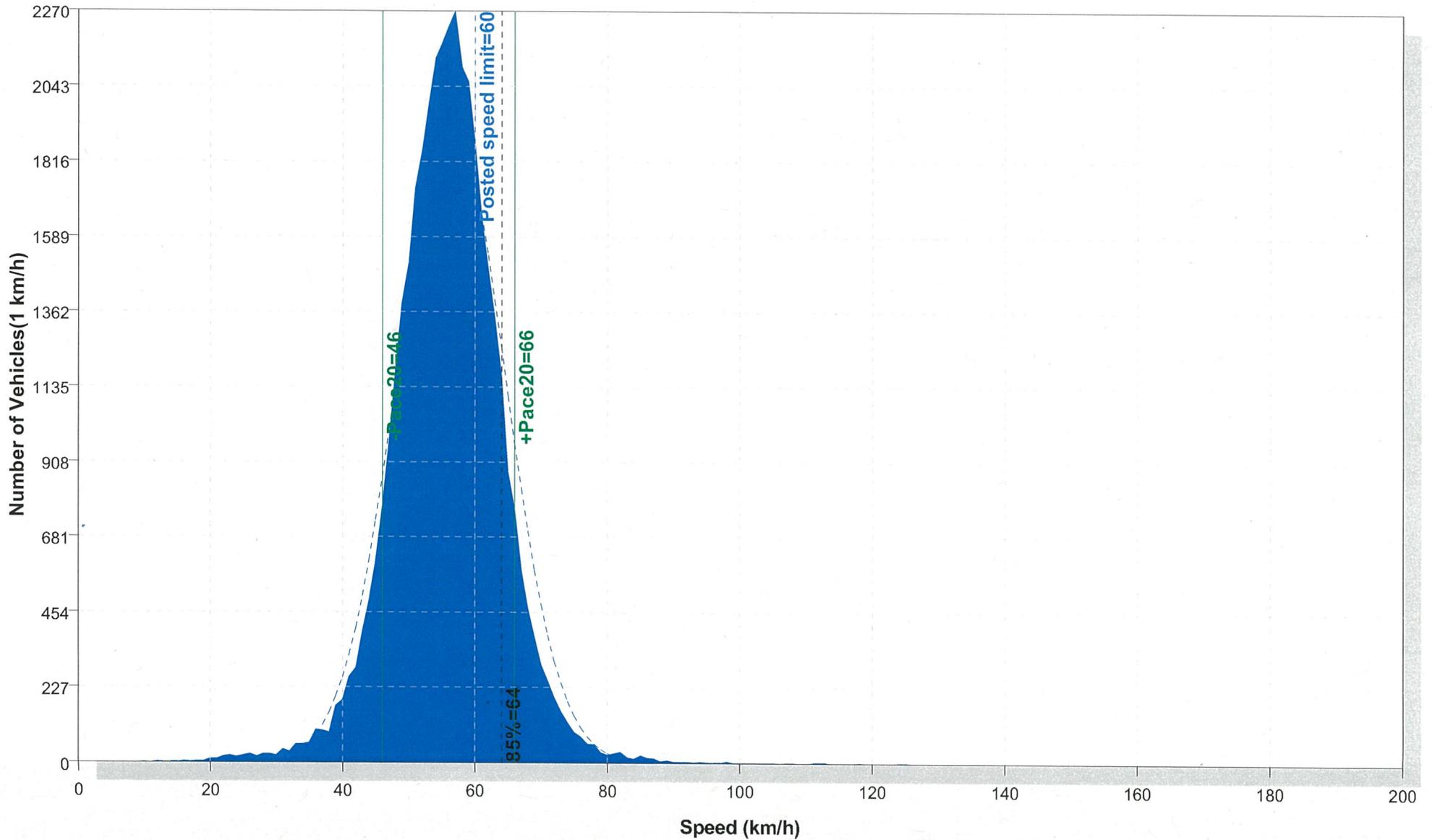
**Filter time:** 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** East, West (bound), P = East  
**Separation:** Greater than 4.00 seconds. - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 39687 / 49007 (80.98%)

#### Speed Statistics

Vehicles = 39687  
 Posted speed limit = 60 km/h, Exceeding = 12155 (30.63%), Mean Exceeding = 65.16 km/h  
 Maximum = 125.7 km/h, Minimum = 10.4 km/h, Mean = 56.4 km/h  
 85% Speed = 63.7 km/h, 95% Speed = 68.8 km/h, Median = 56.2 km/h  
 20 km/h Pace = 46 - 66, Number in Pace = 32935 (82.99%)  
 Variance = 65.91, Standard Deviation = 8.12 km/h

# Speed Histogram

**SpeedHist-481** (Metric) **Site:**Barrington-03.0.0EW  
**Description:** Barrington Street - Between Chaucer Close and Dryden Street<50>  
**Filter time:** 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Filter:** Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(EW) Sp(10,160) Headway(>4)  
**Scheme:** Vehicle classification (AustRoads94)



## MetroCount Traffic Executive Daily Classes

### DailyClass-487 -- English (ENA)

#### Datasets:

**Site:** [Barrington-03] Barrington Street - Between Chaucer Close and Dryden Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:06 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-03 0 2014-05-29 1258.EC0 (Plus )  
**Identifier:** FR62BYDT MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

**Filter time:** 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** East, West (bound), P = East  
**Separation:** All - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 48965 / 49007 (99.91%)

## Daily Classes

**DailyClass-487**

**Site:** Barrington-03.0.0EW  
**Description:** Barrington Street - Between Chaucer Close and Dryden Street<50>  
**Filter time:** 14:07 Monday, 19 May 2014 => 12:59 Thursday, 29 May 2014  
**Scheme:** Vehicle classification (AustRoads94)  
**Filter:** Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(EW) Sp(10,160) Headway(>0)

**Monday, 19 May 2014**

	1	2	3	4	5	6	7	8	9	10	11	12	Total
<b>Mon*</b>	2271	14	90	8	1	1	5	1	0	0	0	0	2391
(%)	95.0	0.6	3.8	0.3	0.0	0.0	0.2	0.0	0.0	0.0	0.0	0.0	
<b>Tue</b>	4954	36	229	24	6	3	8	0	1	0	0	0	5261
(%)	94.2	0.7	4.4	0.5	0.1	0.1	0.2	0.0	0.0	0.0	0.0	0.0	
<b>Wed</b>	4953	43	197	28	9	3	16	3	3	0	0	0	5255
(%)	94.3	0.8	3.7	0.5	0.2	0.1	0.3	0.1	0.1	0.0	0.0	0.0	
<b>Thu</b>	5034	33	212	37	5	6	7	3	3	0	0	0	5340
(%)	94.3	0.6	4.0	0.7	0.1	0.1	0.1	0.1	0.1	0.0	0.0	0.0	
<b>Fri</b>	5104	42	215	22	1	0	6	1	1	0	0	0	5392
(%)	94.7	0.8	4.0	0.4	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Sat</b>	3922	26	104	10	1	2	4	0	1	0	0	0	4070
(%)	96.4	0.6	2.6	0.2	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Sun</b>	3113	12	61	2	2	1	1	0	0	0	0	0	3192
(%)	97.5	0.4	1.9	0.1	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	

**Average daily volume**
**Entire week**

	4512	31	169	20	3	2	6	0	0	0	0	0	4751
(%)	95.0	0.7	3.6	0.4	0.1	0.0	0.1	0.0	0.0	0.0	0.0	0.0	

**Weekdays**

	5010	38	212	27	5	2	9	1	2	0	0	0	5311
(%)	94.3	0.7	4.0	0.5	0.1	0.0	0.2	0.0	0.0	0.0	0.0	0.0	

**Weekend**

	3517	18	82	6	1	1	2	0	0	0	0	0	3630
(%)	96.9	0.5	2.3	0.2	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	

\* - Incomplete

# Crash Factor Matrix

Parameter	Value
Job Id	85394705
Road	1030029 - Barrington St - Rockingham Rd to Marvell Av
From Date	2009
To Date	2013
Accident Type	All
Severity	All
Atmospheric Conditions	All
Horizontal Geometry	All
Vertical Geometry	All
Surface Type	All
Gender Of Driver	All
Road User Type	All
Crash Type	All
Rum Series	All
Crash Location	Not Known; Mid Block Latm Device (Slow Pt Sp Hump Etc.); Pedestrian Refuge Island; Median Opening; Rail Xing; Bridge; Subway; Driveway
Traffic Control	All
Day of Week	All
Time of Day	All
Light Conditions	All
Hit Object Crashes	All
Approach Leg	All

# Crash Factor Matrix

## ROAD USE MOVEMENT (RUM) CODES

	0	1	2	3	4	5	6	7	8	9
	<p>PEDESTRIAN on foot in tow/push</p>	<p>INTERSECTION vehicles from adjacent approaches</p>	<p>VEHICLES FROM OPPOSING DIRECTIONS</p>	<p>VEHICLES FROM ONE DIRECTION</p>	<p>MANOEUVRING</p>	<p>OVERTAKING</p>	<p>ON PATH</p>	<p>OFF STRAIGHT, ON STRAIGHT</p>	<p>OFF PATH, ON CURVE</p>	<p>PASSENGERS AND MISCELLANEOUS</p>
1	<p>NEAR SIDE 1</p>	<p>THRU-THRU 11</p>	<p>SIDE SWIPE HEAD ON 21</p>	<p>VEHICLES in same lanes REAR END 31</p>		<p>HEAD ON 51</p>	<p>PARKED 61</p>	<p>OFF CARRIAGEWAY TO LEFT 71</p>	<p>OFF CARRIAGEWAY RIGHT BEND 81</p>	<p>FELL IN/FROM VEHICLE 91</p>
2	<p>EMERGING 2</p>	<p>RIGHT-THRU 12</p>	<p>THRU-RIGHT 22</p>	<p>LEFT REAR 32</p>	<p>LEAVING PARKING 42</p>	<p>OUT OF CONTROL 52</p>	<p>DOUBLE PARKED 62</p>	<p>LEFT OFF CARRIAGEWAY INTO OBJECT/VEHICLE 72</p>	<p>OFF RIGHT BEND INTO OBJECT/VEHICLE 82</p>	<p>LOAD STRUCK VEHICLE 92</p>
3	<p>FAR SIDE 3</p>	<p>LEFT-THRU 13</p>	<p>RIGHT LEFT 23</p>	<p>RIGHT REAR 33</p>	<p>PARKING 43</p>	<p>PULLING OUT 53</p>	<p>ACCIDENT OR BROKEN DOWN 63</p>	<p>OFF CARRIAGEWAY TO RIGHT 73</p>	<p>OFF CARRIAGEWAY LEFT BEND 83</p>	<p>STRUCK TRAIN 93</p>
4	<p>PLAYING, WORKING LYING, STANDING ON CARRIAGEWAY 4</p>	<p>THRU-RIGHT 14</p>	<p>RIGHT RIGHT 24</p>	<p>U TURN 34</p>	<p>PARKING VEHICLES ONLY 44</p>	<p>CUTTING IN 54</p>	<p>CAR DOOR 64</p>	<p>RIGHT OFF CARRIAGEWAY INTO OBJECT/VEHICLE 74</p>	<p>OFF LEFT BEND INTO OBJECT/VEHICLE 84</p>	<p>STRUCK RAILWAY XING FURNITURE 94</p>
5	<p>WALKING WITH TRAFFIC 5</p>	<p>RIGHT-RIGHT 15</p>	<p>THRU LEFT 25</p>	<p>VEHICLES in parallel lanes LANE SIDE SWIPE 35</p>	<p>REVERSING 45</p>	<p>PULLING OUT REAR END 55</p>	<p>PERMANENT OBSTRUCTION 65</p>	<p>OUT OF CONTROL ON CARRIAGEWAY 75</p>	<p>OUT OF CONTROL ON CARRIAGEWAY 85</p>	<p>ANIMAL OFF CARRIAGEWAY 95</p>
6	<p>FACING TRAFFIC 6</p>	<p>LEFT-RIGHT 16</p>	<p>LEFT LEFT 26</p>	<p>LANE CHANGE RIGHT 36</p>	<p>REVERSING INTO FIXED OBJECT 46</p>	<p>O.T.-RT 56</p>	<p>TEMPORARY ROADWORKS 66</p>	<p>LEFT TURN 76</p>		<p>PARKED CAR RAN AWAY 96</p>
7	<p>DRIVEWAY 7</p>	<p>THRU-LEFT 17</p>	<p>U TURN 27</p>	<p>LANE CHANGE LEFT 37</p>	<p>LEAVING DRIVEWAY 47</p>		<p>TEMPORARY OBJECT ON CARRIAGEWAY 67</p>	<p>RIGHT TURN 77</p>		<p>VEHICLE MOVEMENTS NOT KNOWN 97</p>
8	<p>ON FOOTWAY 8</p>	<p>RIGHT-LEFT 18</p>		<p>RIGHT TURN S/S 38</p>	<p>LOADING BAY 48</p>					
9	<p>STRUCK WHILE BOARDING OR ALIGHTING 9</p>	<p>LEFT-LEFT 19</p>		<p>LEFT TURN S/S 39</p>	<p>FROM FOOTWAY 49</p>		<p>ON CARRIAGEWAY 69</p>			
	OTHER 98	OTHER 10	OTHER 20	OTHER 30	OTHER 40	OTHER 50	OTHER 60 (MISSILE/ FLYING OBJECT)	OTHER 70	OTHER 80	OTHER 90

# Crash Factor Matrix

Road: 1030029 - Barrington St - Rockingham Rd to Marvell Av

ROAD USER MOVEMENT CODE	Number of Crashes by Year						Severity				Surface			Light Condition					Day of Week							Time of Day													
	2009	2010	2011	2012	2013	Total	Fatal	Hospital	Medical	PDO Major	PDO Minor	Dry	Wet	Not Specified	Day	Dusk/Dawn	Dark lights On	Dark lights off	Dark no lights	Not Specified	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	00:00 - 02:59	03:00 - 05:59	06:00 - 08:59	09:00 - 11:59	12:00 - 14:59	15:00 - 17:59	18:00 - 20:59	21:00 - 23:59	Not Specified			
<b>30 Series : VEHICLES FROM ONE DIRECTION</b>																																							
31 - Same Lane Rear End		1				1				1		1			1											1													
30 Series Total		1				1				1		1			1											1													
<b>40 Series : MANOEUVRING</b>																																							
47 - Leaving Driveway					1	1				1		1			1																								
40 Series Total					1	1				1		1			1																								
<b>60 Series : ON PATH</b>																																							
60 - Other	1					1				1				1						1					1			1											
60 Series Total	1					1				1				1						1				1			1												
<b>70 Series : OFF PATH, ON STRAIGHT</b>																																							
72 - Off Left Cway Obj			1			1				1	1			1									1																
70 Series Total			1			1				1	1			1									1																
All RUM Codes	1	1	1	0	1	4	0	0	0	2	2	3	0	1	3	0	0	0	0	1	1	0	1	0	1	1	0	1	0	0	0	1	1	1	0	0	0		

**City of Cockburn Traffic Management Warrant System**

Road name: **Barrington Street**  
 Suburb: **Munster/Spearwood**  
 Location detail: **Between Marvell Avenue and Stock Road (SLK 0.61 - 1.12)**  
 Road classification: **Local Distributor Road**  
 Reason for analysis: **Concerns about traffic**

Analysis officer: John McDonald Date: 22/08/2014

TABLE 1 – Warrant criteria and weightings

**Note:** Maximum road length for each analysis = 500 metres

<u>PARAMETER</u>		<u>VALUE</u>	<u>SCORE</u>
Traffic speed		58	10
Traffic volume		7,333	27
Reported crash data (5-year period)	Fatalities	0	0
	Injuries	2	6
	Non-injuries	3	6
Road design and topography	Restricted sight crest curve	No	0
	Restricted sight horizontal curve	No	0
	Bends with unrestricted sight	No	0
	Steep hill	No	0
Vulnerable road users	Major bicycle or ped. crossing point	No	0
	Important bicycle route	No	0
Activity generators	College	No	0
	School	No	0
	Retail	No	0
Amenity factors	Trucks	5.50%	12
	Peak hour volume	9.50%	0
<b>Total:</b>			<b>60.5</b>

Table 2: Intervention warrant

A technical problem site - identify solutions for consideration for funding and implementation

Comments:

## MetroCount Traffic Executive Weekly Vehicle Counts

### WeeklyVehicle-484 -- English (ENA)

#### Datasets:

**Site:** [Barrington-04] Barrington Street - east of Chesterton Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:01 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-04 0 2014-05-29 1248.EC0 (Plus )  
**Identifier:** FQ885RHP MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

**Filter time:** 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions).  
**Exclusion:** Vehicles are excluded at the following times:  
 None  
 The following entire days are excluded:  
 Monday, 19 May 2014  
 Tuesday, 20 May 2014  
 Wednesday, 21 May 2014  
 Thursday, 22 May 2014

**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** North, East, South, West (bound), P = East  
**Separation:** All - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 39909 / 48307 (82.62%)

Weekly Vehicle Counts

## WeeklyVehicle-484

Site: Barrington-04.0.0EW  
 Description: Barrington Street - east of Chesterton Street<50>  
 Filter time: 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)  
 Scheme: Vehicle classification (AustRoads94)  
 Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>0)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
	19 May	20 May	21 May	22 May	23 May	24 May	25 May	1 - 5	1 - 7
0000-0100	*	*	*	*	24	61	63	24.0	49.3
0100-0200	*	*	*	*	11	23	45	11.0	26.3
0200-0300	*	*	*	*	18	24	23	18.0	21.7
0300-0400	*	*	*	*	18	20	19	18.0	19.0
0400-0500	*	*	*	*	34	28	13	34.0	25.0
0500-0600	*	*	*	*	185	80	27	185.0	97.3
0600-0700	*	*	*	*	287	90	51	287.0	142.7
0700-0800	*	*	*	*	409	165	101	409.0	225.0
0800-0900	*	*	*	*	639	348	163	639.0	383.3
0900-1000	*	*	*	*	452	423	249	452.0	374.7
1000-1100	*	*	*	*	395	485	369	395.0	416.3
1100-1200	*	*	*	*	414	498	398	414.0	436.7
1200-1300	*	*	*	*	416	465	364	416.0	415.0
1300-1400	*	*	*	*	413	418	378	413.0	403.0
1400-1500	*	*	*	*	495	381	365	495.0	413.7
1500-1600	*	*	*	*	796	399	360	796.0	518.3
1600-1700	*	*	*	*	668	369	377	668.0	471.3
1700-1800	*	*	*	*	601	398	385	601.0	461.3
1800-1900	*	*	*	*	367	302	229	367.0	299.3
1900-2000	*	*	*	*	206	173	122	206.0	167.0
2000-2100	*	*	*	*	155	152	118	155.0	141.7
2100-2200	*	*	*	*	123	120	97	123.0	113.3
2200-2300	*	*	*	*	106	106	46	106.0	86.0
2300-2400	*	*	*	*	77	81	25	77.0	61.0
<b>Totals</b>									
0700-1900	*	*	*	*	6065	4651	3738	6065.0	4818.0
0600-2200	*	*	*	*	6836	5186	4126	6836.0	5382.7
0600-0000	*	*	*	*	7019	5373	4197	7019.0	5529.7
0000-0000	*	*	*	*	7309	5609	4387	7309.0	5768.3
AM Peak	*	*	*	*	0800	1100	1100		
	*	*	*	*	639	498	398		
PM Peak	*	*	*	*	1500	1200	1700		
	*	*	*	*	796	465	385		

\* - No data.

Weekly Vehicle Counts

## WeeklyVehicle-484

Site: Barrington-04.0.0EW  
 Description: Barrington Street - east of Chesterton Street<50>  
 Filter time: 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)  
 Scheme: Vehicle classification (AustRoads94)  
 Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>0)

Hour	Mon	Tue	Wed	Thu	Fri	Sat	Sun	Averages	
	26 May	27 May	28 May	29 May	30 May	31 May	01 Jun	1 - 5	1 - 7
0000-0100	14	0	17	41	*	*	*	18.0	18.0
0100-0200	7	0	12	18	*	*	*	9.3	9.3
0200-0300	4	0	18	11	*	*	*	8.3	8.3
0300-0400	17	0	19	25	*	*	*	15.3	15.3
0400-0500	38	0	38	42	*	*	*	29.5	29.5
0500-0600	188	0	197	181	*	*	*	141.5	141.5
0600-0700	297	228	316	314	*	*	*	288.8	288.8
0700-0800	444	409	473	457	*	*	*	445.8	445.8
0800-0900	640	665	658	656	*	*	*	654.8	654.8
0900-1000	406	404	415	408	*	*	*	408.3	408.3
1000-1100	392	341	368	375	*	*	*	369.0	369.0
1100-1200	385	354	374	387	*	*	*	375.0	375.0
1200-1300	415	379	448	308	*	*	*	387.5	387.5
1300-1400	393	407	386	*	*	*	*	395.3	395.3
1400-1500	483	500	490	*	*	*	*	491.0	491.0
1500-1600	702	740	712	*	*	*	*	718.0	718.0
1600-1700	512	681	647	*	*	*	*	613.3	613.3
1700-1800	0	646	704	*	*	*	*	450.0	450.0
1800-1900	0	387	426	*	*	*	*	271.0	271.0
1900-2000	0	180	214	*	*	*	*	131.3	131.3
2000-2100	0	159	173	*	*	*	*	110.7	110.7
2100-2200	0	95	136	*	*	*	*	77.0	77.0
2200-2300	0	80	77	*	*	*	*	52.3	52.3
2300-2400	0	32	39	*	*	*	*	23.7	23.7
<b>Totals</b>									
0700-1900	4772	5913	6101	*	*	*	*	5578.9	5578.9
0600-2200	5069	6575	6940	*	*	*	*	6186.7	6186.7
0600-0000	5069	6687	7056	*	*	*	*	6262.7	6262.7
0000-0000	5337	6687	7357	*	*	*	*	6484.4	6484.4
AM Peak	0800	0800	0800	0800	*	*	*		
	640	665	658	656	*	*	*		
PM Peak	1500	1500	1500	*	*	*	*		
	702	740	712	*	*	*	*		

\* - No data.

## MetroCount Traffic Executive Daily Classes

### DailyClass-491 -- English (ENA)

#### Datasets:

**Site:** [Barrington-04] Barrington Street - east of Chesterton Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:01 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-04 0 2014-05-29 1248.EC0 (Plus )  
**Identifier:** FQ885RHP MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

**Filter time:** 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)  
**Exclusion:** Vehicles are excluded at the following times:  
 None  
 The following entire days are excluded:  
 Monday, 19 May 2014  
 Tuesday, 20 May 2014  
 Wednesday, 21 May 2014  
 Thursday, 22 May 2014

**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** North, East, South, West (bound), P = East  
**Separation:** All - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 39909 / 48307 (82.62%)

Daily Classes

## DailyClass-491

Site: Barrington-04.0.0EW  
 Description: **Barrington Street - east of Chesterton Street<50>**  
 Filter time: **14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)**  
 Scheme: Vehicle classification (AustRoads94)  
 Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>0)

Monday, 19 May 2014

	1	2	3	4	5	6	7	8	9	10	11	12	Total
<b>Mon*</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Tue</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Wed</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Thu</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Fri</b>	6881	70	317	22	7	1	8	0	3	0	0	0	7309
(%)	94.1	1.0	4.3	0.3	0.1	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Sat</b>	5404	39	153	4	0	2	5	1	1	0	0	0	5609
(%)	96.3	0.7	2.7	0.1	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Sun</b>	4254	30	97	0	0	2	4	0	0	0	0	0	4387
(%)	97.0	0.7	2.2	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	

Average daily volume

<b>Entire week</b>	2755	23	94	4	0	0	2	0	0	0	0	0	2884
(%)	95.5	0.8	3.3	0.1	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Weekdays</b>	1720	17	79	5	1	0	1	0	0	0	0	0	1827
(%)	94.1	0.9	4.3	0.3	0.1	0.0	0.1	0.0	0.0	0.0	0.0	0.0	
<b>Weekend</b>	4828	34	125	1	0	1	4	0	0	0	0	0	4998
(%)	96.6	0.7	2.5	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	

\* - Incomplete

**Daily Classes****DailyClass-491****Site:** Barrington-04.0.0EW**Description:** Barrington Street - east of Chesterton Street<50>**Filter time:** 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)**Scheme:** Vehicle classification (AustRoads94)**Filter:** Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>0)

Monday, 26 May 2014

	1	2	3	4	5	6	7	8	9	10	11	12	Total
<b>Mon</b>	4968	57	263	26	11	5	3	3	0	1	0	0	5337
(%)	93.1	1.1	4.9	0.5	0.2	0.1	0.1	0.1	0.0	0.0	0.0	0.0	
<b>Tue</b>	6182	64	327	35	61	5	6	2	5	0	0	0	6687
(%)	92.4	1.0	4.9	0.5	0.9	0.1	0.1	0.0	0.1	0.0	0.0	0.0	
<b>Wed</b>	6870	43	328	39	40	7	20	5	5	0	0	0	7357
(%)	93.4	0.6	4.5	0.5	0.5	0.1	0.3	0.1	0.1	0.0	0.0	0.0	
<b>Thu*</b>	2994	30	170	15	2	4	4	0	3	1	0	0	3223
(%)	92.9	0.9	5.3	0.5	0.1	0.1	0.1	0.0	0.1	0.0	0.0	0.0	
<b>Fri*</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Sat*</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
<b>Sun*</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
(%)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	

**Average daily volume****Entire week**

	6006	54	305	33	37	5	9	3	3	0	0	0	6459
(%)	93.0	0.8	4.7	0.5	0.6	0.1	0.1	0.0	0.0	0.0	0.0	0.0	

**Weekdays**

	6006	54	305	33	37	5	9	3	3	0	0	0	6459
(%)	93.0	0.8	4.7	0.5	0.6	0.1	0.1	0.0	0.0	0.0	0.0	0.0	

**Weekend** No complete days.

\* - Incomplete

## MetroCount Traffic Executive Speed Histogram

### SpeedHist-485 -- English (ENA)

#### Datasets:

**Site:** [Barrington-04] Barrington Street - east of Chesterton Street<50>  
**Direction:** 8 - East bound A>B, West bound B>A. Lane: 0  
**Survey Duration:** 14:01 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014  
**Zone:**  
**File:** Barrington-04 0 2014-05-29 1248.EC0 (Plus )  
**Identifier:** FQ885RHP MC56-L5 [MC55] (c)Microcom 19Oct04  
**Algorithm:** Factory default axle (v4.02)  
**Data type:** Axle sensors - Paired (Class/Speed/Count)

#### Profile:

**Filter time:** 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)

**Exclusion:** Vehicles are excluded at the following times:  
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 The following entire days are excluded:  
 Monday, 19 May 2014  
 Tuesday, 20 May 2014  
 Wednesday, 21 May 2014  
 Thursday, 22 May 2014

**Included classes:** 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12  
**Speed range:** 10 - 160 km/h.  
**Direction:** North, East, South, West (bound), P = East  
**Separation:** Greater than 4.00 seconds. - (Headway)  
**Name:** Default Profile  
**Scheme:** Vehicle classification (AustRoads94)  
**Units:** Metric (meter, kilometer, m/s, km/h, kg, tonne)  
**In profile:** Vehicles = 27804 / 48307 (57.56%)

#### Speed Statistics

Vehicles = 27804  
 Posted speed limit = 60 km/h, Exceeding = 2907 (10.46%), Mean Exceeding = 63.91 km/h  
 Maximum = 93.0 km/h, Minimum = 10.4 km/h, Mean = 52.0 km/h  
 85% Speed = 58.3 km/h, 95% Speed = 62.6 km/h, Median = 51.8 km/h  
 20 km/h Pace = 42 - 62, Number in Pace = 24482 (88.05%)  
 Variance = 49.02, Standard Deviation = 7.00 km/h

# Speed Histogram

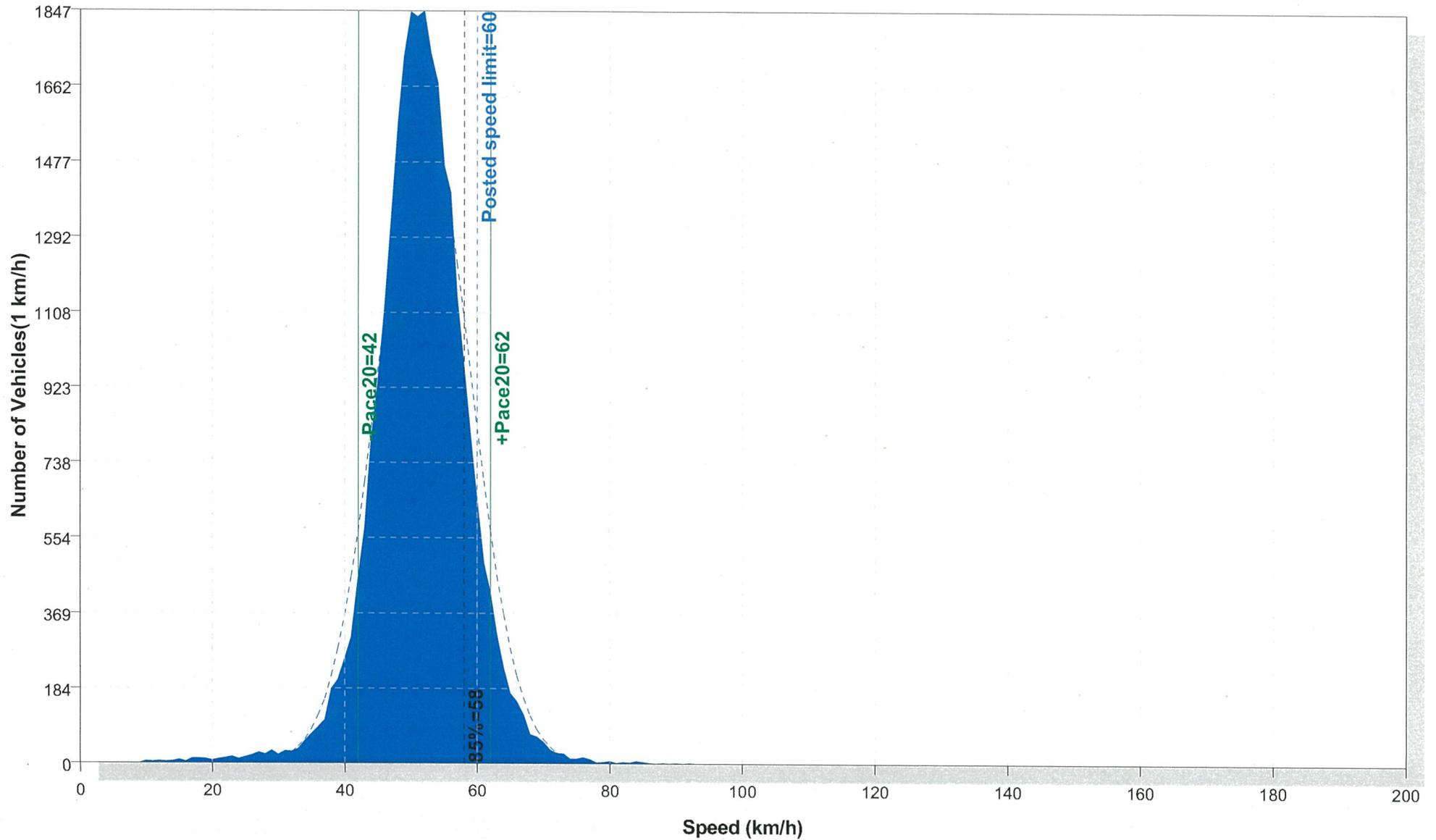
SpeedHist-485 (Metric) Site:Barrington-04.0.0EW

Description: Barrington Street - east of Chesterton Street<50>

Filter time: 14:02 Monday, 19 May 2014 => 12:49 Thursday, 29 May 2014 (With Exclusions)

Filter: Cls(1 2 3 4 5 6 7 8 9 10 11 12 ) Dir(NESW) Sp(10,160) Headway(>4)

Scheme: Vehicle classification (AustRoads94)



# Crash Factor Matrix

Parameter	Value
Job Id	85394722
Road	1030029 - Barrington St - Marvell Av to Stock Rd
From Date	2009
To Date	2013
Accident Type	All
Severity	All
Atmospheric Conditions	All
Horizontal Geometry	All
Vertical Geometry	All
Surface Type	All
Gender Of Driver	All
Road User Type	All
Crash Type	All
Rum Series	All
Crash Location	Not Known; Mid Block Latm Device (Slow Pt Sp Hump Etc.); Pedestrian Refuge Island; Median Opening; Rail Xing; Bridge; Subway; Driveway
Traffic Control	All
Day of Week	All
Time of Day	All
Light Conditions	All
Hit Object Crashes	All
Approach Leg	All

# Crash Factor Matrix

## ROAD USE MOVEMENT (RUM) CODES

	0	1	2	3	4	5	6	7	8	9
	PEDESTRIAN on foot in to/from	INTERSECTION vehicles from adjacent approaches	VEHICLES FROM OPPOSING DIRECTIONS	VEHICLES FROM ONE DIRECTION	MANOEUVRING	OVERTAKING	ON PATH	OFF STRAIGHT, ON STRAIGHT	OFF PATH, ON CURVE	PASSENGERS AND MISCELLANEOUS
1	NEAR SIDE 1	THRU-THRU 11	SIDE SWIPE HEAD ON 21	REAR END 31	Vehicles in same lanes	HEAD ON 51	PARKED 61	OFF CARRIAGEWAY TO LEFT 71	OFF CARRIAGEWAY RIGHT BEND 81	FELL IN/FROM VEHICLE 91
2	EMERGING 2	RIGHT-THRU 12	THRU-RIGHT 22	LEFT REAR 32	LEAVING PARKING 42	OUT OF CONTROL 52	DOUBLE PARKED 62	LEFT OFF CARRIAGEWAY INTO OBJECT/VEHICLE 72	OFF RIGHT BEND INTO OBJECT/VEHICLE 82	LOAD STRUCK VEHICLE 92
3	FAR SIDE 3	LEFT-THRU 13	RIGHT LEFT 23	RIGHT REAR 33	PARKING 43	PULLING OUT 53	ACCIDENT OR BROKEN DOWN 63	OFF CARRIAGEWAY TO RIGHT 73	OFF CARRIAGEWAY LEFT BEND 83	STRUCK TRAIN 93
4	PLAYING, WORKING LYING, STANDING ON CARRIAGEWAY 4	THRU-RIGHT 14	RIGHT RIGHT 24	U TURN 34	PARKING VEHICLES ONLY 44	CUTTING IN 54	CAR DOOR 64	RIGHT OFF CARRIAGEWAY INTO OBJECT/VEHICLE 74	OFF LEFT BEND INTO OBJECT/VEHICLE 84	STRUCK RAILWAY XING FURNITURE 94
5	WALKING WITH TRAFFIC 5	RIGHT-RIGHT 15	THRU LEFT 25	LANE SIDE SWIPE 35	REVERSING 45	PULLING OUT REAR END 55	PERMANENT OBSTRUCTION 65	OUT OF CONTROL ON CARRIAGEWAY 75	OUT OF CONTROL ON CARRIAGEWAY 85	ANIMAL OFF CARRIAGEWAY 95
6	FACING TRAFFIC 6	LEFT-RIGHT 16	LEFT LEFT 26	LANE CHANGE RIGHT 36	REVERSING INTO FIXED OBJECT 46	O.T-RT 56	TEMPORARY ROADWORKS 66	LEFT TURN 76		PARKED CAR RAN AWAY 96
7	DRIVEWAY 7	THRU-LEFT 17	U TURN 27	LANE CHANGE LEFT 37	LEAVING DRIVEWAY 47		TEMPORARY OBJECT ON CARRIAGEWAY 67	RIGHT TURN 77		VEHICLE MOVEMENTS NOT KNOWN 97
8	ON FOOTWAY 8	RIGHT-LEFT 18		RIGHT TURN S/S 38	LOADING BAY 48					
9	STRUCK WHILE BOARDING OR ALIGHTING 9	LEFT-LEFT 19		LEFT TURN S/S 39	FROM FOOTWAY 49		ON CARRIAGEWAY 69			
	OTHER 98	OTHER 10	OTHER 20	OTHER 30	OTHER 40	OTHER 50	OTHER 60 (MISSILE/ FLYING OBJECT)	OTHER 70	OTHER 80	OTHER 90

# Crash Factor Matrix

Road: 1030029 - Barrington St - Marvell Av to Stock Rd

ROAD USER MOVEMENT CODE	Number of Crashes by Year						Severity					Surface			Light Condition					Day of Week							Time of Day															
	2009	2010	2011	2012	2013	Total	Fatal	Hospital	Medical	PDO Major	PDO Minor	Dry	Wet	Not Specified	Day	Dusk/Dawn	Dark lights On	Dark lights off	Dark no lights	Not Specified	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	00:00 - 02:59	03:00 - 05:59	06:00 - 08:59	09:00 - 11:59	12:00 - 14:59	15:00 - 17:59	18:00 - 20:59	21:00 - 23:59	Not Specified						
<b>30 Series : VEHICLES FROM ONE DIRECTION</b>																																										
31 - Same Lane Rear End					1	1				1		1			1											1																
33 - Same Lane Right Rear				2		2		1	1			2			2										2																	
30 Series Total				2	1	3		1	1	1		3			3										2	1																
<b>60 Series : ON PATH</b>																																										
60 - Other			1			1					1	1				1					1																		1			
64 - Open Car Door	1					1				1		1			1								1																		1	
60 Series Total	1		1			2				1	1	2			1	1					1		1																		1	
All RUM Codes	1	0	1	2	1	5	0	1	1	2	1	5	0	0	4	1	0	0	0	0	1	0	1	0	2	1	0	0	0	0	1	1	1	1	1	1	0	0				

9.0

ROAD ACCESS SYSTEM

Jandakot Airport Master Plan

## 9.0 ROAD ACCESS SYSTEM

To maximise Jandakot Airports's potential for aviation and non-aviation land uses, additional transport links to the Perth Metropolitan Region are essential and are therefore included in this Master Plan. These are also very important for emergency access.

Since approval of the Master Plan 2005, JAH has contributed \$3.8 million to a bridge over the railway to negate the dangerous level crossing that existed at the entrance to the Airport. The entry road has also been upgraded to a four-lane carriageway with bicycle lanes in both directions at a cost of approximately \$3 million.

### 9.1 ROAD ACCESS

Access to Jandakot Airport is currently provided from Berrigan Drive or Karel Avenue via the Karel Avenue extension. Karel Avenue, up to but not including the Berrigan Drive intersection, has been upgraded by JAH from a two-lane undivided road to a four-lane divided road.

The Key Primary Distributor Roads providing access to Jandakot Airport include the Kwinana Freeway, Roe Highway and South Street. Key District Distributor Roads include Ranford Road, Berrigan Drive and Karel Avenue.

REFER TO FIGURE 14 – ROAD HIERARCHY PLAN

### 9.2 PROPOSED ROAD ACCESS SYSTEM

Access to Jandakot Airport is to be provided by the following connections:

- Access from Berrigan Drive or Karel Avenue via the Karel Avenue extension (existing);
- South Link - This road is proposed to connect from Karel Avenue to Jandakot Road and Berrigan Drive at the existing Jandakot/ Berrigan/ Dean intersection (proposed under this Master Plan); and
- East Link - The connection of this road outside of the Airport boundary will be determined in discussions with the State Government and local councils (proposed under this Master Plan).

REFER TO FIGURE 3 – MASTER PLAN 2009

REFER TO FIGURE 14 – ROAD HIERARCHY PLAN

All proposed road connections will be undertaken in a manner that minimises noise and security impacts to local residents, especially in the vicinity of Glendale Crescent and Berrigan Drive.

### 9.3 TRAFFIC GENERATION

Traffic generation has been forecast at 31,940 vehicles/day for the commercial land uses on the Jandakot Airport based on the complete development of the airport non-aviation areas, which includes a development gross floor area of approximately 590,000m<sup>2</sup> and a workforce of 5,753 employees. In addition approximately 4,000 vehicles/day for existing aviation activities and 1,000 vehicles/day for growth in aviation activities are forecast to provide the total vehicle traffic generation of approximately 36,940 vehicles/day.

### 9.4 TRAFFIC FORECAST

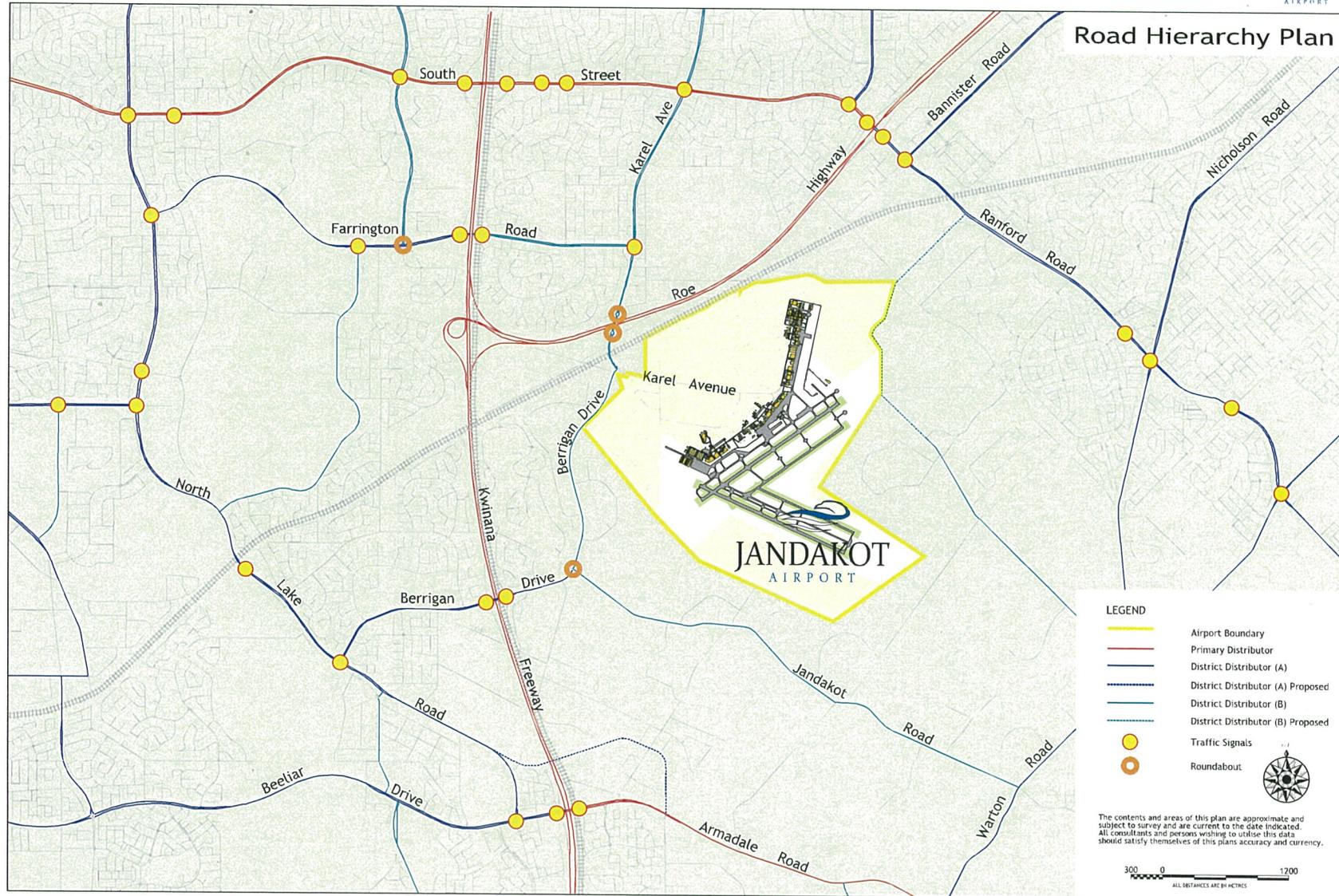
Road modelling assumes Jandakot Airport to be substantially complete by year 2016, in order to understand longer term road network requirements, traffic modelling has been undertaken for year 2016 and year 2031. The traffic modelling assumes full development of the airport in both the 2016 and 2031 scenarios. The summary results from the traffic modelling are reported in sections 9.5, 9.6 and 9.7.

### 9.5 ROAD UPGRADES: YEAR 2016 'NON-DEVELOPMENT' SCENARIO

The Year 2016 traffic modelling identified a number of road infrastructure improvements that would be required even without traffic associated with Jandakot Airport development. The upgrades for the 'non-development' scenario relate to 'regional roads' at the perimeter of the study area and are described briefly below:

- Nicholson Road (south of Ranford Road) – the existing two-lane arterial road is above capacity and a four-lane divided facility will be required for good PM peak hour service. The City of Canning has indicated that this road is already scheduled for upgrade in the short term.

FIGURE 14 – ROAD HIERARCHY PLAN



- Kwinana Freeway (Berrigan Drive to Roe Highway) – the existing four-lane Freeway is forecast to be above capacity at year 2016 and congestion would be expected in the PM peak. It should be noted that the base case network assumes six-lanes north of Farrington Road which is adequate for the forecast PM peak hour traffic at Year 2016. It is understood that Main Roads is investigating ways in which freeway operations can be optimised (e.g. Ramp Metering, etc) and this may help mitigate freeway congestion at year 2016.
- Armadale Road (east of Kwinana Freeway) – Armadale Road is currently being upgraded to four-lane arterial standard to Tapper Road. East of Tapper Road it transitions back to a two-lane road. Traffic modelling for year 2016 assumes full build out of Cockburn Central and the Jandakot Industrial Area. The modelling also assumes that the North Lake Road extension across the Kwinana Freeway is not in place. Under these conditions, a four-lane arterial road between Kwinana Freeway and Tapper Road may be congested in PM peak hour. East of Tapper Road the existing two-lane would need to be upgraded to a four-lane arterial road to adequately handle PM peak hour traffic.
- Karel Avenue (between Farrington Road and Roe Highway) - Upgrade of the approach and departure lanes to two-lanes in each direction near the Roe Highway interchange with tapering back of the two-lanes prior to Farrington Road will be required (year 2016);
- Karel Avenue (between Karel Avenue and Roe Highway) – A four-lane divided arterial road will be required (additional investigation is required to determine the Karel Avenue/ Roe Highway interchange upgrade requirements);
- Karel Avenue extension – The two-lane arterial road was not adequate. A four-lane divided arterial road is required to handle the forecast traffic and has been completed;
- East Link - Requires a two-lane arterial road with turn lanes at controlled junctions (junction locations to be determined in discussions with State Government and local councils); and
- South Link - Requires a two-lane arterial road with turn lanes at controlled junctions (should be a four-lane divided road for the section between Berrigan Drive (north) and Jandakot Road).

REFER TO FIGURE 15 – ROAD IMPROVEMENT TO YEAR 2016

Figure 16 demonstrates traffic flows at 2016 for the non-development and development scenarios for Jandakot Airport.

REFER TO FIGURE 16 – NON-DEVELOPMENT AND DEVELOPMENT TRAFFIC FLOWS 2016

## 9.6 ROAD UPGRADES: YEAR 2016 'DEVELOPMENT' SCENARIO.

The proposed development scenario will require access upgrades at year 2016 over and above the 'non-development' scenario as outlined below. The upgrades will provide improved access to the Airport, which will benefit growth in the aviation precinct.

The additional improvements triggered by regional growth and the Airport development are summarised as follows:

- Berrigan Drive between Kwinana Freeway and Jandakot Road - A four-lane divided road will likely be required soon after the Jandakot Airport development is substantially complete (i.e. shortly after year 2016);

## 9.7 BERRIGAN DRIVE (NORTH)

As a result of discussions with the City of Cockburn engineering and planning departments, the following strategy is proposed in order to achieve the desired reduction in traffic on Berrigan Drive (north) and to address right turn traffic safety at the South Link junction:

- Signal control at the Berrigan Drive (north)/ Karel Ave/ Karel Avenue extension intersection. This will allow green time to be restricted in peak periods for traffic moving in and out of Berrigan Drive (north);

### Road Improvements for Jandakot Airport Development Scenario to Year 2016

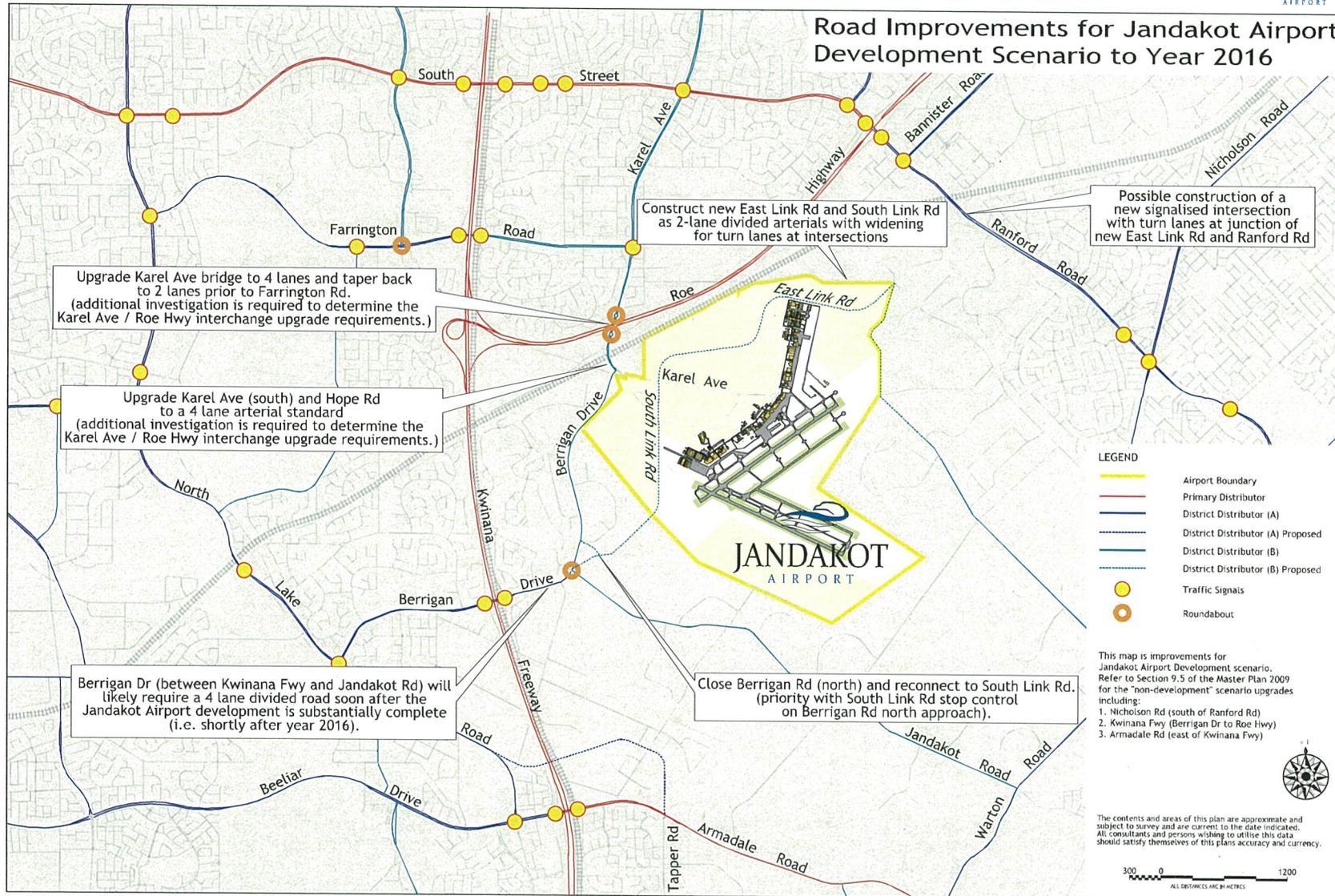


FIGURE 15 – ROAD IMPROVEMENT TO YEAR 2016

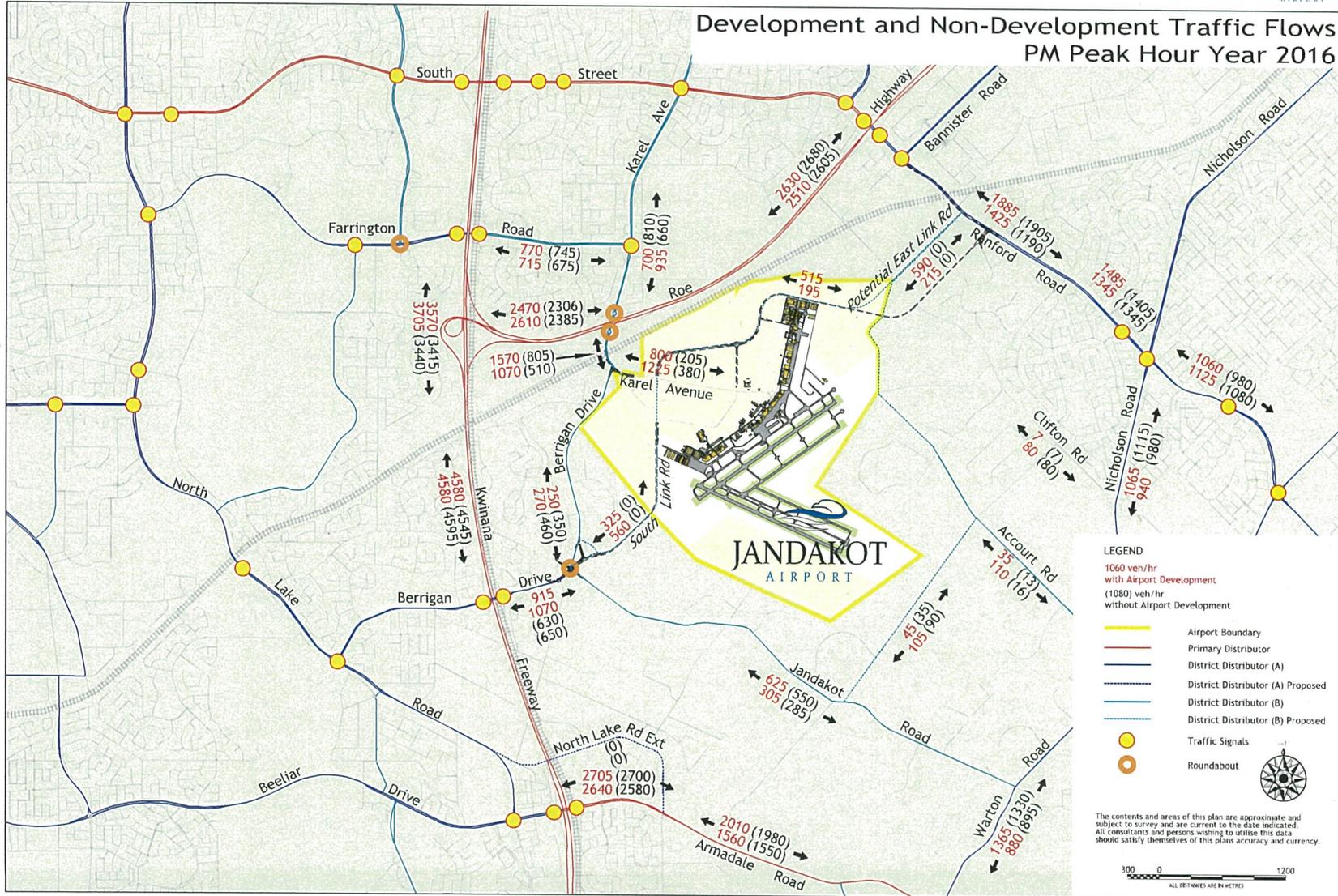


FIGURE 16 – NON-DEVELOPMENT AND DEVELOPMENT TRAFFIC FLOWS 2016

- Signal control at the Jandakot Road/ Dean Street/ Berrigan (south) intersection to provide 'inter phase' gaps in the traffic flow, thus allowing safe gaps for right turns from Berrigan Drive (north) onto the South Link;
- Erect 'Local Traffic Only' signs at the north and south junctions on Berrigan Drive (north); and
- If necessary, supplement the above with traffic calming treatments (e.g. chicanes, speed bumps, etc) on Berrigan Drive (north).

## 9.8 YEAR 2031 ROAD UPGRADES: CHANGED REQUIREMENTS RELATIVE TO YEAR 2016

In addition to the road upgrades reported above for the year 2016 assessment there are other regional road capacity issues identified in the 2031 traffic modelling (e.g. Kwinana Freeway and Ranford Road and Armadale Road capacity). Those issues are related primarily to 'non-site' regional traffic generation and are matters to be resolved by State and Local Government.

## 9.9 RESPONSIBILITY/ FUNDING OF ROAD UPGRADES

### 9.9.1 KAREL AVENUE EXTENSION, THE EAST LINK ROAD AND THE SOUTH LINK ROAD

These roads are located on Jandakot Airport property and will be constructed and maintained by the Airport. The intersections of these access roads with perimeter roads that are controlled by local government will require modification to accommodate new lane geometry and intersection control. Final design and funding of these intersections will be determined and agreed through discussions between Jandakot Airport and the relevant local governments. Ownership, responsibility and maintenance of roads off the Airport will remain with local government.

### 9.9.2 BERRIGAN DRIVE (NORTH), BERRIGAN DRIVE (SOUTH) AND KAREL AVENUE

Funding for the initiatives identified for these roads will be resolved with local government and state government agencies but it is proposed that ownership and responsibility for maintenance will remain with the local government.

## 9.10 PUBLIC TRANSPORTATION & PEDESTRIAN/ CYCLIST FACILITIES

### 9.10.1 BUS SERVICES

There is currently limited bus transport service in close proximity to the site. Discussions were held with Transperth to understand how service improvements might develop as the Jandakot Airport evolves.

JAH have committed to provide regular shuttle bus services between the airport and Murdoch Bus/ Rail interchange while Transperth bus services are being upgraded to the necessary standard.

Adequate public transport systems are recognised by JAH as increasingly important as employment on the airport grows. For this reason discussions between JAH and the State in regards to improved bus services will continue.

### 9.10.2 PASSENGER RAIL SERVICE

Although current discussions with the Public Transport Authority and State Government officers indicates that conditions are not conducive to delivering passenger rail service to Jandakot Airport, JAH has given consideration to how it might be configured at some future date as shown in Figure 17.

