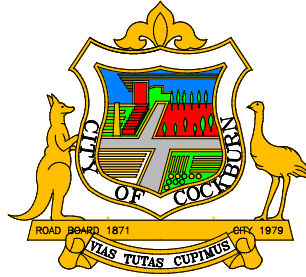


# **CITY OF COCKBURN**



## **ORDINARY COUNCIL**

## **AGENDA PAPER**

### **FOR**

**THURSDAY, 9 MAY 2013**

# CITY OF COCKBURN

## SUMMARY OF AGENDA TO BE PRESENTED TO THE ORDINARY COUNCIL MEETING TO BE HELD ON THURSDAY, 9 MAY 2013 AT 7:00 PM

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**CITY OF COCKBURN****AGENDA TO BE PRESENTED TO THE ORDINARY  
COUNCIL MEETING TO BE HELD ON  
THURSDAY, 9 MAY 2013 AT 7:00 PM**

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- 1. DECLARATION OF MEETING**
- 2. APPOINTMENT OF PRESIDING MEMBER (If required)**
- 3. DISCLAIMER (To be read aloud by Presiding Member)**

Members of the public, who attend Council Meetings, should not act immediately on anything they hear at the Meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

- 4. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS AND CONFLICT OF INTEREST (by Presiding Member)**
- 5. APOLOGIES AND LEAVE OF ABSENCE**
- 6. ACTION TAKEN ON PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE**
- 7. PUBLIC QUESTION TIME**
- 8. CONFIRMATION OF MINUTES**
  - 8.1 (OCM 09/05/2013) - ORDINARY COUNCIL MEETING 11 APRIL 2013**

**RECOMMENDATION**

That Council adopt the Minutes of the Ordinary Council Meeting held on Thursday, 11 April 2013, as a true and accurate record.

**COUNCIL DECISION**

**8.2 (OCM 09/05/2013) - SPECIAL COUNCIL MEETING 4 APRIL 2013**

**RECOMMENDATION**

That Council adopt the Minutes of the Special Council Meeting held on Thursday, 4 April 2013 as a true and accurate record.

**COUNCIL DECISION**

**9. WRITTEN REQUESTS FOR LEAVE OF ABSENCE**

**10. DEPUTATIONS AND PETITIONS**

**11. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (If adjourned)**

**12. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER**

**13. COUNCIL MATTERS**

**13.1 (OCM 09/05/2013) - MINUTES OF THE GRANTS AND DONATIONS COMMITTEE MEETING - 18 APRIL 2013 (162/003) (R AVARD) (ATTACH)**

**RECOMMENDATION**

That Council receive the Minutes of the Grants and Donations Committee Meeting held on 18 April 2013 and adopt the recommendations contained therein.

## COUNCIL DECISION

### Background

The Council of the City of Cockburn established the Grants and Donations Committee to recommend on the level and the nature of grants and donations provided to external organisations and individuals. The Committee is also empowered to recommend to Council on donations and sponsorships to specific groups and individuals.

### Submission

To receive the Minutes of the Grants and Donations Committee and adopt the recommendations of the Committee.

### Report

Council approved a budget for grants and donations for 2012/13 of \$1,010,000 to be distributed as grants, donations and sponsorship.

At its meeting of 16 July 2012 the Committee recommended a range of allocations of grants, donations and sponsorship which were duly adopted by Council on 9 August 2012.

The March 2013 round of grants, donations and sponsorship funding opportunities has now closed and the Committee at its meeting of 18 April 2013, considered revised allocations for the grants and donations budget, as well as the following applications for donations and sponsorship.

A summary of the donations for general operating expenses recommended to Council are as follows:

Business Foundations	\$10,000
City of Cockburn Pipe Band	\$9,000
Second Harvest	\$10,000
Hamilton Hill YouthCARE	\$9,000
South Lake Ottey Family Centre	\$7,000
Cockburn Central YouthCARE Council	\$24,000
Friends of the Community	\$2,160
Constable Care Child Safety Foundation (Inc.)	\$10,000
Training Ship Cockburn Parents Committee (Inc.)	\$2,000

Volunteer Home Support Inc.	\$5,000
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A summary of the sponsorship recommended by the Committee is as follows:

Hamilton SHS - HSHS 50th Anniversary	\$4,000
Austin Keyte - Philippines Immersion	\$500
Zakary Brown - World Challenge Expedition to Sri Lanka	\$500
Melville Cockburn Chamber of Commerce	\$20,000
Suzanne Marsella - Clinical Placement	\$1,000
Atwell College - 2013 Canberra Tour	\$3,000

### **Strategic Plan/Policy Implications**

#### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.
- Communities that take pride and aspire to a greater sense of community.
- Promotion of active and healthy communities.

#### **Leading and Listening**

- A responsive, accountable and sustainable organisation

### **Budget/Financial Implications**

Council approved a budget for grants and donations for 2012/13 of \$1,010,000 to be distributed as grants, donations and sponsorship.

Following is a summary of the grants, donations and sponsorship allocations proposed by the Committee.

Committed/Contractual Donations	\$472,863
Specific Grant Programs	\$335,978
Donations	\$156,160
Sponsorship	\$ 45,000
<b>Total</b>	<b>\$1,010,000</b>

The next Grants and Donations Committee Meeting will be held in July 2013 to recommend allocations for 2013/14.

The next round of grants, donations and sponsorship funding will be advertised in August/September 2013.

### **Legal Implications**

Nil

### **Community Consultation**

In the lead up to the March 2013 round, grants, donations and sponsorship funding opportunities were promoted through the local media and Council networks. The promotional campaign has comprised:

- Three advertisements running fortnightly in the Cockburn Gazette's City Update on 19/02/13, 5/03/13 and 19/03/13.
- One quarter page advertisement in the Cockburn Gazette's wrap on 19/03/2013
- Four advertisements running fortnightly in the City of Cockburn Email Newsletter.
- Advertisement in the February Cockburn Soundings.
- All members of the Cockburn Community Development Group, Regional Parents Group and Regional Seniors Group have been encouraged to participate in the City's grants program.
- Additional Advertising through Community Development Promotional Channels:
  - Gazette part of Full page Ad 12 Feb 2013, 12 March 2012
  - Community Development Calender distributed to all NFP groups in Cockburn
  - Community Development ENews: 13/3/13, 28/2/13, 15/2/13, 16/1/13

### **Attachment(s)**

1. Minutes of the Grants and Donations Committee Meeting on 18 April 2013.
2. Grants and Donations Allocations 2012/13 as recommended by the Committee.

### **Advice to Proponent(s)/Submissioners**

N/A.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

#### 14. PLANNING AND DEVELOPMENT DIVISION ISSUES

##### 14.1 (OCM 09/05/2013) - DEDICATION AS PUBLIC ROAD - LOT 3001 (DEPOSITED PLAN 74231) MIGUEL ROAD, BIBRA LAKE - OWNER MOLTONI HOLDINGS P/L - APPLICANT CITY OF COCKBURN (4113473, 450007) (K SIM) (ATTACH)

###### RECOMMENDATION

That Council:

- (1) request that the Minister for Lands dedicate Lot 3001 on Deposited Plan 74231 Miguel Road, Bibra Lake as road reserve pursuant to Section 56(1) of the *Land Administration Act 1997*; and
- (2) indemnify the Minister for Lands against reasonable costs incurred in considering and granting the request in 1 above.

###### COUNCIL DECISION

#### Background

Council at its Special Council Meeting held on 20 September 2006 resolved as follows in respect of acquiring portion of Lot 410 Miguel Road, Bibra Lake for construction of Spearwood Avenue:

*That Council pays \$90/m2 for the land required from Lot 410 Miguel Road with settlement of the purchase price to be made by the 30 November 2006 unless some other suitable arrangements are agreed between the parties;*

Following the Council meeting of September 2006 a legal agreement was completed between Moltoni Holdings P/L and the City. The agreement sets out the terms of the transfer of that portion of Lot 410 Miguel Road (which is Lot 3001 on DP 74231) shown as 'Other Regional Roads' within the region and local planning schemes. The City has paid the purchase price and lodged a caveat on the title of Lot 410 to protect its interests in the land.

The purpose of this report is to finalise the matter by way of ensuring that portion of land for the road is dedicated finally as a road reserve.



## Submission

NA

## Report

The portion of Lot 410 Miguel Road, Bibra Lake will be dedicated as a road reserve for part of the completed Spearwood Avenue. The land has been surveyed such that an accurate lot descriptor now exists as Lot 3001 on DP 74231.

Lot 3001 is identical to the land description in the legal contract. Delays in preparing the survey plan have been brought about due to the nature of the earthworks being carried out on the site. Representatives from Moltoni Holdings P/L have also in the past asked for a modification to the alignment of the western boundary of Lot 3001. The modification sought was to straighten the boundary and thus increase the size of the balance land (Lot 202 on DP74231). Moltoni Holdings P/L has since been placed into receivership. The receiver has stated that he is not interested in a modification to the boundary, and will proceed on the basis of the legal contract.

The contract sets out that the road land has been acquired by agreement, pursuant to Section 168 of the *Land Administration Act 1997*. The Department for Regional Development and Lands have advised that a Road Dedication request pursuant to Section 56 of the *Land Administration Act 1997* is now required.

Following Council's resolution, the request will be forwarded to the Department. They will then instigate a process whereby the dedication will proceed and a balance title for Lot 202 will be issue.

## Strategic Plan/Policy Implications

### Growing City

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

### Leading & Listening

- Manage our financial and infrastructure assets to provide a sustainable future.

### Moving Around

- An integrated transport system which balances environmental impacts and community needs.

### **Budget/Financial Implications**

Nil. Financial transactions have been completed and road constructed.

### **Legal Implications**

Provisions of the *Land Administration Act 1997* refer.

### **Community Consultation**

N/A

### **Attachments**

1. Deposited Plan 74231
2. Location Plan

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **14.2 (OCM 09/05/2013) - PROPOSED LOCAL PLANNING POLICY ROBB JETTY AND EMPLACEMENT DESIGN GUIDELINES (CONSIDERATION OF ADOPTION FOR FINAL APPROVAL) (110/051) (C CATHERWOOD) (ATTACH)**

### **RECOMMENDATION**

That Council adopt the draft Local Planning Policy (Robb Jetty and Emplacement Design Guidelines), as shown in Attachment 2, for final approval subject to the following modifications:

1. All changes as shown as 'tracked changes' in Appendix 1 of the draft Local Planning Policy.
2. All diagrams to be updated to be legible (including legends and increase in font size annotating dimensions).
3. Correction of all grammatical and typographical errors (especially use of semi colons).
4. Ensure Building Height plan reflects that in Local Structure Plans.
5. All imagery to be updated to ensure building materials

contrary to the content of the Design Guidelines is removed.

## COUNCIL DECISION

### Background

For a number of years the State Government has been working toward realising the vision for the Cockburn Coast development. The project is intended to see the redevelopment of the former Robb Jetty industrial area and the South Fremantle Power Station. A number of planning stages have been realised in recent years briefly described below.

1. The Cockburn Coast District Structure Plan 2009 ("CCDSP 2009") was prepared to guide future land use and transport initiatives within the area stretching between South Beach and the Port Coogee marina.
2. In 2012 this was supplemented and in part refined by the Cockburn Coast District Structure Plan Part 2 ("CCDSP Part 2") prepared on behalf of Landcorp.
3. In 2011 the Metropolitan Region Scheme ("MRS") Amendment No. 1180/41 was made effective on 16 August 2011 to rezone the North Coogee industrial area from 'Industry' to 'Urban' to reflect the outcomes of the CCDSP Part 2. The South Fremantle Power Station site has been predominately rezoned to 'Urban Deferred', with a portion south of the Power Station building remaining 'Parks and Recreation' reserve.
4. During 2011 and 2012 Council undertook several modifications to City of Cockburn Town Planning Scheme No. 3 ("TPS3") to reflect the change in the MRS, including replacement of previous zones with Special Use areas to reflect the desired use mix in the Newmarket Precinct area and introduction of a 'Development' zone for the area south of Rollinson Rd.
5. At the January 2013 DAAPS Committee meeting and subsequent February 2013 Council meeting, the Design Guidelines were

adopted as a Draft Local Planning Policy to enable them to be advertised for public consultation.

The report on this item has been presented directly to Council, rather than back through the DAAPS Committee first. The adoption of Design Guidelines prior to the local structure plans is a requirement under the Town Planning Scheme provisions for Cockburn Coast. The local structure plans were advertised in late 2012. Advertising for the Design Guidelines closed on 25 March 2013. The next available DAAPS Agenda the Design Guidelines could be included would have been 23 May 2013. These minutes would then need to go to the 13 June 2013 Council meeting. This would mean the local structure plans would need to wait until this June meeting also. This would create a dilemma given the Town Planning Scheme also requires the consideration of submissions on local structure plans within 60 days of the close of submissions.

The Design Guidelines which are the topic of this report reflect the requirements of the City's TPS3 which require an appropriate set of Design Guidelines to be adopted either before or with the local structure plans. This forms the topic of this report, to specifically consider the Design Guidelines Local Planning Policy for adoption.

A report was previously presented to the 11 April 2013 Council meeting. Council deferred their consideration pending further liaison with landowners who had expressed concerns. A meeting with these landowners was held 16 April 2013 and a range of issues were discussed including development contributions, traffic, existing businesses and the waste water pumping station.

None of those issues warrant further changes to the draft Design Guidelines and therefore the officer recommendation remains unchanged. However, there are additional inclusions in the related officer recommendations on the local structure plans.

### **Submission**

The Robb Jetty and Emplacement Design Guidelines have been submitted by HASSELL on behalf of Landcorp.

### **Report**

The purpose of this report is for Council to consider adopting the Robb Jetty and Emplacement Design Guidelines Local Planning Policy for adoption.

The Design Guidelines have been prepared to guide the development and urban form of the Cockburn Coast redevelopment area. The

design guidelines aspire to create a quality development that ensures the design aspirations of the Robb Jetty and Emplacement Local Structure Plans (LSPs) are achieved.

It is intended that these Design Guidelines be adopted as a Local Planning Policy pursuant to Clause 2.3.1 of TPS3. This will enable the Design Guidelines to be applied according to of TPS3.

The TPS3 provisions set out the matters that Design Guidelines shall address, which include: building heights, bulk and scale; private open space; walls and fencing; parking and access arrangements; and sustainable building design. This is achieved by the Draft Design Guidelines.

#### Modifications required

A number of modifications have been prepared to ensure the contents of the Design Guidelines are practical as well as capable of being assessed and implemented.

The majority of these modifications are shown as 'tracked changed' in the copy of Appendix 1 to the Design Guidelines. Most of the changes relate to minor corrections on the way the document is written - they are considered non-substantive in that regard.

Sections which have been recommended for deletion (on the basis they can be found elsewhere) include Affordable Housing, Ancillary Accommodation, Public Realm and sections of the Noise and Vibration Assessment section. These are more substantive changes.

#### Affordable Housing

It is unnecessary to duplicate the floor space bonus proposal which is outlined in the draft local structure plans and confusing to separate the incentives into two separate documents. The associated agenda item to consider the local structure plans includes recommendations to refine this section within the local structure plan documentation. This will include adding relevant definitions as well as providing a calculation methodology for the incentives proposed.

#### Ancillary Accommodation

The section on ancillary accommodation is also unnecessary. This aspect of development is already guided by requirements spelt out in the Residential Design Codes.

### Public Realm

The section on public realm does not belong in a Local Planning Policy to guide private realm development. However, there is a need to document expectations for public realm development in areas such as this where there are multiple landowners.

This includes identifying proposed landscaping themes, verge treatments (including items such as street furniture, bollard types, lighting types, paver styles) to achieve the desired streetscape character, including cross sections showing the location and extent of verge treatments. If these issues are not clearly documented then it will be difficult to achieve a cohesive streetscape character, particularly given that much of the land is in fragmented landownership.

Landcorp has recently commenced a guide for the Public Realm which it intends to discuss with the City's technical staff that approve and ultimately need to manage public realm areas and their infrastructure. This will be an important piece of work for the City to progress before the commencement of subdivisional works (estimated to start in 2014).

### Noise and Vibration Assessments

This section made no mention of the issue of vibration and this is recommended to be included. The scope of what a report into these matters needs to include should not be documented in the Design Guidelines. They should simply refer back to the relevant State Planning Policy and Quiet House Design Principles. This will ensure the robustness of the Design Guidelines should the requirements in these related documents ever change. It also makes clear to applicants the scope of such assessments.

### Additional commentary on car parking

The design guidelines provide for an appropriate response to car parking, noting this continues to be an issue of interest as the City transforms towards more medium density development. The amount of car parking to be provided is detailed in the Scheme. The design guidelines provide for a response to car parking management through appropriate screening of car parking areas to reduce their dominance. This will assist in the delivery of an attractive environment but with a sufficient level of car parking accommodated.

### Community Consultation Outcomes

The draft Robb Jetty and Emplacement Design Guidelines were advertised for public comment for a period of 21 days, ending on 25 March 2013.

Seven submissions were received on the Design Guidelines. Most submissions raised issues with the local structure plans which have already been raised as part of the reports on those items.

There were a number of typographical errors noted and these have been included in the attachment indicating the changes required. The most significant change recommended is to the 'end of trip' facilities for bicycles which seek to improve the standards proposed in the advertised version of the Design Guidelines.

### Conclusion

The Robb Jetty and Emplacement Design Guidelines are generally consistent with the underlying intent of the CCDSP 2009 and CCDSP Part 2. However, there are a number of modifications which are required to improve the clarity of their content, ensure they are complementary to the associated local structure plans and that they can provide sufficient guidance to subdivision and development proposals.

Subject to the modification of the Robb Jetty and Emplacement Design Guidelines in line with the officer recommendation and as shown as 'tracked changes' (see Attachment 2), it is recommended the Design Guidelines be adopted as a Local Planning Policy and forwarded to the Western Australian Planning Commission for their information.

### **Strategic Plan/Policy Implications**

#### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.

#### **Budget/Financial Implications**

N/A

## **Legal Implications**

N/A

## **Community Consultation**

Once the draft Robb Jetty and Emplacement Design Guidelines were lodged with the City advertising of the proposal took place in line with the requirements of the City's Scheme for local planning policy proposals. This advertising period ran for a period of 21 days from 5 to 25 March 2013.

Advertising included the following:

- Letters to all landowners with Robb Jetty and Emplacement areas of Cockburn Coast;
- Notices in the Cockburn Gazette inviting comment;
- Displays at the City's administration building and the City's libraries;
- Dedicated webpage on the City of Cockburn's website;

## **Attachment(s)**

1. Cockburn Coast Precinct Plan
2. Draft Local Planning Policy (Robb Jetty and Emplacement Design Guidelines) with changes shown tracked.
3. Schedule of Submissions.

## **Advice to Proponent(s)/Submissioners**

The applicant has been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

## **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

### **14.3 (OCM 09/05/2013) - LOCAL STRUCTURE PLAN (EMPLACEMENT) COCKBURN COAST (CONSIDERATION OF ADOPTION FOR FINAL APPROVAL) (110/067) (D DI RENZO) (ATTACH)**

#### **RECOMMENDATION**

That Council:

- (1) adopt the Emplacement Local Structure Plan, in line with the proposed rezoning of this area to 'Development' zone via Amendment No. 89 to the City's Town Planning Scheme No. 3 and subject to the following:
  1. Modification and Adoption of the Local Planning Policy



## Robb Jetty and Emplacement Design Guidelines.

2. Preparation of a Fire Management Plan in accordance with Planning for Bushfire Protection, which includes any interim fire management measures.
3. Updates to Section 3.4 of the Local Structure Plan report (Bushfire Hazard) and Figure 16 to reflect the Fire Management Plan, and to demonstrate a fire hazard assessment which includes the Emplacement Local Structure Plan area and adjacent Beeliar Regional Park.
4. Modification to Section 3.1 (Environmental Assets and Constraints) to specifically address the recommendations of the associated Ecological Assessment, and to specify the requirement for a spring flora and vegetation survey to be undertaken by individual landowners prior to subdivision or development of the land (where development proposes works to the land).
5. Modification to Figure 12 (Vegetation Type Analysis) within the Local Structure Plan report to reflect the mapping included within the Ecological Assessment.
6. Modification to Appendix E - Local Transport and Traffic Management Strategy of the Local Structure Plan report to include current and future intersection operations for the two intersections of Emplacement Crescent and Cockburn Road and to include one intersection that maintains a right hand turn from Emplacement Crescent if possible.
7. Modification to Figure 1 and Figure 9 of the Local Structure Plan report to reflect the indicative location of the switchyard/power substation as shown in the Infrastructure and Servicing Report (Appendix F).
8. Corrections to Public Open Space ("POS") figures in Table 3, Table 9, and throughout the Local Structure Plan report to accurately reflect the quantities of proposed POS, including the proportion of unrestricted and restricted open space as shown in the associated Local Water Management Strategy.
9. Identifying Alba Edible Oils as a current land use in Section 1.2.2 of the Local Structure Plan report.
10. Deletion of reference to an 'activity centre' zone under

Section 6.1 of the Emplacement Local Structure Plan report.

11. Inclusion of additional provisions in Section 8 (Part 1) requiring development proposals to ensure adequate interface, including fencing, to the Primary Regional Road Reserve to protect the conservation value of the Beeliar Regional Reserve.
12. Advising affected landowners in the Emplacement Local Structure Plan area of the requirement for a spring flora and vegetation survey to be undertaken by individual landowners prior to subdivision or development of the land (where development proposes works to the land).
13. Removing the footnote from the bottom of the Land Use Table contained in Part 1 and replace with text within Part 1 to explain when discretion may be granted by Council for Singles Houses (in line with the explanation given in Part 2).
14. Update the Affordable Housing sections to reflect the updated Australian Bureau of Statistics data.
15. Updating the Affordable Housing sections to reflect an incentive based approach; all references to mandatory requirements are to be removed.
16. Updating the Affordable Housing sections to also include a further bonus for 2 bedroom dwellings (relative the bonus given for 3 bedroom dwellings).
17. Updating the Affordable Housing section to revise the definition of Affordable Housing to be: "For the purposes of this Local Structure Plan, 'affordable housing' refers to either of the following:
  - \* Dwellings that are sold to Eligible Households at or below the benchmark price outlined in Table 4; or
  - \* Dwellings that are sold or transferred to a recognised affordable housing provider, which in turn leases or sells the properties to Eligible Households (under an approved affordable housing program); or
  - \* Private Provider selling to Eligible Households; or

\* Commonwealth or State endorsed affordability program”.

and include supplementary definitions for the terms “Eligible Households” and “Recognised affordable housing provider”.

18. Updating Part 1 to delete the reference to car parking standards being a ‘maximum’ rather than a ‘minimum’ and update the reference from the benchmarks being the Residential Design Codes to being as per the City of Cockburn’s Town Planning Scheme No. 3.
19. Updating the sections regarding Detailed Area Plans to provide clarity as to when they may be required and that in some instances the need may be negated due to the Design Guidelines Local Planning Policy.
20. Expanding the discussion in Part 2 (Regional Planning) to broaden the reference to Directions 2031 to discuss other elements of this plan.
21. Updating the discussion in Part 2 (Policies) to include reference to State Planning Policy 1 State Planning Framework.
22. Updating Part 2 (Residential Zone) list of criteria where Council may choose to use its discretion to punctuate this list and include the term ‘and’ so it is clear all of these items are expected to be met, not one or the other.
23. Updating Part 2 (Residential – Densities) to remove the unnecessary replication of the Town Planning Scheme No. 3 provision relating to calculation of density targets.
24. Updating Part 2 (Movement Networks) to ensure correlation between cross-sections and network plans.
25. Corrections to Table 05 of the Local Structure Plan report to include all landholdings within the local structure plan area.
26. Deletion of any references to ‘Cockburn Coast Redevelopment Area’ within the Local Structure Plan report.
27. Corrections to Figure 1 within the Local Structure Plan report to include a scale and to relabel ‘low density’ to

'medium density'.

28. Inclusion of a scale and cadastre on Figure 27 (Existing industrial buffers) to make the extent of the buffers clear.
  29. Reviewing the entire document to identify and correct basic grammar and typographical errors, including section numbering.
  30. Include an annotation (in bold text) on the local structure plan to highlight the need to refer to the Part One statutory provision 'Existing Industrial Buffer Zones' where contemplating residential or other sensitive land uses as well as the Part One statutory provision 'Use Class Permissibility'.
  31. An additional statutory provision be added to Part One of the local structure plan (under 'Subdivision and Development Requirements') to require Transport Assessment to be provided with subdivision and development proposals.
- (2) in pursuance of Clause 6.2.10.1 of the Scheme, send the Structure Plan once modified to the Western Australian Planning Commission for endorsement;
  - (3) endorse the schedule of submissions prepared in respect of the Structure Plan;
  - (4) advise the proponent and those persons who made a submission of the Council's decision;
  - (5) advise the proponent that the site is subject to Development Contribution Area No. 13, as well as a future Development Contribution Area (Cockburn Coast) which is in the final stages of preparation; and
  - (6) advise Main Roads that Council is unlikely to support any change to the Primary Regional Road Reservation under the Metropolitan Region Scheme for Cockburn Coast Drive which relinquishes opportunities for future road planning in the absence of committed and secured funding for an alternative option.

**COUNCIL DECISION****Background**

For a number of years the State Government has been working toward realising the vision for the Cockburn Coast development. The project is intended to see the redevelopment of the former Robb Jetty industrial area and the South Fremantle Power Station.

A number of planning stages have been realised in recent years briefly described below.

The Cockburn Coast District Structure Plan 2009 ("CCDSP 2009") prepared to guide future land use and transport initiatives within the area stretching between South Beach and the Port Coogee marina.

In 2012, this was supplemented and in part refined by the Cockburn Coast District Structure Plan Part 2 ("CCDSP Part 2") prepared on behalf of LandCorp.

The Metropolitan Region Scheme ("MRS") Amendment No. 1180/41 was made effective on 16 August 2011 to rezone the North Coogee industrial area from 'Industry' to 'Urban' to reflect the outcomes of the CCDSP Part 2. The South Fremantle Power Station site has been predominately rezoned to 'Urban Deferred', with a portion south of the Power Station building remaining 'Parks and Recreation' reserve.

Council has undertaken several modifications to its Town Planning Scheme to reflect the change in the MRS, including replacement of previous zones with Special Use areas to reflect the desired use mix in the Newmarket area and introduction of a 'Development' zone for the area south of Rollinson Rd.

This 'Development' zone is the most appropriate zone for new urban areas, as it provides a degree of flexibility through structure planning to robustly coordinate development.

The Scheme provisions, combined with the Department of Planning's Structure Plan Guidelines, set out the requirements to be addressed in local structure plans which will apply land use zoning and permissibility and subdivision and development requirements.

A report was previously presented to the 11 April 2013 Council meeting. Council deferred their consideration pending further liaison with landowners who had expressed concerns. A meeting with these landowners was held 16 April 2013 and a range of issues were discussed including development contributions, traffic, existing businesses and the waste water pumping station. The issue of consultation was also raised as it was claimed landowners only had three days to comment. This is incorrect. As detailed in the 'Consultation' section of this report, the local structure plans were advertised for an extended period of 28 days.

All landowners received an acknowledgment letter in January thanking them for their submission and advising that due to the volume of submissions; a report to Council would not be presented till at least the April meeting. This letter noted a further letter would be sent when the meeting date and time was able to be confirmed. A confirmation letter the item was listed on the April agenda was sent Friday 5 April which is the day the agenda is published and the earliest possible date officers can be sure the report has been included. Nevertheless, in the intervening period from close of submissions till the release of the April Council agenda, City officers spoke to a number of landowners including those who expressed their concern at the April Council meeting.

As a result of these discussions there are additional officer recommendations for this local structure plan relating to these matters. These are modifications number 30 and 31 to the local structure plan and Part 6 of the officer recommendation.

### **Submission**

The Emplacement Local Structure Plan has been submitted by HASSELL on behalf of LandCorp.

### **Report**

The purpose of this report is for Council to consider submissions on the Emplacement Local Structure Plan and whether endorsement of the plan is appropriate.

### **Purpose of the Emplacement Local Structure Plan**

The Emplacement Local Structure Plan applies to the Cockburn Coast project area bounded by Cockburn Road to the west, and the Primary Regional Road Reservation to the east, as shown in the Precinct Plan (Attachment 1).

On the western side of Cockburn Road is the local structure plan area known as 'Robb Jetty'. This is also an item on this Council agenda for Council's consideration.

The Emplacement Local Structure Plan proposes a mix of zones, primarily being residential with a density ranging from R40 to R160.

A Mixed Use zone is proposed along Cockburn Road, with a residential coding of R100 applying to any residential component within this area.

The local structure plan includes a land use table that sets out the range of permissible uses, which varies slightly from that in the Scheme, and which includes a range of uses for the Mixed Use zone, because it is not a zone included in the Scheme.

The local structure plan provides for building heights generally between 6-8 storeys, with greater heights provided along the eastern boundary. These building heights are consistent with those shown in the CCDSP Part 2.

There are development incentives included to encourage the provision of Affordable Housing. This was a target of the District Structure Planning. This encouragement is suggested by way of extra floor space being granted to a proposal. The outcome of this, if developers took up the opportunity could be a potential increase in the size of a building on a site. Given the need to set back from boundaries, this increase is most likely to be realised by building form becoming higher in storeys. For example, a 3 storey building through using the Affordable Housing bonus may become a 5 storey building (provided it can still meet other development requirements such as car parking and open space).

The Emplacement Local Structure Plan includes the provision of 12% Public Open Space ("POS"), consistent with what was shown in the CCDSP Part 2. The gun emplacement is proposed to be retained in a neighbourhood park, and a number of other POS corridors are proposed to provide a variety of recreational opportunities.

It is intended Emplacement Local Structure Plan would be adopted as a structure plan pursuant to Clause 6.2.9 of the Scheme applying land use zoning and permissibility. The Local Structure Plan needs to effectively demonstrate how coordinated development of the subject land can occur.

The Emplacement Local Structure Plan is generally consistent with the underlying intent of the CCDSP 2009 and CCDSP Part 2. However, there are a number of modifications which are required to improve the clarity of its content, address issues raised during the advertising

period, and to ensure that it can provide sufficient guidance to future subdivision and development proposals. These modifications are set out in detail in the officer recommendation and discussed further below in the Report section of this agenda item under their respective headings.

There are also some important projects associated with the local structure plans which are discussed at the end of the Report section of this agenda item. These include the Design Guidelines, Public Realm and Public Art.

### Affordable Housing

The CCDSP sets a target of achieving 20% affordable housing across the Cockburn Coast project area.

Affordable housing does not simply refer to public housing, and there are many current and potential residents facing affordability problems in the Perth Metropolitan Area who would fall outside the eligibility criteria for public housing, or would be unlikely to meet criteria for priority housing allocation.

Following on from studies undertaken by the Department of Planning, LandCorp have undertaken an Affordable Housing Strategy for the Cockburn Coast area. To examine the content of this and more importantly provide input into the local structure plan provisions regarding this issue, the City coordinated a working group to meet and discuss implementation. Representation was provided by Landcorp, the City of Cockburn, Department of Planning, Department of Housing and several affordable housing providers.

It has been made clear by the Department of Planning the only provisions which it would support in the local structure plans were to be non-mandatory. This is a shift from the CCDSP 2009 which recommended mandatory provisions. Given this change and the advice of the working group, there are a number of modifications needed to the current wording in the local structure plan text.

Using an incentive driven approach, affordable housing provision will be encouraged by a range of 'bonuses' to the ordinary development standards which apply. Bonuses will be higher for those developments which provide for more than 1 bedroom in their affordable housing component.

Modification is also required to update the income and price point indicated as updated data is now available given the recent census data release.



As discussed earlier in this report, Elected Members should be comfortable with the potential built form impact by offering these incentives. If these incentives are included as proposed (and are taken up by developers) the height of the built form would increase.

### Public Open Space

Within the Emplacement Local Structure Plan report there are some discrepancies between the stated quantities of proposed POS, including the proportion of unrestricted and restricted open space as shown in the associated Local Water Management Strategy.

It is therefore recommended that corrections be made to the POS figures in Table 3, Table 9, and throughout the Local Structure Plan report to accurately reflect the quantities of proposed POS, including the proportion of unrestricted and restricted open space as shown in the associated Local Water Management Strategy.

### Annotation of local roads

Currently a number of local roads are shown on the local structure plans. These are not required by the Department of Planning's Structure Plan Guidelines and therefore it is acceptable to remove them. What would be appropriate to annotate is any areas where a link does need to be provided. This can be provided with an arrow annotating where links are desirable.

### Additional commentary on car parking

The design guidelines provide for an appropriate response to car parking, noting this continues to be an issue of interest as the City transforms towards more example of medium density development. The amount of car parking to be provided is detailed in the Scheme. The design guidelines provide for a response to car parking management through appropriate screening of car parking areas to reduce their dominance. This will assist in the delivery of an attractive environment but with a sufficient level of car parking accommodated.

Initially the local structure plans had proposed to provide for reduced car parking standards, in line with the Integrated Transport Plan ("ITP"). As recorded in the ITP, City officers expressed concern with the notion of reduced parking (i.e. less than the Scheme and Residential Design Codes would require) in the absence of the area being adequately serviced by public transport. In lieu of this public transport being provided, the Scheme requirements will apply.

### Community Consultation Outcomes

The Emplacement Local Structure Plan was advertised for a period of 28 days, commencing on 20 November 2012.

All submissions have been outlined and addressed in the Schedule of Submissions (Attachment 3). The key issues that have been raised are summarised below.

### Assessment of Fire Management

The Emplacement Local Structure Plan report does not include a bushfire hazard assessment, with the relevant section only addressing the fire hazard associated with the Foreshore Reserve located within the Rob Jetty area. Beeliar Regional Park and remnant vegetation within the Primary Regional Road reservation and the Local Structure Plan area itself pose a fire hazard which should be addressed.

Planning for Bush Fire Protection Guidelines (Edition 2) Guidance Statement A1 and stipulates that unless it is clear to the decision-making authority that the land in question is not in an area that has a moderate or extreme bush fire hazard level any new proposals to intensify development should include a bush fire hazard assessment; and should identify any bush fire hazard issues arising from that assessment and address those issues in a report which demonstrates that all fire protection requirements can be achieved.

The Emplacement Local Structure Plan proposes development within 100m of vegetation which may be considered a 'moderate to extreme' bush fire hazard, being Beeliar Regional Park, the Cockburn Coast Primary Regional Road Reservation, and vegetation within privately owned land. Therefore according to the Planning for Bushfire Protection the Local Structure Plan should be supported by a bush fire hazard assessment.

The Department of Planning and the Department of Environment and Conservation have raised this issue in their submission (see Schedule of Submissions at Attachment 3).

It is therefore recommended that a bush fire hazard assessment and fire management plan be prepared, and any requirements that result from the fire management plan be incorporated into the local structure plan.

### Assessment of Flora and Fauna

The Local Structure Plans are each supported by Ecological Assessments undertaken by GHD. The Department of Environment

and Conservation ("DEC") have noted in their submission that these field studies were not conducted in spring, which is considered the optimal time for flora surveys within the Swan Coastal Plain Bioregion. The DEC therefore do not consider that these surveys have been conducted in accordance with Environmental Protection Authority's (EPA's) Guidance Statement 51 - Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia.

The timing of the flora and vegetation survey is not an issue for the Robb Jetty Local Structure Plan, given the degraded condition of the vegetation. However, the Ecological Assessment for the Emplacement LSP outlines the identified vegetation type 1 located on the eastern side of the project site has similarities to a DEC-listed threatened ecological community. DEC have advised that to accurately determine the floristic community types present at the project site, plots need to be established and scored (typically spring and late spring), and data analysed using appropriate statistical techniques. An appropriately timed flora survey in accordance with Guidance Statement 51 is required to determine the presence of priority and/or threatened ecological communities within the Emplacement Local Structure Plan area.

The Ecological Assessment also indicates that rare flora (e.g. *Caladenia huegefit*) and priority flora (e.g. *Dodonaea hackettiana*) are likely to occur within the Emplacement Local Structure Plan area. DEC recommends that another flora and vegetation survey of all potentially affected areas of native vegetation be conducted by an environmental consultant, in accordance with Guidance Statement 51. The survey should determine the presence of priority flora, rare flora or other significant flora.

It is therefore recommended that a spring flora and vegetation survey be undertaken within the Emplacement Local Structure Plan prior to subdivision or development of the land (where development proposes works to the land). It is recommended that the Emplacement Local Structure Plan report be modified to reflect this requirement, and that Council advise landowners of this requirement to ensure they can factor it into the timing of any proposals.

The Ecological Assessment identifies that there are patches of vegetation in good condition that would provide potential foraging habitat for Carnaby Black Cockatoos. DEC have reiterated that Carnaby's Black Cockatoo are protected by the Commonwealth's Environment Protection and Biodiversity Conservation Act 1999 ("EPBC Act"). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities

(DSEWPaC) to determine what responsibilities they have under the EPBC Act.

DEC concurs that clearing of high quality foraging habitat for Carnaby's Black Cockatoo should be minimised or avoided, if possible; and recommends that it is retained and incorporated into future POS. The Emplacement Local Structure Plan identifies the proposed areas of POS, being a neighbourhood park containing the gun emplacement, and a number of other green linkages. This is consistent with the CCDSP Part 2.

Vegetation within the green POS links will be retained where possible to provide a physical and ecological link between the foreshore and Beeliar Regional Park. However, the key function of the proposed POS is to provide a variety of recreational functions for residents and visitors, cognisant of the fact that it will be a high density environment. The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense and diverse urban environment with high levels of accessibility.

It is noted that the local structure plan report does not address the recommendations of the Ecological Assessment. It is therefore also recommended that the report be modified to address the specific recommendations.

#### Interface with Beeliar Regional Reserve

The DEC have recommended in their submission that until such time that the Cockburn Coast Drive is constructed, the proponent should ensure there is adequate fencing between any development site and areas retained for conservation, and between any development site and Beeliar Regional Park.

To address this issue it is recommended that additional provisions be included in Section 8 (Part 1) requiring development proposals to ensure adequate interface, including fencing, to the Primary Regional Road Reserve in order to protect the conservation value of the Beeliar Regional Reserve.

#### Transport – Freight Corridors (Cockburn Road)

Several submissions have raised traffic concerns with access to Cockburn Road. Main Roads has noted work is being undertaken on a design concept and vehicle access strategy for Cockburn Road. They have also expressed their intent to pursue removal of the Primary

Regional Road Reservation for the proposed Cockburn Coast Drive once Cockburn Road is upgraded.

The applicant can be required to lodge the design concept and vehicle access strategy for Cockburn Road prior to the local structure plans being forwarded to the WA Planning Commission for their endorsement.

The Transport and Traffic Management Strategy does not include designs for the intersections for Emplacement Crescent and Cockburn Road, however the Local Structure Plan report states that both of these intersections will be left in left out only intersections.

Currently the southern intersection of Emplacement Crescent and Cockburn Road allows for right turns; and two objections were made to the proposed restriction. In the future this will pose significant problems for existing businesses in Emplacement Crescent. It is therefore recommended that the Local Transport and Traffic Management Strategy (Appendix E) be modified to include intersection designs for Cockburn Road and Emplacement Crescent, and to provide for one of the intersections of Emplacement Crescent and Cockburn Road to maintain a right hand turn from Emplacement Crescent.

#### Transport – internal (existing uses)

At the meeting with landowners held 16 April 2013, the concerns of traffic were discussed. In particular the concerns of existing business operators and the ability to continue to access their sites and a desire to see construction traffic managed.

It is a reality that during construction and when the ultimate development is built out there will be changes to the level (and type) of traffic seen within this area.

Recent discussion with Landcorp now indicates they are planning to manage their construction traffic in the adjacent Robb Jetty Local Structure Plan area through the 'Main Street' access which should alleviate concern for businesses in Rollinson Rd. Temporary car parking for construction workers during the civil construction works is also being considered. This could greatly assist in minimising the traffic impact during the construction phase. With the Emplacement area, a similar approach could also be applied.

It is difficult to establish an agreed approach at local structure plan stage given the number of landowners involved. However, individual subdivision and development proposals will be able to establish with certainty how integration with existing uses will occur. Therefore is it

recommended an additional statutory provision be added to Part One of the local structure plan (under 'Subdivision and Development Requirements') to require Transport Assessment to be provided with subdivision and development proposals. The scope of what these assessments require can be found in the Western Australian Planning Commission's Transport Assessment – Guidelines for Development (Volume 3: Subdivision and Volume 4: Individual Developments).

#### Existing Industrial Land Use Buffers and Transitional Arrangements

There are a number of existing businesses and land uses (most of an industrial nature) within the Cockburn Coast area that will continue to operate into the future, dependent on the aspirations of landowners.

Under the Scheme, when the zoning changes to 'Development' zone any existing lawful development within the area that would not ordinarily be permitted under the new proposed zoning would be afforded non-conforming use rights under the Scheme. Pursuant to Clause 4.8 of the Scheme, the continued use of land is allowed for the purpose for which it was being lawfully used immediately prior to the date of gazettal of the zoning change.

Several submissions raised the issue of modifying the intent of the Mixed Use zone to note their existing business operations and the contribution this makes in terms of employment. They appear to have interpreted the flexibility attributed to this zone to mean it should allow for their uses as well.

A Mixed Use zoning has been identified throughout much of the project area, and along Cockburn Road in order to allow a range of compatible uses to co-locate adjacent to one another, and vertically in individual buildings. This is consistent with the CCDSP.

The Mixed Use zone is critical in promoting sustainable living opportunities by allowing people to pursue a lifestyle that integrates living, working and leisure in one location.

Given that the Scheme does not currently include a Mixed Use zone the Local Structure Plans set out the specific permissibility of land uses. The CCDSP outlines the types of uses that are not considered suitable for the Mixed Use zone which include 'light and service industry' and 'general industrial' uses. Therefore it is not considered appropriate to state that the Mixed Use zone will allow for businesses to remain. In many circumstances existing businesses will remain in accordance with non-conforming use rights pursuant to the Scheme, rather than because the use will be permissible under the Mixed Use zoning.

The non-conforming use rights provisions are the most appropriate method to accommodate the existing businesses. It is not considered in line with the vision for the Cockburn Coast area to alter the intent of the Mixed Use zone to make these uses permissible.

A number of submissions have expressed concerns that the proposed transitional arrangements are inadequate, and do not sufficiently protect existing businesses. However existing businesses can continue to operate in accordance with their non-conforming use rights. Non-conforming use rights also allow the carrying out of development that was approved prior to the date of gazettal of the zoning change.

The City is also able to consider applications for changes to uses to bring them closer to the intended purpose of the zone and where they would be less detrimental than the current situation. An example may be considering an enclosure to reduce noise from an existing piece of equipment, or changing from a use that generated an impact (e.g. noise or odour) to another use which did not.

Importantly, these non-conforming use rights are set out in the Scheme and they are consistent with the Model Scheme Text as prescribed by the Town Planning Regulations 1967. This provides a high level of certainty for existing businesses as the City's Scheme is expected to be consistent with the Model Scheme Text (therefore unlikely to change).

The proposed Emplacement Local Structure Plan addresses potential conflict between existing industrial uses and future sensitive land uses through noise attenuation requirements in Sections 8, and requirements for sensitive land uses proposal within buffers to industrial uses to demonstrate through technical analysis how impacts from the industrial uses are to be mitigated in Section 4.7 Industrial Activities (Part 2).

Interim buffer arrangements have been identified on a plan that maps the existing uses which generate an offsite buffer impact. These have been established with regard to the generic buffers set out in the relevant State Planning Policy and Environmental Protection Authority's Guidance Statement, then further examined in light of their current approval conditions and the City's knowledge of the nature of their operation. This is why some of the identified buffers differ from the generic buffers set out in the Environmental Protection Authority's Guidance Statement No. 3.

A process has been provided for, as per the relevant State Planning Policy for developers seeking to establish a sensitive land use within those buffers. They can undertake a further technical analysis which if approved may reduce or refine a buffer. In the meantime designation of

a Mixed Use zoning in proximity to existing industrial uses that are likely to remain for the medium to long term ensures landowners have the flexibility for options other than sensitive land uses available to them.

The Local Structure Plan reports could include further information regarding each of the existing industrial buffers. It is also recommended that Figure 27 include a scale and the cadastre to make the extent of the buffers clear. It is recommended that the LSP be amended accordingly.

### Heritage Conservation

A number of submissions raised concerns regarding the impact of the proposals on the heritage values of the area. Both Local Structure Plans are supported by a Cultural Heritage Strategy which builds on the Cockburn Coast Heritage Strategy that accompanied the CCDSP (2009).

The Emplacement Local Structure Plan and associated Cockburn Coast Cultural Heritage Strategy will ensure the retention and protection of the gun emplacement. Specifically, the Emplacement Local Structure Plan includes the gun emplacement within public open space to ensure that this important feature is not subject to development pressure. It should be noted that the two other gun emplacements were dismantled circa 1970 and the area where these two emplacements were has been redeveloped. The preparation of the Heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated with the South Beach Battery site.

There were also concerns expressed regarding the impact on the use of the area as a horse exercise area. It is agreed that this is an important consideration, which is why it has been considered from the District Structure Planning stage through to the Local Structure Plans. The Local Structure Plans and associated Cockburn Coast Cultural Heritage Strategy identify and recognise the importance and heritage value of the South Beach Horse Exercise Area. The Local Structure Plan states 'the aim is for horse facilities to remain at McTaggart Cove to provide facilities for horses with a horse float car park, where the dunes are lower and there will be less disturbance to future residential uses, thus minimising potential land use impacts.' A key objective of the Heritage Strategy is that "South Beach should continue to be used for the horse training, a use with which it has had a long association".



### Minor Modifications

There are a number of other modifications that are recommended to ensure that the report accurately reflects the appendices:

- \* Figure 12 (Vegetation Type Analysis) within the Local Structure Plan report should be modified to show the full extent of the vegetation mapping included within the Ecological Assessment.
- \* The Local Structure Plan report shows the incorrect indicative location of the switchyard/power substation, and should be amended to reflect what was shown in the CCDSP Part 2, and the Infrastructure and Servicing Report.

A number of other corrections to the Emplacement Structure Plan report are also recommended as follows:

- \* The current land use section of the report should identify Alba Edible Oils as a current land use.
- \* The report makes reference to an 'activity centre', however there is no 'activity centre' zone in the Emplacement Local Structure Plan, and such references should be deleted.
- \* Table 5 of the Plan report lists current landholdings, however some are missing. It is recommended that the table be corrected to include all landholdings within the local structure plan area.
- \* The report includes references to 'Cockburn Coast Redevelopment Area' which should be deleted, as the subject area is not included within a redevelopment area.
- \* The Emplacement Local Structure Plan does not include a scale which makes it difficult to identify the boundaries of each proposed zone.

### Associated Projects

As noted earlier in this report, there are some important projects associated with the local structure plans. These include the Design Guidelines, Public Realm, Public Art and Development Contributions.

### Design Guidelines

The 'Development Area' provisions specify that Local Structure Plans must have associated Design Guidelines. These must be adopted by the Local Government prior to or as a part of the formal consideration

of the associated Local Structure Plan. Included in this Council agenda, is an item to consider adoption of the Design Guidelines as a Local Planning Policy for the Robb Jetty and Emplacement areas of Cockburn Coast. Should Council not adopt the Design Guidelines, then it is not able to endorse either of the local structure plans submitted. This is why the officer recommendation is predicated on the Design Guidelines being endorsed.

Given the density of the proposed development, and the mix of uses, comprehensive Design Guidelines are imperative to manage built form outcomes.

Detailed discussion on the Design Guidelines may be found in the related agenda item in this Council agenda. The Design Guidelines were recently advertised to affected landowners and government agencies. The Design Guidelines are recommended for adoption as a Local Planning Policy, subject to a number of modifications.

### Public Realm

Achieving a cohesive and attractive streetscape character and public realm is considered to be an important objective for the Cockburn Coast area. The need to ensure continuity between Local Structure Plan areas and different land ownership parcels is noted in the local structure plans; however, it will not be the structure plans themselves that provide this.

Guidance will need to be outlined at a detailed technical level which goes beyond the parameters which a local structure plan can achieve. This includes identifying proposed landscaping themes, verge treatments (including items such as street furniture, bollard types, lighting types, paver styles) to achieve the desired streetscape character, including cross sections showing the location and extent of verge treatments. If these issues are not clearly documented then it will be difficult to achieve a cohesive streetscape character, particularly given that much of the land is in fragmented landownership.

LandCorp has recently commenced a guide for the Public Realm which it intends to discuss with the City's technical staff who approve and ultimately need to manage public realm areas and their infrastructure. This will be an important piece of work for the City to progress before the commencement of subdivisional works (estimated to start in 2014).

### Development Contributions

Following on from the local structure plans will be the mechanism to equitably distribute some of the development's infrastructure costs.

This will require another Scheme Amendment to introduce a Development Contribution area.

There are a number of Robb Jetty and Emplacement specific infrastructure items, such as local public open space, which LandCorp will propose for Council's consideration as part of a Scheme Amendment.

The principles outlined in State Planning Policy 3.6 'Developer Contributions for Infrastructure' will need to be satisfied by any Scheme Amendment(s) which LandCorp lodge and these are subject to public consultation including the provision of a Cost Apportionment Schedule to clearly indicate to affected landowners an estimate of development contribution rates.

### Public Art

The local structure plans for Robb Jetty and Emplacement areas are accompanied by a Public Art Strategy

Public Art is not intended for inclusion in a development contribution plan. This is a matter which would need to be the subject of a Percent for Art Policy, which at this stage has not been considered by Council and is a matter considered broader than Cockburn Coast.

City officers are currently preparing a report for Council to consider whether the implementation of a Percent for Art Policy is appropriate for the City of Cockburn. Any such policy would require public consultation should it be initiated.

### Conclusion

The Emplacement Local Structure Plan is generally consistent with the underlying intent of the CCDSP 2009 and CCDSP Part 2. However, there are a number of modifications which are required to improve the clarity of its content, ensure adherence to the Department of Planning's Structure Plan Guidelines and that it can provide sufficient guidance to subdivision and development proposals.

It is therefore recommended that the Emplacement Local Structure Plan be adopted subject to modifications and forwarded to the Western Australian Planning Commission for their approval.

## **Strategic Plan/Policy Implications**

### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

### **Infrastructure**

- Community facilities that meet the diverse needs of the community now and into the future.

### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.

### **Budget/Financial Implications**

The Structure Plan assessment fee has been calculated in accordance with the *Planning and Development Regulations 2009*, and has been paid by the applicant.

### **Legal Implications**

N/A.

### **Community Consultation**

In preparing the Emplacement Local Structure Plan, the applicant (LandCorp) undertook a consultation process with relevant stakeholders. This included two landowner forums and liaison with various State agencies in the preparation of some of the draft background strategies which informed the local structure plan content.

Once the draft Emplacement Local Structure Plan was lodged with the City advertising of the proposal took place in line with the requirements of the City's scheme for local structure plan proposals. This advertising period ran for a period of 28 days (the Scheme only requires 21 days) commencing on 20 November to 2012.

Advertising included the following:

- \* Letters to all landowners with Cockburn Coast, Port Coogee, South Beach and the Newmarket precinct, and a number of landowners within nearby parts of Hamilton Hill;
- \* Notices in the Cockburn Gazette inviting comment;
- \* Displays at the City's administration building and the City's libraries;
- \* Signage at the beach car parks at Rollinson Road and McTaggart Cove Road;
- \* Dedicated webpage on the City of Cockburn's website;

**Attachment(s)**

1. Cockburn Coast Precinct Plan
2. Draft Emplacement Local Structure Plan (plan only)
3. Schedule of Submissions Emplacement Local Structure Plan

**Advice to Proponent(s)/Submissioners**

The applicant and persons/agencies who lodged a submission have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

**Implications of Section 3.18(3) Local Government Act, 1995**

NA.

**14.4 (OCM 09/05/2013) - LOCAL STRUCTURE PLAN (ROBB JETTY)  
COCKBURN COAST (CONSIDERATION OF ADOPTION FOR FINAL  
APPROVAL) (110/06) (C CATHERWOOD) (ATTACH)**

**RECOMMENDATION**

That Council

- (1) adopt the Robb Jetty Local Structure Plan, in line with the proposed rezoning of this area to 'Development' zone via Amendment No. 89 to the City's Town Planning Scheme No. 3 and subject to the following:
  1. Modification and Adoption of the Local Planning Policy Robb Jetty and Emplacement Design Guidelines.
  2. Local access road detail to be removed from the local structure plan as per the Department of Planning's Structure Plan Guidelines.
  3. Update Part 1 to include discussion on the development contribution plan as per the Department of Planning's Structure Plan Guidelines.
  4. Update Part 2 to note that the indicative cross-sections shown for Rollinson Rd/South Beach under 'Movement Network' do not supersede the development standards and requirements for the South Beach development.
  5. Update Part 1 and Part 2 to reflect a revised public open space schedule which does not include the proposed oval as local public open space.

6. Update the cover page to either remove the imagery or update to more accurately reflect the existing freight railway line.
7. Update Part 2 to attribute the statement about current operation in non-peak periods to Brookfield Rail.
8. Update figure of Existing Buffer Zones to correctly reflect current technical analysis data, including the Waste Water Pumping Station as a 25m buffer measured from the property boundary.
9. Remove all references to a community and/or commercial facility at Catherine Point and update to ensure text reflects location at 'Main Street'.
10. Remove all references to horse facilities being located at Catherine Point and update to reflect the location being McTaggart Cove Rd beach car park.
11. Remove all non-numbered full page photographs and drawings as they are not required by the Department of Planning's Structure Plan Guidelines.
12. Ensure the Height Plan correlates appropriately to the Local Structure Plan, specifically the area designated as Residential R40.
13. Remove the footnote from the bottom of the Land Use Table contained in Part 1 and replace with text within Part 1 to explain when discretion may be granted by Council for Singles Houses (in line with the explanation given in Part 2).
14. Replace the residential density assigned to the District Centre with RAC-0 and specify development standards are prescribed in the Design Guidelines Local Planning Policy.
15. Update the Affordable Housing sections to reflect the updated Australian Bureau of Statistics data.
16. Update the Affordable Housing sections to reflect an incentive based approach; all references to mandatory requirements are to be removed.
17. Update the Affordable Housing sections to also include a

further bonus for 2 bedroom dwellings (relative the bonus given for 3 bedroom dwellings).

18. Update the Affordable Housing section to revise the definition of Affordable Housing to be:

“For the purposes of this Local Structure Plan, ‘affordable housing’ refers to either of the following:

- \* Dwellings that are sold to Eligible Households at or below the benchmark price outlined in Table 4; or
- \* Dwellings that are sold or transferred to a recognised affordable housing provider, which in turn leases or sells the properties to Eligible Households (under an approved affordable housing program); or
- \* Private Provider selling to Eligible Households; or
- \* Commonwealth or State endorsed affordability program”.

and include supplementary definitions for the terms “Eligible Households” and “Recognised affordable housing provider”.

19. Update Part 1 to also require assessment as appropriate for the issue of vibration (from the freight rail).
20. Update Part 1 to delete the reference to car parking standards being a ‘maximum’ rather than a ‘minimum’ and update the reference from the benchmarks being the Residential Design Codes to being as per the City of Cockburn’s Town Planning Scheme No. 3.
21. Update the sections regarding Detailed Area Plans to provide clarity as to when they may be required and that in some instances the need may be negated due to the Design Guidelines Local Planning Policy.
22. Update the designation of ‘Mixed Business’ and ‘Mixed Use’ to reflect a thicker line marking on the local structure plan.
23. Expand the discussion in Part 2 (Regional Planning) to broaden the reference to Directions 2031 to discuss other elements of this plan.
24. Update the discussion in Part 2 (Policies) to include reference to State Planning Policy 1 State Planning Framework.

25. Review entire document to identify and correct basic grammar and typographical errors, including section numbering.
  26. Update Part 2 (Residential Zone) list of criteria where Council may choose to use its discretion to punctuate this list and include the term 'and' so it is clear all of these items are expected to be met, not one or the other.
  27. Update Part 2 (Residential – Densities) to remove the unnecessary replication of the Town Planning Scheme No. 3 provision relating to calculation of density targets.
  28. Update Part 2 (Movement Networks) to ensure correlation between cross-sections and network plans.
  29. Delete any references to the 'Cockburn Coast Redevelopment Area'.
  30. Corrections to Figure 1 within the Local Structure Plan report to include a scale.
  31. Inclusion of a scale and cadastre on Figure 25 (Existing industrial buffers) to make the extent of the buffers clear.
  32. Include an annotation (in bold text) on the local structure plan to highlight the need to refer to the Part One statutory provision 'Existing Industrial Buffer Zones' where contemplating residential or other sensitive land uses as well as the Part One statutory provision 'Use Class Permissibility'.
  33. An additional statutory provision be added to Part One of the local structure plan (under 'Subdivision and Development Requirements') to require Transport Assessment to be provided with subdivision and development proposals.
- (2) in pursuance of Clause 6.2.10.1 of the Scheme, send the Structure Plan once modified to the Western Australian Planning Commission for endorsement;
  - (3) endorse the schedule of submissions prepared in respect of the Structure Plan;
  - (4) advise the proponent and those persons who made a



submission of the Council's decision;

- (5) advise the proponent that the site is subject to Development Contribution Area No. 13, as well as a future Development Contribution Area (Cockburn Coast) which is in the final stages of preparation;
- (6) advise Main Roads that Council is unlikely to support any change to the Primary Regional Road Reservation under the Metropolitan Region Scheme for Cockburn Coast Drive which relinquishes opportunities for future road planning in the absence of committed and secured funding for an alternative option; and
- (7) advise the Water Corporation of Council's expectation that Water Corporation will manage the pump station so as to minimise the associated buffer impact where possible.

## COUNCIL DECISION

### Background

For a number of years the State Government has been working toward realising the vision for the Cockburn Coast development. The project is intended to see the redevelopment of the former Robb Jetty industrial area and the South Fremantle Power Station.

A number of planning stages have been realised in recent years briefly described below.

The Cockburn Coast District Structure Plan 2009 ("CCDSP 2009") prepared to guide future land use and transport initiatives within the area stretching between South Beach and the Port Coogee marina.

In 2012, this was supplemented and in part refined by the Cockburn Coast District Structure Plan Part 2 ("CCDSP Part 2") prepared on behalf of LandCorp.

The Metropolitan Region Scheme ("MRS") Amendment No. 1180/41 was made effective on 16 August 2011 to rezone the North Coogee industrial area from 'Industry' to 'Urban' to reflect the outcomes of the

CCDSP Part 2. The South Fremantle Power Station site has been predominately rezoned to 'Urban Deferred', with a portion south of the Power Station building remaining 'Parks and Recreation' reserve.

Council has undertaken several modifications to its Town Planning Scheme to reflect the change in the MRS, including replacement of previous zones with Special Use areas to reflect the desired use mix in the Newmarket area and introduction of a 'Development' zone for the area south of Rollinson Rd.

This 'Development' zone is the most appropriate zone for new urban areas, as it provides a degree of flexibility through structure planning to robustly coordinate development.

The Scheme provisions, combined with the Department of Planning's Structure Plan Guidelines, set out the requirements to be addressed in local structure plans which will apply land use zoning and permissibility and subdivision and development requirements.

A report was previously presented to the 11 April 2013 Council meeting. Council deferred their consideration pending further liaison with landowners who had expressed concerns. A meeting with these landowners was held 16 April 2013 and a range of issues were discussed including development contributions, traffic, existing businesses and the waste water pumping station. The issue of consultation was also raised as it was claimed landowners only had three days to comment. This is incorrect. As detailed in the 'Consultation' section of this report, the local structure plans were advertised for an extended period of 28 days.

All landowners received an acknowledgment letter in January thanking them for their submission and advising that due to the volume of submissions; a report to Council would not be presented till at least the April meeting. This letter noted a further letter would be sent when the meeting date and time was able to be confirmed. A confirmation letter the item was listed on the April agenda was sent Friday 5 April which is the day the agenda is published and the earliest possible date officers can be sure the report has been included. Nevertheless, in the intervening period from close of submissions till the release of the April Council agenda, City officers spoke to a number of landowners including those who expressed their concern at the April Council meeting.

As a result of these discussions there are several additional officer recommendations for this local structure plan relating to these matters. These are modifications number 32 and 33 to the local structure plan and Parts 6 and 7 of the officer recommendation.

## Submission

The Robb Jetty Local Structure Plan has been submitted by HASSELL on behalf of LandCorp.

## Report

The purpose of this report is for Council to consider submissions on the Robb Jetty Local Structure Plan and whether endorsement of the plan is appropriate.

### Purpose of the Robb Jetty Local Structure Plan

The Robb Jetty Local Structure Plan applies to the Cockburn Coast project area bounded by Rollinson, Cockburn and McTaggart Cove Roads and the foreshore reserve and freight railway line as shown in the Precinct Plan (Attachment 1).

The local structure plan proposes to develop this land for a mix of zones, including a dense activity centre, residential (ranging up to R160 density), public open space, mixed business, mixed use, and a primary school with a shared oval. The oval will fulfil a role in providing for junior sport for surrounding suburbs and is in addition to the local public open space a development ordinarily provides for.

On average the development provided for by this plan would be 3-5 storeys in height. There are development incentives included to encourage the provision of Affordable Housing. This was a target of the District Structure Planning. This encouragement is suggested by way of extra floor space being granted to a proposal. The outcome of this, if developers took up the opportunity could be a potential increase in the size of a building on a site. Given the need to set back from boundaries, this increase is most likely to be realised by building form becoming higher in storeys. For example, a 3 storey building through using the Affordable Housing bonus may become a 5 storey building (provided it can still meet other development requirements such as car parking and open space).

Included in the plan are proposals indicating how the foreshore area may be capable of improvements (note the foreshore is outside the development area). Ultimately the development in this area is the role of the City and the area has heritage values (both European and Indigenous) and the relevant approvals to undertake works in this area would need to be sought.

Along the western boundary of the Robb Jetty local structure area is an existing freight rail line and Cockburn Rd bounds the area to the east. On the east side of Cockburn Rd is the contiguous local structure plan

area known as 'Emplacement'. This is also an item on this Council agenda.

It is intended Robb Jetty Local Structure Plan would be adopted as a structure plan pursuant to Clause 6.2.9 of the Scheme applying land use zoning and permissibility. The Local Structure Plan needs to effectively demonstrate how coordinated development of the subject land can occur.

The Robb Jetty Local Structure Plan is generally consistent with the underlying intent of the CCDSP 2009 and CCDSP Part 2. However, there are a number of modifications which are required to improve the clarity of its content, ensure adherence to the Department of Planning's Structure Plan Guidelines and that it can provide sufficient guidance to subdivision and development proposals. These modifications are set out in detail in the officer recommendation and discussed further below in the Report section of this agenda item under their respective headings. A number of modifications also arose from the community consultation process.

There are also some important projects associated with the local structure plans which are discussed at the end of the Report section of this agenda item. These include the Design Guidelines, Public Realm and Public Art.

#### Noise and Vibration Management

The Noise and Vibration Study indicates vibration is an issue ranging from 50-80m along the railway line. While vibration is discussed in Part 2 of the structure plan, it does not contain a related statutory requirement in Part 1. This needs to be modified to also include vibration to be assessed where applicable.

There is already a suitable Part 1 provision to deal with the issue of noise. For the freight rail this is within 150m of the railway line. For Cockburn Road, it is the first row of buildings affected.

#### Waste Water Pumping Station

The applicant has taken the opportunity afforded to them via the draft State Planning Policy 4.1 State Industrial Buffer ("SPP4.1") and the Environmental Protection Authority's Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses ("GS No.3") to submit a technical analysis to further assess and refine the buffer.

GS No. 3 notates the following impacts apply: Odour, Noise, Gas and Risk and the key agency for advice or approvals is the local government.

The Technical Analysis submitted as an appendix to the Robb Jetty Local Structure Plan has been referred for government agency comment. The only related submission was from Water Corporation (owner of the infrastructure). They have Ministerial instruction a 25 metre buffer measured the property boundary may be applied.

The affected surrounding landowners have previously lodged a letter of advice from the Department of Environment and Conservation ("DEC") noting odour is not an issue currently, but this does not consider if the infrastructure was upgraded.

The Technical Analysis is predominantly concerned with the issue of odour and pays very limited attention to the issues of gas, noise and risk. This is contained in a few paragraphs which appear to be assembled by the landowner's town planner rather than a person or company who specialises in such assessments.

City officers are not comfortable endorsing the technical analysis at this stage given it has not given sufficient regard to three of the four impacts this infrastructure presents. The landowners are welcome to lodge an updated technical analysis which does consider all these issues sufficiently, should they choose to apply for subdivision or development approval. In the interim though, no sensitive land uses will be deemed acceptable in this area. The local structure plan will be modified to reflect the boundary of the buffer as 25m measured from the property boundary.

### Affordable Housing

The CCDSP sets a target of achieving 20% affordable housing across the Cockburn Coast project area.

Affordable housing does not simply refer to public housing, and there are many current and potential residents facing affordability problems in the Perth Metropolitan Area who would fall outside the eligibility criteria for public housing, or would be unlikely to meet criteria for priority housing allocation.

Following on from studies undertaken by the Department of Planning, LandCorp have undertaken an Affordable Housing Strategy for the Cockburn Coast area. To examine the content of this and more importantly provide input into the local structure plan provisions regarding this issue, the City coordinated a working group to meet and discuss implementation. Representation was provided by LandCorp,

the City of Cockburn, Department of Planning, Department of Housing and several affordable housing providers.

It has been made clear by the Department of Planning the only provisions which it would support in the local structure plans were to be non-mandatory. This is a shift from the CCDSP 2009 which recommended mandatory provisions. Given this change and the advice of the working group, there are a number of modifications needed to the current wording in the local structure plan text.

Using an incentive driven approach, affordable housing provision will be encouraged by a range of 'bonuses' to the ordinary development standards which apply. Bonuses will be higher for those developments which provide for more than 1 bedroom in their affordable housing component.

Modification is also required to update the income and price point indicated as updated data is now available given the recent census data release.

As discussed earlier in this report, Elected Members should be comfortable with the potential built form impact by offering these incentives. If these incentives are included as proposed (and are taken up by developers) the height of the built form would increase.

#### Public Open Space

The area of public open space has been discussed with the applicant (LandCorp) as the proportion proposed in the Robb Jetty area was substantially higher than that proposed for the Emplacement area.

It should be remembered that the ceding of 10% of land suitable for subdivision is only a policy of the Commission and is variable according to the assessment of the circumstances of each case. It is not a statutory requirement and the need for public open space and drainage will differ from site to site, depending on the characteristics of the land, the availability of open space already existing within the locality and a number of other considerations. In the case of each of these areas, they are quite similar and accordingly should contain a similar proportion of public open space. Allowing for drainage capacity and noting the obviously higher densities, around 12% local public open space would be quite reasonable.

A key difference between the Robb Jetty and Emplacement areas is the location of an oval (to be shared with the primary school) in Robb Jetty which also provides for junior level clubs (AFL and cricket overflow). This oval will service an area slightly larger than just the Cockburn Coast development. It will cater for the whole suburb of

North Coogee and Coogee. Therefore it is more appropriate to distribute the cost for this oval beyond this development to be a local item for Coogee and North Coogee.

A revised POS schedule has been provided and this now indicates the oval as a proposed item to include in Development Contribution Plan 13 (community infrastructure). Landcorp will need to justify this further as part of an amendment to the City's Scheme to include this item. This means the proportion of local public open space for both the Robb Jetty and Emplacement local structure plans is just below 12%.

### Community Facilities

Initially as part of the CCDSP 2009 a community/commercial facility had been proposed at Catherine Point. This has now been negotiated by City staff to more appropriately be located within the 'Main Street' area. This is reflected in the CCDSP Part 2. There are several references left within the local structure plan text which still need to be updated to reflect this.

A site has been appropriately annotated on the local structure plan which satisfies the principles listed below. The site is located directly adjacent to the railway line (east side) and south of the main street. It is noted this site also has the ability to be sleeved with retail/other uses facing the main street.

- \* Good ability to integrate with sports oval site and associated parking.
- \* Good ability to integrate with other main street uses, particularly school and also retail and cafe/food options.
- \* An adjacent open space (such as the 'V' shaped POS west of the oval) which could enable spill over from some of the ground floor activities (such children's activities, mother's group meeting areas)
- \* Not directly on the coast to avoid climatic conditions which would compromise particularly some of the ground floor activities and render the development essentially an enclosed 'function centre' rather than a proper community centre.

There are servicing difficulties with the Catherine Point site (being isolated on the west side of the railway line) and the location is at the northern most point of the land it is intended to service. It is more appropriately located in the 'Main Street'.

### Annotation of local roads

Currently a number of local roads are shown on the local structure plans. These are not required by the Department of Planning's

Structure Plan Guidelines and therefore it is acceptable to remove them. There are two lots within the Robb Jetty area on Garston Rd which are almost entirely taken up with road. One is required for the road which will also accommodate the bus route. This lot is under offer to purchase by LandCorp. As they are the applicant, this does not present a concern.

The other lot is further east and the local road shown would form an extension south of Garston of the existing Darken Ave. This landowner has noted this issue in their submission. Other local roads will eventuate through the subdivision process; this particular section of road is not an integral road (such as the 'Main St' or the bus route) and therefore does not need to be shown now.

What would be appropriate to annotate is any areas where a link does need to be provided. This can be provided with an arrow annotating where links are desirable. Designation of local roads over another landowner's property may result in the request of that owner to undertake purchase of the property. Council has other funding priorities and does not need to leave itself open for such a request.

#### Additional commentary on car parking

The design guidelines provide for an appropriate response to car parking, noting this continues to be an issue of interest as the City transforms towards more example of medium density development. The amount of car parking to be provided is detailed in the Scheme. The design guidelines provide for a response to car parking management through appropriate screening of car parking areas to reduce their dominance. This will assist in the delivery of an attractive environment but with a sufficient level of car parking accommodated.

Initially the local structure plans had proposed to provide for reduced car parking standards, in line with the Integrated Transport Plan ("ITP"). As recorded in the ITP, City officers expressed concern with the notion of reduced parking (i.e. less than the Scheme and Residential Design Codes would require) in the absence of the area being adequately serviced by public transport. In lieu of this public transport being provided, the Scheme requirements will apply.

#### Community Consultation Outcomes

The Robb Jetty Local Structure Plan was advertised for a period of 28 days, commencing on 20 November 2012.

All submissions have been outlined and addressed in the Schedule of Submissions (Attachment 3). The key issues that have been raised are summarised below.



## Coastline

Concerns were raised about sea level rise and the continuity of access to the existing sand beach, particularly for animals (dogs and horses).

The applicant has provided a Coastal Vulnerability Assessment (CVA) with their local structure plan. The document has been prepared by an appropriately qualified person and a company who specialise in these assessments.

The assumed sea level rise in the CVA is 0.9m to 2110. This is as per the current requirements of the Department of Planning. When the State Planning Policy 2.6 (SPP2.6) State Coastal Planning Policy was gazetted in 2003 a sea level rise of 0.38m needed to be included in assessments.

Based on updated data, the Department of Planning issued a new Position Statement in 2010 to increase the sea level rise to be factored into assessments to 0.9m to 2110. In February 2012, the Department advertised a new draft SPP2.6; this reiterates the requirement for 0.9m to 2110. No advice to the contrary has been provided to the City by the Department and therefore it is prudent to apply an assumed sea level rise of 0.9m to 2110.

In terms of access for animals, there was a desire for this to remain unchanged. This is not a matter which the local structure plans control, however it is worth noting this is not a realistic expectation.

The broader Perth Metropolitan Area is facing growth of half a million people over the next two decades. Within the City of Cockburn, it is expected the population will grow by approximately 30,000 people in that time. This development will be able to provide for 10,000 people. This growth will place additional pressure onto the CY O'Connor Beach.

The current extent of the Dog Exercise Area is nearly two kilometres in length. The Coastal Vulnerability Assessment indicates the area just south of the Point Catherine groyne (in line with Rollinson Rd) is likely to erode over time and is not expected to remain as a continuous sand beach in the longer term.

The beach is also important historically given the long term use of this beach to exercise horses. It should also be remembered that while some people have no issue with dogs being on the beach, there are people who do and want access to beaches where there is no dog access.

Council must be cognisant of all of these issues and the need to balance expectations. It is likely that use of the beach will be changed over time.

### Public Open Space

Several submissions indicated a concern the amount of public open space in the Robb Jetty precinct was too high. There was a belief the public open space here was making up for a perceived shortfall in the Power Station precinct and this was unfair to landowners in Robb Jetty.

The proposed public open space adjacent to the Water Corporation pumping station also garnered concern due to its shape and the perception access would be limited.

The local POS to the north provides a local POS opportunity for some of the northern lots. This is where some of the higher density residential is located and it is appropriate to ensure those lots have good amenity POS. The POS is also adjacent to the existing Water Corporation Pump Station. Water Corporation has recently advised the City they plan to reduce the area which is currently fenced and landscape the area. This will be a welcome addition to the POS and enable access through to Bennett Avenue to the west.

### Transport – Freight Corridors (Noise and Vibration)

As with previous consultations, the issue of freight corridors was raised. The issue of noise and vibration from the railway line and noise from Cockburn Rd and proposed Cockburn Coast Drive received much attention. Questions were raised as to the appropriateness of the methodology used in the noise and vibration assessments as well as the proposed mitigation measures proposed.

What has become very apparent in assessing these submissions is there are several interest groups and government agencies who do not believe the methodology has been followed properly. It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.

The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.

The Noise and Vibration Strategy forms an addendum to the local structure plan ("LSP") and shows the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The Design Guidelines will also outline the requirements for compliance with noise and vibration for land within the impact zone. Both the LSP and the draft Design Guidelines also include requirements for Notification on titles and refer back to SPP5.4 where the specifications for these more detailed assessments reside.

The Department of Transport (representing the views of the Public Transport Authority and Main Roads) has specifically requested a Noise Management Plan be done at the local structure plan stage. The applicant has indicated this plan will be done at the development approval stage (i.e. on a lot by lot basis). This appears consistent with the intent of SPP5.4 which does not specify the Noise Management Plan must be done at the local structure plan. Looking at the content of a Noise Management Plan outlined in the guidelines which accompany the SPP5.4, it seems most of this information is already captured via the Noise and Vibration Assessment Study.

Spatially, the local structure plan would not change if this Noise Management Plan were undertaken at this early stage. Opportunities for setting back of development lots further from the railway line have effectively been lost. Planning since the CCDSP 2009 has indicated urban development abutting the railway line. This situation was compounded by the rezoning to 'urban' under the MRS and there is very little scope to see a different land use response to that of a built form response on a lot by lot basis.

City officers, given there is no indication otherwise from the DEC and given the apparently reasonable approach to the methodology used in the Noise and Vibration Assessment Study do not recommend withholding endorsement of this local structure plan on this matter. The Department of Transport are welcome to raise their concerns with the Department of Planning prior to their consideration of the plan.

#### Transport – Freight Corridors (Cockburn Road)

Several submissions have raised traffic concerns with access to Cockburn Road. Main Roads has noted work is being undertaken on a design concept and vehicle access strategy for Cockburn Rd. They have also expressed their intent to pursue removal of the Primary Regional Road Reservation for the proposed Cockburn Coast Drive once Cockburn Rd is upgraded.

The applicant can be required to lodge the design concept and vehicle access strategy for Cockburn Road prior to the local structure plans being forwarded to the WA Planning Commission for their endorsement.

#### Transport – internal (existing uses)

At the meeting with landowners held 16 April 2013, the concerns of traffic were discussed. In particular the concerns of existing business operators and the ability to continue to access their sites and a desire to see construction traffic managed.

It is a reality that during construction and when the ultimate development is built out there will be changes to the level (and type) of traffic seen within this area.

Recent discussion with LandCorp now indicates they are planning to manage their construction traffic through the 'Main Street' access which should alleviate concern for businesses in Rollinson Rd. Temporary car parking for construction workers during the civil construction works is also being considered. This could greatly assist in minimising the traffic impact during the construction phase. It will also clearly establish 'Main Street' as the entry to their land development.

It is difficult to establish an agreed approach at local structure plan stage given the number of landowners involved. However, individual subdivision and development proposals will be able to establish with certainty how integration with existing uses will occur. Therefore is it recommended an additional statutory provision be added to Part One of the local structure plan (under 'Subdivision and Development Requirements') to require Transport Assessment to be provided with subdivision and development proposals? The scope of what these assessments require can be found in the Western Australian Planning Commission's Transport Assessment – Guidelines for Development (Volume 3: Subdivision and Volume 4: Individual Developments).

#### Density provision

Some submissions have suggested the densities indicated are too conservative. Others, primarily from landowners within the development area have expressed concern they should not be expected to deliver the same densities as the State Government owned land. These landowners feel they should be able to develop more traditional housing types which are easier to sell.

It is generally perceived that multiple dwelling developments are more difficult to undertake than standard green titled lots. This is for a variety of reasons including financing and building standards. However, it must be remembered the State Government has set a vision for how this area must be developed. Well located industrial zoned land has been rezoned by the State Government to provide for urban development. Both development types (industrial and urban) are important for the continuing growth of the Perth metropolitan area. The sacrifice of well-located industrial land must not be taken lightly and a substandard outcome in terms of housing density provided in Cockburn Coast must not be accepted.

To this end, the City has included specific Scheme provisions to ensure that density targets are adhered to. The allocation of residential densities on the draft local structure plans is considered appropriate and is intended to ensure the target of housing 10,000 residents overall within Cockburn Coast can be met. This may well mean that development takes a longer period to unfold than if the area was developed with single houses. It should not be disregarded that much of this land has been undeveloped for decades already. With this land now rezoned at no cost to landowners (to date all costs have been borne by the State Government) hopefully this will now incentivise development of this area.

#### Mixed Use zone and existing businesses

Several submissions raised the issue of modifying the intent of the Mixed Use zone to note their existing business operations and the contribution this makes in terms of employment. They appear to have interpreted the flexibility attributed to this zone to mean it should allow for their uses as well.