### 9.10.3 PEDESTRIAN/ CYCLIST FACILITIES

The existing paths located at the perimeter of the site include the following:

- A principle shared path is located on the north side of the Roe Highway reserve;
- A shared path is located on the west side of Karel Avenue, along the bridge over the Roe Highway. North of the Roe Highway the path continues on the west side to up to Farrington Road. There is also a section of shared path on the east side of Karel Avenue south of the Dimond Court intersection; and
- A shared path is located on the north side of Berrigan Drive (south of Jandakot Road).

Additional paths will be constructed as part of the Jandakot Airport Development which will provide a significant and convenient network for pedestrians and cyclists including mid block and intersection crossings as demonstrated in Figure 17.

REFER TO FIGURE 17 – TRANSPORTATION ROUTES

### 9.11 CAR PARKING

JAH currently requires similar car parking standards to the City of Cockburn requirements. This practice will continue.

# Transportation Routes

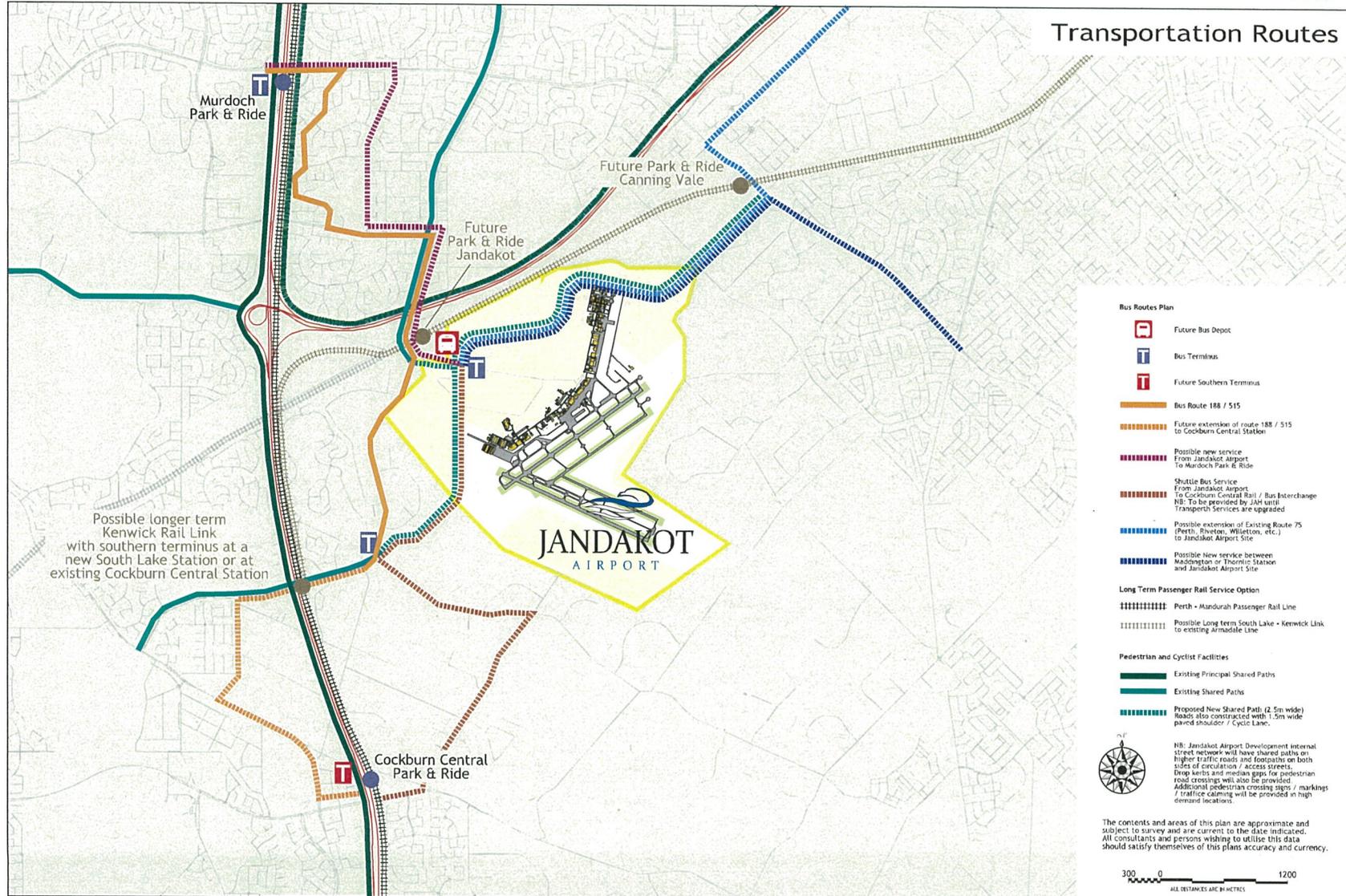
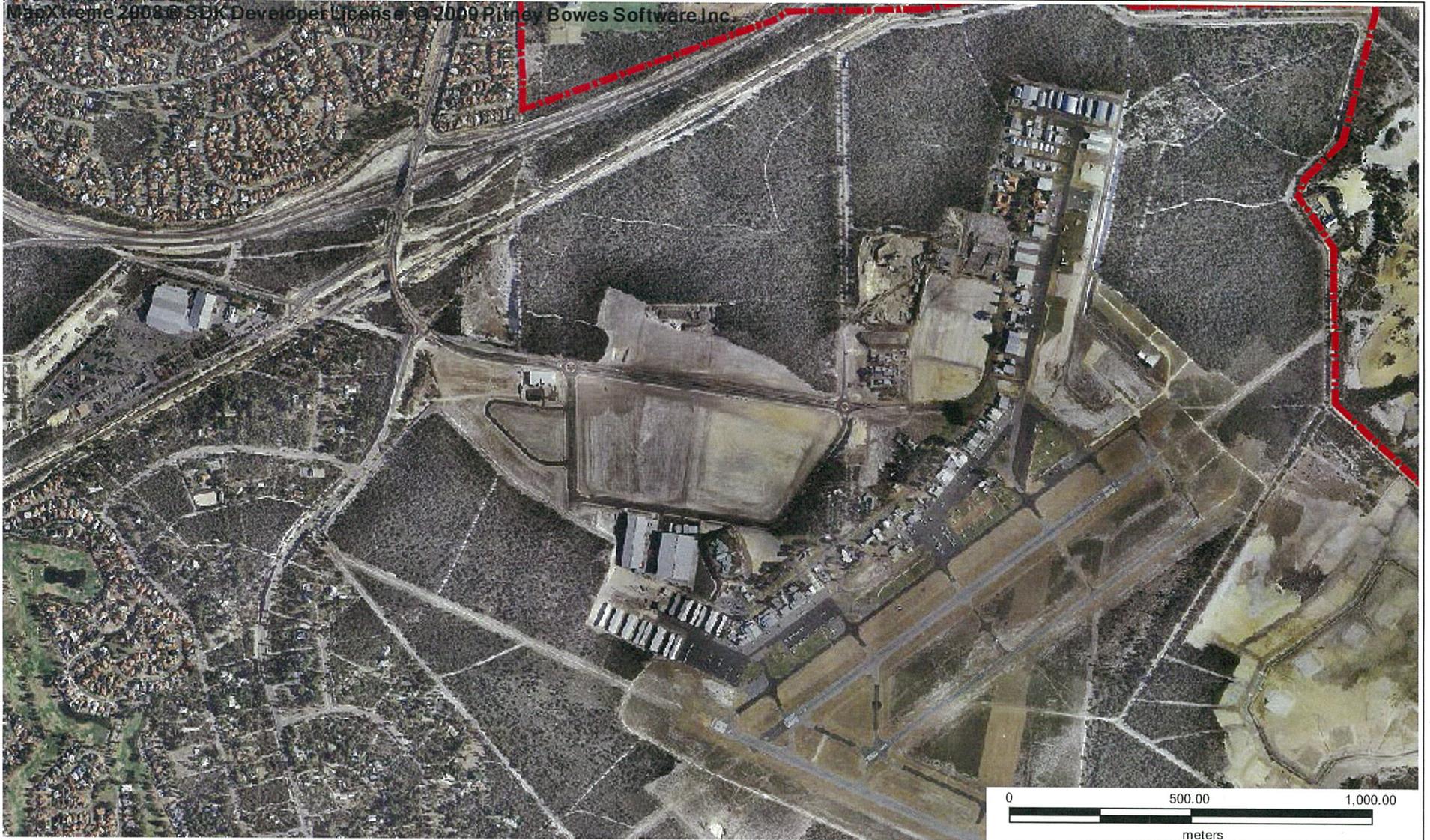


FIGURE 17 – TRANSPORTATION ROUTES

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**Jandakot Airport – January 2009**

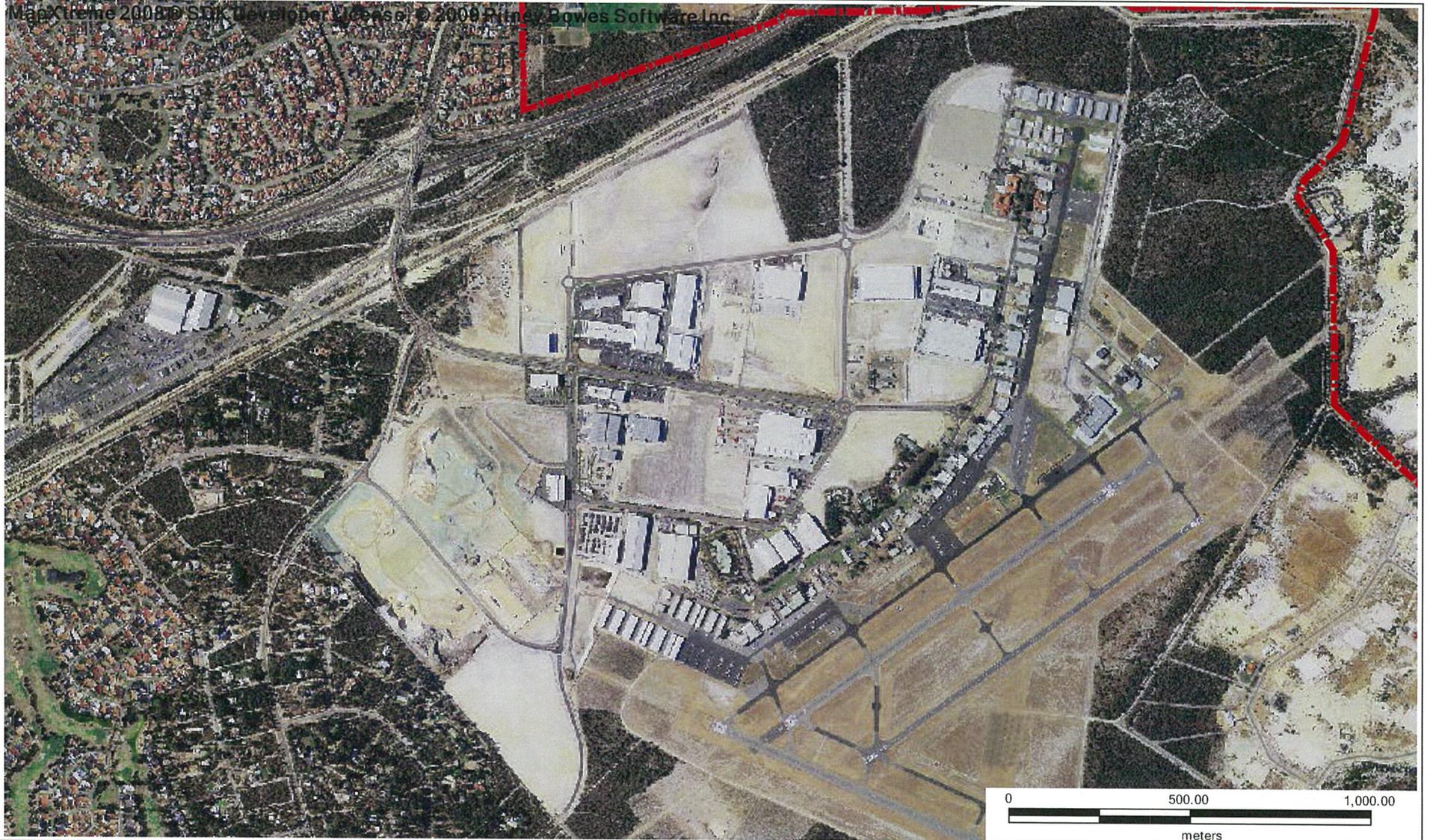
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**City of Cockburn**  
G.I.S Services Department

**Jandakot Airport - February 2014**

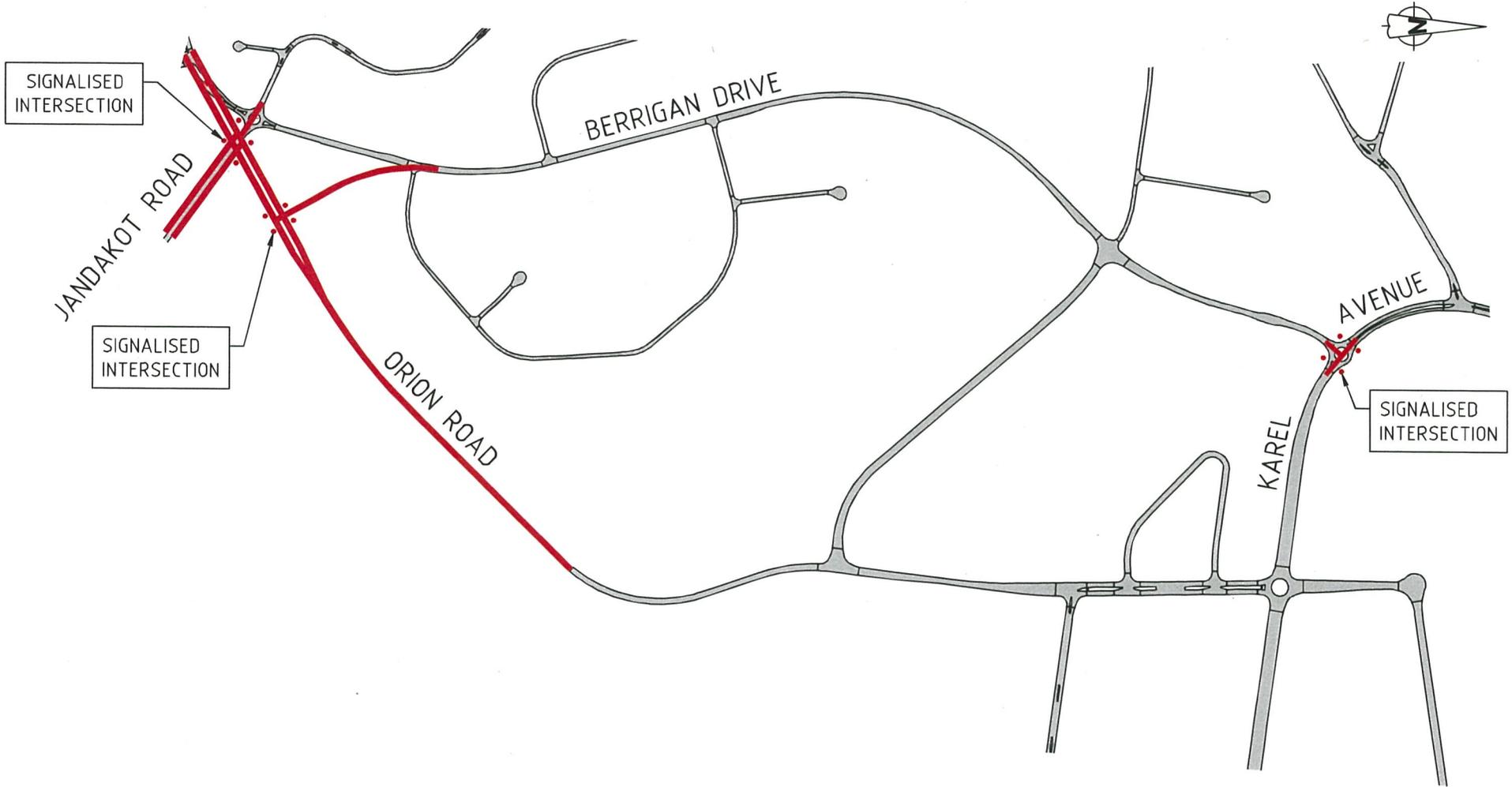
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 PHONE: (08) 9411 3444 FAX: (08) 9347 3333

TITLE  
**JANDAKOT AIRPORT  
 NETWORK CONNECTION OPTION 1**

DESIGNED	APPROVED	SCALE 1:10000	JOB No.
DRAWN T.C	DESIGN MANAGER	DWG No.	SHEET No.
CHECKED		1	1

ATTACH 3

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Our Ref: 450503; 450025  
Enquiries: J Kiurski 9411 3585

22 May 2014

Mr Albert Symcox  
Traffic Services Manager South  
P O Box 6202  
East Perth WA 6892

Dear Albert

**Approval in Principle – Installation of Traffic Signal at Intersections of Karel Avenue/Berrigan Drive, Berrigan Drive/Jandakot Road and Berrigan Drive/Orion Road**

The City of Cockburn and Jandakot Airport Holdings (JAH) have been in discussions regarding the best possible treatment for the road network servicing Jandakot Airport and the commercial development. The matter has been complicated by the lack of regulatory control over the federal government lease area and the implications that has on broad engineering and planning responsibilities.

**Jandakot Road / Dean Road / Orion Road**

The 2009 Masterplan proposed a new road, Orion Road (or South Link Road), which would act as a major distributor road servicing the development area. This road would link with an unmade road reserve and connect to the current network at the intersection of Jandakot/Berrigan/Dean. The initial proposal recommended a signalised intersection. Since providing its support to the Masterplan in 2010, JAH have commissioned consultants to review the proposed intersection configuration to ensure that it provided the best outcome. In all, six (6) options were considered for the intersection of Jandakot/Berrigan/Dean. From these 6 options, 2 options (the signalised intersection and roundabout) were supported for more detailed modelling and assessment. The City of Cockburn engaged Urbsol to conduct modelling of those 2 options to determine the most suitable intersection configuration. The signalised intersection was recommended and is subsequently supported by Council (the modelling report prepared by Urbsol is appended).

**On the basis of the modelling that has been undertaken, the City of Cockburn provides its support for the installation of signals at Jandakot Road / Dean Road / Orion Road as proposed.**

**Orion Road configuration – signalising Berrigan Drive (north) / Orion Road**

The 2009 Masterplan also recommended duplication of Orion Road to cater for the traffic generation proposed from the commercial development and the regional traffic that would use the link created through the broader connections proposed to the south (Berrigan Drive – Freeway) and east (Ranford Road) of the development area. In 2011, JAH revised their traffic generation data which suggested that duplication was not necessary (at least in the short term).

The City provided support for the 2009 Masterplan on the basis of downgrading Berrigan Drive (north) to a local road. For the most part Orion Road is contained within the federal government lease area. The lack of control over Orion Road will make it difficult to ensure

that this road can reasonably provide for regional traffic movement into the future. In fact the current design and development of Orion Road would suggest that it will be constrained in the longer term. Whilst this is a little short-sighted, it highlights the concern and suggests that downgrading Berrigan Drive (north) may not be the best outcome under the current circumstances.

An outcome of retaining Berrigan Drive (north) as a DDA is the need to signalise this intersection. Urbsol has modelled this intersection in conjunction with the signals at Jandakot/Dean/Orion and has determined that signalisation of the two sites provides relative stable conditions with none of the movement delay (as compared to the roundabout option) and also improves movement on the Berrigan Drive (Kwinana Freeway - Jandakot / Dean / Orion) approach.

**On the basis of the modelling provided by Urbsol, the City of Cockburn provides its support for the installation of signals at Berrigan Drive (north) / Orion Road as proposed.**

Berrigan Drive (north) / Karel Avenue

Egress out of Jandakot Airport has been somewhat constrained for some time. Approximately 18 months ago the City and MRWA supported the establishment of temporary traffic management during the PM peak to assist in the traffic flow. Whilst this has helped to some degree, the area is still heavily congested in the afternoon periods. JAH have been proposing to signalise this intersection to improve traffic flow out of their development for some time; however, the City has not been prepared to support further development until the south link road issue had been resolved and development works committed. That remains the case. The road network layout that is now proposed and supported by the City of Cockburn will require the establishment of signals at Berrigan Drive (north) / Karel Avenue. These signals will need to be installed as part of the broader network upgrade and must not be undertaken in isolation.

**The City provides its support for the installation of signals at Berrigan Drive (north) / Orion Road as proposed on the basis that they are installed as part of the broader network upgrade detailed above and they shall not be undertaken in isolation.**

The City of Cockburn will enter into a legal agreement with Jandakot Airport Holdings which will stipulate the construction and timing of the network upgrades as detailed in this letter prior to any works commencing.

Please feel free to contact the City should you require any further clarification.

Yours sincerely



Michael Littleton  
Director Engineering & Works

Encl:

Copy: Mr Ian Edwards and  
Mr John Fraser



ABN: 50 860 676 021

Enquiries: Mark Ellis on 9323 4221  
 Our Ref: 05/70, /3546 (D14#501218)  
 Your Ref: 450503 & 450025

15 September 2014

Chief Executive Officer  
 City of Cockburn  
 P.O Box 1215  
 BIBRA LAKE WA 6965

ATTENTION: Michael Littleton

Dear Michael

**JANDAKOT ROAD NETWORK OPTIONS – TRAFFIC SIGNALS AT JANDAKOT ROAD, ORION ROAD AND DEAN ROAD INTERSECTION AND BERRIGAN DRIVE AND KAREL AVENUE INTERSECTION**

Thank you for attending the meeting at Main Roads on Monday, 8 September 2014 to discuss the Jandakot Airport Road Network Operations.

Main Roads has reviewed the options presented to us and is willing to support road network option 1, with a minor modification.

In detail Main Roads gives 'Agreement in principle' to traffic signals at the Jandakot Road, Orion Road and Dean Road intersection and at the Karel Road and Berrigan Drive intersection. In regards to the Berrigan Drive and Orion Road intersection treatment, Main Roads only supports regulatory sign control in the short term. However, we are willing to reconsider the Berrigan Drive and Orion Road intersection treatment option subject to a review of the performance of the existing treatment which needs to occur no earlier than six months after the completion of the Kwinana Freeway widening project.

Please note this agreement is conditional and allows Council to further develop the project to final design. The agreement does not guarantee the installation of traffic signals will proceed without;

- 1) Council demonstrating all necessary conditions can be satisfied. These conditions include but are not necessarily limited to;
  - Determine the preferred system of traffic signal operation.
  - Design the road layout indicating lane configurations for the preferred system of signal operation.
  - Main Roads approval of the design of the road layout, traffic signal phasings and layout and signing and pavement marking layout.
  - Roadway Lighting;
    - Extent of the lighting shall be recommended in AS/NZS 1158 Part 1.1 (2005) Section 3.4 plus at least an additional two spans of road section lighting on each approach.
    - Level of the lighting shall be recommended in AS/NZS 1158 Part 1.1 (2005) Section 3.4 for the intersection and AS/NZS 1158 (2005) "V" subcategory appropriate for the approaches.

Don Aitken Centre, Waterloo Crescent, East Perth or PO Box 6202 EAST PERTH Western Australia 6892  
 Telephone: (08) 9323 4111 Facsimile: (08) 9323 4174 TTY: (08) 9428 2230  
 Email: roadtraff@mainroads.wa.gov.au Website: www.mainroads.wa.gov.au  
 TRIM D06#138922

- Pedestrian facilities to the current access and mobility standards to be provided at the intersections and included in the new design.
  - Closed Circuit Television (CCTV) to monitor operation of the signals to be provided.
  - Provision of the specifications and software documentation
  - The entire project to be fully funded by City of Cockburn or Jandakot Airport Holdings.
  - Any other supporting data as necessary.
- 2) Council obtaining formal approval from Main Roads to the installation of the traffic signals. Given formal approval being granted, Council will be required to;
- Arrange the installation of the traffic signals, including its software, in accordance with Main Roads requirements.
  - Following installation, arrange handover of Main Roads Traffic Signal Infrastructure to the relevant Main Roads Contractor and forward the associated completed documentation.

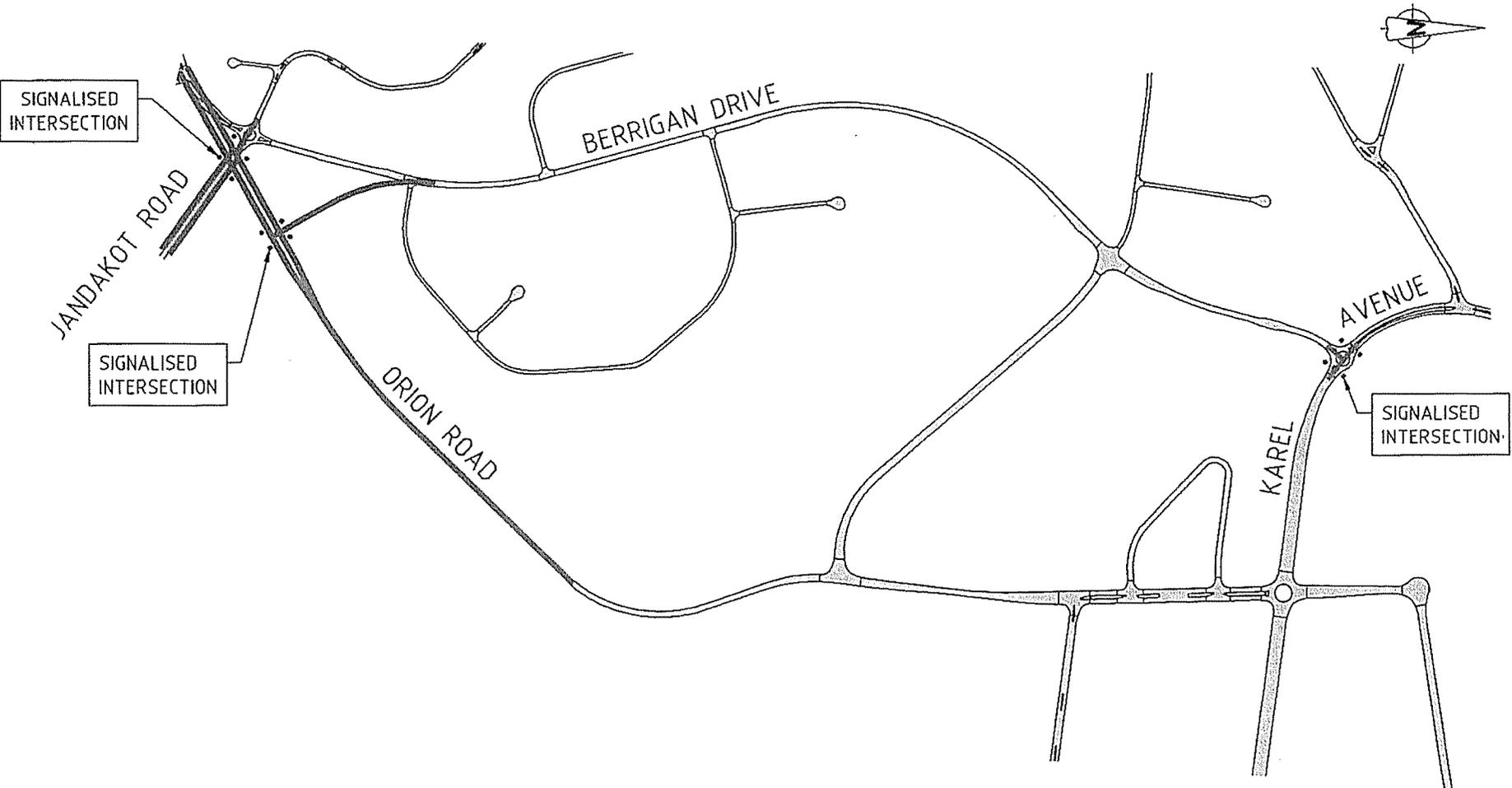
I trust that this is satisfactory however if you require any further information please contact Albert Symcox on 9323 4586. In reply please quote file reference 05/70 (D14#501218).

Yours sincerely



Gary Manning  
Acting Manager Traffic Management Services

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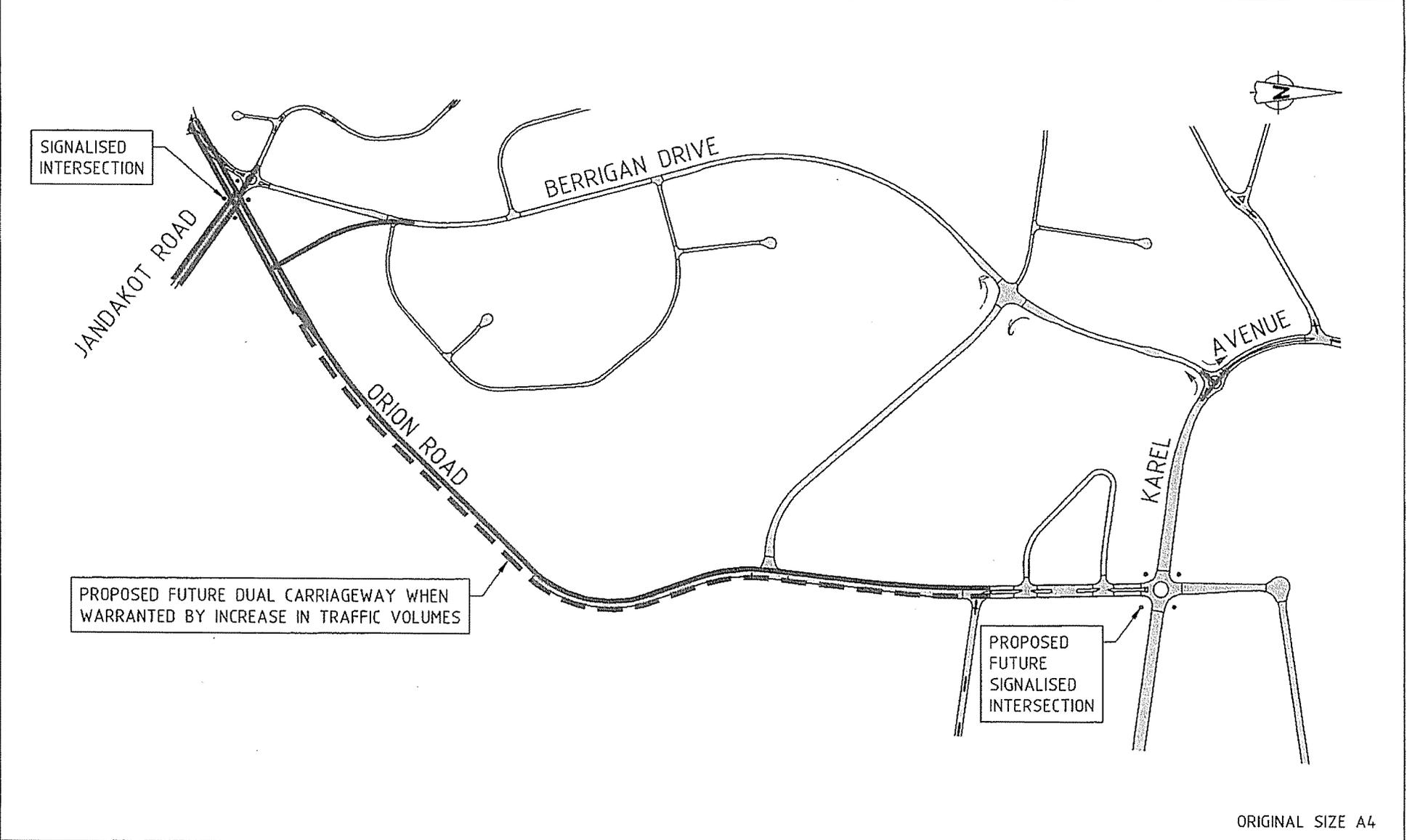
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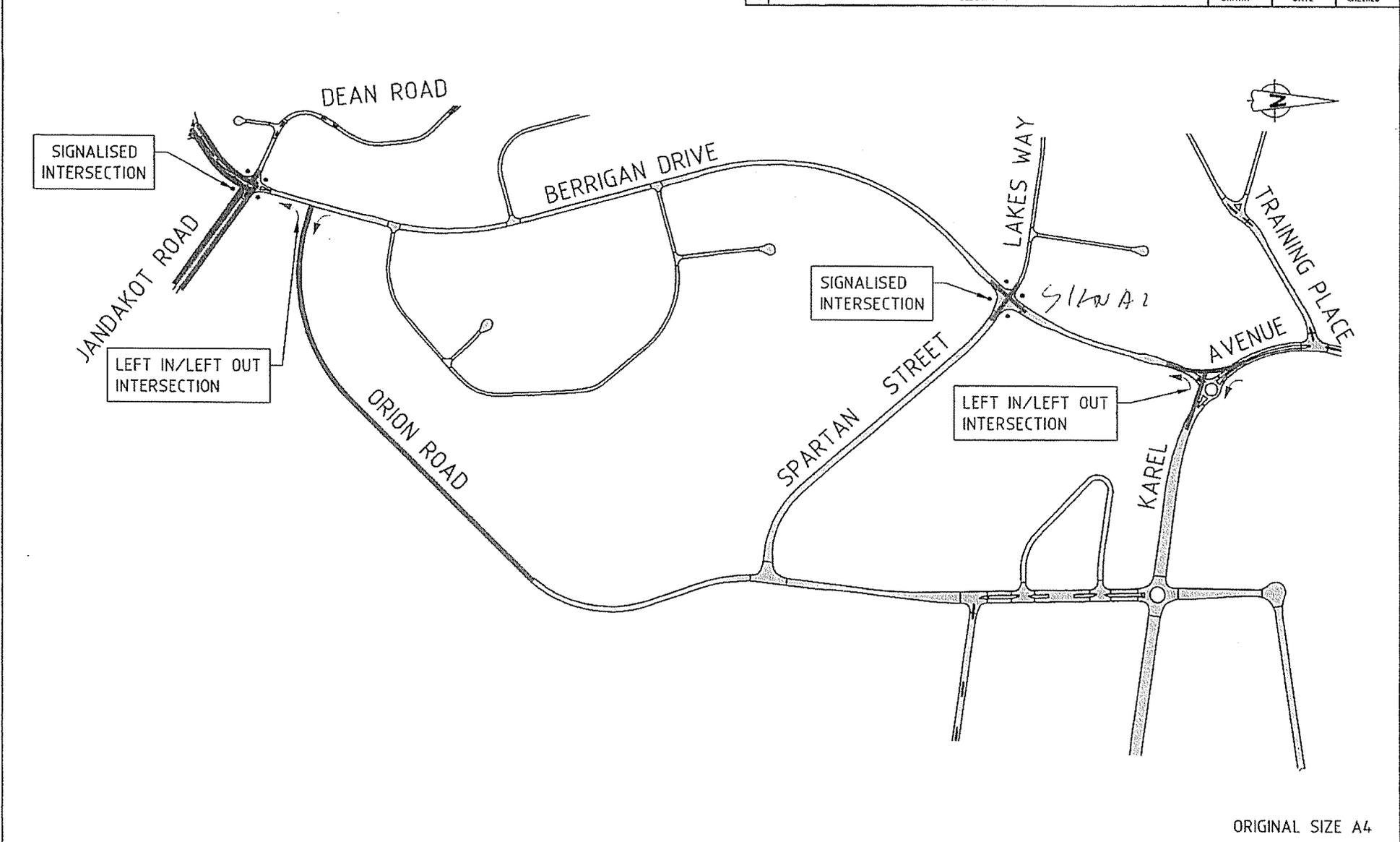
TITLE  
**JANDAKOT AIRPORT  
 NETWORK CONNECTION OPTION 2**

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TITLE  
**JANDAKOT AIRPORT  
 SPARTAN STREET OPTION**

DESIGNED	TRANSCORE	APPROVED
DRAWN	T.C	DESIGN MANAGER
CHECKED		

SCALE	1:10000	JOB No.
DWG No.	SHEET No.	REV
	1	

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# Agreement for Upgrades to the External Road Network Servicing the Jandakot Airport

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City of Cockburn

Jandakot Airport Holdings Pty Ltd



McLEODS

Barristers & Solicitors

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Ref: FG:COC:35782

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# Details

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## Parties

### City of Cockburn

of PO Box 1215, Bibra Lake, Western Australia  
(City)

### Jandakot Airport Holdings Pty Ltd (ACN 081 643 156)

of 16 Eagle Drive, Jandakot, Western Australia  
(JAH)

## Background

- A JAH leases the land described in **Item 1** of the Schedule to this Deed from the Commonwealth of Australia for the purposes of an airport (**Land**).
- B In 2009, JAH prepared a revised masterplan, the “2009 Jandakot Airport Masterplan” (**Masterplan**), which detailed future development over the Land (**Development**).
- C At the Council meeting on 8 October 2009, Council considered the Masterplan and resolved as follows:

“That Council:

- (1) forward a submission regarding the Draft 2009 Jandakot Airport Master Plan to Jandakot Airport Holdings (“JAH”) on the basis of the officer’s report, emphasising the following specific points:

...

2. All specific requirements in relation to traffic and transport infrastructure as discussed under the ‘Traffic and Transport Issues’ section of the officer’s report being undertaken by JAH;

3. JAH being responsible for the constructing/upgrading of all the existing infrastructure outside of the airport site identified as being required to facilitate the Master Plan;

...”

### (Council Resolution)

- D The officer’s report referred to in the Council Resolution is annexed hereto as **Annexure 1** and details the suite of road network upgrades contemplated at that time.
- E JAH originally predicted that the Development will generate approximately 37,000 vehicles per day. JAH have revised their land use data and now predict that the Development will generate approximately 24,000 vehicles per day.

- F On the basis of the general reduction of traffic predicted through proposed changes to land use, the parties acknowledge that the suite of road network upgrades contemplated in the Masterplan will need to be staged.
- G To better facilitate development in the short term, JAH has agreed to fund and construct the following works as a priority:
- (a) the extension of Orion Road from the intersection of Berrigan Drive/Jandakot Road/Dean Road to the constructed portion of Orion Road within the Jandakot Airport boundary via the designated road reserve depicted in **Annexure 2 (Orion Road Extension)**, such works to include but not be limited to:
    - i. constructing the Orion Road Extension as a single carriageway road and connecting the Orion Road Extension with the external road network at the intersection of Berrigan Drive/Jandakot Road/Dean Road; and
    - ii. purchasing sufficient land to construct the Orion Road Extension as a dual carriageway and ceding that land to the Crown as road land;
  - (b) signalling and constructing the intersection of Jandakot Road/Berrigan Drive (south)/Dean Road/Orion Road generally in accordance with the design concept endorsed by the City and MRWA and annexed hereto as **Annexure 3**;
  - (c) constructing the intersection of Berrigan Drive (north)/Orion Road generally in accordance with the design concept endorsed by the City and MRWA and annexed hereto as **Annexure 3** and providing appropriate signage control to the intersection in accordance with MRWA's approval annexed hereto as **Annexure 6**; and
  - (d) signalling and constructing the intersection of Berrigan Drive (north)/Karel Avenue generally in accordance with the design concept endorsed by the City and MRWA and annexed hereto as **Annexure 4**.

**(Proposed Works)**

- H The City has supported JAH's application to MRWA for approval of the proposed signalised intersections referred to at **Recitals G(b) to (d)** and which are more particularly set out in the letter from the City to MRWA dated 22 May 2014, a copy of which is annexed hereto as **Annexure 5**.
- I MRWA has subsequently supported the road network upgrades referred to in **Recitals G(b) and (d)**. MRWA only supports regulatory sign control at the signalised intersection referred to a **Recital G(c)** in the short term and will reconsider the signalisation and construction of this intersection at a later point in time. The terms of MRWA's approval are more particularly set out in the letter from MRWA to the City dated 15 September 2014, a copy of which is annexed hereto as **Annexure 6**.
- J The parties enter into this Deed to set out the terms of the City's approval to the Proposed Works and to satisfy the City's requirements.

# Agreed Terms

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## 1. Defined terms and interpretation

### 1.1 Defined terms

In this Deed, unless otherwise required by the context or subject matter:

**Approved Plans** means any plans or drawings identifying the areas where the Proposed Works are to be constructed/installed and detailing the works required to complete the Proposed Works as approved by the City and MRWA;

**Authorised Person** means:

- (a) an agent, employee, licensee or invitee of JAH; and
- (b) any person visiting the Land with the consent or implied consent of any person mentioned in paragraph (a);

**Bank Guarantee** means any bank guarantee (including the Initial Bank Guarantee) required pursuant to the provisions of **clause 4**;

**Business Day** means a day other than a Saturday, Sunday or public holiday in Perth, Western Australia;

**Completion Date** means:

- (a) in respect of the item of the Works listed at **Recital G(a)**, 12 months from the date of execution of this Deed;
- (b) in respect of the item of the Works listed at **Recital G(b)**, 12 months from the date of execution of this Deed;
- (c) in respect of the item of the Works listed at **Recital G(c)**, 12 months from the date of execution of this Deed; and
- (d) in respect of the item of the Works listed at **Recital G(d)**, 31 December 2017 or when traffic volumes on Karel Avenue reach 15,000 vehicles per day, whichever is the sooner.

Notwithstanding (a) to (d) above, the City, acting reasonably and in consultation with JAH, may bring forward the Completion Date for an item of the Works if there is a clearly demonstrable need to complete that item of the Works prior to the relevant Completion Date listed above;

**Council** means the council of the City;

**Deed** means this deed as supplemented, amended or varied from time to time;

**Defects Liability Period** has the meaning given to that term in **clause 10(1)**;

**Dispute** means any dispute, difference of opinion or disagreement between the parties arising out of or in connection with this Deed;

**Initial Bank Guarantee** means a bank guarantee in the amount specified at **Item 2** of the Schedule;

**Land** means the land described in **Item 1** of the Schedule;

**MRWA** means Main Roads WA;

**Notice** means each notice, demand, consent or authority given or made to any person under this Deed;

**Orion Road** means Orion Road (also referred to as South Link Road and Pilatus Road);

**Proposed Works** means the proposed works described at **Recital G**;

**Schedule** means the schedule to this Deed; and

**Works** means the works described in **clause 3(1)(a)** and **clause 7(a)**. For the sake of clarity, a reference to an 'item of the Works' is a reference to an item listed at **Recital G**.

## 1.2 Interpretation

In this Deed -

- (a) headings have been inserted for guidance only and shall be deemed not to form part of the text;
- (b) where a reference to a party includes more than one person the rights and obligations of those persons shall be joint and several;
- (c) reference to the parties includes their personal representatives, successors and lawful assigns; and
- (d) the Schedule and Annexures (if any) form part of the Deed.

## 2. JAH's Covenants: Design Drawings for the Proposed Works

JAH HEREBY COVENANTS AND AGREES with the City that it will submit design drawings for the Proposed Works for approval by the City and MRWA to enable completion of the Proposed Works by the Completion Date.

### 3. Completion of Works in accordance with Approved Plans

(1) JAH COVENANTS AND AGREES with the City that it shall:

- (a) at its own expense complete the Proposed Works in accordance with the Approved Plans and to the satisfaction of the City (acting reasonably) (**Works**) by the Completion Date; and
- (b) not make any substantial or material change to the Approved Plans unless otherwise agreed to in writing by the City.

(2) The City COVENANTS AND AGREES with JAH that:

- (a) if JAH seeks to vary the Approved Plans the City will consider and determine that request within a reasonable period of time; and
- (b) if the request made in **clause 3(2)(a)** is approved then the amendment forms part of the Approved Plans.

### 4. Performance Security

#### 4.1 Payment of Initial Bank Guarantee

Within 30 days after the date of execution of this Deed, JAH must provide to the City the Initial Bank Guarantee.

#### 4.2 Replacement of Bank Guarantee

The parties COVENANT AND AGREE:

- (a) at any time after completion of all of the items of the Works listed at **Recital G(a) to (c)** in accordance with **clause 3** and the issue of certificates of Practical Completion in respect of those items of the Works in accordance with **clause 9**, JAH may provide a replacement Bank Guarantee to the City in the amount of one million and five hundred thousand dollars (\$1,500,000.00); and
- (b) the City must return the Initial Bank Guarantee it holds in exchange for any replacement Bank Guarantee provided by JAH under **clause 4.2(a)**.

#### 4.3 Defects Liability Bank Guarantee

The parties COVENANT AND AGREE:

- (a) upon completion of the Works in accordance with **clause 3** and the issue of certificates of Practical Completion in respect of all items comprising the Works in accordance with **clause 9**, JAH may provide a replacement Bank Guarantee to the City in the amount of two hundred thousand dollars (\$200,000.00) as security against any defects, shrinkages or other faults in the Works; and
- (b) the City must return any Bank Guarantee it holds in exchange for any replacement Bank Guarantee provided by JAH under **clause 4.3(a)**.

#### 4.4 Return of Bank Guarantee

The City COVENANTS AND AGREES:

- (a) upon expiration of the Defects Liability Period, the City shall on written request from JAH refund the remaining portion of any Bank Guarantee it holds plus all interest payments less bank fees, to JAH within 21 days; and
- (b) where the City has carried out the Works pursuant to **clause 6** and/or undertaken works to remedy a defect, shrinkage or any other fault in the Works pursuant to **clause 10** and the Bank Guarantee held by the City has been insufficient to cover the costs of those works, then any costs in excess of the Bank Guarantee shall be a liquidated debt recoverable by the City in a court of competent jurisdiction from JAH.

#### 4.5 Requirements of Bank Guarantee

Any Bank Guarantee provided pursuant to the terms of this clause must be:

- (a) an irrevocable and unconditional guarantee;
- (b) issued in favour of the City by a bank or financial institution authorised to carry on banking in Australia under the *Banking Act 1959*; and
- (c) in a form reasonably acceptable to the City.

#### 4.6 Genuine pre-estimate of Bank Guarantee

JAH covenants and agrees that the amount of any Bank Guarantee provided pursuant to the terms of this clause is a genuine pre-estimate of the cost the City would incur if it was required to undertake the Works (or, in the event that some items of the Works have been completed, the remaining Works) pursuant to this Deed.

### 5. Extension of time for Completion Date

- (1) Subject to **clause 5(2)** below, the parties agree the Completion Date for the Works may be extended for a period determined by the City acting reasonably and in consultation with JAH in the event that the Works cannot be completed on or before the Completion Date for a reason or reasons beyond the reasonable control of JAH, including but not limited to:
  - (a) acts of God, including fire, bushfire, lightning, storm, tidal wave, cyclone, hurricane, earthquake, landslide, mudslide, washouts and flood;
  - (b) epidemics, public health scares or outbreaks of disease;
  - (c) war, revolution or other state of armed hostility of a like nature;
  - (d) insurrection, civil disturbances or riot (except where arising within the custodial areas);

- (e) collisions or accidents which constitute a major catastrophe, an example being an aircraft crash or nuclear contamination;
  - (f) unavailability or lack of reasonable availability in the State of labour and or building and construction materials; and
  - (g) a strike, lockout, or other industrial disturbance or restraint of labour, involving employees,
- (2) To permit the City to consider extending the Completion Date for the Works in accordance with **clause 5(1)** JAH must provide to the City reasonable evidence within a reasonable period of time, as to the reason for and the extent of the delay and the reasonable steps taken by JAH to overcome that delay.

## 6. Default

If the Works are not completed to the reasonable satisfaction of the City by the Completion Date, or such other date agreed to by the City in accordance with **clause 5**, and that default is not remedied within 30 days after written notice from the City, the City may:

- (a) enter onto the Land (or any part thereof) and complete the Works; and
- (b) recover the cost of the Works from JAH, and the parties agree that:
  - (i) such costs shall be a liquidated debt recoverable in a court of competent jurisdiction; and
  - (ii) the City may draw upon the Bank Guarantee to cover the cost of the Works, and any amount in excess of the Bank Guarantee shall be a liquidated debt recoverable in a court of competent jurisdiction.

## 7. Standard of Works

JAH COVENANTS AND AGREES with the City that:

- (a) JAH shall complete the Works in accordance with the Approved Plans and to the satisfaction of the City (acting reasonably);
- (b) all materials used in the works specified in **clause 7(a)** and standards of workmanship shall be in conformity with the specifications normally applied to the works specified in **clause 7(a)** by the City;
- (c) in the absence of any express provision made by the City in respect of any material or standard of workmanship that material or that standard of workmanship as the case may be shall be of a kind which is both suitable for its purpose and consistent with the nature and character of that part of the works specified in **clause 7(a)** for which it is intended to be used;

- (d) if required by the City, JAH shall produce evidence that the materials used or to be used in connection with the works specified in **clause 7(a)** comply with the provisions of this **clause 7**;
- (e) JAH shall comply with the City's standard requirements in relation to the inspection of works before those works are covered up;
- (f) subject to compliance by JAH with **clause 7(e)**, should the City issue instructions to JAH to open up for inspection any works covered up or to arrange for or carry out any testing of any materials (whether or not already incorporated in the works specified in this **clause 7**) or of any executed work, JAH shall comply with those instructions and the cost of any such opening up or testing (together with the cost of making good in consequence thereof) shall be paid by the City to JAH unless:
  - (i) such opening up or testing is consequent upon a failure of JAH to comply with some provision of this **clause 7**;
  - (ii) the opening up or testing shows that such work or materials are not in accordance with this **clause 7**; or
  - (iii) JAH has not complied with **clause 7(e)**;
- (g) if the quality of the work or materials is not in accordance with the requirements of this **clause 7** then JAH must remove and/or re-execute and/or replace any such work or materials and make good any defect at JAH's expense;
- (h) all works on existing City infrastructure, including but not limited to roads, footpaths, drainage, parks and verges, shall be completed and reinstated in accordance with the *Public Utilities Code of Practice 2000, Restoration and Reinstatement Specification for Local Government 2002* and the City's *Excavation Reinstatement Standards 2002* as a minimum standard; and
- (i) JAH shall prepare and implement a traffic management plan for the Works and JAH must submit the traffic management plan to the City prior to its implementation for the City's approval.

## 8. Insurance

### 8.1 Insurance to be effected

- (1) Unless JAH self-insures, JAH must effect and maintain with insurers approved by the City and noting the interests of the City and JAH for their respective rights and interests in the Land for the time being adequate public liability insurance for a sum not less than the sum set out at **Item 3** of the Schedule in respect of any one claim or such greater amount as the City may from time to time reasonably require.
- (2) If JAH insures under a policy covering more than one property (**Master-type Policy**) to satisfy its insurance obligations outlined in **clause 8.1(1)**, the parties acknowledge and agree that it may not be possible for the City's interest to be noted on such

insurance and such failure will not constitute a breach of JAH's obligations pursuant to **clause 8.1(1)**.

## **8.2 Details and Receipts**

Unless JAH self-insures, in respect of the insurance required by **clause 8.1** JAH must:

- (a) on demand supply to the City details of the insurance and give to the City copies of certificates of currency;
- (b) promptly pay all premiums and produce to the City each certificate of currency and each receipt for premiums paid; and
- (c) notify the City immediately:
  - (i) when an event occurs which gives rise or might give rise to a claim; or
  - (ii) any cancellation of the policy.

## **8.3 Right to self-insure or insure with affiliated insurance company**

- (1) Notwithstanding anything in this clause, JAH may at its option and in accordance with law elect to self-insure or to procure all or any portion of the insurance coverage required by **clause 8.1** with an affiliated insurance company or companies which insurance coverage and insurers will be deemed approved by the City.
- (2) If JAH procures all or any portion of its insurance coverage with an affiliated insurance company or companies:
  - (a) JAH must provide, upon written notice of the City, details in relation to its insurance; and
  - (b) the insurance must include adequate public liability protection to ensure coverage for a sum not less than the sum set out at **Item 3** of the Schedule in respect of any one claim or such greater amount as the City may from time to time reasonably require.

## **8.4 Not to invalidate**

JAH must not knowingly do or omit to do any act or thing or bring or keep anything on the Land which might render any insurance effected under this clause on the Land, or any adjoining premises, void or voidable.

## **8.5 Report**

Each party must report to the other promptly in writing, and in addition verbally in an emergency:

- (a) any material damage to the Land of which they are or might be aware; and

- (b) any circumstances of which they are aware and which are likely to be a danger or cause any material damage or danger to the Land or to any person in or on the Land.

## 9. Practical Completion

The City COVENANTS AND AGREES with JAH that:

- (a) within 14 days of the date of notice that an item of the Works has reached completion, the City will issue to JAH a certificate of Practical Completion in respect of that item of the Works provided that the item of the Works has in the opinion of the City (acting reasonably) been carried out in accordance with the Approved Plans and the standards referred to in **clause 7** and are suitable for their proposed use despite that there may be some items of work that have not been completed but which do not prevent the lawful use of the Works (**Practical Completion**);
- (b) within 14 days of notice that the Works have reached completion, the City will issue to JAH a written list of outstanding items that must be completed before the certificate of Practical Completion can be issued; and
- (c) within 14 days of completing the list of works referred to in **clause 9(b)** the City will issue the certificate of Practical Completion.

## 10. Defects Liability

- (1) JAH COVENANTS AND AGREES with the City that any defects, shrinkages or other faults in the Works which may be evident and be notified in writing by the City to JAH within twelve (12) months after the issue of the certificate of Practical Completion in respect of the last item of the Works to be issued with a certificate of Practical Completion (**Defects Liability Period**) shall upon the issue of written instructions from the City be made good by JAH within a reasonable time but in any event within three (3) months after the issue of the instructions.
- (2) If JAH fails to commence the work necessary to remedy the defect, shrinkage or fault in an item of the Works within the time specified in paragraph (1) above, the City may (but only after giving JAH a notice requiring JAH to commence works to remedy the defect, shrinkage or other fault in the Works and JAH has failed to comply with that notice) proceed to do the work, or engage another party to do the work, and in respect of such works the City may draw upon any Bank Guarantee it holds to cover the cost of completing such works, and any amount in excess of the Bank Guarantee will be a liquidated debt recoverable by the City in a court of competent jurisdiction from JAH.

## 11. Risk and Liability

JAH COVENANTS AND AGREES with the City that the Works shall in all respects be at the risk of JAH but only until the date of the certificate of Practical Completion is issued and until that date JAH is liable for and HEREBY INDEMNIFIES the City against:

- (a) any liability, loss, claim or proceeding in respect of any injury, loss or damage whatsoever to any property real or personal insofar as such injury, loss or damage arises out of or in the course of or by reason of the carrying out the Works except to the extent that any liability, loss, claim or proceeding is caused or contributed to by the negligent act or omission of the City or any employee, contractor or agent of the City; and
- (b) any liability, loss, claim or proceeding whatsoever arising under any statute or at common law in respect of personal injury to or death of any person whomsoever arising out of or in the course of or caused by such works except to the extent that any liability, loss, claim or proceeding is caused or contributed to by the negligent act or omission of the City or any employee, contractor or agent of the City.

## 12. Consultation with City

JAH agrees to provide the City with written notice:

- (a) in the event that the nature or quality of the Works varies from the Approved Plans; and
- (b) following completion of the Works.

## 13. Indemnity

(1) JAH indemnifies, and shall keep indemnified, the City from and against all actions, claims, costs, proceedings, suits and demands whatsoever which may at any time be incurred or suffered by the City, or brought, maintained or made against the City, in respect of:

- (a) any loss whatsoever (including loss of use);
- (b) injury or damage of, or to, any kind of property or thing; and
- (c) the death of, or injury suffered by, any person,  
caused by, contributed to, or arising out of, or in connection with, whether directly or indirectly:
  - (i) the use of the Land by JAH or any Authorised Person;
  - (ii) JAH carrying out the Works;
  - (iii) JAH's activities, operations or business on, or other use of any kind of, the Land;
  - (iv) any default by JAH in the due and punctual performance, observance and compliance with any of JAH's covenants or obligations under this Deed; or
  - (v) an act or omission of JAH.

- (2) Without limiting JAH's obligations under **clause 13(1)**, in the event of default by JAH in the completion of the Works in accordance with **clause 3(1)(a)** and the City carrying out the Works in accordance with **clause 6**, JAH HEREBY COVENANTS and AGREES to indemnify and keep indemnified the City and any employees, contractors or agents authorised by the City to enter onto the Land and carry out the Works in respect of all claims, demands, suits, actions or legal proceedings of whatsoever nature and howsoever arising in respect of any act done or attempted to be done in connection with the completion of the Works.
- (3) The obligations of JAH under this **clause 13**:
  - (a) are unaffected by the obligation of JAH to take out insurance, and the obligations of JAH to indemnify are paramount, however if insurance money is received by the City for any of the obligations set out in this **clause 13** then JAH's obligations under this **clause 13** will be reduced by the extent of such payment; and
  - (b) continue after the expiration or earlier determination of this Deed in respect of any act, deed, matter or thing occurring or arising as a result of an event which occurs before the expiration or earlier determination of this Deed.
- (4) The parties agree that nothing in this clause shall require JAH to indemnify the City, its officers, servants, or agents against any loss, damage, expense, action or claim:
  - (a) arising out of an act or omission of the City, or its servants, agents, contractors or invitees; or
  - (b) after the date on which the certificate of Practical Completion is issued (except in respect of JAH's defects liability works under **clause 10**).

## 14. Acknowledgement of JAH

JAH acknowledges and agrees with the City that if it does not complete the Works by the Completion Date, or such other date agreed to by the City in accordance with **clause 5**, it will be open to the City to take action to secure compliance with the terms of this Deed, including commencing an action for breach of contract, and/or obtaining an order for specific performance.

## 15. Dispute Resolution

If at any time a Dispute arises between the parties, either party must, as soon as reasonably practicable, give to the other notice of the existence of the Dispute giving full particulars of the nature and extent of the Dispute. Prior to either party taking any other dispute resolution proceedings (other than applications for urgent interlocutory relief), the parties must attempt, for a period of 14 days after receipt by one party of a notice from the other party of the existence of the Dispute, to settle such Dispute amicably by mutual discussions between the parties.

## 16. Assignment

JAH COVENANTS AND AGREES with the City that in the event that it transfers or otherwise disposes of its interest in the Land prior to completion by it of the obligations set out in this Deed, it shall remain liable to carry out those obligations unless the assignee or transferee of such rights enters into a deed of covenant with the City at the cost of JAH whereby the transferee or assignee covenants to observe and perform such of the covenants, conditions and stipulations herein contained (including this covenant) as the City shall require, as if that person had been a party to this Deed.

## 17. Costs

Each party shall pay their own costs (including legal costs) associated with the preparation (including all preliminary drafts), negotiation, execution and stamping of this Deed and duty payable hereon.

## 18. Notices

Any communication under or in connection with this Deed -

- (a) must be in writing;
- (b) must be addressed in the manner specified in **Item 4** of the Schedule;
- (c) must be signed by the party making the communication or on its behalf by the solicitor for, or by any attorney, director, secretary, or authorised agent or officer of, any party;
- (d) must be delivered or posted by prepaid post to the address, or sent by fax to the number, of the addressee, in accordance with **clause 18(b)** of this Deed;
- (e) will be deemed to be given or made -
  - (i) if by personal delivery, when delivered;
  - (ii) if by leaving the Notice at an address specified in **clause 18(b)** of this Deed, when left at that address unless the time of leaving the Notice is not on a Business Day or after 5 pm on a Business Day, in which case it will be deemed to be given or made on the next following Business Day;
  - (iii) if by post, on the second Business Day following the date of posting of the Notice to an address specified in **clause 18(b)** of this Deed; and
  - (iv) if by facsimile, when despatched by facsimile to a number specified in **clause 18(b)** of this Deed unless the time of dispatch is not on a Business Day or after 5 pm on a Business Day, in which case it will be deemed to be given or made on the next following Business Day.

## 19. Variation

A variation to this Deed must be in writing and signed by the parties.

## 20. Further assurances

Each party must execute and deliver all such documents, instruments and writings and must do and must procure to be done all such acts and things as may be necessary or desirable to implement and give full effect to the provisions and purpose of this Deed.

## 21. Severance

If any part of this Deed is, or becomes, void or unenforceable that part is or will be, severed from this Deed to the intent that all parts that are not, or do not become, void or unenforceable remain in full force and effect and are unaffected by that severance.

## 22. Waiver

The parties mutually covenant and agree that -

- (a) no right under this Deed is waived or deemed to be waived except by notice in writing signed by the party waiving the right;
- (b) a waiver by one party under **clause 22(a)** of this Deed does not prejudice its rights in respect of any subsequent breach of this Deed by the other party; and
- (c) a party does not waive its rights under this Deed because it grants an extension or forbearance to the other party.

## 23. Applicable law

This Deed shall be governed by the laws of the State of Western Australia, and where applicable the Commonwealth of Australia.

# Schedule

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Item 1 Land

[JAH to provide details of the land leased to JAH from the Commonwealth for the purposes of the Jandakot Airport].

Item 2 Initial Bank Guarantee

Eight million dollars (\$8,000,000.00).

Item 3 Public Liability Insurance

Twenty million dollars (\$20,000,000.00).

Item 4 Notices

**City** City of Cockburn

**Address:** PO Box 1215  
BIBRA LAKE WA 6163

**Fax No:** (08) 9411 3333

**Attention:** Chief Executive Officer

**JAH** Jandakot Airport Holdings Pty Ltd

**Address:** 16 Eagle Drive  
JANDAKOT WA 6164

**Fax No:** (08) [JAH to advise]

**Attention:** [JAH to advise]

# Signing page

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EXECUTED

2014.

THE COMMON SEAL of the CITY OF COCKBURN )  
was hereunto affixed in the presence of: )

\_\_\_\_\_  
(Signed) (Position) (Print Full Name)

\_\_\_\_\_  
(Signed) (Position) (Print Full Name)

EXECUTED by JANDAKOT AIRPORT HOLDINGS )  
PTY LTD (ACN 081 643 156) pursuant to Section )  
127 of the Corporations Act: )

\_\_\_\_\_  
Full Name of Director Signature of Director

\_\_\_\_\_  
Full Name of Director/Secretary Signature of Director/Secretary  
**(Delete whichever designation is incorrect)**

35782-14.09.23-MR-Deed

# Annexure 1 – Officer's Report

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[City to provide]



14.5 **(MINUTE NO 4064)** (OCM 8/10/2009) - DRAFT 2009 JANDAKOT AIRPORT MASTER PLAN - OWNER: JANDAKOT AIRPORT HOLDINGS (1211) (A TROSIC, R DONG) (ATTACH)

**RECOMMENDATION**

That Council:

- (1) forward a submission regarding the Draft 2009 Jandakot Airport Master Plan to Jandakot Airport Holdings ("JAH") on the basis of the officer's report, emphasising the following specific points:
  1. While the City does not object to the proposed fourth runway and associated aviation development, issues with the proposed non-aviation commercial development on the airport must be adequately addressed to the satisfaction of relevant State Government agencies and the City before support can be indicated. The City also needs to be satisfied that environmental issues have been adequately managed, and that threats to the Jandakot Water Mound have been mitigated against. The City also requires JAH to ensure that residents adjoining the airport site will not experience adverse amenity impacts associated with the commercial development;
  2. All specific requirements in relation to traffic and transport infrastructure as discussed under the 'Traffic and Transport Issues' section of the officer's report being undertaken by JAH;
  3. JAH being responsible for the constructing/upgrading of all the existing infrastructure outside of the airport site identified as being required to facilitate the Master Plan;
  4. All environmental issues and concerns raised by the City being suitably addressed by JAH to the satisfaction of the City. In this respect, the City maintains significant concerns whenever extensive clearing of vegetation is proposed and in terms of the Master Plan, the loss of almost 167ha of good to excellent quality bushland will have a significant impact, both on flora diversity and fauna movement in the immediate area;
  5. The clearing and development of Precinct 5 not taking place until it can be clearly demonstrated that rehabilitation of Precincts 7 and 8 by JAH (outside the airport site) is producing viable and successful rehabilitation results to the satisfaction of the City. In making this determination, the City will be seeking evidence which demonstrates that the rehabilitation will produce an adequate environmental offset to the

- bushland proposed to be cleared as part of Precinct 5;
6. The description of zones and land use permissibility within the commercial development precinct being consistent with the provisions of the Local Planning Scheme;
  7. A detailed Economic Impact Assessment being prepared by JAH which demonstrates that their proposed major retail and commercial development can coexist and remain viable in conjunction with other existing and planned centres, and without adversely affecting the viability of such centres;
  8. Any retail activity being considered on the basis of serving the immediate airport precinct, and being most suitably located in close proximity to the residential colleges. In this respect retail development should be limited to limited to a 'Local Centre' scale, with a total maximum aggregated floor space of 2000m<sup>2</sup>, in order to minimise the impact on the City's Local Commercial Strategy;
  9. A formal protocol being established by way of Memorandum of Understanding between the City and JAH which requires involvement by the City in terms of the preparation and processing of structure plans, the processing of subdivision and development applications including parks and landscaping design and construction standards for infrastructure;
  10. The Master Plan being modified to indicate the provision of an appropriate buffer between the interface of all commercial land use which adjoins 'Resource' zoned allotments within the City;
  11. Precincts 6 and 6A being clearly labelled as subject to a future Master Plan, so as to make it clear that this is not approved as part of the 2009 Master Plan. Similar to Precinct 5, the City will also base its future position regarding Precinct 6 and 6A on whether rehabilitation of Precincts 7 and 8 by JAH outside the airport site has produced viable and successful rehabilitation results to the satisfaction of the City;
  12. Should the fourth runway be approved, all residences located within the 25-30 and 20-25 ANEF contour being acoustically treated at JAH's expense;
  13. If the fourth runway is approved, the number of flights per



- year being permanently capped at 514,000 fixed wing and 76,000 helicopter flights;
14. The informal night time curfew and Fly Neighbourly Policy being converted into a legally binding set of regulations upon both JAH and pilots;
  15. JAH to provide copies of all sub-leases within the commercial precinct to the City, upon JAH entering into any sub-lease within that precinct;
  16. Adequate servicing (especially reticulated sewer) being a pre-requisite for any expansion of existing aviation or non-aviation development on the airport;
  17. The Bush Forever Officer of the Department of Planning being specifically consulted by JAH regarding any future development that may impact on remnant bushland.
  18. The Department of Water being specifically consulted by JAH regarding future water and drainage management design, with the view to implementing best practice water sensitive design treatments to manage both water and drainage runoff;
- (2) provide a copy of its submission to the City of Melville, City of Canning and Western Australian Planning Commission for their information.

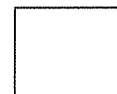
#### **COUNCIL DECISION**

MOVED Clr T Romano SECONDED Clr I Whitfield that the recommendation be adopted.

**CARRIED 6/1**

#### **Background**

At its meeting held on 8 September 2005 Council resolved to support the fourth runway addition to Jandakot Airport in accordance with selected Option 1 of the Draft 2005 Jandakot Airport Master Plan ("2005 Master Plan") (Attachment 1 refers). This support was subject to a series of recommended conditions, in order to ensure that the proposal would be implemented in an acceptable manner (Attachment 2 refers). The Draft 2005 Master Plan included plans for the construction of a fourth runway, and the allocation of non-aviation land



for commercial development. The proponent was Jandakot Airport Holdings ("JAH").

Following approval of the Draft 2005 Master Plan by the Federal Government in 2006, the City of Cockburn ("City") received a copy of the Draft Major Development Plan ("MDP") which proposed to undertake development of the airport in accordance with the 2005 Master Plan. Council considered the Draft MDP on 8 November 2007 and resolved to recommend approval subject to a number of recommended conditions (Attachment 3 refers). The City advised JAH on 15 November 2007 of Council's decision in this respect.

JAH have now released the Draft 2009 Master Plan for public comment, with comments closing on 30 October 2009 (Attachment 4 refers). This report considers the new Draft 2009 Master Plan, in the context of it being the strategic plan for development of the airport over the next 20 year period.

### **Submission**

NA.

### **Report**

#### **Overview**

In terms of statutory context, the development of airports is undertaken within the regulatory framework of the *Airports Act 1996*. This requires that for each airport, there is to be an approved Master Plan that indicates the airport planning for the next 20 years. A requirement of the *Airports Act 1996* is the preparation of a Master Plan every five years, and accordingly this Draft Master Plan represents a review and refinement of the approved 2005 Master Plan. The Master Plan is subject to approval of the responsible Federal Minister, being the Minister for Infrastructure, Transport, Regional Development and Local Government (Hon Anthony Albanese MP).

Part 5 of the *Airports Act 1996* sets out the statutory process for preparing and consulting in relation to a Draft Master Plan. In specific respect of consultation, Section 79(2) of the *Airports Act 1996* states:

- (2) *If members of the public (including persons covered by subsection (1A)) have given written comments about the preliminary version in accordance with the notice, the draft plan submitted to the Minister must be accompanied by:*
- (a) *copies of those comments; and*
  - (b) *a written certificate signed on behalf of the company:*
    - (i) *listing the names of those members of the public; and*



- (ii) summarising those comments; and
- (iii) demonstrating that the company has had due regard to those comments in preparing the draft plan; and
- (iv) setting out such other information (if any) about those comments as is specified in the regulations.

The Federal Government have released guidelines in respect of consultation, making it clear that the responsible Federal Minister expects that consultation is undertaken with all stakeholders and that prudent consideration is given to comments received during the consultation period, with the stakeholders' comments adequately summarised and the airport-lessee company (JAH) response appropriately documented in this respect. This information, along with copies of the public comment provided during the consultation period, forms part of the package that the Minister has regard to when considering whether to approve or refuse the Draft Master Plan.

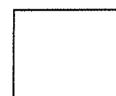
In terms of local government, the responsible Minister expects detailed and meaningful consultation to occur, within the specified timeframe of 60 days. As the consultation period for this Draft Master Plan closes on 30 October 2009, it is imperative that the City makes its submission direct to JAH, in accordance with the relevant legislative requirements. JAH must then, before submitting the Draft Master Plan to the responsible Minister for approval, demonstrate that they have had due regard to all the comments received in relation to the Draft Master Plan. It is stated that:

*“the Minister must be satisfied that the airport-lessee company gave regard which is adequate in all the circumstances to the comments received.”*

Relevant to this would be matters such as how the issues raised in the consultations will be/have been dealt with by the JAH, whether JAH considered properly those comments received, whether the comments were accepted or not into the Draft Master Plan and the grounds for rejecting particular comments. It does, however, need to be remembered that having due regard to public comment does not mean that the draft documents must automatically be changed to reflect those comments. This philosophy exists across most phases of public consultation, especially in relation to local government and town planning.

### **Draft 2009 Master Plan**

The Draft 2009 Master Plan indicates both aviation and non-aviation development across the airport site. This has been planned on a precinct basis, with six main precincts (and additional sub-precincts) identified as follows:



**Precinct 1A (31ha)** - This is a new conservation area to protect significant flora located within this area of the airport and retain connectivity to Ken Hurst Park. Precinct 1A was previously identified for commercial development under the approved 2005 Master Plan.

**Precinct 1B (47ha)** - This is an existing conservation area identified under the 2005 Master Plan which includes significant flora. This conservation area has been amended to take into account potential aviation requirements at the end of the runways and the requirement for the East Link road.

**Precinct 2 (39ha)** - This is an existing conservation area identified under the 2005 Master Plan. The conservation area has been amended to take into account potential aviation requirements at the end of the runways.

**Precinct 3 (225ha)** - This is the proposed aviation development that includes the following facilities:

- Enhancements to runways 06L/24R and 12/30 (future 12R/30L);
- No changes to runway 12L/30R;
- The proposed fourth runway 12L/30R;
- Existing and proposed taxiways and aprons;
- Aircraft support facilities.

**Precinct 4A (22ha)** - This is existing commercial development identified for bulky goods/showrooms under the 2005 Master Plan.

**Precinct 4B (9ha)** - This is also existing commercial development identified for office/commercial land uses.

**Precinct 4C (13ha)** - This is an area subject to a minor variation of the 2005 Master Plan which is now identified for commercial mixed use and aviation land uses.

**Precinct 4 (72ha)** - This is existing commercial development identified for warehouse/storage land uses.

**Precinct 5 (40ha)** - This is a new commercial area identified for warehouse/storage land uses in exchange for Precinct 1A, which was previously identified for commercial development in the 2005 Master Plan 2005.

**Precinct 6 and 6A (53ha)** - This precinct is identified as future development under the Draft 2009 Master Plan. The development of Precinct 6 will be further investigated following a review of this Master Plan in five years as required by the *Airport Acts 1996*.

In summary, the abovementioned precincts have resulted in land use within the airport site as being:



**Conservation (Precincts 1A, 1B, 2, 6 and 6A) - 170ha (27.3%)** including 53ha being earmarked for 'future development'. There is an additional 32ha of bushland identified for conservation under the Draft 2009 Master Plan, however this 32ha of bushland may be potentially used future runway extensions and operations.

**Aviation Development (Precinct 3, includes runways and taxiways) - 225ha (36.2%).**

**Non-aviation Development (Precincts 4, 4A, 4B, 4C, and 5) - 156ha (25.1%).**

**Roads and Services - 39ha (6.3%).**

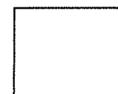
### **Land Use Issues**

In terms of land use, the Draft 2009 Master Plan retains the principal concepts of the approved 2005 Master Plan with the following revisions:

- The confirmation, provision and location of the fourth runway and associated taxiways;
- Proposed extension of runways 06L/24R and 12/30 (future 12R/30L);
- Proposed South Link - this road is proposed to connect from Karel Avenue to Jandakot Road and Berrigan Drive at the existing Road/Berrigan Drive/Dean Road intersection;
- Proposed East Link - The connection of this road outside of the airport boundary will be determined in discussions with the State Government and local governments;
- Change Precinct 1A from mixed business to conservation;
- Change Precinct 5 from conservation to mixed business.

Once complete, the proposed mixed business development will provide for up to 40,000m<sup>2</sup> of business and office space; 80,000m<sup>2</sup> of bulk retail and showroom space; and 470,000m<sup>2</sup> of warehouse and storage space. Once the commercial estate is fully occupied, it is predicted to accommodate approximately 190 businesses and 5,753 workers. This is of major significance and importance to the local, regional a state economies of Western Australia.

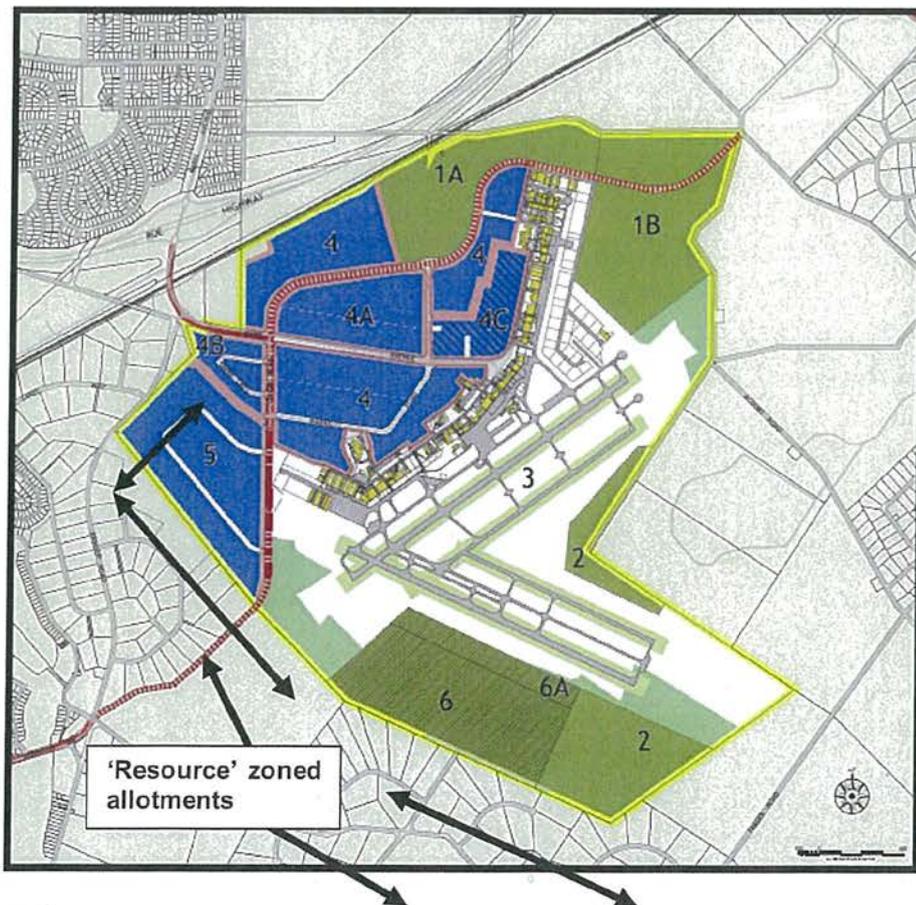
While stating this economic importance, it is also very important to ensure that this major commercial development does not undermine the viability of existing or planned commercial centres within the City. The City's Local Commercial Centres Strategy identified a portion of the airport land as future mixed business, but with no retail floor space recommended. The objective of this was to minimise the impacts on the planned and existing centres within the City, consistent with the prevailing state planning policy.



It is acknowledged that state planning policy is significantly changing in this respect, so as to remove the anti-competitiveness of restricting retail and commercial floorspace across the metropolitan area. The changing policy still however imposes a requirement for economic impact assessments to be produced, in order to demonstrate that major retail or commercial development can coexist and remain viable in conjunction with other existing or planned centres.

It is noted that the Draft 2009 Master Plan does not include a robust Economic Impact Assessment to reveal the positive or negative impacts associated with the planned retail and commercial developments at the airport. It is therefore considered imperative to require an Economic Impact Assessment to be prepared, so that the City has confidence that the extent of development will be appropriate in the context of the retail and commercial hierarchy which exists.

As also mentioned above, the Draft 2009 Master Plan proposes an important shift in both the location and distribution of commercial development on the airport site. This poses some important questions, specifically related to how the interface between higher intensity commercial development will be managed with the lower intensity 'Resource' zoned lots which adjoin the airport site. This is diagrammatically depicted following:



Considering that the current 2005 Master Plan depicts these Precincts 5 and 6 as part of the wider conservation area which spans the southern interface of the airport site, there will be implications for those lots which are now proposed to adjoin commercial precincts. Given such lots currently benefit from a well vegetated buffer between the developed parts of the airport and its perimeter boundaries, there needs to be consideration given to the potential impacts of this change and what can be done to manage such impacts. Questions to be asked include:

- i. Will the resulting commercial development have external impacts? Given the nature of the airport as a specialised centre dealing with logistical warehousing and storage functions, it is reasonable to suggest that some land uses by their nature will have some external impacts. This could include potential noise, traffic and light spill impacts. Given also that the hours of operation for such uses may extend into the night, such impacts may have a higher degree of magnitude than if just restricted to daytime hours. It will therefore be important to ensure that the placement of higher impact commercial uses is not on the boundary close to sensitive residential development, but alternatively placed internally on the airport site. Lower intensity uses could then buffer those higher intensity uses as the transition occurs out to the boundary of the airport site. This should be managed by the City continuing to be involved with the planning of the airport as has been facilitated by JAH.
- ii. At what topography level will the resulting commercial development operate? It is noted that the current topography of the airport site at its external boundaries is quite undulating. As resulting commercial development will need to tie in with the current operational level of the airport, there will need to be significant cutting and/or filling of the current topography. Depending on how this alteration to the topography is managed at the interface with the 'Resource' zoned lots, will depend on whether potential noise and other amenity impacts are able to be reduced. While it is acknowledged that the Master Plan is not the mechanism in which to delve into such technical detail, this issue should be identified and addressed by the City continuing to be involved with the detailed planning of the airport site.
- iii. How will the resulting commercial precinct be arranged? While the 2009 Draft Mater Plan indicates a basic road pattern for Precinct 5, this is only indicative and will be subject to further detailed planning. The road layout is important in terms of how traffic and other impacts can be mitigated against. For instance, should a commercial road interface be proposed against the 'Resource' zoned lots, then it is likely that traffic impacts will be exacerbated. Alternatively, should the road layout be designed such that the



back of commercial lots are provided at the interface with the 'Resource' zoned lots, then this may help buffer against commercial traffic impacts. Such is another detailed design issue that can be pursued through the City continuing to be involved with the detailed planning of the airport site.

From the above it is essential that a continued protocol occur in terms of dealing with the detailed planning of the proposed retail and commercial areas. This needs to cover the preparation and adoption of structure plans for the precincts, development standards, zones and permitted uses and specifications for roads, drainages and other infrastructure. This collaborative approach is supported by JAH, and is logical given that mutual benefits can be achieved if the airport development takes place in an optimal planning manner.

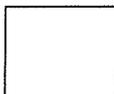
### **Traffic and Transport Issues**

Proposed access to the airport includes the following connections:

- Access from Berrigan Drive or Karel Avenue via the Karel Avenue extension (existing);
- South Link - this road is proposed to connect from Karel Avenue to Jandakot Road and Berrigan Drive at the existing Jandakot Road/Berrigan Drive/Dean Road intersection;
- Proposed East Link - The connection of this road outside of the airport boundary will be determined in discussions with the State Government and local governments.

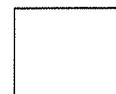
In respect of the proposed access and development, there are a number of issues in terms of the traffic, transport and road system which need to be addressed. These include:

- Karel Avenue needs to be a full four lanes all the way between Farrington Road and the airport to accommodate the increased traffic and to seamlessly tie in with the existing four lanes north of Farrington Road. The railway and Roe Highway bridges will need to be widened to accommodate this and a pedestrian/cycling path link retained;
- Berrigan Drive South will need to be widened to four lanes between the South Link Road and Kwinana Freeway to accommodate the additional traffic generated by the airport development;
- The airport roads are considered public roads, and they should integrate with and complement the existing surrounding road system. In this respect, the new South Link Road through the airport connecting Karel Avenue and Berrigan Drive South should replace Berrigan Drive North as a District Distributor A in the City's road hierarchy. Consequently, it should be a four lane road



through the airport with limited and controlled property access, and encouraged as the priority route;

- Through traffic needs to be discouraged from using Berrigan Drive North, and its connections to Karel Avenue and the South Link Road need to take this into account;
- The connections at South Link Road/Dean Road/Jandakot Road/Berrigan Drive will require detailed consideration, particularly as a large number of roads converge at this interchange. Private land acquisition may be required for provision and construction of South Link Road connection (outside of the airport). Should the feasibility of land acquisition become an issue, alternative options for this road connection will need to be identified as part of this Master Plan;
- Fraser Road is an unmade road. It is shown as carrying traffic under the airport's development plan. If this road is required to facilitate the airport's road access system, then it needs to be constructed as part of the airport's road development program;
- As the airport may be subject to emergency situations, ready, separate and controlled full turn emergency access should be provided to and from the adjacent Roe Highway away from the regular interchanges. This would be appropriate with the new Fiona Stanley Hospital being located off Roe Highway. This access point could also be utilised by other vehicles in order to reduce traffic impacts on the regular interchanges;
- A separate study is required to model the traffic, access requirements and upgrades to accommodate the traffic generation and transport impacts on the road feeder system and the freeway/highway system. The upgrade options for Karel Avenue, Berrigan Drive, Jandakot Road, freeway accesses etc (including treatments to encourage Karel Avenue traffic to drive through the airport rather than using Berrigan Drive North) can be modelled for various scenarios to identify the most appropriate treatments. A traffic model of the area was set up by the Roe7 Alliance and this model, if available, could be readily calibrated and modified to reflect the current traffic flow characteristics and the scenarios required for the airport development;
- The airport roads function as public roads and consequently should be designed and constructed to a minimum standard, particularly for public safety. They should be designed and constructed to the minimum requirements and procedures set by the new Local Government Guidelines for Subdivisional Development, published by the Institute of Public Works Engineering Australia (WA Division). Such should also be consistent with the City's Guidelines and Standards for the



Design, Construction and Handover of Subdivision within the Municipality;

- The subdivision process and civil works will need to be guided and monitored to ensure compliance with drawings, standards and the guidelines, and appropriate records and conformance declarations kept for any subsequent legal requirements, on-going asset management or should the care and management of the infrastructure be handed over into the future;
- The cost of all required civil upgrade works to external infrastructure as a result of the airport's expansion is considered the responsibility of JAH to fund.

It is noted that the proposed South and East Link Roads traverse some of the best quality vegetation on the site. The East Link Road also goes through a resource enhancement wetland. Recommendations are made to request JAH to reassess the specific location of these alignments so as to avoid these sensitive environmental features where practical.

The City also strongly supports JAH's initiative in terms of its proposed action in liaising with the State Government to provide feeder bus services to and from Murdoch Station on the Perth to Mandurah railway. JAH's commitment to pedestrian and cycling facilities is also strongly supported. The City also notes the importance of ensuring the flexibility of a future bus station/interchange to be integrated with the airport's development.

### **Environmental Issues**

Philosophically, the City maintains significant concerns whenever extensive clearing of vegetation is proposed. In terms of the Draft 2009 Master Plan, the loss of almost 167ha of good to excellent quality bushland will have a significant impact, both on flora diversity and fauna movement in the immediate area.

The City also acknowledges JAH's intention to rehabilitate the ex-sand mining areas adjoining the airport to the east (identified as Areas 7 and 8 in Attachment 6). These areas measure approximately 120ha in size, with approximately 90ha proposed for rehabilitation. While rehabilitation will never produce the same outcome as remnant bushland, it is still some form of an offset in this respect.

Attachment 5 contains the comments made by the City's Environmental Services Division in relation to the Draft 2009 Master Plan, associated Draft 2009 Environmental Strategy as well as the associated referral which has been received under the *Environment Protection and Biodiversity Conservation Act 1999*. This referral indicates that the Draft 2009 Master Plan and its associated



environmental impacts are being formally assessed in terms of their acceptability or not under the genus of the *Environment Protection and Biodiversity Conservation Act 1999*.

It needs to be noted that the comments made under Attachment 5 have already been provided to the Federal Department of the Environment, Water, Heritage and the Arts. This was on the basis that the advertising process for the *Environment Protection and Biodiversity Conservation Act 1999* formal assessment process closed on 3 October 2009, making it important for the City's Environmental Services Division to make its comments before this date.

Similar to the state process of environmental impact assessment, it is appropriate that the environmental acceptability of any proposal be determined relatively independent of other non-environmental related matters. That is, a proposal is either environmentally acceptable or not, and that an impartial environmental assessment can stand up to scrutiny where it has been based on environmental issues. JAH's willingness to request formal assessment of their Draft 2009 Master Plan under the *Environment Protection and Biodiversity Conservation Act 1999* indicates a commitment to it being impartially judged as whether it is environmentally acceptable.

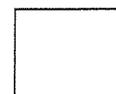
In addition to this formal assessment, it is recommended that all of the environmental issues and concerns raised by the City should be addressed by JAH.

### **Noises Issues**

#### **Aircraft Noise**

The proposed fourth runway is predicted to increase annual fixed wing flights by about 14,000, reaching a maximum of 514,000 flights per year in 2015. The Australian Noise Exposure Forecast ("ANEF") system is the recognised method of predicting likely impacts of aircraft noise on the general public. The Draft 2007 MDP recognised that "*for affected residents, the noise impacts associated with the use of the 12/30 direction runways will be intense, at frequent intervals and of relatively short duration*".

Following a review of the ANEF contours, the predicted new 20-25 and 25-30 ANEF contours will result in noise impacts on increased numbers of residences in the City. It is recognised that some of the affected properties are industrial and/or commercial in nature and are not noise sensitive. However any increase in the noise contours is certain to have an impact on a larger number of residents and it is estimated that there will be between 70 and 80 more residential properties located within the new 20-25 contour within the City. Of these, there will be about 20 residential lots located within the new 25-30 contour.



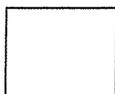
The 25-30 contour is recognised in AS2021 Table of Building Site Acceptability as the category 'unacceptable' for houses and schools. The 20-25 contour is recognised in AS2021 as the category 'conditional' for houses and schools. 'Conditional' typically acknowledges that buildings should be acoustically treated to minimise noise nuisance. In addition, the Draft MDP states that *"at the 20 ANEF approximately 10% of people are "seriously affected" and approximately 45% are "moderately affected" by aircraft noise"*.

Should the fourth runway be approved, all residences located within the 25-30 contour will be significantly affected by aircraft noise and these properties (approximately 20 within the City) need to be acoustically treated at JAH's expense. All existing houses and residential accommodation located within a 20-25 contour should also be acoustically treated at JAH's expense. This will mitigate some of the aircraft noise experienced inside the buildings, however it is noted that the noise nuisance will continue to exist when residents are enjoying their external living areas. It is also noted that the aircraft noise will be restricted to daytime only as the runways will not be lit for night time flights.

Full capacity in terms of aircraft flights and therefore aircraft noise impacts is predicted to be reached by about 2025. The predicted number of flights in the Draft 2007 MDP did not anticipate the significant increase in helicopter flights and this has resulted in the proposed development of a dedicated helicopter training area for the maximum 76,000 helicopter movements per year predicted by 2030. The reason for making this point now is to clarify for local residents, the ultimate worst case scenario of noise from the airport. This capacity is based upon no further runways, no additional helicopter training areas, and in particular, extremely limited flights during the night time.

At this strategic planning stage it is important to identify the final capacity of the airport which will be determined due to several criteria especially safety and noise impacts. The 25-30 contour is recognised in AS2021 as unacceptable for houses and schools and there is a compelling argument that this contour should not be permitted to extend into residential areas. However the ANEF map for 2025 indicates that the 25-30 contour does extend into residential areas. If the fourth runway is approved then the number of flights per year should be capped at 514,000 fixed wing and 76,000 helicopter flights. This will represent the final fixed capacity of the airport and will provide certainty for all stakeholders.

On top of this cap, it is recommended that a cap also be applied to the permitted number of night time flights and that the informal night time curfew and the Fly Neighbourly Policy be converted into a legally binding set of regulations upon both JAH and pilots. This reflects the



common complaint that some pilots do not comply with the policy and that there appears to be no legal compulsion to ensure compliance.

### **Trucks and Vehicle Noise**

The proponent should provide a comprehensive acoustic consultant's report showing that the development will comply with the State Planning Policy concerning road and rail transport noise.

### **Industrial Premises and Impacts**

When fully developed, the airport will have the potential to accommodate 250 businesses. While the potential employment and economic benefits are noted, the large scale of this proposal is significant. The most significant cause of both noise complaints and related health effects is excessive night time noise. This occurs rarely around the airport due to the low number of night time flights. However the occurrence of noise nuisance from industries operating 24 hours a day including vehicle movements, plant, machinery and reversing beepers is considered to be a possibility due to the local topography and the limited buffer to residential allotments. This needs to be carefully considered and responded to so as to ensure that no external noise impacts occur from commercial development on adjoining residential development.

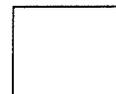
There are concerns also about the potential for the scale of commercial development to impact upon the Jandakot Water Mound. A significant part of the site is located over the Jandakot Water Mound, specifically proposed Precincts 4B and 5 and a portion of Precincts 4, 6 and 6A. These exist within Priority 1 Source Protection Area as shown under State Planning Policy No. 2.3 (Attachment 7 refers). It is therefore crucial that potentially polluting industries are not permitted to operate and/or that these industries are regulated to minimise the chance of contaminating this water supply. Any permission of these land uses must only occur if State and Federal Government regulatory bodies are satisfied regarding the level of risk on the Jandakot Water Mound.

### **Health Premises**

It is the City's understanding that the State Health legislation relating to Food Premises, Lodging Houses, Public Buildings and the installation of onsite effluent treatment and disposal systems is applicable and will continue to be administered via the City.

### **Conclusion**

The City does not object to the proposed fourth runway and associated aviation development. However, issues with the proposed non-aviation commercial development on the airport must be adequately addressed to the satisfaction of relevant State Government agencies and the City



before complete support can be indicated. The City also needs to be satisfied that environmental issues have been adequately managed, and that threats to the Jandakot Water Mound have been appropriately mitigated against. The City also requires JAH to ensure that residents adjoining the airport site will not experience adverse amenity impacts associated with the commercial development.

### **Strategic Plan/Policy Implications**

#### **Infrastructure Development**

- To construct and maintain parks and bushland reserves that are convenient and safe for public use, and do not compromise environmental management.

#### **Natural Environmental Management**

- To ensure development of the district is undertaken in such a way that the balance between the natural and human environment is maintained.

#### **Transport Optimisation**

- To construct and maintain roads which are convenient and safe for vehicles, cyclists and pedestrians.

### **Budget/Financial Implications**

NA.

### **Legal Implications**

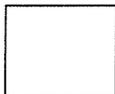
In terms of statutory context, the development of airports is undertaken within the regulatory framework of the *Airports Act 1996*. This requires that for each airport, there is to be an approved Master Plan that indicates the airport planning for the next 20 years. A requirement of the *Airports Act 1996* is the preparation of a Master Plan every five years, and accordingly this Draft Master Plan represents a review and refinement of the approved 2005 Master Plan.

### **Community Consultation**

NA.

### **Attachment(s)**

1. Option 1 of 2005 Master Plan
2. Council resolution dated 8 September 2005
3. Council resolution dated 8 November 2007
4. Draft 2009 Master Plan
5. Environmental Services Unit Submission
6. Map Showing Proposed Rehabilitation Areas
7. Map From State Planning Policy No. 2.3



**Advice to Proponent(s)/Submissioners**

The proponent has been advised that this matter will be considered at the Council meeting of 8 October 2009.

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.



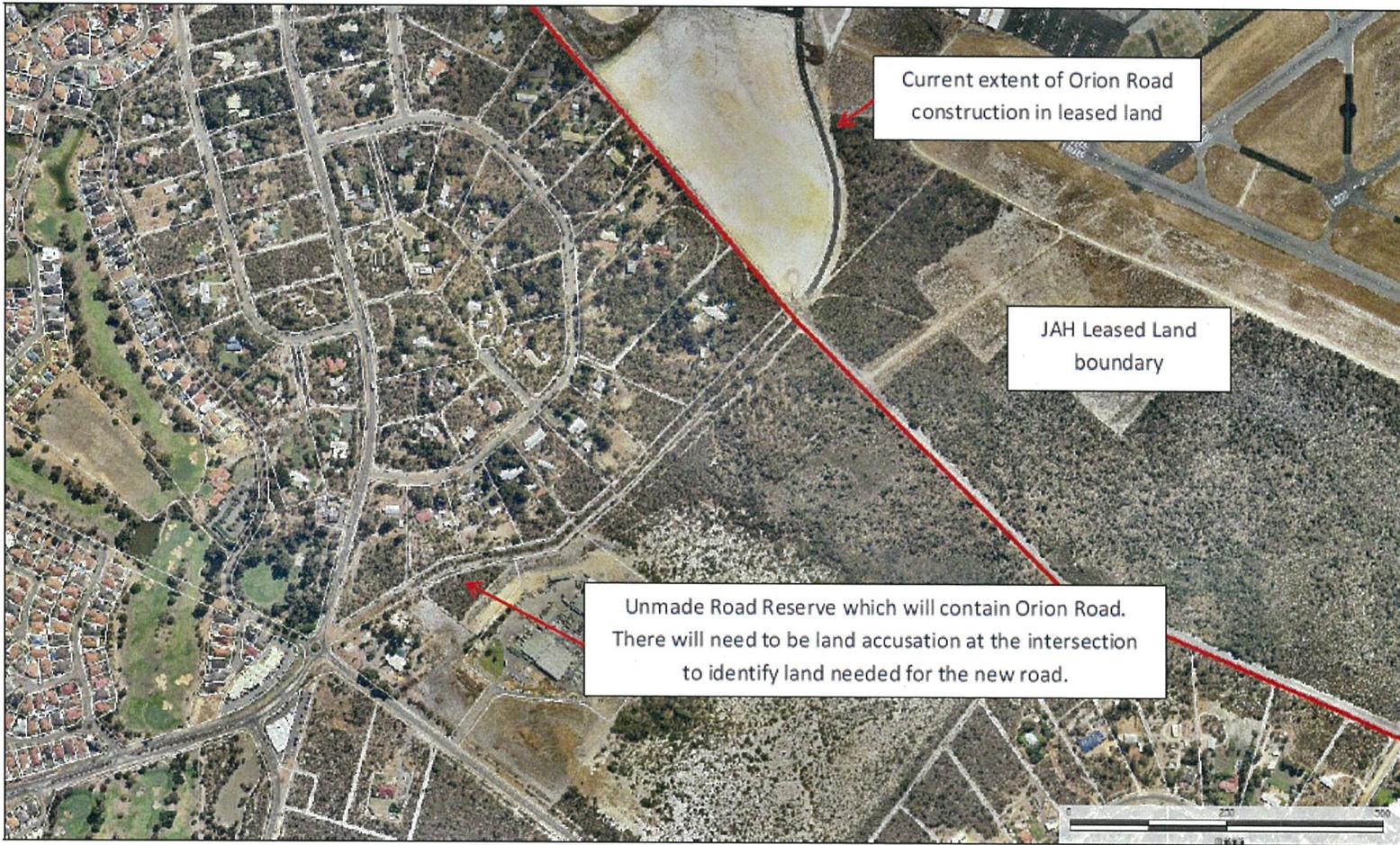
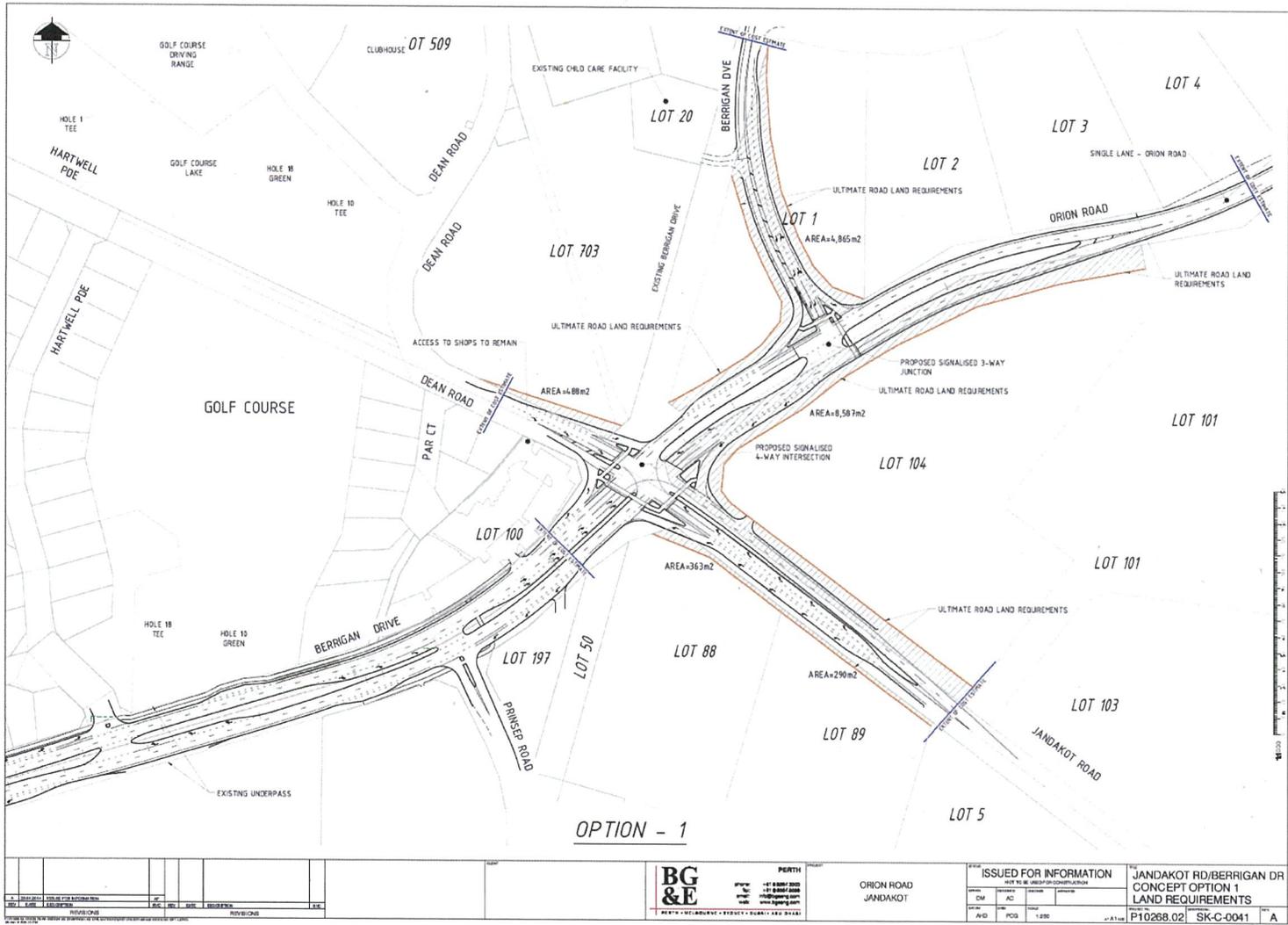
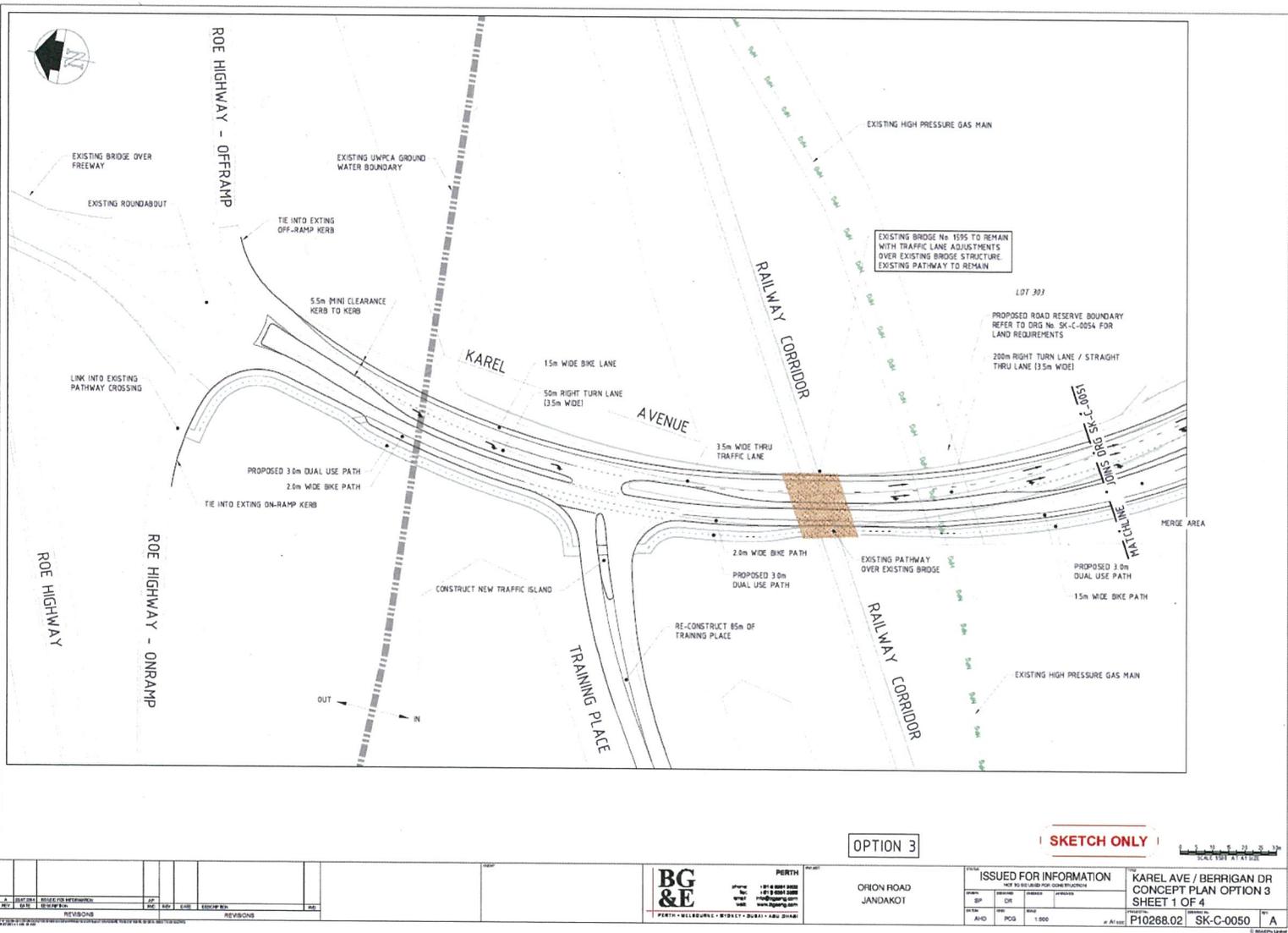


Figure 1 – Aerial Photo of unmade road (Orion Road) to JAH boundary

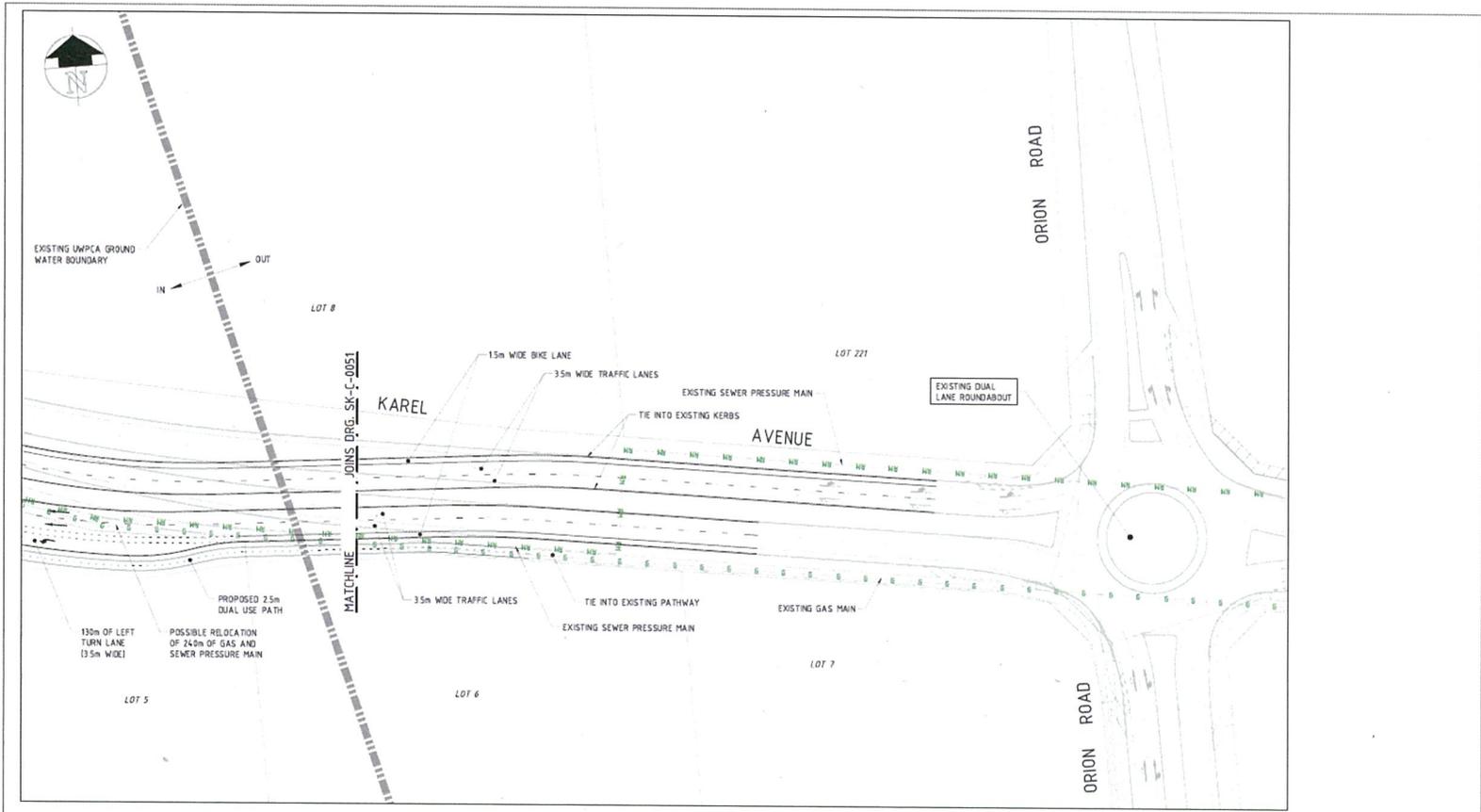
# Annexure 3 - Design Concept (Jandakot Road/Berrigan Drive (south)/Dean Road/Orion Road intersection and Berrigan Drive (north)/Orion Road intersection)



# Annexure 4 – Design Concept (Berrigan Drive (north)/Karel Avenue intersection)







LOT 29

OPTION 3

SKETCH ONLY

SCALE 1:500 AT A1 SIZE

NO.	DATE	BY	CHKD.	REVISIONS

NO.	DATE	BY	CHKD.	REVISIONS

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**BG & E**

PERTH

1100 GARDNER STREET  
PERTH WA 6000  
TEL: (08) 9447 3333  
WWW.BGANDER.COM.AU

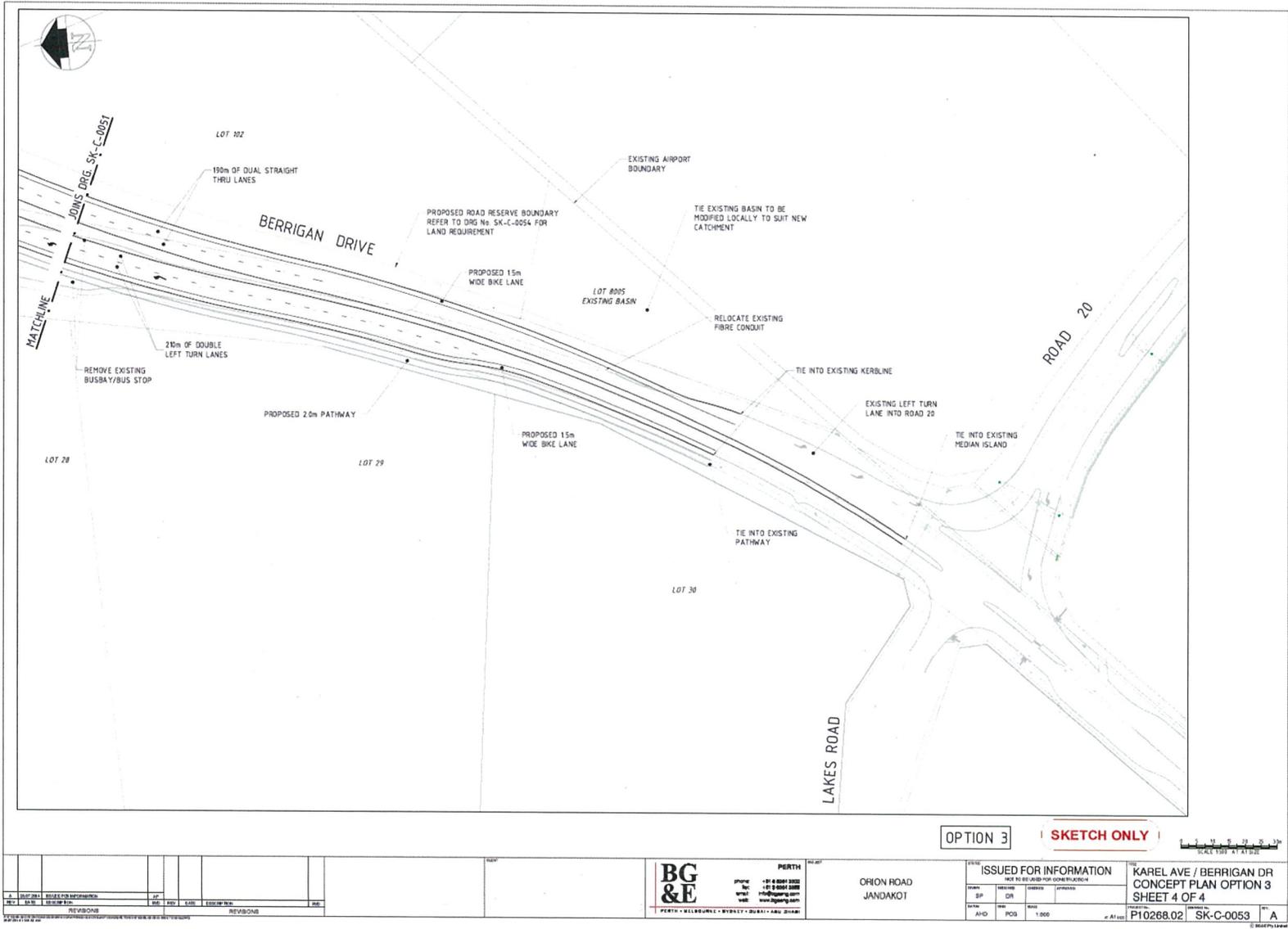
PERTH • MELBOURNE • SYDNEY • SUVA • AUCKLAND

ORION ROAD  
JANDAKOT

ISSUED FOR INFORMATION			
NOT TO BE USED FOR CONSTRUCTION			
DATE	BY	CHKD.	REVISED
DATE	BY	CHKD.	REVISED

KAREL AVE / BERRIGAN DR  
CONCEPT PLAN OPTION 3  
SHEET 3 OF 4

PROJECT NO: P10268.02  
DRAWING NO: SK-C-0052  
SCALE: A



OPTION 3 SKETCH ONLY



NO.	DATE	BY	CHKD.	REVISIONS

**BG & E**  
 PERTH  
 PHONE: +61 8 9447 3000  
 FAX: +61 8 9447 3001  
 WWW: www.bge.com.au

ORION ROAD  
 JANDAKOT

DATE	ISSUED FOR INFORMATION	SCALE

KAREL AVE / BERRIGAN DR  
 CONCEPT PLAN OPTION 3  
 SHEET 4 OF 4  
 P10268.02 SK-C-0053 A

# Annexure 5 – City’s Letter to MRWA

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Our Ref 450503, 450025  
Enquiries: J Kiurski 9411 3585

22 May 2014

Mr Albert Symcox  
Traffic Services Manager South  
P O Box 6202  
East Perth WA 6892

Dear Albert

## **Approval in Principle – Installation of Traffic Signal at Intersections of Karel Avenue/Berrigan Drive, Berrigan Drive/Jandakot Road and Berrigan Drive/Orion Road**

The City of Cockburn and Jandakot Airport Holdings (JAH) have been in discussions regarding the best possible treatment for the road network servicing Jandakot Airport and the commercial development. The matter has been complicated by the lack of regulatory control over the federal government lease area and the implications that has on broad engineering and planning responsibilities.

### **Jandakot Road / Dean Road / Orion Road**

The 2009 Masterplan proposed a new road, Orion Road (or South Link Road), which would act as a major distributor road servicing the development area. This road would link with an unmade road reserve and connect to the current network at the intersection of Jandakot/Berrigan/Dean. The initial proposal recommended a signalised intersection. Since providing its support to the Masterplan in 2010, JAH have commissioned consultants to review the proposed intersection configuration to ensure that it provided the best outcome. In all, six (6) options were considered for the intersection of Jandakot/Berrigan/Dean. From these 6 options, 2 options (the signalised intersection and roundabout) were supported for more detailed modelling and assessment. The City of Cockburn engaged Urbsol to conduct modelling of those 2 options to determine the most suitable intersection configuration. The signalised intersection was recommended and is subsequently supported by Council (the modelling report prepared by Urbsol is appended).

**On the basis of the modelling that has been undertaken, the City of Cockburn provides its support for the installation of signals at Jandakot Road / Dean Road / Orion Road as proposed.**

### **Orion Road configuration – signalising Berrigan Drive (north) / Orion Road**

The 2009 Masterplan also recommended duplication of Orion Road to cater for the traffic generation proposed from the commercial development and the regional traffic that would use the link created through the broader connections proposed to the south (Berrigan Drive – Freeway) and east (Ranford Road) of the development area. In 2011, JAH revised their traffic generation data which suggested that duplication was not necessary (at least in the short term).

The City provided support for the 2009 Masterplan on the basis of downgrading Berrigan Drive (north) to a local road. For the most part Orion Road is contained within the federal government lease area. The lack of control over Orion Road will make it difficult to ensure

that this road can reasonably provide for regional traffic movement into the future. In fact the current design and development of Orion Road would suggest that it will be constrained in the longer term. Whilst this is a little short-sighted, it highlights the concern and suggests that downgrading Berrigan Drive (north) may not be the best outcome under the current circumstances.