A Mixed Use zoning has been identified throughout much of the project area, and along Cockburn Road in order to allow a range of compatible uses to co-locate adjacent to one another, and vertically in individual buildings. This is consistent with the CCDSP.

The Mixed Use zone is critical in promoting sustainable living opportunities by allowing people to pursue a lifestyle that integrates living, working and leisure in one location.

The Mixed Use zoning needs to be carefully managed so that it does not detract or disperse activity from the two proposed activity centres. Given that the Scheme does not currently include a Mixed Use zone the Local Structure Plans set out the specific permissibility of land uses. Design Guidelines will also be critical in ensuring the desirable built form outcomes are achieved for the Mixed Use zone. In accordance with the CCDSP 2009 and CCDSP Part 2, the Mixed Use

zoning is not intended to be overly prescriptive, providing that the uses can demonstrate a positive contribution to promoting a vibrant mixed use urban environment and do not detract from the two primary activity centres.

The CCDSP outlines the types of uses that are not considered suitable for the Mixed Use zone which include 'light and service industry' and 'general industrial' uses. Therefore, it is not considered appropriate to state that the Mixed Use zone will allow for businesses to remain. In many circumstances existing businesses will remain in accordance with non-conforming use rights pursuant to the Scheme, rather than because the use will be permissible under the Mixed Use zoning.

In accordance with the CCDSP uses such as residential, small showrooms, shops, offices and community facilities will be generally supported within the Mixed Use zone. In the land use table these uses are either permitted or discretionary.

Interim buffer arrangements are considered as part of the Local Structure Plan. A plan is included which maps existing uses which generate an off-site buffer impact. These have been established with regard to the generic buffers set out in the relevant State Planning Policy and Environmental Protection Authority's Guidance Statement, then further examined in light of their current approval conditions and the City's knowledge of the nature of their operation. A process has been provided for, as per the relevant State Planning Policy for developers seeking to establish a sensitive land use within those buffers. They can undertake a further technical analysis which if approved may reduce or refine a buffer. In the meantime designation of a Mixed Use zoning in proximity to existing industrial uses that are likely to remain for the medium to long term ensures landowners have the flexibility for options other than sensitive land uses available to them.

It is acknowledged that there are a number of existing businesses and land uses (most of an industrial nature) within the Cockburn Coast area that will continue to operate into the future, dependent on the aspirations of landowners.

Under the Scheme, when the zoning changes to 'Development' zone any existing lawful development within the area that would not ordinarily be permitted under the new proposed zoning would be afforded non-conforming use rights under the Scheme. Pursuant to Clause 4.8 of the Scheme, the continued use of land is allowed for the purpose for which it was being lawfully used immediately prior to the date of gazettal of the zoning change. Non-conforming use rights also allow the carrying out of development that was approved prior to the date of gazettal of the zoning change. The City is also able to consider

applications for changes to uses to bring them closer to the intended purpose of the zone and where they would be less detrimental than the current situation. An example may be considering an enclosure to reduce noise from an existing piece of equipment, or changing from a use that generated an impact (e.g. noise or odour) to another use which did not.

Importantly, these non-conforming use rights are set out in the Scheme and they are consistent with the Model Scheme Text as prescribed by the Town Planning Regulations 1967. This provides a high level of certainty for existing businesses as the City's Scheme is expected to be consistent with the Model Scheme Text (therefore unlikely to change).

Pursuant to clause 4.9 of the Scheme a person cannot alter or extend a non-conforming use without planning approval. If a non-conforming use is discontinued for a period of six months the use of the land and buildings thereafter must be consistent with the provisions of the Scheme relating to the new zoning.

The non-conforming use rights provisions are the most appropriate method to accommodate the existing businesses. It is not considered in line with the vision for the Cockburn Coast area to alter the intent of the Mixed Use zone to make these uses permissible.

### Associated Projects

As noted earlier in this report, there are some important projects associated with the local structure plans. These include the Design Guidelines, Public Realm, Public Art and Development Contributions.

### Design Guidelines

The 'Development Area' provisions specify that Local Structure Plans must have associated Design Guidelines. These must be adopted by the Local Government prior to or as a part of the formal consideration of the associated Local Structure Plan. Included in this Council agenda, is an item to consider adoption of the Design Guidelines as a Local Planning Policy for the Robb Jetty and Emplacement areas of Cockburn Coast. Should Council not adopt the Design Guidelines, then it is not able to endorse either of the local structure plans submitted. This is why the officer recommendation is predicated on the Design Guidelines being endorsed.

Given the density of the proposed development, and the mix of uses, comprehensive Design Guidelines are imperative to manage built form outcomes.

Detailed discussion on the Design Guidelines may be found in the related agenda item in this Council agenda. The Design Guidelines were recently advertised to affected landowners and government agencies. The Design Guidelines are recommended for adoption as a Local Planning Policy, subject to a number of modifications.

### Public Realm

Achieving a cohesive and attractive streetscape character and public realm is considered to be an important objective for the Cockburn Coast area. The need to ensure continuity between Local Structure Plan areas and different land ownership parcels is noted in the local structure plans; however, it will not be the structure plans themselves that provide this.

Guidance will need to be outlined at a detailed technical level which goes beyond the parameters which a local structure plan can achieve. This includes identifying proposed landscaping themes, verge treatments (including items such as street furniture, bollard types, lighting types, paver styles) to achieve the desired streetscape character, including cross sections showing the location and extent of verge treatments. If these issues are not clearly documented then it will be difficult to achieve a cohesive streetscape character, particularly given that much of the land is in fragmented landownership.

LandCorp has recently commenced a guide for the Public Realm which it intends to discuss with the City's technical staff who approve and ultimately need to manage public realm areas and their infrastructure. This will be an important piece of work for the City to progress before the commencement of subdivisional works (estimated to start in 2014).

### Development Contributions

Following on from the local structure plans will be the mechanism to equitably distribute some of the development's infrastructure costs.

An item mentioned earlier in this report is the oval proposed within Robb Jetty area. This is proposed for inclusion in the City's existing Development Contribution Plan 13 (DCP13) which is for community infrastructure as a 'local' catchment item for North Coogee/Coogee. The catchment of this oval will be greater than simply Robb Jetty area (and the entire Cockburn Coast development). It will be able to service the suburbs of North Coogee and Coogee. Other community infrastructure which similarly has a larger catchment will be proposed for Council's consideration as part of a Scheme Amendment.

There are also a number of Robb Jetty and Emplacement specific infrastructure, such as local public open space, which Landcorp will



also propose for Council's consideration as part of a Scheme Amendment.

The principles outlined in State Planning Policy 3.6 will need to be satisfied by any Scheme Amendment(s) which Landcorp lodge and these are subject to public consultation including the provision of a Cost Apportionment Schedule to clearly indicate to affected landowners an estimate of development contribution rates.

### Public Art

The local structure plans for Robb Jetty and Emplacement areas are accompanied by a Public Art Strategy

Public Art is not intended for inclusion in a development contribution plan. This is a matter which would need to be the subject of a Percent for Art Policy, which at this stage has not been considered by Council and is a matter considered broader than Cockburn Coast.

City officers are currently preparing a report for Council to consider whether the implementation of a Percent for Art Policy is appropriate for the City of Cockburn. Any such policy would require public consultation should it be initiated.

### Conclusion

The Robb Jetty Local Structure Plan is generally consistent with the underlying intent of the CCDSP 2009 and CCDSP Part 2. However, there are a number of modifications which are required to improve the clarity of its content, ensure adherence to the Department of Planning's Structure Plan Guidelines and that it can provide sufficient guidance to subdivision and development proposals.

Subject to the modification of the Robb Jetty Local Structure Plan in line with the officer recommendation, it is recommended the plan be endorsed and forwarded to the Western Australian Planning Commission for their approval.

## **Strategic Plan/Policy Implications**

### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

### **Infrastructure**

- Community facilities that meet the diverse needs of the community now and into the future.

### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.

### **Budget/Financial Implications**

The Structure Plan assessment fee has been calculated in accordance with the *Planning and Development Regulations 2009*, and has been paid by the applicant.

### **Legal Implications**

N/A

### **Community Consultation**

In preparing the Robb Jetty Local Structure Plan, the applicant (LandCorp) undertook a consultation process with relevant stakeholders. This included two landowner forums and liaison with various State agencies in the preparation of some of the draft background strategies which informed the local structure plan content.

Once the draft Robb Jetty Local Structure Plan was lodged with the City advertising of the proposal took place in line with the requirements of the City's scheme for local structure plan proposals. This advertising period ran for an extended period of 28 days (the Scheme only requires 21 days) from 20 November to 17 December 2012.

Advertising included the following:

- \* Letters to all landowners with Cockburn Coast, Port Coogee, South Beach and the Newmarket precinct, and a number of landowners within nearby parts of Hamilton Hill;
- \* Notices in the Cockburn Gazette inviting comment;
- \* Displays at the City's administration building and the City's libraries;
- \* Signage at the beach car parks at Rollinson Road and McTaggart Cove Road;
- \* Dedicated webpage on the City of Cockburn's website;

### **Attachment(s)**

1. Cockburn Coast Precinct Plan
2. Draft Robb Jetty Local Structure Plan (plan only)
3. Schedule of Submissions Robb Jetty Local Structure Plan

### **Advice to Proponent(s)/Submissioners**

The applicant and persons/agencies who lodged a submission have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

#### **14.5 (OCM 09/05/2013) - PROPOSED METROPOLITAN REGION SCHEME AMENDMENT - LOCATION: LOT 821 ARMADALE ROAD, BANJUP - OWNER: DEPARTMENT OF HOUSING - APPLICANT: GREG ROWE & ASSOCIATES (108/001) (C HOSSEN) (ATTACH)**

##### **RECOMMENDATION**

That Council write to the Western Australian Planning Commission indicating its support for the proposed Metropolitan Region Scheme ("MRS") amendment on Lot 821 Armadale Road Banjup, to place the land into the 'Urban' zone under the MRS.

##### **COUNCIL DECISION**

### **Background**

At the Ordinary Council held on 11 November 2010 Council considered the Draft Outer Metropolitan Perth and Peel Sub-regional Strategy ("Draft Strategy"). This Strategy was prepared by the Western Australian Planning Commission ("WAPC") in order to provide further guidance at a local level to how the new Strategic Plan for Perth and Peel (Directions 2031 and Beyond) will be implemented.

The Draft Strategy identified a major expansion area within the locality of Banjup, adjoining the Cockburn Central Activity Centre. This aspect of the Strategy has been supported by Council.

In light of Council's support for the above urban expansion, Council at the Ordinary Meeting on 9 December 2010 was asked to provide support towards the initiation of a Metropolitan Region Scheme ("MRS") Amendment 1211/41 over Lot 9002 Jandakot Road, Lot 9004 Armadale Road, Lot 132 Fraser Road and Lot 1 Armadale, totalling

152.83 hectares. This was to take the land from the 'Rural – Water Protection' to 'Urban' zone under the MRS. This proposal is commonly referred to as the Banjup Quarry Development.

Council again provided its support, as part of the formal advertising period of the abovementioned MRS Amendment 1211/41 at the 8 December 2011 Council Meeting. MRS Amendment 1211/41 was gazetted on 08 January 2013.

As both Directions 2031 and the Draft Strategy have included the Banjup expansion area proposal as a key strategic urban infill opportunity, the proponent (Greg Rowe & Associates) on behalf of the landowner (Department of Housing) has compiled a MRS Amendment proposal over Lot 821 Armadale Road, Banjup for Council's consideration. This will effectively complete the urban expansion area indicated by Directions 2031.

### **Submission**

Lodged by Greg Rowe & Associates on Behalf of the Landowner the Department of Housing.

### **Report**

#### Background to the Draft Outer Metropolitan Perth and Peel Sub-regional Strategy

In August 2010 the WAPC released the new Strategic Plan for Perth and Peel titled Directions 2031 and Beyond. This document provides the highest level of strategic metropolitan planning, guiding the development of more detailed policies, strategies and planning actions. As an important mechanism to demonstrate how Directions 2031 is implemented at a local level, sub-regional strategies have been developed.

The Draft Strategy covering the South West urban corridor provides information about the levels of expected population growth by local government area, and highlight development opportunities as well as opportunities for increased residential densities. They provide a framework for delivering the objectives of Directions 2031.

In respect of the City, it falls within the south-west subregion, together with the City of Kwinana and City of Rockingham. The Draft Strategy identifies future growth areas, both planned (already approved) and potential urban expansion opportunities. These growth areas are tied back to the future population and dwelling growth targets which each local government have been set.

The Draft Strategy also provides forecasts and targets for economic development, industrial land and major infrastructure (water, energy etc.). A critical component to the City and broader south-west subregion in respect of accommodating growth targets is the Banjup urban expansion area. This is discussed following.

#### Banjup Urban Expansion Area

The Draft Strategy identifies the Banjup urban expansion area covering the ex-sand mining land adjoining the Cockburn Activity Centre. It has been identified for urban expansion commencing between 2011–2015, and covers the following specific land parcels:

- Lot 9002 Jandakot Road – 6291 ha
- Lot 9004 Armadale Road – 36.52 ha
- Lot 132 Fraser Road – 45.32 ha
- Lot 821 Fraser Road – 20.50 ha

The land is predominately cleared and flat having been previously utilised as a sand quarry.

Council has previously resolved to support the Banjup urban expansion area in line with the following comments (refer 8 December 2011 Council Meeting):

1. Infill targeted around activity centres like the Banjup proposal creates synergies between investment, infrastructure, employment and activity which are required as a component to achieving more sustainable urban development within Perth. On this basis effective integration of the Banjup proposal with the Cockburn Activity Centre represents a critical planning objective;
2. The Banjup proposal exists above the Jandakot Groundwater Mound. This provides an important environmental context for the proposal, being that protection of the groundwater resource (both from a quality and quantity viewpoint) will be pivotal as part of urbanisation of the land. This will require urbanisation to be approached in a manner which utilises beyond best practice water sensitive urban design, so as to guarantee protection of the groundwater resource;
3. The decision to consider the Banjup proposal is not considered to be ad hoc, and will be subject to extensive planning and environmental rigor to ensure its development reflects expectations of sustainable development principles.

As the major component of the Banjup urban expansion area has been placed into the Urban Zone under the MRS, this proposal seeks to complete the strategic guidance provided by Directions 2031 by moving Lot 821 Armadale Road into the Urban zone also. This is discussed following.

### The Metropolitan Region Scheme Proposal

The MRS amendment proposal is considered to demonstrate compliance with the key comments made by Council, as well as the broader strategic planning framework provided by the Draft Strategy and Directions 2031.

The actual proposal constitutes rezoning of Lot 821 Fraser Road, Banjup from 'Rural – Water protection' to 'Urban'. See Attachment 1.

The total land area represents 20.50 ha, resulting in a potential yield of 340 dwellings.

In addressing the strategic framework (and the ultimate merit of the MRS amendment proposal), it is important that consideration be given to the five key themes embodied in Directions 2031. These themes - a liveable city, a prosperous city, an accessible city, a sustainable city and responsible city - provide a key test to whether the MRS amendment proposal reflects the strategic planning context which is being used to support it.

In this respect the following comments against the five key themes are relevant of this proposal:

#### A Liveable City

1. The proposal represents an example for urban expansion in an appropriate location which is considered rational on a regional scale;
2. The proposal will enhance the activity and diversity of the Cockburn Central Activity Centre;
3. The urban community will be an accessible, well-connected and sustainable community that supports and enables effective participation and inclusion in the community for all residents;
4. The proposal provides for urban growth and residential living opportunities in close proximity to activity and employment centres and public transport corridors; and
5. The proposal has the ability and the potential to contribute to the supply of strategically located affordable housing within the existing urban fabric.

#### A Prosperous City

1. The proposal will enhance the economic activity of the Cockburn Central Activity Centre, and other lower order activity centres in the district, by providing a greater residential catchment to support these centres;

2. The proposal will increase the catchment for the Cockburn Central Train Station and enhance the economic efficiency of the public transport system;
3. An increase in the workforce servicing nearby industrial, light industrial, service commercial, retail and other economic activities can be expected as a result of the proposal;
4. The proposal will provide an economic use for a degraded ex-sand quarry site; and
5. The proposal will create a diverse mix of housing types, services and amenities that facilitate economic development and employment.

#### An Accessible City

1. The future community will be highly connected to employment, education, recreation and community services given the immediate proximity to the Cockburn Central Activity Centre, Cockburn Central Train Station, Kwinana Freeway and other regional roads.
2. Community members will have ready access to the nearby Perth to Mandurah railway line via the Cockburn Central Train Station and other public transport networks operating on the surrounding regional road system.
3. Due to the proximity of Cockburn Central Train Station, the proposal will contribute towards increased usage of the railway as an alternative transport mode and will contribute to managing and reducing road congestion through reduced use of private cars.
4. The proposal will contribute to maximising the efficiency of road infrastructure given the immediate proximity to a number of regional roads, including the Kwinana Freeway.
5. Based on proximity, the proposal will support and sustain public transport use and will achieve integration of land use and public transport infrastructure.

#### A Sustainable City

1. The proposal will protect the groundwater resource as a key consideration through adapting beyond best practice water sensitive urban design principles to the land, ensuring this resource is able to keep being drawn upon as a public drinking water supply into the future;
2. The proposal provides for the creation of a diverse range of housing types on land that has been totally degraded through previous land use activities;
3. The proposal will ensure that areas of biodiversity value are protected and managed;
4. The proximity to the railway line and a Cockburn Central Activity Centre provides significant opportunity to reduce car dependency

and therefore reduce greenhouse gas emissions and other air quality concerns; and

5. The proposal assists with the optimisation of the economic utilisation of existing and proposed urban infrastructure.

#### A Responsible City

1. The proposal is contiguous with the existing physical and social infrastructure network surrounding the site.
2. The proposal represents a true infill development opportunity through the use of degraded land and the surrounding physical and social infrastructure.
3. The proposal assists in optimising and supporting increased and effective utilisation of existing urban infrastructure.

#### Future Structure Planning

A conceptual structure plan (see Attachment 2) has been prepared as part of this proposal and is provided as Attachment 2. It is not proposed to go into specific assessment detail at this very early stage, given structure planning design will be a process by which the City will have a very close involvement in should the proposal for rezoning be supported by the WAPC.

The Concept Plan has been based upon a number of key principles in line with the Department of Housing's (the owner of the subject site) Sustainability Objectives. Of important note is:

1. Relationship to Banjup Quarry LSP.
2. Retention of existing landform and vegetation along Fraser and Armadale Road to provide amenity, environmental and noise attenuation and minimise earthworks.
3. Creation of an open space circuit link incorporating Bush Forever Site 390.
4. Establishment of a Local Centre.

The Concept Plan provides for approximately 338 dwellings with densities between R25 and R60, the movement network and POS provision has been designed in line with the guiding principles listed above.

#### **Conclusion**

The proposal is considered to fulfil the various strategic objectives embodied within Directions 2031, the Draft Outer Metropolitan Perth and Peel Sub-regional Strategy and related State Planning Policies. It represents a significant urban infill targeted around the Cockburn Central Activity Centre, creating strategic synergies between



investment, infrastructure, employment and activity which are required as a component to achieving more sustainable urban development. It is on this basis that it is recommended that Council write to the WAPC indicating its support for the proposal.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.

#### **Community & Lifestyle**

- Communities that are connected, inclusive and promote intergenerational opportunities.

#### **Moving Around**

- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

The City will need to undertake amendments to its Scheme to provide an appropriate zoning and special control area arrangement to cover both structure planning requirements and the need for developer contribution arrangements. This will be a matter for future consideration, if the proposal to initiate an amendment to the MRS receives support of the WAPC.

### **Legal Implications**

The *Planning and Development Act 2005* and related *Town Planning Regulations 1967* provide the statutory basis in which an amendment to a region scheme is to be considered. This includes the statutory referral and consent processes of the Environmental Protection Authority. If the proposal is supported, the City will also need to undertake amendments to its Scheme to provide an appropriate zoning and special control area arrangement to cover both structure plans and the need for developer contribution arrangements.

This will be a matter for future consideration if the proposal to initiate an amendment to the MRS receives support of the WAPC.

## Community Consultation

Community consultation has occurred in the form of both the Directions 2031 Strategic Plan and Draft Outer Metropolitan Perth and Peel Subregional Strategy. This however has not been specific to the Banjup proposal, and accordingly future advertising and deliberation of the proposed MRS amendment (if supported by the WAPC) will provide the opportunity for detailed community consultation.

## Attachment(s)

1. Proposed MRS Amendment Map
2. Conceptual Structure Plan
3. Regional Location Plan

## Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

## Implications of Section 3.18(3) Local Government Act, 1995

Nil.

### 14.6 (OCM 09/05/2013) - CONSIDERATION TO ADOPT SCHEME AMENDMENT NO. 92 FOR FINAL APPROVAL - BUSH FIRE PRONE AREAS - APPLICANT: CITY OF COCKBURN - OWNER: VARIOUS (109/025) (C HOSSEN) (ATTACH)

#### RECOMMENDATION

That Council

- (1) endorses the Schedule of Submissions prepared in respect of Amendment No. 92 to City of Cockburn Town Planning Scheme No.3 ("Scheme");
- (2) resolves to prepare a Local Planning Policy to help guide development within designated Bushfire Prone Areas to provide consistency to officers, landowners and developers going forward;
- (3) modify the advertised Amendment No. 92 of the Scheme in accordance with the following requirements:
  1. Addition of point (e) to Section 6.6.2 to read:
 

*(e) in accordance with the Building Code of Australia activate Australian Standard 3959 which is*

*construction of building in a bush fire prone area*

2. Modify point (a) of section 6.6.13 to read:
  - (a) *a bushfire attack level assessment carried out in accordance with the methodology contained in the Planning for Bush Fire Protection Guidelines (Latest Edition) or Australian Standard 3959;*
- (4) once modified in accordance with 3, adopt for final approval Amendment No. 92 to the Scheme in pursuance of Section 75 of the Planning and Development Act 2005 for the purposes of:
  1. Modifying Clause 8.2.1 (b) of the Scheme Text by including an additional sub-clause as follows:
    - (v) *the development is included in a Bushfire Prone Area, as defined by clause 6.6.1 of the Scheme;*
  2. Modifying Clause 6.1.1 of the Scheme Text by including an additional sub-clause as follows:
    - (c) ***Bushfire Prone Areas***, *being all land in the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone, shown on the Scheme Map as BPA.*
  3. Amending the Scheme Map and Legend to introduce the Bushfire Prone Area Special Control Area designation.
  4. Including a new Clause 6.6 in the Scheme Text as follows:

**6.6 Bushfire Prone Areas**

- 6.6.1 *For the purposes of this clause, a Bushfire Prone Area means any area located in the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone, identified by the Local Government and shown on a Bushfire Hazard Assessment Map.*
- 6.6.2 *The purpose of Bushfire Prone Areas are to:*
  - (a) *implement State Planning Policy 3.4 Natural Hazards and Disasters;*
  - (b) *identify land that is subject, or likely to be subject, to bushfire hazard;*
  - (c) *ensure a bushfire attack level assessment is carried out on land that is subject, or likely to be subject, to bushfire hazard;*

- (d) *ensure that development effectively addresses the level of bushfire hazard applying to the land;*
- (e) *in accordance with the Building Code of Australia activate Australian Standard 3959 which is construction of building in a bush fire prone area.*

6.6.3 *A Bushfire Hazard Assessment Map shall indicate Bushfire Prone Areas.*

6.6.4 *If a Local Government resolves to prepare a Bushfire Hazard Assessment Map, the Local Government:*

- (a) *is to notify in writing the owner and occupier of all the properties in the affected area;*
- (b) *is to publish a notice once a week for two consecutive weeks in a newspaper circulating in the Scheme area, giving details of:*
  - a. *where the draft Map may be inspected;*
  - b. *the subject nature of the draft Map;*
  - c. *in what form and during what period (being not less than 21 days from the day the notice is published) submissions may be made.*
- (c) *may publish a notice of the proposed Bushfire Hazard Assessment Map in such manner and carry out such other consultation as the Local Government considers appropriate.*

6.6.5 *After the expiry of the period within which a submission may be made, the Local Government is to:*

- (a) *review the proposed Bushfire Hazard Assessment Map in light of any submissions made;*
- (b) *resolve to adopt the Bushfire Hazard Assessment Map with or without modification, or not proceed with the amendment.*

6.6.6 *If the local government resolves to adopt the Bushfire Hazard Assessment Map, the local government is to publish a notice of the Bushfire*

*Hazard Assessment Map once in a newspaper circulating in the Scheme area.*

- 6.6.7 *The Bushfire Hazard Assessment Map has effect on publication of a notice under clause 6.6.6.*
- 6.6.8 *A copy of the Bushfire Hazard Assessment Map, as amended from time to time, is to be kept and made **available for public inspection** during business hours at the offices of the Local Government.*
- 6.6.9 *A land owner may dispute the classification of their land as set out on the Bushfire Hazard Assessment Map in writing to the Local Government for consideration.*
- 6.6.10 *Prior to the adoption or amendment of a structure plan resulting in the introduction or intensification of development or approval of a subdivision or development application within a Bushfire Prone Area, a bushfire attack level assessment satisfactorily addressing the level of bushfire hazard applying to the land is to be submitted.*
- 6.6.11 *In addition to development which otherwise requires approval under the Scheme, planning approval is required for any development within a Bushfire Prone Area, that does not comply with an approved bushfire hazard assessment undertaken as part of the structure planning or subdivision of an area or is inconsistent with the WAPC's and FESA's Planning for Bush Fire Protection Guidelines (Latest Edition).*
- 6.6.12 *In determining an application to carry out development in the Bushfire Prone Area, the Local Government may refuse the application, or impose conditions on any planning approval as to:*
  - (a) *the provision of a fire fighting water supply;*
  - (b) *the provision of fire services access;*
  - (c) *the preparation of a fire management plan in accordance with the Planning for Bush Fire Protection Guidelines (Latest Edition) and implementation of specific fire protection measures set out in the plan;*
  - (d) *the implementation of measures to ensure*

*that prospective purchasers are aware of the relevant Scheme provisions, fire management plan and publications addressing fire safety.*

*6.6.13 An application for development approval must be accompanied by:*

- (a) a bushfire attack level assessment carried out in accordance with the methodology contained in the Planning for Bush Fire Protection Guidelines (Latest Edition) or Australian Standard 3959;*
- (b) a statement or report that demonstrates that all relevant bushfire protection acceptable solutions, or alternatively all relevant performance criteria, contained in the Planning for Bush Fire Protection Guidelines (Latest Edition) have been considered and complied with, and effectively address the level of bush fire hazard applying to the land.*

*6.6.14 If, in the opinion of the Local Government, a development application does not fully comply with the bushfire protection acceptable solutions contained in the Planning for Bush Fire Protection Guidelines (Latest Edition), the application shall be referred to the FESA for advice prior to a decision being made.*

*6.6.15 Despite any existing assessment on record, the Local Government may require a bushfire risk assessment to be carried out prior to the approval of any development proposed within a Bushfire Prone Area as designated on the Bushfire Hazard Assessment Map.*

- (5) resolves to prepare Bushfire Hazard Assessment Map(s) for areas identified as Special Control Area – Bushfire Prone utilising the previously endorsed methodology, in anticipation of the Hon. Minister's advice that final approval will be granted for the Scheme amendment;*
- (6) following the adoption of any Bushfire Hazard Assessment Map write to those landowners who are within 100m of an identified bushfire prone area but not within the Bush Fire Prone Special Control Area to inform them of the recent change to the risk status of their immediate environs;*

- (7) in anticipation of the Hon. Minister's advice that final approval will be granted, the amendment documents be signed, sealed and forwarded to the Western Australian Planning Commission with modification; and
- (8) advise all submitters to Amendment No. 92 of Council's decision accordingly.

## COUNCIL DECISION

### Background

At its Ordinary Council Meeting held on 12 April 2012 Council resolved to initiate Amendment No. 92 to City of Cockburn Town Planning Scheme No. 3 ("Scheme"). The purpose of the amendment is to place a Special Control Area over land currently zoned Rural, Resource, Rural Living and Conservation under the Scheme, dealing with bushfire risk management through the planning process. The amendment also proposes a number of alterations and additions to the Scheme Text.

The amendment was referred to the Environmental Protection Authority who granted consent to advertise. The amendment was subsequently advertised for public comment for a period of 42 days between the 26 June 2012 and the 7 August 2012, in accordance with the requirements of the *Town Planning Regulations 1967*.

A total of 19 submissions were received. The purpose of this report is to consider the amendment for final adoption in light of the advertising process having taken place.

The Amendment was presented to Council at its Ordinary Meeting held on 11 October 2012 for final adoption. Council deferred the matter until a full briefing/workshop was provided to Councillors. This briefing was to include, but not limited to:

- (1) advice from FESA on proposed areas suggested in the report and whether FESA consider them sufficient;
- (2) advice from the Bush Fire Reference Group and Volunteer Bush Fire Brigades on the areas specified and whether they concur and whether they suggest any additional inclusions;

- (3) advice from the City's Aerial Mapping Department whether some of the Market Garden areas and cleared areas warrant inclusion as Bush Fire Prone Areas;
- (4) written advice from Local Government Insurance Services (LGIS) on potential insurance premium increased costs, and legal consequences of being declared a Bush Fire Prone Area.
- (5) advice from the City's Environmental Officers on consequences of Bush Fire prone areas around our wetlands adjacent to residential areas and whether those areas of concern should be included; and
- (6) advice from an independent Fire Assessor on whether declaration of Bush Fire Prone Areas is appropriate for those areas specified within the City of Cockburn.

Information pertaining to the Amendment and the above matter were presented to a briefing of Council on 4 April 2013. Following the completion of the tasks as outlined in the Motion of Deferral, Amendment No. 92 is being presented again to Council for Final Adoption.

### **Submission**

Nil.

### **Report**

This amendment proposes additions of a number of provisions related to Bushfire Protection and Management. The intent of the amendment is to:

1. Identify land that is subject, or likely to be subject, to bushfire hazard;
2. Ensure a bushfire attack level assessment is carried out on land that is subject, or likely to be subject, to bushfire hazard; and
3. Ensure that development effectively addresses the level of bushfire hazard applying to the land.

The amendment will include a process for the introduction of a Bushfire Hazard Assessment Map(s); these maps will sit outside the Scheme and indicate the level of fire hazard for land located within the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone. These maps will be independently approved by Council and will be subject to their own community consultation process. Where land is shown to be subject to a potential bushfire hazard, the Scheme will trigger the process for requiring planning approval for development and as part of that planning approval requiring an appropriate bushfire attack level assessment to be undertaken by the proponent.



## Background

Australia and Western Australia specifically is a land mass prone to incidences of bushfire. In recent times a number of fire events have come under scrutiny from various State Governments to ascertain the cause, appropriateness of response and need for change. The need for the City of Cockburn to identify Bushfire Prone Areas and take reasonable and appropriate responses to this issue reflects a desire to be proactive in terms of bush fire management issues.

In Western Australia, unlike other states, the declaration of bushfire prone areas is currently at the discretion of Local Government. Local Government can indicate a Bushfire Prone Area by two main mechanisms:

1. Within a Town Planning Scheme; or
2. By powers conferred by the Local Government Act 1996;

Only two Local Governments in Western Australia have used legislative powers available to them to designate Bushfire Prone Areas. A number of Local Governments have included Bushfire Prone Areas within their Town Planning Schemes.

A Shared Responsibility: The Report of the Perth Hills Bushfire February 2011 Review ("Keelty Report") and The Major Incident Review – Lake Clifton, Red Hill and Roleystone Fires – June 2011 offered a number of reviews of the role of Local Government. In the review of the Red Hill fire, the review stated that there is no doubt that the areas burnt out, being the Darling Scarp, were bushfire prone. The review highlighted the problems in not having the area declared appropriately as bushfire prone. The review went on to state that "failing to declare these areas bushfire prone inhibits the effectiveness of FESA's operational response".

Within both the Major Incident Review and Keelty Report it is recommended that planning undertaken by Local Government seek to appropriately respond to bushfire risks. This Scheme amendment is viewed in this light, of the City of Cockburn seeking to ensure its Scheme is appropriately structured in a way to ensure bushfire issues are dealt with through the planning system. This is no different to (for example) how traffic issues need to be dealt with as part of a planning application, how environmental issues need to be dealt with etc. It is seen as a positive step for the City to be taking.

On 22 March 2012 the Delegated Authorities, Policies and Position Statements Committee ("DAPPS") recommended Council adopt a new position statement, PSPD22 Fire Management Plans. The Position Statement noted that in the absence of any identified Bushfire Prone

Areas and until the Scheme is amended, it is recommended that Council adopt a position that clearly articulates that a precautionary approach will be taken. Therefore the Position Statement calls on Council to clearly communicate to the community that approved Fire Management Plan recommendations and requirements will be implemented through the issue of a building permit whether or not the subject land is within a declared bushfire prone area.

The City is proactively seeking to deal with bushfire risks, through ensuring the risk posed by bushfire prone areas are recognised and dealt with through all relevant planning, subdivision and development considerations.

### Special Control Area

Amendment 92 proposes to create an additional Special Control Area within the Scheme, titled Bushfire Prone Areas. This area specifically applies to the entirety of the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zones of the Scheme. It is proposed that Clause 6.1.1 be modified to show the entirety of the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zones on the Scheme Map as BPA. This Special Control Area would be Clause 6.6 of the Scheme.

The purpose of a Bushfire Prone Areas is to:

- (a) implement State Planning Policy 3.4 Natural Hazards and Disasters;
- (b) identify land that is subject, or likely to be subject, to bushfire hazard;
- (c) ensure a bushfire attack level assessment is carried out on land that is subject, or likely to be subject, to bushfire hazard; and
- (d) ensure that development effectively addresses the level of bushfire hazard applying to the land.

The identification of the Resource zone, Rural Living and Rural zones reflect the risk of bushfires in those areas. In terms of residential areas, it was considered that their existence in a fully urban environment needed to be treated differently to rural zones which are a significantly heightened risk in terms of fire.

The Scheme amendment will allow for the creation of a Bushfire Hazard Assessment Map. This map and Clause 6.6.3 states that only a Bushfire Prone Assessment Map shall indicate Bushfire Prone Areas. Although Clause 6.1.1 will designate all areas on the Scheme Map via the BPA Special Control Area, it is only the identification of land as Bushfire Prone on a Bushfire Hazard Assessment Map that initiates the various requirements of proposed Clause 6.6.

The Bushfire Hazard Assessment Map will sit outside the Scheme and be separately approved by Council as required. The process for approval and modification of a Bushfire Hazard Assessment Map is set out in Clause 6.6.

As stated above, the Bushfire Hazard Assessment Map will sit outside the Scheme, though be directly referenced by the Scheme. Placing the Hazard Assessment Map outside the Scheme is in response to the need for flexibility in the process of identification and the changing nature of the urban/rural and natural environment of the City. By placing the Map outside the Scheme it allows Council to adapt the map as situations require and new information comes to hand.

Bushfire Prone Areas, as a norm will require development to be subject to the construction standards set out in *Australian Standard 3959-2009: Construction of buildings in the bushfire-prone areas* (AS3959-2009). Properties in fire prone areas will also require a Fire Management Plan, as identified in proposed Clause 6.6.12 of the Scheme, as a condition of planning approval. These conditions may be imposed as reference in the proposed clause below:

*6.6.12 In determining an application to carry out development in the Bushfire Prone Area, the Local Government may refuse the application, or impose conditions on any planning approval as to:*

- (a) the provision of a fire fighting water supply;*
- (b) the provision of fire services access;*
- (c) the preparation of a fire management plan in accordance with the Planning for Bush Fire Protection Guidelines (Latest Edition) and implementation of specific fire protection measures set out in the plan;*
- (d) the implementation of measures to ensure that prospective purchasers are aware of the relevant Scheme provisions, fire management plan and publications addressing fire safety.*

Proposed development within the new Special Control Area that has previously undergone a fire assessment, compliant with the WAPC's and FESA's *Planning for Bush Fire Protection Guidelines (Latest Edition)*, at either the Subdivision or Structure Planning stage, will not be required to undergo additional fire assessment. This is subject to such development complying with the previous assessment for that area.

#### Identification of Bushfire Prone Land

For the purpose of the Bushfire Hazard Assessment Map, the following protocol is utilised when identifying an area of bushfire prone land:

- Inclusion of identified native vegetation of 1 Ha or greater (by aerial photograph);
- Identification of native vegetation less than 1 Ha in size but within 50m of identified native vegetation (>1 Ha);
- Buffering of all the above by 100m (shown in different colour from main hazard area).

For the purpose of registering an accurate assessment of bushfire risk; the identification of bushfire prone areas includes land meeting the first two criteria but not within area covered by Bushfire Hazard Assessment Map, including areas of native vegetation within neighbouring Local Government Areas.

Council at its meeting on 12 April 2012 endorsed the above methodology for the use on any future Bushfire Hazard Assessment Map.

#### Bushfire Hazard Assessment Map

The processes and mechanism for the creation of a Bushfire Hazard Assessment Map is set out in Clause 6.6. Appropriate levels of community consultation including; advising all affected landowners directly and notice in a local publication (for two consecutive weeks) is required. Review of any map is then necessary prior to consideration for final adoption.

A landowner may at any time dispute the assessment of their land in writing to the Local Government. The onus would be on the landowner to provide evidence to support their claims.

A draft version of a Bushfire Hazard Assessment Map, covering the southern section of Banjup has been included as Attachment 2. This map was created using the methodology endorsed by Council on 12 April 2012. The map is a guide only and subject to change both from further analysis and any submissions received through future community consultation. Final approval of any Bushfire Hazard Assessment Map would be at the discretion of Council.

#### Requirements for Planning Approval

Clause 8.2 of the Scheme sets out the types of development that are exempt from planning approval, referred to as Permitted Development. The amendment proposes changes to Clause 8.2(b) that deals with the erection of a single house on a lot, including any extension, ancillary outbuilding and swimming pools. This Scheme amendment introduces an additional exception to Clause 8.2(b) where:

- (v) *the development is included in a Bushfire Prone Area, as defined by clause 6.6.1 of the Scheme.*

Historically within the areas subject to the proposed amendment planning approval has not been required for the development of a single house within a designated building envelope (should one exist). Planning approval has been required for development outside and/or relocation of building envelopes. The amendment proposes a major shift in the approval process of rural residential development in these areas.

For the purpose and intent of the amendment to be fulfilled it is deemed necessary to alter the status quo in these areas. The development approval process is the appropriate stage of the development assessment process to ensure that the requirements outlined in proposed Clause 6.6 are adhered.

It would be envisioned that the requirement for landowners to apply for development approval in these areas will place an additional financial and time constraint on those individuals. However, as both the Keelty Report and the Major Incident Review identified, these hesitations have existed and continue to exist within Local Government, and that not exercising these power for the reasons outlined earlier in this document is not appropriate.

The disincentives of imposing higher building costs thorough bush fire designation must be carefully weighed against the wider responsibility of Local Government. Local Government through building and planning controls can have an important and positive influence on the survivability of development (and thus human life) during a fire event.