An outcome of retaining Berrigan Drive (north) as a DDA is the need to signalise this intersection. Urbsol has modelled this intersection in conjunction with the signals at Jandakot/Dean/Orion and has determined that signalisation of the two sites provides relative stable conditions with none of the movement delay (as compared to the roundabout option) and also improves movement on the Berrigan Drive (Kwinana Freeway - Jandakot / Dean / Orion) approach.

**On the basis of the modelling provided by Urbsol, the City of Cockburn provides its support for the installation of signals at Berrigan Drive (north) / Orion Road as proposed.**

Berrigan Drive (north) / Karel Avenue

Egress out of Jandakot Airport has been somewhat constrained for some time. Approximately 18 months ago the City and MRWA supported the establishment of temporary traffic management during the PM peak to assist in the traffic flow. Whilst this has helped to some degree, the area is still heavily congested in the afternoon periods. JAH have been proposing to signalise this intersection to improve traffic flow out of their development for some time; however, the City has not been prepared to support further development until the south link road issue had been resolved and development works committed. That remains the case. The road network layout that is now proposed and supported by the City of Cockburn will require the establishment of signals at Berrigan Drive (north) / Karel Avenue. These signals will need to be installed as part of the broader network upgrade and must not be undertaken in isolation.

**The City provides its support for the installation of signals at Berrigan Drive (north) / Orion Road as proposed on the basis that they are installed as part of the broader network upgrade detailed above and they shall not be undertaken in isolation.**

The City of Cockburn will enter into a legal agreement with Jandakot Airport Holdings which will stipulate the construction and timing of the network upgrades as detailed in this letter prior to any works commencing.

Please feel free to contact the City should you require any further clarification.

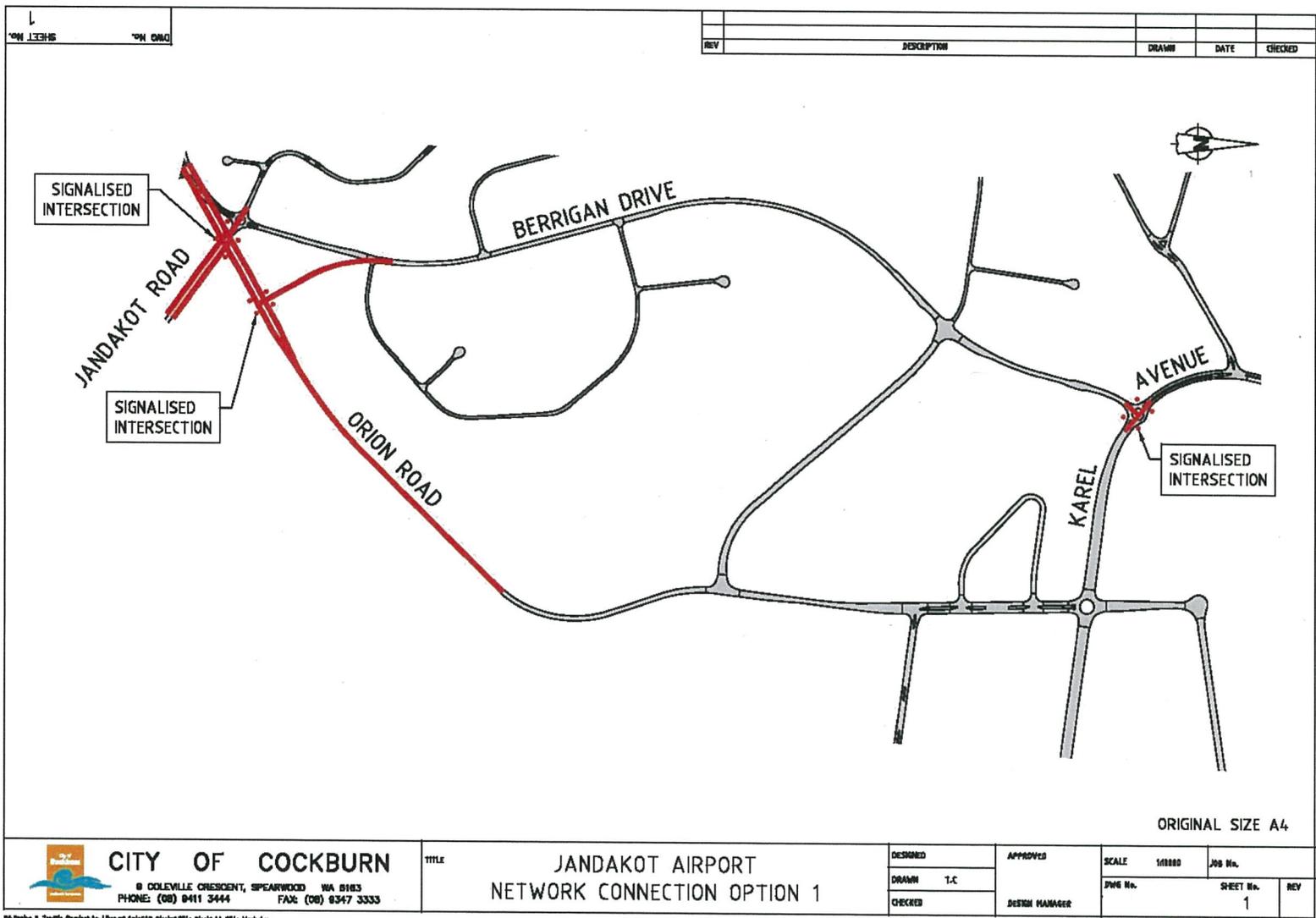
Yours sincerely



Michael Littleton  
Director Engineering & Works

Encl.

Copy: Mr Ian Edwards and  
Mr John Fraser



# Annexure 6 – MRWA's Letter to City

23



ABN: 50 860 676 021

Enquiries: Mark Ellis on 9323 4221  
Our Ref: 05/70, /3546 (D14#501218)  
Your Ref: 450503 & 450025

15 September 2014

Chief Executive Officer  
City of Cockburn  
P.O Box 1215  
BIBRA LAKE WA 6965

ATTENTION: Michael Littleton

Dear Michael

**JANDAKOT ROAD NETWORK OPTIONS – TRAFFIC SIGNALS AT JANDAKOT ROAD, ORION ROAD AND DEAN ROAD INTERSECTION AND BERRIGAN DRIVE AND KAREL AVENUE INTERSECTION**

Thank you for attending the meeting at Main Roads on Monday, 8 September 2014 to discuss the Jandakot Airport Road Network Operations.

Main Roads has reviewed the options presented to us and is willing to support road network option 1, with a minor modification.

In detail Main Roads gives 'Agreement in principle' to traffic signals at the Jandakot Road, Orion Road and Dean Road intersection and at the Karel Road and Berrigan Drive intersection. In regards to the Berrigan Drive and Orion Road intersection treatment, Main Roads only supports regulatory sign control in the short term. However, we are willing to reconsider the Berrigan Drive and Orion Road intersection treatment option subject to a review of the performance of the existing treatment which needs to occur no earlier than six months after the completion of the Kwinana Freeway widening project.

Please note this agreement is conditional and allows Council to further develop the project to final design. The agreement does not guarantee the installation of traffic signals will proceed without;

- 1) Council demonstrating all necessary conditions can be satisfied. These conditions include but are not necessarily limited to;
  - Determine the preferred system of traffic signal operation.
  - Design the road layout indicating lane configurations for the preferred system of signal operation.
  - Main Roads approval of the design of the road layout, traffic signal phasings and layout and signing and pavement marking layout.
  - Roadway Lighting;
    - o Extent of the lighting shall be recommended in AS/NZS 1158 Part 1.1 (2005) Section 3.4 plus at least an additional two spans of road section lighting on each approach.
    - o Level of the lighting shall be recommended in AS/NZS 1158 Part 1.1 (2005) Section 3.4 for the intersection and AS/NZS 1158 (2005) "V" subcategory appropriate for the approaches.

Don Aitken Centre, Waterloo Crescent, East Perth or PO Box 6202 EAST PERTH Western Australia 6892  
Telephone: (08) 9323 4111 Facsimile: (08) 9323 4174 TTY: (08) 9428 2230  
Email: roadtraff@mainroads.wa.gov.au Website: www.mainroads.wa.gov.au  
TRIM D06#138922

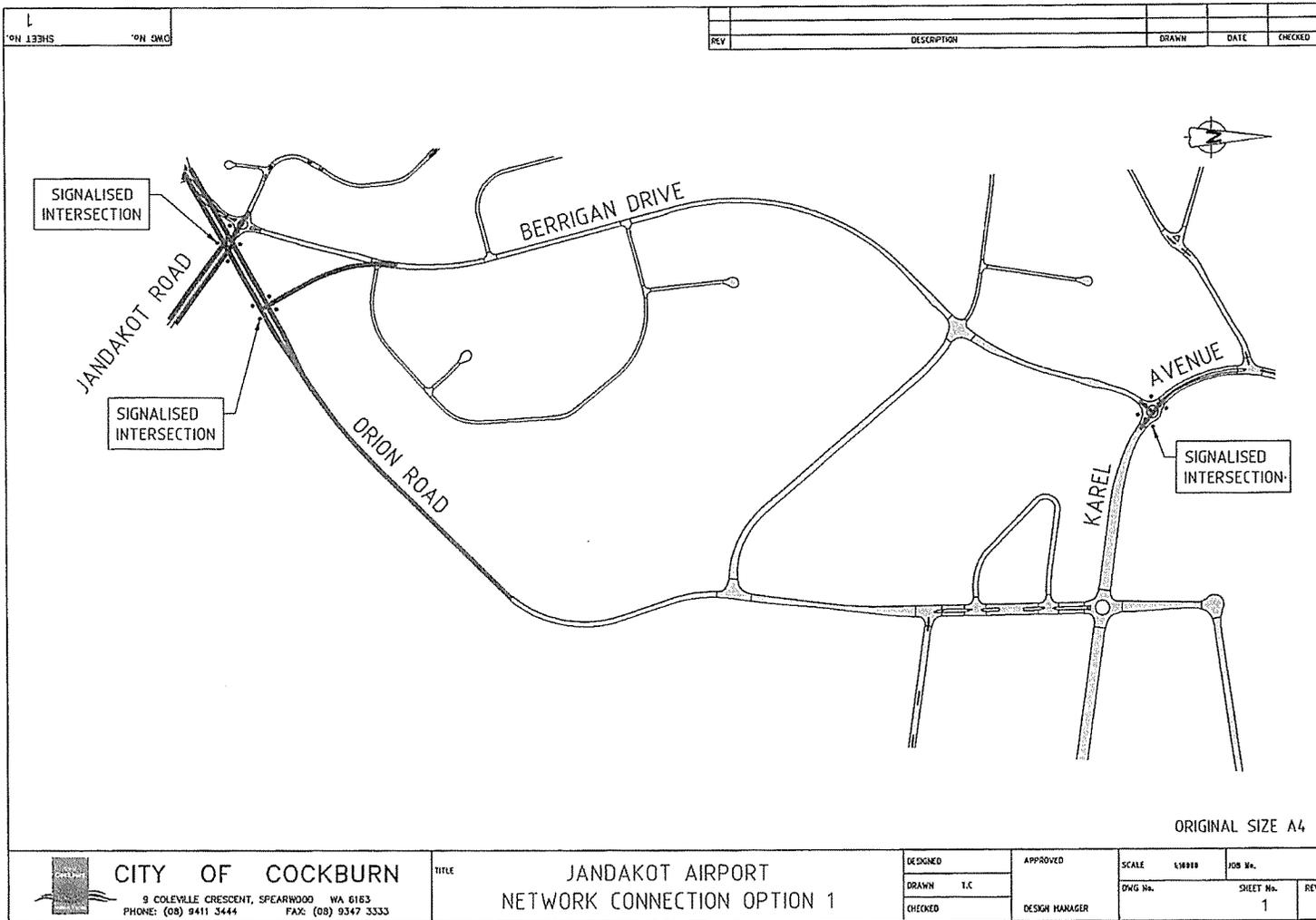
- Pedestrian facilities to the current access and mobility standards to be provided at the intersections and included in the new design.
  - Closed Circuit Television (CCTV) to monitor operation of the signals to be provided.
  - Provision of the specifications and software documentation
  - The entire project to be fully funded by City of Cockburn or Jandakot Airport Holdings.
  - Any other supporting data as necessary.
- 2) Council obtaining formal approval from Main Roads to the installation of the traffic signals. Given formal approval being granted, Council will be required to;
- Arrange the installation of the traffic signals, including its software, in accordance with Main Roads requirements.
  - Following installation, arrange handover of Main Roads Traffic Signal Infrastructure to the relevant Main Roads Contractor and forward the associated completed documentation.

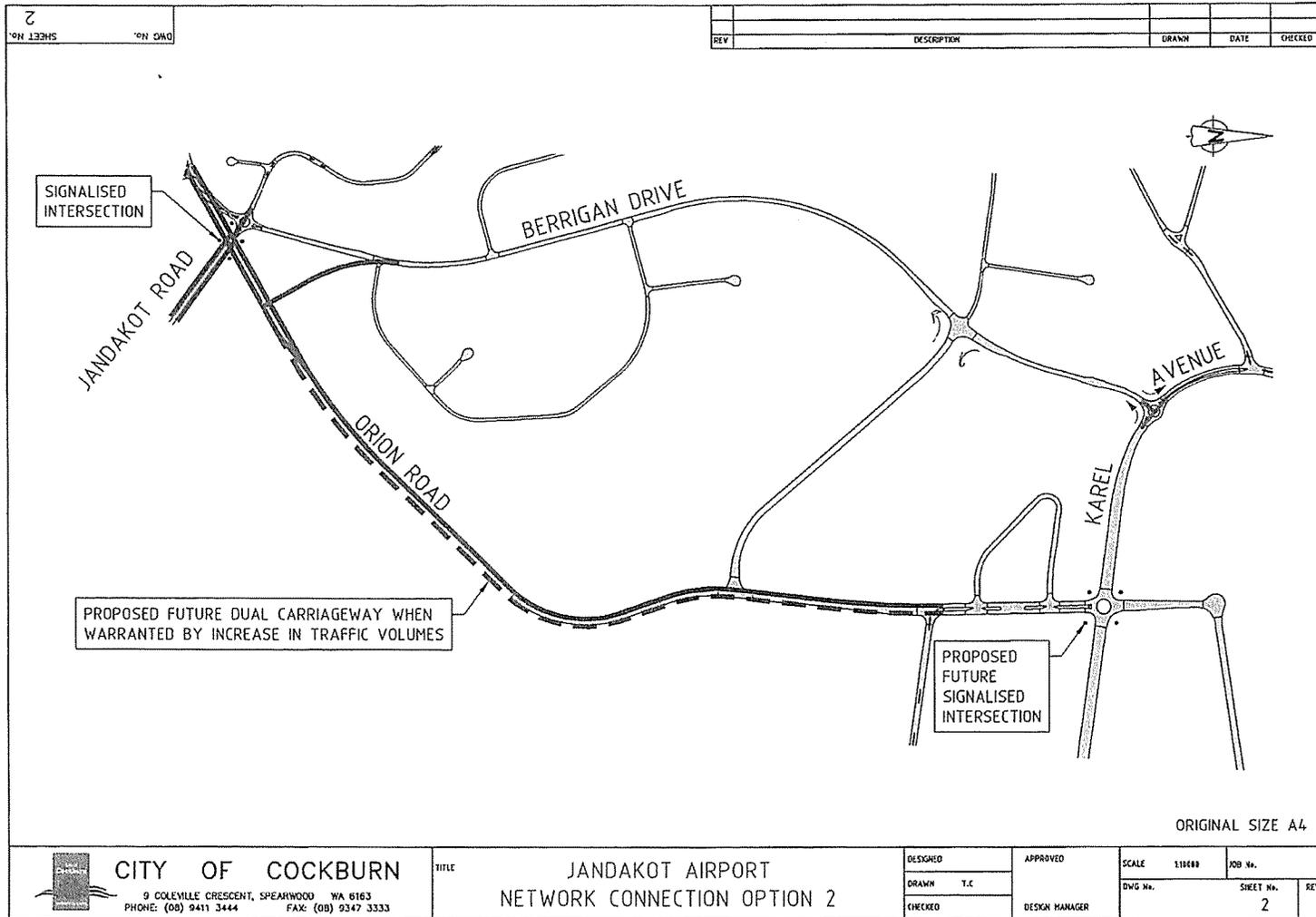
I trust that this is satisfactory however if you require any further information please contact Albert Symcox on 9323 4586. In reply please quote file reference 05/70 (D14#501218).

Yours sincerely



Gary Manning  
Acting Manager Traffic Management Services





2  
SHEET No. DWG No.

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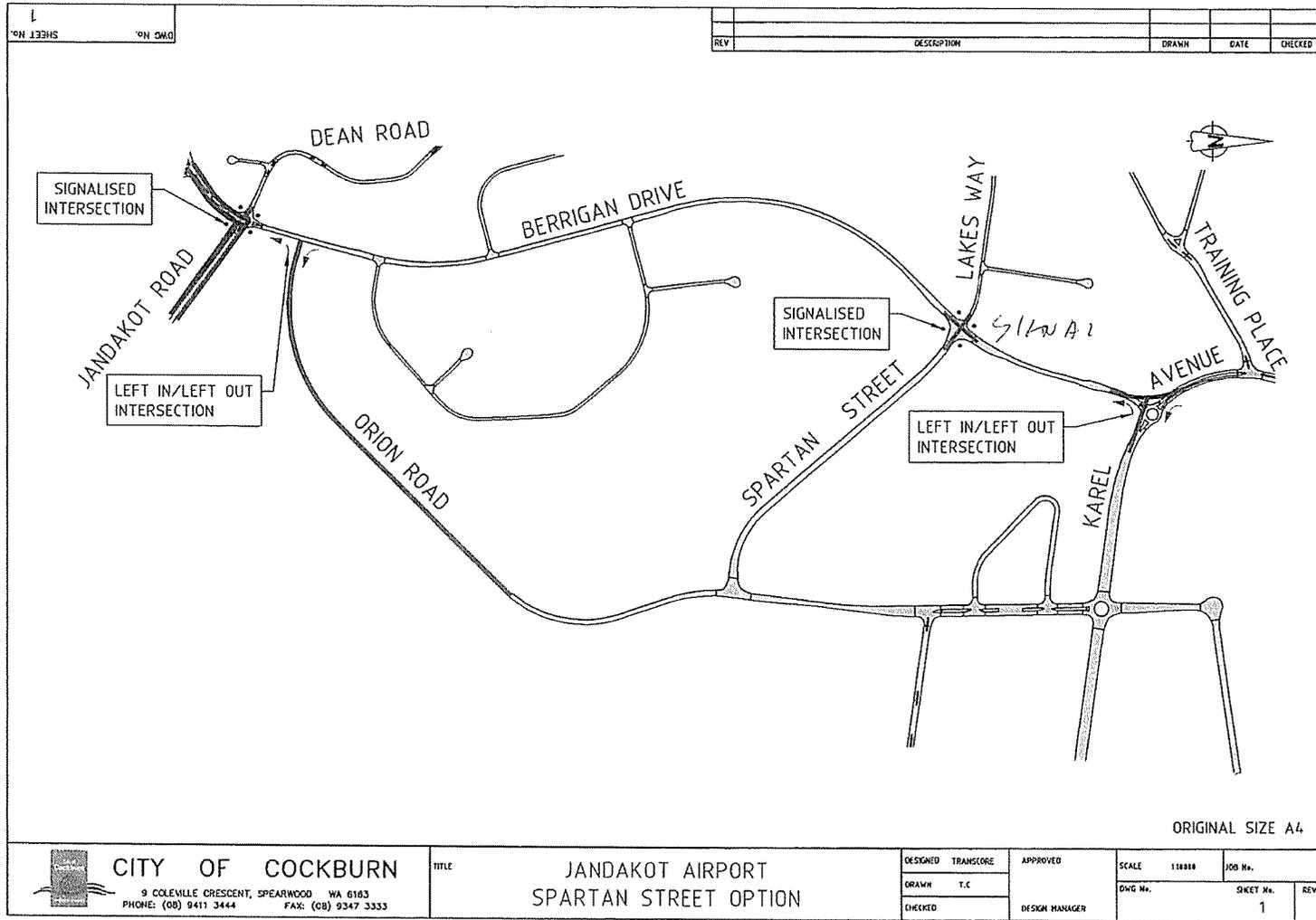
ORIGINAL SIZE A4

**CITY OF COCKBURN**  
 9 COLEVILLE CRESCENT, SPEARWOOD WA 6163  
 PHONE: (08) 9411 3444 FAX: (08) 9347 3333

TITLE  
**JANDAKOT AIRPORT  
 NETWORK CONNECTION OPTION 2**

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DRAWN T.C.	DESIGN MANAGER	DWG No.	SHEET No. 2
CHECKED			REV

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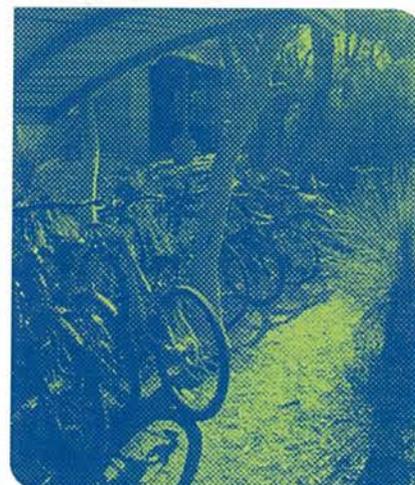
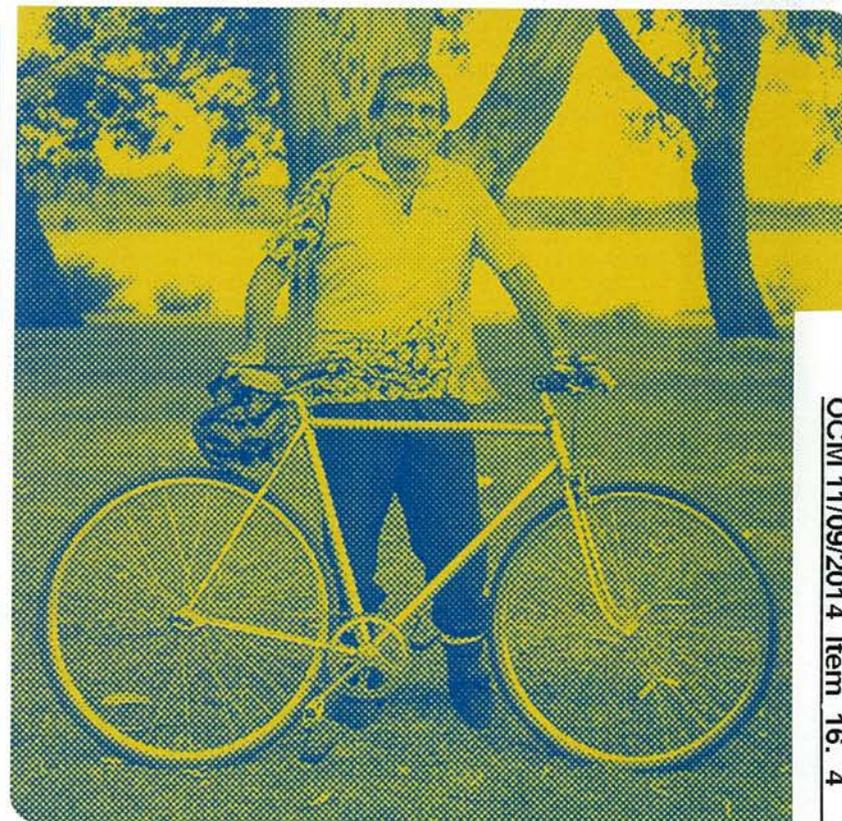
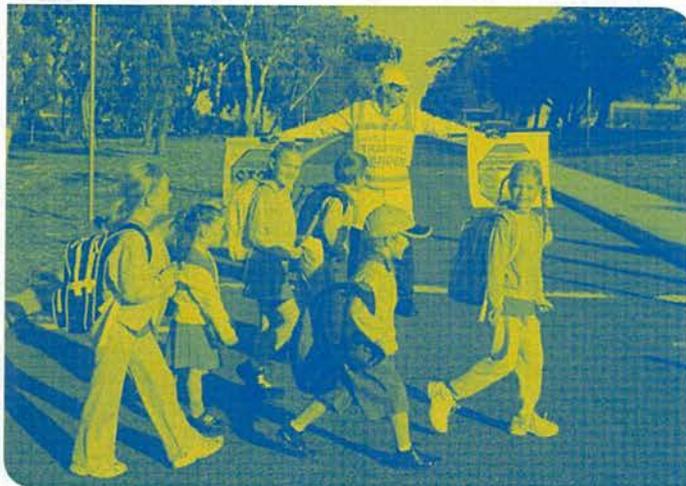
<p><b>CITY OF COCKBURN</b>          9 COLEVILLE CRESCENT, SPEARWOOD WA 6183          PHONE: (08) 9411 3444 FAX: (08) 9347 3333</p>	TITLE <b>JANDAKOT AIRPORT          SPARTAN STREET OPTION</b>	DESIGNED TRANSOCRE	APPROVED	SCALE 1:10000	DWG No.	100 No.	
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# A TRAVELSMART PLAN

## 2014 – 2017



## Overview

The City of Cockburn has employed a TravelSmart Officer to deliver the City's TravelSmart Program since it secured initial seed funding in 2009 from the Department of Planning and Infrastructure. The program has seen much success and has primarily focused its efforts on encouraging staff and community members to opt for active transport for commuting purposes, leaving private vehicles at home. The actions written into the initial planning documents for the City entitled 'TravelSmart Work Plan' and 'TravelSmart Workplace Plan' have been achieved. This new plan will provide direction for the TravelSmart Program in the City over the next three years.

As 'Your Move' concludes its implementation phase with nearly 10,000 households engaged in setting personal goals to be more active by opting for active transport modes and being more physically active. The feedback from participants and success of this individualized marketing program will be gauge via an evaluation that will be completed between the end of May to August 2014. In the meantime, the important task of sustaining support for engaged households will include succession planning for managing the Your Move website and associated social media pages, print resources and also providing a suitable point of contact at the City for locals interested in events and initiatives around these themes. This will rest with the City's TravelSmart Officer (TSO), Health Promotion Officer and Club Development Officer.

Primary Schools are an important focus for active travel initiatives. 2013 saw 15 of Cockburn's 26 primary schools engage with the TravelSmart to School program. 10 of these schools expressed an interest in applying for grant money as part of the first round of Connecting Schools Grants funded by both the City and the Department of Transport. Grant submissions for all ten were compiled by the City's TSO and approved seeing \$70k funding awarded to improve existing or provide new cycling infrastructure at these schools. The end of year results saw eight schools return survey data that showed a 6.3% increase in active travel trips and a 3% reduction in car trips. Phoenix Primary School reported a 32.4% increase in active travel trips.

## Traffic – A hot issue

"Traffic" was identified by the local community and local businesses as the highest priority issue in both the 2012 and 2013 Community Perceptions Surveys produced by Catalyse. The most recent report advises that the City needs to address the communities' dissatisfaction around traffic issues around hot spots such as Gateway Shopping Centre and the Cockburn train station by 'investing in better information, improved communications and service delivery or new infrastructure'.

The City's Strategic Community Plan 2012-22, Section 6 entitled 'Moving Around' requires the City should look to 'develop road, pedestrian and cycle way networks to facilitate the safe movement of people and goods' and 'advocate for improvements to the public transport system'. As a growing

City, our population is set to grow from 91,000 residents in 2011 to approximately 116,000 residents in 2021, so how Cockburn residents and visitors travel around is an important factor that will impact on the local transport network, household budgets, individual health and wellbeing and our natural environment.

### **TravelSmart Program in City's Engineering Directorate**

With a focus on behavior change programs to alleviate the issues of traffic and congestion, the City's TravelSmart Officer is now part of the Road Planning and Transport team in the Engineering Directorate. This will benefit of the role of the TSO to see it expanding community education and promotion via media and liaison activities to include transport issues like road safety, parking and congestion.

The TSO would also be instrumental in monitoring and coordinating delivery of new infrastructure by the City for pedestrians, cyclists and public transport users including the City's bus shelter/stop program, monitoring the implementation of our bicycle and footpath and trails program, input into future mountain biking facilities and Perth Bicycle Network Grants projects.

The City's Road Safety and TravelSmart Reference Group was created at the end of 2013 and is a medium to communicate with external stakeholders and Counselors about current initiatives and priority issues. The TSO will coordinate the operation of this reference group.

### **A Car Centric City**

Perth has an unenviable label of one of the most car dependent cities in the world, with around 80% of all trips undertaken by car. 400,000 trips are made in WA every day that are less than 1km; an opportunity for a 10 minute walk. In contrast, non-motorized forms of transport not only offer the potential to increase physical activity – 40% of the Western Australian adult population are not active enough for good health – but also reduce environmental harm - active communities benefit in terms of a raft of economic, environmental, health and social outcomes<sup>1</sup>. Health statistics are particularly poor in the City of Cockburn as compare to the State average; this is highlighted by 74% of adults are either overweight or obese.

### **Costs – Car Vs. Public Transport**

The cost involved with running the family car includes depreciation – that totals around 40 – 50% of total cost, fuel, car loan interest charges and repayments, insurance and registration. RAC WA look at the costs for different size vehicles and for a medium size car they state a medium sized family car can cost between \$183/week up to \$211/week to run, a family wagon is considerably more ranging from \$228/week to \$298/week.

This can be compared to the cost of active travel modes – cycling incurs an initial set up cost depending on the individual whereas public transport has an ongoing cost. If you are travelling across two zones in Perth for your work day commute you can expect to pay the following (please note, columns with weekly cost are calculated for 7 days not 5), the table below demonstrates that public transport is an affordable option in Perth:

	Standard adult fare/day	Standard adult fare/week	Concession adult fare/day	Concession adult fare/week
<b>Smartriders with autoload</b> (25% discount on full price fares)	\$6	\$42	\$2.40	\$16.80
<b>Tickets paid with cash</b>	\$8	\$56	\$3.20	\$22.40

## Barriers to Active Travel

Lack of time is consistently reported as a major constraint on participation of physical activity – therefore building physical activity into routines of daily life makes regular physical activity achievable for people who don't have the time, interest or money to participate in sport or fitness programs. Behaviour change programs that encourage adults to opt for active transport such as walking, cycling or combining these modes with public transport (trip chaining) for short trips are ideal for accumulating 30 minutes of moderate physical activity on most days<sup>2</sup>.

## Cycling

Commuter cycling has been shown to improve physical fitness particularly with low initial physical fitness and reduce overweight and obesity<sup>2</sup>. Cyclists take up 20 times less room and pedestrians 75 times less space than a car with a single occupant. Australians underestimate the cost of car travel by about half and overestimate travel speed – making these factors barriers to changing behavior to opt for active travel.

Examining barriers and benefits around active travel vs. travel by car is important and evidence based research shows the benefits of supporting behavior change programs that aim at increasing active transport and decreasing private vehicle use include policies that restrict car use, provision of suitable traffic calming treatments on roads, provision of active transport infrastructure, bicycle loan programs, increasing access to safe bicycle parking at train stations, skills training and provision of one off event days. The City's TravelSmart Program looks to provide these types of initiatives to the City's staff, local primary schools students, local businesses and householders.

The Australian Bicycle Council National Cycling Strategy 2011-16 aims to double the number of people cycling in Australia by 2016 via 6 priority areas that include cycling promotion for both transport and recreation with an emphasis on women and children and provision of end of trip facilities, provision of connected networks, integrated planning and safety for riders.

The Strategy boasts that bike sales have exceeded car sales for the 10<sup>th</sup> consecutive year.

This strategy is a guiding document for the Department of Transport Draft WA Bicycle Network Plan that was released in 2013 for comment. The City provided comment on the draft plan on issues pertaining to improving our current network for cyclists and pedestrians as per our current City of Cockburn Bicycle Plan 2010 and inclusion of new behavior change programs that are planned to be piloted by the Department of Transport (Connecting Schools & Connecting Stations programs). The WA Bicycle Plan 2014-31 was officially launched by the Minister for Transport in March 2014. This overarching plan aims to make WA a place where cycling is safe, connected and a convenient and widely accepted form of transport.

The City's TravelSmart Program participates biannually in the Super Tuesday commuter bike count which indicated in 2011 what is visible on Cockburn roads, an increased number of people commuting by bike. 2013 saw the coming together of like minded locals to form a new Cockburn Bicycle User Group; the BUG is looking to promote a safe and healthy cycling environment, network and communicate with like-minded organizations, sporting and community groups, be a local advocacy group to provide feedback to the City of Cockburn, Main Roads and developers about the condition of cycle roads and paths within the locality and promote regular recreational rides for community members within the City of Cockburn, the BUG will promote cycling and cycling infrastructure.

## Public Transport

Department of Transport's "Public Transport for Perth in 2031" reports that in the past 10 years public transport use in Perth has increased by 67% or three times the rate of population growth over the same period. Both the current government and the opposition committed to the provision of a new Aubin Grove train station for the City, a 2,000 bay car park, purchase of an additional two three railcar sets – an \$80million commitment that will benefit residents of the City of Cockburn. This new station about 3km south of Cockburn Central will be the biggest park and ride on the rail network and cater for the growth in population in Aubin Grove, Atwell and Hammond Park.

The Department of Transport report has four imperatives for action, initiatives that encourage the use of public transport that are a part of the TravelSmart Program can be aligned with Action #4 that states 'Transport contributes 14% to Australia's total greenhouse gas emissions. Of this, 90% is generated by private vehicles. Encouraging a greater uptake of public transport by making a high quality transport system available to more people will make a significant contribution to the reduction in greenhouse gases'. It also goes on to state that 'public transport commuters will on average,

accumulate seven times more incidental exercise than private motorists and that each additional hour spent behind the wheel of a car increases the likelihood of obesity by 6%.’

## Walking for the daily commute

Active Living for All – A framework for physical activity in WA 2012-16 strongly features walking as part of the key messages to increase the amount of PA for adults and families such as “walk for active transport where possible, find out about local walking groups, organize a walk with family and friends, take walk breaks throughout the day, walk to school or a friend’s house and enjoy a family bushwalk”.

Walking is beneficial for health with the evidence based research supporting this. ‘Active Transport: Adults’ reports “analysis of 18 studies on walking reported risk reductions of 31% for cardiovascular disease and 32% for all cause mortality”. Walking is a healthy mode of travel that contributes to sustainability, social connectedness and improved community livability.

With West Australians making 400,000 trips of less than 1km in their cars, or the equivalent of about a 1km journey each day, there is much opportunity for encouraging short trips to be completed on foot. A 20 minute walk or about 2kms is achievable and attractive when locations ‘with convivial landscapes; good access to public transport and a wide range of nearby destinations such as shops, schools, workplaces, recreational activities and services like a library or post office’<sup>3</sup>.

Barriers to walking, similar to cycling and using transport, are a lack of a convenient and connected route, a perceived lack of personal safety and comfort and a lack of awareness of options and misconceptions about the ease of walking instead.

## Key Themes

The TravelSmart Plan 2014-17 key themes are:

### **1. Events and competitions**

The City's TravelSmart Program will continue to promote active travel initiatives via a structured program that will include:

- taking part in whole of community events e.g. Spring Fair, Coogee Beach Festival
- providing stand-alone TravelSmart events e.g. Bikeweek events and events for riders throughout the year such as free bike maintenance and bike education sessions for adults and community bike rides, seniors and children, Walk Over October events, World Car Free day events, TravelSmart Trek as a joint initiative with the City of Fremantle each October
- offering support for one off event days at local primary schools who are engaged with the TravelSmart to School program and local businesses who are engaged with the TravelSmart Workplace program,
- provision of community competitions,
- support and encouragement for the City's staff to take part in Corporate Challenges that promote physical activity such as Bike Hike For Asthma, Walk Over October and Corporate Triathalons,
- celebrating our successes and promotion of our achievements via all streams of media, and
- advocating for and supporting Cockburn BUG meetings and community bike rides and events.

### **2. Infrastructure**

The City will continue to build and improve our connected network for all modes of transport, including public transport and non motorized means such as cycling, walking and scooting by providing suitable paths, bike racks, bike maintenance stations, way finding signage and end of trip facilities throughout Cockburn. We will review the action list in the City's Bike Plan annually and apply for Perth Bicycle Network grant funding from the Department of Transport each year.

### 3. Plan

The TravelSmart Officer will prepare an active transport policy and provide input into local strategic plans and operational documents produced across all service areas at the City of Cockburn. The City's TravelSmart Officer will also be a key stakeholder providing input into an 'Integrated Transport Strategy' as championed by Engineering Services.

Active transport information will be an integral part of the City's Trails Master Plan to ensure that physical activity and TravelSmart is a feature in the interpretation messages provided for users of our trails.

### 4. Change & Deliver

The City's TravelSmart Program will look to change our residents and visitor's behavior to opt for active travel options more often by delivering tools and programs to community, businesses, school and our staff such as:

- TravelSmart Workplace.
- TravelSmart to Schools – provision of assistance engaging local primary schools, provide financial assistance for small grants to fund breakfast events and also fund Connecting Schools Grants to improve cycling infrastructure in Cockburn schools.
- Your Move – This program has taken shape in the format of a partnership between Department of Transport and Department of Sport and Recreation and the City of Cockburn. The key objectives of the program match that of Health Services TravelSmart Program and Recreation Services Youth Active Program now known as 'Kids Sport Initiative' and the 'Sport and Recreation Grants' and will target reducing car use in place of increasing use of public transport, increasing walking and cycling trips and also increasing the amount of minutes of physical activity undertaken each day by Cockburn residents.

This program is has been a pilot program and has been an exciting opportunity for the City. The program saw an investment of \$4million of cash and in kind resources with program development commencing in early 2013 and has engaged 9,000 households from June 2013 to May 2014. Your Move will move into an evaluation phase from May-August 2014.

The City's TravelSmart Officer has worked with the teams from state government to enable smooth delivery of the project throughout program development, delivery and then managing the continuation of many elements of Your Move such as the electronic and paper resources and as a contact for local households at the end of the implementation phase.

- Cycle 100, Bike Rescue Projects, participation in the Super Tuesday commuter count, cycling education and bike maintenance courses and walking groups.
- Provision of feedback to the Public Transport Authority on local issues around our bus and train services and facilities as identified by our local community.
- Data will be collected to evaluate these program/initiatives to monitor behaviour change in our community and engaged workplaces and schools.

## Targets

Targets for this Plan from July 2014 to June 2017 are:

### **Whole of Community:**

1. To decrease the number of private vehicle trips made in the City of Cockburn by 10% by :
  - a. increasing the number of walking trips made in the City of Cockburn by 10%,
  - b. increasing the number of cycling trips made in the City of Cockburn by 10% and
  - c. increasing the number of public transport trips made in the City of Cockburn by 10%.

### **Community Sector Targets:**

#### **Workplace Program**

2. To reduce the number of single occupancy private vehicle trips made by staff by 10% at local businesses that takes part in the TravelSmart Workplace Program.

#### **Schools Program**

3. To increase by 10% the number of primary school children who walk, cycle, scoot or take public transport to school where their school is involved with City's TravelSmart to School program.

#### **Community Recall**

4. Improve the recall of TravelSmart messages among our local community by 20% from 2013 levels by 2017.

It is important to note that these targets will assist in achieving the National Partnership Agreement on Preventative Health. The NPAPH provides \$932.7 million over nine years from 2009-10. This is a significant investment by the Australian Government in health prevention. It builds on the COAG Australian Better Health Initiative and the National Reform Agenda's Type II Diabetes Initiative, and supplements the National Health Care Agreement.

The NPAPH aims to address the rising prevalence of lifestyle related chronic disease by laying the foundations for healthy behaviours in the daily lives of Australians through settings such as communities, early childhood education and care environments, schools and workplaces, supported by national social marketing campaigns (*MeasureUp* and an anti-smoking campaign).

## **Recommendations**

- 1. Council to endorse the TravelSmart Plan 2014-17 as the direction of the TravelSmart program for the same timeframe.***
- 2. Council to approve the continuation of the TravelSmart Officer position as a full time contract position to July 2017 to coordinate the TravelSmart Program.***
- 3. Council to continue funding of the TravelSmart program as per the budget included as part of this Plan.***

## Stakeholders

- City of Cockburn elected members and all staff working across all service units
- Residents of the City and visitors to the City
- Local businesses
- Local primary schools
- Local high schools
- Our neighbouring local government authorities
- State government agencies including Department of Transport, Department of Sport and Recreation, Department of Environment & Conservation
- Department of Health, South Metropolitan Public Health Unit, Main Roads, Public Transport Authority, West Cycle
- Heart Foundation
- Non government organizations including the Cockburn BUG, Dismantle Fremantle, Aust Cycle, Bicycling WA, Cycling WA.

## Action Plan

### Events and competitions

#### Objectives:

- To raise awareness of TravelSmart messages in local community.
- To consolidate the TravelSmart messages and implement programs that facilitate long term travel behavior change among staff who work for the City of Cockburn.

Action	Performance Indicator	Who	When	Approximate annual cost	Priority
Attend a stall at the City's Spring Fair to provide information and educate to the public on the TS Program.	Number of materials distributed. Number of attendees at the stall	TSO	Annually October	\$2,000	High
Attend a stall at the City's Coogee Beach Festival to provide information and educate the public on the TS Program	Number of materials distributed. Number of attendees at the stall	TSO	Annually April	\$2,000	High
Promote Bikeweek and provide a Bikeweek Event for staff and local commuter cyclists to promote cycling and the TS Program	Surveys completed by attendees Number of attendees	TSO	Annually March	\$2500 DoT Part sponsored	High
Promote Walk Over October, RAC Bike Hike for Asthma, Corporate Triathlon Event and assist staff to register corporate teams and	Number of events Number of attendees	TSO	Annually October	\$2500	High

provide information on the TS Program.					
Organise, in partnership with the City of Fremantle, the Cockburn Fremantle TravelSmart Trek to promote walking and TS program.	Number of materials distributed Number of attendees	TSO EO City of Fremantle Staff	October	\$2500	<b>High</b>
Advocate for and promote the Cockburn Bicycle User Group (BUG) and assist in the set up and promotion of BUG cycling events.	Number of attendees Number of materials Number of adverts, paper and electronic	TSO Cockburn BUG	Bi Annually November April	\$1000	<b>Medium</b>
Offer support, financial and merchandise/prizes to local primary schools and businesses providing one off events for active travel related campaigns.	Number of events Number of attendees Pre and post data on students and staff who change behavior to adopt active travel modes Number of materials distributed	TSO PS Bus	As required All year round	\$13500	<b>High</b>
Promote TS competitions and provide prizes in association with TS initiatives.	Number of adverts, paper and electronic Number of competition entries	TSO Communications	As required	\$2000	<b>High</b>

<p>Promote TS information on programs we provide and highlight our success stories via local newspapers, the City's website and social media, Be Active website, Your Move website and social media, printed and electronic newsletters, posters and broadcast emails.</p>	<p>Number of adverts</p> <p>Number of stories in the local newspapers, newsletters.</p> <p>Number of articles posted via electronic media</p>	<p>TSO</p> <p>Communications</p>	<p>As required</p>	<p>No cost</p>	<p><b>High</b></p>
<p>Provide information to staff about our TS Program as part of the "Welcome Pack" distributed to all new Council employees.</p>	<p>Number of welcome packs provided to new staff.</p>	<p>TSO</p> <p>HR</p>	<p>As required</p>	<p>No cost</p>	<p><b>Medium</b></p>

## Infrastructure

### Objectives:

- To plan for and provide appropriate infrastructure and enable active transport use.

Action	Performance Indicator	Who	When	Annual Cost	Priority
Quantify demand and identify gaps in PTA services in the City's industrial and commercial areas (AMC, Bibra Lake, Cockburn Central, Phoenix Park)	Number of enquiries/complaints received by the City of Cockburn, PTA.	PTA TSO	Ongoing	None	High
Monitor and coordinate the bus shelter/bus stop program	Number of bus shelters, bus stops improved, provided or replaced	PTA TSO	Ongoing	See Engineering Budget	High
Lobby the Public Transport Authority for more services to commercial business districts in the City.	Production of service demand report.  Number of new bus services.  Route of new bus services.	TSO DoT	Ongoing	None	High
Lobby the Public Transport Authority throughout the planning process around the proposed Success Train Station to ensure easy accessibility for buses and end of trip facilities for cyclists.	Number of new bus services.  Provision of end of trip facilities.	TSO DoT	Ongoing throughout planning process	None	High

Lobby the Public Transport Authority on behalf of the Cockburn community to highlight concerns about services, routes and timing of public transport that travels through the City.	Number of contacts made to the City of Cockburn by residents.	TSO	Ongoing	None	<b>High</b>
Update and distribute the City's TravelSmart East and West Guides and z fold pocket maps	New Guides produced and distributed to each household bi annually.	TSO	2014 2016 2018	\$14000 \$16000 \$18000	<b>Medium</b>
Completion of the action items listed in the City's Bike Plan via provision of PBN grant funding and Council co-contribution.	Number of action items completed each financial year.	Traffic Engineer/TSO	Ongoing	See Engineering Budget	<b>High</b>
Completion of remedial works to repair existing paths and fill in missing links where reported by network users.	Number of jobs completed each year.	Traffic Engineer/TSO	Ongoing	See Engineering Budget	<b>High</b>
Provision of a new Bike Plan – PBN grant funding to be applied for to pay for 50% of consultants costs.	Provision of a new plan.	Traffic Engineer Parks Manager DAIO TSO External consultant	2013-14	\$40,000 – Engineering Budget	<b>High</b>

Ensure provision of adequate infrastructure and EOTF at all City of Cockburn service locations	Number of bike racks installed, upgrading of shower and change room facilities.	Infrastructure Manager  Traffic Engineer/TSO	Ongoing	Infrastructure Budgets	<b>High</b>
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## Plan & Report

### Objectives:

- To ensure active transport messages are featured in new and existing local policies, guidelines and plans.
- To implement policy that ensures the use of active transport is embedded at and around our City.

Action	Performance Indicator	Who	When	Annual Cost	Priority
Provide an annual report to elected members, staff and the community on completed works as per the Bike Plan 2010.	Annual report	Traffic Engineer  TSO	June each year	No cost	<b>High</b>
Prepare an Active Transport Policy that will provide a guiding statements and outcomes of the TravelSmart Program as taken from this document.	Policy document completed	TSO  Traffic Engineer	TBC	No cost	<b>Medium</b>
Contribute to the development of an Integrated Transport Strategy with the City's Engineering Services staff and assist in the compilation of this document and implementation of actions pertaining to behavior change and active travel.	ITS completed	Traffic Engineer  TSO  Strategic Planning Manager	2014 on	No cost	<b>High</b>

Review all local strategic plans and operational documents produced across all service areas to ensure active travel initiatives/programs are present and relevant.	Report completed providing review.	TSO	TBC	No cost	<b>Medium</b>
Make active travel messages an integral part of the City's Trails Master Plan as part of the interpretative information provided to users.	Final copy of TMP.	Environmental Services Manager  TSO	2013	No cost	<b>High</b>
Work with Phoenix Park Shopping Centre & Cockburn Gateways Shopping Centre to develop a TravelSmart access plans.	Provision of finalized TS access plans.	TSO	TBC	No cost	<b>Medium</b>

## Change & Deliver

### Objectives:

- To raise the awareness of the TravelSmart message within Cockburn primary schools, local businesses, in our community and amongst our staff.
- To increase the skills and knowledge of children and young adults in Cockburn to foster life long active travel habits.
- To increase the skills and knowledge of those employed at local businesses, local residents and our City staff to enable them to make more sustainable transport decisions by delivering effective behavior change programs.

Action	Performance Indicator	Who	When	Annual Cost	Priority
Partner with Department of Transport and Department of Sport and Recreation to deliver a new version of the TravelSmart Household Program – Your Move - to residents within the City providing in kind and financial support to the program til August 2014.					
This will include offering tailored versions of:					
TravelSmart to Schools	Number of schools participating. % increase in active transport modes for school day commute.	TSO/DoT TSTS team	Ongoing		High
TravelSmart Workplace	Number of workplaces and staff participating. % increase in active transport modes for work day commute.	TSO/DoT TSW team	Ongoing		High

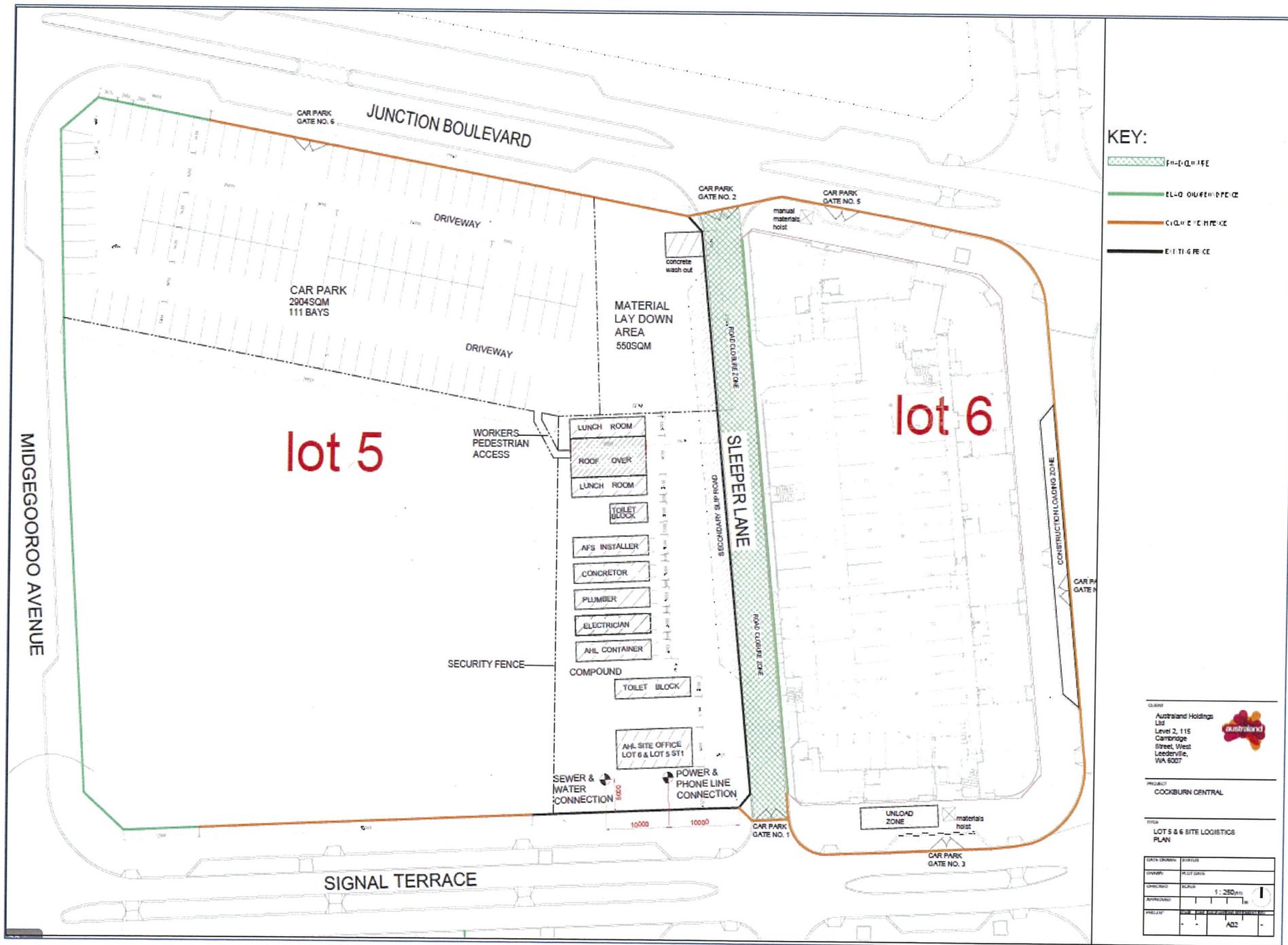
RAC Sponsored Primary School grants to improve infrastructure and way finding signage.	Number of schools, students participating.	TSO	Ongoing	Grant funded	<b>High</b>
	Number of Connecting Schools Grants approved and supported by the City and Dept of Transport Cycling and Urban Transport team.	TSO		\$10,000	
Partner with HBF to provide free PA sessions to residents in the target suburbs	Number of way finding signs provided on footpaths in Cockburn suburbs.	DoT Your Move Team/TSO	Design and install 2014.	\$40,000 in 2013-14	<b>Medium</b>
	Number of sessions delivered. Number of participants.	DSR Your Move Project Officer	Maintenance and additions ongoing.	\$2,500	
			Delivered 2013-14	Funded by HBF	<b>High</b>
Coordinate the delivery of cycling education courses for children and novice adults especially women and seniors.	Number of courses provided. Number of participants attending.	TSO	Ongoing	\$4,000	<b>High</b>

Coordinate the delivery of bike maintenance courses for 'at risk' youth through the Bike Rescue Project provided by Dismantle.	Course delivery; annually Number of participants completing course.	TSO Youth Services Manager	Annually	\$10,000	High
Coordinate the collection of data for Super Tuesday commuter bike count in the City of Cockburn.	Provision of report.	TSO	Bi-annually	\$3000	High
Coordinate and support local Heart Foundation Walking Groups.	Number of walking groups. Number of participants.	Co-Health Coordinator	Ongoing	\$1000	Medium
Coordinate the delivery of the City's internal TravelSmart Program for staff including provision of fleet bikes, corporate smartriders, staff shuttle bus, carpooling register, annual staff survey into travel patterns, events subsidies and breakfasts (see Promote and Events).	Number of bookings for fleet bikes, Smartriders. Number of passengers on shuttle bus. Savings in terms of CO2 emissions, costs to individuals in terms of running a car, time. Additional minutes of PA gained as a result of opting for active travel for the work day commute and travel during work hours.	TSO	Ongoing	\$5000	Medium

<p>Coordinate and promote the City's TravelSmart Pledge program.</p>	<p>Number of registered pledgers.</p> <p>Number of calendars distributed.</p> <p>Number of adverts promoting the initiative.</p>	<p>TSO</p>	<p>Ongoing</p>	<p>\$2000</p>	<p><b>Medium</b></p>
<p>Collect appropriate data for all programs delivered within the City to gauge behavior change around active transport messages.</p>	<p>Provision of evaluation reports.</p>	<p>TSO</p>	<p>Ongoing</p>	<p>No cost</p>	<p><b>High</b></p>

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- KEY:**
- FIN-DIAPHRAGM
  - EL-DIAPHRAGM
  - C/DIAPHRAGM
  - E/DIAPHRAGM

CLIENT  
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PROJECT  
 COCKBURN CENTRAL

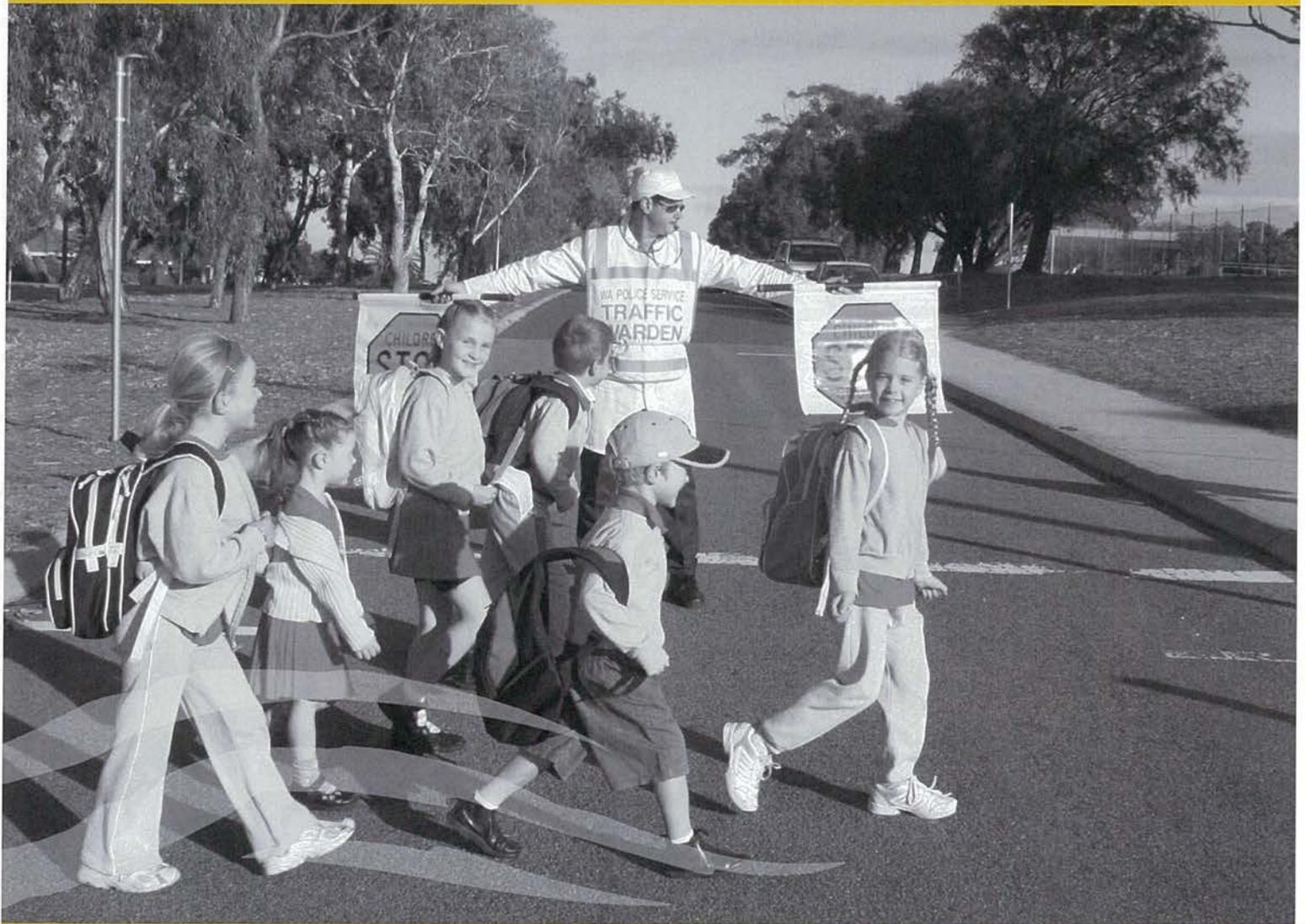
TITLE  
 LOT 5 & 6 SITE LOGISTICS PLAN

DATE DRAWN	STATUS
DRAWN	PLAT DASH
DESIGNED	SCALE 1:250 (M)
APPROVED	
PROJECT	A02



# ROAD SAFETY STRATEGY

## 2014 - 2020



Version Control

This Document is a live Council document and is subject to periodic review.

Document Control						
Version No	Date	Version Details	Author	Reviewer	Approver	
1	July 2014	Draft to Director	Engineering Services Jadrnka Kiurski	Road Design & Development	Ossie Pereira	
				Road Construction	Colin McMillan	
				Traffic & Transport	John McDonald	
2	July 2014	Final Version for Adoption	Engineering Services	Engineering & Works Director	Michael Littleton	
3	_____ 2014	Adopted by Council				

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## EXECUTIVE SUMMARY

The Western Australian government and Road Safety Council have undertaken to develop a new road safety strategy for the period 2008 to 2020 within the Safe System or Vision Zero frameworks.

In line with Towards Zero 2008-2020 the City of Cockburn will develop a Road Safety Strategy and implement the Action Plan, which creates and maintains as safe as possible road and roadside environment, embedding features that help protect road users in their use of the road network. Actions delivered will be evidence based and will address local crash data and local road safety issues.

There were 8208 crashes on Cockburn Roads during the 2009 – 2013 period; 21 fatal, 427 hospital, 1200 medical and 6500 property damage crashes. The number of total crashes (fatal, serious and other injury) in Cockburn has reduced from 1807 crashes (2012) to 1554 crashes (2013).

During the period of 2009-2013 the main road users group involved in serious and fatal crashes were drivers (90%) and pedestrians (3.5%) and most of the crashes took place in 60km/h and 70km/h speed zones.

City of Cockburn plans to use the “Safe System Approach” to reduce road trauma in the municipality. The “Safe System”, based on approaches used in Sweden’s “Vision Zero” and the Netherlands “Sustainable Safety”, uses three main themes to promote a reduction in road crashes and the incidence and severity of associated road trauma: Safer Roads and Roadsides, Safer Vehicles and Safer Road Users.

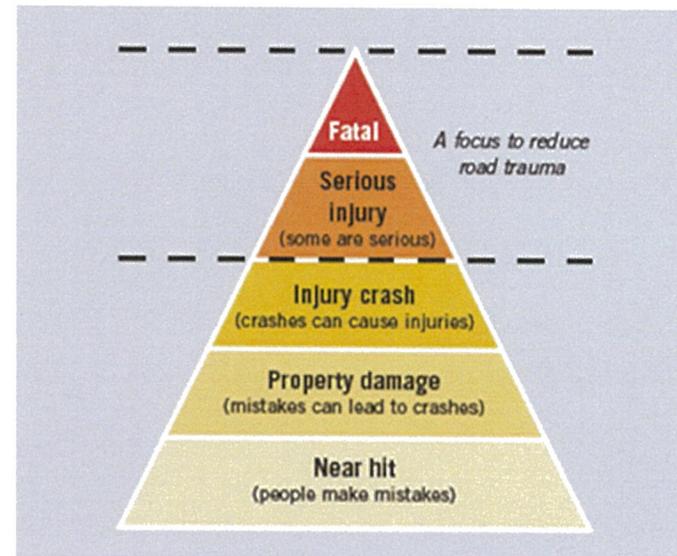
The City of Cockburn Road Safety Strategy will draw on these elements to identify gaps and areas of improvement to create a safe system for all road users, with the City taking a lead role in coordinating and providing a “Safe System Approach” for

everyone who lives, works, learns, spends leisure time or travels within or through Cockburn LGA.

The City of Cockburn aim is to reduce road fatalities in Cockburn LGA by 30% in the next 3 year period. The reduction in road crashes will be achieved through Engineering, Education and Encouragement.

In line with Road Safety Strategy 2014-2020 Council will document and implement six year action plans. Actions delivered will be evidence based and will address local crash data and local road safety issues.

Council will evaluate and review the Road Safety Policy Action Plan 2014 - June 2020 in 2017 and at the same time prepares a subsequent action plan for the period 2017 –2020.



## 1. OVERVIEW

The City of Cockburn is currently developing a new Road Safety Strategy 2014 – 2024, on behalf of key stakeholders and the local community. The Strategy is a fluid and adaptable document that will support programs that delivers actions in the 'Moving Around' Strategic Objectives of the City's Strategic Community Plan 2012 – 2022 and meets the changing road safety issues within the City of Cockburn.

The City is aware of the increasing number of vehicles on roads in general and the City in particular. The Integrated Transport Strategy that was recently adopted advanced the number of actions to reduce the number of vehicles on roads, raises the community awareness of other transport alternatives and considered the alternatives to improve safety within the road network. The City will monitor delivery of the action plan and review future road safety actions based on information obtained.

The City is also aware of the number of crashes that occur within the City and recognizes the negative and significant impact of road trauma on families and the broader community. Council will utilise the safe system framework by bringing about changes to our road safety practices and culture, to eliminate death and serious injury on our roads.

Council is fully aware of its role in supporting Federal and State road safety programs at local level. The City's Road Safety Strategy will support programs delivered by State and Federal Government and place an action plan to improve the road safety of residents of the City of Cockburn.

### 1.1 Strategic Intent

The Road Safety Strategy 2014 – 2024 supports programs and delivers actions in the 'Moving Around' Strategic Objectives of the City of Cockburn Strategic Community Plan 2012 – 2022.

In line with the Strategic Community Plan, Council is aimed at bringing on changes to our road safety practices and culture, and eliminates death and serious injury on our roads. In doing so it will:

- **Develop a safe road network system.**
- **Embed the Safe System approach into the Council and community.**
- **Provide assurance to government and community that we are achieving results.**

Given the challenge of implementing such an ambitious approach, Council determined that the strategy should be developed in a consultative and transparent way to maximize stakeholder and community acceptance.

The reduction in road crashes will be achieved through the following approaches:

- **Engineering** - keeping road and roadside infrastructure and roadside environment at a safe standard;
- **Education** - informing road users of road safety and awareness of risk; and
- **Encouragement** - encouraging the use and ownership of safe vehicles and to be a responsible road user.

## 1.2 Road Safety Vision

Road Safety Vision is about to deliver a “safe system” “which eliminates death and serious injury crashes on the road network. The safe system philosophy combines the best elements of Sweden’s “Vision Zero” and the Netherlands “Sustainable Safety” road safety philosophies.

Like its Swedish and Dutch counterparts, Australasia’s Safe System philosophy also aspires to prevent death and serious injury within its road transport systems.

### 1.2.1 National Context

The National Road Safety Strategy 2011–2020 is firmly based on Safe System principles and is framed by the guiding vision that no person should be killed or seriously injured on Australia’s roads. The strategy sets out a range of high-level directions and priority actions to drive national road safety performance to the end of 2020. These are grouped under the following four ‘cornerstone’ areas of the strategy:

- Safe roads
- Safe speeds
- Safe vehicles
- Safe people.

Figure 1 is an extract from the National Road Safety Strategy 2011–2020 presenting the specific aims and actions on each of the four cornerstone areas

<b>Safe Roads</b>	Roads and roadsides designed and maintained to reduce the risk of crashes occurring and to lessen the severity of injury if a crash does occur. Safe roads prevent unintended use through design and encourage safe behaviour by users.
<b>Safe Speeds</b>	Speed limits complementing the road environment to manage crash impact forces to within human tolerance; and all road users complying with the speed limits.
<b>Safe Vehicles</b>	Vehicles which not only lessen the likelihood of a crash and protect occupants, but also simplify the driving task and protect vulnerable users. Increasingly this will involve vehicles that communicate with roads and other vehicles, while automating protective systems when crash risk is elevated.
<b>Safe People</b>	Encourage safe, consistent and compliant behaviour through well-informed and educated road users. Licensing, education, road rules, enforcement and sanctions are all part of the Safe System.

Figure 1: National strategic intent in each of the cornerstone areas.

### 1.2.2 State Context

The Western Australia Road Safety Strategy - Towards Zero 2008-2020 is designed to change the way of thinking and provide safe road access to ensure success.

Toward Zero incorporates the Toward Safe System, which aims to improve road safety through four foundations:

- Safe Road Use
- Safe Roads and Roadsides
- Safe Speeds
- Safe Vehicles



Figure 2 is an extract from the WA Road Safety Strategy 'Towards Zero 2008-2020' presenting the specific aims and actions on each of the four cornerstone areas.

<b>Safe Roads Use</b>	Integrating behaviour change programs with improved enforcement to make them more powerful and addressing impaired driving (alcohol, drugs, fatigue and distraction), restraint use, graduated licensing and speed choice.
<b>Safe Roads and Roadsides</b>	Investing in Safe System infrastructure improvements.
<b>Safe Speeds</b>	Enhancing speed enforcement and further reflecting on the appropriateness of WA's speed limits.
<b>Safe Vehicle</b>	Promoting the uptake of safer vehicles and key safety features, particularly by government and corporate fleets.

Figure 2: State strategic intent in each of the cornerstone areas

### 1.2.3 The City of Cockburn Context

The City of Cockburn’s Road Safety Strategy is aligned with the National Road Safety Strategy and Western Australia’s Towards Zero – Road Safety Strategy, based on the “Safe System Approach”. The City of Cockburn utilises the Safe System framework to achieve its Vision Zero goals by delivering actions in three main areas:

- Safer Roads and Safe Roadsides
- Safer Vehicles
- Safer Road Users.

The City’s Road Safety Strategy will draw on these elements to identify gaps and areas of improvement to create a safe system for all road users within the City.



Figure 3 presenting the City’s specific aims and actions on each of the three cornerstone areas.

<p><b>Safer Roads and Safe Roadsides</b></p>	<p>Implementing appropriate infrastructure treatments, maintaining existing infrastructure to the required standards and introducing new initiatives.</p> <p>Supporting programs (such as installation of red light and speed cameras, enforcing of school speed limits, enforcing of speed limits in built-up areas).</p>
<p><b>Safer Vehicles</b></p>	<p>Provide targeted information about safer vehicles to a range of groups in the community.</p> <p>Provide and promote enhanced and up-to date vehicle safety features, via information on vehicle safety features and associated benefits, and information relevant to learner motorcyclists.</p>
<p><b>Safe Road Users</b></p>	<p>Engage all road users through targeted communication and tailored education and engineering programs.</p>

Figure 3: The City of Cockburn road safety strategic intent

### 1.3 Strategic Context of the City of Cockburn’s Road Safety Plan

The following table shows key road safety initiatives that are planned and delivered in Australia and Statewide through state and locally funded programs.

Regional - Cockburn	State Wide	National
<b>Infrastructure and Land Use Planning:</b> Corporate Business Plan 2012/13 – 2016/17 Strategic Community Plan (2012 – 2022) Land management Strategy (2011-2016) Regional & Major Roadwork 2013 – 2030 Annual Business Plan	MRWA Black Spot Program MRWA Road Improvement Program MRWA Road Rehabilitation Program DTA Cycling Infrastructure Program	Federal Black Spot Program Roads to Recovery Program
<b>Transport Planning:</b> City of Cockburn District Traffic Study 2013 City of Cockburn Integrated Transport Plan 2014 Annual Road Program	MRWA Strategic Plan 2020 "The Road Towards Zero" Department of Transport Strategic Plans Public Transport Plan 2031 WA Bicycle Network Plan	National Road Safety Strategy 2011 - 2020
<b>Enforcement and Rules:</b> City of Cockburn (Local Government Act) Local Law 2000  Local Laws covering parking, building Health (alcohol / physical activity)	WA Police Strategic Plan 2010-2013  WA Road Rules: Road Safety Act 1974 and Road Traffic Code 2000	National Road Safety Strategy 2011–2020
<b>Local road safety activities</b> Annual Program Cycling Strategy Active Transport Network Local initiatives targeting alcohol and drugs	Community Road Safety Grants Program	

Table 1: Road Safety Initiatives

## 2. ROAD SAFETY IN THE CITY OF COCKBURN

### 2.1 Introduction

City of Cockburn is located on the coastline of Western Australia, approximately 15km south of the Perth CBD, and consists of a total land area of 170 square kilometres. It is bounded in the north by the Cities of Fremantle, Melville, and Canning, in the east by the City of Armadale, in the south by the Town of Kwinana, and in the west by the Indian Ocean.

Cockburn is very much a developing City, with a good mix of residential and rural areas, and is at an exciting stage of its growth. With about 30,000 households and over 2000 businesses, it still has the capacity for another 20,000 households and the city centres and communities facilities that must come with such residential growth to meet community needs and expectations.

The municipality has a population just over 100,000 (2014) and is expected to grow to more than 130,000 by the year 2031.

### 2.2 Objectives of the Road Safety Strategy

According to the MRWA Crash Statistics, 1,113 were killed and nearly 1,640 were seriously injured on Western Australian roads in the last 5 years; and there were 21 fatal crashes and 426 serious injuries on Cockburn's roads during the same period.

The City of Cockburn's Road Safety Strategy 2014 – 2020 aims to reduce road crashes and road trauma as follows:

***“Provide safe road usage to all Cockburn residents and other travelers, irrespective of their age or physical attributes by using the encouragement, education, engineering and enforcement to provide and promote Safer Roads and Safe Roadsides, Safer Vehicles and Safer Road Use”***



### 2.3 The Planning Process for a Road Safety Strategy

To address the City's vision to have a robust, safe and integrated network that meets people and industry needs while minimising environmental impacts, in 2012 the City reviewed the existing District Traffic Study, which provides the foundation for the future road network planning of the City. The District Traffic Study highlighted that it is not economically viable or sustainable to increase road network capacity to alleviate forecast congestion and that a mode shift away from private vehicle travel will be required for travel to / from and within the City.

To define how travel and access will be improved by considering all types of travel movement and improvements including road, public transport (buses, trains and taxis), pedestrian movement, cycling, and parking, in 2014 the City developed an Integrated Transport Plan.

The Integrated Transport Plan has been developed in close collaboration with the City of Cockburn community, and liaison with the Department of Transport, Department of Planning, Public Transport Authority and Main Roads Western Australia.

During the engagement process the community picked up strong linkages between traffic congestion and road safety issues including poor driver behavior such as risk taking and issues for pedestrians crossing busy arterial roads.

The Integrated Transport Plan presented the implementation plan that directs actions focusing on road safety improvements. Development of a road safety strategy has been noted as one of a short time action.

To prepare the road safety strategy, the City conducted extensive data analysis of crash histories and road safety issues across Cockburn LGA. The safety initiatives will be facilitated to an Action Plan that identifies the existing and potential activities.

An associated communication plan will also be developed to ensure relevant issues, responsibilities and targeted programs are promoted within the local community.

Figure 4 is an extract from the City's Integrated Transport Plan presenting the outcome of the Collaborative Map, the public survey on traffic and transport issues held in August 2013.

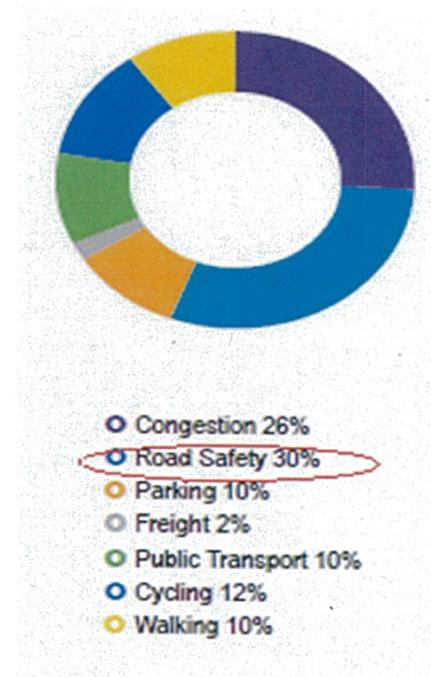


Figure 4: Community feedback on transport issues in Cockburn

## 2.4 Road Safety Issues of Today

The purpose of the road safety strategy is to raise the profile of and address road safety issues at a community level, with a specific goal to reduce the incidence and severity of road crashes and associated road trauma in Cockburn LGA.

A review of the Crash Data 2009 -2013 composed by WA Police and MRWA, and the outcome of the Collaborative Map public survey held in August 2013, identified the number of traffic and road safety issues. The main issues identified are:

### 2.4.1 General Issues

- Speeding in residential streets and through school zones;
- Aggressive driving “hoon” driving;
- Road network congestion;
- “Black Spot” areas;
- Safety for walking and cycling;
- Pedestrian and school crossing;
- Drivers driving under the influence of alcohol and other drugs;

### 2.4.2 Pedestrian Issues

- Provision of pedestrian and cycling paths to key destinations;
- Promotion of improved footpath and cycling networks
- Provision of infrastructure to support “active travel”;
- Bus stops and walking connectivity;
- Road crossing facility;
- Parking and walking around school area.

### 2.4.3 Cyclists

- Provision on road and off road cycling facility;
- Promote and encourage safe cycling practices
- Promote safe cycling to schools and other activity areas

### 2.4.4 Motorcyclists

- Improved maintenance of roads and roadsides to cater specifically for motorcyclists;
- Visibility of motorcycles;
- Promote and encourage motorcycle and scooter riding safety;
- Regular maintenance programs to improve road conditions and road surface markings.

### 2.4.5 Vulnerable Road Users

- Users Provision of accessible paths for vision impaired and/or physically impaired people;
- Access to public transport for visually and / or physically impaired people;
- Design and implementation of tactile ground surface indicators along path routes and at key public transport connections.

## 2.5 Key Road Trauma Issues in Cockburn

There were 21 fatal crashes and 427 serious injuries on Cockburn's roads during the 2009-2013 period. Figure 5 below highlights the difference between the Western Australia significance of fatal and critical injury crashes and those in Cockburn LGA.

Although the number of deaths in Cockburn is only 2% in comparison with the number of total deaths in Western Australian, the number of fatalities within Cockburn is increasing more than 30% each year.

During the period of 2009 – 2013 the main road users group involved in serious and fatal crashes were drivers (90%) and pedestrians (3.5%) and most of the crashes took place in 60km/h and 70km/h speed zones.

The highest risk group is the 25 to 39 year olds; they accounted for 29.6% of crashes in Cockburn.

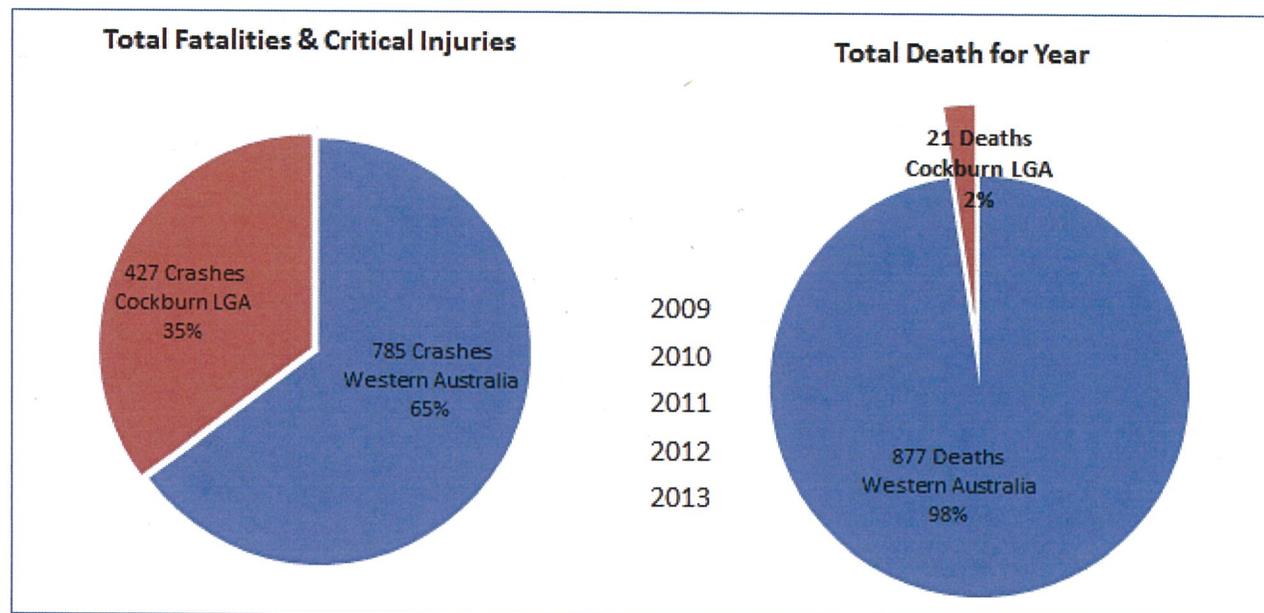


Figure 5: Comparison of fatality & critical injuries and total death per year  
(Source: MRWA's & WA Police Crash Statistics)

## 2.6 Cockburn Community

The City of Cockburn's population was estimated at 103,675 people in 2014 with an expected annual growth rate of 27% until 2031. Figure 6 shows the population forecast in the City of Cockburn.

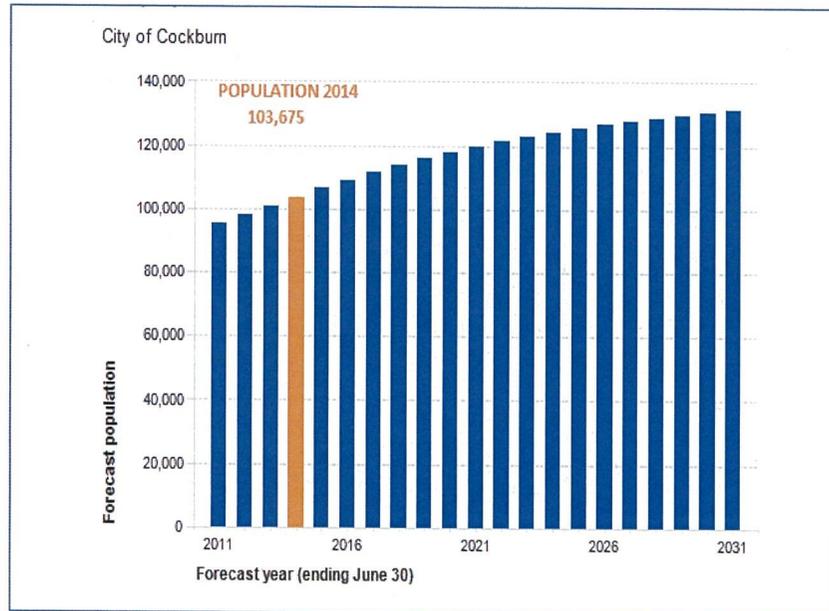


Figure 6: Population forecast in City of Cockburn  
(Source: www.forecast2.id.com.au)

Funding for road safety infrastructure and community-based initiatives is generally based on the number of fatalities and this chart highlights the significance of serious injuries. Figure 7 shows the relationship between fatal, serious and "other" injuries on our roads across a five year period from 2009 to 2013.

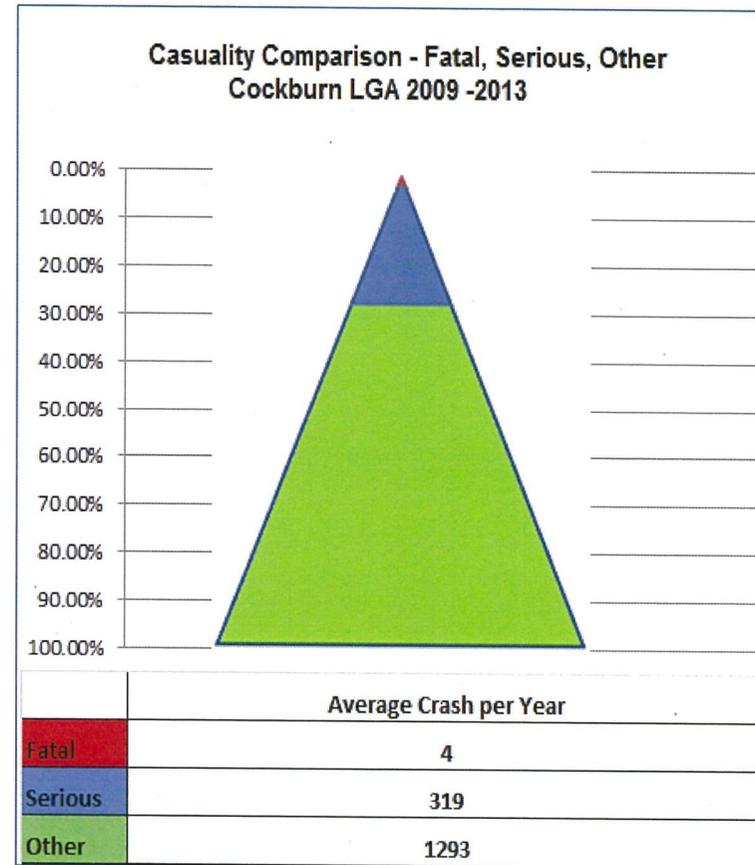


Figure 7: Fatal, Serious and Other crashes in Cockburn LGA, 2009 - 2013

## 2.7 Cockburn Community Involved in Crashes

Figure 8 below compares the size of various sections of the City of Cockburn population with each age group's involvement in crashes by 2013. Comparison between the "population pie chart" on the left and "crash involvement" on the right highlights and shows an over-representation of people aged 25 to 39 in road crashes in Cockburn.

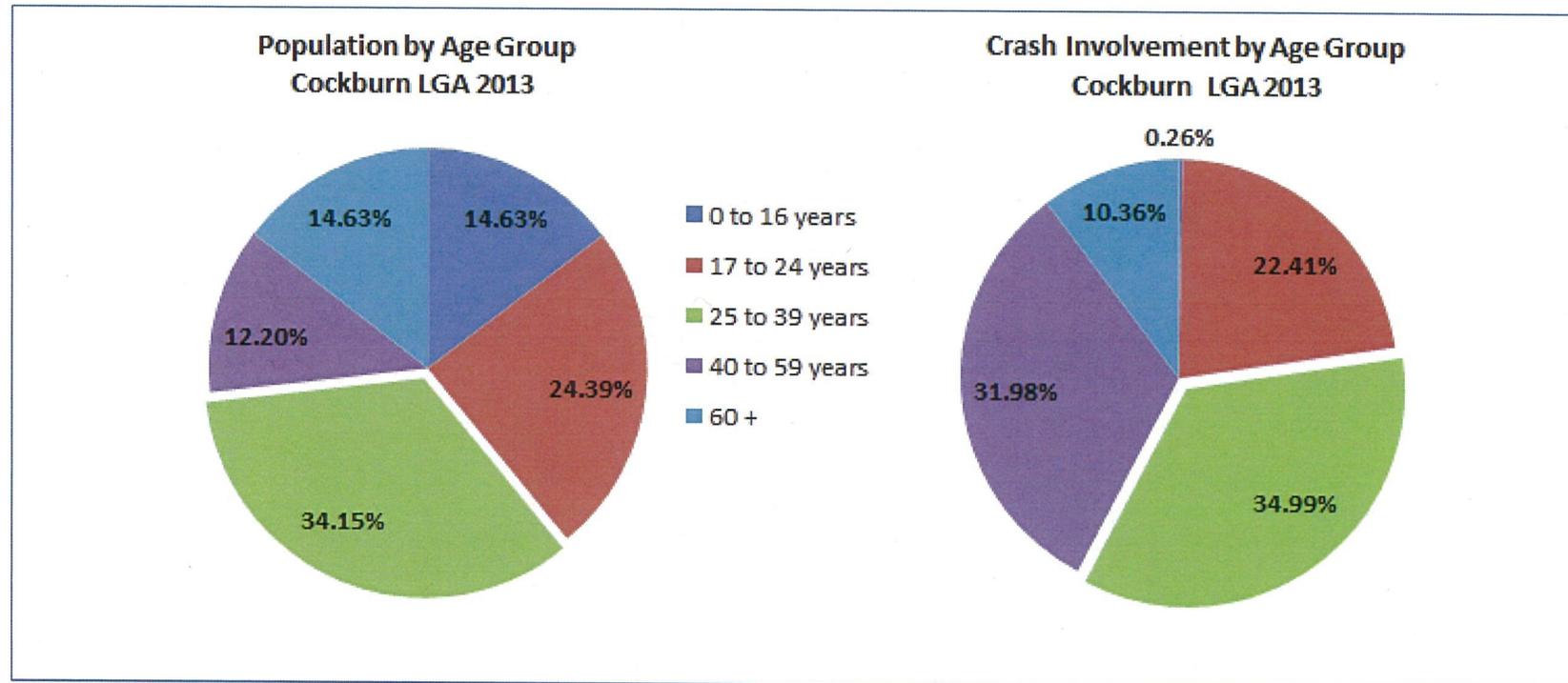


Figure 8: Cockburn Population Age Groups and Their Involvement in Road Crashes 2013

A similar pattern is seen across the five year period 2009 – 2013, when Cockburn LGA had 34.08% crashes involving 25 to 39 age group, 30.76% for the 40 to 59 age group, and 24.39% for the 17 to 24 age group. Again, this highlights that the 17-39 year age group is over-represented in road trauma, involved in more than 50% of the entire crashes on Cockburn roads.

Figure 9 shows the City of Cockburn population with each age group's involvement in crashes across the five year period 2009-2013.

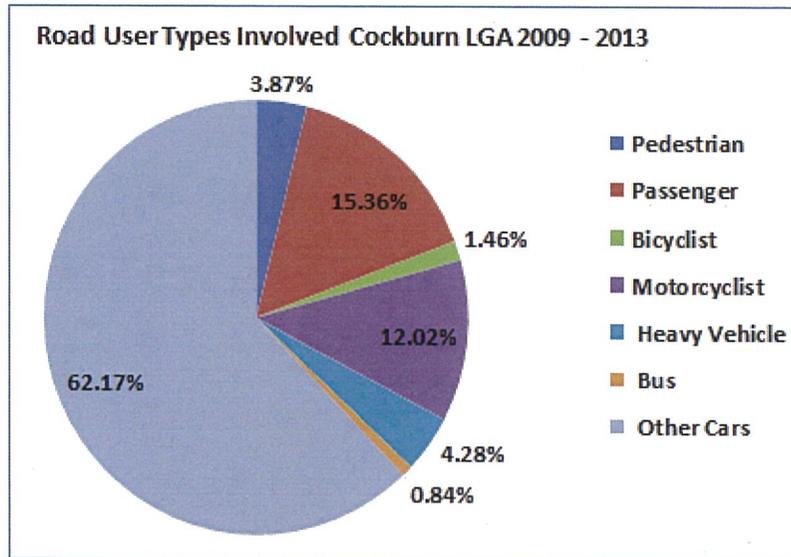


Figure 9: Crash Involvement by Age Group City of Cockburn 2009 – 2013

In Cockburn LGA, car drivers (62.17%) and passengers (15.36%) formed the main portion of the total crashes. Next were motorcyclist (12.02%) and heavy vehicles (4.28%).

Although the number of pedestrians (3.87%) and bicyclist (1.46%) is lower in comparison with other road users in the five year period 2009 – 2013, there were 6 fatalities involving pedestrians and 1 fatality was a cyclist; a total of 21 fatalities in the same period.

Figure 10 shows the types of road users involved in crashes on Cockburn roads during the 2009 – 2013 period.

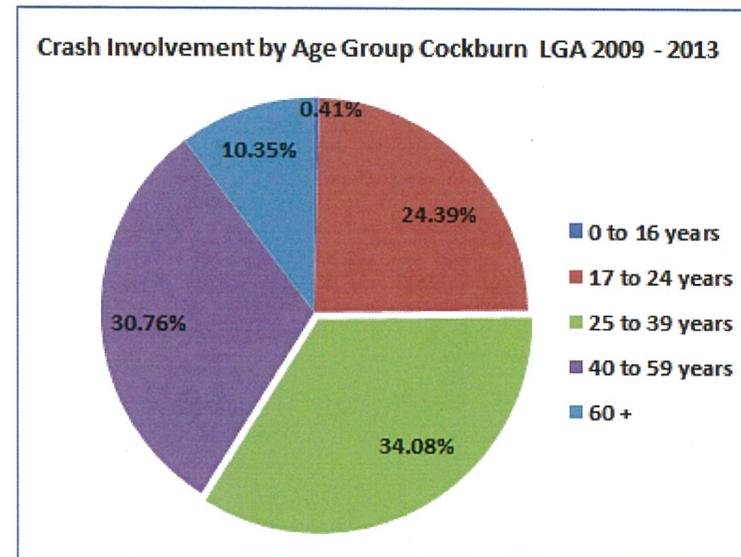


Figure 10: Crash Involvement by Road Users Types City of Cockburn 2009 – 2013



The regional road network is planned and determined by Main Roads WA, provides for through traffic and the distribution of local traffic within the district. The regional road network in the vicinity of Cockburn has a north south orientation with a lesser emphasis on east west road linkages.

The regional road networks within the district are Kwinana Freeway (north – south), Stock Road (north – south), Fremantle to Rockingham Highway (north – south) and Roe Highway (east – west). The Other Regional Road Reservations are North Lake Road / Armadale Road, Spearwood Avenue, Beeliar Drive, Hammond Road and Russell Road.

Table 2 below shows that there were 1,938 road related crashes on Primary and Regional Distributor roads, 3,969 road crashes on District and Local Distributor and 1,853 crashes on local Access roads during the period of 2009 – 2013.

Figure 12 shows the crashes by road type during the 2009 – 2013 period. In Cockburn LGA, District Distributors A (30.92%) and local Access Roads (23.87%) are the roads that the majority of crashes occurred.

Crashes by Road Type	Fatal	Hospital	Others
Primary Distributor	4	76	1250
Regional Distributor	4	40	688
District Distributor A	7	113	2418
District Distributor B	1	49	881
Local Distributor	1	47	670
Access Road	4	102	1853
<b>TOTAL CRASHES</b>	<b>21</b>	<b>427</b>	<b>7760</b>

Table 2: Breakdown of crashes by road type

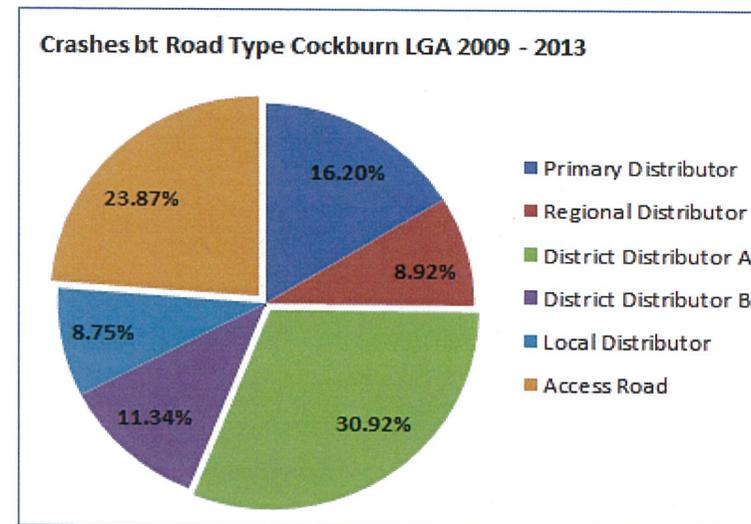


Figure 12: Breakdown of crashes by road type 2009-2013 (Source: MRWA's & WA Police Crash Statistics)

## 2.9 Public Transport

The City of Cockburn is serviced currently by buses and rail, as shown in Figure 12 Cockburn Central Station is on the Perth – Mandurah rail line and has a bus feeder service and a large Park'n Ride facility. A significant increase in passengers alighting or boarding train services at the Cockburn Station resulted in a Government proposal to construct new rail station at Aubin Grove (near Russell Road) in 2015/16.

The PTA's Transperth division provides public transport services in the Cockburn area. There is a good bus connectivity with the CBD Perth, City of Fremantle and City of Rockingham.

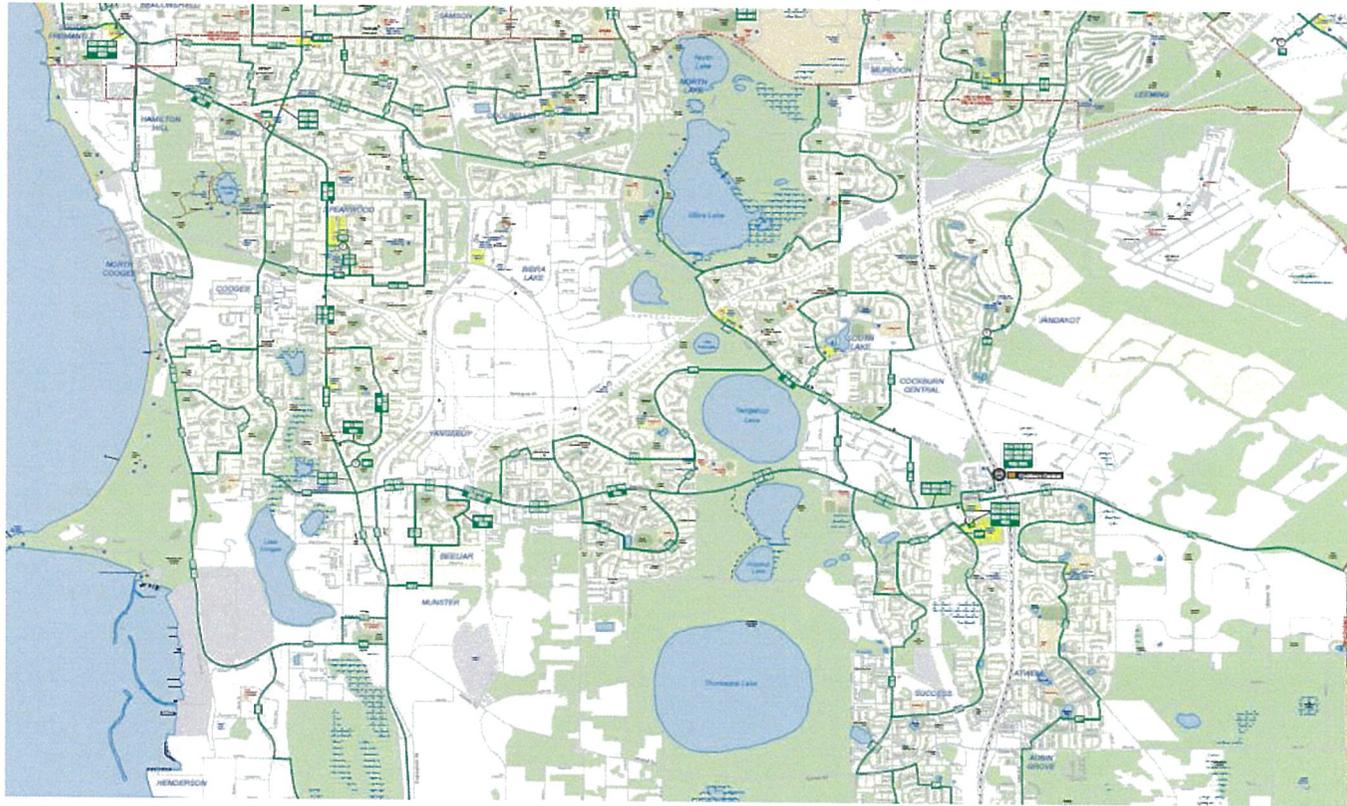


Figure 13: Transperth network maps (Source: PTA -Transperth)

## 2.10 Walking and Cycling in Cockburn

The City of Cockburn has provided 490km of off road bicycle paths, 87km of bicycle paths within a public open space and 73.4km of training and recreation tracks.

Cycling is promoted at many levels, as a valuable travel mode or recreational pursuit for commuters, school children and their families and those riding for leisure or at the elite level. TravelSmart and Your Move are two programs instrumental in inspiring local residents to opt for active travel modes for short trips in and around the City.

The five year crash data in Figure 8 shows that pedestrians, cyclists and motorcyclists continued to be a highly vulnerable group. In 2013, 18 pedestrians were injured; this is a continuation of the relatively high number in 2012.

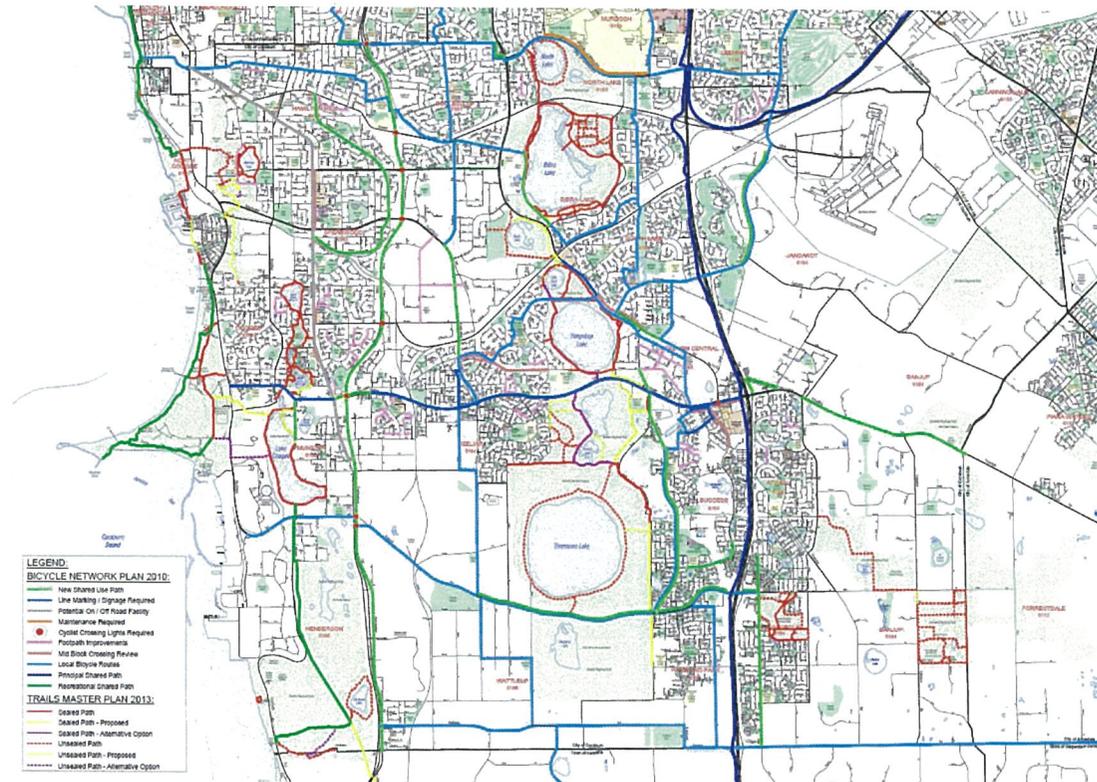


Figure 14: Walking and Cycling Network Map

### 3 ROAD CRASHES IN THE CITY OF COCKBURN

Cockburn is very much a growing City, with a diverse mix of residential, retail, commercial, industrial and rural areas. With more than 35,000 dwellings (2011) and over 6,400 registered businesses (2009), it has the planned capacity to grow to 130,000 people by 2031 supported by a rich mix of centers, community's facilities and diverse employment opportunities.

There were 8208 crashes on Cockburn Roads during the 2009 – 2013 period; 21 fatal, 427 hospital, 1200 medical and 6500 property damage crashes.

The road crashes analyses within this document aim to identify patterns and emerging trends in road trauma across the City of Cockburn, in order to target issues of concern and relevant short-term and long-term solutions to help increase safety on our roads, for all road-users

The financial cost of road trauma by injury category in Cockburn LGA shows that the major portion is due to fatal crashes (\$152M), hospital crashes (\$146M), medical crashes (\$91M) and property damages (\$78M).

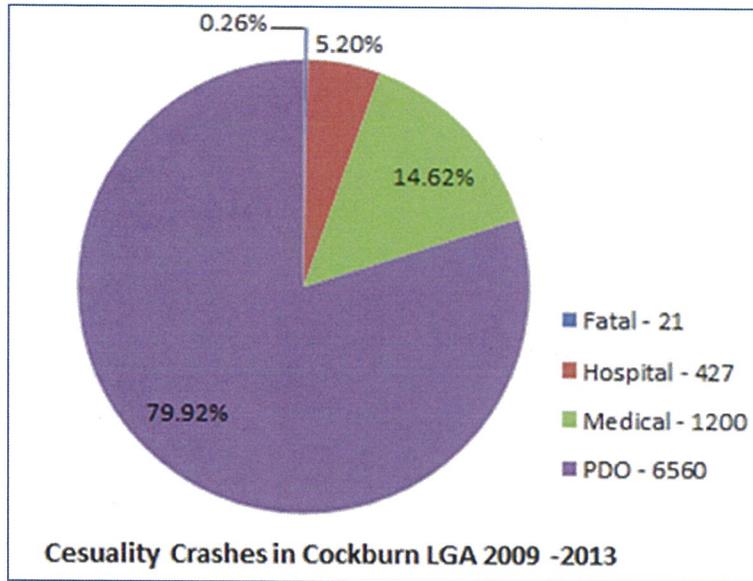


Figure 15: Summary of Crashes  
(Source: MRWA)

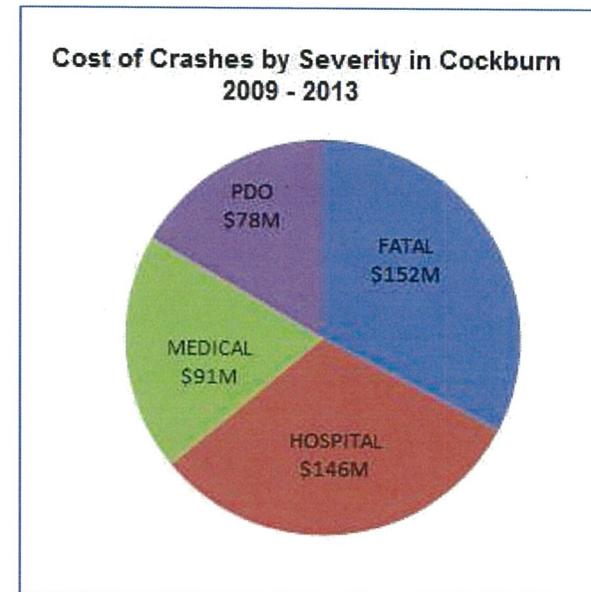


Figure 16: Financial cost of road crashes by severity  
(Source: MRWA)

### 3.1 Comparison with Other Regional Councils

The comparison carried out with the City of Fremantle, City of Melville, City of Kwinana and City of Rockingham, shows the City of Melville has a slightly larger number of crashes than the City of Cockburn, but Cockburn has a higher number of fatalities than other LG areas. Figure 17 shows a comparison in the total number of crashes by severity in each of the LG areas.

The comparison based on crashes per 10,000 population shows

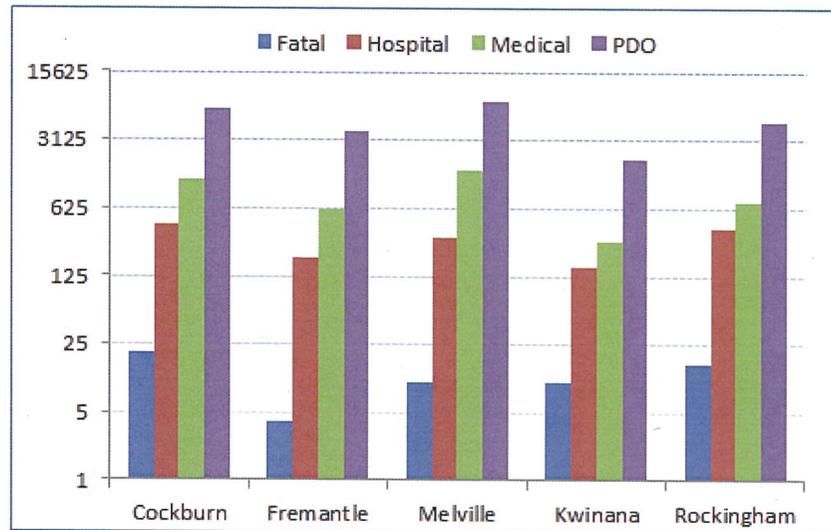


Figure 17: Comparison with other regional Councils 2009 – 2013

that a large number of crashes in the category of hospital, medical and property damage are in the Fremantle area, but the fatality crashes per 10,000 populations is larger in the City of Rockingham.

The City of Cockburn has a similar road trauma profile to the City of Melville. Figure 18 shows a comparison in the number of crashes per 10,000 populations.

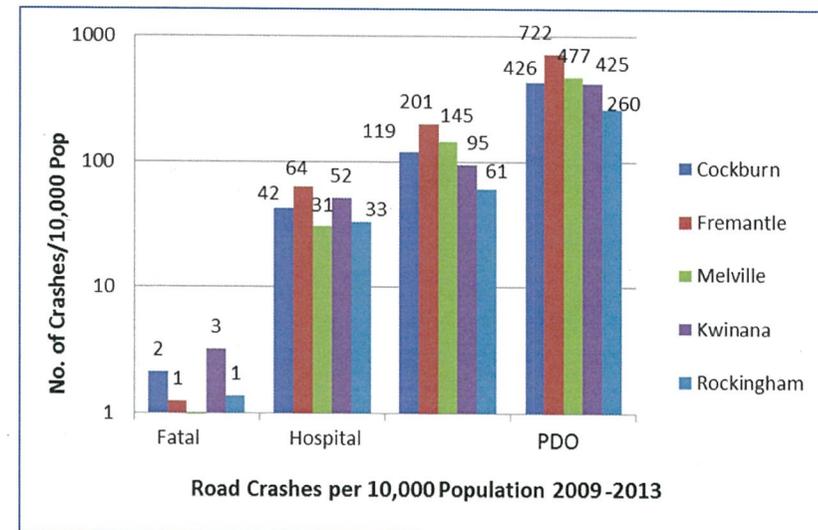


Figure 18: Comparison with other regional Councils- Road Crashes /10,000 population

### 3.2 Crash History

In the five-year period 2009 – 2013, a total of twenty-one people were killed on roads in the City of Cockburn and a total of 426 were seriously injured. The following chart from the MRWA report provides a breakdown of fatalities and injuries for each year.

Note that a 'serious injury' is one where a person is taken to hospital.

SEVERITY	2009 - 2013				
	2009	2010	2011	2012	2013
Fatal	2	3	4	5	6
Hospital	70	99	83	86	79
Medical	210	245	254	276	193
PDO Major	713	861	876	981	808
PDOMinor	397	451	452	459	468
<b>TOTAL</b>	<b>1392</b>	<b>1659</b>	<b>1669</b>	<b>1807</b>	<b>1554</b>

Table 3: Total Crashes by Severity in Cockburn, 2009 – 2013

Table 3 shows there has been a marked reduction in the total number of crashes but fatality crash numbers have increased.

The trend line by Figure 19 is a reduction of total crashes in 2013.

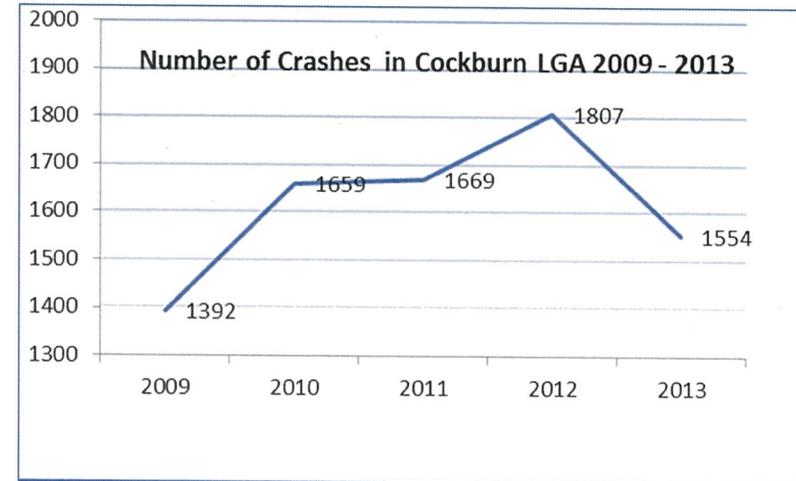


Figure 19: Fatal and Injuries crashes in Cockburn, 2009 – 2013

### 3.3 Casualty Crashes by Speed Zones 2009-2013

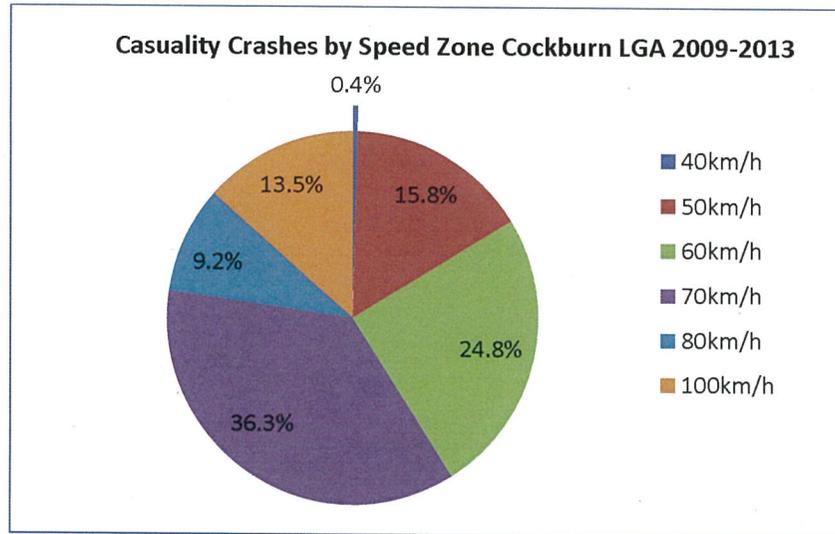


Figure 20 Casualty crashes by speed zone

Figure 20 shows that casualty crashes occurred most frequently in speed zones of 70km/h (36.3% of total crashes) and 60km/h speed zones (24.8% of total crashes).

Although the percentage of crashes in 40km/h speed zone is less than 1%, the number of crashes showing more than a 20% increases each year.

### 3.4 Casualty Crashes by Vehicle Type

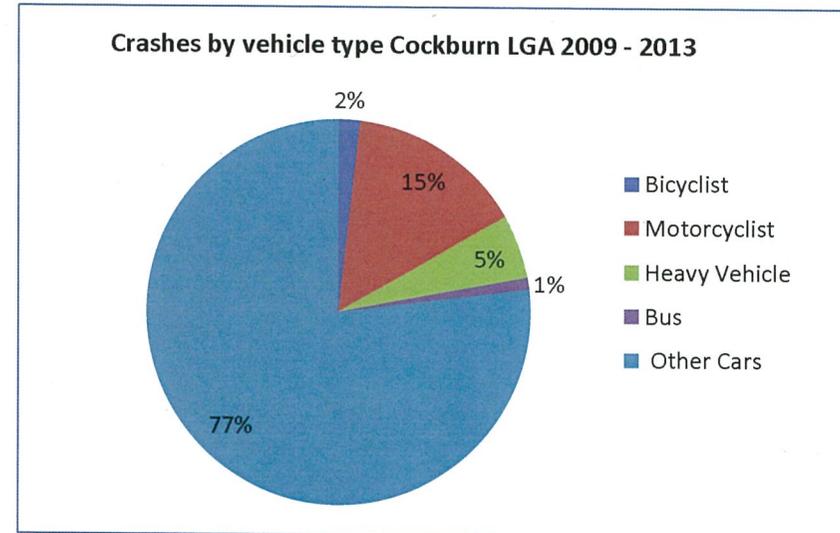


Figure 21 Casualty crashes by vehicle type

During the period of 2009 – 2013 the main vehicle group involved in serious and fatal crashes was vehicles from Class 1 & 2 (AUSTROADS Vehicle Classification System – Sedan, Wagon, 4WD, Utility, Short – Towing Trailer, Caravan or Boat).

The highest risk group is the motorcyclists. They accounted for 15% of crashes on Cockburn roads.

## 4. ROAD SAFETY STRATEGY

### 4.1 Safe System Approach

*The 'Safe System' views the road transport system holistically by seeking to manage the interaction between road users, roads and roadsides, travel speeds and vehicles. The Safe System recognizes it is probably not possible to prevent all crashes but aims to prevent those that result in death and serious injury.*

The Western Australia road safety strategy, Towards Zero, sets an ambitious target of 11,000 fewer deaths and serious injuries by 2020. The "Safe System" approach, which underpins Towards Zero, views the road transport system holistically by seeking to manage the interaction between road users; roads and roadside; travel speeds and vehicles.

The City of Cockburn Strategy is about creating the environment, procedures, systems and tools to make the strategic objective a reality. It complements the State Government's Road Safety Strategy with initiatives and actions that will enhance and build capability to deliver the Towards Zero outcomes and target.

By taking a TOTAL view of the COMBINED factors involved in road trauma and road safety, the City aims to design and build a whole road system that will protect all road users and reduce the number of deaths and serious injuries.

### 4.2 Elements of Safe System

The City's Safe System comprises three key principles: Safer Roads and Safe Roadsides, Safer Vehicles and Safer Road Users.

Figure 22 below is a flow chart that explains the Safe System Approach. The chart gives some examples of what sort of actions, activities or products the City can put in place to provide the very best level of prevention of road crashes and protection of road users if crash does occur.

All of factors are shown on the chart, with all of these ideas and activities will be delivered through action plans to achieve reduced risk of being killed or seriously injured.