#### Bushfire Building Cost Comparison

By designating an area bush fire prone it places an additional upfront financial encumbrance on the owners of that land in that they need to comply with AS3959-2009. The amendment proposes to require planning approval for the erection of a single house, including any extension, ancillary outbuildings and swimming pools where the lot is identified as Bushfire Prone on a Bushfire Hazard Assessment Map. A requirement of said approval will be for the application to be built to AS3959-2009, with the provision for additional bush fire related conditions as outlined in Clause 6.6.12 of the proposed amendment.

FESA in their submission to Keelty Report produced the following table derived from the Australian Building Codes Board publication the “*Final Regulatory Impact Statement for Decision (RIS 2009-02)*”.

*Table: Cost of Compliance with AS3959-2009*

Category of bush fire attack	Predicated bush fire attack and level of exposure.	Base house	Large two story	Elevated light weight construction
BAL – Low	Insufficient risk to warrant specific construction requirements.	\$0	\$0	\$0
BAL – 12.5	Ember attack.	\$11,535	\$14,981	\$21,428
BAL – 19	Increasing levels of ember attack and burning debris ignited by windborne embers together increasing heat flux.	\$11,535	\$14,981	\$21,428
BAL – 29	Increasing levels of ember attack and burning debris ignited by windborne embers together increasing heat flux.	\$15,471	\$17,095	\$35,024
BAL – 40	Increasing levels of ember attack and burning debris ignited by windborne embers together increasing heat flux with the increased likelihood of exposure to flames.	\$17,107	\$19,751	\$62,357
BAL – FZ	Direct exposure to flames from fire front in addition to heat flux and ember attack.	\$20,885	\$28,905	\$76,679

Alternatively to the table above, the Shire of Busselton in Council Agenda dated 11 May 2011 on a matter concerning the identification of bushfire prone areas provided the following information that estimated the costs (above standard constructing standards) of compliance with AS3959-2009:

- BAL – 12.5 – 3-4%
- BAL – 19 – 4-5%
- BAL – 29 – 6-6.5%
- BAL – 40 – 6-10%
- BAL – FZ – 8-10%

An application for Planning Approval on land subject to the proposed amendment would be required to provide a bush fire attack level assessment carried out in accordance with the methodology contained in the *Planning for Bush Fire Protection Guidelines (2010)*; and a statement or report that demonstrates that all relevant bush fire protection acceptable solutions, or alternatively all relevant performance criteria, contained in the *Planning for Bush Fire Protection Guidelines (2010)* have been considered and complied with, and effectively address the level of bush fire hazard applying to the land.

It would be anticipated that due to the prevailing geology, topography and built form of the City of Cockburn and specifically the areas subject to the proposed amendment; the majority of dwellings subject to

increased AS3959-2009 standards would fall within the base house and large two story categories.

Accordingly issues such as minor increases in development costs are noted, but do not represent a magnitude which should dissuade this being seen for the broader importance of development being undertaken in a more appropriate manner cognisant of the bushfire risk.

### Public Consultation

In accordance with the *Town Planning Regulations 1967* Amendment 92 was advertised for public comment for a period of 42 days between the 26 June 2012 and the 7 August 2012. Consultation included; letters to all affected landowners (almost 1,000), advertisement in the Cockburn Gazette and letter to relevant State Government Authorities. Additional consultation was also undertaken with the Banjup Residents' Association.

In total 19 submissions were received:

- 1 from an Elected Member;
- 4 from State Authorities;
- 1 from the Banjup Residents' Association;
- 13 from affected landowners or representative of affected landowners.

All submissions that were received are set out and addressed in the Schedule of Submissions (Attachment 3).

A number of objections were received from affected landowners and the Banjup Residents' Association. These are addressed in the Schedule of Submissions; however the main areas of concern are outlined and addressed in detail below.

- ***Exclusion of urban areas/regional and local reserves***  
***A number of submissions noted the inclusion of only rural residential land in Scheme Amendment 92.***

The decision to include on the Rural, Rural Living, and Resource and Conservation zones in the Bushfire Prone Special Control Area was on the basis that those zones reflect the clear and present risk of bushfires. While residential areas may be subject to risks, it was considered that their existence in a fully urban environment needed to be treated differently to our rural zones which are a significantly heightened risk in terms of fire.

HOWEVER it needs to be noted that this doesn't include new residential areas, as it has been common practice over the last

decade to ensure new structure plans for residential areas include an appropriate fire management plan to inform their design and assessment. Accordingly new residential areas will commonly have increased building provisions imposed via fire management plans, as a way of dealing specifically with the risks faced by individual lots. Accordingly the main urban areas excluded are older suburbs, such as the central and west ward areas.

The exclusion of regional and local reserves from the Amendment area is not a reflection of their level of fire risk but more a reflection on the type of uses found on such land (i.e. not intended to be developed for residential purposes now or into the future). Amendment 92 is primarily focused on the protection of human life through the implementation of higher building standards. Such development is not traditionally found within reserved land. Such land is reserved for recreation, preservation or conservation as such their inclusion in a Special Control Area would not lead to a lowering of bushfire risk to people or property across the locality.

As mentioned above, the risk associated with newer residential areas was also noted, particular attention was drawn to areas within Aubin Grove. New developments within 'Development Zone' and subject to the Structure Planning process are able to be adequately planned for bushfire risk. All Structure Plans where there's bush land in close proximity are required to undertake a Fire Management Plan. This document accompanies the Structure Plan and is utilised to guide the design of such plans. These Fire Management Plans must conform to the Guidelines. More detailed Fire Management Plans are also required at the subdivision stage. Therefore the risk within such areas can be appropriately managed by addressing the risk from the outset.

Responsible authorities also have other obligations under other Acts concerning the maintenance and upkeep of such land in respect of bushfire risk.

- ***Insurance Premiums***

A number of submissions noted concern that the zoning of an area bushfire prone would place upwards pressure on insurance premiums. The Insurance Council of Australia has noted that risk assessment is for the most part undertaken on a property specific basis and the declaration of an area as bushfire prone should have negligible impact on premiums as such risk is already factored into calculations. The declaration of large areas of New South Wales and Victoria, following recent fire events,



has not lead to a significant shift in premiums. The insurance industry advises that dwellings built to ASC3959-2009, could over time, experience a lowering of insurance premiums due to the lowering of risk through a designated bushfire prone area.

Advice was sought from the Local Government Insurance Service ("LGIS") as part of the Motion of Deferral. The LGIS did not comment on the individual circumstances of private landowners but provided useful information of the wider insurance ramifications of including lands in a bushfire prone area.

- ***Cost of building/upgrades***

As noted above, the disincentives of imposing higher building costs through bush fire designation must be carefully weighed against the wider responsibility of Local Government.

While Amendment 92 will place additional cost impositions on landowners and developers seeking to undertake development in bushfire prone areas, these are considered not to represent a magnitude which should dissuade this being seen for the broader importance of development being undertaken in a more appropriate manner cognisant of the bushfire risk. In general these additional costs will be incurred by the following:

1. Application for planning approval;
2. Requirement to address the bushfire risk of the land through a bushfire attack level assessment;
3. Cost of building new dwellings to AS3959-2009; and
4. Future cost of compliance with planning approval

Submissioners through the advertising process requested clarification on the need to comply with AS3959-2009 when undertaking an upgrade or extension to an existing dwelling in a bushfire prone area.

In general, such requirement will only be necessary when an extension or upgrade is deemed '**major**'. Moreover when a 'minor' extension is undertaken there is, for the most part, no requirement to build the extension to AS3959-2009 as doing so would have negligible impact on the overall safety of the dwelling during a fire event.

Following consultation with other local governments on how this definition is determined in their localities; it was deemed appropriate to address such matter in through a Local Planning Policy, the creation of which is a recommendation of this item.

The future policy will provide landowners, developers and City staff with consistent guidance going forward.

- ***Native vegetation removal***

The Banjup Residents' Association and a number of other submissioners have noted their concerns surrounding the need for clearing of vegetation, as part of a hazard separation zone, around dwelling built to ASC3959-2009. Their concerns go to the potential loss of the current amenity of the area, environmental concerns, increased temperatures and inconsistency with current clearing regimes.

As noted above all new dwellings within an identified bushfire prone area will be required to undertake a bushfire attack level assessment. This assessment will be required to be undertaken in line with the WAPC's Planning for Bushfire Protection Guideless. Where an extreme or high fire risk exists in close proximity to a proposed dwelling; low fuel areas known as Hazard Separation and Bushfire Protection Zones need to be identified on a bushfire attack level assessment. These areas are critical in ensuring that flames do not come in direct contact with buildings.

In general the following criteria apply in order to fulfil the requirements of the WAPC's Guidelines:

1. Fuel load reduced and maintained at 2 tonnes per hectare;
2. Trees are low and pruned;
3. No tall shrubs or tree is located within 2m of building; and
4. No tree crowns to be overhanging buildings.

Although it is favourable that these areas have limited vegetation, be grassed or paved; the presence of native vegetation is not restricted in totality. Moreover, such requirements will only apply where development is proposed and a bushfire attack level assessment recommends such action.

Accordingly, each lot that is created and zoned to provide for development for residential purposes, will result in the creation of the protection zone which will result in modification to native vegetation. This issue cannot be avoided, and is the result of development to create private allotments taking place.

The WAPC Guidelines note that the enforcement of such restrictions can have a negative impact on remnant vegetation through clearing. The proposed Local Planning Policy, a recommendation of this agenda item, will provide consistent

guidance to landowners and developers on this matter to ensure the balance between vegetation conservation and preservation of life can be appropriately met.

#### Additional DFES Consultation

As per the Motion of Deferral the Department of Fire and Emergency Services ("DFES") were again requested to comment on Scheme Amendment No. 92. DFES noted that they have no objection to the Amendment and the areas identified as Bush Fire Prone. A number of minor text additions and modifications were recommended to be made. These form part of the recommendation to Council.

#### Independent Review

As part of the Motion of Deferral Council requested an independent review of Amendment No. 92 be undertaken to determine whether declaration of Bush Fire Prone Areas is appropriate for those areas specified within the City of Cockburn. This review has been undertaken and the results included as an appendix to the Scheme Amendment Report. The Independent Review Summary of findings is below:

1. City of Cockburn Scheme Amendment 92 was found to be warranted, necessary and defensible.
2. Areas proposed to be classified as Bushfire Prone were found to have significant fire hazards and applicable and necessary for property and personnel protection. The methodology to determine and confirm the suitability was found to be appropriate and meeting 'industry' standards.
3. There were also other areas within the City boundaries that certainly met Bushfire Prone criteria and should be considered and determined as such, but with a different title "Bush Fire Hazard Special Control Area". These areas should also be subject to the same building and fire protection constraints as those areas determined 'Bush Fire Prone'.
4. There are significant areas of vegetated lands both within and outside of BPA's which pose a significant threat to life and property for which urgent action is required to address this issue. Some current properties pose a significant risk to property and the lives of occupants and fire fighters;
5. As part of the solution to point 4 above, Building Protection Zones to a depth of 25m should be implemented within the City Boundaries (as per Keelty Report).
6. An educational programme, in the form of letters and other means, should be circulated to all landowners in both proposed "Bush Fire Prone" and "Bush Fire Hazard Special Control" areas to alert them of the current dangers and recommended methods to address them.

7. Fire Management Plans to meet WAPC and DFES guidelines be mandatory for rural subdivisions within designated Bushfire Prone Areas and Bush Fire Hazard Special Control areas.
8. All applicable recommendations in the Keelty Report (Hills Bushfires in 2012) be considered and implemented as soon as practicable.

### **Conclusion**

It is considered that the proposed Scheme amendment will provide better bushfire safety and prevention within the City of Cockburn's more vulnerable areas. It will designate bushfire prone zones, showing where higher building standards and fire management plans are needed.

As noted above, through community consultation, it has been demonstrated that there is a need for a number of matters to be clarified through a Local Planning Policy. Therefore, such an action has been added to this recommendation.

It is therefore recommended that Council proceed to adopt the Scheme Amendment as outlined.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

#### **Budget/Financial Implications**

There are nil direct financial impacts faced by the Local Government. It is noted however throughout the report financial impacts associated with higher building costs in order to address fire prone areas.

#### **Legal Implications**

*Planning and Development Act 2005*

City of Cockburn Town Planning Scheme No. 3  
*Town Planning Regulations 1967*  
*Local Government Act 1995*  
*Bush Fires Act 1954*

### **Community Consultation**

In accordance with the *Town Planning Regulations 1967* consultation was undertaken subsequent to the local government initiating the Scheme Amendment and the Environmental Protection Authority ("EPA") advising that the proposal is environmentally acceptable. The amendment was advertised for 42 days.

### **Attachment(s)**

1. Scheme Amendment Map
2. Draft Bushfire Hazard Assessment Map
3. Schedule of Submissions

### **Advice to Proponent(s)/Submissioners**

Those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

#### **14.7 (OCM 09/05/2013) - PROPOSED MODIFICATION TO STRUCTURE PLAN - LOCATION: LOT 9000 NINGHAN LOOKOUT, LOT 9007 BEELIAR DRIVE AND LOT 9031 SPEARWOOD AVENUE, BEELIAR - OWNER: VARIOUS - APPLICANT: WHELANS TOWN PLANNING (100/080) (C HOSSEN) (ATTACH)**

#### **RECOMMENDATION**

That Council

- (1) in pursuance of Clause 6.2.9.1 of City of Cockburn Town Planning Scheme No. 3 ("Scheme") adopts the Structure Plan for Lot 9000 Ninghan Lookout, Lot 9007 Beeliar Drive and Lot 9032 Spearwood Avenue, Beeliar subject to the following modifications:
  1. depict on the Structure Plan map the area limited to a maximum height of two (2) stories as outlined in Section 6 of Part One (Statutory Section) of the Structure Plan report.

- (2) subject to compliance with (1) above, in pursuance of Clause 6.2.10.1 of the Scheme, the Structure Plan be sent to the Western Australian Planning Commission for endorsement;
- (3) endorse the Schedule of Submissions prepared in respect to the Structure Plan;
- (3) advise the landowners within the Structure Plan area and those who made a submission of Council's decision accordingly; and
- (4) advise the proponent that Development Contribution Area 13 - Community Infrastructure is now in operation under the Scheme. Landowners subdividing to create residential allotments and/or developing grouped/multiple dwellings will therefore be required to make contributions in accordance with the development contribution plan requirements.

## COUNCIL DECISION

### Background

The purpose of this report is to consider for adoption the Proposed Structure Plan modification for Cell 9, Yangebup and Cell 10, Beeliar Consolidated Structure Plan. The modification is specific for Lot 9000 Ninghan Lookout, Lot 9007 Beeliar Drive and Lot 9032 Spearwood Avenue, Beeliar ("subject land"). The Proposed Structure Plan seeks to alter the existing zoning and reservation from 'Residential R20' and 'Local Reserve – Local Road' to 'Residential R60', to facilitate a medium density urban outcome.

The Proposed Structure Plan has been advertised for public comment and also referred to authorities for comment. This report now seeks to consider the Proposed Structure Plan for adoption, subject to modification, in light of the advertising process and assessment by officers.

### Submission

Whelans Town Planning has lodged the proposal on behalf of Terranovis Pty Ltd.

## **Report**

### Planning Background

The subject land is 6,424m<sup>2</sup> in size and generally bound by the Beeliar Drive to the north, Spearwood Avenue to the east and existing residential development to the south and west. Attachment 1 contains a location plan.

The subject land is zoned 'Urban' under the Metropolitan Region Scheme ("MRS") and 'Development' under City of Cockburn Town Planning Scheme No. 3 ("Scheme"). The subject land is also located within Development Area 4 ("DA 4"), Development Contribution Area No. 5 ("DCA 5") and Development Contribution Area No. 13 ("DCA 13").

The subject land is located within Cell 10 Beeliar of the Consolidated Structure Plan which was endorsed by the Western Australian Planning Commission ("WAPC") on 30 October 2001.

Pursuant to Clause 6.2.4 and Schedule 11 of the Scheme, a structure plan is required to be prepared and adopted to guide future subdivision and development.

### Proposed Modified Structure Plan

As noted above the proposed modification to the Cell 10 Beeliar Consolidated Structure Plan proposes to alter the approved land use from low density residential development and associated road network to a medium density outcome.

The proposed modification also places additional requirements over the underlying zone imposing additional development restrictions on the subject land. These restrictions relate to building height, particularly in relation to the interface to existing residential developments to the south, and will be discussed in detail later in this report.

### Residential Density

As noted above the proposal seeks to increase the residential density of the subject site from low density Residential R20 to medium density Residential R60.

Under the existing residential zoning a dwellings yield of approximately 11 lots is possible for the subject site. Under the proposed zoning a maximum yield of 35 single houses or grouped dwellings would be possible. However, the applicant has noted that it is expected that the

site will be developed for multiple dwellings (apartments). Dwelling yield calculations for multiple dwellings are more complex and would be determined by the average size of apartments in any future proposal.

For the purpose of providing existing residents with an understanding of the size of any future proposal a concept plan was developed and included within the Structure Plan Report (see Attachment 6). This plan shows 54 dwellings on the subject land, which equates back to an average dwelling size of approximately 85m<sup>2</sup>, or large two (2) bedroom apartments.

Directions 2031 and Beyond ("Directions 2031") and Liveable Neighbourhoods ("LN") promote 15 dwellings per gross hectare as the 'standard' density for new greenfield development in urban areas and an overall target of 47% or all new dwellings as infill development. Medium and higher residential densities are generally considered to be appropriate adjacent to areas of POS as these areas offer a high level of amenity and convenience. In terms of high amenity and accessibility in support of a higher density, the subject site has the following attributes;

1. Within 400m (5min walk) of the future Beeliar Drive Local Centre;
2. Located on high frequency bus route with direct access to Cockburn Central, Fremantle, Phoenix and the Australian Marine Complex. Also located on future high frequency bus route as identified in the 2031 Public Transport Plan for Perth;
3. Within 800m (10m walk) of the Meve Neighbourhood Centre; and
4. Close proximity to local parks.

The Draft Outer Metropolitan Perth and Peel Sub-Regional Strategy ("Draft Strategy") identifies the subject land as being part of the "BEE1" area with a future dwelling target of 800+. As the Consolidated Structure Plan was developed prior to both Directions 2013 and the Draft Strategy the expected number of dwelling per gross hectare is expected to be below that required by Directions 2031. This proposal will assist in ensuring that this target is reached, while adding a much needed addition of housing diversity to the area.

### Building Height

Considering the established low density residential nature of the adjacent land to the south it was important that building height and bulk be addressed in a manner that ensures the existing residential amenity is not impacted upon.

Under Table 3 of the Residential Design Codes ("R-Codes") single or grouped dwellings under the R60 density code are allowed to be



developed to two (2) stories as of right. However, under Table 4 of the R-Codes, which regulate development of multiple dwellings, R60 developments can be built to three (3) stories.

The proposal seeks to allow for three (3) story development on the subject land where it will have negligible impact on the existing residences to the south. Both the Structure Plan map and Statutory Section of the Structure Plan Report restrict development on the land directly abutting the existing residential development to two (2) stories. (see attachment 4)

By restricting building height on the southern boundary, it limits the likelihood of issues relating to privacy, setback, overlooking and solar access from originating. Such approaches are supported by the Explanatory Guidelines of the R-Codes for dealing with such situations.

See the Concept Plan (Attachment 6) for an example of how such an outcome can be achieved. The Concept Plan utilises the rear boundary for single story store rooms to remove any solar access issues and also to create an additional buffer between existing residents and the vehicle movements and common areas of the concept development. Such design approaches are noted and recommended within the Explanatory Section of the Structure Plan Report

#### Access and Traffic

The proposal seeks to increase the density and subsequently allow for a potential increase in dwellings and therefore demand on the adjacent road network. The proponent has included a traffic impact assessment ("TIA") as part of the Structure Plan Report to provide assurance that any increase in traffic can be managed safely and efficiently by the existing road network.

The TIA notes that the proposal outlined in the concept plan would generate approximately 220 additional vehicle trips per day, with efficient equal distribution of these trips out of the immediate area along either Tindal Avenue or McLaren Avenue. Importantly when considering the final maximum vehicle trip numbers for the wider development cell, bounded by Tindal Avenue and McLaren Avenue, the maximum daily traffic volumes on roads directly affected by the proposal (Wooleen Parkway and Waterbank Avenue) will be considerable less than the designed maximum carrying capacity of those roads.

The TIA has been subject to assessment by the City's Traffic Engineer.

### Noise

A Preliminary Noise Assessment has been carried out and accompanies the Structure Plan Report. This report noted that any future development on the subject site should be able to achieve compliance with State Planning Policy 5.4 – Road and Rail Transport Noise and Freight Considerations in Land Use Planning (“SPP 5.4”). The Noise Report notes that a more detailed noise report will be required to accompany any future development application for the site.

### Community Consultation

The Proposed Modified Structure Plan was advertised for public comment from 19 March 2013 to 9 April 2013. The Proposed Structure Plan was advertised to nearby and affected landowners, published in the Cockburn Gazette for 21 days, published on the City’s website and also referred to relevant government authorities.

In total 14 submissions were received for the Proposal, including:

- 8 from adjoining landowners.
- 6 from government agencies.

The Submissions from government authorities were received, none objecting to the proposal.

The eight submissions lodged by adjoining landowners all objected, for various reasons, to the proposal or aspects of the proposal. These matters are addressed in detail in the schedule of submissions; however the pertinent and common concerns are discussed below.

The following matters were raised by multiple submissions and will be directly addressed below:

1. General opposition to higher density development in the location;
2. Loss of solar access;
3. Traffic; and
4. Noise Concerns;

A number of submissioners noted their objection to the scale and density of the proposal, particularly considering the existing low density environment that it will sit within. Noting that the subject site is not the correct location for such a development.

When considering the appropriateness and suitability of the location for medium density housing both the macro and micro context must be assessed. The following matters were considered in determining the appropriateness of the site.

1. The site fronts two major roads and is situated on a number of frequent bus routes providing ease of access by various modes to residents. Beeliar drive is also identified as a High Frequency Transit Route in the State Government's 2031 Public Transport Plan for Perth
2. The Site is well situated in terms of accessibility to locations of daily need. It is within 400m (5 min walk) of the future Beeliar Drive Local Centre and 800m (10 min walk) of the Meve Neighbourhood Centre. Both centres along with Cockburn Central, Phoenix and the future Beeliar Central Neighbourhood Centre are also accessible frequent bus services.
3. The Subject site is within walking distance of a number of local parks.

On the above measures; when considering the proposal against the relevant academic literature and relevant State Government Planning Policies and Guidelines the level of proposed development is appropriate for this site.

On the micro scale, it is important to consider the appropriateness of the density in terms of its impact on the existing residents and also how it will incorporate into the existing urban fabric. The Proposal incorporates a number of additional development controls (e.g. height restrictions) on the southern boundary to ensure that the development will be sympathetic to the existing residential dwellings. These additional restrictions when incorporated with the various controls within the R-Codes and Council Policies will provide a robust enough platform to ensure that the amenity of existing residents is not impacted.

A number of submissioners noted the likelihood of the loss of their access to sun light should such a proposal be allowed. As noted above a number of additional development controls have been placed on the proposal to ensure that any future medium density development is carried out in a sympathetic manner. The imposition of a two story height limit and the existing solar access requirements of the R-Codes will ensure that the solar access of existing dwellings on Ninghan Lookout is not adversely impacted.

A number of submissioners noted concerns regarding increased traffic volumes emanating from any medium density proposal. The proponent has prepared a TIA as part of the Structure Plan Report to provide assurance that any increase in traffic can be managed safely and efficiently by the existing road network.

The TIA notes that the proposal outlined in the concept plan would generate approximately 220 additional vehicle trips per day, with

efficient equal distribution of these trips out of the immediate area along either Tindal Avenue or McLaren Avenue. Importantly when considering the final maximum vehicle trip numbers for the wider development cell, bounded by Tindal Avenue and McLaren Avenue, the maximum daily traffic volumes on roads directly affected by the proposal (Wooleen Parkway and Waterbank Avenue) will be considerable less than the designed maximum carrying capacity of those roads.

A number of submissions noted the likelihood of additional noise issues emanating from any development that could occur on land zoned R60. Particularly the future residents and traffic noise from the additional traffic. Noise is an unavoidable consequence of development in any urban environment. That being said, various mechanisms are in place to regulate the accepted level of noise emanating from such developments at various times of the day to ensure that the enjoyment of others is not impacted. The final design of any medium density development will determine the level of noise emanating from them. The additional development controls in place as part of this proposal will assist in alleviating this likelihood. By positioning the development to the north of the site, away from existing residents the impact will be greatly lessened. Such matters would be addressed in detail in any future development application.

Therefore, although submissioners raise legitimate matters with planning merit; the proposal, its supporting documentation and additional planning controls in place, should be sufficient to ensure that such matters can be dealt with in a way that is positive for existing residents.

All of the submissions that were received are set out and addressed in the Schedule of Submissions (Attachment 5).

### Conclusion

It is recommended that Council adopt the Structure Plan for Lot 9000 Ninghan Lookout, Lot 9007 Beeliar Drive and Lot 9032 Spearwood Avenue, Beeliar, subject to modification, and pursuant to Clause 9.2.10 of the Scheme refer it to the Western Australian Planning Commission for their endorsement.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

- Diversity of housing to respond to changing needs and expectations.

### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.

### **Moving Around**

- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

The required fee was calculated on receipt of the proposed Structure Plan and has been paid by the proponent. There aren't any other direct financial implications associated with the Proposed Structure Plan.

### **Legal Implications**

Clause 6.2.9.1 of the Scheme requires Council to make a decision on the application within 60 days from the end of the advertising period of such longer period as may be agreed by the applicant. The advertising period concluded on 09 April 2013.

### **Community Consultation**

In accordance with Clause 6.2.8 of the City's Scheme, public consultation was undertaken from 19 March 2013 to 9 April 2013. This included a notice in the Cockburn Gazette, letters to landowners within the Structure Plan area, adjoining landowners and State Government agencies.

Analysis of the submissions has been undertaken within the 'Report' section above, as well as the attached Schedule of Submissions (Attachment 5).

### **Attachment(s)**

1. Location Plan
2. Local Context Plan
3. Proposed Local Structure Plan
4. Local Structure Plan Modification Plan
5. Schedule of Submissions
6. Development Concept Plan

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**14.8 (OCM 09/05/2013) - SCHEME AMENDMENT NO. 95 AND DRAFT BANJUP QUARRY LOCAL STRUCTURE PLAN - LOCATION: LOT 9004 ARMADALE ROAD, LOT 9002 JANDAKOT ROAD AND LOT 132 FRASER ROAD, BANJUP - OWNER: STOCKLAND WA DEVELOPMENT PTY LTD - APPLICANT: DEVELOPMENT PLANNING STRATEGIES (109/028 & 110/060) (R COLALILLO) (ATTACH)**

#### **RECOMMENDATION**

That Council:

- (1) endorse the Schedule of Submissions prepared in respect of Scheme Amendment No. 95 to City of Cockburn Town Planning Scheme No. 3 ("Scheme") and the Draft Banjup Quarry Local Structure Plan – Lot 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road, Banjup ("Draft Structure Plan");
- (2) adopt Scheme Amendment No. 95 for final approval as set out in Attachment 2, subject to the following modifications:
  1. Correcting the alignment of the existing 'Lakes & Drainage' Local Reserve so that it accords with the property boundaries of Reserve 47751 Dollier Road, Banjup.
  2. Updating the Scheme Amendment map to ensure the southern boundary of the proposed 'Development Area' follows the current alignment of the Armadale Road Primary Regional Road Reservation under the Metropolitan Region Scheme.
  3. Updating the report to reflect the current MRS zoning of the site.
  4. Rewording Provision 2 to more accurately reflect the proposed future developments for the subject area.
- (3) once modified in accordance with resolution (2) above, adopt for final approval Amendment No. 95 to the Scheme for purposes of:

1. Excluding Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road, Banjup from 'Resource' and including these in 'Development' zone.
2. Amending Schedule 11 of the Scheme text to add new 'Development Area 37' as follows:

"Schedule 11 – Development Areas

Ref. No.	Area	Provisions
DA37	Banjup Quarry Redevelopment	<ol style="list-style-type: none"> <li>1. An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.</li> <li>2. The Structure Plan is to provide for an appropriate mix of residential and non-residential land uses, in order to support the objective for a mixed use neighbourhood. Non-residential land uses may include compatible commercial and industrial (light and service industry) land uses, as a means to provide an appropriate interface and transition to the western adjoining Solomon Road Development Area 20.</li> <li>3. The Structure Plan is to provide for safe and efficient pedestrian connections between DA37 and the Cockburn Central Railway Station.</li> <li>4. Land uses classified on the Structure Plan apply in accordance with clause 6.2.6.3.</li> <li>5. The Local Government may adopt Detailed Area Plan(s) pursuant to Clause 6.2.6.3 for any part of the Development Area as defined on the Approved Structure Plan. All land use and development for a particular lot or lots the subject of a Detailed Area Plan shall accord with the adopted Detailed Area Plan.</li> <li>6. The standards and requirements</li> </ol>

		<p>applicable to zones and R Codings under the Scheme shall apply to the same extent to the areas having corresponding designations under the Approved Structure Plan. Notwithstanding this, an Approved Structure Plan may by a clear statement of intent to do so, make provision for any standard or requirement applicable to zones or R Codings to be varied, and the standard or requirement varied in that way shall apply within the area of the Approved Structure Plan, or any stipulated part of that area, as if it was a variation incorporated in the Scheme.</p>
	<ol style="list-style-type: none"> <li>3. Modifying the boundaries of the 'Lakes &amp; Drainage' Local Reserve to align with the correct cadastral boundaries of Reserve 47751 Dollier Road, Banjup.</li> <li>4. Amending the Scheme map accordingly.</li> </ol>	
(4)	require the amendment documentation be signed and sealed and then submitted to the Western Australian Planning Commission ("WAPC") along with the endorsed Schedule of Submissions and steps taken to advertise the amendment with a request for the endorsement of final approval by the Hon. Minister for Planning.	
(5)	<p>subject to the gazettal of Scheme Amendment No. 95, pursuant to Clause 6.2.9.1 of the Scheme, adopt the Draft Structure Plan (as shown in Attachment 3) subject to the following conditions and/or modifications:</p> <ol style="list-style-type: none"> <li>1. The Banjup Quarry Redevelopment Local Water Management Strategy being approved by the Department of Water.</li> <li>2. Finalisation of the voluntary legal agreement for hard infrastructure items pursuant to State Planning Policy 3.6 – Development Contributions for Infrastructure between Stockland WA Development Pty Ltd and the City of Cockburn.</li> <li>3. Incorporation of the comments provided by the WAPC in their assessment determination dated 22 March 2013 which deal with ensuring the Structure Plan reflects the requirements of the Structure Plan Guidelines and</li> </ol>	



- includes appropriate updated information to reflect the guidelines.
4. Correct minor grammar and typographical errors, including section numbering and the use of acronyms and abbreviations.
  5. A specific notation being added to the Structure Plan requiring the location and design of the Armadale Road and southern entry access intersection being to the satisfaction of Main Roads WA.
  6. The Structure Plan map being modified to provide an additional strip of POS along the northern boundary of the Structure Plan map in order to achieve a continuous POS link along this boundary and to achieve additional vegetation retention.
  7. A specific notation being added to the Structure Plan text under Part 1 to require all sensitive development to integrate appropriate noise amelioration standards as part of development and an appropriate notification be placed on the title of all lots advising of this requirement to build to a higher noise standard.
  8. A specific notation being added to the Structure Plan text under Part 2 indicating that prior to subdivision and/or clearing the Department of Environment and Conservation will need to be satisfied adequate studies and mitigation measures have been undertaken in relation to vegetation and flora.
  9. A specific notation being added to the Structure Plan text under Part 1 requiring the developer to undertake a thorough information program for prospective purchasers, based on the proximity of Jandakot Airport and information about needing to building to higher noise amelioration standards and that such standards must be achieved.
- (6) subject to compliance with (5) above, in pursuance of Clause 6.2.10.1 of the Scheme, the Draft Structure Plan be sent to the WAPC for endorsement;
- (7) require a Memorandum of Understanding to be established between the City and Stockland WA Development Pty Ltd committing to monthly (or any alternative period as agreed to by both parties) design review meetings. The design review

meetings shall cover all matters related to the subdivision and development of the subject site;

- (8) advise the proponent that the site is subject to Development Contribution Area No. 13, as well as the proposed separate Scheme Amendment which adds additional items to Development Contribution Area No. 13;
- (9) advise the proponent and those parties that made a submission of Council's decision accordingly;
- (10) note that the City will continue to liaise with the Department of Planning ("DoP") and WAPC with a view to pursue the timely strategic review of the Jandakot Water Mound and its related strategies and policies;
- (11) advise the proponent to liaise with the Department of Indigenous Affairs as early as possible in order to ensure their proposal complies with the requirements of the Aboriginal Heritage Act 1972.; and
- (12) advise the WAPC of the City's position in respect of State Planning Policy No. 5.3, and also the City's position taken in respect of the points raised in the late submission received from the Department of Environment.

## COUNCIL DECISION

### Background

Council at its meeting held on 11 October 2012 resolved to initiate Amendment No. 95 to Town Planning Scheme No. 3 ("Scheme") for the purpose of advertising. The amendment proposes to rezone Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road, Banjup (the subject land) from 'Resource' to 'Development' and to allow appropriate Special Control Area provisions in the Scheme text to control development which is the approach taken in respect to all development areas within the City.

The site was the subject of a Metropolitan Region Scheme ("MRS") Amendment (1221/41) to rezone the land from 'Rural Water Protection

Zone' to 'Urban Zone', 'Primary Regional Roads Reservation', 'Water Catchments Reservation' and 'Urban Deferred Zone'. The MRS Amendment process is now complete with the site's rezoning being gazetted on 8 January 2013 (refer to Attachment 2 for MRS Amendment map). This has enabled the advertising and consideration of Amendment No. 95 to progress.

Consistent with the provisions of Scheme Amendment No. 95, a Draft Structure Plan has been prepared for the subject land to guide future residential subdivision and development.

Both Amendment No. 95 and the Draft Structure Plan have been advertised for public comment in accordance with the Scheme which provides for concurrent advertising of these types of proposals. The purpose of this report is for Council to now consider Amendment No. 28 and the Draft Structure Plan for final adoption in light of submissions received on the proposals.

## **Submission**

The proposed Scheme Amendment was lodged by Development Planning Solutions on behalf of Stockland WA Development Pty Ltd, the owner of the majority of the subject site (refer to Attachment 3 for full Amendment report).

The Draft Structure Plan (as shown in Attachment 4) was also lodged by Development Planning Solutions on behalf of Stockland WA Development Pty Ltd. The Draft Structure Plan has been prepared in support of the proposed urbanisation of the subject land and provides for residential development (ranging in density from R25 to R60), (potential) retirement living, public open space, a town centre, a primary school and an area of light/service industry.

## **Report**

### Overview

By way of recap, the subject land is located adjoining the Cockburn Central Regional Centre to the east. It represents approximately 145ha of previously sand quarried land, with the potential to represent a major new urban expansion area for both the City of Cockburn and wider metropolitan region. Its strategic planning has been occurring over the last seven years, aligning with the State Government's Directions 2031 initiative which has looked to reorientate Perth's growth towards urban containment focussed on activity centres. This land precinct represents a key opportunity to demonstrate the reorientation of growth to maximise the strategic capabilities of land.

As part of this strategic planning detailed environment investigations have taken place, to ensure that the land use change occurs in a way which still protects the groundwater resource associated with the Jandakot Water Mound. This forms an important environmental context for the land, in that it is expected that excellence in environmental and water sensitive urban design takes place. The proponent has integrated this as a common theme underpinning all elements of the proposal.

In terms of overview, Scheme Amendment No. 95 comprises the following key parts:

1. Excluding Lot 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' zone and include these in 'Development' zone.
2. Introduce a new 'Special Control Area' covering the subject land, to be known as 'Development Area 37' and formulating appropriate provisions.
3. Amend the Scheme Map accordingly.

The Scheme Amendment reflects the planning objectives for the area from both the local and State planning perspectives, particularly noting that the land has transferred into the urban zone under the MRS to provide now for residential development.

In terms of the Draft Structure Plan, it covers approximately 145 hectares of land with an expected residential yield of 1801 lots and population of 4862 persons. It is proposed to provide approximately 2,800m<sup>2</sup> of commercial net lettable area and 25.8 hectares of public open space. It includes a wide range of residential densities, (potential) retirement living, public open space, a town centre, a primary school and an area of light/service industry.

The following parts of the report detail both the Scheme amendment and Structure Plan assessment.

#### Scheme Amendment No. 95

The purpose of the Scheme Amendment is to assist in the proper and orderly planning of the site through the implementation of an appropriate 'Development' zone across the entire site to be known as 'Development Area – DA37'. The new 'Development' zone will replace the existing 'Resource' zone and establish the need for a structure plan that identifies residential development, community and educational facilities, pedestrian connections to Cockburn Central Railway Station and overall land uses consistent with the Scheme. The proposed Development Area provisions will also prescribe the requirement for detailed area plans and outlines how the R-Codes will be applied in the

Development Area allowing for variations in lot sizes to facilitate innovative residential development where appropriate.

The overall intent and purpose of Scheme Amendment No. 95 is consistent with the requirements of the City and will provide conformity with the MRS. The Scheme amendment also seeks to create an appropriate zoning mechanism such that the objectives set for the land precinct can be achieved through having a performance based planning approach underpinned via a structure planning process. These objectives specifically include:

6. Ensuring the proposal will enhance the activity and diversity of the Cockburn Central Activity Centre.
7. Ensuring the urban community will be an accessible and well-connected community with a focus upon public transport integration.
8. Ensuring the proposal provides for a range of urban growth and residential living opportunities in close proximity to activity and employment centres and public transport corridors.
9. Ensuring the proposal will enhance the economic activity of the Cockburn Central Activity Centre, and other lower order activity centres in the district, by providing a greater residential catchment to support these centres.
10. Ensuring the future community will be highly connected to employment, education, recreation and community services, and to ensure the provision of these services early on as part of development.
11. As a must, ensuring that the proposal will protect the groundwater resource through adapting beyond best practice water sensitive urban design principles to the land, ensuring this resource is able to keep being drawn upon as a public drinking water supply into the future.
12. Ensuring the proposal protects and enhances areas of biodiversity value.

In terms of initiating the Scheme amendment for advertising it has been assessed that the proposal reflects these set objectives for the land. However following advertising, a minor modification is proposed to the Scheme amendment in order to ensure consistency with zoning and cadastral boundaries. This concerns the existing 'Lakes & Drainage' Local Reserve which applies to the City's drainage site at Reserve 47751 Dollier Road, Banjup. It is recommended that the Scheme Map be modified so that the amendment area to create the Development zone follows the up-to-date cadastral boundaries of Reserve 47751.

Another minor modification is required to the Scheme Amendment map to ensure the southern boundary of the proposed 'Development Area' follows the current alignment of the Armadale Road Primary Regional

Road Reservation which was modified as part of the MRS rezoning of the site. This again ensures that the amendment area to create the Development zone follows the correct boundaries.