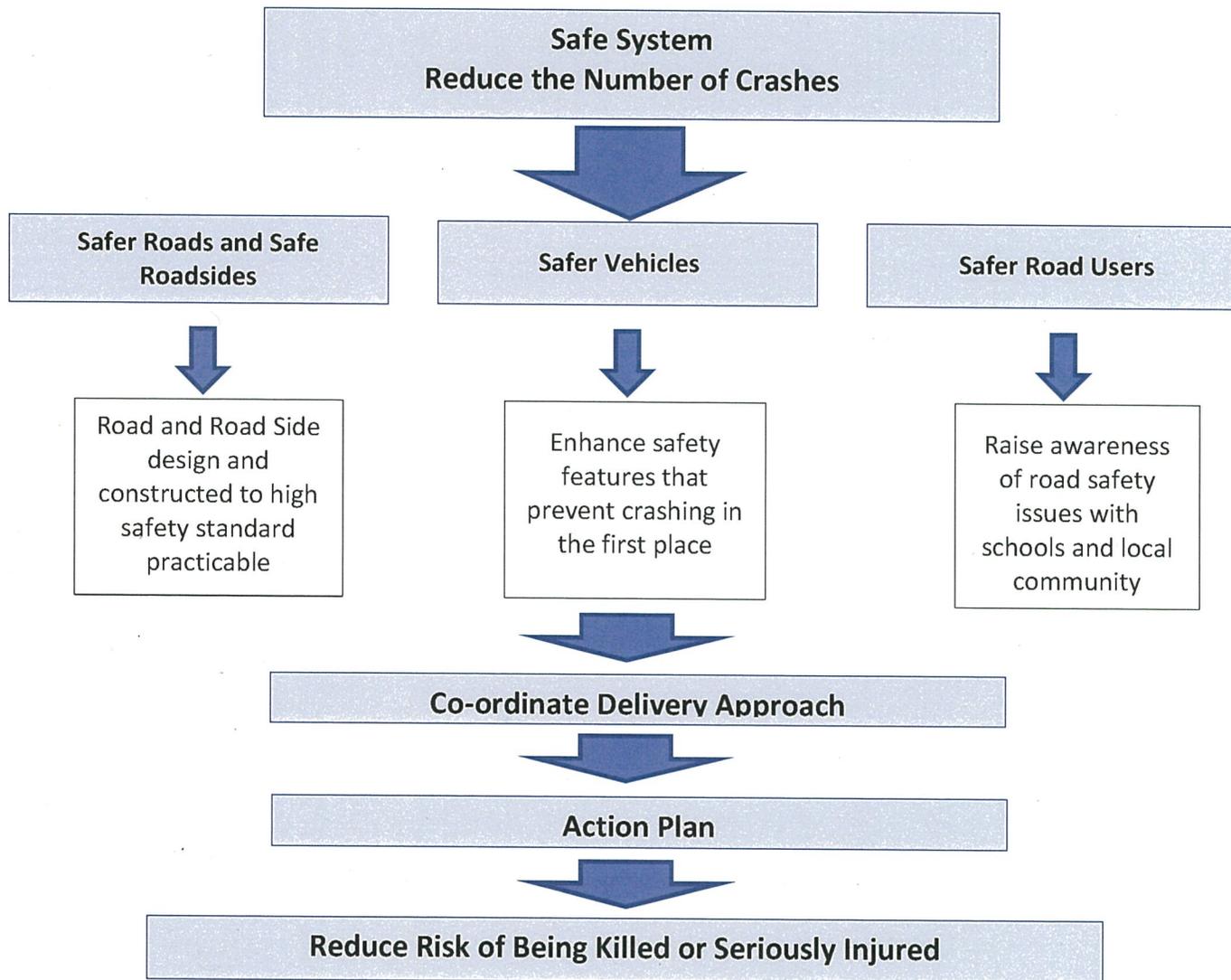


Figure 22: Cockburn LGA Safe System Framework

#### **4.2.1 Safer Roads and Safe Roadsides**

A “road” by definition in the Local Government Act 1989 includes a street; right of way, cul-de-sac, by-pass; bridge, footpath, bicycle path or nature strip, any culvert or other land or works forming part of the road. The “roadside” includes all the land abutting a road between adjacent property boundaries.

Appropriate road and roadside design and construction, including speed limits that are appropriate for the environment, land use and road are necessary for safer roads and safe roadsides functions.

Regular maintenance and infrastructure improvements can influence crash outcomes in two ways. Firstly, general investment in roads improves the overall quality of the road network, resulting in indirect safety benefits. Secondly, expenditure on safety improvements such as shoulder sealing, median barriers and intersection treatments directly reduce road trauma by creating infrastructure that is more forgiving of human error.

#### **4.2.2 Safer Vehicles**

Vehicles with high-standard safety features reduce not only the likelihood of crashes occurring, but also the severity of crash outcomes on all those involved, whether they are within or outside the vehicle.

Improvements in vehicle safety have contributed significantly to road trauma reduction. These improvements reflect steady advances in automotive safety design, including occupant protection performance, braking, handling, lighting and the inclusion of life saving safety features such as seatbelts and airbags.

The Safer Vehicle aspect refers to fleet management with fleet owners encouraged to purchase vehicles that have the highest safety ratings in their class. Council has already demonstrated Safer Driver best practice by having in place its proposed Light Vehicle Purchasing Guidelines.

As part of the guideline, Council would purchase vehicles for its fleet that have the highest safety ratings in their class. This would make a valuable contribution to the safety of Council employees while they are driving Council vehicles.

Having a Safer Vehicles Guideline that includes the purchasing of the safest vehicles available in their class is considered to be best practice and good corporate governance to vulnerable users.

#### **4.2.3 Safer Road Users**

Most road users respect the law, have good safety awareness and use the roads in a sensible manner. But even these people make unintended mistakes — and sometimes those mistakes result in death or serious injury.

Many behaviors increase the risk of a serious crash, including speeding, driving or riding while under the influence of alcohol or drugs, driving or riding while fatigued or distracted and travelling without a seatbelt.

The City will deliver behavioral road safety programs that aim to improve the safety of at-risk road user groups and to address road safety issues identified in the Road Safety Policy. Communicating and promoting key road safety messages, events and campaigns with emphasis on the “Safe System” the City will raise community responsibility /compliance with safe framework.

The City cannot reduce the road toll alone. Strong community partnerships are critical for delivering the outcomes of this action plan.

## 5. ACTION PLAN

All road safety issues listed in section 2.4 are summarised and detailed in the Road Safety Action Plan.

The initiatives are listed according to the Safe System Approach divided into three major sections:

- **Safer Roads and Roadside;**
- **Safe Vehicles and**
- **Safe Road Users.**

The major sections have guided the development of the priority actions of the plan that have been grouped under three 'E' approaches:

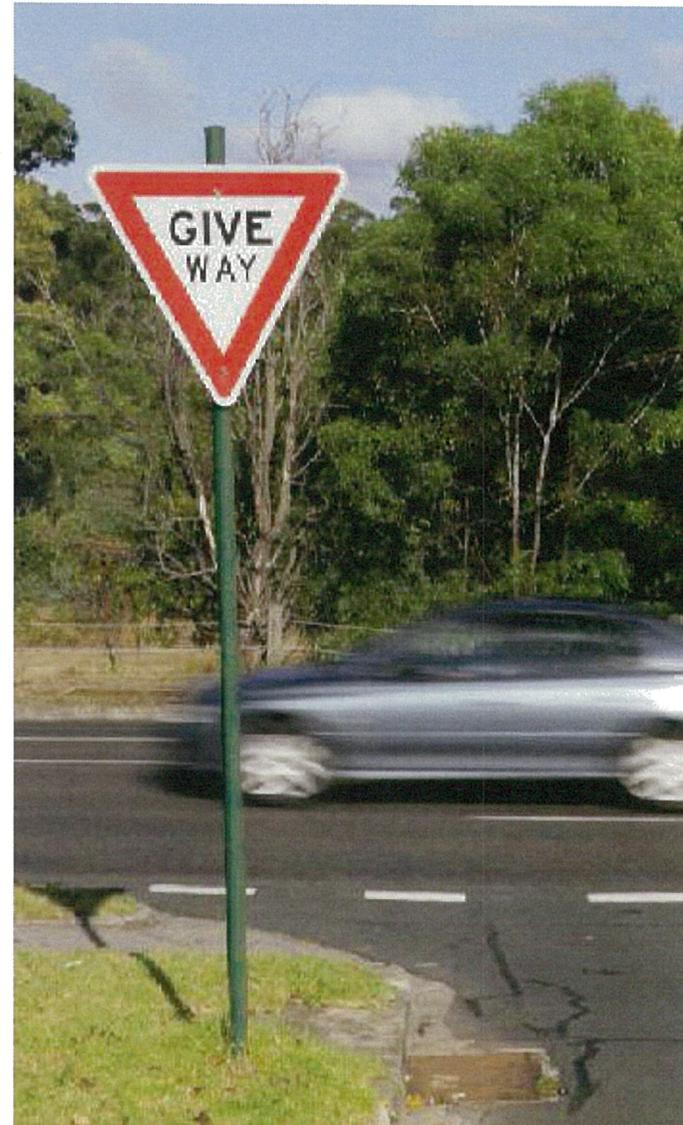
- **Engineering** – to keep road and roadside infrastructure and roadside environment at a safe standard;
- **Education** – to inform road users of road safety and awareness of risk;
- **Encouragement** – to encourage the use and ownership of safe vehicles and to be a responsible road user.

The actions within the plan have been given a priority according to the following timescales:

- **Short: to 2017**
- **Medium: 2017 to 2020**

### Scale of Cost

- 0 Operational
- \$ Low (< \$100,000)
- \$\$ Medium (\$100,000 - \$500,000)
- \$\$\$ High (>\$500,000)



## Safer Roads and Safe Roadsides

Action	Description / Justification	Priority	Responsibility	Cost
Engineering	Continually improve the safety of the road and roadside environment by:			
	- Develop a safe, effective and efficient transport network for the Cockburn LGA;	Medium	CoC/MRWA	\$\$\$
	- Increase funds allocated to the Safer Roads and Black Spot programs to fast-track engineering treatments at higher risk sites on the state-controlled road network;	Medium	CoC/MRWA	\$\$\$
	- Reduce speed in local streets; there is a direct correlation between speed and the number and severity of pedestrian crashes;	Short	CoC	0
	- Prioritise existing Black Spot locations within the municipality and work with MRWA to address in a prompt manner;	Short	CoC/MRWA	\$\$\$
	- Provide high standard infrastructure that supports and promotes „walkability“ and active“ travel modes across all ages and abilities, with mode separation where feasible;	Medium	CoC/MRWA	\$
	- Improve pedestrian safety by installing pedestrian safety devices including median strips, fencing and traffic calming devices;	Short	CoC	\$\$
	- Provide appropriate rest areas and secure truck parking, away from urban areas;	Medium	CoC	\$\$
	- Improve visibility for drivers and passengers;	Short	CoC	\$
Continually improve the road and road side maintenance regime by:				
- Maintain high quality road surfacing, line-marking, lighting etc. to minimise driver distraction, assist with the driving task and to protect vulnerable road users, including motorcyclists,	Short	CoC/MRWA	\$\$\$	

	<p>cyclists and pedestrians of all ages and abilities;</p> <ul style="list-style-type: none"> <li>- Improve roadsides by removing hazards in the event of run-off crashes;</li> <li>- Control and monitor potentially on verge distracting signage and billboards.</li> </ul>	Short	CoC	0
		Short/Medium	CoC	0
	Local Area Traffic Management			
	<ul style="list-style-type: none"> <li>- Improve traffic flow and made it more predictable by installing signals and roundabouts;</li> <li>- Introduction of part road closure to improve safety around schools and reduce "rat-running."</li> <li>- Provide traffic calming infrastructure to improve safety for walking and cycling;</li> <li>- Provide traffic calming infrastructure to improve safety for pedestrian and school crossing;</li> <li>- Provide traffic calming infrastructure or posted speed reductions as needed, in liaison with WA Police and MRWA.</li> </ul>	Medium	CoC/MRWA	\$\$\$
		Short	CoC	\$
		Medium	CoC/MRWA	\$\$
		Short	CoC/MRWA	\$
		Short	CoC/MRWA	\$
Education	<ul style="list-style-type: none"> <li>- Provide ongoing professional development as relevant to key personnel;</li> <li>- Implement "best practice" road and roadside design and maintenance;</li> <li>- Update existing City's Bicycle Network and Footpath Plan - 2010 to improve connectivity of new and existing infrastructure;</li> <li>- Raise awareness of posted speed limits;</li> <li>- Display safety messages on existing variable message signs on local-controlled roads;</li> <li>- Raise community awareness of the need to use crossing infrastructure where provided;</li> </ul>	Short	CoC	\$
		Short	CoC	\$
		Short	CoC	\$
		Short	CoC	0
		Short	CoC/Schools	0
		Short		

	<ul style="list-style-type: none"> <li>- Increase awareness of the needs of motorcyclists in terms of road maintenance, road cleaning / road opening reinstatement;</li> <li>- Identify and trial innovative safety engineering treatments to further increase visibility of road markings and signage.</li> </ul>	Short	CoC/MRWA	0
			CoC	0
		Medium	CoC/MRWA	\$
Encouragement	<ul style="list-style-type: none"> <li>- Encourage the community to identify and report missing or damaged infrastructure;</li> <li>- Promote adoption of “active” travel.</li> <li>- Promote rest areas to freight industry.</li> <li>- Raise awareness of key messages around braking distances, impact speeds and associated road trauma;</li> </ul>	Short	CoC	0
		Short	CoC/Schools	0
		Medium	CoC/Business	\$\$
		Short	Coc	0

### Safer Vehicles

Action	Description / Justification	Priority	Responsibility	Cost
Engineering	- Increased uptake of “safer” vehicles in the City’s “fleet”.	Short	CoC	\$\$
	- Safer trucks and operational plants on road network;	Short	CoC	\$\$
	- Improve the City’s vehicle maintenance regime;	Short	CoC	0
	- Review patterns of older vehicle occupant injury and vehicle crash test results to investigate vehicle safety features;	Short	CoC	0
	- Increased use of seat belts and other protective gear.	Short	WA Police	0
	- Reduction in the number of unregistered /worthy vehicles and	Short	WA Police	0

	increased attention to vehicle maintenance			
Education	- Provide ongoing safe driving development as relevant to operational personnel;	Short	CoC	\$
	- Promote four star and five star vehicles;	Short	CoC	0
	- Raise awareness of road rules and level of protection offered;	Short	CoC	0
	- Promote the benefits of safety features in new and used vehicles;	Medium	CoC	0
	- Promote safe vehicle information to the business community to curb work-related crashes, which are a significant part of the road toll and account for a high proportion of all work-related deaths and injuries;	Medium	CoC	0
	- Improve availability and accessibility of consumer information about used vehicle safety;	Medium	CoC	0
	- Increased attention to vehicle maintenance.	Short	CoC	0
Encouragement	- Develop a Safe Vehicle Policy that includes the purchasing of the safest vehicles available in their class is considered to be best practice and good corporate governance;	Short	CoC	\$
	- Develop industry partnerships to communicate the importance of fleet safety and improve the uptake of safer vehicles in WA commercial fleet;	Medium	CoC	0
	- Raise awareness of hazards of poorly maintained vehicles;	Short	CoC	0
	- Target young people and heavy vehicle operators through road safety programs.	Short	Coc	0

## Safer Road Users

Action	Description / Justification	Priority	Responsibility	Cost
Engineering	- Identify and implement engineering solutions to help reduce speeding where relevant and as feasible.	Short	CoC	0
	- Develop a more predictable network in terms of posted speed limits suitable for specific road types and road use.	Medium	CoC	\$
	- Ensure road infrastructure is of high quality to minimise driver distraction.	Short	CoC	0
	- Trial innovative and cost-effective level crossing safety technologies to improve driver behaviour at rail level crossings.	Medium	CoC/MRWA	\$\$
	- Provide quality infrastructure to support pedestrians and „active“ travel including cycling, near schools and across the general community.	Short	CoC	\$\$
	- Continue to install flashing light signs at school zones as part of the government’s school safety program.	Medium	CoC/MRWA	0
Education	- Develop public information campaigns and education resources about fatigue for all road users, with a particular focus on educating novice drivers.	Medium	CoC/WA Police/Schools	0
	- Provide programs that raise awareness of fatigue issues and fatigue management, tailored for specific target groups including freight sector, young people, students, shift-workers and families with young children.	Medium	CoC	0
	- Raise community awareness of the risks of excessive speed and the potentially negative outcomes of higher speeds;	Short	CoC	0
	- Promote relevant road rules and “share the road” messages;	Short	CoC	0
	- Provide support for Traffic Safety Education and associated	Medium		

	<p>policies;</p> <ul style="list-style-type: none"> <li>- Increase commitment of safer vehicles and improve vehicle maintenance and use of protective features and equipment.</li> </ul>	Short	CoC	0
			CoC	0
Encouragement	- Raise community perceptions of the likelihood of being detected if speeding.	Short	CoC	0
	- Encourage compliance within posted speed limits by raising awareness of those limits.	Short	CoC	0
	- Promote use of existing "best practice" Traffic Safety Education programs and resources and associated "enrichment activities" in schools.	Medium	CoC	0
	- Provide "safer" vehicle information, especially to vulnerable groups, such seniors and young people and their families to help inform purchases.	Medium	CoC	0
	- Encourage safer road crossing and driving practices and review.	Short	CoC	0
	- Support and highlight local agencies "road safety campaigns".	Short	CoC	0

## 6. EVALUATION AND REVIEW

The City will evaluate and review the Road Safety Policy Action Plan 2014 – 2020 in 2017 and at the same time prepares a subsequent action plan for the period 2017 – 2020.

### 6.1 Rationale

Each action:

- Will be capable of having its effectiveness assessed;
- Will have a financial cost that needs to be accounted for;
- Will have inherent measures of “success”; and needs to be more than just a delivery of action;
- Will have an intrinsic implementation period that will determine the timing of the assessment.

### 6.2 Review / Evaluation

- Engineering Service to coordinate review / evaluation of Action Plan;
- Review / evaluation to be conducted in 2017;
- Some actions will be reviewed / evaluated separately / upon completion.

### 6.3 Accountability

This Policy will be assessed in terms of its accountability to:

- Council
- Management
- Community.

### 6.4 On-going Delivery

- In 2014 a new Action Plan for 2014 – 2020 will be adapted;
- A review and evaluation of the Action Plan for 2014 – 2017 will be undertaken when the Action Plan for the next period is documented.

### 6.5 Legislation, regulation and standards

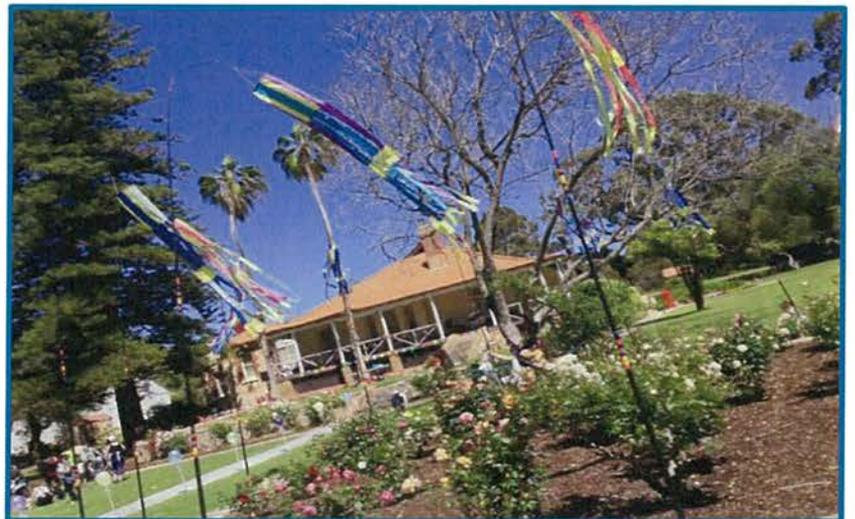
- Any new legislation, regulation and standards proposed by National and State updated to support new directions to improve the safety on Cockburn LGA.
- Some initiatives, such as nationwide improvements to driver licensing or driving rules will require decisions at a national level in order for them to be implemented effectively.
- The City will update current policies, standards and process to ensure that a design and construction of road network utilize safe approaches.

### 6.6 Funding and resource allocation

Some initiatives are likely to require additional funding and options for this could include:

- Current funding being re-prioritised to activities which are deemed more important;
- Additional funding being provided for specific initiatives;
- A partnership arrangement with stakeholders.

# City of Cockburn Public Open Space Strategy 2014 – 2024



## Document Review

This strategy is designed to be reviewed every five years. Periodic reviews can be undertaken if significant or policy changes occur.

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## EXECUTIVE SUMMARY

Public Open Space (POS) is the most visible asset the City is responsible for providing, managing and maintaining and provides a key opportunity for communicating the City's vision and values. POS provides a number of different services, including opportunities for social and recreational pursuits, community development, improved amenity, green spaces and ecological functions.

The City manages over 300 parks and 36,173 street trees along 878km of roads and this incredible responsibility necessitates a strategic approach. For the purposes of this strategy, POS is not confined to a traditional understanding, but includes roadside or streetscape areas, regional parks and foreshore reserves

The City has developed this strategy in order to strategically manage these assets for the community now and into the future. This Strategy sits within the City's broader framework of corporate strategic planning, ensuring alignment with the organisation's vision and values and is cognisant of state and local planning frameworks and regulations, which will guide the sustainable management of POS.

POS contains a number of different values, which must be understood to ensure maximum benefit for the community. The City combines these values with an understanding of trends – demographic, health, population, recreation and other – to support the creation and maintenance of community assets, which are accessible, sustainable, functional and well utilised. Because of this, POS management must be flexible in order to respond to the many factors which affect it.

The Action Plan contained within this document, enables a tangible approach to achieving the Strategy's vision, values and objectives. They are classed according to time frames and costs, which will guide future planning.

As well as outlining a classification of the City's streetscapes, this Strategy highlights the network of roads and streetscapes, which are under active management. This Strategy outlines the level of service per annum for key activities as well as a comprehensive inventory of the City's POS.

The City welcomes your feedback on this document.

# 1 INTRODUCTION

## Purpose of the Public Open Space Strategy

The Public Open Space Strategy will provide direction to guide the future provision, enhancement and management of open space in the City of Cockburn (the City). The strategy will assist the City in its future allocation of resources, decision making and adequately respond to the trends and issues associated with public open space.

## What is Public Open Space?

For the purposes of this strategy document, the description Public Open Space (POS) is used to refer to:

*Publicly owned land which has existing or potential value for a variety of purposes including recreation, sporting facilities, community development, conservation and public amenity.*

This definition widens the narrower definition commonly used by the planning profession in that it also encompasses:

- Roadside or streetscape areas
- Regional parks
- Foreshore reserves

Apart from their potential to accommodate recreational pursuits, these spaces are valuable in that they link adjacent POS areas and create

- Wildlife corridors linking remnant vegetation and
- Pedestrian and cycle links enhancing the value of smaller POS areas

POS in this strategy is subject to many forms of tenure and management, with maintenance, management and ownership being both shared and dispersed between the City and the State's various authorities.



## **Why is Public Open Space Important?**

POS is extensively used by the local community and visitors for recreation and leisure functions, social gatherings, events and tourism. Furthermore public open space is important for biodiversity, mitigation of urban heat island effect, public health and has heritage value.

POS is a major contributor to the City's vision to be the most attractive place to live, work, visit and invest in, within the Perth Metropolitan area.

### **POS must include:**

- Equitable provision of facilities and resources
- Responsible and responsive management of natural and financial resources
- The need to address and reflect changing public concerns and priorities
- An awareness of local, regional and even global issues through the manner in which POS is developed, managed and maintained.

### **This strategy will provide a blue print for developing, managing and maintaining the City's POS so that it:**

- Actively contributes to achieving the City's corporate vision.
- Successfully communicates the commonly held concerns and values of the community
- Provides an appropriate environment for the communities recreational, health and social needs

### **In order to do this, POS must:**

- Provide a robust resource that can respond to changing circumstances and demands from society
- Perform as a seamless web of spaces interfacing and connecting the private and public activities of the city
- Reconcile competing and conflicting interests
- Act as a vehicle for soliciting public approval and appreciation of the City
- Provide respite from the pressures of living in an urban environment

### **POS is generally expected to accommodate the following activities:**

- Natural Areas - conservation and appreciation of local ecology by residents and the City's staff
- Parkland Areas - Informal social gatherings, solitary relaxation, exercise of dogs, fitness training, and play
- Sports fields - Organised sports and athletics activities
- Street verges – expression of pride by residents, pedestrian movement, car parking

The following strategies have formed the basis of POS management to date in the City of Cockburn:

### **City of Cockburn Open Space Strategy (1999)**

This strategy was undertaken to record and evaluate all active and passive parks and develop recommendations for the existing network of parks within the City, excluding regional open space and conservation reserves.

### **City of Cockburn Passive Parks Review (2006)**

This was a review of Passive Parks within the City in order to develop a policy to guide the City in the provision of Passive Parks that suitably reflect the leisure, cultural and social needs of the community. Secondly, the preparation of an implementation strategy was required.



## 2 VISION, GOAL AND OBJECTIVES

Public open space is vital in maintaining the high quality of our public domain and perpetuating the City's vision:

*“Council’s vision is to build on the solid foundations that our history has provided to ensure that the Cockburn of the future will be the most attractive place to live, work, visit and invest in, within the Perth Metropolitan area”*

This POS Strategy will provide the City with a foundation to provide aesthetically pleasing, functional and sustainable public spaces, which beautify our City today and will remain equally impressive for future generations to enjoy.

The principle vision of the Public Open Space Strategy is to:

*“Create a sustainable hierarchy of accessible and resource efficient quality open spaces that enhance the recreational, environmental, social and health needs of current and future generations”*

### Objectives

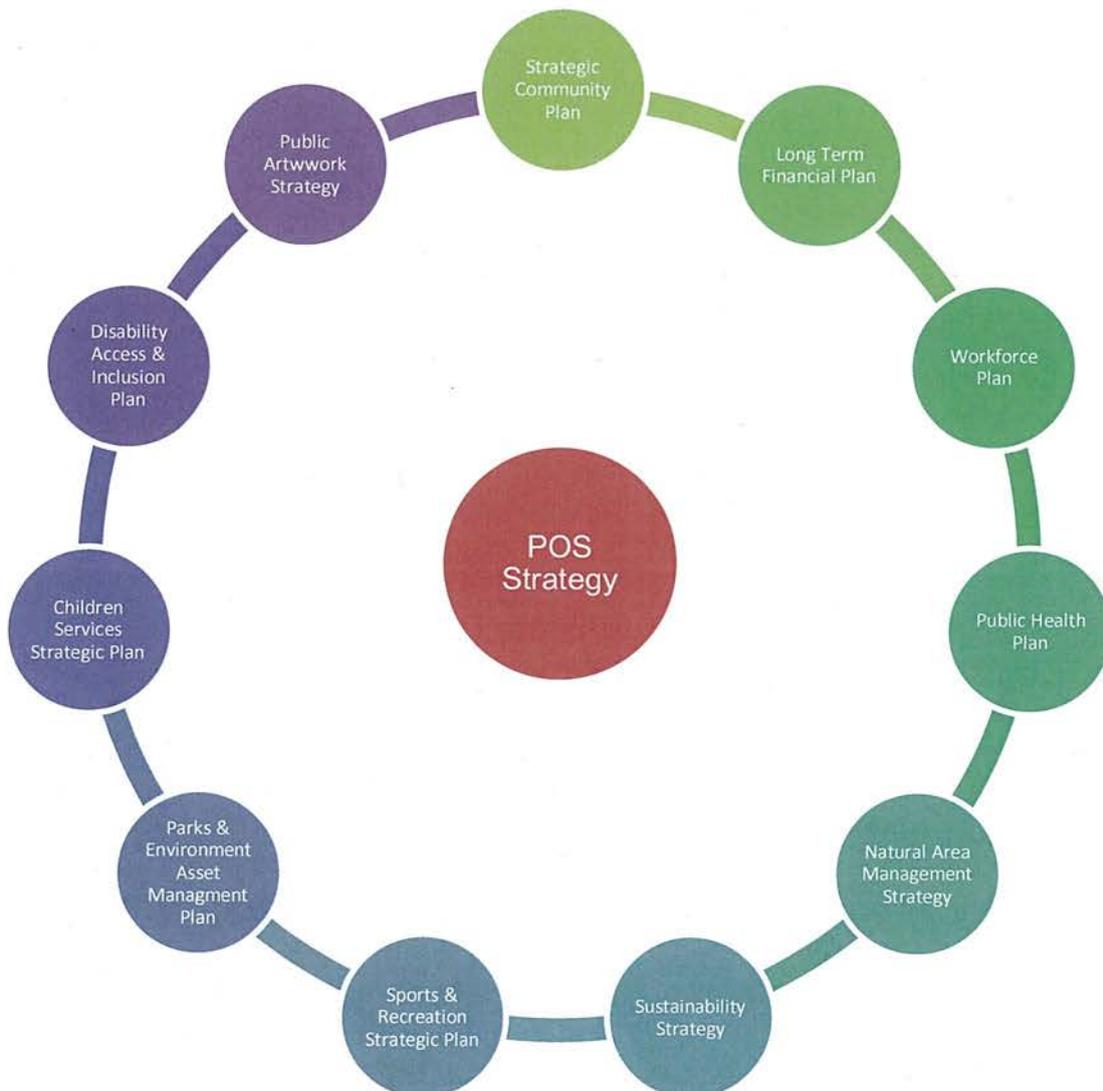
The following five themes have been established for the preparation and implementation of the Public Open Space Strategy:

- **Classification** – Establish a hierarchy to inform the community of the provision of POS and Streetscapes, including how the City will manage and invest in infrastructure to ensure sustainability for future generations.
- **Value** – Define the character and diversity of our community. Our parks and community hubs will be safe and functional spaces that enable sporting and social recreational pursuits and provide relaxation nodes for the community. Our streetscapes will contribute to a rich culture, align with our sister cities relationships and link ecological corridors.
- **Participation** – Create a strong alliance with the community, state and local governments, education and health departments and the private sector to achieve shared use and common goals and ensure that parks are accessible to people of all abilities in the community.
- **Connectivity** – Establish connectivity through POS and streetscape provision to increase community movement and link ecological corridors. POS provides the destination point between journeys to community hubs, educational pursuits or the business sector. Strong tree-lined streets ensure the trip is safe and enjoyable without being impacted by the elements.
- **Responsible Management** – Invest in POS maintenance to provide substantial benefits to the community. Sustainable development combined with strong asset management principles will ensure functional, leisure, recreational and ecological spaces. Addressing community needs and emerging trends will require well-articulated and responsive management.

### 3 STRATEGIC ALIGNMENT

The POS Strategy is aligned to the following corporate strategic documents. An explanation for each of the documents, including their hierarchical structure within the organisation, please refer to Appendix 1.

- Strategic Community Plan 2012 – 2022
- Long Term Financial Plan 2012/13 – 2021/22
- Workforce Plan 2012 – 2017
- Public Health Plan 2013 – 2018
- Natural Area Management Strategy 2012 – 2022
- Sustainability Strategy 2013 - 2017
- Sport and Recreation Strategic Plan 2009
- Parks & Environment Asset Management Plan 2014
- Disability Access & Inclusion Plan 2012 – 2017
- Public Artworks Strategy 2009



## 4 PLANNING FRAMEWORK

The Strategy is cognisant of the following planning frameworks, which inform development within the City of Cockburn. For a detailed explanation of these frameworks, please refer to Appendix 2.

### State Government Planning Framework

- Directions 2031 and Beyond
- State Planning Framework
- Strategic Planning
- Statutory Planning
- Liveable Neighbourhoods

### City of Cockburn Planning Framework

- Town Planning Scheme
- Local Planning Strategy
- Structure Plans
- Subdivision Plans and Development Applications
- POS Landscape Plan



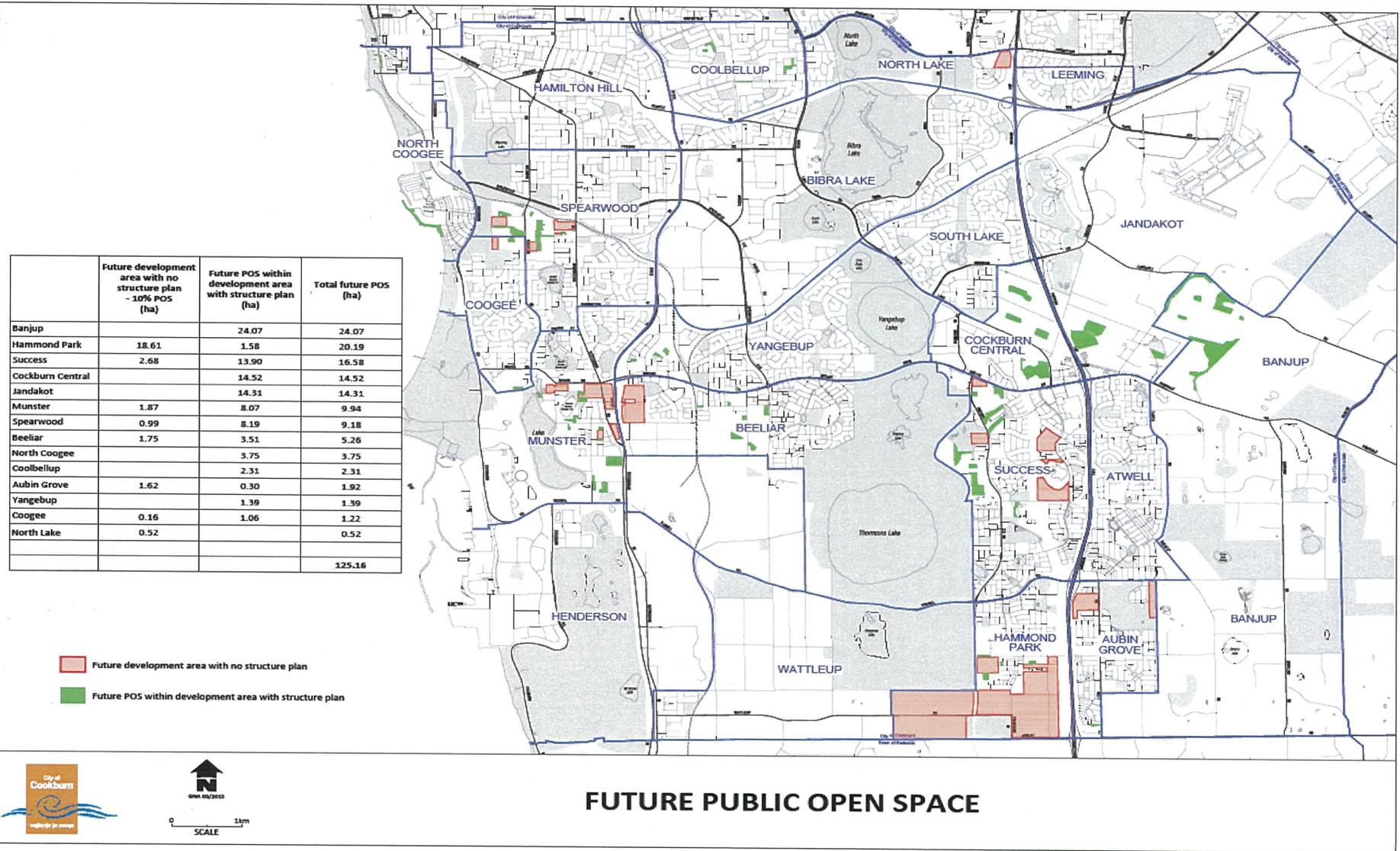
(Photo courtesy: Landcorp)

## 5 FUTURE PUBLIC OPEN SPACE

The City of Cockburn is one of WA's major coastal cities encompassing an area totalling 170 square kilometres. The City is comprised of a diverse mix of residential, retail, commercial, industrial and rural areas. The City has at its heart two chains of wetlands running north to south including a RAMSAR protected area.

The City of Cockburn is within the south west growth corridor with a current population of 101,973 (at the end of 2013-14). To meet this anticipated growth in population, the City has developed strategies for the development of greenfield lots and redevelopment (infill) of older areas. The City can expect to receive 125ha of POS from future development areas with the majority of these areas requiring embellishment to meet the community needs. The map adjacent identifies future POS.





## 6 POPULATION GROWTH & LOT SIZES

### Future Population

In recent years the City has experienced significant growth particularly in the southern precincts. There is capacity for continued growth in the short term with ongoing development of the greenfield areas of Success, Aubin Grove, Hammond Park and Jandakot. Further development through the re-zoning of market gardens in areas such as Munster and Spearwood will ensure sustained growth in the medium to long term.

More recently, development aimed at specific markets has been occurring along the coast, in particular attracting second and third homebuyers and 'empty nesters' and retirees, with the development of North Coogee and the Port Coogee marina. In the longer term, these patterns are expected to change. The availability of large tracts of 'greenfield' land and other suitable sites for traditional family housing is decreasing within the City and are likely to be substantially exhausted before 2030.

The development of higher density areas around Cockburn Central, with its rail links to inner Perth, Rockingham and Mandurah and the proposed revitalisation and regeneration of Hamilton Hill, Spearwood and Coolbellup are likely to provide housing opportunities to younger and older smaller households in higher density developments. The proposed Cockburn Coastal Structure Plan is also likely to have considerable impact in the longer term, providing further coastal development aimed principally at mature households and older families.



## Lot and Dwelling Sizes

As lot sizes are decreasing and dwellings are increasingly consuming the majority of allotments, residents and families have little front and rear yards to use as space for recreational activities. Therefore, local, neighbourhood, regional and district POS is increasing being used as areas for all ages.

Residential zoning density increases elevate the value of quality POS as areas where people can meet and participate in unstructured recreational activities.

The City continues to experience growth in a number of areas. This growth is being sustained in commercial, industrial and residential areas. The City continues to plan for this expansion by being able to provide an adequate level of infrastructure in public open space to support this level of development.

### Forecast dwellings and development

City of Cockburn	2011		2031		Change between 2011 and 2031	
Area	Number	%	Number	%	Number	%
City of Cockburn	36,068	100.0	52,870	100.0	+16,802	+46.6
Atwell	2,945	8.2	3,135	5.9	+190	+6.5
Aubin Grove - Banjup	2,024	5.6	3,132	5.9	+1,108	+54.7
Beeliar	2,075	5.8	3,114	5.9	+1,039	+50.1
Bibra Lake	2,391	6.6	2,581	4.9	+190	+7.9
Coogee - North Coogee	1,915	5.3	5,420	10.3	+3,505	+183.0
Coolbellup	2,402	6.7	2,617	4.9	+215	+9.0
Hamilton Hill	4,707	13.1	5,506	10.4	+799	+17.0
Hammond Park - Wattleup - Henderson	1,210	3.4	3,743	7.1	+2,533	+209.3
Jandakot	929	2.6	1,039	2.0	+110	+11.8
Leeming (part)	746	2.1	766	1.4	+20	+2.7
Munster	1,435	4.0	2,297	4.3	+862	+60.1
North Lake	482	1.3	588	1.1	+106	+22.0
South Lake - Cockburn Central	2,714	7.5	5,810	11.0	+3,096	+114.1
Spearwood	4,012	11.1	4,959	9.4	+947	+23.6
Success	3,227	8.9	4,701	8.9	+1,474	+45.7
Yangebup	2,776	7.7	3,384	6.4	+608	+21.9
Rottnest Island	78	0.2	78	0.1	0	0

*Population and household forecasts, 2011 to 2031, prepared by .id the population experts, November 2009.*

## 7 POS VALUE AND BENEFITS

### Value

POS provides a combination of social, economic and environmental value to the community. A principal role of POS is to support health, recreation and leisure functions including active pursuits such as organised sports, exercise, children's play and social activities, as well as encompassing passive uses such as quiet reflection in an attractive setting.

Environmental protection is an essential role of public open space, through habitat and biodiversity conservation and air and water quality management. It also provides a strong connection to nature for people living in highly urbanised environments. Natural areas are highly valued for their natural beauty, contribution to neighborhood character and community identity, cultural heritage value, tourism potential and the emotional connection people may attach to them.

### Health

Environment is a major influence on health and recent research has shown that we cannot continue to deal with illness only after it appears, or encourage individuals to change their attitudes and lifestyles, when the environments in which they live and work gives them little choice or support. Recent studies show that the leading causes of illness are no longer infectious diseases, but chronic ones such as cardiovascular disease, obesity, cancer, mental disorders, neurological and sensory disorders and chronic respiratory conditions, as well as injuries. The City is actively working to improve POS and parks in industrial and other commercial areas, to assist the working community in maintaining and improving their health.

State and local planning has recently been focusing on a holistic approach to public health by considering the overall impact that four environmental factors have on promoting and protecting the health and wellbeing of our communities. These are:

#### **Built / Physical**

Appropriate allocation, design and maintenance of public open spaces with associated built and natural infrastructure can promote healthy physical activity. Built infrastructure can also create opportunities for informal meeting places and provide a mechanism for connecting community members.

#### **Social Environment**

A well designed and maintained hierarchy of POS can create opportunities for community participation, leading to enhanced mental and physical well-being, including sporting clubs, walking groups, dog walkers and friends groups. Social engagement ensures connected communities and enables people to become involved in the programs offered by the City's Community Services Unit.

## **Economic Environment**

Community participation in activities that open space networks provide and promote will stimulate the building of a robust local economy. Community interaction through varying age ranges, gender and cultural diversity have the capacity to develop stronger communities, leading to investment in community infrastructure.

## **Natural Environment**

The presence, availability and ease of access to the natural environment provides the community with a healthier and more sustainable future, including improved water quality, protection of indigenous vegetation. Open space provides for important ecological aspects such as waterways, flora and fauna. Parks containing vegetation assist in combating greenhouse gas emissions. Vegetation in open space also provides habitat for wildlife, including corridor links to and along waterways and assists in the maintenance of water quality in urban waterways. The City's Natural Areas Management Strategy addresses the management of Natural Areas within the City of Cockburn.

## **Urban Liveability**

Well-designed and maintained areas of open space add to the liveability of our urban areas. They provide a 'green break' between houses, buildings, roads etc. Parks also bring aesthetic value to an area, particularly highly developed areas, providing visual relief from a harsh built environment.



## 8 TRENDS

### Multi-use of Public Open Space

Public open spaces often serve more than one purpose, yet have traditionally been allocated to a single or dominant use, for example football and/or cricket ovals, playgrounds or drainage. In most cases, single use POS does not ensure the most efficient use of land resources and opportunities.

The conservation of natural areas raises the issue of the tension between conserving the natural environment and promoting recreation in public open space. While there is an increasing concern for the environment from individuals or groups seeking to protect and conserve natural areas, there is an increasing number seeking to utilise or develop natural areas for a range of activities.

Whilst the City seeks to establish a balance between conservation and recreation, it is acknowledged that in some instances these different objectives cannot be reconciled. For example, the management of small bushland sites, which are generally not sustainable and require extensive resources if they are to be maintained. This issue again highlights the need for appropriate classification and management of spaces.

There are also competing uses among recreational and sporting activities that can be difficult to resolve. For example, a number of different sports codes or clubs wanting to use the same reserve, resulting in overuse, impacts on ground condition and amenity or exclusion of some groups. There may also be competing uses relating to different functions of a space that may not compliment each other, such

as quiet reflection, dog exercise and organised sport.



These issues must be addressed on a number of levels, firstly through ensuring the adequate provision of public open space — both in total area and in different types of opportunities to meet a range of diverse community

needs (an understanding of community need and the space needed to meet these needs is also required). Secondly this issue can be addressed through the design of open spaces, through a planned approach to public open space zones and location and siting of facilities. Uses can change over time as a result of emerging community needs, trends, issues and demographics and the need to design spaces for flexibility and variety of use is important (multi-use spaces). There also needs to be due consideration and understanding of the key needs of a range of users. For example,

the dimensions of playing fields for different sports are relatively fixed to meet training and match play requirements. The issue of competing uses must also be addressed through ongoing innovative management of public open spaces.

## **Sustainability**

The sustainability concept is more than just an influential trend; it is the foundation principle on which planning and decision-making should be based. Sustainability objectives affect the future provision of open space in several ways including:

- **Resource Use** — The need for sustainable use of natural and community resources, including facility provision, land and water use, conservation of habitats and accountable public spending on provision and maintenance.
- **Walkability** — Reducing car use and pollution and promoting public health and equitable access via a range of transportation options through the appropriate location and distribution of open space.
- **Multi-Use** — Developing spaces that can be used for a variety of uses, including sport, recreation, leisure, conservation and water management to enable the most efficient land use. This usually requires larger open space areas that can accommodate a number of activities and uses.
- **Community Building** — Providing places for social interaction and connectivity, community events and developing a civic heart of neighbourhoods and suburbs.

## **Lifestyle Changes**

Although lifestyle is an individual choice, it is evident that certain lifestyle trends are evolving in Australian society that influences public open space provision. These include:

- Changes in recreational pursuits particularly with an emphasis on sporting and recreational opportunities, with a need for open space areas to accommodate changing interests and popular pursuits. The popularity of the typical Australian 'outdoor lifestyle' remains evident. Additionally, there is increasing competition for sport and recreation time from personal computers, entertainment systems and private recreation facilities.
- Workforce changes — longer working hours for many resulting in less recreational time, yet also more flexible and casual hours resulting in different sectors of the community recreating at different times. Current economic prosperity also provides many with increased disposable income to spend on recreational pursuits.
- The trend to construct larger dwellings on smaller residential allotments makes POS increasingly more important in the modern community. Neighbourhood and district POS becomes the pseudo front and back yard for many children, as these spaces support their physical activity whether for active sport or unstructured recreation.

## **Risk Management**

Risk management is paramount to the provision of services and infrastructure within the City's POS. Risk management is a critical factor when designing and constructing new POS and ensuring that ongoing maintenance can be performed efficiently and effectively. With an increasingly litigious society, the City needs to balance the needs of the community and risks associated when new or replacement infrastructure is installed. In addition this risk mitigation environment is impacting on sporting and community groups to comprehend legislation, practices and policies to mitigate their liabilities.

## **Communication**

There is a continual need to disseminate information to the community regarding POS management, improvements and general servicing of parks. Although the City has a myriad of communication methods, the POS strategy will be the guiding document for enabling the community to understand the City's POS values, objectives and actions.

## **Linkages**

It is important to maintain and improve ecological linkages and corridors within the POS network and other areas of ecological significance. This is necessary to help maintain the diversity and vibrancy of the City's POS and ecological systems and to help integrate areas of POS within the broader urban and industrial landscapes.

## **Minimising Impact on Environmental Areas**

An increased environmental awareness and concern about environmental quality in terms of natural areas is essential. Retention of mature trees and new plantings in POS also presents an opportunity to reduce pollution and improve air quality. There are also opportunities to address water pollution through incorporating open spaces into the water management system, such as the use of constructed wetlands for filtration and pollution attenuation.

## **Technology**

Managers of POS are increasingly embracing new and emerging technology to apply best practice management techniques. Technology needs to be carefully implemented to ensure cohesion with the City's existing information services infrastructure and ongoing management can be facilitated through current budgets. Emerging technology elements include:

- Central irrigation control
- Weather stations
- Works and assets mobility
- Renewable energy assets
- Lighting upgrades and conversions

## Skate Parks

The City currently has two skate parks with a third being developed in 2014-15. These sites are distributed within each of the City's three wards and are located on POS conducive to the hierarchy. The demand for skate parks is on the increase across the City by individuals and community groups.

Although skate parks create an environment conducive to strong links with the community, they need to be carefully balanced with the long term values and function of the particular POS. The City's Youth Service Plan addresses the provision of skate parks throughout the City in consultation with the Parks & Environment Business Unit. The POS strategy identifies that the preferred provision of skate parks is within District or Regional Open Spaces.

## Wellbeing and Obesity

Health and wellbeing can be directly improved through effective POS management. In particular:

- The Department of Health predicts that life expectancy is expected to increase, however the total disease burden will increase with a predicted increase in population size and age. Unfortunately, it is predicted that the burden attributed to physical inactivity and obesity will be the highest of the major risk factors especially among females.
- The community in general has increased its awareness of the benefits of physical activity that has led to more regular exercise and the need for spaces for recreation, cycling and walking. This is supported by Federal and State Government promotion of 'active living' to promote and enable physical activity.



The City will need to provide infrastructure, exercise equipment, within POS that is readily accessible and functional to increase the ability of the community to undertake activities to improve health and wellbeing. The City has undertaken a review of its open space to encourage greater physical activity.

Through the City's Public Health Plan it has been identified POS can provide formal or informal recreational pursuits to reduce current and anticipated future obesity levels. One key element is the investment in outdoor exercise equipment to encourage activity in a natural environment. The equipment provides community members an approach to physical activity without being overwhelmed or influenced by individuals. Exercise equipment will be delivered to designated Neighbourhood and District Parks in each suburb to facilitate larger catchments of the community.

### **Dog Exercise Parks**

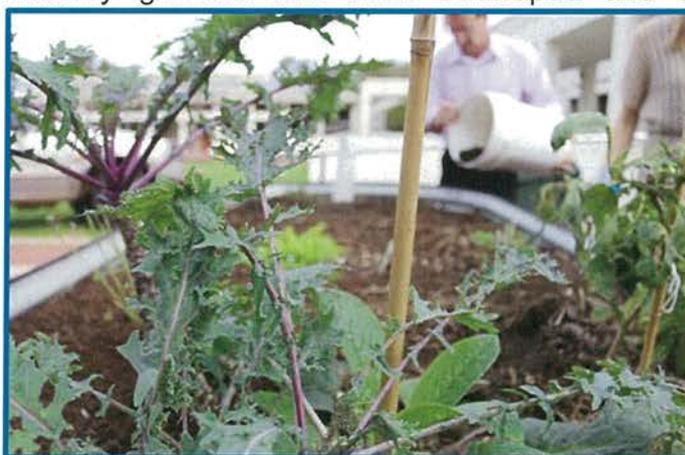
There is an increasing demand for dog exercise parks located within the City's POS network. Dog exercise parks require a relatively large land area to enable dogs to run unleashed, provided they are under the control of the owner, and include dedicated areas for structured activities. The location of dog exercise parks requires extensive community consultation to ensure full acceptance by the community and that the City is cognisant of its ongoing management.

### **Community Gardens**

Community gardens provide an environment for people to produce food and actively build the health and wellbeing of the community through shared activities.

The City is supportive of the establishment of these gardens, however the onus on creation and ongoing management must be through an incorporated body. A policy on the establishment of a community gardens has been developed and a supplementary set of guidelines have been provided for ease of reference.

A feasibility study into the appointment of a Community Garden Officer to facilitate community gardens will be completed during the course of this strategy, in response to repeated requests by the community for the City to take a more active role in their establishment.



### **Playground Shade Sails**

The City receives numerous requests from the community and interested groups for the provision of shade sails over playground equipment. This demand has been influenced by POS embellished by new developments and the Cancer Council of Australia suggesting that 90% of all cancers could be prevented if sun protection measures were adopted. In addition there is a general inference that shade provision should be a standard piece of infrastructure and installed with all new playground equipment.

Management of shade sail provision to playground equipment is facilitated by the Playground Shade Sail Strategy 2013-2023. This strategy outlines a criterion for shade provision and a detailed implementation program over a 10 year period. The criterion for shade sail provisions aligns with the POS classification framework outlined in this strategy.

### **Multipurpose Courts**

The City's community services division has identified a growth in the need for multipurpose courts within POS. These half size courts facilitate basketball, netball, handball, mini tennis and other recreational activities on a hard surface. Although the provision of these courts is relatively low across the City's existing POS network, a framework for development is required. This strategy has determined their preferred location within the POS classification hierarchy to ensure they can be readily accessed by the local and wider community.

### **Memorial Gardens / Plaques**

The City receives requests for the installation of plaques attached to park infrastructure or as standalone structures within the POS. The plaques are primarily a dedication to eminent men or women who have contributed to the City through business, sporting or community work. In addition to plaques, memorial gardens are being created to develop areas of reflection or dedication to an individual or institution. The locations of these sites are predominately focused on areas where high patronage prevails, enabling good accessibility and the provision of functional park infrastructure. The development of a policy and guidelines are needed to guide the future placement of memorial gardens and plaques.

### **Lighting**

Lighting helps make public open spaces more enjoyable and functional for the community to utilise during late evening or early morning. Lighting is provided to high use activity nodes and connecting pathways to create a safer environment. Lighting can create an atmosphere which can increase the feel of the City and mitigate inappropriate behaviour by individuals.

Whilst residents generally request lighting when anti-social behaviour is prevalent, sometimes requests are purely for self-interest. The latter approach places significant impost on the City and limits the opportunity to areas where lighting should be installed.

The City is currently preparing Open Space Lighting Guidelines to ensure the community can achieve the benefits associated with lighting. The guidelines will outline key objectives, where lighting should be installed and the types of lighting. The guidelines will also enable clarity for internal officers, developers and the community.

## Storm Water Retention

Through the Liveable Neighbourhood Policy up to 2% of the available POS within a new development can be utilised for storm water retention. The policy outlines a set of conditions which ensure storm water percolates into the ground within a specified period of time. However evidence suggests these periods of inundation are exceeding the guidelines and thereby reducing the availability of function recreational areas.

In addition to Liveable Neighbourhoods, the City's Land Management Strategy identifies the potential sale of drainage sumps should the feasibility study confirm relocation of the storm water to a POS is viable. Although this application has merit it is increasingly impacting on the function and recreational components of the POS.

To ensure the community is not adversely impacted by the facilitation of stormwater in our POS a set of engineering solutions is required. These solutions will enable the function and recreational components of the POS to be retained in perpetuity.

## Playground Fencing

The City receives requests from the community for playgrounds to be fully fenced in close proximity to the road network. In addition, fencing is also sought by parents with children who are not developmentally advanced so they can feel comfortable when left to play by themselves. To ensure the City can meet the expectations of these community interests a playground fencing policy will be developed.



## 9 CLASSIFICATION FRAMEWORK FOR PUBLIC OPEN SPACE

The classification framework for the City's public open space will follow the recently accepted framework by the Western Australian Department of Sport and Recreation. The framework was developed through extensive consultation with the Department of planning, local governments (including Cockburn), Parks and Leisure WA and professional industry groups. The framework will enable universally recognised terms to be applied to each open space during planning the process and management applications. The framework is structured in two central categories;

Function (primary use and activities) has three primary types of open space:

- Recreation Spaces
- Sport Spaces
- Nature Spaces

Catchment Hierarchy (size and distance to open space) has four categories:

- District Open Spaces
- Regional Open Space
- Neighbourhood Open Space
- Local Open Space

The framework has been applied to open spaces management by the City to inform future development and management applications. A detailed breakdown of the two categories is provided below.

### Recreation Spaces

Recreation spaces provide an environment for informal play and physical activity, relaxation and social interaction. Recreation spaces enhance the physical and mental health through activity and are accessed by the community for play, socialising, intrinsic rewards and stimulation. Recreation spaces are framed through open parklands with a scattering of garden beds, providing corridor links, community gardens and also providing community facilities or forming parts of a civic square.

### Sporting Spaces

Sporting spaces provide a setting for formal structured sporting activities such as team competitions, physical skill development and training. Sporting spaces are designed to accommodate playing surfaces, buffer zones and infrastructure for sporting pursuits. Sporting players and spectators attend these locations for the express purpose of organised sport, training or viewing the activities on display. These spaces are easily accessed by local and broader community members.

### Nature Spaces

Nature spaces provide an atmosphere where people can enjoy the natural environment and care for the local biodiversity. Nature spaces provide the opportunity for minor recreational activities including walking, cycling, and general

play and exploring the natural elements. Nature spaces include bushlands, coastal environments, wetlands and natural features in the landscape. These spaces are managed to ensure recreational access whilst protecting local ecological and biodiversity values.

## District Open Space

District open space is a term used to describe areas that generally serve the whole municipality or reasonably several suburbs within the City. The expectation is that residents are prepared to drive to these places. These locations may have significance for the municipality as a whole or a substantial part of it due to size, function or diversity. Municipal level public open spaces may be significant for their conservation or cultural values.



### District Open Spaces Criteria:

- Generally within 2km or a 5 minute drive
- Typical size >5ha
- Centrally located
- Accommodate required sporting dimensions for a variety of codes
- Be collocated with schools
- Located close to district distributor roads
- Services by public transport
- Accessible by car, cycling or walking
- Good footpath connectivity with local community
- Good passive surveillance

### Development Status

District open spaces are master planned though the strategic planning stage and are fully developed to enable immediate use by the local and wider community. Where additional features are required after this process, community and club consultation will be completed and subject to funding availability.

### Level of Embellishment

- Clubrooms / Public Toilets
- Playground Equipment & Shade Structure
- Signs ( informative / interpretive/ entrance)
- Picnic Setting
- Shelters / Gazebos

- Skate Parks
- Advanced landscaping
- Perimeter fencing
- Drinking Fountains
- Irrigation
- Footpaths
- Bins
- Lighting
- Sporting Infrastructure
- Car park

## **Regional Open Space**

Regional Open Space are large signature spaces, parks or reserves that are of high recreational, leisure, social, environmental and tourism value. Regional open space generally serves catchments broader than one Local Government Area (LGA). They may host significant sites or species of flora and fauna. By virtue of their size, diversity of opportunities or level of development, they may attract high numbers of people, including tourists in some cases. Residents will not necessarily expect these types of spaces to be available in each LGA.

### Regional Open Spaces Criteria:

- Significant biodiversity & environmental values
- Provide for organised sporting activities
- Attracts visitors from outside the LGA
- Size if variable and can exceed 20ha
- Accessed by vehicle or public transport
- Parking provision is essential
- Well connected to major road networks
- House community events

### Development Status

Regional Open Spaces are typically developed in accordance with a master plan with community consultation. Currently the City has Five (5) regional open spaces: Manning Park, Bibra Lake, Market Garden Swamp (North and South), C Y O'Connor Beach and Coogee Beach. Bibra Lake is currently managed in accordance with the Bibra Lake Management Plan, whilst Coogee Beach has a management plan for the coastal component and a new plan is being formulated for the recreational areas. Due to the significant nature of these reserves funding is provided through Municipal rate income, Developer contribution funds and grant funds where available. Regional open space development is permanently at an advanced level. Manning Park requires the development of a management strategy to address the diversity of the site and ensure development is addressed systematically in accordance with key objectives.

## Level of Embellishment

- *Playground Equipment & Shade Structure*
  - *Exercise equipment*
  - *Skate Parks*
  - *Car park*
  - *Public toilets*
  - *CCTV*
  - *Shelters / Gazebos*
  - *Advanced landscaping*
  - *Perimeter fencing*
  - *Multipurpose Courts*
  - *Drinking Fountains*
  - *Designated Dog Exercise Area*
  - *Irrigation*
  - *Signs ( informative / interpretive/ entrance)*
  - *Footpaths*
  - *Lighting*
  - *Conservation areas*
  - *Sporting Infrastructure ( ½ Court Basketball, set football / soccer goals, etc.)*

## Neighbourhood Open Space

Neighbourhood open spaces serve as places of leisure and social interaction for the community, offering a variety of recreational activities and facilities. Parks also frame community facilities and can function as community spaces, such as town squares. Neighbourhood open spaces also enable the community to connect to the environment and provide habitat for fauna in dense urban residential areas.



### Neighbourhood Open Spaces Criteria:

- Within 800m or a 10 minute walk of the surrounding community
- Central with the surrounding neighbourhood
- Accessibility friendly
- Good pedestrian and cycling connections
- Typical size 1ha-5ha
- Support good passive surveillance
- Create a sense of place

## Development Status

The development of neighbourhood open spaces will range from primary to advanced levels of embellishment. Basic aspects ensure the park is functional and provides a range of recreational infrastructure for the community. Advanced development includes additional elements that support an increase in social interaction and recreational pursuits based on a park's location within a suburb. A change in status will require consultation with the community and be subject to capital and operational funding availability.

### **Primary Level Embellishment**

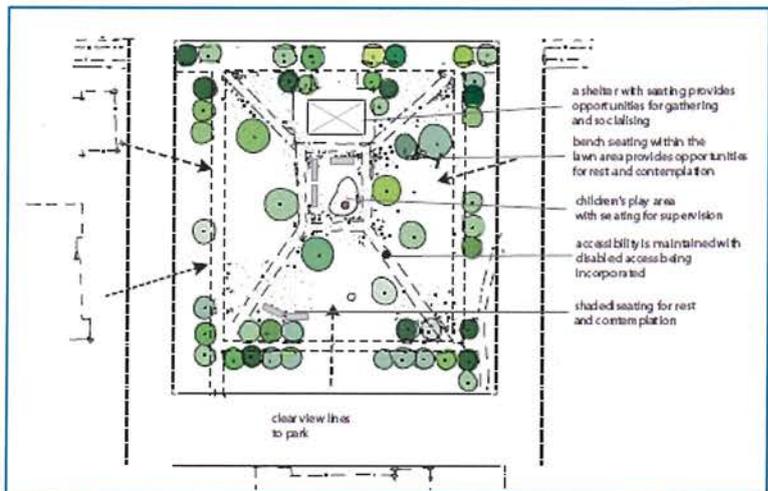
- *Irrigated grass*
- *Playground equipment*
- *Picnic settings*
- *Shelters / Gazebo's*
- *Major landscaping*
- *Perimeter fencing*
- *Footpaths*
- *Bins, including dog poo stations*
- *Signs*
- *Sporting Infrastructure ( ½ Court Basketball, set football / soccer goals, etc.)*

### **Advanced level of Embellishment**

- *Playground Shade Sails*
- *Exercise Equipment*
- *Skate Parks*
- *Drinking Fountain*
- *BBQs*
- *Lighting*
- *Public Toilets (feasibility study per site to be completed)*
- *Major landscaping*
- *Designated Dog Exercise Areas*

## **Local Open Space**

Local open space describes small local parks that are predominantly provided for the needs of the immediate residential population. They are predominately for casual and informal recreation by the community and easily accessed by bicycle or foot. Local open spaces include places for play, informal recreation and sport, at a relatively low level of complexity. The provision of parking is generally not required.



### Local Open Spaces Criteria:

- Generally within 400m walking distance of homes in the local area
- Accessibility friendly, no major road impediments
- Good pedestrian and cycling connections
- Typical size 0.4ha-1ha
- Support good passive surveillance
- Create a sense of place
- May include the preservation of local biodiversity

### Development Status

The development of local open spaces ranges from primary to advanced embellishment. Primary aspects will ensure parks are functional and provide basic infrastructure for the community. Advanced development include additional elements that have been determined essential, due to an under provision of public open space in a suburb or as a result of urban infill in the surrounding precinct. A change in status will require consultation with the community and be subject to capital and operational funding availability.

### **Primary Level Embellishment**

- *Unirrigated grass*
- *Minor landscaping*
- *Local biodiversity*
- *Seating*
- *Park name sign*

### **Advanced Level of Embellishment**

- *Irrigated or unirrigated grass*
- *Playground equipment*
- *Major landscaping*
- *Footpaths*
- *Bins*
- *Park name sign*



## 10 POS MANAGEMENT

### Strategic Approach

POS can no longer be viewed in isolation, as more importance is being placed on the better integration of strategies and programs, partnerships and effective community involvement.

There is an increased understanding of the vital role of green infrastructure in providing environmental services such as urban cooling and habitat for wildlife. This has now become a key part of urban planning and should be considered a fundamental factor in all built development.

A comprehensive open space strategy spanning the planning, design, management and maintenance of open spaces is an essential tool for delivering this multi-functional network of spaces as well as the broader objectives of health, social inclusion and community cohesion – in short, the creation of excellent places.

### Strategic Key Performance Indicators

Strategic Key Performance Indicators (KPIs) can be defined as measures that provide senior management with the most important information to enable them and their stakeholders to understand the performance of an organisation. This process enables the management of POS to be transparent and provides a mechanism for improvement. KPIs should clearly link to the strategic objectives of the organisation and help monitor the performance of the business unit.

### Managing Existing POS

The City currently manages its POS network through a workforce committed to ensuring high presentation and a safe and functional environment for the community's use. The workforce is continually reviewed in accordance with the Workforce Plan.

Not all works can be achieved through the internal workforce and such the City engages contractors to supplement this void. The City primarily engages the services of playground specialists, irrigation specialists, turf consultants, Arboriculturalist, etc.

### Managing New POS

New developments require the provision of 10% POS under the state planning framework, which ensures that planning principles seek to provide a range of open spaces that are user friendly, safe and conveniently located. It does not consider a range of other key considerations or issues.

There is a need to ensure that all future public open space provision and developments undertaken through subdivision development and upgrades align with this Public Open Space Strategy. It is vital that the City proactively manages developer public open space proposals and contributions accordingly.

The City has a 'POS Development Guide' which provides guidance to developers on the typical embellishment levels of POS within the City. The guide provides a framework for approvals and ensures the City receives asset details which are easily uploaded into databases for reporting through the Parks and Environment Asset Management Plan.

### **Challenges in Managing POS**

Some of the key challenges for the City in relation to planning, management and operation of POS in the coming decade include:

- Ensuring that the quality of POS is maintained and further enhanced to ensure that it meet the community's changing needs into the future
- Striking an appropriate balance between protecting the City's environmental values as well as providing appropriate community facilities and infrastructure
- The continued onset of climate change and the need to find alternative ways to supply water to POS
- Coping with increased infill development where reliance on POS will be highly important to resident's wellbeing
- Meeting the recreational and POS need of an ageing population
- Providing infrastructure that adequately caters for multi-use activities
- Providing safe connections between parks, reserves and POS
- Implementing sustainable management and operational practices

### **POS Rationalisation**

Decisions need to be made where tiny or small allotments in isolation are surplus to requirements and may not make any meaningful contribution to the overall POS hierarchy into the future. Detailed investigations are required to determine whether these areas may be valuable assets in the future when the pressure of urban development increases. In some cases it may be reasonable to dispose of a number of small areas of undeveloped POS and purchase a larger allotment that better caters for the recreational needs of a suburb or larger area of the City. An integrated approach with the City's Strategic Planning Division will be required to further this.

### **Process Improvement**

Process improvement is a systematic approach to ensure an organisation enhances its principal procedures to achieve more efficient results. It is imperative that a continual process is engendered to identify, analyse and improve business process to meet organisational goals and objectives. The continual review ensures efficiencies in resources are constantly modified to a changing business environment.

### **Fire Management**

The City is required to ensure POS management is compliant with the Bush Fires Act 1954. The City is required to ensure POS, which is not formally manicured and is adjacent to residential property, has fire breaks or is clear of vegetation to mitigate fire risk. An increased focus on fire management will require additional resources to ensure the City can adequately comply with legislative requirements.

## Safety and Risk Management

All reasonable and practical efforts are taken to minimise risks to visitors and users of areas of POS, however there is always an element of risk in outdoor recreation activities.

Visitor and user safety are integral components of works programs and capital developments within POS. Recreation facilities and amenities for visitors will be developed and maintained in accordance with relevant Australian Design Standards.

## Asset Management

The Parks and Environment Asset Management Plan 2014 outline the management of assets in compliance with regulatory requirements and highlight the funding necessary to deliver the required levels of service. The asset management plan requires continual updating to include new POS, changes in POS classification and changes to service levels through legislative changes to infrastructure management.

## Level of Service

To ensure POS is maintained to a level which is proportionate to community needs, industry standards and financial limitations, a service level matrix has been developed. The matrix outlines the open space classification and service frequency for each of the key maintenance activities required. These levels of service ensure a consistent presentation to the open space depending on its classification. Parks are maintained to ensure they are continually used, reach their anticipated lifespans and compensation claims to the City are reduced. Appendix 3 POS Classification Level of Service per annum sets out the service level for key infrastructure, turf and landscaped areas.

## Improvement Plan

Development of public open space requires assessment of existing infrastructure and development plans, impacts from population growth, demographic changes, revitalisation strategies, emerging issues and distribution of open space across the City and each suburb. The development of the POS Strategy has enabled the formulation of a 10 year improvement plan to ensure commensurate investment each year. The plan outlines the function and classification of each open space, key assets and current development level as identified for each catchment hierarchy. The improvement plan outlines the current recreational components of each POS and lists their future development status. Development options are listed to ensure alignment with key aspects of the development status for each classification. The plan is included in Appendix 5



# 11 STREETSAPES

## Streetscape Strategic Approach

Streetscapes within the City of Cockburn frame the road network to create aesthetically pleasing and inviting streets. Landscaping major road corridors can increase people's daily travel experience and reduce the speed of traffic. The diversity of landscape for these road corridors is contingent on the road hierarchy and how it changes from urban to rural to industrial. Streetscapes also provide ecological corridors which encourage the movement of native fauna from one bushland environment to another or from bushland environments to parks.

## Streetscape Rationale

The City has previously managed the development of streetscapes through the Greening Plan, which was a strategy for the conservation and management of bushland, amenity and rationalised approach to the cleared requirements or parks, major and minor roads reserves, non-council land and private land. To ensure greater streamlining of corporate strategic documents, the Greening Plan will now be absorbed into the Natural Area Management Strategy 2012-2022 and this POS Strategy.

## Urban Forestry

Urban Forestry is used to describe the management and care of the totality of trees and shrubs in a built environment and is measured as a canopy cover percentage of the total area. Trees are seen as a highly critical component of urban infrastructure. Trees command a symbolic and physical presence which can frame community entry statements and individual residential properties. Mature trees can evoke community passion and create a sense of place and ownership.

Urban forests have clear environmental, social and economic benefits. Large tree canopies can shade road networks and associated infrastructure reducing the absorption of heat and increasing the lifetime of the asset. In addition shade can reduce the requirement for air conditioners in homes or buildings, thereby reducing demand on energy and reducing greenhouse gas emissions. The trees' structural formation can slow wind speeds and during intense heats can cool the air. Urban forests improve air quality, absorb rainwater and lead to improvements in biodiversity.

Research has identified the effect of well-manicured trees and landscaped environments on increased investment in business and residential areas. In addition residential property prices are generally higher surrounding POS with large mature trees or bushland environments.

The evaluation of urban forests to capture their economic and social benefit to the community comes in a variety of methods. The City has been liaising with the CSIRO to enable further analysis of the tree canopy across residential, commercial, private and state government land areas. The information will enable the prioritisation of future tree planting programs throughout the City's Streetscapes and POS. In

addition the City will have the ability to leverage businesses and state government agencies to achieve increases in canopy cover to benefit the entire community.

### **Streetscape Development**

Landscaping streetscapes is an established practice on street verges and median strips. This program increases the number of street trees and compensates for the loss of trees and vegetation due to urban development. Landscape development is framed on ecological corridors, themed precinct approaches and major roads following upgrades.

Subdivision plans include landscape plans for the placement of trees and vegetation within streetscapes and areas of public open space. These plans are subject to the planning approval process with the Parks Manager approving street tree species selection.

### **Streetscapes as Ecological Linkages**

The Natural Areas Management Strategy 2012-2022 provides the framework for ecological linkages through the City. These linkages are contiguous natural areas or revegetated areas that directly connect larger areas of natural bushland to allow movement of organisms. The linkages identified are as follows:

- North /south road linkages
- East/west road linkages
- Rail linkages
- Western power transmission line linkages
- BP refinery oil pipe line linkages
- Private rural lot linkages
- Residential linkages

The development of streetscapes will help facilitate ecological corridors and create opportunities for fauna movement through diversity of plant species.

### **Themed Landscape Precinct Approach**

Related to the principle of a consistent and coordinated theme for individual streets is the concept of "precinct" planting. All new planting will be based on a precinct approach where species selection and location of planting will reinforce the distinct physical character of each area and be responsive to its unique environmental conditions. A number of key species will be identified suitable

#### **Precinct 1 - Coastal Landscapes**

Consist of undulating dunes close to the coast which are generally devoid of tall vegetation except in residential/industrial areas. Most of the heritage sites within the City occur within the coastal landscape. This landscape offers a number of panoramic views from existing roads.

#### **Precinct 2 - Inter-dunal Areas**

Consist of residential, commercial, industrial, market gardens and open space areas in areas between dune ridges. Remaining bushland in this area is

generally tall open forests of Tuart-Jarrah-Marri. This landscape offers some panoramic views from existing roads.

### **Precinct 3 - Wetland Landscapes**

Wetland areas include surrounding residential, industrial or parkland areas. The native vegetation is generally Flooded Gum and Melaleuca Woodlands mixed with other wetland-dependent species.

### **Precinct 4 - Bushland Plains**

Generally rural land use with open paddocks, some residential, special use or public open space areas. Bushland areas include Low Open Sclerophyll Forests comprising of Banksia-Sheoak-Pricklybark and Heath.



## 12 ROADS

### Major Road Streetscapes

The City of Cockburn has a number of major roads with significant environmental characteristics and streetscapes aligned to their cultural diversity and history.

#### Armadale Road

Armadale Rd is an east / west link emanating at Warton Rd and terminating at Kwinana Freeway.

The majority of this road is in the rural areas of Banjup and is framed by natural vegetation. Though the future “Calleya” development in Banjup will alter the streetscape environment, it will require a treatment aligned with its key function as an ecological corridor. Landscaping will form part of the subdivision conditions, reducing the City’s requirement for improvements.



#### Beelias Drive

Beelias Drive is an east/west link which frames the entry to the City of Cockburn from the Kwinana Freeway through to Rockingham Rd. Beelias Drive will be extended to Cockburn Road providing a strategic link to the coast. Beelias Drive has been identified as an ecological corridor and has been landscaped accordingly in specific sections. Landscaping of the section from Kwinana Freeway and Poletti Way will be completed during the 2014/15 financial year. Landscaping of Beelias Drive will continue following completion of the dual carriageway.

#### Cockburn Road

Cockburn Road navigates its way through the City in a North/South orientation with views of the Cockburn Coast. Cockburn Rd has been upgraded to a dual carriageway adjacent to the Port Coogee Development and landscaped consistent with coastal vegetation. Streetscaping of the road to the north of Port Coogee will be completed during construction of the Cockburn Coast development. Landscaping to the large southern portion of the road reservation will be determined by future expansion of the road. The streetscape will be tailored to the coastal environment with coastal tolerant species planted.

#### Hammond Road

Hammond Rd runs North/South and is a recognised ecological corridor. The streetscape has been landscaped to ensure connectivity to natural areas that are adjacent to this road and at either end. Future development to the south of Russell Rd will commence following development of the dual carriageway. Additional tree planting will be undertaken on the section to the north of Beelias Drive.

## North Lake Road

North Lake Road commences at Kwinana Freeway and meanders through in a north easterly direction to Cockburn's boundary with the City of Melville at Winterfold Rd. The North Lake Rd streetscape is consistent with the requirements of its status as an ecological corridor and upgraded at specific intersecting roads. Landscape improvements to the section from Hammond Rd to Midgegooroo Avenue will be completed following current developments. Improvements to the streetscape from Phoenix Road to Farrington Rd will be undertaken following completion of the Friendship Way project, which commemorates the City's sister cities relationships.

## Phoenix Rd

Phoenix Avenue is an East/West road network linking North Lake Road to Hamilton Road. The manicured features dominate the western portion whilst a natural environment extends to the east. These two different aspects coexist and provide sympathetic streetscape for the pedestrian and driver. Public art prevails within the median island through the west section creating interest. Improvements to the western portion, Stock Rd to Phoenix Rd have been identified and listed following completion of the Friendship Way project.

## Rockingham Road

Rockingham Rd is an integral link that connects with Stock Rd to the east and the Cockburn Rd border with the City of Fremantle to the west. Rockingham Road traverses through residential and commercial properties and has a restricted streetscape appearance due to no median island. The Phoenix Revitalisation Strategy identifies upgrades to Rockingham Road between Phoenix Road Spearwood Avenue, which will create an environment conducive to pedestrian movement and an attractive streetscape. The remaining areas of Rockingham Road will be planted with trees to improve the nature of the road network and provide shade to footpaths provided on both sides of the road.

## Russell Road

Russell Road is an East/West link that traverses through Success and Wattleup enabling traffic movements from the Kwinana Freeway to Stock Road. Russell Road is currently landscaped from the Freeway to Hammond Road providing an attractive and aesthetically pleasing aspect. Russell Road is another major road providing the framework for

an ecological corridor as it passes between Thompsons Lake Reserve and Harry Waring Marsupial Reserve. Future landscaping of Russell Road will be premised on the future development of the road to dual carriageway from Hammond to Stock Road.



## Spearwood Avenue

Spearwood Avenue is home to what is commonly referred to as Friendship Way, commencing at Cockburn Rd and terminating at Beeliar Drive. The Friendship Way Master Plan outlines for the embellishment of the streetscape aligned with the Sister Cities program. The relationship extends to:

- City of Mobile, USA
- City of Yueyang, China
- City of Split, Croatia

Friendship Way includes a section dedicated to the local Nyungar people of the south coast of WA and a dedication to Australian history. The Friendship Way Program will continue to be developed through to 2018/19.



## Stock Road

Stock Road dissects the City in a north/south alignment commencing at the border with the City of Kwinana and continuing through to the City of Fremantle. Stock Road is managed and maintained by Main Roads WA. The City intends to begin a dialogue with Main Roads WA in relation to the retention of native vegetation and future landscaping as the City has identified this road as an important ecological corridor.

## Warton Road

Warton Road is located on the boundary with the City of Armadale and was recently developed into a dual carriageway to Armadale Road. The City of Armadale will be landscaping the verge and median within the next three years subject to available funding. It is anticipated that ongoing maintenance of the streetscape will be shared between the two cities.

## **Minor Road Streetscapes**

The City has a large volume of landscaped streets in the form of roundabouts, median islands, public access ways, car parks and entry statements. These landscaped environments create a feature of the road environment and reduce the heat island effect created by surrounding hard surfaces. Landscape to these streetscapes is predominately minor to ensure standard vision clearance standards are achieved and meet current levels of service. Although these sites are restricted in landscape they provide distinct characteristics that define suburbs and entry points into the City. Public access ways have minimal landscaping due to the difficulty in access, risk associated with limb failure and potential damage to fences owned by the adjacent property owner. Car parks provide the destination and drop off point for residents at community facilities and are landscaped to achieve a large canopy cover over the parking surface.

## **Road Reserve Management**

This Strategy considers the City's road verges to be an essential part of the public domain, being the interface between private and public activities and interests. For this reason the management of the road verge has been included and considered as an adjunct to the POS.

Management of road reserves involves maintaining the vegetation contained within the verges and median islands of the City's road reserves. Primarily the service includes the mowing of verges, selective weed control through herbicide applications, and responding to customer requests. The delivery of works is through a service level that is commensurate with the current service unit funding levels.

### **Priority One (1) Road Reserves**

Priority One (1) road reserves are primarily district distributor roads throughout the City which include Beeliar Drive, Farrington Road, Hamilton Road, Coolbellup Avenue, North Lake Road, Phoenix Road, Spearwood Avenue. These roads define the City and are high profile in nature with large traffic volumes thus requiring a heightened appearance. Maintenance of these verges is from kerb to property line and includes unkempt verges adjacent to residential properties where applicable.

### **Priority Two (2) Road Reserves**

Priority Two (2) roads are defined as local distributor roads located throughout the City which include Tapper Road, Southwell Crescent, Elderberry Drive, etc. Maintenance of these road reserves is predominately kerb to property line and limited to verges that are not adjacent to residential property.

### **Rural Road Reserves**

Rural Roads are primarily local roads within the rural areas of the City which predominately include the suburbs of Banjup, Beeliar, Jandakot, and Wattleup. A small number of rural roads are still maintained within other suburbs of the City however these are reducing through sub divisional development. These rural road

reserves are adjacent to large private properties and are in general not maintained by property owners.

### **Industrial Road Reserves**

Industrial roads are contained within the suburbs of Bibra Lake, Cockburn Central and Henderson. Maintenance of the verges within industrial areas is primarily contained within the older sections, as new developments consider the verge as an entry point to their business and therefore maintained by the property owner. Servicing of these areas can provide difficulties for contractors based on moderate to high vehicle movement and the propensity of businesses or their customers to park on the verges.

### **Unkempt Verges**

Unkempt verges are identified as verges which are not regularly maintained by the adjacent property owner. Maintenance of an individual verge will be performed up to a maximum of four times per annum based on current resourcing levels. Works in this category can include one or more of the following maintenance activities: mowing, pruning of vegetation and weed control.

### **Level of Service**

The level of service for streetscapes has been aligned to industry standards and the inherent knowledge of vegetation growth rates in specific locations. These levels of service ensure the minimum frequencies are achieved with limited scope to add services due to changing environmental conditions. Funding for streetscapes has typically been driven by financial limitations without consideration of aesthetic aspects. The current levels of service are outlined in Appendix 4

In 2013 and 2014, results from the City's annual Customer Perceptions Survey clearly identified that the community is dissatisfied with the current service levels and amenity of streetscapes. Both surveys highlight the community's desire for additional tree planting to verges, assistance with management of residential verges and new or upgraded entry statements to suburbs.

Addressing the community's requirements will require additional funding to increase the frequency of maintenance to roads and streetscapes. To facilitate entry statements an action plan will be required to ensure key deliverable designs are developed, endorsed by the community and investment applied in a systematic method.



## 13 STREET TREES

The City is a constructed cultural and urban landscape consisting of streets, buildings (residential, industrial and commercial), natural areas and POS. Trees play an important role in enriching the cultural experience of a location, therefore the aesthetic characteristics of the trees need to be an important selection consideration.

The selection of species may be made to reinforce climatic, historical, cultural or natural associations that satisfy local requirements. Very large trees in confined spaces often result in unacceptably high management costs. Conversely small growing trees in broad spaces rarely contribute positively to visual quality or canopy coverage. Trees selected will be in scale with other components of the streetscape and subject to service alignment and surrounding infrastructure and the largest growing species will be selected.

One of the key street tree selection objectives is to ensure the selection of the 'right tree for the right location'. This means ensuring that the selection of the species is appropriate to local environmental conditions and the constraints of the planting location. The species selection aims to ensure that the tree makes a positive contribution to environmental, amenity, aesthetic and heritage values of the area and any negative impacts are minimised.

In order to ensure the health and longevity of street trees, aesthetic and design considerations will be accommodated only where optimum conditions for plant growth are available. The proven performance of the species in particular environmental conditions and functional requirements will be the prime considerations for street tree selection.

### Arboricultural Inspections

The management of street trees is currently managed through officers in the Parks Service Unit with the engagement of a consulting arborist as required. The incorporation of a consulting arborist in the management program achieves a degree of independence in the decision making process. The assistance of a consultant is generally required when the request could be controversial, subject to the political process or escalate to involve the media in an attempt to garner community support for the removal of a tree or trees. In addition the consulting arborist can provide Quantified Tree Risk Assessments and reports to determine the potential risk of a tree.

### Clearance from Power Lines

The Western Australian Office of Energy Safety sets out the regulations for the clearance of vegetation from power lines. The regulations list each power line classification and the clearance zones required to mitigate disruption to the network. In addition the regulations outline that management of naturally occurring vegetation is performed by the service provider whilst planted street trees are the responsibility of the local municipal authority.

Compliance with regulations is facilitated by the contracts issued by service providers that require inspection, issue notices of non-compliance, action works not completed and reinspection.

Management of the clearance of vegetation from power lines will be facilitated through the street tree database. The database generates reports that list all trees under power lines orientated to each suburb, previous pruning history and a monetary value for the works. All street trees under power lines will be pruned on an annual basis, subject to growth habits and additional requirements by the regulators.

### **Street Tree Database**

To enable informed decisions on street tree selection and ongoing management a street tree database has been developed. The database has been collated by an Arboricultural firm to ensure accuracy of data. The information gathered identified the following key criteria:

- Location (Address, Suburb, Ward)
- Inspection date
- Tree species
- Under power lines
- Height
- Canopy
- Trunk diameter
- Age
- Health structure
- Photograph
- Value
- Any works required

A series of reports have been developed to provide key information on total number of trees, trees per suburb, trees requiring clearance from power lines, dominant tree species per suburb and citywide.

### **Street Tree Master Plan**

The City is developing a street tree master plan, developed from the City's database of street trees, and will be implemented in 2015-16. A street tree master plan identifies the street trees to be planted in each street throughout the City. The master plan will ensure the City's policy of avenue planting (i.e. same species on each side of the street) is achieved to mitigate random plantings.

The selection of street trees will be determined by the database with consideration of the Urban Forestry documentation and the objectives outlined above. The aim of the master plan will be to retain the existing themes within each street where they are well established and the species meets the criteria. In addition the plan will focus on the use of Australian natives where feasible subject to existing road network constraints. Tree selection will also be cognisant of solar panels on adjacent residential homes in high density settings to ensure the penetration of sunlight during winter periods.

It is envisaged that the Street Tree Master Plan will be implemented through annual funding allocations per suburb and through natural attrition. Also there will be no mass removal of street trees except where it is part of a street tree upgrade program or property development which will requires offsets for approval.



## 14 Action Plan

To achieve the vision of the Public Open Space Strategy a range of strategic actions have been prepared. These actions bring together physical planning and policy directions. The actions build on the current projects and programs delivered by the City and direct future priorities in public open space planning and management.

Within each objective there are measurable actions to be completed. Each measure has been assigned to enable monitoring and review of the strategy.

The following information explains the terminology in the Action Plan.

### Toolkit

The tools used to achieve the actions include:

- Advocacy – marketing, discussions, promotions
- Activation – improved utilisation of existing space
- Asset management – whole of life costing
- Alliances – forming partnerships and working together.

### Cost

Indicative costs for the proposed actions have been prepared:

Priority	Indicative Cost
Low	< \$10,000
Medium	\$10,000-100,000
High	\$100,000-250,000
Funded	to be resourced from operational budgets

### Priority

The timing of actions is based on priority and achievability. Priorities are subject to budget allocation, both capital and operational:

Priority	Timeframe
Short	With 2 years
Medium	Within 5 years
Long	Beyond 5 years
Ongoing	Continual Implementation

## Theme 1: Classification

Action	Toolkit	Measure	Cost	Priority	
<b>Strategy 1.1: Establish the hierarchy of POS and Streetscapes to inform the City's community of its function and recreational composition.</b>					
1.1.1	Ensure current POS and streetscapes are identified in accordance with the defined hierarchy.	Advocacy	POS and Streetscapes identified	Funded	Ongoing
1.1.2	Develop guidelines that enable new POS and streetscapes are classified upon acceptance.	Advocacy	Guidelines developed	Funded	Short
1.1.3	Ensure existing open space which provides significant tourist and event opportunities is protected and enhanced for the future.	Advocacy	Prominent locations identified	Funded	Ongoing
<b>Strategy 1.2: Foster and promote the POS and Streetscape hierarchy.</b>					
1.2.1	Develop strategic promotional material which informs the community of POS and streetscapes.	Advocacy	Promotional material developed	High	Short
1.2.2	Foster ownership of POS to community groups to increase its value.	Advocacy	Develop framework	High	Long
<b>Strategy 1.3: Develop open space that has the capacity to support a diverse range of complementary industries that contribute to the City of Cockburn economy.</b>					
1.3.1	Develop management plans for regionally significant open space which support the diversity of the site and encourage tourism.	Activation	Develop management plan for Manning Park	High	Short
1.3.2	Ensure service levels reflect the need for spaces to accommodate markets, festivals and events.	Asset Management	Implement level of Service	Funded	Ongoing
1.3.3	Develop strategic alliances with government agencies, education establishments and private providers to develop facilities to increase sport, recreation and cultural opportunities.	Alliances	Integrated team	Funded	Ongoing

## Theme 2: Value

Action	Toolkit	Measure	Cost	Priority	
<b>Strategy 2.1: Provide POS that stimulate opportunities for the vibrant, diverse and creative settings which have a strong sense of place and strengthen community identity.</b>					
2.1.1	Develop character and identity themes and styles through community involvement, creativity and artistic expression to inform POS planning and development.	Activation	Character and identity themes are completed and integrated.	High	Long
2.1.2	Complete a framework that encourages and promotes integration of art and creative expression in public places and spaces.	Activation	Arts Policy integrated with POS design principles	Medium	Long
2.1.3	Recognise the community value of trees and landscaping through the preparation of a POS and Street Tree Master Plan.	Activation	Master plans developed	High	Short
2.1.4	Develop policies and guidelines for playground designs, which incorporate and stimulate community participation.	Activation	Policy & guidelines developed	Medium	Short
2.1.5	Develop lighting guidelines that enable vibrant night settings that harmonise the community.	Activation	Guidelines developed	Funded	Short
2.1.6	Develop guidelines that identify POS that facilitates dog activation which will strengthen community spirit	Activation	Guidelines developed	High	Medium
<b>Strategy 2.2: Work collaboratively with the community for the protection and enhancement of our cultural heritage in open space.</b>					
2.2.1	Strengthen linkages and interpretation of cultural heritage in open space.	Activation	Develop guidelines	Funded	Ongoing
2.2.2	Work collaboratively with traditional owners to strengthen connections through projects and programs.	Advocacy	Develop framework for collaboration	Low	Ongoing
2.2.3	Foster the provision of plaques and memorial gardens to retain cultural heritage through guidelines.	Activation	Develop guidelines	Medium	Medium

<b>Strategy 2.3: Strengthen the concept of community hubs and pursue opportunities to provide open space in key locations.</b>					
2.3.1	Identify and audit existing community hubs to inform further planning and activation of public open space.	Advocacy	Community hubs identified	Low	Ongoing
2.3.2	Activate community hubs by improving visual and physical integration with parks and civic spaces through communication and negotiation with property owners and community groups.	Advocacy	Activation plans developed and implemented	High	Long

## Theme 3: Participation

Action	Toolkit	Measure	Cost	Priority	
<b>Strategy 3.1: Provide spaces and infrastructure to support and encourage active and creative pursuits and community participation.</b>					
3.1.1	Undertake monitoring and performance reviews of open space with the community to inform future planning and management.	Advocacy	Annual user surveys	Medium	Ongoing
3.1.2	Prepare guidelines to support the establishment of community gardens.	Advocacy	Guidelines developed	Funded	Ongoing
3.1.3	Prepare a business plan for the appointment of a Community Garden Officer to facilitate the development of community gardens in POS and engender community participation.	Advocacy	Plan completed and presented to Council	Low	Short
3.1.4	Annually update the POS development guide for the planning and development of a diverse range of activities.	Advocacy	Guidelines developed	Funded	Ongoing
3.1.5	Work in partnership with the City's Health Department to plan for outdoor recreation opportunities to improve the health and wellbeing of the community.	Advocacy	Integrated team	Medium	Ongoing
3.1.6	Develop guides that facilitate the delivery of storm water to POS which mitigates the loss of recreational activities.	Advocacy	Guidelines developed	Medium	Short
3.1.7	Develop a plan for the distribution for outdoor exercise equipment to support community health.	Advocacy	Plan developed	Medium	Ongoing
3.1.8	Develop a playground shade sail strategy which outlines key functions and supports active use by the community.	Advocacy	Implementation	Funded	Ongoing
3.1.9	Ensure that parks are accessible to people of all abilities, through compliance with the City's Disability, Access and Inclusion Plan (DAIP).	Alliances	Compliance with DAIP	Low	Ongoing

3.2.0	Provide workers with opportunities for leisure and recreation in the City's commercial and industrial areas through the provision of parks and POS.	Activation	Audit of facilities	Medium	High
3.2.1	Identify the most appropriate parks for the installation of infrastructure and signage to support group fitness sessions and personal trainers.	Activation	Parks identified	Low	Medium

**Strategy 3.2: Ongoing provision of quality active sports ovals to encourage improved physical wellbeing of the community.**

3.2.1	Prepare design guidelines that promote the utilisation of existing sports grounds for recreation space for the local communities outside of sports usage times (e.g. shade, seating and paths).	Advocacy	Guidelines developed	Medium	Long
3.2.2	Undertake a Sport Oval Carry Capacity study to determine the maximum patronage limitation of the active sporting area to ensure that functionality is maintained.	Advocacy	Annual audits completed	Medium	Ongoing
3.2.3	Develop a policy for the management of active sports ovals to ensure quality, bookings and usages are delineated to meet community expectations.	Advocacy	Guidelines developed	Medium	Short
3.2.4	Align the POS Strategy with the Sport & Recreational Strategic plan to ensure the management and provision of active sports ovals is facilitated to meet the demands of future the communities.	Advocacy	Integrated team	Funded	Ongoing

## Theme 4: Connectivity

Action	Toolkit	Measure	Cost	Priority	
<b>Strategy 4.1: Create well-connected public open space through the provision of linked trails, streetscapes, ecological corridors, community hubs and parks and reserves.</b>					
4.1.1	Strengthen pedestrian links to parks and community hubs to improve transport options.	Activation	Linkages improved	Funded	Medium
4.1.2	Ensure recreational path planning provides for walking, cycling, skate boarding, etc.	Activation	Paths and trails provide a diverse range of users	Low	Ongoing
4.1.3	Identify park locations with opportunities for public transport, cycle links and integration into transport planning.	Activation	Identify POS	Low	Medium
4.1.4	Use endemic native vegetation, where practical, along trails, streetscapes, community hubs, streetscapes, parks and reserves to provide and enhance ecological linkages.	Activation	Majority of landscaping utilises endemic vegetation	Low	Medium
<b>Strategy 4.2: Ensure linkages and access to sensitive areas is appropriately planned.</b>					
4.2.1	Incorporate regionally significant POS in the Trails Master Plan and Bicycle Network and Footpath Plan Strategies	Advocacy	Update plans	Funded	Ongoing
4.2.2	Within coastal foreshore planning, prepare policies to address beach access to guide location, size and level of infrastructure for financial sustainability and to ensure minimal environmental impact.	Advocacy	Develop polices	Low	Medium
4.2.3	Ensure linkages and access to sensitive areas is planned and appropriate to minimise environmental impacts.	Activation	Appropriate design	Medium	Ongoing
<b>Strategy 4.3: Further develop the balance between old and new suburban open space planning to encourage the protection of distinct communities.</b>					
4.3.1	Locate, where appropriate, larger parks and sports grounds to contribute to and enhance the	Advocacy	Integrated team	Funded	Ongoing

	benefits of old and new suburbs.				
4.3.2	Identify opportunities to utilise amenity reserves, parks, streetscapes, trails and paths to develop ecological corridors.	Advocacy	Ecological corridors identified	Funded	Short

## Theme 5: Responsible Management

Action	Toolkit	Measure	Cost	Priority	
<b>Strategy 5.1: Proactively plan and deliver open space that encourages sound investment and meets community needs.</b>					
5.1.1	Continue to apply the Levels of Service to ensure the land and embellishments are responsive to community values and changing needs	Asset Management	Desired levels of service implemented	Low	Ongoing
5.1.2	Integrate 'whole of life' costs into open space planning, landscape development and infrastructure selection.	Asset Management	Whole of life costs integrated into planning	Low	Ongoing
5.1.3	Develop landscape guidelines to consider low maintenance designs that minimise ongoing costs but achieve acceptable aesthetic outcomes.	Asset Management	Guidelines developed	Funded	Short
5.1.4	Incorporate into the City's Long Term Financial Plan allocation of funding for the planning, management and maintenance of public open space and streetscapes to ensure viable and sustainable assets capable of delivering the desired levels service and incorporate into the Long Term Financial Plan.	Asset Management	Plan developed and implemented	Funded	Ongoing
5.1.5	Undertake annual benchmarking audits which compare the City to other local government authorities.	Asset Management	Annual Audits completed	Low	Ongoing
<b>Strategy 5.2. Support innovative and sustainably designed public open space and streetscape that minimise maintenance and whole of life costs.</b>					
5.2.1	Review design standards to identify more natural and cost effective outcomes over the whole of the asset life.	Asset Management	Design standards reviewed	Low	Short
5.2.2	Prepare guidelines for donated assets to achieve desired standards while minimising operational and whole of life costs within the planning scheme.	Asset Management	Guideline developed	Low	Short

5.2.3	Apply resource efficient and effective strategies in development (e.g. hydrozoning, native planting, etc) to minimise natural resource use.	Asset Management	Ongoing program implemented	Funded	Ongoing
5.2.4	Review and implement the Water Action Plan to include initiatives which promote sustainable design.	Asset Management	Ongoing program implemented	Funded	Ongoing
<b>Strategy 5.3: Ensure that the open space network provides for the changing and increasing needs of the community.</b>					
5.3.1	Develop a process to maintain, develop and refine spatial information systems that contributes timely and valuable data to inform POS planning and asset management and maintenance of streetscapes and the urban forest.	Asset Management	Process developed and implemented	Funded	Ongoing
5.3.2	Prepare a guideline for community input to consider open space outcomes in relation to urban infill strategies.	Advocacy	Guideline developed	Low	Short
5.3.3	Monitor use of open space to identify low use reserves and develop enhancement strategies.	Asset Management	Monitor	Funded	Ongoing
5.3.4	Ensure development in the open space network is consistent with universal access and safety guidelines.	Asset Management & Activation	Designs comply with standards	Funded	Ongoing
5.3.5	Actively seek to reduce POS space lost to secondary uses, especially in foreshore areas (e.g. car parking, commuter paths, storage spaces).	Asset Management & Activation	Increased usable space	Funded	Ongoing
<b>Strategy 5.4: Encourage and promote community involvement in management of public open space.</b>					
6.4.1	Develop guidelines and protocols that promote and enable volunteer involvement in management and operation of POS.	Alliances	Guidelines developed	Low	Long
5.4.3	Continue to engage with communities on detailed planning, management and delivery in accordance with the City's Community Engagement Policy.	Advocacy	Engagement in accordance with policy	Low	Ongoing

5.4.4	Prepare a suite of information tools (e.g. print, web, social media & radio) to promote access and provide information.	Advocacy	Information tools developed	Medium	Medium
<b>Strategy 5.5: Utilise Council's open space to assist in the mitigation and adaptation to climate change.</b>					
5.5.1	Incorporate into the Sustainability Action Plan actions that enable Council to consider the design of open space areas help to mitigate and adapt to the effects of climate change.	Advocacy	Annual updates	Funded	Ongoing
5.5.2	Recognise the value of open space and trees in supporting local biodiversity, enhancing amenity and user comfort to assist climate change adaptation and mitigation.	Advocacy	Local biodiversity supported	Funded	Ongoing

## 15 IMPLEMENTATION AND REVIEW

The Public Open Spaces Strategy 2014-2014 is a long term document which identifies key principles in order to deliver its vision:

*“Create a sustainable hierarchy of accessible and resource efficient quality open spaces that enhances the recreational, environmental, social and health needs of current and future generations”*

Monitoring of the strategy’s key actions and objectives is critical to its success. The strategy will be reviewed every five years, which will include a report on the strategy’s barriers and achievements.

## **Appendix 1 Strategic Alignment**

### **Strategic Community Plan 2012 – 2022**

The Strategic Community Plan is a long term Community Plan. The Plan was developed in conjunction with the community and sets the vision for where the City wishes to be in 2022. To ensure the future needs are met, seven key themes have been developed which focus on the communities priorities. The POS strategy is aligned with the following themes:

#### **Community and lifestyle**

- Safe communities and to improve the community sense of safety
- Promotion of active and healthy communities.

#### **Environment and Sustainability**

- Community that uses resources in a sustainable manner
- To protect, manage and enhance our natural environment, open spaces and coastal landscapes

#### **Infrastructure**

- Community facilities that meet the diverse needs of the community now and into the future.
- Community infrastructure that is well planned, managed, safe, functional, sustainable and aesthetically pleasing.
- Facilities that promote the identity of Cockburn and its communities.

### **Long Term Financial Plan 2012/13 – 2021/22**

The Long Term Financial Strategy (LTFS) exists to provide the following outcomes for the City of Cockburn:

- Establish a prudent and sound financial framework, combining and integrating financial strategies to achieve a planned outcome;
- Establish a financial framework against which City's strategies, policies and financial performance can be measured;
- Ensure that City complies with sound financial management principles, as required by the Local Government Act (1995) and plan for the long-term financial planning as outlined by the Department of Local Government - Long Term Financial Planning – Frame Work and Guidelines; and
- Allow the City to meet the objectives of the Local Government Act (1995) to promote the social, economic and environmental viability of municipal district and its role in maintaining the viability of City to ensure that resources are managed in a responsible manner.

The themes of POS Strategy will be embedded in the LTFS to ensure they are delivered through a financially sustainable framework.

### **Workforce Plan 2012 – 2017**

The Workforce Plan forms part of the City of Cockburn's Integrated Planning Framework through the Community Strategic Plan 2012 – 2022. The Workforce Plan allows the City to set priorities within its resourcing capacity and deliver short, medium and long term community priorities and aspirations. The plan is continually evolving in response to internal and external changes. A staffing forecast has been mapped which lists the Parks Services staff requirements based on POS growth through new subdivisions and urban revitalisation strategies.

### **Public Health Plan 2013 – 2018**

The Public Health Plan (PHP) outlines actions necessary to ensure the occupants of the City have an acceptable level of health today and into the future. This should help to reduce the predicted increase in the cost of providing health services for the aging population and minimise the number of people whose lifestyle is compromised by the symptoms or preventable diseases. The PHP identifies number actions which will require delivery through the implementation of the strategy.

### **Natural Area Management Strategy 2012 – 2022**

The Natural Area Management Strategy provides a framework to manage the City's vested natural reserves and to build and enhance the capacity to effectively manage our natural areas for the conservation of biodiversity. The ultimate goal is for all the City's natural areas to have a vegetation condition rating of good or better based on the Keighery 1994 definition of vegetation condition.

The Natural Area Management Strategy and the Public Open Space strategy will form a strong relationship and link intrinsically due to the cohesion of natural areas, ecological corridors and recreation areas.

### **Sustainability Strategy 2013 - 2017**

The Sustainability Strategy is designed to assist with the development of achievable sustainability objectives into the City's Corporate Strategic Plan, adopted plans, strategies and policies. The strategy aims to create a culture of sustainability within the organisation, and the community, with an emphasis on integrating the social, economic and environmental considerations into the decision making and delivery outcomes.

### **Sport and Recreation Strategic Plan 2009**

The Sport and Recreation Strategic Plan outlines a planning framework that addresses the needs of sporting groups and ensures a balanced mix of sports and facilities throughout the City. The plan specifically identifies current and future facilities, sports provision and the viability of clubs, identifies future needs of the community, peak body information, trends in sport & recreation, sporting opportunities & constraints and a development schedule.

The Sport and Recreation Strategic Plan aligns with the POS Strategy to ensure the level of service implemented to the sporting oval and associated infrastructure provides clubs with a functional and useable facility.

### **Parks & Environment Asset Management Plan 2014**

The Parks and Environment Asset Management Plan has been developed to deliver sustainable financial management and continuous improvements to the infrastructure assets within POS and streetscapes. The plan covers irrigation, park infrastructure, playgrounds, lighting, amenities, artwork and fencing.

The Parks and Environment Asset Management Plan identifies the level of services delivered to the community, existing risk management control and ways to mitigate future risks, future growth and demand, lifecycle cost management and renewal forecasts.

### **Children Services Strategic Plan 2010 – 2015**

This plan is to inform new directions and strategies needed to meet the challenges of a complex and changing policy and regulatory environment at federal (i.e. Council of Australian Governments) and state government levels, and emerging demographic challenges for Children's Services. The POS strategy will assist in the facilitation of the objectives and strategies contained within the plan.

### **Disability Access & Inclusion Plan 2012 – 2017**

Local Governments play a vital role in the lives of people with disabilities because of their broad mandate. All Local Government functions directly or indirectly have an impact upon the quality of life of the people with disabilities who live, work and socialise in their local communities. It is a requirement of the Disability Services Act 1993 (Western Australia amended 2004) that all Local Governments develop and implement a Disability Access and Inclusion Plan that outlines the ways in which they will ensure that people with a disability have equal access to its facilities and services. The POS strategy will ensure the key objectives of the plan will be incorporated during design and upgrades to POS and Streetscapes.

### **Public Artworks Strategy 2009**

Cockburn's local government area is an eclectic mix of urban and rural spaces which provides for a diverse community experience, from major urban developments, established coastal suburbs, newly developed business precincts and working market gardens to beautiful community parks and quiet residential streets. The City area is also comprised of distinct neighbourhoods with particular environmental settings, social histories, cultural identities and spirit.

Public Open Space and Streetscapes provide an ideal location where items of public art can be located to showcase the diversity of the local community.

## Appendix 2 POS Planning Framework

State and local planning frameworks guide the City's development and management of POS.

### Metropolitan Regional Scheme

- Establishes future urban areas
- Major highway alignments
- Regional open spaces

### Liveable Neighbourhoods

Establishes:

- The desired distribution and hierarchy of parklands in new neighbourhoods
- Intended roles of different sized parks
- Street widths and hierarchies within a neighbourhood
- Public areas where people will gather to socialise – neighbourhood centres, public transport networks and so on.

### Directions 2031

Describes where existing neighbourhoods should be enhanced through:

- Increasing the density and variety of homes through subdivision/multiple dwellings on existing lots.
- The variety of activities available within walking distance of people's homes – making streets and public areas more intensively used and used over a wider time of day.
- Increased demand for on street parking, footpaths, refuse collection, and tree protection.

### City of Cockburn Town Planning Scheme

- Provides detailed guidance on the massing, appearance and design of buildings addressing the street.
- Establishes the character of commercial precinct streets and public areas.

### Structure Plans

- Establish the location, scale, and character of streets by determining their widths and the size of housing and other lots addressing the street.
- Establish the location, scale, and character of parklands and natural areas.
- District/Local Water Management Plans (D/LWMS).
- Provide the first conceptual ideas about how new parks are to be arranged and set out internally.

## **Subdivision Stage**

- Provides first indications of the intended method of complying with principles set out in the Structure Plan and the LWMS.
- The character of the future suburb is largely established now.
- Oversees integration of footpaths, roads, parking embayments, drainage basins,
- sports and remnant vegetation and how they are tempered by the need to accommodate level changes (retaining walls, slopes and drainage swale locations) and traffic (medians, roundabouts, parking embayments) is established at this stage.

## **Street and POS design drawings.**

- Resolution of the interrelationship between elements indicated in the Subdivision Stage. For example, how are street trees, parking embayments, and crossover locations to be fitted together to ensure that streets are walkable in the heat of summer and refuse can be collected without clashing with parked cars.
- The materials (planting paving, walling, furniture), finishes, levels and gradients to be used in new parklands is described and agreed. The cost of maintaining the new parkland is calculated and adjustments made to fit the budget available to the City.
- This is the final opportunity to temper the detailed shape and appearance of the new park before it is handed over to the City to maintain and manage in perpetuity.

## Appendix 3 POS Classification Level of Service Per Annum

Open Space Classification	Public Open Space Levels of Service per Annum										
	Turf Mowing	Turf Renovation & Fertilising	Landscape Maintenance	Play Equipment Inspection	Shade Sail Removal & Installation	Irrigation Inspection	Park Amenities Inspection & Maintenance	Skate Park Maintenance	Dog Exercise Area Maintenance	BBQ Cleaning	Public Toilet Cleaning
District Open Space	52	4	26	1 Major 2 Minor	2	52	1	12	N/A	26	52
Regional Open Space	52	1	26	1 Major 2 Minor	2	26 – 40	1	12	26	365	365
Neighbourhood Open Space – Advanced Embellishment	26	2	26	1 Major 2 Minor	2	26	1	12	26	12	52
Neighbourhood Open Space – Primary Embellishment	26	1	26	1 Major 2 Minor	2	26	1	N/A	N/A	N/A	N/A
Local Open Space – Advanced Embellishment	26	1	26	1 Major 2 Minor	N/A	26	1	N/A	N/A	N/A	N/A
Local Open Space – Primary Embellishment	17	N/A	17	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A

## Appendix 4 – Streetscape Level of Service Per Annum

Streetscape	Streetscape Levels of Service per Annum			
	Turf Mowing	Landscape Maintenance	Irrigation Inspection	Tree Management
High Profile Streetscapes	26	26	26	In accordance with street tree database / customer request
Medium Profile Streetscape	12	12	26	In accordance with street tree database / customer request
Priority 1 Roads	Min 4 / Max 6	N/A	N/A	In accordance with street tree database / customer request
Priority 2 Roads	Min 2 / Max 4	N/A	N/A	In accordance with street tree database / customer request
Rural Roads	2	N/A	N/A	In accordance with street tree database / customer request
Industrial Roads	2	N/A	N/A	In accordance with street tree database / customer request
Unkempt Verges	Up to 4 times (upon request)	N/A	N/A	In accordance with street tree database / customer request
Pruning of Trees - Powerlines	N/A	N/A	N/A	Annually
Pruning of Trees – Residential	N/A	N/A	N/A	In accordance with street tree database / customer request
Public Access Ways	N/A	N/A	N/A	In accordance with street tree database / customer request
Roundabouts	N/A	12	N/A	In accordance with street tree database / customer request
Entry Statements	26	26	26	In accordance with street tree database / customer request



BIBRA LAKE		Reserve No	Ward	Area Ha	Function					Classification		Status	Key Assets					Recreation Components					Status	Development Options					Comments															
Reserve Name	Street Address				Recreation Space	Sport Space	Nature Space	District	Regional	Neighbourhood	Local	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Amenities - Seating	Seating	Benches	Fencing	Irrigation	Minor Structures - Gazebos	Shelters	Playgrounds	Structured Sports	Unstructured Sports	Child Play Elements	Social Gathering	BBQ	Exercise Equipment		Walking	Cycling	Dog Exercise Areas	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Playground	Playground Shade Sail	Exercise Equipment	Park Lighting	Public Toilets	BBQs	Picnic Seating / Gazabo	Irrigation	Landscaping
BIBRA LAKE RESERVE	MELLER ROAD	6208	East	257.16	•	•	•	•	•	•	•	•	59	19	72	1	69	7	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	Bibra Lake Management Plan
BRAMLEY PARK	BRAMLEY PARK	45224	East	0.41	•	•	•	•	•	•	•	•	1	2	1	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified
COCOS PARK RESERVE	COCOS PARK RESERVE	45113	Central	2.04	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
DUFFIELD RESERVE	NEEDWELL ROAD	36073	East	0.22	•	•	•	•	•	•	•	•	1	1	3	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	Minor improvements required
ELIZA CAVE PARK	BIBRA DRIVE	40057	East	1.41	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
FERRERES RESERVE	FERRERES RESERVE	37783	East	0.52	•	•	•	•	•	•	•	•	2	1	•	•	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
INVERCAULD FORREST	FORREST ROAD	Lot 3	Central	0.18	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
LA FAYETTE RESERVE	PHOENIX ROAD	43365	Central	0.82	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
MALABAR PARK	MALABAR WAY	30997	Central	2.92	•	•	•	•	•	•	•	•	•	•	•	•	2	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
MARSHWOOD RESERVE	MIRABEAU MEWS	43662	Central	3.01	•	•	•	•	•	•	•	•	2	15	•	•	2	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
MEARES PARK	PROUT WAY	37935	East	1.2	•	•	•	•	•	•	•	•	2	2	2	1	2	2	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified
MELLER PARK	PARKWAY ROAD	35532	East	4.5	•	•	•	•	•	•	•	•	1	5	10	1	2	2	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	Minor improvements required
NOLA WATERS RESERVE	ANNOIS ROAD	46392	East	0.79	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
PROVINCIAL RESERVE	PROVINCIAL MEWS	47650	Central	0.13	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
RAMSAY PARK	DOWELL PLACE	35933	East	3.69	•	•	•	•	•	•	•	•	5	2	4	1	2	2	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
SHERBROOKE GARDEN RESERVE	SHERBROOKE GARDENS	42608	Central	2.4	•	•	•	•	•	•	•	•	1	1	9	1	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	NOS Destination
THE GLADE	ORLANDO AVENUE	47276	Central	1.08	•	•	•	•	•	•	•	•	2	3	10	1	2	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
WINDMILL RESERVE	WINDMILL DRIVE	39341	East	0.21	•	•	•	•	•	•	•	•	1	2	•	•	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	Additional footpath required

COOLBELLUP		Reserve No	Ward	Area Ha	Function					Classification		Status	Key Assets					Recreation Components					Status	Development Options					Comments																		
Reserve Name	Street Address				Recreation Space	Sport Space	Nature Space	District	Regional	Neighbourhood	Local	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Amenities - Seating	Seating	Benches	Fencing	Irrigation	Minor Structures - Gazebos	Shelters	Playgrounds	Structured Sports	Unstructured Sports	Child Play Elements	Social Gathering	BBQ	Exercise Equipment		Walking	Cycling	Dog Exercise Areas	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Playground	Playground Shade Sail	Exercise Equipment	Park Lighting	Public Toilets	BBQs	Picnic Seating / Gazabo	Irrigation	Landscaping	Sporting Infrastructure		
DE MARCHI PARK	WAVERLEY ROAD	29976	Central	0.31	•	•	•	•	•	•	•	•	1	9	2	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified	
DOHERTY RESERVE	DOHERTY ROAD	30989	Central	2.42	•	•	•	•	•	•	•	•	1	1	6	2	1	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	Raise status to advanced level
ELINOR RESERVE	RINALDO CRESCENT	38725	Central	0.39	•	•	•	•	•	•	•	•	1	3	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
HARGREAVES PARK	HARGREAVES ROAD	29602	Central	3.07	•	•	•	•	•	•	•	•	7	3	7	2	7	4	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
JARVIS PARK	QUINLAN STREET	38587	Central	2.23	•	•	•	•	•	•	•	•	3	2	7	2	5	3	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
LEN PACKHAM	WAVERLEY ROAD	30190	Central	4.73	•	•	•	•	•	•	•	•	4	4	20	3	5	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	NOS Destination		
LOCKETT RESERVE	LOCKETT STREET	38645	Central	0.54	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified			
MAMILLIUS PARK	MAMILLIUS STREET	38760	Central	0.57	•	•	•	•	•	•	•	•	2	2	9	1	3	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
MATILDA BIRKETT RESERVE	COOLBELLUP AVENUE	39817	Central	0.53	•	•	•	•	•	•	•	•	1	4	•	•	2	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified			
MOENNICH RESERVE	MALVOLIO ROAD	43700	Central	0.35	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified			
PERDITA PARK	PERDITA WAY	28729	Central	0.25	•	•	•	•	•	•	•	•	2	1	1	•	2	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified			
RINALDO PLACE RESERVE	RINALDO CRESCENT	43887	Central	0.07	•	•	•	•	•	•	•	•	1	4	•	•	2	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
RINALDO RESERVE	RINALDO CRESCENT	30992	Central	2.41	•	•	•	•	•	•	•	•	8	2	8	2	4	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified		
ROBB PARK	HARTLEY STREET	32513	Central	0.67	•	•	•	•	•	•	•	•	3	1	20	1	4	3	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	NOS Destination		
TEMPEST	PROSPERO CRESCENT	28615	Central	3.56	•	•	•	•	•	•	•	•	3	5	8	2	9	1	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	•	No Improvements Identified			
VISSER RESERVE	COUNSEL ROAD	34011	Central	0.18	•																																										





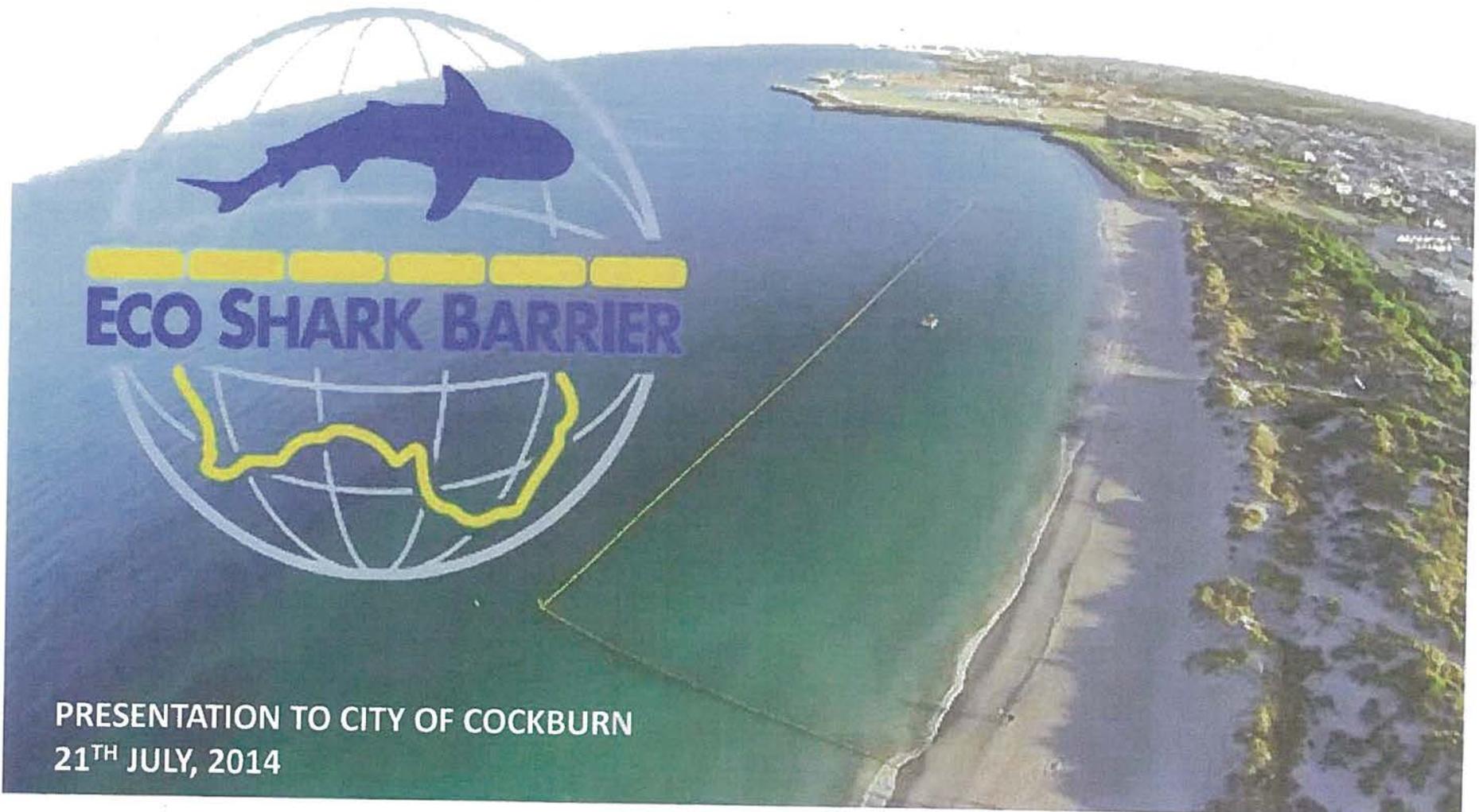






WATTLEUP		Reserve No	Ward	Area Ha	Function			Classification			Status	Key Assets				Recreation Components				Status	Development Options							Comments														
Reserve Name	Street Address				Recreation Space	Sport Space	Nature Space	District	Regional	Neighbourhood		Local	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Amenities - Seab. Picnic Settings	Bins	Fence	Irrigation		Minor Structure - Gazebo, Shelters	Playgrounds	Structured Sports	Unstructured Sports	Child Play Elements	Social Gathering	BBQ		Exercise Equipment	Walking	Cycling	Dog Exercise Areas	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Playground	Playground Shade Sail	Exercise Equipment	Park Lighting	Public Toilets	BBQ's	Picnic Setting / Gazebo
DEEPDENE PARK	MARBAN WAY	25	Central	2.52	•							4	10	2	10							•																				Future Latitude 32 Development Zone
FRANKLAND PARK			Central	24.3	•	•	•	•	•																																Future Sporting Complex	
HOLDSWORTH RESERVE	MORTIMER ROAD	24484	Central	2.12	•					•		2	2		1			•	•																						Raise status to advanced level	
LEES PARK	DALISON AVENUE	45	Central	0.66	•							2			3																									Future Latitude 32 Development Zone		
SRDAROV RESERVE	MIRO STREET	27968	Central	0.25																						•														Future Latitude 32 Development Zone		
TORGOYLE RESERVE	RUSSELL ROAD	1712	Central	28.74			•																																	Managed under the Natural Area Management Strategy		

YANGEBUP		Reserve No	Ward	Area Ha	Function			Classification			Status	Key Assets				Recreation Components				Status	Development Options							Comments														
Reserve Name	Street Address				Recreation Space	Sport Space	Nature Space	District	Regional	Neighbourhood		Local	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Amenities - Seab. Picnic Settings	Bins	Fence	Irrigation		Minor Structure - Gazebo, Shelters	Playgrounds	Structured Sports	Unstructured Sports	Child Play Elements	Social Gathering	BBQ		Exercise Equipment	Walking	Cycling	Dog Exercise Areas	Undeveloped	Primary Level of Embellishment	Advanced Level of Embellishment	Playground	Playground Shade Sail	Exercise Equipment	Park Lighting	Public Toilets	BBQ's	Picnic Setting / Gazebo
DUNRAVEN RESERVE	DUNRAVEN DRIVE	42602	Central	0.15	•				•			1	1	1	1			•	•																							No Improvements Identified
LEVI PARK	PLOVER DRIVE	39774	Central	3.05	•							2	1	4	3	2			•	•	•																					Minor development options
LITTLE RUSH LAKE RESERVE	OSPREY DRIVE	Lot 630	Central	36.52	•																																				Managed under the Natural Area Management Strategy	
LOPRESTI PARK	BELLADONNA DRIVE	47160	Central	0.61	•							3	1	4	4	6	1			•	•	•																			Managed under the Natural Area Management Strategy	
MACROZAMIA PARK	TINDAL AVENUE	48352	Central	0.31	•							2			2																										No Improvements Identified	
MIGUEL RESERVE	MIGUEL ROAD	39656	Central	0.25	•																																				Managed under the Natural Area Management Strategy	
MILGUN RESERVE	MILGUN DRIVE	40452	Central	5.10	•							1	2	10	4	6	3			•	•	•																			No Improvements Identified	
MINORI RESERVE	MINORI GARDENS	49885	Central	0.40	•							1	1	7	2	1				•	•	•																			Minor development options	
NICHOLSON RESERVE	SWALLOW DRIVE	37200	Central	3.53	•							6	5	11	2	7	1			•	•	•																			No Improvements Identified	
PELICAN RAMBLE PLAYGROUND	PELICAN RAMBLE	Lot 299	Central	0.05	•																																				Minor development options	
PERENA ROCCHI RESERVE	TERN LOOP	36587	Central	5.03	•							6	5	20	2	9	2			•	•	•																			Remove playground at EOYL	
RAVELLO RESERVE	SCALA GARDENS	47410	Central	1.54	•																																				NOS Destination.	
RONARD RESERVE	RONARD PLACE	43750	Central	1.45	•							3	1	5	2	4	3			•	•	•																			No Improvements Identified	
SAMUEL CAPHORN RESERVE	GRASSBIRD LOOP	39839	Central	1.03	•							1	1	2	1	1				•	•	•																			No Improvements Identified	
SHORELINE RESERVE	YANGEBUP ROAD	48568	Central	0.64	•										7																										No Improvements Identified	
SPINNAKER RESERVE	SPINNAKER HEIGHTS	48575	Central	0.27	•							1	1	7	2	1				•	•	•																			Minor development options	
VISKO PARK	BIRCHLEY ROAD	47278	Central	4.20	•										10	2	1			•	•	•																			Development subject to Reallocation of Bowling club	
WARTHUYKE PARK	MAGNOJA GARDEN	40263	Central	2.48	•							2	1	4	2	3	1			•	•	•																		No Improvements Identified		
YANGEBUP LAKE RESERVE	OSPREY DRIVE	49078	Central	143.76	•							1	1	1	1	1																									Managed under the Natural Area Management Strategy	



PRESENTATION TO CITY OF COCKBURN  
21<sup>TH</sup> JULY, 2014

For more information, please contact Craig Moss: Founder, Inventor, Patent Owner and  
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# Index



1. Executive Summary
2. History of Eco Shark Barrier
3. The benefits of the barrier
4. Technical aspects
5. Costings for Coogee Beach
6. Helpful Links regarding Eco Shark Barrier
7. Annexures

# 1. Executive Summary



The Eco Shark Barrier does the following:

- Keeps out the sharks completely as it is anchored from seabed to surface and returning to the beach for 365 days a year.
- Supports and does not trap marine wildlife. This is an advantage to Government as the concept is supported by many environmental groups.
- The barrier can be installed into calm water beaches or surf beaches.
- Gives peace of mind for the public swimming inside the barrier, local government, state government and the taxpayers.
- Provides a solution at a small cost for public safety. In fact, the Eco Shark Barrier could provide cost savings to the Government on a short and long term basis. All Leasing have stated they can offer long-term leasing options to Government for any barriers in Western Australia.
- Provides a strong and low maintenance solution that can last up to ten years.