#### Lot 1 Armadale Road

As shown within Attachment 2, Lot 1 Armadale Road, Banjup was rezoned to 'Urban Deferred' rather than 'Urban' under the MRS. The decision to rezone to 'Urban Deferred' is based on outstanding access issues which relate to the site. Main Roads WA requested the exclusion of the site from the MRS Amendment as it may be impacted by the potential realignment of the North Lake Road extension. Rather than full exclusion from the Amendment, the WAPC recommended the site be rezoned to 'Urban Deferred' so that the land can be transferred to the urban zone once Main Roads WA has completed its planning study.

The rezoning of the site under the City's TPS No.3 from 'Resource' to 'Development' is considered to be consistent with the 'Urban Deferred' zoning under the MRS. This is on the basis that no development can occur on the site until the site is rezoned to 'Urban' under the MRS and issues relating to access will need to be dealt with and inform any lifting of Urban deferment.

#### Draft Banjup Quarry Structure Plan

The Draft Structure Plan has been prepared generally in accordance with the WAPC's "Structure Plan Preparation Guidelines". The guidelines have scaled back the level of detail required to be shown on structure plan maps to neighbourhood level with local streets and blocks no longer required to be outlined. As such the Draft Structure Plan only includes the key transport linkages, overall areas of residential development, and general areas of public open space and siting of land uses (i.e. light & service industry, local centre, civic and primary school).

Given the conceptual nature of the structure plan, the City has ensured the written component of the structure plan includes sufficient objectives, provisions and requirements for future subdivision development. This will seek to ensure the applicant undertakes a close liaison role with the City, to ensuring that the actual subdivision and development applications which generate following the structure plan's adoption are reflective of the objectives contained within the structure plan itself.

The general indicative arrangement of street blocks, roads, landscaping and land uses are shown within Attachment 4. This conceptual arrangement will be refined at the future subdivision stage.

Given the significance of this development precinct, a new aspect of the City's consideration is to formulate a memorandum of understanding whereby the City undertakes monthly design meetings with the proponent so that the City has an active role in translating the structure plan objectives to the realities of detailed design. While a new approach, this is something that the City believes to be necessary especially as the new State Government structure plan guidelines have seen fit to reduce the detailed design levels upfront. Accordingly the City (lead by strategic and statutory planning teams) seeks to ensure that there is an ongoing dialogue through the life of the project to ensure that the design objectives eventuate through all aspects of the proposal.

From a detailed assessment viewpoint, the following information is provided.

### Design and Density

The Draft Structure Plan identifies that the subject area will meet an urban density target of 13.5 dwelling units per gross urban hectare. This is slightly below the 15 dwelling target prescribed by the WAPC's 'Directions 2031 and Beyond', for the reason of the site being dissected by the large Western Power power-line easement. Further compounding this is the road widening required for Jandakot Road, the need to demonstrate protection of large areas of remnant vegetation and to ensure non-residential land uses buffer those Industry land uses that exist nearby. These are considered physical barriers which reduce the effective developable land area available for residential purposes.

Although the Draft Structure Plan does not meet the gross residential target, the proposed net density is in the range of 22-25 dwellings per net site hectare. This is particularly important, and lifts the Structure Plan above the target set via Directions 2031 and is considered to reflect the strategic capabilities of the land mentioned in terms of the regional centre location and nearby availability of transport, employment and other service based infrastructure.

A range of residential densities from R25 to R60 have been proposed as part of the Draft Structure Plan. The siting of the residential density cells will be guided by the associated locational criteria specified within the Draft Structure Plan. Higher densities will be required to be situated close to all areas of higher amenity such as public open space, commercial sites, civic facilities etc. Future subdivision applications will need to demonstrate compliance with the density locational criteria to the satisfaction of the City and WAPC. This will form one of the criteria closely assessed in terms of the City's involvement in the design review process of subdivision applications.

### Public Open Space

The Draft Structure Plan proposes 12 areas of POS with half of these areas also fulfilling a drainage function in accordance with water sensitive urban design principles. A notional total of 16.8% POS is provided as part of the Draft Structure Plan and exceeding the minimum 10% POS provision prescribed by Liveable Neighbourhoods. This is considered to be appropriate, given that higher residential densities are being proposed via the structure plan such that these densities will mean on average smaller lots and smaller areas of private open space (backyards etc.) To counter this it is considered appropriate to provide additional POS such that future residents still enjoy access to open space for the full range of recreational and health pursuits.

However it should be noted that the areas of POS shown in the Draft Structure Plan are subject to more detailed design at the subdivision and Urban Water Management Plan ("UWMP") stage. This may require additional POS, especially to account for drainage functionality which underpins the need for water sensitive urban design principles taking place.

### Local Centre Precinct

A Local Centre Precinct has been proposed as part of the Draft Structure Plan. The location of the precinct is close to central to the development, but accounts for the Western Power easement which dissects the area in an east west direction. As such, the centre's catchment has been maximised as far as practical by its location within the northern portion of the subject area which is less constrained and able to accommodate a greater residential yield.

The mix of proposed uses within the precinct of commercial, residential, civic, aged persons and education is supported by the City. In order to ensure the future design and functionality of the Local Centre Precinct and surrounding mixed use areas, Detailed Area Plans will be required at the subdivision stage. These will need to evolve out of a design review process with the City's officers, such as to ensure that the requirements of a mixed use and diverse centre precinct take place.

### Access

The Draft Structure Plan proposes five vehicular access/egress points into the subject site. The three key entries are from Armadale Road (via Fraser Road) to the south, midway from Solomon Road to the west and midway from Jandakot Road to the north. Two minor entry points are from Dollier Road and the northern portion of the unconstructed



Fraser Road reserve. All intersections providing access/egress to the site will be required to be appropriately managed via the use of roundabouts and other suitable treatments to the satisfaction of the City and Main Roads WA.

Entry from Armadale Road is of particular importance given its status as a Primary Regional Road under the MRS and long term projections of carrying approximately 57,000 vehicles per day. Main Roads WA are currently exploring the prospect of upgrading Armadale Road to a triple lane divided carriageway to accommodate increased future movements.

Based on existing and projected traffic volumes, the Armadale Road and southern access entry intersection is proposed to be managed via a signalised T-junction. Main Roads WA has provided preliminary support to this intersection treatment given that this is the only intersection onto Armadale Road proposed. The existing Fraser Road/Armadale Road 'T-intersection' on the eastern perimeter of the subject site will be closed as part of the future subdivision process. All costs associated with the development of the intersection will be the responsibility of the developer.

#### Local Water Management Strategy

In accordance with the requirements of the Department of Water ("DoW") and WAPC, a draft Local Water Management Strategy ("LWMS") has been prepared by PDC Engineering on behalf of the landowner. The LWMS has been assessed by the DoW and the City and modifications have been requested to the document prior to final endorsement being granted.

As the changes are considered minor and not likely to warrant spatial changes to the Draft Structure Plan, it is recommended that approval of the Draft Structure Plan proceed subject to the submission and final endorsement of the revised LWMS by DoW and the City.

#### Hard Infrastructure Upgrading Requirements

Initial discussions between the City and the developer proposed hard infrastructure upgrades and contributions relevant to the Draft Structure Plan and surrounds to be facilitated by a new 'Developer Contribution Area 14' (DCA) and an associated Development Contribution Plan (DCP). This would have been facilitated through an amendment to the Scheme. This initial approach however was modified to better take account of the provisions of the WAPC's Statement of Planning Policy 3.6 – Development Contributions for Infrastructure' (SPP3.6). Particularly, Section 5.3 of SPP3.6 provides flexibility by enabling

required up its emphasis on the need and nexus argument to underpin approaches where DCP's would be appropriate.

In liaison with the applicant, it was determined that a more appropriate approach would be through a voluntary legal agreement which set out the required infrastructure upgrades for the proposal. In this case the voluntary legal agreement will prescribe that the developer contribute to the full (100%) provision of the following hard infrastructure items, pursuant to SPP 3.6 provisions, as summarised (but not limited to) below:

- Full road widening of Jandakot Road between Solomon and Fraser Roads.
- Full contribution towards upgrading of Jandakot Road between Solomon and Fraser Roads.
- Full contribution towards upgrading of Solomon Road between Dollier and Jandakot Roads.
- Construction of a 2.5m wide shared path (LSP side) on both Jandakot Road and Solomon Road.
- Construction of three roundabouts at Solomon Road/Jandakot Road, new internal subdivision road/Jandakot Road and Fraser Road/Jandakot Road intersections.
- Construction of 2.5m wide dual use path along one side of Armadale Road (LSP side) between Fraser Road and the junction of Solomon Road and Knock Place.

Upgrading provisions of the various roads noted above will relate (but are not limited) to the following:

- All preliminaries and detailed design;
- Earthworks and service relocations where required;
- Kerbing, lighting, full traffic controls and on-street cycle lanes (where nominated);
- Stormwater management;
- Landscaping; and
- Modifications to intersection approaches (where nominated).

The above infrastructure items are substantial, but at the same time are considered to reflect the need and nexus for upgrading as a result of the development taking place. Generating this agreement via a voluntary legal agreement is considered to reflect the level of agreement that exists between the applicant and the City in terms of the applicant's share of infrastructure upgrades needed. An imperative part of the agreement will be the specification of timeframes to ensure that the required infrastructure upgrades are undertaken in a timely manner. That is, the legal agreement will need to specify an appropriate timeframe (represented as a percentage of land developed) in which to target the required infrastructure upgrades.

It is recommended that any approval of the Draft Structure Plan be subject to the finalisation of the voluntary legal agreement to the satisfaction of the City. It is also recommended that the City ensure that the timing of the infrastructure upgrades be no later than 50% of the ultimate dwelling yield.

### Sustainability

Given the subject site's location over the Jandakot Water Mound, there is an increased need for future development to exhibit long term sustainability. To this end the applicant has developed a 'Sustainability Plan' which forms an appendix to the Draft Structure Plan. The Sustainability Plan was prepared following a series of workshops with City staff and the developer. The workshops evaluated, discussed and agreed upon a schedule of sustainability initiatives, which are to be embedded into the implementation and delivery of the development. This includes *"the provision of residential and associated supporting infrastructure, and the creation of 'community life' within the project area over the full term of its creation and function as a residential community"*.

As the document has been prepared to provide a balance in expectations between the sustainability objectives of the City and the developer, it has created 'business as usual' and 'stretch target' sustainability scenarios for the development. The seven key - sustainability principles are as follows:

- Sustainable Planning and Development
- Sense of Place and Healthy Communities
- Balanced Economic Growth
- Environmental Management
- Efficient Settlement and Use of Resources
- Management, Accountability, Transparency and Engagement
- Community Involvement

The above represent the overarching principles which are supplemented by specific actions and targets within the Plan. The City will be pursuing the translation of sustainability principles from strategy/plan to meaningful 'on the ground' contributions. As such every subsequent subdivision application, development application, detailed area plan or similar implementation application/plan will need to demonstrate how it responds or complies with the Sustainability Plan to the satisfaction of the City.

This will also align with the City's sustainability framework, as a way of demonstrating how this project exhibits leadership in sustainable urban planning. This will also provide an opportunity for the project to be a local case study for other developments to learn from.

### WAPC endorsement

The Draft Structure Plan was referred to the WAPC for comment in accordance with Clause 6.2.7.2 of the Scheme as it proposes the subdivision of land. The WAPC advised of a number of design additional relating to density ranges, the 20 ANEF noise contour, bushfire management and noise mitigation from Armadale Road.

The City supports the requested modifications and additions and the applicant is aware of its obligations in this regard. It is therefore recommended that approval of the Draft Structure Plan be subject to the applicant addressing all the requirements of the WAPC.

### Memorandum of Understanding for Implementation Stage

As already mentioned, given the size of the future subdivision and development of the subject site, it is considered appropriate for the City and the developers to enter into an appropriate Memorandum of Understanding ("MOU"). The MOU is anticipated to secure monthly design review meetings between the City and the applicant to cover such matters as subdivision design, staging and other related issues.

It should be noted that the WAPC's "Structure Plan Preparation Guidelines" have reduced the level of planning and design detail required at the structure plan stage. It is therefore considered imperative that a MOU be established to ensure the design objectives contained within the structure plan report translate to the subdivision plans submitted to the WAPC.

### Community Consultation Outcomes

Scheme Amendment No. 95 and the Draft Banjup Quarry Structure Plan were advertised for public comment for a period of 42 days. A total of 24 submissions were received, with 17 submissions of support or no objection subject to conditions or modifications and 7 submissions expressing concerns or objecting.

All submissions have been outlined and addressed in detail in the Schedule of Submissions (Attachment 6). The key issues that have been raised are summarised below.

### Traffic

Several submissions raised concerns in relation to traffic management in the locality. The basis for concern is that Jandakot Road and various intersections are currently constrained due to vehicle movements increasing as a result of residential developments to the east in the City of Armadale accessing the Freeway via rural and industrial standard

roads such as Jandakot Road and Solomon Road. It is perceived that the addition of development within the Banjup Quarry site will further compound existing problems.

The City has identified this as a concern also, and forms the basis of the required upgrades to infrastructure as a result of the Proposed Structure Plan. It is the City's position that upgrades to Jandakot and Solomon Roads be secured in a timely manner to ensure the effective management of additional traffic generated by the development. It is for this reason that the legal agreement with the developer is required to ensure the timely delivery of the required infrastructure upgrades.

The specific upgrades sought in relation to Solomon and Jandakot Roads will enable these sections of road to operate safely and efficiently where they abut the subdivision area. This will support the City's other strategies for infrastructure upgrades which include aspects of the North Lake Road bridge, Armadale Road and the Kwinana Freeway which are State Government infrastructure responsibilities. This is considered the appropriate strategy in which to deal with traffic issues for the future.

Main Roads WA raised concerns in relation to Armadale Road and the proposed southern entry into the proposed development as the final design of this intersection has not been determined. There are concerns that the final design may impact on broader access issues. The City acknowledges the importance of this intersection and as such recommends a specific notation be added to the Draft Structure Plan requiring that the location and design of the intersection be to the satisfaction of Main Roads WA.

#### Prospect for wider rezoning/urban development

Numerous submissions provided their support for the proposals on the basis that their properties within the Banjup 'Resource' zone also be afforded the ability to subdivide for residential purposes. This is not considered consistent with orderly and proper planning and is therefore not supported by the City.

It is noted that the subject site was rezoned from 'Rural – Water Protection' to 'Urban' under the MRS based on the recommendations of the WAPC's Draft Outer Metropolitan Perth and Peel Sub-regional Strategy which identified the site as having urban potential based on its proximity to the Cockburn activity centre. It should be noted that no other such investigations are being undertaken by the State Government for other areas of the 'Resource' zone above the Jandakot Water Mound.

Gazettal of the MRS rezoning was based on the proposal meeting and exceeding the planning and environmental objectives and requirements of various State Government approval authorities. The proposal was supported by extensive research into the potential environmental impacts and in particular the impact on the groundwater mound.

At this stage there is no support by the City for any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound) which hasn't been based upon strategic planning via Directions 2031 and the Draft Sub-Regional Strategy.

In view of the above, it is considered appropriate that Council note that City staff will continue their efforts in liaising with the DoP and WAPC in regards to the overall review of the Jandakot Water Mound and all associated strategies and policies. It is recommended this form part of a separate resolution to ensure the DoP and WAPC are aware of Council's desires for active engagement and participation in the review process.

#### Adjoining future development within Lot 1 Armadale Road

The landowners of Lot 1 Armadale Road, Banjup raised concerns with the proposed zoning for their site and the Draft Structure Plan's potential impact on their future development potential. With regards to the site zoning, it is their belief that Lot 1 is more suited to a 'Mixed Business' zone under the Scheme, rather than a 'Development' zone. This is not supported by the City as no formal proposal has been lodged or considered for Lot 1 at this stage. Accordingly it is too early to contemplate any degree of 'in principle' (or otherwise) support for land use alternatives for Lot 1.

The Scheme provides for the objective of the Development zone to *"provide for future residential, industrial or commercial development in accordance with a comprehensive Structure Plan prepared under the Scheme."*

Accordingly there is built in flexibility within the Development zone to provide for land use alternatives, based upon broader assessment of State and local planning policy. In order to make this clear within the Amendment document, it is recommended that Provision 2 of DA37 be modified as follows:

- "2. The Structure Plan is to provide for an appropriate mix of residential and non-residential land uses, in order to support the objective for a mixed use neighbourhood. Non-residential land uses may include compatible commercial and industrial light and service industry) land uses, as a means to provide an

appropriate interface and transition to the western adjoining Solomon Road Development Area 20.”

The City believes that the magnitude and complexity of issues facing the future land use of Lot 1 are such that a comprehensive planning approach is required in order to deal with these. Accordingly it is recommended that the Development zone approach be retained with the above modification to the DA37 provisions accordingly.

### Conclusion

It is recommended that Council adopt Amendment No. 95 for the purposes of providing a suitable framework and provision for the future subdivision and development of the subject area. As outlined in this report the Amendment is consistent with the site's MRS zoning of 'Urban' and conforms to the expectations of Directions 2031.

The associated Draft Structure Plan is generally consistent with the requirements of the City and WAPC however relevant modifications and conditions are required prior to approval as outlined in this report. It is therefore recommended that Council, subject to the gazettal of Amendment No. 95, approve the Draft Structure Plan subject to conditions including the finalisation of the associated LWMS, voluntary legal agreement, WAPC requirements and general editing of the Draft Structure Plan report document.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.
- Development that is soundly balanced between new and existing areas.

### **Budget/Financial Implications**

In accordance with the requirements of SPP3.6 and the Scheme, an analysis of community facilities and services requirements for the Draft Structure Plan area has been undertaken by the applicant in consultation with the City. As a result of the analysis, Scheme Amendment No. 98 was initiated by Council at its 13 December 2013 to add two infrastructure items to the existing Development Contribution Area 13 - Community Infrastructure being a full size playing field and a community centre.

Scheme Amendment No. 98 is currently awaiting consent to advertise from the Environmental Protection Authority.

## **Legal Implications**

*Planning and Development Act 2005*  
City of Cockburn Town Planning Scheme No. 3  
*Town Planning Regulations 1967*

## **Community Consultation**

Community consultation was carried out for a period of 42 days. The proposals were advertised in the newspaper, on the City's website, signs placed on site and letters were sent to affected landowners and government/servicing authorities in accordance with the Scheme requirements.

A total of 24 submissions were received. Analysis of the submissions has been undertaken within the 'Report' section above, as well as the attached Schedule of Submissions.

Staff also undertook a briefing of the Banjup Residents Association on 19 March 2013. This was a well-attended event (approximately 30 people) and was provided as a basis for broader discussion and to assist residents in making submissions on the proposal.

## **Attachment(s)**

1. Location Plan
2. MRS Amendment 1221/41 Map
3. Scheme Amendment No.95 Report
4. Banjup Quarry Structure Plan
5. Banjup Quarry Master Plan
6. Schedule of Submissions

## **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

## **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.



**14.9 (OCM 09/05/2013) - AGRICULTURAL - INTENSIVE (RETROSPECTIVE GREEN HOUSES, SHEDS & USE OF EXISTING BUILDINGS) - LOCATION: 365 (LOT 813) WATTLEUP ROAD HAMMOND PARK - OWNER: THANG VAN NGUYEN - APPLICANT: JET DESIGN & DRAFTING SERVICE (4411233) (A LEFORT) (ATTACH)**

**RECOMMENDATION**

That Council

- (1) grant temporary planning approval for Agriculture – Intensive (Retrospective Greenhouses & Use of Existing Buildings) at 365 (Lot 813) Wattleup Road subject to the following conditions and Advice Notes:

Conditions

1. This planning approval is valid for a period of two years from the date of the approval after which time the use of all temporary and permanent buildings on the site for agricultural – intensive purposes shall cease.
2. All agricultural activities on site shall take place within the greenhouse and shed buildings with no activities to occur outside the buildings.
3. A survey of the site being undertaken within 60 days from the date of this approval and any structures or buildings identified within 3metres of the property boundary being removed within 60 days from the survey date to the satisfaction of the City of Cockburn.

Advice Notes

1. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, or with any requirements of the City of Cockburn Town Planning Scheme No. 3 or with the requirements of any external agency. Prior to the commencement of any works associated with the development, a building permit may be required.
2. The City's Building Services Department advises that it has identified that some of the structures are failing structurally which will be required to be significantly upgraded to comply with the requirements of the National Construction Code or removed. In this regard, please

liaise directly with the City's Building Department.

- (2) notify the applicant and those who made a submission of Council's decision; and
- (3) issue a Directions Notice under section 214 of the *Planning and Development Act* for the removal of any buildings that have been illegally constructed within 3 metres of the property boundaries, subject to Condition No. 3 of the planning approval.

## COUNCIL DECISION

### Background

The subject site is 2.0414ha in area and is located on Wattleup Road, Hammond Park. The site is zoned 'Development' and is surrounded by other large sites, some vacant, some containing a dwelling and some containing other rural uses. The subject site contains a number of existing shed and greenhouse structures which were previously used for agricultural – intensive purposes (horticulture). The buildings have been disused for more than 12 months. The site does not contain a dwelling or any significant vegetation.

A search of the City's records reveals that approvals have been issued for:

- The existing shed on the north eastern corner of the site (1993).
- The existing shed on the north eastern corner of the site behind that approved in 1993 (1999).

There are no records to indicate that any of the greenhouse structures have planning or building approval from the City. In addition, site inspections undertaken by the City's Building Compliance Officer have revealed that one or more of the buildings may not have been constructed in accordance with previous approvals in relation to maintaining sufficient fire breaks.

The site and surrounding lots were previously rezoned from Rural to Development. Whilst there is no Local Structure Plan (LSP) for the subject site, a draft LSP has been prepared for the adjoining land to the east consisting of 5 lots. The LSP has not been finalised and an associated subdivision application was refused by the Western

Australian Planning Commission (WAPC) due to an issues associated with the Alcoa Residue Storage Areas. The refusal was reviewed by the State Administrative Tribunal (SAT) in 2011 which ordered that an air quality study be undertaken for a minimum period of 12 months which commenced in August 2012.

Scheme amendment (28) for the subject site and surrounding lots to introduce future Development Area 27 and associated Development Contribution Area into Town Planning Scheme No.3 has been adopted by Council and is currently with the Western Australian Planning Commission for endorsement.

This application was advertised to surrounding landowners for comment and objections were received which is why the application is being referred to Council for determination.

### **Submission**

This application proposes to recommence use of the existing shed structures and also seeks retrospective approval for a number of greenhouse/poly-tunnel structures and sheds which were previously constructed without approval for agricultural – intensive purposes. The owner of the land seeks to lease the property for this purpose and therefore specific detail about what is to be grown in the greenhouses is unknown at this stage.

### **Report**

#### Statutory Framework

##### *Metropolitan Region Scheme (MRS)*

The subject site is zoned 'Urban' under the Metropolitan Region Scheme (MRS). The current use does not accord with this zone. See comments below regarding this.

##### *City of Cockburn Town Planning Scheme No. 3 (TPS 3)*

The site is zoned 'Development' under the City of Cockburn's TPS 3 and the objective of this zone is:

*'To provide for future residential, industrial or commercial development in accordance with a comprehensive Structure Plan prepared under the Scheme'.*

No Structure Plan has been prepared, submitted or approved for the subject site. Clause 6.2.4.1 of TPS 3 states that:

*'The local government is not to: -*

- (a) consider recommending subdivision; or*
- (b) approve development of land within a Development Area unless there is a structure plan for the Development Area or the relevant part of the Development area.'*

Clause 6.2.4.2 however states that:

*'Notwithstanding clause 6.2.4.1, a local government may recommend subdivision or approve the development of land within a Development Area prior to a structure plan coming into effect in relation to the land, if the local government is satisfied that this will not prejudice the specific purposes and requirements of the Development Area and the owner's liability for the proportion of land or development can be fulfilled pursuant to clause 6.3.5.'*

Based on the above scheme provisions, Council is able to approve the development if it is of the opinion that it will not prejudice future development in the area which will be discussed later in the report.

It should be noted that in accordance with Clause 4.8 of TPS 3, the use of the site for agricultural activities would have enjoyed non-conforming use rights after the property was rezoned to Development several years ago. However Clause 4.10 states that:

*'Where a non-conforming use of any land or buildings has been discontinued for a period of **six months** such land or buildings shall not thereafter be used otherwise than in conformity with the provisions of the Scheme'.*

Based on the above scheme provision, since the use has been discontinued for a period greater than 6 months, the site no longer enjoys non confirming use rights for agricultural purposes.

The activities for which approval is sought constitute 'Agriculture–Intensive' and the definition of this under TPS 3:

*'means premises used for trade or commercial purposes, including outbuildings and earthworks, associated with the following –*

- (a) the production of grapes, vegetables, flowers, exotic or native plants, or fruit or nuts;*
- (b) the establishment and operation of plant or fruit nurseries;*
- (c) the development of land for irrigated fodder production or irrigated pasture (including turf farms); or*
- (d) aquaculture, whereby any fish farming operation for which a fish farm licence issued pursuant to the provisions of Part V*

*of the Fisheries Act 1905 (as amended) and the Fisheries Regulations 1938 (as amended) is required.'*

### Community Consultation

The application was advertised to four adjoining and nearby landowners and two objections were received from entities with multiple lot ownership in this area.

A summary of the objections is as follows:

1. The land is identified in the Southern Suburbs District Structure Plan as suitable for urban development and the use of the site for greenhouses conflicts with residential zoning.
2. Concerns that the proposed use may prejudice the development of adjoining land holdings into residential development into the future should the DEC or WAPC require buffers to protect the agricultural use of this land in the event approval is granted.
3. Approval of this use ahead of air quality monitoring being undertaken on Lot 809 (due for completion August 2013) is inappropriate and would potentially prejudice the residential development of the land should the monitoring show that there are no issues in the surrounding area.
4. Concerns about potential overspray from the use of chemicals associated with the agricultural use which may impact on neighbouring properties. This would then prejudice development of future properties by way of buffers or notifications on title which would impact on values of properties or reduce overall development yield.
5. Objection to the issue of a temporary approval as residential development is intended to progress on the adjoining site as soon as the air quality monitoring is completed for Lot 809 (should the monitoring program show that there are no issues in the surrounding area).

### Issues

#### ***Off-Site Impacts***

Whilst the exact nature of the agricultural use inside the greenhouses is unknown at this stage, the proposal does not include the use of any of the land outside the greenhouses and sheds for agricultural purposes. The buffers and development restrictions associated with open air Market Gardens as contained in the WAPC Planning Bulletin No.63 do not apply to this proposal. Should Council consider entertaining some form of temporary approval for the use, a condition could be imposed restricting activity to within the enclosed greenhouses and sheds.

Other off-site impacts include noise associated with heavy vehicles entering and exiting the site. This is not considered to be a current issue given the lack of development in the area, but could cause conflict with future residential development of the area. In this respect a temporary use of the site would be appropriate.

### ***Air Quality Monitoring Programme***

A subdivision application was lodged on the adjoining land (Lot 809, 811 & 9002) and refused on the basis that a portion of the land is situated within the 1.5km exclusion area associated with the Alcoa Residue Storage Areas. The decision was reviewed at the State Administrative Tribunal (Wattleup Road Development Company Pty Ltd v Western Australian Planning Commission [2011] WASAT 160) which resolved that a monitoring program should be undertaken for a period of at least 12 months to confirm if the subject land is suitable for residential development. In reaching its decision SAT explicitly stated that the precautionary principle should prevail based on the information available at the time of the decision. SAT also stated that *“the buffer should not be reflected in the town planning framework at this time”, or until the monitoring has been completed*. Due to the unknown outcome of the air quality monitoring programme, a permanent approval of agricultural use on this land would be inappropriate.

### ***Timing of New Residential Development***

Should the Air Quality Monitoring Programme due for completion in August 2013 reveal that there are no significant air quality issues in the area; residential development on adjoining and nearby land is likely to occur reasonably quickly. However the Draft LSP is still required to be finalised by WAPC, a subdivision application would be required to be lodged and determined by the WAPC, civil works would need to be completed then residential dwellings constructed. Without detailed knowledge of the developer's programme, it would be unlikely that dwellings would be constructed before May 2015 which is two years from now. Given this, should Council grant planning approval, it is recommended a condition be imposed restricting the timeframe for approval of the development to two years only.

### ***Fire Breaks***

Site inspections by City Officers revealed that part of one of the existing sheds has not been developed in accordance with the approved plans and may be set back less than 3m from the side boundary. As the minimum requirement for a firebreak is 3m, should Council consider granting approval, it is recommended a condition be imposed requiring any building that has been constructed within 3m of

the property boundary to be removed or modified to comply with a 3m minimum setback.

## **Conclusion**

The application for planning approval for use of the existing approved sheds and retrospective approval for the greenhouses and unapproved sheds on site for agricultural – intensive purposes is supported on a temporary basis for the following reasons:

- While it is recognised that this area is likely to be developed for residential purposes in the future, a temporary planning approval is considered appropriate as the timing of any future works remains unclear;
- The use of the permanent and temporary buildings on site for a period of two years (from May 2013-May 2015) is not considered to negatively impact on the amenity of future residents due to the time taken for:
  1. the completion of the air quality programme – assuming that this supports residential development;
  2. the adjoining landowner securing subdivision approval from the WAPC; the undertaking of civil works;
  3. the issue of building permits for dwellings; and
  4. the construction and completion of dwellings.
- It is considered to be highly unlikely given the above matters that any dwelling construction will be being completed on adjoining or land within two years.
- A two year temporary approval will ensure any potential impacts on the amenity of possible future residents can be addressed with finality.
- Considering that there are nearby and surrounding rural uses which are currently being undertaken.
- The proposed operations shall be contained within enclosed buildings
- It is considered reasonable that the land owner be able to undertake a suitable use on land provided that sufficient fire breaks are provided and structures comply with the National Construction Code.

## **Strategic Plan/Policy Implications**

### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

- Development that is soundly balanced between new and existing areas.

### **A Prosperous City**

- Promotion and support for the growth and sustainability of local businesses and local business centres.

### **Environment & Sustainability**

- Identification and minimisation of impacts to human health risk.

### **Budget/Financial Implications**

Costs involved in defending the decision in the State Administrative Tribunal which can be met by the Statutory Planning Operational Budget.

### **Legal Implications**

Nil

### **Community Consultation**

See Community Consultation section of the report above.

### **Attachment(s)**

1. Location Plan
2. Aerial Photo (January 2013)
3. Site Plan
4. Floor Plans/Elevations

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.



**15. FINANCE AND CORPORATE SERVICES DIVISION ISSUES****15.1 (OCM 09/05/2013) - LIST OF CREDITORS PAID - MARCH 2013 (076/001) (N MAURICIO) (ATTACH)****RECOMMENDATION**

That Council adopt the List of Creditors Paid for March 2013, as attached to the Agenda.

**COUNCIL DECISION****Background**

It is a requirement of the Local Government (Financial Management) Regulations 1996, that a List of Creditors be compiled each month and provided to Council.

**Submission**

N/A

**Report**

The List of Accounts for March 2013 is attached to the Agenda for consideration. The list contains details of payments made by the City in relation to goods and services received by the City.

**Strategic Plan/Policy Implications****Leading & Listening**

- Effective and constructive dialogue with all City stakeholders.
- A responsive, accountable and sustainable organisation.

**Budget/Financial Implications**

N/A

**Legal Implications**

N/A

**Community Consultation**

N/A

**Attachment(s)**

List of Creditors Paid – March 2013.

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**15.2 (OCM 09/05/2013) - STATEMENT OF FINANCIAL ACTIVITY AND ASSOCIATED REPORTS - MARCH 2013 (FS/S/001) (N MAURICIO) (ATTACH)**

**RECOMMENDATION**

That Council adopt the Statement of Financial Activity and associated reports for March 2013, as attached to the Agenda.

**COUNCIL DECISION**

**Background**

Regulations 1996 prescribes that a local government is to prepare each month a Statement of Financial Activity.

Regulation 34(2) requires the Statement of Financial Activity to be accompanied by documents containing:–

- (a) details of the composition of the closing net current assets (less restricted and committed assets);
- (b) explanations for each material variance identified between YTD budgets and actuals; and

- (c) any other supporting information considered relevant by the local government.

Regulation 34(4)(a) prescribes that the Statement of Financial Activity and accompanying documents be presented to Council within 2 months after the end of the month to which the statement relates.

The regulations require the information reported in the statement to be shown either by nature and type, statutory program or business unit. The City chooses to report the information according to its organisational business structure, as well as by nature and type.

Financial Management Regulation 34(5) requires Council to annually set a materiality threshold for the purpose of disclosing budget variance details. To this end, Council has adopted a materiality threshold variance of \$100,000 for the 2012/13 financial year.

### **Submission**

N/A

### **Report**

The City's overall financial performance to the end of March remains strong, with outperformance of the operating budget one of the key factors. Significant under spending in the City's capital program has also boosted the net current asset position, whilst the receipt of funds from the sale of land on Beeliar Drive to Coles has had a positive impact on the City's cash position.

### Closing Funds

The City's closing municipal position of \$62.4M is \$20.8M higher than the YTD budget target of \$41.6M. This favourable position is representative of numerous factors detailed further in this report.

The revised budget for the end of year closing position is currently showing a \$16k surplus, little changed from \$12k last month.

The closing funds position fluctuates throughout the year, as it gets impacted by various Council decisions and minor system adjustments and corrections. Details on the composition of the budgeted closing position are outlined in Note 3 to the financial report.

### Operating Revenue

YTD operating revenue of \$107.5M is tracking ahead of budget by \$3.2M. This has narrowed somewhat from last month's variance of

\$3.7M. The key contributor to this result continues to be Waste Services, with commercial landfill fees providing \$1.5M in additional revenue.

Other significant areas of outperformance include:

- \$0.6M additional revenue from part year rating and rate interest and penalties.
- \$0.2M extra raised for underground power charges
- \$0.7M of operating subsidies received ahead of budget in the Human Services business unit.

Areas where actual performance is trending behind the budget include:

- \$0.2M of fees and charges in the Human Services business unit (particularly comprising out of school care service fees).
- Fees and charges for the SLLC are \$0.1M behind target.
- \$0.3M of administration fees for administering the developer contribution schemes are yet to be internally accounted for.

Further details of material variances are disclosed in the Agenda attachment.

#### Operating Expenditure

Overall operating expenditure of \$77.4M (including depreciation) is tracking under budget by around \$3.3M.

The significant areas contributing to this positive result include:

- Waste collection expenses are \$0.9M below budget primarily due to lower RRRC gate fees incurred to date.
- Environment Services are showing a net underspend of \$0.5M against their YTD budget with \$185k underspent for Spearwood Ave offsets and \$245k for general reserves maintenance.
- Parks Maintenance costs are \$0.4M under their YTD budget with underspending in wages and materials & contracts.
- Engineering Services has underspending of \$0.4M comprised mainly of savings in street lighting costs of \$322k.
- Community Services is collectively \$0.6M under budget comprising favourable variances in CoSafe (\$142k), SLLC (\$171k) and Council's donation program (\$122k).
- There are savings of \$0.1M in Human Services salary costs due to the closure of the out of school care programs at Atwell and Harvest Lakes.

- Corporate Communications are currently showing a budget underspend of \$0.2M in the Summer of Fun Events program.
- Contract spending under Information Services is \$0.1M below YTD budget
- Admin charges of \$0.3M for developer contribution schemes are yet to be allocated.
- Health Services are \$0.3M under YTD budget primarily due to non-spending on contaminated sites remediation and clean-up activities.
- Libraries costs are nearly \$0.2M below budget due to YTD savings in salaries and contracts.
- Depreciation is tracking around \$0.3M below budget overall.

Detracting from the overall positive result is additional landfill levy accrued of \$1.6M to cover a potential liability.

The following table shows operating expenditure budgetary performance at a consolidated nature and type level:

Nature or Type Classification	Actual	YTD Amended Budget	Variance to Budget
	\$	\$	%
Employee Costs	\$28.7M	\$29.0M	1.1%
Materials and Contracts	\$23.3M	\$26.7M	12.7%
Utilities	\$2.9M	\$3.3M	12.5%
Insurances	\$1.8M	\$1.9M	2.2%
Other Expenses	\$7.7M	\$6.6M	-18.0%
Depreciation (non cash)	\$15.4M	\$15.7M	1.7%

Other expenses are adversely impacted by the additional accrual of landfill levy as referred to previously.

### Capital Expenditure

The City's capital budget has incurred expenditure of \$34.5M versus an YTD budget of \$54.3M. This results in an YTD variance of \$19.8M, up from \$18.6M last month.

This under spend is split across the following asset classes:

- Building construction works - \$10.5M
- Roads, footpaths & drainage - \$4.4M
- Plant & machinery - \$1.5M
- Computer infrastructure & software - \$1.3M
- Land development and acquisition - \$1.4M
- Landfill Infrastructure - \$0.2M
- Parks infrastructure development - \$0.6M

The significant spending variances by project are disclosed in the attached CW Variance analysis report.

### Capital Funding

Capital funding sources are highly correlated to capital spending and the sale of assets. Given the high underspend within the capital budget, capital funding sources are also showing large variances.

Significant variances include:

- Proceeds from land sales are \$1.2M behind the YTD budget, comprised mainly of subdivision of Lot 702 Bellier Place and Lot 65 Erpingham Road yet to be sold.
- Proceeds from plant and vehicle sales are \$0.4M behind the YTD budget.
- Loan funds of \$1.0M for the Emergency Services building project are yet to be raised, but has now been scheduled for June.
- Grants and developer contributions towards roads and buildings projects were collectively \$3.2M behind YTD targets. \$1.1M of this variance relates to federal funding for the GP Super Clinic project, which has been delayed.
- Transfers to Reserves are \$13.5M behind budget, mainly due to the \$11.9M sale proceeds for Ivankovich Ave (Coles site on Beeliar Drive) not being transferred as yet. This will occur before the end of the financial year.
- Transfers from Reserves are \$13.0M behind budget, consistent with the under spend in the capital budget mainly for the GP Super Clinic/Success Library.

### Cash & Investments

Council's cash and current/non-current investment holding increased to \$114.0M from \$103.6M the previous month as a result of the \$13.6M (GST incl.) settlement received during the month for the sale of Ivankovich Ave.

\$43.4M represents the balance currently held in the City's cash backed reserves, whilst another \$5.2M represents funds held for other restricted purposes such as bonds, restricted grants and capital infrastructure contributions. The remaining \$65.4M represents the cash and investment components of the City's working capital, required to fund ongoing operations, the capital program and annual reserve transfers.

The City's investment portfolio made a weighted annualised return of 4.69% for the month of March. This compared favourably against the adopted BBSW benchmark result of 2.94%.

The majority of investments are held in term deposit (TD) products placed with highly rated APRA (Australian Prudential Regulation Authority) regulated Australian banks. These are predominantly invested for terms ranging between three and six months in order to maximise the value offered within the current interest yield curve and to mitigate against cash flow liquidity risks. Whilst the Reserve Bank has reduced interest rates over recent times by 100 basis points, this investment strategy has ensured interest earnings are somewhat buffered from a marked downturn.

Interest earnings are on track to achieve the revised budget target of \$5.1M for 2012/13.

### Description of Graphs and Charts

There is a bar graph tracking Business Unit operating expenditure against budget. This provides a very quick view of how the different units are tracking and the comparative size of their budgets.

The Capital Expenditure graph tracks the YTD capital spends against the budget. It also includes an additional trend line for the total of YTD actual expenditure and committed orders. This gives a better indication of how the capital budget is being exhausted, rather than just purely actual cost alone.

A liquidity graph shows the level of Council's net current position (adjusted for restricted assets) and trends this against previous years. This gives a good indication of Council's capacity to meet its financial commitments over the course of the year.

Council's overall cash and investments position is provided in a line graph with a comparison against the YTD budget and the previous year's position at the same time.

Pie charts included show the break-up of actual operating income and expenditure by nature and type and the make-up of Council's current assets and liabilities (comprising the net current position).

## **Strategic Plan/Policy Implications**

### **Leading & Listening**

- A responsive, accountable and sustainable organisation.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A culture of risk management and compliance with relevant legislation, policy and guidelines

### **Budget/Financial Implications**

Material variances identified of a permanent nature (i.e. not due to timing issues) may impact on Council's final budget position (depending upon the nature of the item).

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

Statement of Financial Activity and associated reports – March 2013.

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **15.3 (OCM 09/05/2013) - TENDER NO. RFT 29/2012 - TEMPORARY PERSONNEL SERVICES (RFT 29/2012) (M PATTERSON) (ATTACH)**

### **RECOMMENDATION**

That Council accept Tender No.RFT29/2012 - Temporary Personnel Services commencing 1 July 2013, from the following submissions:

#### Category 1 – Clerical and Administrative Services

Adecco Australia Pty Ltd  
DFP Recruitment Services Pty Ltd  
Bluestone Global Limited (Humanis Group Ltd)

#### Category 2 – Professional and Executive Services

DFP Recruitment Services Pty Ltd  
Adecco Australia Pty Ltd  
Bluestone Global Limited (Humanis Group Ltd)

#### Category 3 – Technical and Tradespeople

Adecco Australia Pty Ltd  
DFP Recruitment Services Pty Ltd  
Bluestone Global Limited (Humanis Group Ltd)



Category 4 – Information Technology and Communications

Adecco Australia Pty Ltd  
 DFP Recruitment Services Pty Ltd  
 Data#3 Limited

Category 5 – Financial and Accounting Services

Adecco Australia Pty Ltd  
 DFP Recruitment Services Pty Ltd  
 Bluestone Global Limited (Humanis Group Ltd)

for an indicative total contract value of \$5,100,000.00 (Inc. GST) (\$4,590,000.00 Ex GST) for the duration of contract term, of three (3) years, with principal instigated extension options of one (1) year and twelve months, to a maximum of five (5) years in accordance with the submitted Schedule of Rates, for determining orders, variations and additional services. Contract commencement date as of 1<sup>st</sup> July 2013.

**COUNCIL DECISION****Background**

The City of Cockburn has been engaging with a number of companies for the supply of Temporary Personnel Services to ensure the continuity of service by the various business units. The range of personnel required across each business unit varies considerable and collectively requires the appointment of a panel of Tenderers.

Specifications for the supply of Temporary Personnel Services to facilitate each business unit's key requirements was developed and tenders subsequently called.

Tender No.RFT29/2012 – Temporary Personnel Services was advertised on Wednesday, 28 November 2012 in the Local Government Tenders Section of The West Australian Newspaper. It was also displayed on the City's e-tendering website between 28 November and 19 December 2012.

## Submission

Tenders closed at 2:00 p.m. (AWST) on Wednesday 19 December 2012 and 28 submissions were received. Tender submissions were received from:

1	Adecco Australia Pty Ltd
2	Bluestone Global Limited (Humanis Group Ltd)
3	Boston Kennedy (Operations) Pty Ltd
4	Clarius Group Pty Ltd
5	Core Business Australia Pty Ltd
6	Corestaff
7	Data#3 Limited
8	DFP Recruitment Services Pty Ltd
9	Drake Australia Pty Ltd
10	Flexi Staff Pty Ltd
11	Goldsilk Nominees Pty Ltd - ATF Spriggs Family Trust
12	Green Skills Inc.
13	Hays Specialist Recruitment (Australia) Pty Ltd
14	Hudson Global Resources (Aust.) Pty Ltd
15	IPA Personnel Pty Ltd
16	M2 Recruitment Pty Ltd
17	Mars Partnership Pty Ltd
18	Michael Page International Pty Ltd
19	Programmed Integrated Workforce Ltd (Programmed Group)
20	Randstad Pty Ltd
21	RecruitWest Pty Ltd
22	Robert Half Australia Pty Ltd
23	Skilled Group Pty Ltd
24	Spectrum Community Outcomes Pty Ltd
25	Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd
26	Steelcap Recruitment Pty Ltd
27	Talent International Pty Ltd
28	Toll Personnel Pty Ltd

## Report

### Compliance Criteria

The following index was used to determine whether the submissions received were compliant.

DESCRIPTION OF COMPLIANCE CRITERIA
(a) Compliance with the Conditions of Tendering – <a href="#">Part 1</a> of this Request
(b) Compliance with the Specification – <a href="#">Part 2</a> contained in the Request.
(c) Completion and submission of Form of Tender – <a href="#">Section 3.1</a>
(d) Compliance with Insurance Requirements and completion of <a href="#">Section 3.2.9</a> .
(e) Compliance with Fixed Price and completion of <a href="#">Section 3.6.2</a>
(f) Compliance with Sub-Contractors Requirements and completion of <a href="#">Section 3.7</a> .
(g) Compliance with and completion of the separate Price Schedule – <a href="#">Part 4</a> in the format provided. Refer to Section <a href="#">1.10.2</a>
(h) Compliance with the OSH Requirements and completion of <a href="#">Appendix A</a> .
(i) Compliance with ACCC Requirements and completion of <a href="#">Appendix B</a> .
(j) Acknowledgement of any Addenda issued.

Twenty seven (27) submissions were deemed compliant.

Core Business Australia Pty Ltd was deemed non-compliant as their Pricing Schedule was for hourly rates not the percentage fees as requested and therefore Core Business Australia Pty Ltd was not evaluated.

### **Evaluation Criteria**

Tenders were assessed against the following criteria:

Evaluation Criteria	Weighting Percentage
Demonstrated Experience in providing Temporary Personnel Services	20%
Tenderer's Resources including Key Personnel	20%
Demonstrated Understanding	35%
Tendered Price (Percentage Fees)	25%
Total Weighting	100%

Tender Intent / Requirement

The intent of the tender is to ensure effective management of Temporary Personnel Services by sourcing suitably qualified, experienced and licensed companies for the provision of temporary personnel services for the below outlined categories. Contractors were encouraged to submit for either the full scope of works or the category relevant their service provisions.

**Category 1 – Clerical and Administrative Services****Category 2 – Professional and Executive Services****Category 3 – Technical and Tradespeople****Category 4 – Information Technology and Communications****Category 5 – Financial and Accounting Services**Evaluation Panel

The tender submissions were evaluated by:

- 1) Melanie Carter, Employee Relations Manager (Chairperson)
- 2) Nelson Mauricio, Manager Financial Services
- 3) John West, Manager Building Services
- 4) Anton Lees, Manager Parks and Environment

Scoring Table

<b>Category 1 - Clerical &amp; Administrative</b>			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
<b>Adecco Australia Pty Ltd**</b>	<b>61.33%</b>	<b>20.28%</b>	<b>81.61%</b>
<b>DFP Recruitment Services Pty Ltd**</b>	<b>59.31%</b>	<b>19.46%</b>	<b>78.77%</b>
<b>Bluestone Global Limited (Humanis Group Ltd)**</b>	<b>54.38%</b>	<b>20.72%</b>	<b>75.10%</b>
Drake Australia Pty Ltd	54.09%	19.61%	73.70%
Skilled Group Pty Ltd	52.68%	20.67%	73.35%
Flexi Staff Pty Ltd	54.09%	17.75%	71.84%
Hays Specialist Recruitment (Australia) Pty Ltd	54.21%	17.51%	71.73%
Clarius Group Pty Ltd	53.95%	17.63%	71.58%
Spectrum Community Outcomes Pty Ltd	48.45%	22.13%	70.58%
IPA Personnel Pty Ltd	51.68%	18.40%	70.08%
Hudson Global Resources (Aust.) Pty Ltd	53.61%	15.47%	69.09%
Michael Page International Pty Ltd	49.54%	19.10%	68.64%
Programmed Integrated Workforce Ltd (Programmed Group)	49.23%	17.35%	66.58%

Category 1 - Clerical & Administrative			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
Randstad Pty Ltd	48.58%	16.29%	64.86%
Toll Personnel Pty Ltd	45.69%	18.88%	64.57%
Goldsilks Nominees Pty Ltd - ATF Spriggs Family Trust	50.45%	13.51%	63.96%
RecruitWest Pty Ltd	44.71%	18.13%	62.84%
Boston Kennedy (Operations) Pty Ltd	46.11%	16.43%	62.54%
Mars Partnership Pty Ltd	41.89%	18.63%	60.51%
M2 Recruitment Pty Ltd	40.49%	19.05%	59.54%
Steelcap Recruitment Pty Ltd	42.49%	14.37%	56.86%
Robert Half Australia Pty Ltd	43.04%	12.54%	55.58%
Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd	34.78%	18.83%	53.61%
Corestaff	33.36%	13.52%	46.88%
Data#3 Limited	No submission for this Category		
Green Skills Inc.	No submission for this Category		
Talent International Pty Ltd	No submission for this Category		

### \*\* Recommended Submission

Category 2 - Professional and Executive			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
<b>DFP Recruitment Services Pty Ltd**</b>	<b>59.31%</b>	<b>20.45%</b>	<b>79.76%</b>
<b>Adecco Australia Pty Ltd**</b>	<b>61.33%</b>	<b>18.08%</b>	<b>79.41%</b>
<b>Bluestone Global Limited (Humanis Group Ltd)**</b>	<b>54.38%</b>	<b>20.31%</b>	<b>74.68%</b>
Flexi Staff Pty Ltd	54.09%	19.72%	73.80%
Skilled Group Pty Ltd	52.68%	20.02%	72.69%
Clarius Group Pty Ltd	53.95%	17.70%	71.65%
Drake Australia Pty Ltd	54.09%	17.32%	71.41%
Hays Specialist Recruitment (Australia) Pty Ltd	54.21%	16.28%	70.49%
Spectrum Community Outcomes Pty Ltd	48.45%	22.01%	70.46%
IPA Personnel Pty Ltd	51.68%	18.62%	70.30%
Michael Page International Pty Ltd	49.54%	19.44%	68.98%
Hudson Global Resources (Aust.) Pty Ltd	53.61%	14.66%	68.27%
Programmed Integrated Workforce Ltd (Programmed Group)	49.23%	17.57%	66.80%
Randstad Pty Ltd	48.58%	16.45%	65.03%
Toll Personnel Pty Ltd	45.69%	19.00%	64.69%
Goldsilks Nominees Pty Ltd - ATF Spriggs	50.45%	14.03%	64.48%

Category 2 - Professional and Executive			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
Family Trust			
Boston Kennedy (Operations) Pty Ltd	46.11%	16.80%	62.91%
RecruitWest Pty Ltd	44.71%	18.15%	62.86%
M2 Recruitment Pty Ltd	40.49%	19.31%	59.80%
Mars Partnership Pty Ltd	41.89%	16.46%	58.35%
Steelcap Recruitment Pty Ltd	42.49%	14.83%	57.32%
Robert Half Australia Pty Ltd	43.04%	12.32%	55.36%
Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd	34.78%	18.77%	53.54%
Corestaff	33.36%	13.68%	47.04%
Data#3 Limited	No Submission for this category		
Green Skills Inc.	No Submission for this category		
Talent International Pty Ltd	No Submission for this category		

### \*\* Recommended Submission

Category 3 - Technical & Tradespersons			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
<b>Adecco Australia Pty Ltd**</b>	<b>61.33%</b>	<b>18.98%</b>	<b>80.30%</b>
<b>DFP Recruitment Services Pty Ltd**</b>	<b>59.31%</b>	<b>19.98%</b>	<b>79.30%</b>
<b>Bluestone Global Limited (Humanis Group Ltd)**</b>	<b>54.38%</b>	<b>20.66%</b>	<b>75.03%</b>
Clarius Group Pty Ltd	53.95%	19.98%	73.93%
Drake Australia Pty Ltd	54.09%	19.51%	73.60%
Skilled Group Pty Ltd	52.68%	20.80%	73.47%
Hays Specialist Recruitment (Australia) Pty Ltd	54.21%	18.71%	72.92%
Flexi Staff Pty Ltd	54.09%	18.32%	72.41%
IPA Personnel Pty Ltd	51.68%	19.87%	71.54%
Spectrum Community Outcomes Pty Ltd	48.45%	20.90%	69.35%
Programmed Integrated Workforce Ltd (Programmed Group)	49.23%	19.42%	68.64%
Randstad Pty Ltd	48.58%	19.80%	68.38%
Toll Personnel Pty Ltd	45.69%	19.62%	65.31%
RecruitWest Pty Ltd	44.71%	19.68%	64.39%
Green Skills Inc.	47.80%	14.90%	62.70%
Mars Partnership Pty Ltd	41.89%	19.78%	61.67%
M2 Recruitment Pty Ltd	40.49%	19.81%	60.30%
Robert Half Australia Pty Ltd	43.04%	15.54%	58.58%
Steelcap Recruitment Pty Ltd	42.49%	16.08%	58.57%
Staff Link (WA) Pty Ltd - Staff Link	34.78%	17.58%	52.35%

Category 3 - Technical & Tradespersons			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
Personnel Pty Ltd			
Corestaff	33.36%	14.54%	47.91%
Boston Kennedy (Operations) Pty Ltd	No Submission for this Category		
Data#3 Limited	No Submission for this Category		
Goldsilk Nominees Pty Ltd - ATF Spriggs Family Trust	No Submission for this Category		
Hudson Global Resources (Aust.) Pty Ltd	No Submission for this Category		
Michael Page International Pty Ltd	No Submission for this Category		
Talent International Pty Ltd	No Submission for this Category		

**\*\* Recommended Submission**

Category 4 - Information Technology and Communications			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
<b>Adecco Australia Pty Ltd**</b>	<b>61.33%</b>	<b>19.06%</b>	<b>80.39%</b>
<b>DFP Recruitment Services Pty Ltd**</b>	<b>59.31%</b>	<b>19.98%</b>	<b>79.30%</b>
<b>Data#3 Limited**</b>	<b>55.99%</b>	<b>17.90%</b>	<b>73.88%</b>
Drake Australia Pty Ltd	54.09%	18.97%	73.06%
Clarius Group Pty Ltd	53.95%	18.72%	72.67%
Flexi Staff Pty Ltd	54.09%	18.30%	72.39%
Skilled Group Pty Ltd	52.68%	19.58%	72.26%
Hays Specialist Recruitment (Australia) Pty Ltd	54.21%	17.14%	71.35%
Spectrum Community Outcomes Pty Ltd	48.45%	21.80%	70.25%
Hudson Global Resources (Aust.) Pty Ltd	53.61%	16.05%	69.66%
IPA Personnel Pty Ltd	51.68%	17.90%	69.58%
Talent International Pty Ltd	49.40%	19.45%	68.85%
Michael Page International Pty Ltd	49.54%	19.10%	68.64%
Programmed Integrated Workforce Ltd (Programmed Group)	49.23%	16.88%	66.11%
Randstad Pty Ltd	48.58%	15.74%	64.31%
Toll Personnel Pty Ltd	45.69%	18.38%	64.07%
Boston Kennedy (Operations) Pty Ltd	46.11%	16.08%	62.20%
M2 Recruitment Pty Ltd	40.49%	18.78%	59.27%
Mars Partnership Pty Ltd	41.89%	17.19%	59.07%
Steelcap Recruitment Pty Ltd	42.49%	13.93%	56.42%
Robert Half Australia Pty Ltd	43.04%	12.23%	55.27%
Corestaff	33.36%	13.11%	46.47%
Bluestone Global Limited (Humanis Group Ltd)	No Submission for this Category		
Goldsilk Nominees Pty Ltd - ATF Spriggs Family Trust	No Submission for this Category		

Category 4 - Information Technology and Communications			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
Green Skills Inc.	No Submission for this Category		
RecruitWest Pty Ltd	No Submission for this Category		
Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd	No Submission for this Category		

**\*\* Recommended Submission**

Category 5 - Financial and Accounting			
Tenderer's Name	Non Cost Evaluation	Cost Evaluation	Total
	75%	25%	100%
<b>Adecco Australia Pty Ltd**</b>	<b>61.33%</b>	<b>19.02%</b>	<b>80.34%</b>
<b>DFP Recruitment Services Pty Ltd**</b>	<b>59.31%</b>	<b>20.00%</b>	<b>79.31%</b>
<b>Bluestone Global Limited (Humanis Group Ltd)**</b>	<b>54.38%</b>	<b>20.37%</b>	<b>74.75%</b>
Flexi Staff Pty Ltd	54.09%	19.19%	73.28%
Drake Australia Pty Ltd	54.09%	18.97%	73.06%
Skilled Group Pty Ltd	52.68%	19.56%	72.23%
Clarius Group Pty Ltd	53.95%	17.28%	71.23%
Spectrum Community Outcomes Pty Ltd	48.45%	21.78%	70.23%
Hays Specialist Recruitment (Australia) Pty Ltd	54.21%	15.56%	69.77%
Hudson Global Resources (Aust.) Pty Ltd	53.61%	14.76%	68.37%
Michael Page International Pty Ltd	49.54%	18.75%	68.29%
Programmed Integrated Workforce Ltd (Programmed Group)	49.23%	17.00%	66.23%
Randstad Pty Ltd	48.58%	15.94%	64.52%
Toll Personnel Pty Ltd	45.69%	18.53%	64.22%
Gold silk Nominees Pty Ltd - ATF Spriggs Family Trust	50.45%	13.16%	63.61%
Boston Kennedy (Operations) Pty Ltd	46.11%	16.08%	62.19%
M2 Recruitment Pty Ltd	40.49%	18.70%	59.19%
Mars Partnership Pty Ltd	41.89%	17.25%	59.13%
Steelcap Recruitment Pty Ltd	42.49%	14.02%	56.51%
Robert Half Australia Pty Ltd	43.04%	12.19%	55.23%
Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd	34.78%	18.48%	53.26%
Corestaff	33.36%	13.17%	46.53%
Data#3 Limited	No Submission for this Category		
Green Skills Inc.	No Submission for this Category		
IPA Personnel Pty Ltd	No Submission for this Category		
RecruitWest Pty Ltd	No Submission for this Category		
Talent International Pty Ltd	No Submission for this Category		

**\*\* Recommended Submission**



## Evaluation Criteria Assessment

### Demonstrated Experience

Adecco Australia Pty Ltd, DFP Recruitment Services Pty Ltd, Bluestone Global Limited (Humanis Group Ltd), Clarius Group Pty Ltd, Flexi Staff Pty Ltd, Goldsilk Nominees Pty Ltd - ATF Spriggs Family Trust and Programmed Integrated Workforce Ltd (Programmed Group) clearly demonstrated that they had previous relevant experience in particular within the Local Government Environment in managing large personnel supply tenders. All these companies were able to satisfy the panel that they had a strong ability to resolve issues that may arise as part of the contract and could demonstrate that they had a proven track record of achieving favourable outcomes.

Data#3 Limited, Drake Australia Pty Ltd, Skilled Group Pty Ltd, Spectrum Community Outcomes Pty Ltd, Hays Specialist Recruitment (Australia) Pty Ltd, Hudson Global Resources (Aust.) Pty Ltd, Michael Page International Pty Ltd, Programmed Integrated Workforce Ltd (Programmed Group), Randstad Pty Ltd, Toll Personnel Pty Ltd, Goldsilk Nominees Pty Ltd - ATF Spriggs Family Trust, Boston Kennedy (Operations) Pty Ltd, M2 Recruitment Pty Ltd, Mars Partnership Pty Ltd, Steelcap Recruitment Pty Ltd, Robert Half Australia Pty Ltd, Staff Link (WA) Pty Ltd - Staff Link Personnel Pty Ltd, Corestaff, Green Skills Inc., IPA Personnel Pty Ltd, RecruitWest Pty Ltd and Talent International Pty Ltd outlined their experience in the supply of personnel, were able to resolve issues that may arise as part of the contract and could demonstrate that they had a proven track record of achieving favourable outcomes however were evaluated less favourably than the other tenderers.

### Tenderer's Resources including Key Personnel

All tenderers showed they had sufficient key personnel and resources to fulfil the scope of works. All Tenderers clearly outlined their key personnel and demonstrated their capacity to supply and sustain the necessary human and other resources throughout the duration of the proposed contract. Adecco Australia Pty Ltd, DFP Recruitment Services Pty Ltd, Bluestone Global Limited (Humanis Group Ltd) and Hudson Global Resources (Aust.) Pty Ltd were evaluated to have a more favourable capacity to supply and sustain the necessary human and other resources throughout the duration of the proposed contract.

### Demonstrated Understanding

All tenderers were considered to have a satisfactory level of demonstrated understanding of the scope of works and an adequate

process for delivering services in line with the City's policies and demonstrated sound customer service guidelines. Adecco Australia Pty Ltd, Bluestone Global Limited (Humanis Group Ltd), DFP Recruitment Services Pty Ltd, Drake Australia Pty Ltd, Hays Specialist Recruitment (Australia) Pty Ltd, Hudson Global Resources (Aust.) Pty Ltd, Clarius Group Pty Ltd and Flexi Staff Pty Ltd were evaluated to have superior understanding of the scope of works and were scored accordingly.

### Tendered Price

The schedule of rates supplied by each tenderer was assessed in accordance with the different categories. A cumulative score was determined for the tenderer in the respective category and ranked accordingly.

### **Summation**

The evaluation panel recommends that Council accept the submissions received from Adecco Australia Pty Ltd, DFP Recruitment Services Pty Ltd, Bluestone Global Limited (Humanis Group Ltd) and Data#3 Limited as being the most advantageous to perform the supply of temporary personnel supply to achieve its strategic objectives

This recommendation is based on:

- Well demonstrated experience in performing similar work at other local authorities.
- A range of personnel that have the experience in managing the works associated with the requirements of the contract.
- Have the required resources and contingency measures to undertake the works.
- The schedules of rates submitted by each tenderer is considered fair and reasonable.

### **Strategic Plan/Policy Implications**

#### **Leading & Listening**

- A responsive, accountable and sustainable organisation.
- Quality customer service that promotes business process improvement and innovation that delivers our strategic goals.
- Manage our financial and infrastructure assets to provide a sustainable future.
- A skilled and engaged workforce.

## Budget/Financial Implications

The appointment of a tender panel will enable the City to better facilitate the supply of temporary personnel and manage and control the cost of Temporary Personnel Services. Tendered price was a consideration in the evaluation of tenders (25%) and it is perceived that the introduction of this tender panel will reduce the cost of employing Temporary Personnel. Budget allocation will be from each general ledger or operational budgets each financial year.

The schedule of rates submitted by the panel of contractors will be utilised in the budgeting process to determine the required budget.

The table below indicates an indicative expenditure for Temporary Personnel Services over the past three (3) years.

The three (3) year indicative average has been used as the Contract costs per annum for this tender.

<b>Financial Year</b>	<b>Indicative Turnover (incl GST)</b>
2009/10	\$1, 700, 000
2010/11	\$1, 700, 000
2011/12	\$1, 700, 000
Total 2009 - 2012	\$5, 100, 000
<b>Three Year AVG</b>	<b>\$1, 700, 000</b>

## Legal Implications

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers

## Community Consultation

N/A

## Attachment(s)

The following Confidential Attachments are provided under separate cover:

1. Compliance Criteria Assessment;
2. Consolidated Evaluation Sheet; and
3. Tendered Prices

### **Advice to Proponent(s)/Tenderers**

Those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil

## **16. ENGINEERING AND WORKS DIVISION ISSUES**

### **16.1 (OCM 09/05/2013) - PROPOSED ESTABLISHMENT OF A ROAD SAFETY AND TRAVELSMART REFERENCE GROUP (ES/R/002) (J MCDONALD) (ATTACH)**

#### **RECOMMENDATION**

That Council

- (1) adopt the Terms of Reference for the purposes of establishing a Road Safety and Travelsmart Reference Group;
- (2) endorse Mayor Logan Howlett, Cllr ..... (East Ward), Cllr ..... (West Ward) and Cllr ..... (Central Ward) as Council representatives in the Road safety and Travelsmart Reference Group;
- (3) seek nominations from the following stakeholders to be represented on the Road safety and Travelsmart Reference Group:
  - WALGA
  - Western Australian Police Service
  - Main Roads Western Australia
  - Travelsmart Officer
  - Youth Advisory Committee (YAC representative)
  - Emergency Services
  - Road Safety Group representative
- (4) coordinate the inaugural meeting of the Road Safety and Travelsmart Reference Group for August 2013.

#### **COUNCIL DECISION**

## Background

At the Ordinary Meeting of Council on 13 December 2012 the following Matter to be noted for investigation was requested by Mayor Logan Howlett:

*That a report be presented to the March 2013 Council Meeting aimed at establishing a Road Safety and Traffic Management Committee of Council.*

*The objectives to include but not be limited to:*

- *Establishing a Youth Driver Education and Training Centre.*
- *Creating an 'on-line' district wide car-pooling facility*
- *Examining speed reduction strategies on identified roads*
- *Signalised intersections*
- *Pedestrian safety*
- *Bike rider safety*
- *Improved bus routes*
- *Major road infrastructure projects & local road synergies*
- *TravelSmart Program Initiatives*
- *Exploring potential partnerships and funding opportunities including:*
  - *Local governments in the south west metropolitan area*
  - *The Western Australian Police*
  - *Department of Health*
  - *Road Safety Council*
  - *Royal Automobile Association of WA*
  - *The Department of the Attorney General (Confiscation Grants program)*
  - *Insurance Council of Australia*
  - *Lotterywest*
  - *Department of Education*
  - *Motor Vehicle Dealers Association*

A report was presented to Council and adopted on 14 March 2013 with the following recommendation:

*That Council:*

- (1) *support the concept of establishing a Road Safety and Travelsmart Reference Group based on the WALGA Roadwise framework;*
- (2) *seek a briefing on the Roadwise Program by WALGA at its April General Briefing; and*

- (3) *receive a Draft Terms of Reference for the Road Safety and Travelsmart Reference Group at the May Ordinary Council Meeting.*

This report seeks endorsement of the Terms of Reference and establishing the Road Safety and Travelsmart Reference Group.

### **Submission**

N/A

### **Report**

The scope outlined by Mayor Howlett for the Road Safety and Traffic Management Committee generally reflects the scope of the WALGA initiated Roadwise Program. WALGA's RoadWise Program was formed in 1994 and has served as an important, effective framework by which the Association has pursued road safety objectives throughout Western Australia in conjunction with its stakeholder partners. The Program is aimed at securing greater community and regional stakeholder involvement in delivering road safety initiatives.

A briefing on the Roadwise Program has been provided by WALGA's Regional Road Safety Officer Metro South, Ms Melissa Pickering. Melissa has also assisted in the development of the Term of Reference for the City of Cockburn Road Safety and Travelsmart Reference Group.

### **Road Safety and Travelsmart Reference Group - Term of Reference**

The Terms of Reference has been developed to outline the purpose and structure of the Road Safety and Travelsmart Reference Group. It details the Vision, objectives, scope and guiding principles, roles and responsibilities of the group.

### **Purpose**

The following guiding principles have been developed for the Reference Group:

- Promote an integrated transport system which balances environmental impacts and community needs.
- Raise community awareness of road safety issues and initiatives in local communities.
- Review road safety strategies that may be adopted by the City of Cockburn, Main Roads WA, the Western Australian Police Service or any other statutory authority that has the ability to influence road safety in the community.

- Identify community concerns about road safety and road safety issues, potential black spot projects and poor road user behaviour and develop initiatives to address these identified road safety issues.
- Facilitate and promote healthy transport opportunities by promoting the City's TravelSmart initiative and implementation of walkway, bike and trails master plans.
- Identify a holistic regional approach to freight management

### Membership & Governance

The Reference Group is to be established and Membership appointed by Council. The membership of the Road Safety and Travelsmart Reference Group shall generally comprise the following:

- Up to four (4) elected members as delegates of the City of Cockburn. The Elected Member representation will consist of the Mayor (or his delegate) and an elected Member from each Ward.
- One (1) WALGA RoadWise representative
- Up to six (6) representatives of organisations relevant to the promotion of road safety issues, which may be drawn from groups such as the following:
  - Western Australian Police Service
  - Main Roads Western Australia
  - Travelsmart Officer
  - Youth Advisory Committee (YAC representative)
  - Emergency Services
  - Road Safety Group representative

The presiding member shall be appointed by the Reference Group at its inaugural meeting under a procedure general agreed to by members present. The Presiding Member is responsible for the good and reasonable conduct of Reference Group meetings and shall determine the meeting procedures as required.

### Meeting Frequency

Meetings will generally be held on a quarterly basis in February, May, August and November, with the start time and venue being determined by the Group. The Group will however determine meeting frequency based on the level of business required to be transacted.

Members of the Reference Group shall endeavour to attend all scheduled meetings of the Reference Group. The quorum of any meeting shall be a half plus one of the number of appointed members and voting shall be by consensus of the members present or by a simple majority if deemed necessary by the Presiding Member.

### Administrative Support

Provision of administrative support (agenda and minutes) for meetings is generally provided by Local Government and would be the preferred option. The City's Traffic and Transport Engineer is the officer nominated to provide administrative support to the Reference Group. All activities and communications will be coordinated through the Traffic and Transport Engineer and all enquiries and requests for support should be directed through this officer.

### Conclusion

It is recommended Council adopt the Terms of Reference for the purposes of establishing a Road Safety and Travelsmart Reference Group and call for nominations from the identified stakeholder groups in preparation for an inaugural meeting in early August 2013.

### **Strategic Plan/Policy Implications**

#### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

#### **Moving Around**

- Facilitate and promote healthy transport opportunities.
- A safe and efficient transport system.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

Additional staff resources, administration may be required dependant on the scope of the Group.

### **Legal Implications**

Any committee would need to be established and operated in compliance with the requirements of the Local Government Act 1995

### **Community Consultation**

Nil.

### **Attachment(s)**



## 1. Terms of Reference

### Advice to Proponent(s)/Submissioners

The Proponent(s) have been advised that this matter is to be considered at the 9 May 2013 Council Meeting

### Implications of Section 3.18(3) Local Government Act, 1995

Nil.

## 16.2 (OCM 09/05/2013) - ROCKINGHAM ROAD - INTRODUCTION OF A 40KPH ZONE FROM PHOENIX ROAD TO SPEARWOOD AVENUE (450498) (J KIURSKI) (ATTACH)

### RECOMMENDATION

That Council endorse an approach to MRWA seeking a review and reduction of the speed restriction along Rockingham Road between Spearwood Avenue and Phoenix Road from 60km/hr to 40km/hr.

### COUNCIL DECISION

## Background

At the Ordinary Meeting of Council on 13 December 2012 the following Matter to be noted for investigation was requested by Mayor Logan Howlett that a report be prepared for the February 2013 Ordinary Council Meeting on the introduction of a 40kph zone on Rockingham Road from Phoenix Road to Spearwood Avenue, Spearwood. A subsequent report was presented to the 14/02/13 OCM seeking a deferral until May 2013.

## Submission

N/A

## Report

Rockingham Road is classified as a District Distributor A road under the road hierarchy classification of roads within the City of Cockburn.

The function of these roads is to collect and distribute traffic within the residential, industrial and commercial areas.

A preliminary assessment of the current traffic environment has been completed which includes a traffic survey, a review of traffic count data and a review of traffic crash history over the last 5 years particularly at the intersections between Phoenix Road and Spearwood Avenue. The following information details the outcomes of the assessment undertaken to date.

The traffic counts for the nominated section of Rockingham Road between Phoenix Road and Spearwood Avenue

The traffic survey was completed in March 2013 on location between Lancaster Street and Coleville Crescent. The existing traffic volume is:

<b>Rockingham Road - Between Lancaster St &amp; Coleville Cr</b>	
<b>PARAMETARS</b>	<b>VALUE</b>
Traffic Volume (AWT)	17470
85 <sup>th</sup> Percentile Speed	65.2
Traffic as Peak-hour Percentile of 24h volume	8%
Heavy Vehicle Percentile of Total Traffic Flow	4%

The section of Rockingham Road between Phoenix Road and Spearwood Avenue is a bus route with more than 300 busses per day.

The review of traffic crash data for intersections and section of road

The following provides a summary of crash data that we have been able to ascertain for the last 5 years.

<b>Summary of Intersections and Section of Road Crashes</b>		
Rockingham Road	Phoenix Road	72
Rockingham Road	Lancaster Street	14
Mid-block Phoenix Rd to Lancaster St (includes 3 major crossovers/accesses)		46
Rockingham Road	Kent Street	7
Rockingham Road	Coleville Crescent	9

Mid-block Kent St to Coleville Cr (includes 6 major crossovers/accesses)		49
Rockingham Road	Spearwood Avenue	59
Mid-block Coleville Cr to Spearwood Ave (includes 2 major crossovers/accesses)		11
<b>Total Crashes:</b>		267

The crash data of the intersections and the mid-blocks indicates 267 reported crashes over the last 5 years. The report indicates a high and increasing incidence of crashes since 2008. Majority of crashes are at the signalised intersections of Rockingham Rd/Phoenix Rd, Rockingham Rd/Lancaster St and Rockingham Rd/Spearwood Ave. The mid-block crashes are directly related to access/egress of the Phoenix Shopping Centre and other businesses in the area.

#### Traffic Management Evaluation

Whilst the application of the City's Policy SEW3 'Local Area Traffic Management and the "Warrant Criteria and Weightings"' does not strictly relate to DDA's, Rockingham Road would not warrant further treatment as the overall weighted score of 33.2 is still below 40, which is a baseline for consideration and the installation of traffic calming treatments.

#### Conclusion

Main Roads WA is responsible for regulatory devices including speed zones. If Council wished to introduce a 40mk/hr speed restriction along Rockingham Road it would need to seek a review from MRWA. Officers have sought preliminary feedback from MRWA on the likelihood of support for a reduction in the posted speed limit and are awaiting feedback from that approach. It is recommended that Council formally endorses an approach to MRWA for a review of the speed restriction along Rockingham Road between Spearwood Avenue and Phoenix Road.

#### **Strategic Plan/Policy Implications**

##### **Community & Lifestyle**

- Safe communities and to improve the community's sense of safety.

### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

It is not possible to cost the need for road modifications until MRWA has examined the proposal and identified what road modifications are required.

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

1. Traffic warrants criteria and weighting report.
2. Aerial Photograph of subject road.

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.3 (OCM 09/05/2013) - CITY OF COCKBURN WATER CONSERVATION PLAN 2013-2018 (144/001) (A LEES) (ATTACH)**

### **RECOMMENDATION**

That Council adopt the City of Cockburn Water Conservation Plan 2013 – 2018.

<b>COUNCIL DECISION</b>
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### Background

In 2007 Council adopted the Water Conservation A Sustaining Strategy which outlined a number of water management and broader climate change initiatives.

The strategy outlined specific water management techniques, meeting future ground water demands, joining the ICLEI Water Campaign, Irrigation Operating Strategies, Port Coogee–Groundwater Interception and climate change considerations.

Since the implementation of the Water Conservation Strategy a number of other strategies have been adopted by Council which has impacted on this strategy requiring a revision.

This report seeks endorsement of a revised strategy, Water Conservation Plan 2013-2018, which reviews existing strategies and introduces new actions to be achieved over the next 5 years.

### Submission

N/A

### Report

The Water Conservation Plan 2013–2018 has been developed to provide strategic direction on water conservation and quality initiatives within the City's current and future public open space. The plan provides a coordinated approach to sustainable water management and demonstrates leadership in meeting specific and achievable water reduction targets.

Climate change impacts are becoming increasingly evident and are impacting on ecosystems and water supplies throughout the City and the wider metropolitan area. The Cities' Climate Change Adaption Plan identifies "a reduction in water availability for watering parks and natural wetlands" as viable risks which could be catastrophic and requires treatment plans.

The Water Conservation Plan 2013-2018 identifies the following actions to ensure a sustainable water environment.

1. ***Ensure developers have a licenced water allocation for the POS associated with the subdivision development and the licence is transferred to the City at the expiration of the maintenance period***

The City will be receiving an additional 125ha of public open space based on future development areas which will need groundwater for irrigation purposes. Developers will be required to obtain a licence to extract groundwater from the Department of Water and transfer that licence to the City at the conclusion of the maintenance period. This process will ensure that the City can continue to irrigate parks into the future in accordance with the licence conditions.

2. ***Adopt the City of Cockburn's Irrigation Operating Strategy April 2011- April 2014 and the Hammond Road Sporting Complex Irrigation Operating Strategy Sept 2011–Sept 2014***

The Department of Water licence approval conditions for water abstraction include the requirement for the City to submit an Irrigation Operating Strategy. These strategies are comprehensive and are legally binding on the licensee. The City has two Irrigating Operating Strategies endorsed by the Department of Water:

- (1) City of Cockburn Irrigation Operating Strategy April 2011–April 2014: covering GWL's 49535, 49549, 110703, 62672, 99188, 99722 and 49545. This is an amalgamated strategy to enable more efficient management groundwater.
- (2) Hammond Road Sporting Complex Irrigation Operating Strategy Sept 2011–Sept 2014: covering GWL 151 752. This single POS strategy was a requirement by the DoW due to the surrounding environment conditions.

3. ***The City adopts hydrozoning principles to Public Open Space***

Hydrozoning is the segregation of open space areas into categories based on water use and demand to enable the irrigation system to be designed for optimal water delivery. Hydrozoning of POS ensures key outcomes are achieved and enables varying water allocation depending on the park classification.

Hydrozoning for the City's reserve classifications are outlined below.

Hydrozone	Reserve Classification	Water Allocation
High	Sports Ovals & High Profile Regional Parks	9,100 kl/ha

Medium	Sports Oval surrounds, Neighbourhood and Local parks	6,500 kl/ha
Low	Streetscapes	3,000 kl/ha
Dry	Dry park	0 kl/ha

**4. *The City adopt a uniformity coefficient (CU) of >80% for all reticulated open space***

Optimum efficiency of water use is best achieved by ensuring water is very evenly distributed across each Hydrozone. Coefficient of Uniformity (CU) is a method of measuring how uniformly an irrigation system applies water, expressed as a percentage. The higher the number, the more uniform the rate of application. CU is determined by placing catch cups across a turf area and comparing the average precipitation reading and the deviation from the average. Adoption of a uniformity coefficient CU of > 80% will be in accordance with industry standards and achieve uniform irrigation watering applications

**5. *The City continues to implement optimum irrigating operation conditions***

Maintenance of irrigation systems at optimum operating condition is paramount to ensuring the supply and distribution of water in accordance with the individual system design specifications. Irrigation systems not maintained at optimum operating condition are inefficient and can lead to a number of issues. To facilitate the City's POS the minimum technical level of service are outlined below:

- Active Ovals – 52 services per annum
- Neighbourhood & Local POS – 26 services per annum
- Streetscapes & Landscapes–26 services per annum

**6. *The City adopts adaptive irrigation scheduling for all irrigation systems***

Adaptive Irrigation Scheduling is the process used by irrigation system managers to determine the correct frequency and duration of watering based on actuality. Understanding of evaporation and transpiration elements are key components to ensure irrigation frequencies and durations deliver the water required to turf or plant.