Please refer to the following video <http://www.youtube.com/watch?v=C1hDGndPgJg> for an overview of the benefits of the barrier.

Eco Shark Barrier provides the complete solution to the issue of human safety and marine protection that is publicly acceptable to councils, surf clubs, public and environmental groups. The Barrier is designed, manufactured and assembled in Western Australia.

# 1. Executive Summary cont.



There are many ways sharks can be repelled from beaches, including shark catch and kill and shark nets. Eco Shark Barrier offers an alternative solution.

Most shark nets stretch about 200 metres along the beach and down to a depth of six metres. Lines of torpedo floats at the top and sinkers at the bottom keep the net upright in the water. They are anchored at either end, usually about 200 metres from shore in roughly 10 metres of water. The nets are not intended to form a complete barrier, and sharks can still get through.

Eco Shark Barrier is a barrier that 100% encloses a beach area from shore to pylons and from sea bed to surface whilst not being damaging to marine life, the environment or the swimmers.



## 2. History of Eco Shark Barrier



Eco Shark Barrier Pty Ltd was formed in 2012 by Founder, Inventor and Managing Director, Craig Moss. Craig and his family have always been a beach going family, and enjoy boating as well. But Craig and his wife, Leanne, found their children not wanting to go to the beach, because of the media reports about shark attacks and sightings.

It was a constant concern for Craig and Leanne, that the lifestyle they had all so much enjoyed was now tainted with fear. If this is what their family were feeling, they felt there were probably other families that were experiencing a reluctance to go to the beach or venture into the water, too.

Craig became more aware of the increase in shark attacks both in our country as well as internationally and upon researching into this, saw the damaging effects the current shark nets were having on sharks as well as other marine life.

Shark attacks are on the increase and Western Australia is on the front line. Since 2000, there has been 194 occurrences of recorded shark attacks, 47 of which have been fatal. The only relatively effective way to prevent shark attacks is with shark nets, but the problem with this is that Shark Nets kill indiscriminately. Other harmless marine life and in some instances, human beings, can become trapped and killed by shark nets.

## 2. History of Eco Shark Barrier cont.

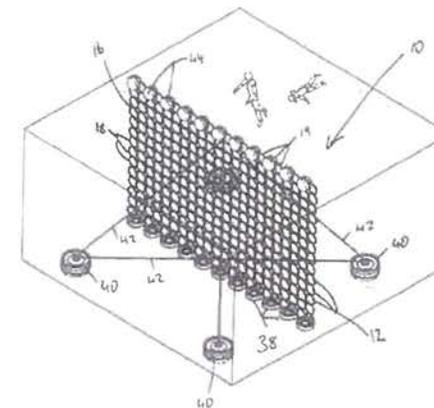


Due to the fact that shark nets do not form a secure swimming area, they are ineffective as barriers. Results have shown that 30% of all sharks caught in shark nets actually become trapped on the shore side of the net. Sharks can still swim underneath a shark net and Shark nets do not always connect with the shore line, which allows sharks to swim through increasing the risk of shark attacks.

Eco Shark Barrier Pty Ltd sought an innovation to provide an ecological solution to the wider problem of shark attacks whilst protecting marine life as well as the environment.

Craig originally came up with a tyre design, ridged, strong and where they would be re-cycling a huge amount of tyres that would not only save money, but an ever increasing problem of what to do with old tyres. Craig knew that tyres had been used in some parts of the world for artificial reefs. But the risk of leaching was not acceptable.

As tyres could not be used, Craig came up with a new design based on the same concept of interlocking modules to create a barrier that was totally adaptable. The new design was staring Craig in the face. The double layer of tyres created a pattern that could not be ignored.



## 2. History of Eco Shark Barrier cont.



Craig experimented with nylon, with various additives at varied amounts until he found the right one. It was extremely strong and flexible and could withstand the salt water environment.

With the new design and new materials, Eco Shark Barrier went into production in late 2013 and installed a trial at Coogee Beach in December, 2013.

Craig and Leanne were very lucky to have the support and guidance from many people and professionals in the marine industry. Together with this input and their own observations they identified some weaknesses and knew they could improve on the original model.

Eco Shark Barrier Pty Ltd employed a new design company in January 2014 and worked with them to come up with a superior barrier.

The new barrier has a new connection method that is stronger and more practical, to allow the modules to be fitted and disconnected with more ease. Connectors are also secured with nylon rivets, for extra security and strength.

The surface area has been cut down, and ease of cleaning was kept in mind with the new design. Less surface area also means less area for growth as the barnacles etc. weigh the barrier down and put more stress on the floats.

## 2. History of Eco Shark Barrier cont.



The barrier also has greater vertical expansion and contraction, so it readily adapts to the tidal changes. Eco Shark Barrier have considered this a priority in the new barrier as it will be staying in the water all year around.

The floats have also been changed to give even more buoyancy and support to the barrier.

The new barrier will be installed at Coogee Beach in September, 2014 for a further three years and will stay in the water for all the seasons.

The Eco Shark Barrier has been through many changes in its short life. Craig and Leanne believe in their product, and want to deliver the best product they possibly can. Watching the Eco Shark Barrier grow and evolve has been like watching a child grow and learn. To date, Craig and Leanne have spent \$950,000 of their own personal funds to get the project to this stage and to provide a product that will do the job most efficiently and effectively.

The Eco Shark Barrier is a win – win for both human and marine life. It gives peace of mind and security to beach goers, lifeguards, local governments, state government and the community as a whole.

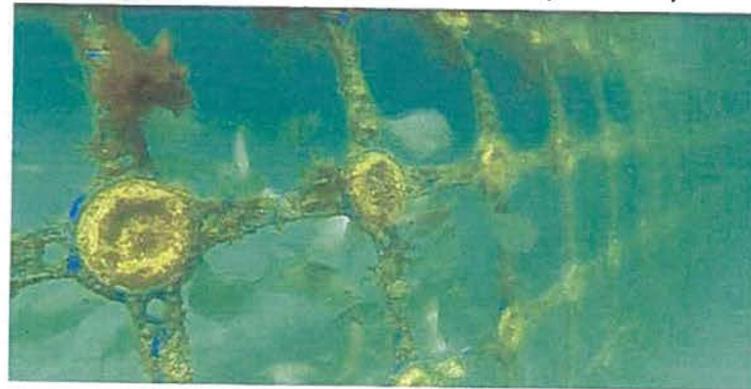
Eco Shark Barrier is proudly a WA family owned business with all forms of manufacturing occurring locally in WA. Craig and Leanne are very hands on and are there 24/7. Eco Shark Barrier give special thanks to the City of Cockburn, for the opportunity to trial our barrier at Coogee Beach from December 2013 – April 2014.

# 3. The benefits of the barrier



The Eco Shark Barrier does the following:

- Keeps out the sharks completely as it is anchored from seabed to surface and returning to the beach for 365 days a year.
- Supports and does not trap marine wildlife. This is an advantage to Government as the concept is supported by many environmental groups.
- The barrier can be installed into calm water beaches or surf beaches.
- Gives peace of mind for the public swimming inside the barrier, local government, state government and the taxpayers.
- Provides a solution at a small cost for public safety. In fact, the Eco Shark Barrier could provide cost savings to the Government on a short and long term basis.
- Provides a strong and low maintenance solution that can last up to ten years.

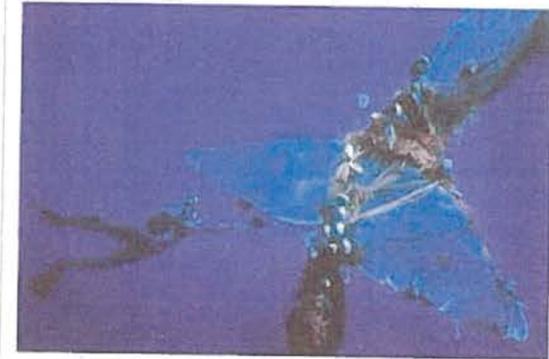
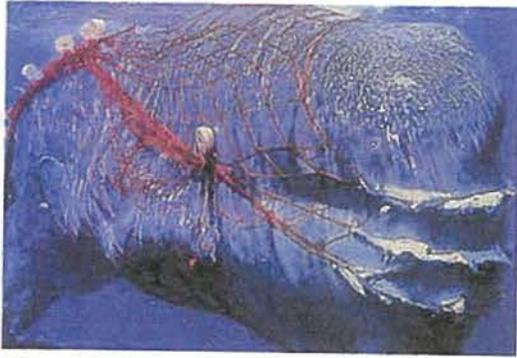


Eco Shark Barrier Pty Ltd

### 3. The benefits of the barrier cont.



Unfortunately Shark Nets can kill indiscriminately



### 3. The benefits of the barrier cont.



Shark Nets are also unstable and break easily, and are not anchored to the seabed. This photo was taken of the Dunsborough Beach Net Enclosure Trial – Summer of 2013/2014.



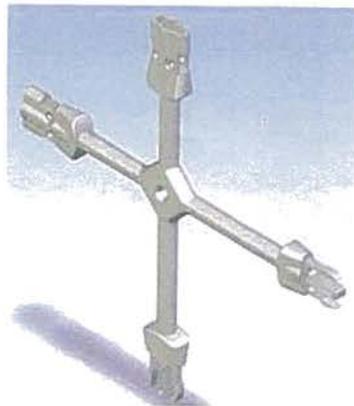


# 4. Technical Aspects

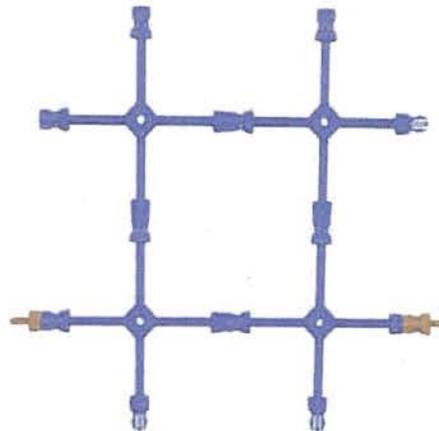
While the original barrier worked 100%, a new enhanced barrier has been designed since the City of Cockburn trial at Coogee Beach in early 2014. The new barrier has been designed to be able to be installed without pylons, is easier to install and easier to maintain. The option of the barrier without pylons will have additional cost savings.

## Barrier Components

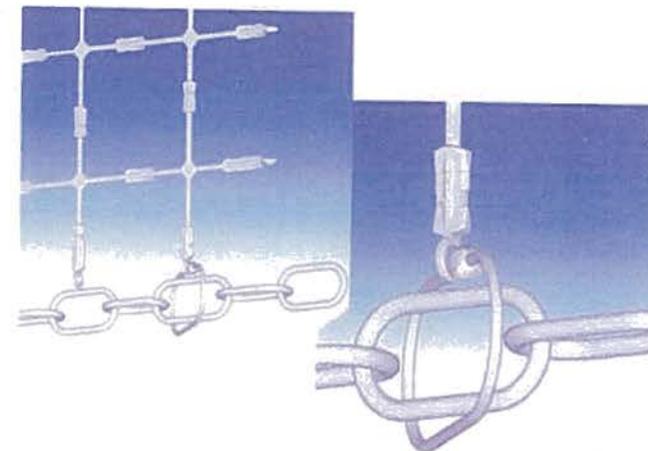
MODULE DESIGNED WITH EASY CLIP IN CONNECTORS WITH PINS TO PREVENT VANDALISM



MODULES CONNECTED TOGETHER TO FORM THE BARRIER



NYLON 6 STRENGTHENED LOOP TO KEEP BARRIER IN PLACE



## 4. Technical Aspects cont.



The 24mm Grade U2 Stud Link Ground Chain is secured to the seabed as a weight. 25kg Superay Drag Embedment Anchors are used to secure the barriers to the seabed. A strengthened nylon loop is used to attach the barrier to the chain.

The barrier is transported in a sea container to the beach. It is removed from the sea container and assembled on the beach, pulled into the water by boat, placed into position, then anchored by commercial divers.

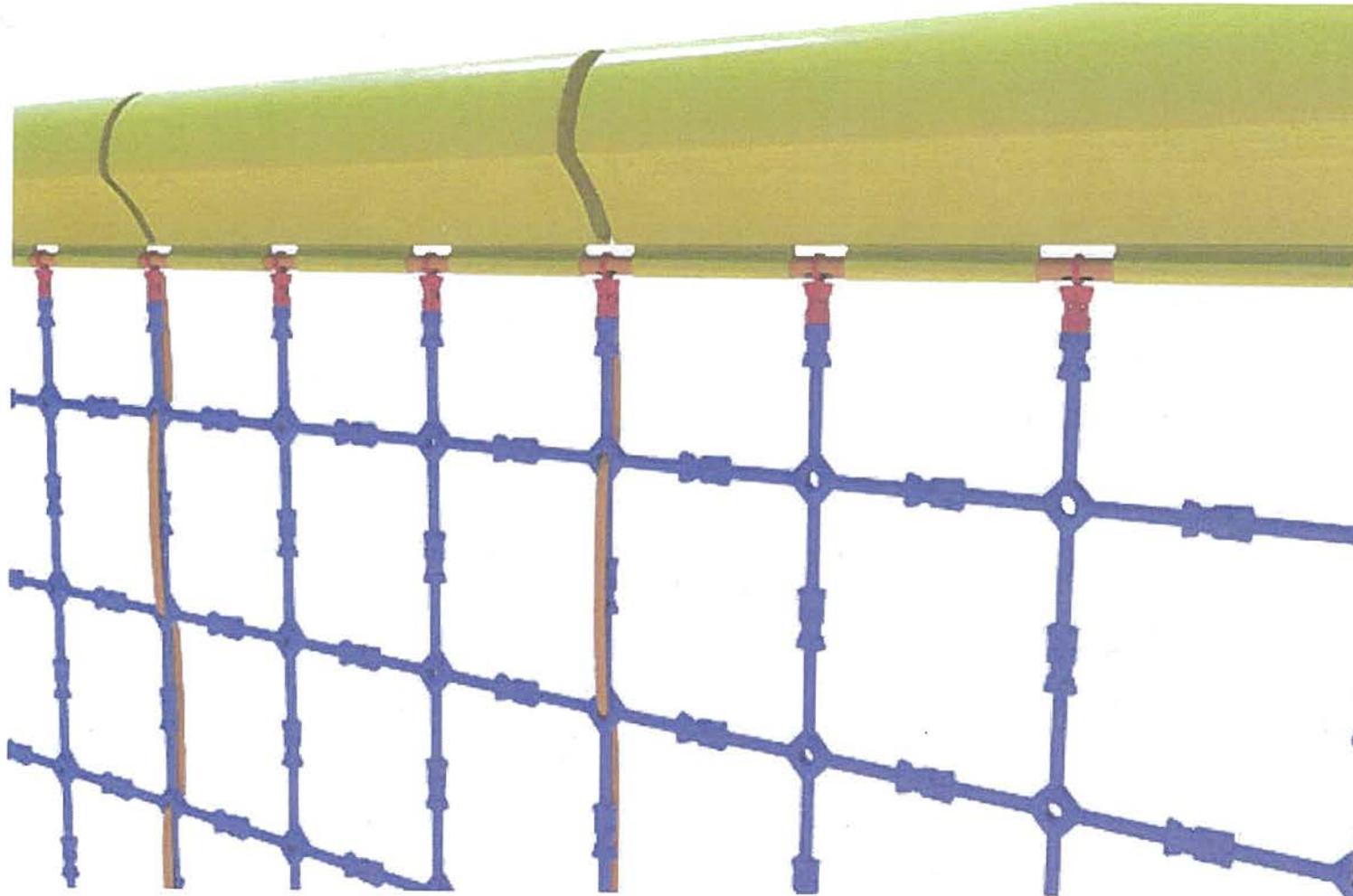
To increase the strength there is a 6mm plasma rope which is threaded through the barrier from top to bottom and tied off to give extra strength. A 6mm plasma rope can be threaded every 300mm apart and has a breaking strain of 3 ½ tonne.

Floats are installed between every Cross Connector Module to create an even and sturdy barrier.

A sealed blow moulded float component is connected to the Cross Connector Modules through the Male/Female End Connector with 20mm 3 Strand SUPERDAN Rope.

The Eco Shark Barrier can be installed in locations with depths of up to 10 metres. With modifications to the buoyancy, chains and anchors, the barrier can be adjusted to be installed in depths of up to 20 metres to go out past surf breaks.

## 4. Technical Aspects cont.



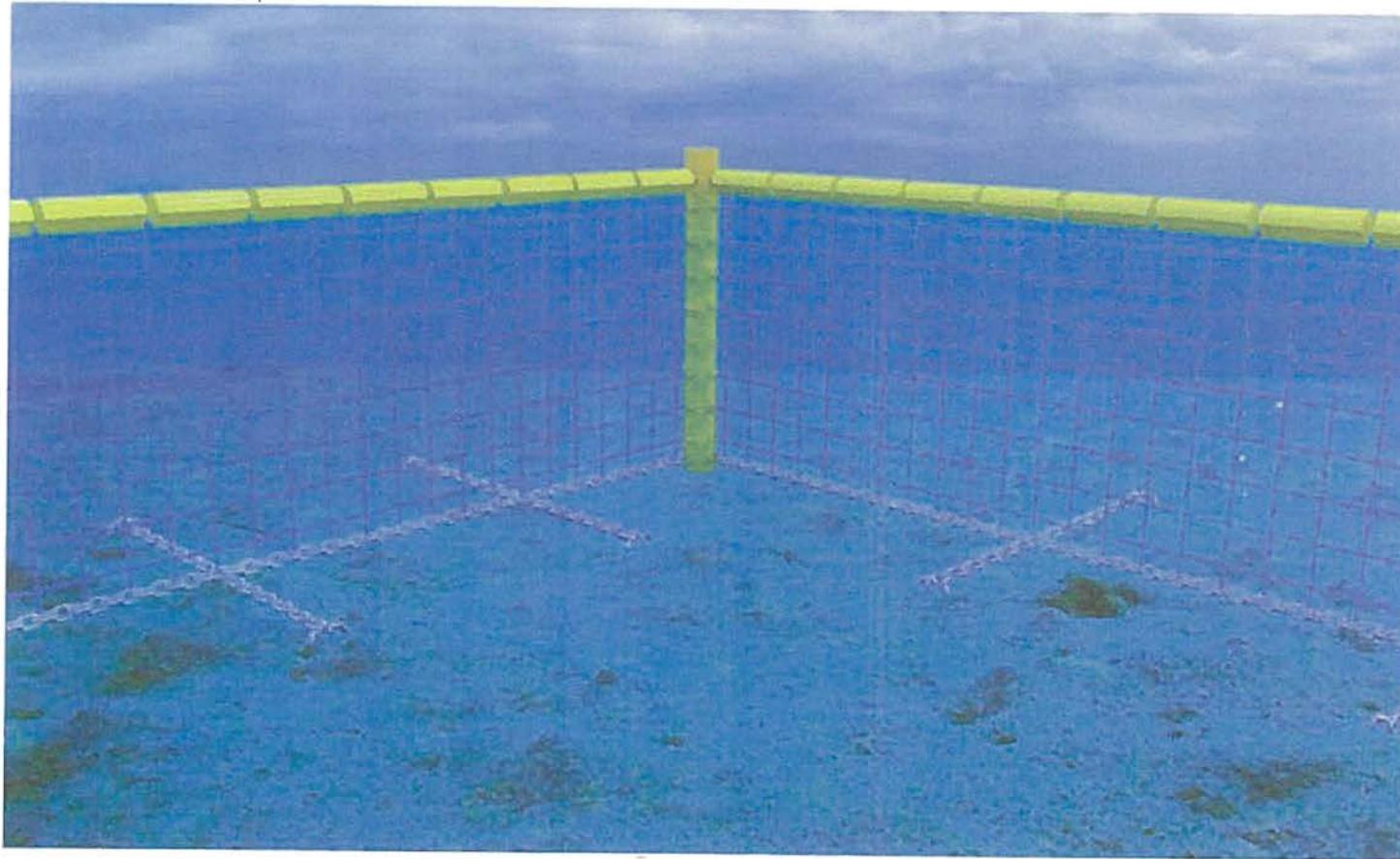
Eco Shark Barrier Pty Ltd

14

## 4. Technical Aspects cont.



Barrier with Pylon

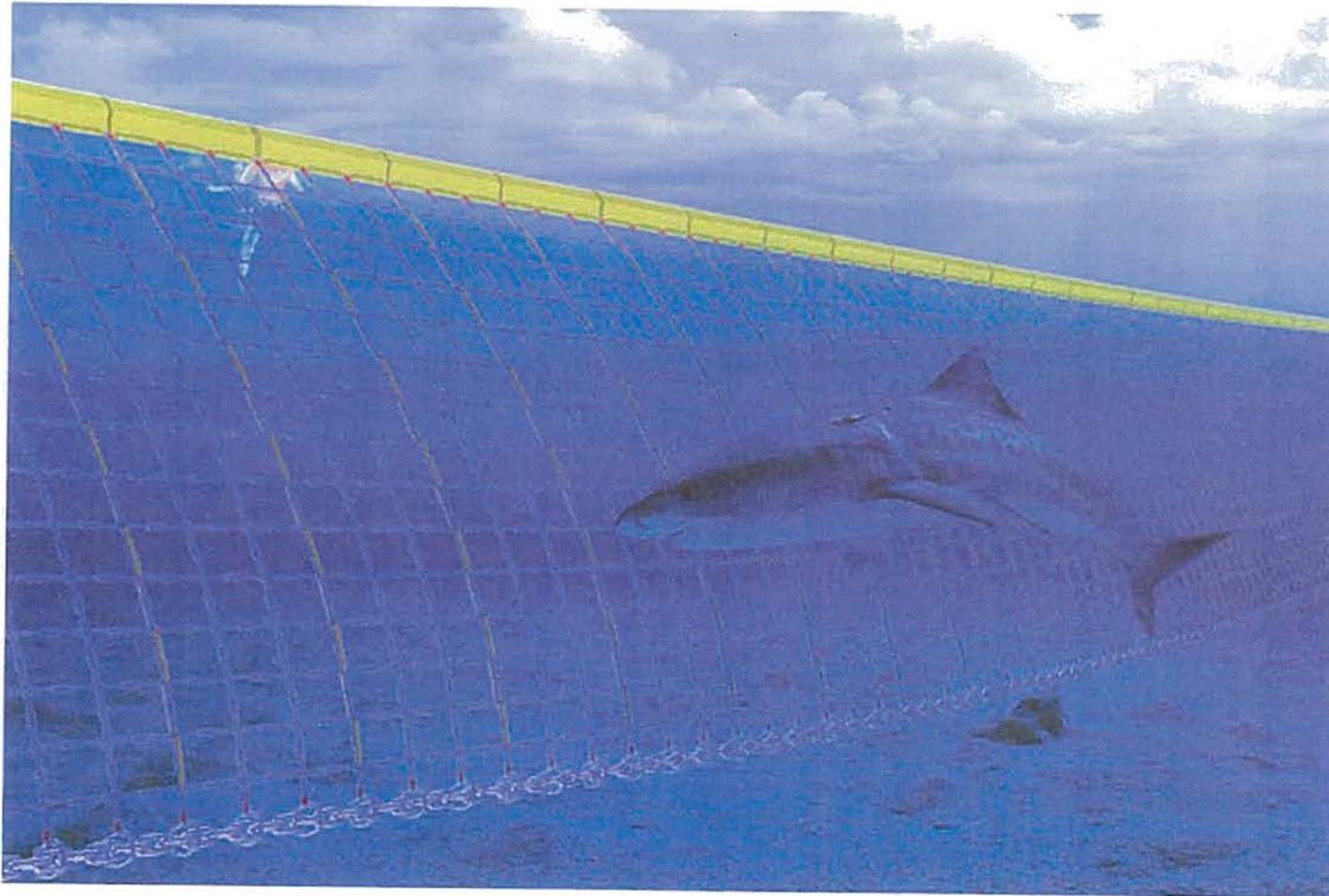


Eco Shark Barrier Pty Ltd

## 4. Technical Aspects cont.

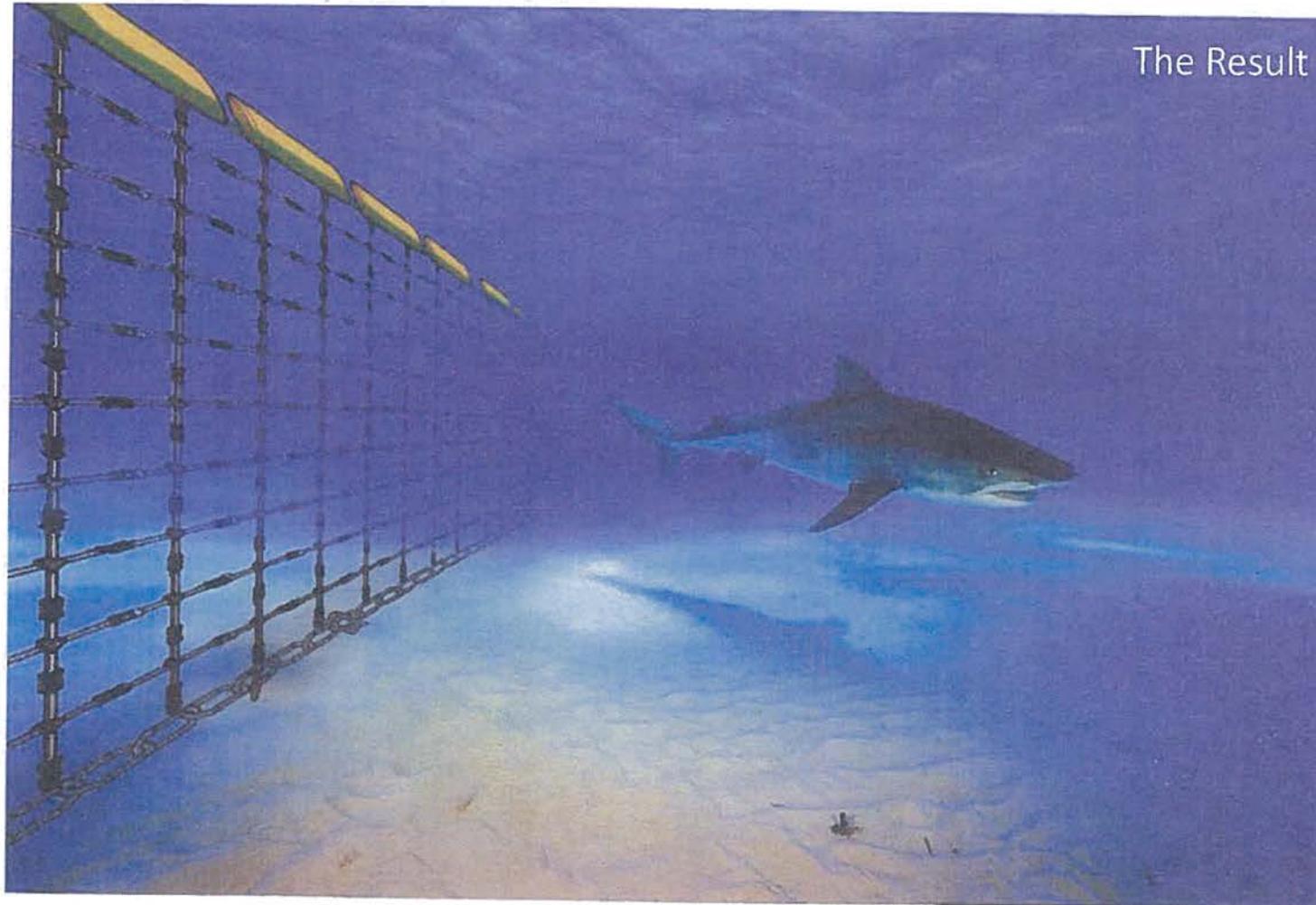


Barrier with no pylons



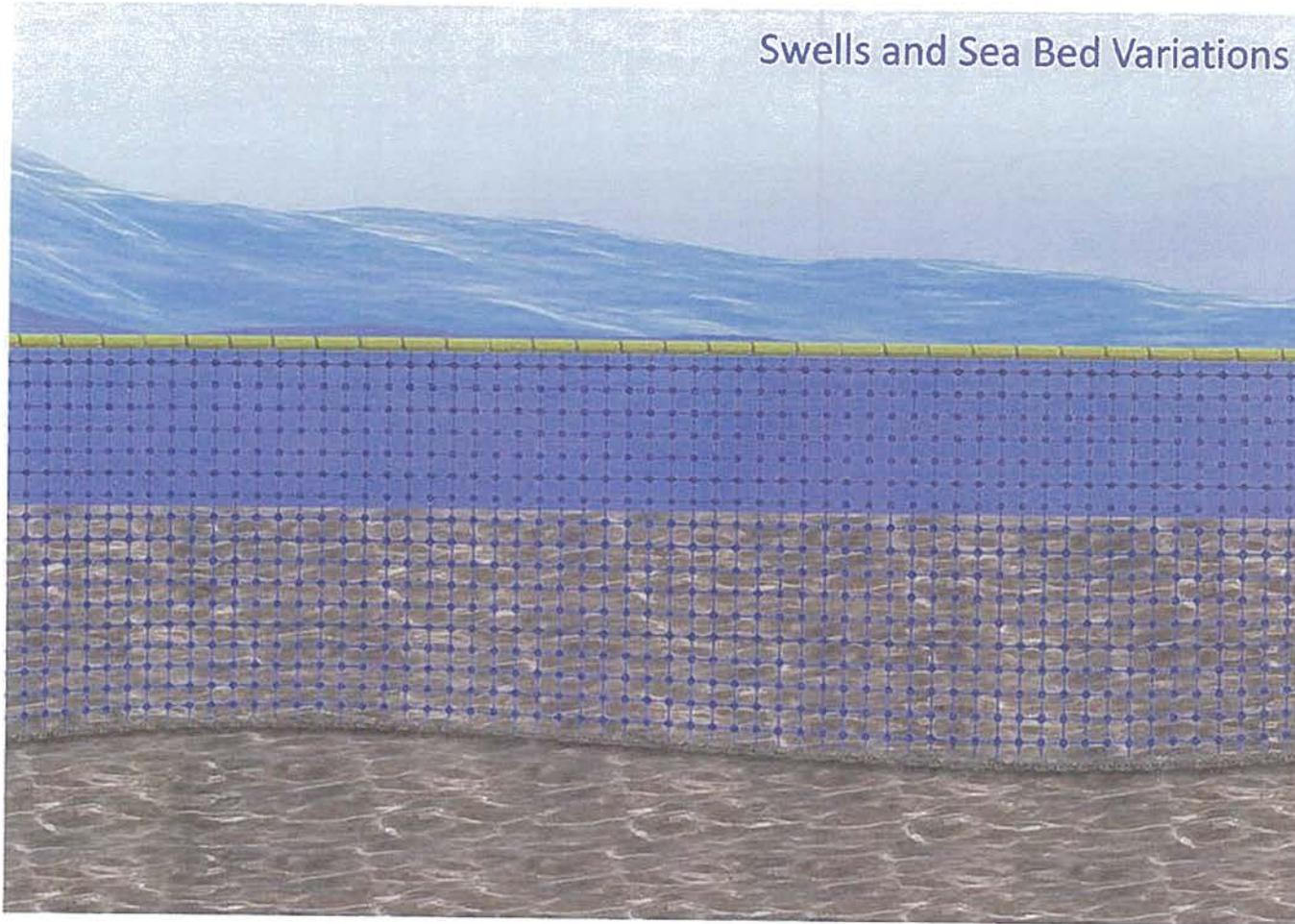
Eco Shark Barrier Pty Ltd

## 4. Technical Aspects cont.

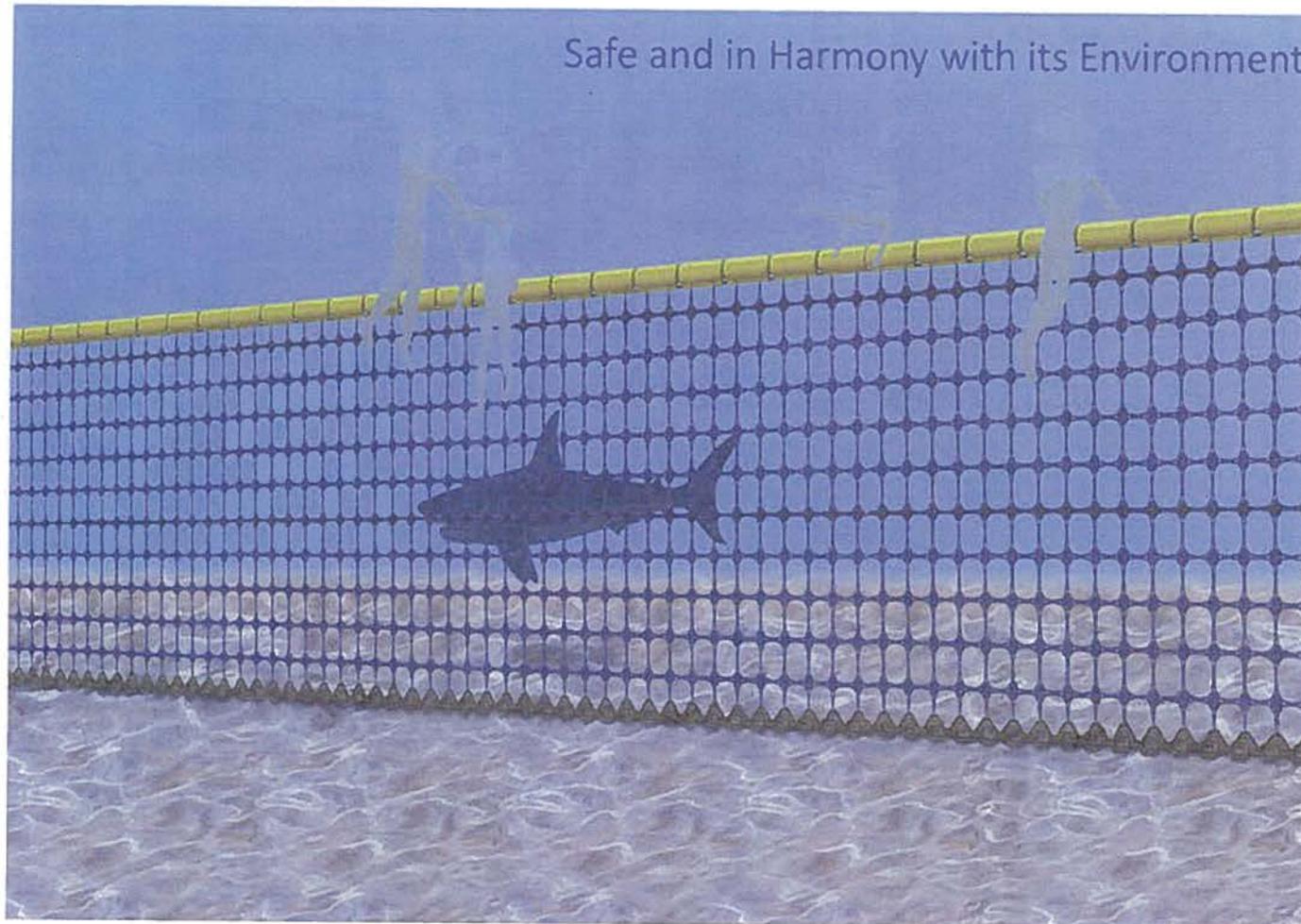


Eco Shark Barrier Pty Ltd

## 4. Technical Aspects cont.



## 4. Technical Aspects cont.



## 4. Technical Aspects cont.

### Frequently asked questions



#### **Does it trap marine life?**

No. The Eco Shark Barrier has been designed not to do so. "Nets" are made from rope and are flimsy. When an animal swims into them, they get caught up, struggle to escape, and often results in them entangling themselves and drowning.

The Eco Shark Barrier is not a "Net", it is a "Barrier". It is made from a strong, hard, but flexible plastic. The openings in the barrier measure at their widest point 30cm, any marine life within these measurements, can freely swim in and out of the barrier with no harm. Any animal larger than the openings will simply bounce off the barrier, think of it as a wall. We have seen Dolphins swimming along side the barrier, which demonstrates that they are aware of it and would lead us to believe that they can identify the structure with their sonar. To date there has been NO BY-CATCH.

#### **Can it be installed in moderate to heavy surf/swells?**

Yes. The Eco Shark Barrier has been designed to adapt to any conditions. Each module of the barrier has re-enforcement holes, which allow rope to be threaded through to strengthen the barrier. The ropes run over and through the barrier, at no time are there any free ropes to entangle any marine life.

#### **Does it affect the sediment (shifting sands)?**

MP Rogers - Consulting engineers specialising in coastal, port and marine projects, found no significant changes to Coogee Beach Western Australia, in the 4 month long trial.

# 4. Technical Aspects cont.

## Our experts



### **Engineered Water Systems:**

EWS prepared a report on the installation of the pylons at Coogee Beach. Refer Annexure 5.1 and will supply and install any pylons with navigational lights if required.

### **Hooper Designs:**

Robert Hooper has assisted us with the design of the new barrier. Robert has 30 years experience in marine construction.

### **MP Rogers and Associates Pty Ltd:**

MP Rogers and Associates provided us with expert opinion on Impacts on Sediment Transport. No significant impact was reported during the Coogee Beach trial

### **NMS Solutions:**

NMS are consulting naval architects and engineers and have provided us with a mooring design to secure the barrier at each end with piles and / or rock anchors. Details of a proposed Mooring Design for Cottesloe Beach can be found in Annexures section 4.1.

### **Indianic Dive:**

Indianic Drive will install every barrier, and supply and install boat exclusion buoys in Western Australia and will certify correct installation according to NMS Solutions specifications.

## 5. Costings for Coogee Beach



### **Cost to Purchase:**

Cost: \$255,000

Estimated Maintenance and Inspection Cost per annum: \$10,000 (payable by City of Cockburn)

### **Cost to Lease from Eco Shark Barrier:**

Cost: \$100,000 per annum over 3 years

Estimated Maintenance and Inspection Cost per annum: \$10,000

Total Leasing Cost: \$110,000 per annum

### **Cost to Lease via All Leasing:**

\$65,000 per annum over 7 years

### **Note – Estimated Maintenance and Inspection Cost per annum includes:**

(During fine conditions, inspection of barrier will be conducted once every four weeks. During stormy conditions, inspection of barrier will be conducted weekly or daily, depending on weather forecast or reports. One off Cleaning will be conducted just before summer season)

# 6. Helpful Links regarding Eco Shark Barrier



Webpage:

<http://www.ecosharkbarrier.com.au/>

YouTube promotion:

<https://www.youtube.com/watch?v=C1hDGndPgJg>

Facebook:

<https://www.facebook.com/ecosharkbarrier?ref=hl>

Twitter:

<https://twitter.com/EcoSharkBarrier>

City of Cockburn:

[http://www.cockburn.wa.gov.au/Meetings and Minutes/Minutes and Agendas/3548-05 bocm agenda 08052014.pdf](http://www.cockburn.wa.gov.au/Meetings%20and%20Minutes/Minutes%20and%20Agendas/3548-05%20bocm%20agenda%2008052014.pdf)

Channel Ten coverage of removal of Barrier:

<https://www.youtube.com/watch?v=vWVgbrIVdAc>

Last day of City of Cockburn trial footage taken by Simon Blears (member of the public)

<http://vimeo.com/93119946>

# 7. Annexures



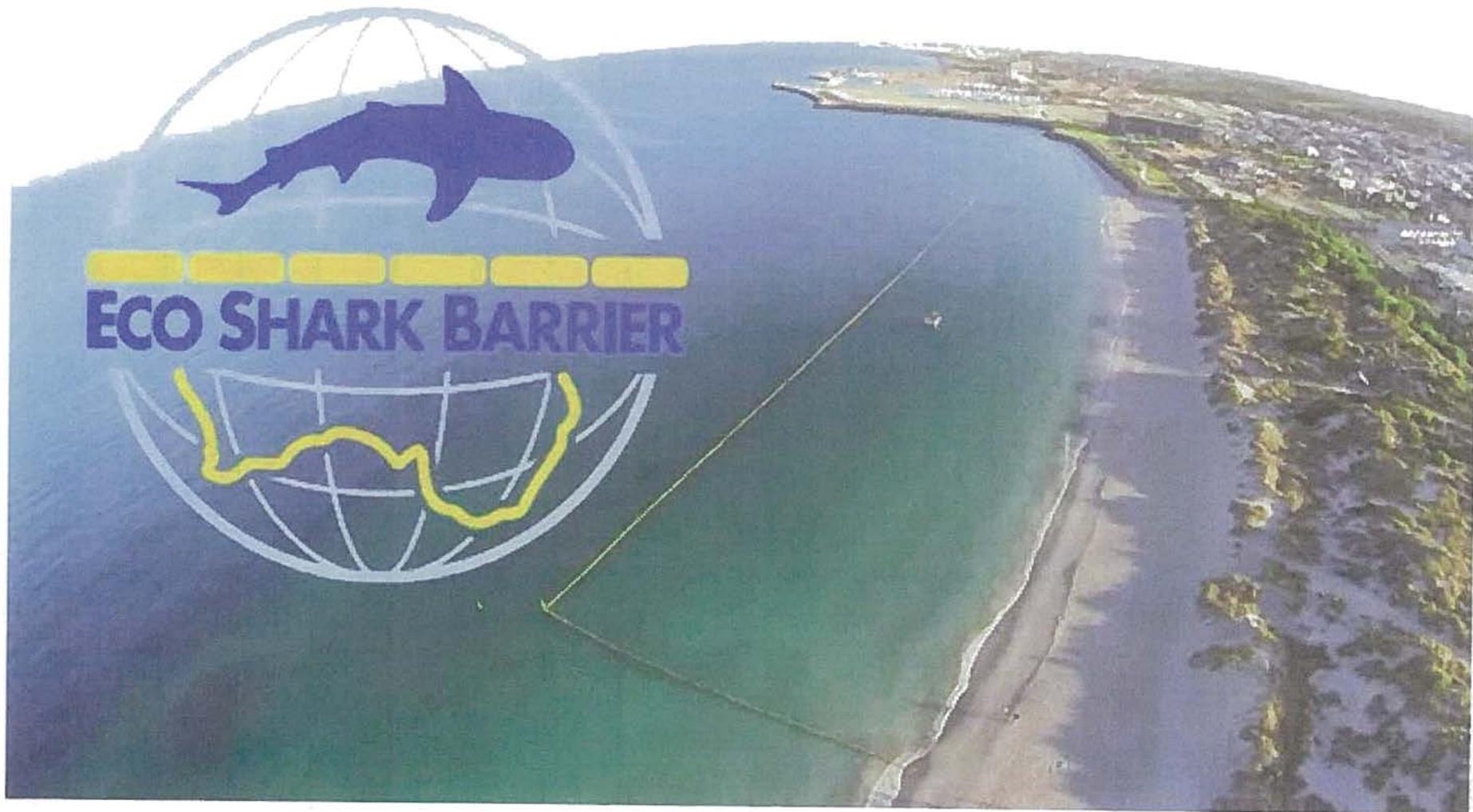
## **Technical Aspects:**

4.1 NMS Naval Architecture and Marine Solutions Covering Letter and Mooring Design for Cottesloe.

## **General Information:**

Patents

Certificate of Company Registration



For more information, please contact Craig Moss: Founder, Inventor, Patent Owner and  
Managing Director of Eco Shark Barrier Pty Ltd. Phone: 0403 185 302.  
Web: [www.ecosharkbarrier.com.au](http://www.ecosharkbarrier.com.au); Email: [enquiries@ecosharkbarrier.com.au](mailto:enquiries@ecosharkbarrier.com.au)

EMAIL ONLY: [ultimatelimestone@inet.net.au](mailto:ultimatelimestone@inet.net.au)

WRAYS

Contact: Peter Hille  
Principal/Associate: Peter Hille

19 June 2014

Eco Shark Barrier Pty Ltd  
59 Kingsley Drive  
KINGSLEY WA 8028

Attention: Mr C J Moss

Dear Mr Moss

**Australian Patent Application 2014202968**  
**Craig Justin Moss**  
**Barrier**  
Our ref: 253297

In accordance with your instructions, the above application has been filed at the Patent Office with the complete specification.

Particulars of the application are as follows:

Application Number	2014202968
Date Lodged	2 June 2014
Examination Due	2 June 2019
Annuity Fee Due	2 June 2018 and annually thereafter

The official filing receipt and a copy of the specification as filed are enclosed. Our invoice for this service will be sent to you at a later date.

Please read the 'Important Notice' which is also enclosed herewith.

It is essential that you inform us of any changes of address so that we can notify you regarding renewal fees.

Although we normally issue reminders concerning renewal dates, we accept no responsibility for any errors or omissions. We therefore strongly recommend that you add this case to your own renewal reminder system to operate independent of ours.

Yours sincerely  
WRAYS



Peter Hille  
Associate

Encl: Filing Receipt  
Specification  
Important Notice

CC - Email Only: Ms L Moss  
E: [lcmoss@inet.net.au](mailto:lcmoss@inet.net.au)

[www.wrays.com.au](http://www.wrays.com.au)

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EMAIL ONLY: [ultimatellimestone@inet.net.au](mailto:ultimatellimestone@inet.net.au)

WRAYS

Contact: Peter Hille  
Principal/Associate: Peter Hille

18 June 2014

Eco Shark Barrier Pty Ltd  
59 Kingsley Drive  
KINGSLEY WA 8026

Attention: Mr C J Moss

Dear Mr Moss

**Australian Innovation Patent Application 2014100588**  
**Craig Justin Moss**  
**Barrier**  
Our ref: 253264

In accordance with your instructions, the above application accompanied by a patent specification has been filed at the Patent Office.

Particulars of the application are as follows:

Application Number: 2014100588  
Date Lodged: 2 June 2014  
Annuity Fee Due: 2 June 2016 and annually thereafter

The official filing receipt is enclosed, together with a copy of the specification as filed. Our invoice in respect of this action will be forwarded to you at a later date.

The application will be reviewed by the Patent Office and if found to be acceptable a patent will be granted thereon. We will advise you of further developments in this matter in due course. It is essential that you inform us of any changes of address so that we can notify you regarding renewal fees.

**Although we normally issue reminders concerning renewal dates, we accept no responsibility for any errors or omissions. We therefore strongly recommend that you add this case to your own renewal reminder system to operate independent of ours.**

Yours sincerely  
WRAYS



Peter Hille  
Associate

Encl: Official Filing Receipt  
Specification

CC - Email Only: Ms L Moss  
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[www.wrays.com.au](http://www.wrays.com.au)

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EMAIL ONLY: [ultimatellimestone@inet.net.au](mailto:ultimatellimestone@inet.net.au)

WRAYS

Contact: Peter Hille  
Principal/Associate: Peter Hille

11 June 2014

Eco Shark Barrier Pty Ltd  
59 Kingsley Drive  
KINGSLEY WA 6026

Attention: Mr C J Moss

Dear Mr Moss

International Patent Application PCT/AU2014/000573  
Craig Justin Moss  
Barrier  
Our ref: 253009

In accordance with your instructions, an International application under the PCT was filed in respect of the above on 2 June 2014 and allotted the number PCT/AU2014/000573. The application claims priority from Australian Patent Applications 2013901977 and 2014900084.

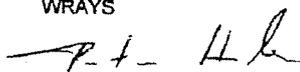
The official filing receipt and a copy of the specification as filed are enclosed for your records, and further advice on the progress of the application will be given to you in due course.

It is essential that you notify us of any change of address so that we may keep you informed of further developments.

Patent applicants should be aware of bogus invoices purporting to offer some form of patent listing or registration. We are aware of several companies that write to patent owners enclosing an invoice apparently relating to some form of patent listing or 'registration'.

The only invoices concerning patents to which clients of Wrays should respond are those received from this firm – we recommend you IGNORE all others. If in any doubt, please contact us.

Yours sincerely  
WRAYS



Peter Hille  
Associate

Encl: Official Filing Receipt  
Specification

CC - Email Only:  
Ms L Moss  
E: [leemoss@inet.net.au](mailto:leemoss@inet.net.au)

[www.wrays.com.au](http://www.wrays.com.au)

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## Certificate of Registration of a Company

This is to certify that

**ECO SHARK BARRIER PTY LTD**

**Australian Company Number 161 951 195**

is a registered company under the Corporations Act 2001 and  
is taken to be registered in Western Australia.

The company is limited by shares.

The company is a proprietary company.

The day of commencement of registration is  
the fifteenth day of January 2013.

Issued by the  
Australian Securities and Investments Commission  
on this fifteenth day of January, 2013.



Greg Medcraft  
Chairman



CERTIFICATE



**Premier of Western Australia**

Your Ref: 064/030  
Our Ref: 24-482741

Mr Logan K. Howlett, JP  
His Worship the Mayor  
City of Cockburn  
PO Box 1215  
BIBRA LAKE WA 6965

Dear Mayor

Thank you for your letter dated 5 June 2014, requesting State Government co-funding for the three year extension of the Eco Shark Barrier beach enclosure trial at Coogee Beach.

The State Government has invested more than \$22 million in shark hazard mitigation measures, including a beach enclosure trial in Dunsborough, and a range of research projects. I commend the City of Cockburn for its trial of a beach enclosure at Coogee Beach.

I understand the Office of Science within the Department of the Premier and Cabinet has advised officers at the City of Cockburn that an evaluation of the beach enclosure installed over summer at Dunsborough Beach (funded by the State Government) is currently underway. Future funding options will be considered once the evaluation is finalised.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'Colin Barnett'.

Colin Barnett MLA  
**PREMIER; MINISTER FOR SCIENCE**

24 JUN 2014



Government of **Western Australia**  
Department of the **Premier and Cabinet**  
Office of Science

Your Ref: 064/030  
Our Ref: D1403379

<b>CITY OF COCKBURN</b>
DOC No
27 JUN 2014
SUBJECT
064/030
RETENTION
83.2 A 5
PROPERTY
APP
ACTION
MICHAEL LITTLETON

Michael Littleton  
Director of Engineering and Works  
City of Cockburn  
PO Box 1215  
BIBRA LAKE DC WA 6965

Dear Mr Littleton

Thank you for your letter dated 4 June 2014, requesting State Government co-funding for the three year extension of the beach enclosure trial at Coogee Beach. I note that Mayor Howlett has written in similar terms to the Premier and Hon Ken Baston MLC, Minister for Fisheries.

As you are probably aware the State Government provided \$165,370 for a netting and pile based beach enclosure trial at Dunsborough. An independent evaluation of that trial is currently underway. The outcome of the evaluation will be used to assist the State Government to develop future shark hazard mitigation strategies, including funding options for beach enclosures.

It may interest you to learn that early indications of the costs associated with the Dunsborough barrier (including annual installation and maintenance) are significantly less than those you appear to envisage for the City of Cockburn's future commitments. However, cost of ongoing maintenance and installation will be considered as part of the evaluation. I will keep you informed of future developments relating to this matter.

Yours sincerely

Wendy Attenborough  
**DIRECTOR**

**19 JUN 2014**

Dumas House, 2 Havelock St, West Perth WA 6005  
Locked Bag 3001 West Perth WA 6872  
Telephone (08) 6552 5444 Facsimile (08) 6552 6733 admin@dpc.wa.gov.au  
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