**7. *The City continues to monitor Groundwater Abstraction, Scheme Water Usage and standing groundwater on all production bores and report annually to the Department of Water***

Monitoring the volume of groundwater abstracted enables the continual comparison with allocations provided under the licence issued by the Department of Water. Flow meters are installed to the bore head with monthly monitoring of the water abstraction volumes which guide the next month's water allocation.

Monitoring production bores standing water levels enables the timely detection of changes in aquifer water levels and may indicate an adverse environmental impact. Early detection of these potential environmental changes will enable remedial action to minimise adverse outcomes.

An annual report is submitted to the DoW on groundwater usage and standing groundwater on all production bores.

**8. *The City undertakes a comprehensive review of suitable Central Control Systems and receives a report by December 2013 on the preferred Central Control System***

Central Control Systems enable real time information on water management at each individual site and collectively across the City. Central Controls Systems are a valuable management tool that integrates a complete system from one source and will provide instant feedback on the irrigation system in the field. A review of current central control systems is prudent to ensure integration with the City's irrigation infrastructure and IS network.

**9. *Investment in a Weather Station in line with the preferred Central Control System***

Weather stations are observation posts where weather conditions are monitored and recorded. Weather stations can be configured to record various environmental data such as rainfall, air temperature, wind speed, etc. The installation of a weather station will form a component of the specifications to be developed for a central control system.

**10. *The City continues to invest in Soil Monitoring Devices***

Soil monitoring devices allow the identification of moisture levels and leachates in the soil profile to improve the scheduling of irrigation and mitigate the potential risk of nutrients entering the



groundwater. Currently, Lysimeters are the only soil monitoring device used. Lysimeters collect the leachate passing through the turf root zone. Monitoring of the quantity and nutrient content of the leachate will ensure that the groundwater is not being compromised by the City's turf management practices. Soil moisture devices; determine water content in the soil profile, are proposed to be trialled at Success Reserve, Hammond Rd and evaluated over the next twelve months.

**11. *The City complete Milestone 4 of the ICLEI campaign and commence works to achieve Milestone 5***

The City of Cockburn is a participant of the International Council for Local Environmental Initiatives (ICLEI) Water Campaign which is a voluntary program which aims to assist in the local government reduce water consumption and improve water quality. The program involves progressing through five milestones, that guide participating councils through a process of local research, policy making, action planning, implementation and evaluation.

The City has just recently received the "Waterwise Council" status by the Water Corporation and Department of Water. This status is one component of Milestone 4 which is currently being completed.

**12. *The City continues to engage with the Department of Water on the "First in First Served" policy review***

The Department of Water (DoW) completed a review of the "First in First Served" (FIFS) Policy in October 2011 to ensure that it encourages the highest value use of water and address other contemporary issues. The FIFS approach is a well-established approach to managing multiple applications in many areas of government. This approach is appropriate where water is plentiful and little competition, however a reducing water resource with significant competition and alternative mechanism is required. The FIFS approach does not result in the best outcomes, as once the available water resource reaches full allocation it does not evaluate the applications concurrently and direct water to the highest priority.

The DoW has proposed the following alternatives for unallocated water; FIFS, Merit Selection, Auctions, Direct Sale and Ballots. The DoW proposes that once 70% of the water resource allocation has been reached, through the FIFS approach, market based allocations are used. Auctions are preferred because they directly address water scarcity by allowing the competing market water users to bid for the remaining limited resource.

City officers consider the proposed mechanism of FIFS to the 70% allocation limit acceptable; however the Merit selection approach should apply to the remaining 30% unallocated resource. This method would ensure a sustainable element is applied to all applications with a particular focus on future land use and the provision of POS.

The Department of Water have advised that a “position” on the FIFO policy has been determined; however with the recent change of Government Ministers no resolution has been enacted. It is recommended that the City continue to operate under the current FIFO policy and wait until further advice is received from the DoW.

**13. *The City reduces its groundwater abstraction levels to achieve the City’s Sustainability Action Plan 2013/14, Environment 5.1 KPI “To improve efficiency in corporate groundwater use by reducing consumption by 10 percent below the 207/08 DoW allocations per hectare by 2017/18”***

Our current license threshold for water usage presents the City with an arduous task of setting ambitious targets for the reduction in groundwater abstraction. The City needs to find a balance between amenity and functionality for our public open space whilst at the same time reducing our impact on the environment. The City is licenced to extract 7,500kilolitres of groundwater per hectare per year from the superficial aquifer. The Department of Water is currently reviewing the volumes for water abstraction, with an anticipated figure of 7,200 kilolitres per hectare per year being adopted in the near future. This 300kL reduction is considered as the first step in a series of water management mechanism to be introduced by the DoW and considered and achievable target with minimal impact on the existing landscape treatments.

The City’s current average groundwater extraction across all sites is below the proposed DoW allocation of 7,200 and is well on its way to achieving the City’s Sustainability Action Plan 2013/14 Environment 5.1 KPI “To improve efficiency in corporate groundwater use by reducing consumption by 10% below the 207/08 DoW allocations per hectare by 2017/18”. To ensure these objects are achieved the following targets for the next 5 years.

Reserve Classification	Water Allocation (kL/ha/per annum)					
	12/13	13/14	14/15	15/16	16/17	17/18
Sports Ovals & Regional Parks	9,100	9,100	9,000	9,000	9,000	9,000
Sports Oval surrounds, Entry Statements, Regional, Neighbourhood and Local parks	6,500	6,400	6,300	6,200	6,100	6,000
Low profile passive parks & median strips	3,000	2,900	2,800	2,700	2,600	2,500
Dry park	0	0	0	0	0	0

**14. *The City continues to monitor the Port Coogee Groundwater Interception resource and consider the availability of this resource for future strategies***

The Port Coogee Marina Development is influenced by an unusual set of circumstances, regarding groundwater usage. For this reason it is assessed separately from the City's overall irrigation watering strategy, in order that potential advantages can be leveraged from these circumstances.

The Port Coogee intercepts 7,400m<sup>3</sup>/day of nutrient rich water from entering the marina. This intercepted water is used for irrigation of the POS and streetscapes throughout the estate with the balance being reinjected into the aquifer through reinjection bores located north of the development. The intercepted water currently considered excess to the Port Coogee's requirements is being considered in the following strategies:

- Water Re-Use System – a secondary reticulation system within the development to be utilised by the property owners for their private lot. Although this system has been installed by the developer, the City has yet to commit to the application and is requiring a comprehensive business plan being submitted that addresses key issues.

- Cockburn Coast Structure Plan – an opportunity exists to divert the intercepted water for general reticulation of POS in this precinct. Further consideration of this strategy will need to be undertaken during the development phase of the Cockburn Coast in consultation with Department of Water.
- Golf Course Proposal - The Long Term Financial Plan 2012/13 -2022/23 lists the establishment of a 9 hole golf course on the Coogee Regional Open Space for 2019/20. Access to the intercepted water will be component of the business case to be presented to Council.

This intercepted water supply is a potential resource to facilitate future projects in proximity to the Port Coogee Development however further investigation regarding management and maintenance costs are warranted before the City capitalise on this resource.

#### **15. *The City continues to adopt the annual maintenance budgets***

The City's annual ongoing irrigation maintenance cost forms approximately 12% of the Parks Services annual operating budget of \$1,183,000 for the 2012/2013 Financial Year. Future growth will realise an additional 100 Ha of POS in the next 10 years which will require additional funding through annual submissions to Council and the appointment of two (2) irrigation fitters. The City's Workforce Plan 2012 – 2017 lists irrigation fitter in 2014/15 and 2017/18.

In addition to the annual operating maintenance, water licencing and usage charges are being considered by the Economic Regulator on a "user pay" cost recovery mechanism. The Government has yet to make decision regarding these charges and will subject to a further report once the City has been informed of their direction.

#### **16. *The City will implement the irrigation asset renewal program outlined in the Parks & Environment Asset Management Plan and annual budget submissions***

The Parks & Environments Asset Management Plan 2013 has the value of the City's irrigation assets at \$18,597,363. There is currently \$1.06m of irrigation assets that are considered to be past their projected renewal date and form the basis of the 10 year renewal plan. The 10 year cumulative funding gap for irrigation infrastructure is \$3,937,547.

The Parks & Environments Asset Management Plan 2013 is included with the Long Term Financial Plan 2012/13 – 2021/22

which identifies an increase in renewal funds over the 10 years to mitigate the funding gap.

**17. *The City will review the plan and report on performance against targets through an annual report. The next report will be September 2014***

The City commits to reviewing the strategy, its action plan, funding requirements, changes in legislation and reporting on performance against targets through the preparation of an annual report. The annual report will be compiled following the completion of the water year (July to June) and will incorporate the City's annual reports to the Department of Water.

The Water Conservation Plan 2013 – 2018 demonstrates the City's commitment to a proactive and better managed water resource through sound policies and guidelines.

### **Strategic Plan/Policy Implications**

#### **Infrastructure**

- Community infrastructure that is well planned, managed, safe, functional, sustainable and aesthetically pleasing.

#### **Environment & Sustainability**

- A community that uses resources in a sustainable manner.
- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

#### **Budget/Financial Implications**

N/A

#### **Legal Implications**

N/A

#### **Community Consultation**

N/A

#### **Attachment(s)**

Water Conservation Plan 2013-2018

**Advice to Proponent(s)/Submissioners**

N/A

**Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

**16.4 (OCM 09/05/2013) - TEMPORARY CLOSURE OF 14 PARKING BAYS AND PEDESTRIAN PATH ON THE SITE OF NO. 37 (LOT 786) ORSINO BOULEVARD NORTH COOGEE (6012859) (J KIURSKI) (ATTACH)**

**RECOMMENDATION**

That Council, in accordance with Section 3.50 of the Local Government Act 1995, institutes a temporary closure for the land incorporating 14 parking bays and pedestrian path on site of No. 37 (Lot 786) Orsino Boulevard North Coogee, subject to:

1. There being no substantial objection received as a result of advertising in accordance with Section 3.50 of the Local Government Act 1995 in the local newspaper.
2. There being no substantial objection from service authorities, emergency services or adjoining owners.
3. The preparation and execution of an appropriate Deed of Agreement for the use of the land for a period of 18 months commencing May 2013 to December 2014 at a fee to be determined by the City's Licensed Valuer. The land is to include the portion of land which involves the 14 car bays, the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard and the portion of Reserve 50980 which is currently being used for the storage of the site offices and signage. All costs associated with this arrangement are to borne by the applicant.
4. The developer engaging an appropriately accredited traffic management contractor to submit a certified traffic management plan to monitor and control traffic movement due to the closure.
5. The developer will construct a temporary car park on Lot 791 Orsino Boulevard and make it available until the completion of the construction works on Lot 786.
6. The developer will install temporary perimeter fencing to the rear of the car parking bays to Socrates Parade, Napoleon

Parade and Orsino Boulevard as detailed on the site fencing plan. The fence be positioned and of a height and form of construction that does not create a traffic hazard for motorists, cyclists and pedestrians, including mobility impaired persons, and including not blocking currently available lines of sight at intersections.

7. All works on existing City infrastructure (roads, footpaths, drainage, parks or verges) completed and reinstated in accordance with the "Public Utilities Code of Practice 2000", "Restoration and Reinstatement Specification for Local Government 2002" and the City of Cockburn "Excavation Reinstatement Standards 2002" as a minimum.
8. The developer to provide a bond of \$100,000 to offset any damage to the City's infrastructure prior to the closure of any parking bays and the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard.
9. The proponent being fully responsible for all legal costs, the cost of the valuation, public liability and damages arising from the works.

## COUNCIL DECISION

### Background

No. 37 (Lot 786) Orsino Boulevard North Coogee development was granted planning approval and building licence for 101 multiple dwellings and it is to commence construction work. The development is being undertaken by Lost Wave Pty Ltd and Diploma Construction Pty Ltd is appointed builder for construction work.

The proposed development of the Ocean Edge apartment complex in Orsino Boulevard North Coogee is surrounded by Socrates Parade, Napoleon Parade, Orsino Boulevard and a public open area on the south side of Lot 786.

## **Submission**

Diploma Construction Pty Ltd, the developer's appointed building contractor, has requested Council implement procedures to temporarily close 14 parking bays and the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard for a period of up to 18 months during the construction of the 101 multiple dwellings on Lot 786 Orsino Boulevard, North Coogee.

## **Report**

During the construction activities of (Lot 786) 37 Orsino Boulevard, North Coogee the parking bays and footpath and closure can be supported for the below reasons:

The 14 parking bays and the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard abutting the worksite will remain closed until the completion of works and the appropriate signage installed to direct pedestrians to the other side of the roads. Diploma Construction will maintain the footpath area and will make good any damage caused by construction vehicles on completion of the project. The footpath closure will have minimum impact on pedestrian movements as pedestrians will be able to use the existing footpath or grass area on the other side of the roads.

Diploma Construction will make available the temporary parking bays to the Australand's Lot 791 Orsino Boulevard available until the completion of construction works on Lot 786.

Diploma Construction has appointed a certified traffic management contractor (Carringtons Traffic Services) to monitor the impact of the footpath closure and access arrangement for the site. Carringtons Traffic Services has already submitted a traffic management plan, which is in line with Australian Standards and Main Roads field guidelines.

The proposal is for eighteen (18) months period and with appropriate traffic management controls in place, including road barriers, signage and protective surfaces covering public footpath and parking area. The closure will not create any undue congestion and impact on the surrounding land uses. Advance warning signs will also be installed and advice of the proposed closure will be placed in both the local newspaper and West Australian newspaper prior to the closure.

Diploma Construction will install temporary perimeter fencing to the rear of car parking bays to Socrates Parade, Napoleon Parade and Orsino Boulevard as detailed on the site fencing plan. The temporary fence will be a mesh panel fencing system and the remainder of the site



will be surrounded by a combination of a solid and mesh fencing system.

The fence be positioned and of a height and form of construction that does not create a traffic hazard for motorists, cyclists and pedestrians, including mobility impaired persons, and including not blocking currently available lines of sight at intersections:

- no clearing of existing native vegetation or ground levelling of the verge;
- no encumbrance to us maintaining the remaining width of our verge;
- suitably maintaining the fence and enclosed verge, e.g. removal of litter collected against it and keeping weeds mowed;
- the fence being kept in a neat, tidy and safe condition, and not be used for the fixing of any advertising signs, banners or similar;
- the fence be removed and the verge made good to our satisfaction at the completion of the building;
- developer/builder having to locate all services within the road reserve prior to any works commencing;
- developer/builder providing indemnity insurance in regard to working within the roadway;

Positioning fence within road reserve is subject to Council approval of a temporary closure of a 14 parking bays and the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard.

The closure of the traffic lines of any roads adjusting to Lot 786 Orsino Boulevard is not part of this report and not going to be supported; only the partial closure for short time during a day for delivery of large construction items.

All works on existing City infrastructure (roads, footpaths, drainage, parks or verges) completed and reinstated in accordance with the "Public Utilities Code of Practice 2000", "Restoration and Reinstatement Specification for Local Government 2002" and the City of Cockburn "Excavation Reinstatement Standards 2002" as a minimum.

Diploma Construction agreed to pay an amount of \$100,000 to any damage to the City's infrastructure prior to the closure of any parking bays and the pedestrian path along Socrates Parade, Napoleon Parade and Orsino Boulevard.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Moving Around**

- A safe and efficient transport system.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

All costs to the closure will be covered by the Diploma Construction.

### **Legal Implications**

Section 3.50 of the Local Government Act.

### **Community Consultation**

To be advertised in a local newspaper and service authorities, emergency services and adjoining owners advised.

### **Attachment(s)**

- 1 Site Fencing Plan
2. Traffic Management Plan

### **Advice to Proponent(s)/Submissioners**

Diploma Constructions have been advised that the matter will be considered by Council at the 9 May 2013 Council Meeting

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.5 (OCM 09/05/2013) - SUSTAINABILITY ACTION PLAN REVIEW 2012 - 2016 (HS/E/003) (H JESTRIBEK) (ATTACH)**

#### **RECOMMENDATION**

That Council adopt the amended Sustainability Action Plan 2013 – 2014.

## COUNCIL DECISION

### Background

In June 2012, Council adopted the City's Sustainability Action Plan 2012 – 2016, with a commitment to an annual review. This Action Plan is aligned with the City's Sustainability Policy (SC37) and Strategy 2012 - 2016. In November 2012, the City adopted its Strategic Community Plan 2012 – 2022.

The Action Plan is the City's blueprint for action towards sustainability and culminates in the release of a *State of Sustainability (SoS) Report* in November each year.

The Action Plan is reviewed by the City's sustainability officer in conjunction with the Executive and Strategic Business Management Group.

### Submission

N/A

### Report

The Focus Areas of the Action Plan have been amended to align with the City's Strategic Community Plan and Sustainability Strategy.

This has resulted in the amendment of overarching objectives and key performance indicators. This Action Plan presents an increasingly balanced reporting system for the City to pursue for sustainability. Each of the overarching objectives have been assigned four key performance indicators, to ensure a balanced system of reporting, which reflects an intent to pay equal attention to each focus area.

Many of the key performance indicators found in the 2012 report have been completed and have been removed accordingly. Those indicators where progress has been made, but are yet to be completed, have remained in the Action Plan for completion in the next iteration of the SoS Report.

Those indicators that are not strategically aligned with the updated Action Plan have been revised or removed.

This Action Plan will be revised annually, and be relevant to each financial year.

### **Strategic Plan/Policy Implications**

#### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

#### **Leading & Listening**

- A responsive, accountable and sustainable organisation.

#### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

### **Budget/Financial Implications**

N/A

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

Sustainability Action Plan

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.6 (OCM 09/05/2013) - REVISED SUSTAINABILITY STRATEGY (HS/E/003) (H JESTRIBEK) (ATTACH)**

### **RECOMMENDATION**

That Council adopt the amended Sustainability Strategy 2013 – 2017.

<b>COUNCIL DECISION</b>
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### **Background**

In March 2012, Council adopted the City's Sustainability Strategy 2012–2016. The strategy is aligned with the City's Sustainability Policy (SC37) and Action Plan. In November 2012, the City adopted its Strategic Community Plan 2012–2022. The strategy has been reviewed to align with the Strategic Community Plan and its term is in alignment with the mid-term review of this Plan.

The Strategy is a succinct overview of the City's focus areas for sustainability.

In order to ensure alignment, this document has been reviewed to ensure that the City's intentions across its strategic plans are complementary.

As the City progresses towards sustainability, it is envisaged there will be greater alignment between all City processes, policies, strategies and reports.

### **Submission**

N/A

### **Report**

The wording of the Sustainability Strategy has been reviewed to ensure that its alignment with other strategic documents is up to date and complementary.

In addition to this, the City's focus areas for sustainability have been amended where necessary to strengthen strategic alignment. This will ensure that the City moves forward in a consistent manner when strategically planning.

The City's Action Plan is also amended accordingly to reflect these changes.

## **Strategic Plan/Policy Implications**

### **Growing City**

- To grow our City in a sustainable way by: using land efficiently, protecting the natural environment and conserving biodiversity.

### **Leading & Listening**

- A responsive, accountable and sustainable organisation.

### **Environment & Sustainability**

- To protect, manage and enhance our natural environment, open spaces and coastal landscapes.

## **Budget/Financial Implications**

N/A

## **Legal Implications**

N/A

## **Community Consultation**

N/A

## **Attachment(s)**

Sustainability Strategy 2013-2017

## **Advice to Proponent(s)/Submissioners**

## **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **16.7 (OCM 09/05/2013) - CAT BUS SERVICE TO SOUTH BEACH VILLAGE (142/007) (D VICKERY) (ATTACH)**

### **RECOMMENDATION**

That Council

- (1) note that an extension of the Fremantle CAT bus service into the City of Cockburn section of South Beach Village is not feasible at this time; and
- (2) forward this Report to the City of Fremantle and the PTA for their information.

## COUNCIL DECISION

### Background

At the Ordinary Council Meeting of 13<sup>th</sup> October 2012, Mayor Logan Howlett requested as follows:

*“That a report be presented at a future Council Meeting on the opportunity to extend the Cat Bus service that operates in the City of Fremantle, south to include the South Beach Village. Transport orientated developments require the provision of enhanced public transport options to encourage people to change their commuting habits.”*

The current Fremantle Blue CAT bus service operates down to Douro Road only. The South Beach Village development currently has no public transport service.

Extending the Fremantle CAT bus service down to the upper section of the South Beach Village development, and possibly the provision of a Transperth service through its eastern side, were both proposed in the development's Structure Plan Report, but neither has as yet been enacted by the City of Fremantle, City of Cockburn or Transperth / PTA.

There has been some history of verge parking and road obstruction issues associated with the South Beach Village development. Contributing is the lack of suitable car parking places on street or within private property for the number of vehicles there and the relatively narrow roadways through the development.

### Submission

N/A

### Report

The South Beach Village development is situated in North Coogee abutting and extending over the northern boundary of the City of Cockburn into the City of Fremantle. Its western boundary is the South Beach foreshore reserve, southern boundary is Rollinson Road, and its eastern boundary is also the boundary with the City of Fremantle and the land occupied by the Fremantle Holiday Village.

As can be seen from the aerial image below, a fair number of the residences have now been constructed, particularly in the more northern and eastern sections of the development.

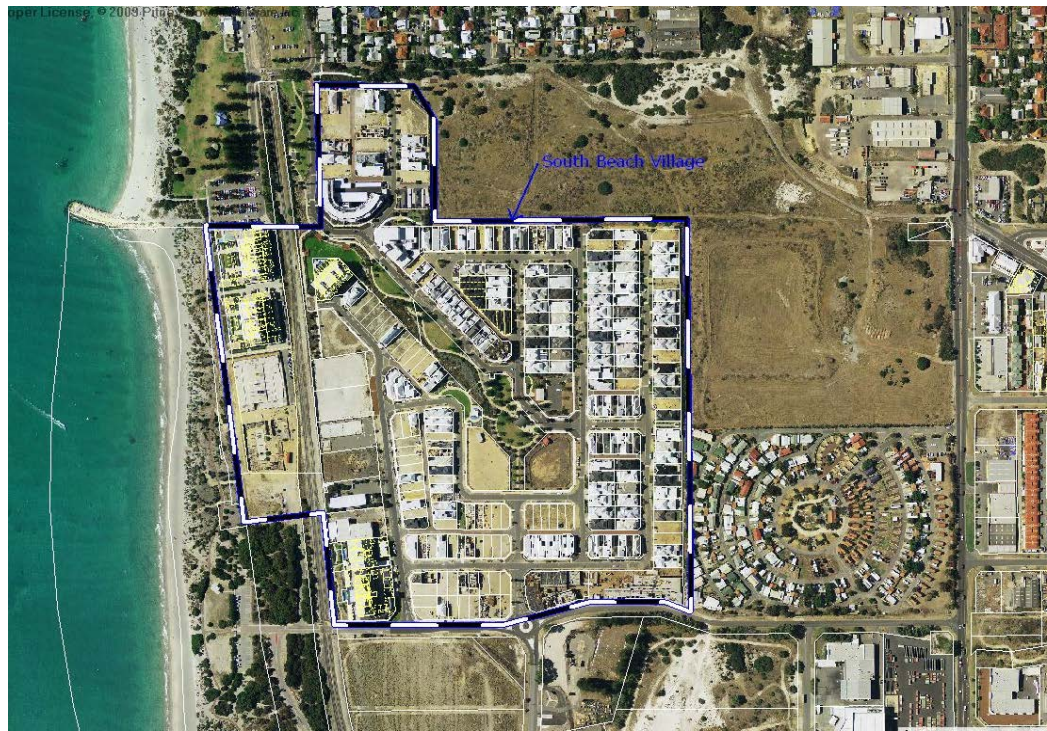


Figure 1: Aerial View of the South Fremantle Village Development

### Existing Bus Routes

The Transperth bus routes and the bus stops on those routes, in vicinity of South Beach Village are as represented on Figure 2 and Figure 3 below. The closest routes are Route 825 travelling up/down Cockburn Road and Route 532 utilising Douro Road.



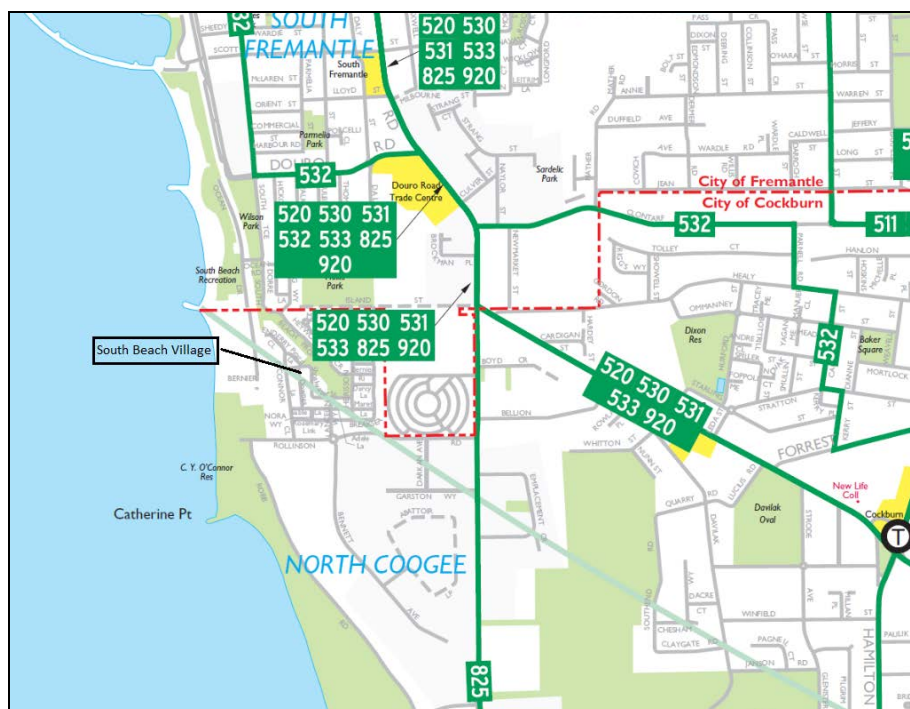


Figure 2: Current Transperth Bus Routes

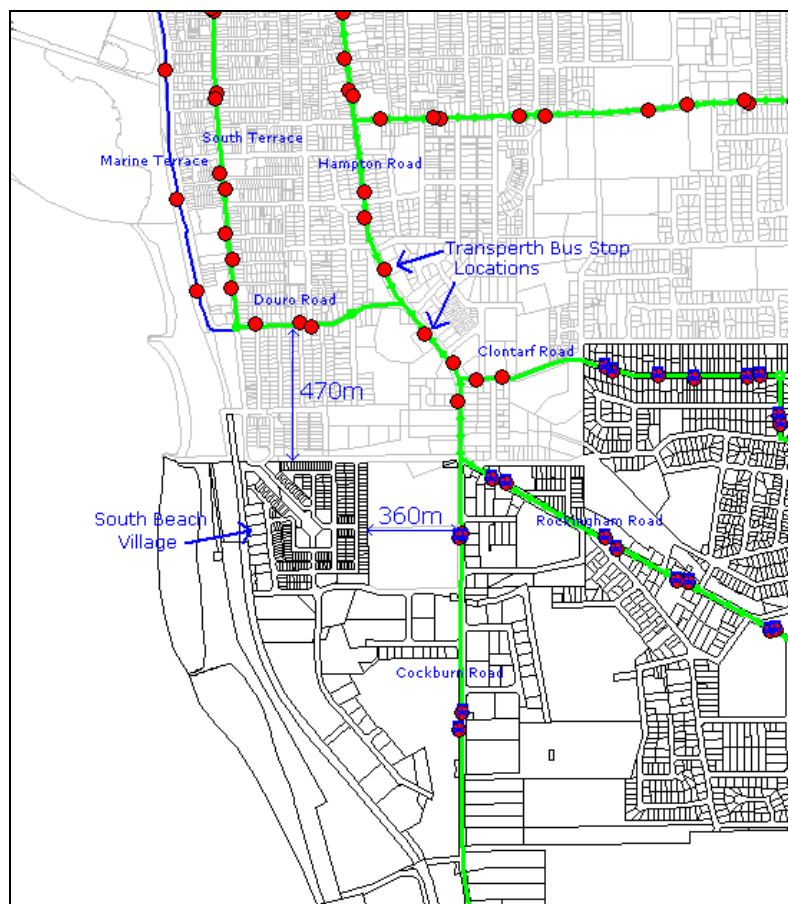


Figure 3: Transperth Bus Stop Locations

The Fremantle Blue CAT bus service route is represented on Figure 4 below (being an extract of the CAT 204 Timetable leaflet). This bus service operates on a 10 minute frequency circular route south down South Terrace to Douro Road and returning northward along Marine Terrace, The return route with stops at the south end of Marine Terrace is also represented on Figure 4 below.

The operating times of this free Blue CAT service are as follows:

Monday to Thursday: ..... 7:30am to 6:30pm

Fridays: ..... 7:30am to 8:00pm

Weekends & Public Holidays: ..... 10:30am to 6:30pm



Figure 4: City of Fremantle CAT Bus Routes

### Future Bus Routes

There is a proposed future Light Rail or Bus Rapid Transit (LRT/BRT) service between Fremantle and the Cockburn Coast Development being planned for the area to the south of Rollinson Road and the South Beach Village. This LRT/BRT route, and the proposed stops on it, is represented on the map at Figure 5 below.

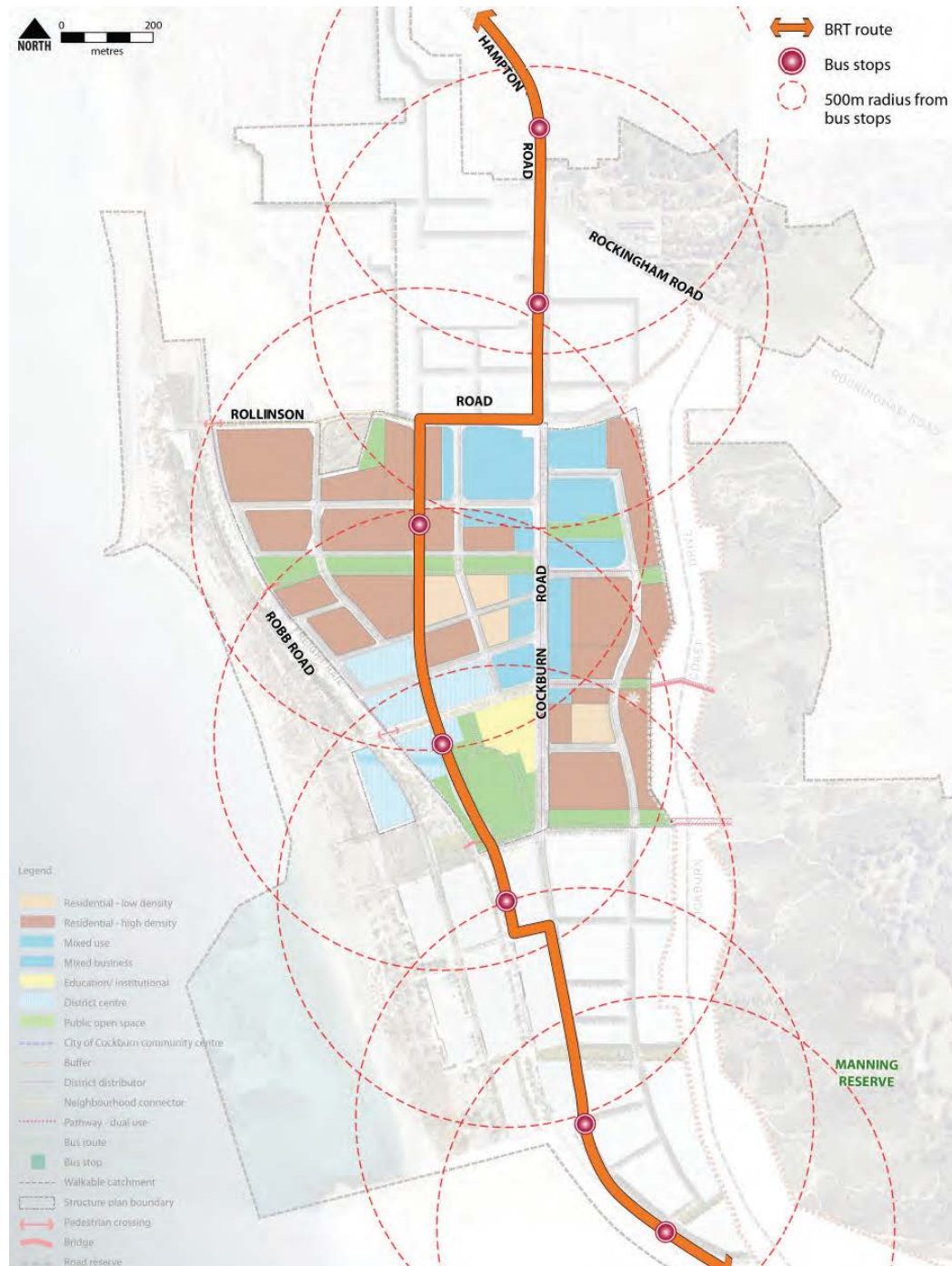


Figure 5

Proposed BRT Route Associated with the Cockburn Coast Development  
(source draft Robb Street Jetty Local Structure Plan)

### South Beach Structure Plan Proposed Bus Routes

The South Beach Structure Plan Report prepared in September 2002 and subsequently endorsed by both the City of Cockburn and

presumably the City of Fremantle, has as a major theme the provision of bus services into the proposed development. This Report (in Section 6.2.2) proposes the provision of an additional CAT bus into the Fremantle CAT bus fleet to enable the CAT bus route to be extended south to a loop through the north (City of Fremantle) section of the South Beach Village as represented on Figure 6 below (the Report Figure 5).

Additionally the Report (as represented in Figure 6 below) proposes for consideration a Transperth bus route running up the eastern portion of the South Beach Village development, via a connection with Rollinson Road and a 'Island Street' connection across to Cockburn Road.

The development appears not to have been planned and constructed with public transport and particularly these two routes in mind, it being noted that as built:

- a) The specific road linkages needed to facilitate a CAT bus circuit / turnaround, and/or a Transperth through route, as contemplated in the Structure Plan Report, have not been constructed, and in the case of the through bus route could not now be constructed due to the subdivision configuration.
- b) The roads and verges within the development are comparatively narrow, which together with the geometry including intersections, makes it not very conducive to the passage of larger vehicles such as buses, nor the placement of embayment's for passenger drop off and pick up.
- c) Any transit through or around the development could be described as somewhat tortuous, with indirect routes needing to be taken to get from one side of the development to the other.
- d) The development is situated away from the existing bus routes of the area, which utilise Cockburn Road, Rockingham Road, Hampton Road and Douro Road.
- e) The size and nature of the development, in regard to the size of the resident population, makes it a small catchment area for any service. The Structure Plan indicates that there will be, when fully taken up, 300 single residential lots and 22 grouped residential lots within the development. It could be considered that the resident and visitor population catchment is not large enough to justify a bus route being diverted through the area.



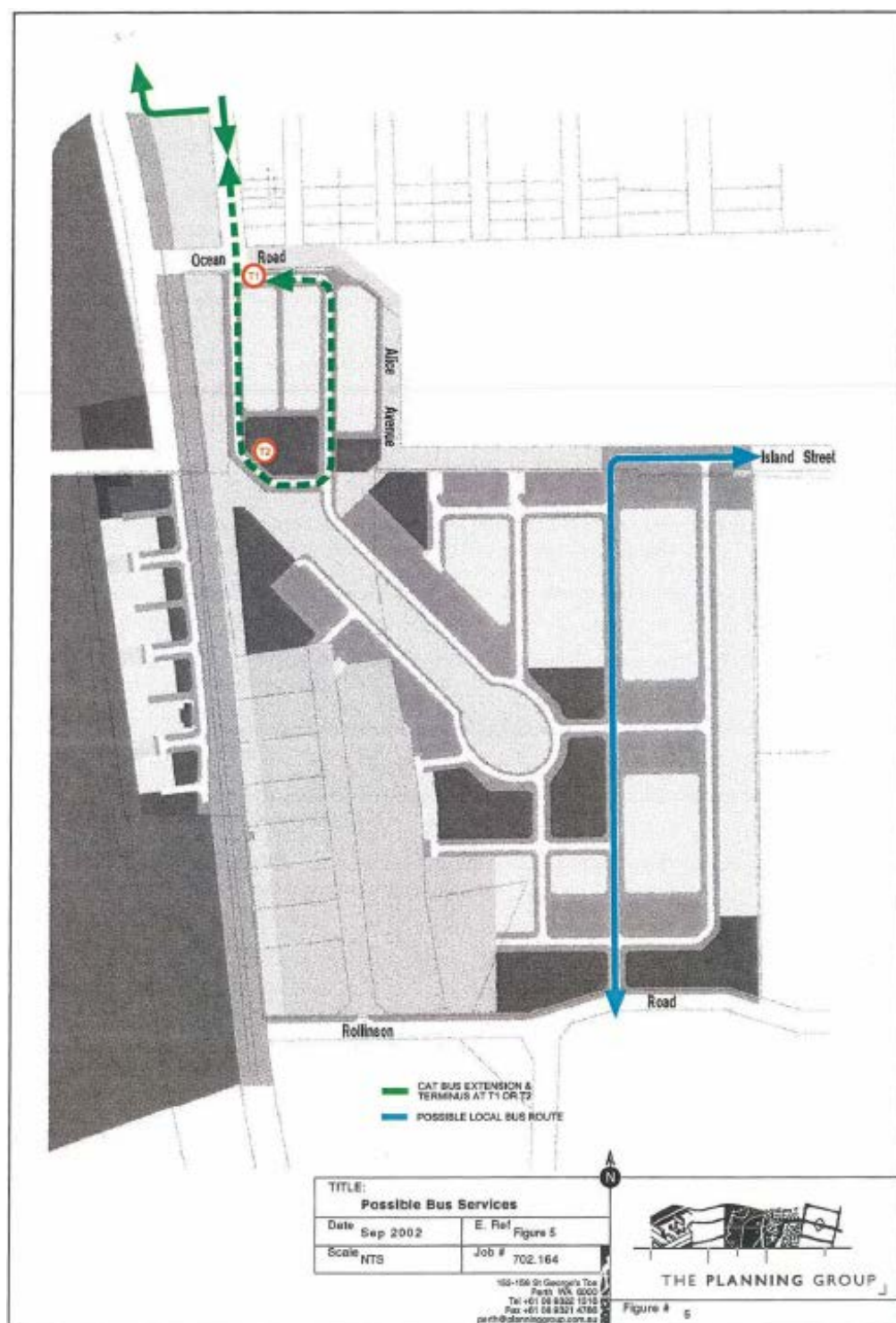


Figure 6: Extract of the South Beach Structure Plan Report  
– Proposed Bus Services

### Acceptable Walking Distances

As dimensioned on Figure 3, the South Beach Village development is some distance from existing Transperth bus routes and stops on those

routes. The closest residences in the City of Cockburn section are approximately 470 metres from the nearest stop on the No. 532 bus route travelling down Douro Road, and 360 metres from the nearest bus stop on the No.825 route travelling down Cockburn Road. For residents at the other (south and west) ends of the development the longest distance to the Douro Road stop as scaled is approximately 920 metres, and potentially up to 1100 metres if traversing out to the Cockburn Road bus stop via Rollinson Road.

For the proposed future LRT/BRT service, if constructed as per this plan, it appears that there will be a stop that is a comfortable walking distance for residents situated towards the southern end of the South Beach Village.

According to the PTA's Network and Systems Planner Simon Cox (the officer concerned with the review and provision of new bus routes), the targeted maximum distance to a Transperth bus stop is 500 metres and to a high frequency Light Rail Transit or Bus Rapid Transit (LRT/BRT) stop up to as much as 1000 metres.

Thus in all, residents living toward the northern and eastern perimeter of the South Beach Village development are currently within the Transperth target of less than 500 metres walking distance to a bus stop, and residents toward the southern end of South Beach Village will potentially in due course be situated within walking distance of the LRT/BRT service associated with the Cockburn Coast development.

However the distance from the nearest Blue CAT bus stop to the northern most residence within the South Beach Village within the City of Fremantle's portion is approximately 460 metres and within the City of Cockburn's portion is approximately 720 metres, thus is well beyond Transperth's target maximum walking distance for all of the development.

#### CAT Bus Extension

Currently the development, as built, would not physically accommodate the Structure Plan Report proposed CAT bus extension and circuit through the upper section of the South Beach Village, due to there being no road linkage at the northern end of the circuit within the development.

A new 'left turn entry only' off South Beach Promenade into Keeling Way could conceivably be constructed however, as represented on Figure 7 below, which would facilitate a clockwise (not anti- clockwise as contemplated in the Structure Plan) circuit for the CAT bus through this northern section.

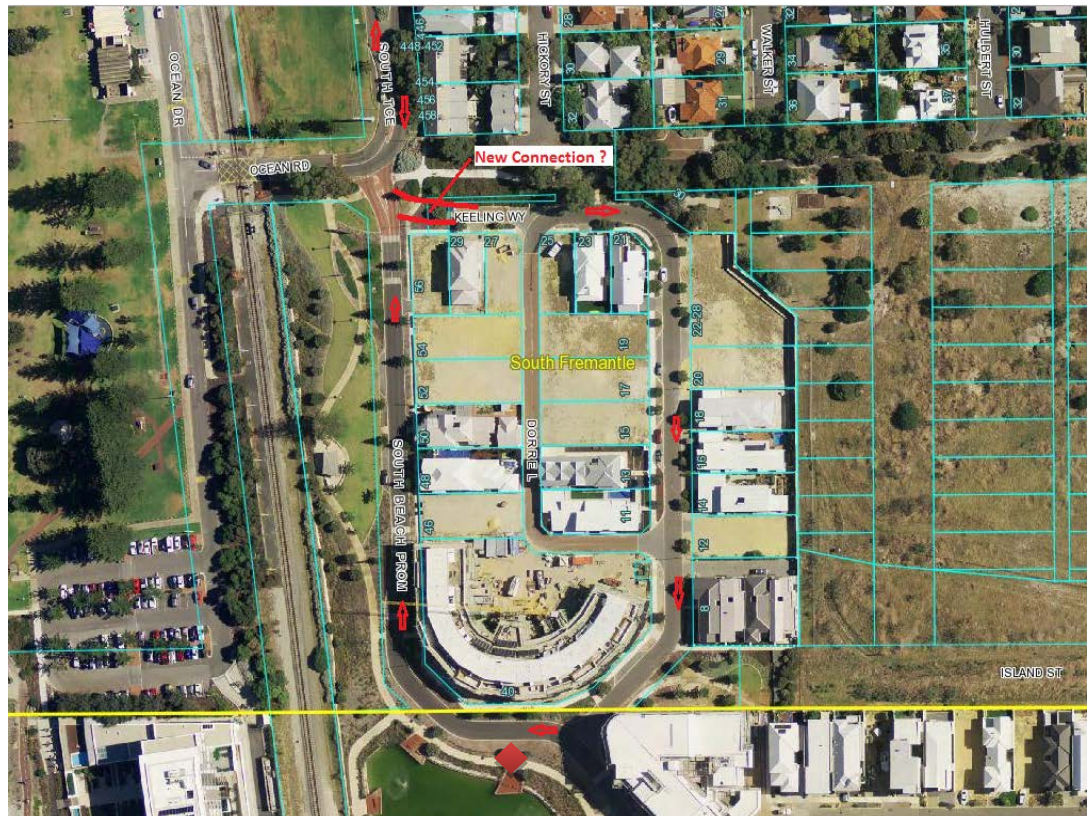


Figure 7: Possible CAT Bus Route Including New Road Connection

Being situated within the City of Fremantle's section of the South Beach Village development, it is that City that we can expect would need to be supportive of the new connection, to liaise with the property owners that would be affected by the proposed CAT bus route, to facilitate its construction and to deal with any arising issues to do with noise, disruption and/or traffic congestion on these streets not currently experiencing bus traffic. It is suggested that the road modifications alone to facilitate the passage of the bus and provision of bus stops could be in the order of \$240,000.

A recent inquiry to Transperth's Simon Cox has provided indicative current day costs for a CAT bus service of \$550,000 capital cost for the additional bus, \$55,000 per year base operating cost and an additional \$7.50 to \$8.00 per km running costs. However he also indicated that Transperth would be unlikely to support an extension of the existing Blue CAT bus route, or contribute funding toward it, on account of:

- (a) The existing blue CAT bus route is a circular route currently (rather than buses travelling in both directions on one route) and so any extension will add to the travel time for current users, perhaps unreasonably.

- (b) That extending the CAT bus route without adding another bus would potentially push the frequency of the service out to at least 18 minutes, which would be unacceptable.
- (c) Transperth funding for existing, new and extended routes has been fully allocated for the next three or more years, for other what would be higher priority, routes.

A separate inquiry to the City of Fremantle's Traffic and Design Officer, Dwight Kostusnic, indicated that whilst he considers it would be good to service the South Beach Village area with public transport, he would not want it to compromise the existing Blue CAT bus service in respect to frequency or travel times, which he considers are currently at their limits. He also noted that in his view any additional or extended CAT bus service to service the South Beach Village area would need to be at the City of Cockburn's cost, that the City of Fremantle would not be in a position to co fund it.

There are currently three funding models for CAT services within the Metropolitan Area, these being:

- (a) Levies on parking, as for the City of Perth which funds \$11m for the Perth CAT services and the free transit zone for conventional Transperth bus passengers.
- (b) Developer funded, as for the Midland Gateway link bus to/from the Midland train station and the Glendalough – Herdsman Park link.
- (c) Local Government / PTA / Other Entity co funded, such as:
  - (i) Fremantle and Joondalup CAT buses, with the LGA providing 60% of funding to meet service, a significant portion of which is drawn from parking 'profits' and some from general revenue stream, and 40% or so from PTA – gradually declining year on year.
  - (ii) The Subiaco Link, which has 4 contributors (QE11, UWA, PTA & Co Subiaco).

An alternative to a CAT bus service additional to existing Transperth services is a Transperth provided bus with passengers paying fares and a Local Authority contributing to its capital and running costs. The costs can be expected to be similar to a CAT bus (\$550,000 capital cost and \$55,000 per year base operating cost but with a reduced running cost to around \$5/km instead of \$7.50 to \$8.00/km.

### Bus Demand

To date (as far as the author is aware) there has been no petition from land owners or residents of the South Beach Village seeking the



provision of public transport through or up to the perimeter of the estate within the City of Cockburn; nor has there been any surveys of landowners and residents undertaken to assess the potential demand.

The most recent census figures for 'trip to work' mode of transport for the respondents in this general area indicated the following statistics in Table 1.

Travel to work, top responses	5115802	%	Western Australia	%	Australia	%
<i>Employed people aged 15 years and over</i>						
Car, as driver	120	57.1	662,948	60.4	6,059,972	60.2
Walked only	10	4.8	35,995	3.3	377,043	3.7
Car, as passenger	7	3.3	63,485	5.8	537,638	5.3
Train	6	2.9	24,271	2.2	388,012	3.9
Bicycle	6	2.9	11,757	1.1	103,914	1.0
People who travelled to work by public transport	25	11.9	102,895	9.4	1,046,721	10.4
People who travelled to work by car as driver or passenger	129	61.4	729,050	66.4	6,620,840	65.8

Table 1 – Travel to Work Mode Statistics

From the statistics it can be concluded that approximately 6% of respondents travelled to work by bus as compared to approximately 61% travelled to work by car.

The inference is that only a relatively small percentage, perhaps less than 10%, of the residents of the South Beach Village would make regular use of a free CAT bus and even less a standard Transperth bus service for daily commute purposes, if either were provided. The relatively small catchment would indicate a dedicated Transperth bus route to/from the area wouldn't be justified, nor necessarily a redirection of an existing Transperth bus away from the core direct routes along Cockburn, Rockingham, Hampton and Douro Roads.

It should be noted too however that there are commercial premises at the South Beach foreshore, and the popular beach area itself each side of the Islands Street groyne, a proportion of the visitors to which would

presumably utilise an extended CAT service or redirected Transperth service to get to and from.

### **Conclusions**

The following conclusions have been drawn:

- 1) The existing road layout of the South Beach Village development is not conducive to the passage of a full size bus on a regular circuit, and even a circuit via Keeling Way within the City of Fremantle section would require roadworks and a new road linkage to accommodate it.
- 2) Extending the blue CAT bus route would require at least one additional CAT bus being acquired and put into the fleet to ensure frequency of the service is not compromised, however the travel time would be increased also by up to say 8 minutes, which may adversely affect the whole services patronage and viability as far as the City of Fremantle and/or Transperth is concerned.
- 3) The City of Fremantle and Transperth/PTA are both unlikely to be inclined to contribute to the capital or running costs of an extended service into South Beach Village, or the cost of roadworks and other bus stop facilities needed to accommodate it.
- 4) Sections of the South Beach Village in the City of Cockburn are within an 'acceptable' walking distance to an existing Transperth bus service and further sections can be expected to be within an acceptable walking distance to a future BRT service running to from the Cockburn Coast area. There are however sections that are outside of a reasonable walking distance.
- 5) The likely patronage of any servicing of South Beach Village with an extended CAT bus route, or fare paying Transperth bus service, were either one to be provided, is unknown.
- 6) Overall, it does not appear to be feasible to extend the Fremantle CAT bus route to include any portion of the South Beach Village development within the City of Cockburn.
- 7) Given budgetary constraints, it is not recommended that the City offer to contribute to the cost of any extension of the CAT bus service into the upper City of Fremantle section of the South Beach Village development, be it for the capital or running costs of the bus or any necessary roadworks and facilities, were such to be contemplated by the City of Fremantle or Transperth/PTA.

It is suggested that this Report be provided to the City of Fremantle and officers from the Public Transport Authority, for their information on the findings.

Also that it be the stated intent that the BRT/LRT being contemplated as part of the Cockburn Coast development also service parts of South Beach Village, by way of positioning stops within a reasonable walking distance.

### **Strategic Plan/Policy Implications**

#### **Moving Around**

- An integrated transport system which balances environmental impacts and community needs.
- Facilitate and promote healthy transport opportunities.
- Infrastructure that supports the uptake of public transport and pedestrian movement.

### **Budget/Financial Implications**

Nil

### **Legal Implications**

N/A

### **Community Consultation**

N/A

### **Attachment(s)**

1. South Beach Structure Plan Report
2. Fremantle CAT bus Route Map / Timetable

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## 17. COMMUNITY SERVICES DIVISION ISSUES

### 17.1 (OCM 09/05/2013) - TENDER NO.RFT01/2013 - PROJECT MANAGEMENT SERVICES - REGIONAL AQUATIC AND RECREATION COMMUNITY FACILITY (CCW) (RFT 01/2013) (S DOWNING) (ATTACH)

#### **RECOMMENDATION**

That Council accept Tender No.RFT01/2013 – Project Management Services – Regional Aquatic and Recreation Community Facility at Cockburn Central West from NS Projects Pty Ltd for a period of four(4) years; in accordance with the price submitted in the confidential attachments.

#### **COUNCIL DECISION**

#### **Background**

The City of Cockburn's Regional Aquatic and Recreation Community Facility at Cockburn Central West is currently in the process of seeking public comment on the Business Plan. In addition funding applications have been submitted to both State and Federal Government Funding agencies. Part of the Business Plan and Funding Applications is a time line in order to construct the proposed facility. In order to meet the timeframe outlined above, a number of tenders are required to be adopted by Council these include the appointment of a suitably qualified Project Manager. This tender is subject to the Council adopting the Business Plan and approving the CCW Project. All tenderers have submitted responses based on the construction of the Integrated Facility including Cockburn and the Fremantle Football Club or the Non-Integrated Facility for the City of Cockburn only.

Tender Number RFT 01/2013 Project Management Services for CCW was advertised on Wednesday, 20 February 2013 in the Local Government Tenders section of "The West Australian" newspaper.

The tender was also displayed on the City's e-Tendering website.

## Submission

Tenders were called for Project Management Services at CCW for a four (4) year period and closed at 2:00p.m. (AWST) on Thursday 14 March 2013. Twelve (12) tender submissions were received from:

1. Davis Langdon Australia Pty Ltd (AECOM)
2. Appian Group Pty Ltd - ATF Appian Group Trust - T/As: Appian Group Pty Ltd
3. Aurora Projects Pty Ltd
4. Benchmark Projects Australasia
5. Blue Visions Management Pty Ltd
6. Coffey Projects (Australia) Pty Ltd
7. GHD Pty Ltd
8. Insight Project Services Pty Ltd
9. International Commercial & Project Services Pty Ltd - T/As: ICP Solutions
10. NS Projects Pty Ltd
11. Savills Project Management Pty Ltd
12. Thinc - T/As: Thinc Projects Australia Pty Ltd

## Report

### (a) Compliance Criteria

	Compliance Criteria
(a)	Compliance with the Specification contained in the Request.
(b)	Compliance with the Conditions of Tendering this Request
(c)	Compliance with Fixed Price and completion of Clause 3.4.2A
(d)	Compliance with Insurance Requirements and completion of Clause 3.2.7.
(e)	Compliance with Occupational Safety & Health Requirements and completion of Appendix A.
(f)	Compliance with ACCC Requirements and completion of Appendix B.
(g)	Compliance with and completion of the Price Schedule, in the format provided in this Request in Part 4.
(h)	Compliance with Subcontractors (Proposed) and completion of Clause 3.5

### (b) Compliant Tenderers

All twelve (12) Tender submissions were deemed compliant.

### (c) Evaluation Criteria

Tenderers were assessed against the following criteria:

Evaluation Criteria	Weighting Percentage
Demonstrated Experience	30%
Key Personnel Skills and Experience	25%
Tenderer's Resources	25%
Tendered Price – Estimated Lump Sum Contract Value	20%
<b>TOTAL</b>	<b>100%</b>

(d) Tender Intent/ Requirements

The City of Cockburn (the Principal) in conjunction with the Fremantle Football Club (FFC) are seeking the services of an independent (i.e. not associated with any design consultants or construction contractors), qualified and experienced Project Manager/Consultant to undertake project management services for the development of the Cockburn Integrated Regional Physical Activity and Education Centre located at Cockburn Central West, Western Australia.

The Project at this stage is expected to deliver an integrated community facility that includes the Principal's community aquatic and recreation and the FFC's elite training and administration facilities. The current estimated cost of the integrated Centre (including construction, associated works and fit-out costs, consultants' fees, contingencies and other costs associated with the development) is \$107 million GST Exclusive; and is dependent on final stakeholder involvement and scope.

Tenderers are advised that consideration may be given to scaling back the Centre to only include the Principal's community aquatic and recreation facilities and therefore the Price Schedule - Part 4 includes a requirement for two (2) tendered prices. The estimated cost for the scaled back Centre (including construction, associated works and fit-out costs, consultants' fees, contingencies and other costs associated with the development) is \$65 million GST Exclusive.

The proposed Contract is for a period of four (4) years from the date of award which is the agreement for Council to construct the facility.

(e) Evaluation Panel

The tender submissions were evaluated by the following City of Cockburn officers:

1. Mr Stuart Downing Director, Finance and Corporate Services
2. Mr Daniel Arndt – Director, Planning and Development
3. Mr Rob Avard – Manager, Community Development

4. Mr Adrian Lacquiere, Coordinator, Recreation Services
5. Mr Brad Paatsch, General Manager Development FFC
6. Mr John Townsend, Independent Consultant

(f) Scoring Table

Tenderer's Name	Percentage Scores		
	Non Cost Evaluation	Cost Evaluation	Total
	80%	20%	100%
<b>Thinc Projects Australia Pty Ltd</b>	<b>59.90</b>	<b>15.40</b>	<b>75.30</b>
<b>NS Projects Pty Ltd</b>	<b>59.06</b>	<b>14.95</b>	<b>74.01</b>
<b>Davis Langdon Australia Pty Ltd</b>	<b>58.78</b>	<b>15.99</b>	<b>74.77</b>
Appian Group Pty Ltd	58.75	11.25	70.00
GHD Pty Ltd	55.18	14.10	69.27
Coffey Projects (Australia) Pty Ltd	54.05	14.19	68.24
Benchmark Projects Australasia	52.03	10.88	62.91
Savills Project Management Pty Ltd	50.13	15.75	65.87
Blue Visions Management Pty Ltd	46.98	2.40	49.38
Insight Project Services Pty Ltd	46.33	15.70	62.02
ICP Solutions	44.25	20.00	64.25
Aurora Projects Pty Ltd	42.75	7.01	49.76

Based on the above, three tenderers were requested to present to the above panel for an interview. The chosen three were:

1. Davis Langdon Australia Pty Ltd
2. NS Projects Pty Ltd
3. Thinc Projects Australia Pty Ltd

Summation

The Panel has evaluated all submissions and recommends that NS Projects Pty Ltd would be the most advantageous Project Management Firm to undertake this role for the City of Cockburn. This was a unanimous decision of the Tender Evaluation and Interview Panel.

**Strategic Plan/Policy Implications**

**Leading & Listening**

- A responsive, accountable and sustainable organisation.

### **Budget/Financial Implications**

The four (4) year contract would be funded from the CCW project fund equally over the four years subject to the Council endorsing the Cockburn Central West Business Plan.

### **Legal Implications**

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers

### **Community Consultation**

N/A

### **Attachment(s)**

The following Confidential Attachments are provided under separate cover:

1. Compliance Criteria Assessment;
2. Consolidated Evaluation Sheet; and
3. Tendered Prices

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

## **17.2 (OCM 09/05/2013) - TENDER NO. RFT02/2013 - QUANTITY SURVEYING SERVICES - REGIONAL AQUATIC AND RECREATION COMMUNITY FACILITY (CCW) (RFT 02/2013) (S DOWNING) (ATTACH)**

### **RECOMMENDATION**

That Council accept Tender No.RFT02/2013 – Quantity Surveying Services for the Regional Aquatic and Recreation Community Facility at Cockburn Central from WT Partnership Australia Pty Ltd for a period of four (4) years; in accordance with the price submitted in the confidential attachments.



## COUNCIL DECISION

### Background

The City of Cockburn's Regional Aquatic and Recreation Community Facility at Cockburn Central West is currently in the process of seeking public comment on the Business Plan. In addition funding applications have been submitted to both State and Federal Government Funding agencies. Part of the Business Plan and Funding Applications is a time line in order to construct the proposed facility. In order to meet the timeframe outlined above, a number of tenders are required to be adopted by Council; these include the appointment of a suitably qualified Quantity Surveyor. This tender is subject to Council adopting the Business Plan and approving the CCW Project. All tenderers have submitted responses based on the construction of the Integrated Facility including Cockburn and the Fremantle Football Club or the Non-Integrated Facility for the City of Cockburn only.

Tender Number RFT 02/2013 Quantity Surveying Services for CCW was advertised on Wednesday, 20 February 2013 in the Local Government Tenders section of "The West Australian" newspaper.

The tender was also displayed on the City's e-Tendering website.

### Submission

Tenders were called for Quantity Surveying Services at CCW for a four (4) year period and closed at 2:00p.m. (AWST) on Thursday 4 April 2013. Seven (7) tender submissions were received from:

1. Donald Cant Watt Corke
2. Aquentia Consulting
3. Turner Townsend Pty Ltd
4. WT Partnership Australia Pty Ltd
5. Ralph Beattie Bosworth
6. Rider Levitt Bucknall
7. Altus
8. Aecom Davis Langdon
9. Slattery Australia Pty Ltd

## Report

### (a) Compliance Criteria

	Compliance Criteria
(a)	Compliance with the Specification contained in the Request.
(b)	Compliance with the Conditions of Tendering this Request
€	Compliance with Fixed Price and completion of Clause 3.4.2A
(d)	Compliance with Insurance Requirements and completion of Clause 3.2.7.
€	Compliance with Occupational Safety & Health Requirements and completion of Appendix A.
(f)	Compliance with ACCC Requirements and completion of Appendix B.
(g)	Compliance with and completion of the Price Schedule, in the format provided in this Request in Part 4.
(h)	Compliance with Subcontractors (Proposed) and completion of Clause 3.5

### (b) Compliant Tenderers

All nine (9) Tender submissions were deemed compliant.

### (c) Evaluation Criteria

Tenderers were assessed against the following criteria:

Evaluation Criteria	Weighting Percentage
Demonstrated Experience	30%
Key Personnel Skills and Experience	25%
Tenderer's Resources	25%
Tendered Price – Estimated Lump Sum Contract Value	20%
<b>TOTAL</b>	<b>100%</b>

### (d) Tender Intent/ Requirements

The City of Cockburn (the Principal) in conjunction with the Fremantle Football Club (FFC) are seeking the services of an independent (i.e. not associated with any design consultants or construction contractors), qualified and experienced quantity surveying consultants to undertake cost management and QS services for the development of the Cockburn Integrated

Regional Physical Activity and Education Centre located at Cockburn Central West, Western Australia.

The Project at this stage is expected to deliver an integrated community facility that includes the Principal's community aquatic and recreation and the FFC's elite training and administration facilities. The current estimated cost of the integrated Centre (including construction, associated works and fit-out costs, consultants' fees, contingencies and other costs associated with the development) is \$107 million GST Exclusive; and is dependent on final stakeholder involvement and scope.

Tenderers are advised that consideration may be given to scaling back the Centre to only include the Principal's community aquatic and recreation facilities and therefore the Price Schedule - Part 4 includes a requirement for two (2) tendered prices. The estimated cost for the scaled back Centre (including construction, associated works and fit-out costs, consultants' fees, contingencies and other costs associated with the development) is \$65 million GST Exclusive.

The proposed Contract is for a period of four (4) years from the date of award which is the agreement for Council to construct the facility.

e) Evaluation Panel

The tender submissions were evaluated by the following City of Cockburn officers:

1. Mr Stuart Downing Director, Finance and Corporate Services
2. Mr Daniel Arndt – Director, Planning and Development
3. Mr Rob Avar – Manager, Community Development
4. Mr Adrian Lacquiere, Coordinator, Recreation Services
5. Mr Brad Paatsch, General Manager Development FFC
6. Mr John Townsend, Independent Consultant

In this instance, the evaluation panel assessed all tender submissions under a 'two envelope' system with the panel only having access to the qualitative criteria during their individual deliberations. Prices tendered were assessed separately.

f) Scoring Table

Tenderer's Name	Percentage Scores		
	Non Cost Evaluation	Cost Evaluation	Total
	80%	20%	100%
Ralph Beattie	76.01	13.68	89.69
Aecom Davis Langdon	75.74	12.33	88.08
Donald Cant	73.83	11.11	84.94
WT Partnership**	72.22	16.29	88.51
Rider Levitt	71.40	18.52	89.92
Slattery	66.34	10.42	76.77
Aqunta	63.50	12.27	75.77
Altus	55.67	20.00	75.67
Turner Townsend	52.91	15.35	68.26

**\*\* Recommended Submission**

The Panel members met on Tuesday, 23 April at 2.30pm to discuss the tenders and further analysed each of their submissions based on:

- The key personnel,
- Relevant experience,
- Hours allocated for the project and
- Pricing

Whilst all three tenders were capable of providing the level of service, WT Partnership Australia Pty Ltd was identified as providing the best value for price offered. Rider Levett Bucknall were eliminated early due to the under scoping of hours when compared to the other 2 tenders. The panel voted 5/0 in favour of appointing WT Partnership to provide the Quantity Surveying services for the project on behalf of the City of Cockburn and the Fremantle Football Club.

The formal appointment of WT Partnership as the preferred tender is subject to the approval of the Fremantle Football Club Board and the City of Cockburn Council.

Summation

The Panel having evaluated all submissions recommends that WT Partnership Australia Pty Ltd would be the most advantageous tender

to undertake the role of cost managers and QS consultants for the City of Cockburn.

This was a unanimous decision of the Tender Evaluation and Interview Panel.

### **Strategic Plan/Policy Implications**

#### **Leading & Listening**

- A responsive, accountable and sustainable organisation.

### **Budget/Financial Implications**

The four (4) year contract would be funded from the CCW project fund equally over the four years subject to the Council endorsing the Cockburn Central West Business Plan.

### **Legal Implications**

Section 3.57 of the Local Government Act 1995 and Part 4 of the Local Government (Functions and General) Regulations 1996 refers.

### **Community Consultation**

N/A

### **Attachment(s)**

The following Confidential Attachments are provided under separate cover:

1. Compliance Criteria Assessment;
2. Consolidated Evaluation Sheet; and
3. Tendered Prices

### **Advice to Proponent(s)/Submissioners**

The Proponent(s) and those who lodged a submission on the proposal have been advised that this matter is to be considered at the 9 May 2013 Council Meeting.

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil.

18. EXECUTIVE DIVISION ISSUES
19. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN
20. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING
21. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY COUNCILLORS OR OFFICERS
22. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE
23. CONFIDENTIAL BUSINESS
24. (OCM 09/05/2013) - RESOLUTION OF COMPLIANCE (SECTION 3.18(3), LOCAL GOVERNMENT ACT 1995)

**RECOMMENDATION**

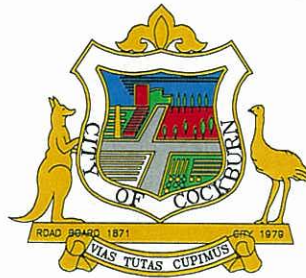
That Council is satisfied that resolutions carried at this Meeting and applicable to items concerning Council provided services and facilities, are:-

- (1) integrated and co-ordinated, so far as practicable, with any provided by the Commonwealth, the State or any public body;
- (2) not duplicated, to an extent Council considers inappropriate, services or facilities as provided by the Commonwealth, the State or any other body or person, whether public or private; and
- (3) managed efficiently and effectively.

**COUNCIL DECISION**

25. CLOSURE OF MEETING

# **CITY OF COCKBURN**



## **ORDINARY COUNCIL**

## **ATTACHMENTS**

## **FOR**

**THURSDAY, 9 MAY 2013**

# CITY OF COCKBURN

## SUMMARY OF MINUTES OF THE GRANTS & DONATIONS COMMITTEE MEETING HELD ON THURSDAY, 18 APRIL 2013 AT 4:30 PM

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# CITY OF COCKBURN

## MINUTES OF THE GRANTS & DONATIONS COMMITTEE MEETING HELD ON THURSDAY, 18 APRIL 2013 AT 4:30 PM

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### PRESENT:

Mr K Allen	-	Deputy Mayor (Presiding Member)
Mr L Howlett	-	Mayor
Mr T Romano	-	Councillor
Mrs C Reeve-Fowkes	-	Councillor
Mrs V Oliver	-	Councillor
Mr B Houwen	-	Councillor
Ms L Smith	-	Councillor

### IN ATTENDANCE:

Mr R. Avar	-	Manager, Community Services
Ms C. Robinson	-	Grants & Research Officer

### 1. DECLARATION OF MEETING

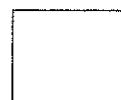
The Grants and Donations Committee Chairman Cr Allen declared the meeting open at 4:32pm.

### 2. APPOINTMENT OF PRESIDING MEMBER (If required)

N/A

### 3. ACKNOWLEDGEMENT OF RECEIPT OF WRITTEN DECLARATIONS OF FINANCIAL INTERESTS & CONFLICT OF INTEREST (BY PRESIDING MEMBER)

No written declarations of interests were received. Cr T Romano verbally declared a conflict of interest in Item 9.2 and two portions of Item 9.3. It was resolved by the Committee that the portions of Item 9.3 relating to Business Foundations Inc and Melville Cockburn Chamber of Commerce be considered separately from the remainder of Item 9.3 to allow Cr T Romano to debate and vote on the remainder of Item 9.3.



4. APOLOGIES & LEAVE OF ABSENCE

Nil

5. CONFIRMATION OF MINUTES

5.1 **(MINUTE NO 61)** (GAD 18/4/2013) - MINUTES OF THE GRANTS AND DONATIONS COMMITTEE MEETING - 16/10/2012 (ATTACH)

**RECOMMENDATION**

That the minutes of the Grants and Donations Committee meeting held on 16 October 2012 be adopted as a true and accurate record.

**COMMITTEE DECISION**

MOVED Mayor L Howlett SECONDED Clr T Romano that the recommendation be adopted.

**CARRIED 6/0**

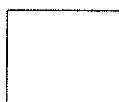
**COUNCIL DECISION**

6. DEPUTATIONS & PETITIONS

Presentation was given by representatives from the Cockburn Wetlands Precinct regarding current activities and achievements by both the Wetlands Education Centre and Native ARC. The Cockburn Wetlands Precinct is becoming an integrated facility and will be putting forward a joint proposal for funding to be considered at the next Committee meeting.

CLR C REEVE-FOWKES entered the meeting, the time being 4:46pm.

7. BUSINESS LEFT OVER FROM THE PREVIOUS MEETING (IF ADJOURNED)



Nil

**8. DECLARATION OF COUNCILLORS WHO HAVE NOT GIVEN DUE CONSIDERATION TO MATTERS IN THE BUSINESS PAPER**

Nil

**9. COUNCIL MATTERS**

**9.1 (MINUTE NO 62) (GAD 18/4/2013) - REVIEW OF SPONSORSHIP FUNDING (162/003) (R AVARD)**

**RECOMMENDATION**

That Council adopt the recommended changes to the sponsorship funding program. The Council adopt the recommended changes to the sponsorship funding program.

**COMMITTEE RECOMMENDATION**

MOVED Cllr C Reeve-Fowkes SECONDED Cllr B Houwen that that Item 9.1 be deferred to the next GAD Committee meeting to be held on 16 July 2013.

**AMENDMENT CARRIED 7/0**

**COUNCIL DECISION**

**Reason for Decision**

The Committee would like clarification on the use of the word extraordinary and further assurance that only sponsorships that add value to the profile of the City will be eligible under the new guidelines. The Committee would also like a further investigation into the possibility of opening up the Sponsorship program to a rolling intake year round rather than it being restricted to the two funding rounds in March and September.

The above will be reported on at the next GAD Committee Meeting.



## **Background**

The City's sponsorship program was initiated in 2008 to allow the City to take advantage of a number of opportunities to raise its profile and support local endeavours. The City's Sponsorship Funding program was successfully implemented in 2008/2009, 2009/2010 and 2011/2012. In 2010/2011 and 2011/12 a budget of \$40,000 was allocated towards this program. In 2012/2013 a budget of \$82,000 has been allocated to this program.

## **Submission**

N/A

## **Report**

The Sponsorship program is currently open to community groups or organisations based in the City of Cockburn who primarily service residents of the City of Cockburn. Applications are invited from not for profit community managed groups, including sporting, welfare, education, arts/cultural, youth seniors, children, ethnic and related groups. The program is also available to individuals who are residents of the City of Cockburn who are involved in a significant state, national or international event or activity.

The uptake of the Sponsorship throughout this time is as follows:

### **September 2008/March 2009**

A total of 9 applications received, 3 applications from individuals and 6 from groups. For the September 2008 and March 2009 rounds, 7 applications were approved totalling \$32,000, and 2 applications were declined.

### **September 2009/March 2010**

A total of 6 applications received, 0 applications from individuals and 6 from groups. For the September 2009 and March 2010 rounds, 4 applications were approved totalling \$30,000, and 2 applications were declined.

### **September 2010/March 2011**

A total of 12 applications received, 6 applications from individuals and 6 from groups. For the September 2010 and March 2011 rounds, 10 applications were approved totalling \$39,800, and 2 applications were declined.

### **September 2011/March 2012**



A total of 20 applications received, 10 applications from individuals and 10 from groups. For the September 2011 and March 2012 rounds, 15 applications were approved totalling \$80,500, and 5 applications were declined.

It has become apparent that there are some aspects of the Sponsorship program that need to be reviewed in order to refine its effectiveness. These aspects are:

- 1) Currently the Sponsorship Guidelines state that Individuals involved in a significant state, national or international event or activity may apply for sponsorship. However it is not currently a requirement for individual applicants to have been selected based on individual endeavour to represent their chosen activity, whether it is at state, national or international level. Following this, it is therefore also not a requirement to supply proof of selection and representation level. Thus in the past funding has been awarded to applicants whom have supplied no proof of selection or support letters.

It is important to recognise the original intent of the sponsorship program in that it is both an opportunity to add value to the community and an investment in recognising opportunities that will advance the long term interests of the City of Cockburn and the recipient.

It is therefore recommended that the wording on the Sponsorship guidelines is changed to reflect the wording below.

Current wording: Individuals involved in a significant state, national or international event or activity may apply for sponsorship.

Recommended new wording: Individual applicants attending an event or activity at an International, National or State level at which he or she has been selected on the basis of their extraordinary endeavours in their chosen activity may apply.

It is also recommended that a requirement to supply a letter from the organisation or governing body confirming selection of the individual be inserted into the current Sponsorship Application Form under:

'Step 1: Tell us about your organisation or individual details' and included in 'Step 7: Checklist'

The second line should read: Name of Organisation you will be representing (*a letter from the organisation confirming your selection is required*)



- 2) The Grants and Donations application form states the following criteria:
- a. You cannot apply if your project begins less than two months after the application close date
- Currently the above requirement is not stated on the Sponsorship application form.

It is recommended that the below wording be added to the Sponsorship application form:

- You cannot apply if your activity or event begins less than two months after the application close date

- 3) In the current round of funding, March 2013, the City received quite a few applications for sponsorship from local sporting clubs. There are in excess of 70 local junior and senior sporting clubs in the City of Cockburn. Providing sponsorship to one of these clubs will set precedence for all clubs.

It has been recommended that the sponsorship applications received from sporting clubs in the March funding round are not supported.

It is recommended that only exceptional sporting clubs receive support through the City's Sponsorship funding.

## **Strategic Plan/Policy Implications**

### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.
- Communities that take pride and aspire to greater sense of community.
- Promotion of active and healthy communities.

### **Leading and Listening**

- A responsive, accountable and sustainable organisation.

## **Budget/Financial Implications**

In 2012/13 Council approved a budget for grants, donations and sponsorships for 2012/13 of \$1,010,000.

## **Legal Implications**



N/A

### **Community Consultation**

The availability of Sponsorship as with the availability of Grants and Donations has been advertised through the City's website, local media, Cockburn Soundings, Council networks and related means.

It is recommended that advertising continue following the Council decision to ensure a wide representation of applications.

### **Attachment(s)**

1. Current Sponsorship Guidelines
2. Current Sponsorship Application Form

### **Advice to Proponent(s)/Submissioners**

N/A

### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil

At this point CLR T ROMANO left the meeting, the time being 5:32pm.

Conflict of interest - the nature of his interest is that CLR T ROMANO is the president of Melville Cockburn Chamber of Commerce.

## **9.2 (MINUTE NO 63) (GAD 18/4/2013) - MELVILLE CHAMBER OF COMMERCE (162/003) (R AVARD)**

### **RECOMMENDATION**

That the request for an Annual Sponsorship Commitment in the form of a Memorandum of Understanding (MOU) between the Melville Cockburn Chamber of Commerce and the City of Cockburn not be supported. That the request for an annual sponsorship Commitment in the form of a Memorandum of Understanding (MOU) between the Melville Cockburn Chamber of Commerce and the City of Cockburn not be supported.

### **COMMITTEE RECOMMENDATION**

MOVED Mayor L Howlett SECONDED Cllr C Reeve-Fowkes that that





the recommendation be adopted.

**CARRIED 6/0**

## **COUNCIL DECISION**

### **Background**

The City has provided funding to the Melville Cockburn Chamber of Commerce for several years to assist with the operational cost of delivering timely information, events and services to the business community of the City of Cockburn. Previous funding includes:

October 2006	-	\$20,000
October 2007	-	\$20,000
September 2008	-	\$20,000
September 2009	-	\$20,000
August 2010	-	\$10,000 (interim funding)
September 2011	-	\$20,000
March 2012	-	\$20,000

The MCCC uses these funds to promote and foster building of relationships, exchange of business and social contacts and exchange of business knowledge between members.

Sponsorship and branding benefits include organisational naming rights, logo inclusion on MCCC stationery and documentation, signage, website and promotional material and recognition and signage at a number of events. The City will be promoted as 'Partner'. It is recommended to support this sponsorship proposal.

### **Submission**

Council received a request from the President of the MCCC, Tony Romano, on behalf of the Board of the MCCC that the sponsorship Commitment of \$20,000 per annum from the City of Cockburn Grants and Sponsorship Committee to the MCCC be awarded in two split payments of \$10,000 to be paid December 2012 & June 2013, rather than one payment in June 2013. The MCCC have also requested that this arrangement be put under an ongoing agreement or an 'MOU', rather than presenting a sponsorship application each year.



The City has since received updated correspondence from MCCC requesting to enter into an MOU with the City of Cockburn under the following guidelines.

1. Sponsorship for 2012/2013 to proceed with payment of \$20,000 payable May 2013
2. 'MOU' Sponsorship to start July 1, 2013 going forward for financial year 13/14
3. 'MOU' agreed amount, paid each quarter September/December/March/June
4. First payment due under 'MOU' would be September 1st 2013  
KPI's and performance report to be discussed at an annual meeting between the City of Cockburn and the MCCC.

### **Report**

The City has for a number of years supported the MCCC based on an annual decision by Council which has not committed the City to an ongoing sponsorship agreement.

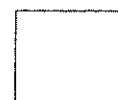
The mission of the MCCC is to encourage and foster business growth and development by facilitating and promoting events and activities to enhance business growth and encourage the development of the Melville and Cockburn district.

The MCCC mission directly aligns with the City of Cockburn Strategic Community Plans theme to create a Prosperous City through strengthening our economic base, particularly through promotion and support for the growth and sustainability of local businesses and local business centres. Therefore the MCCC and the City share a common goal in facilitating business development within the local community.

On 12 August 2010 Council approved an allocation of \$10,000 to provide interim funding to the MCCC. This funding was requested to implement a review of the current organisational business model to ensure long term financial and operational sustainability. The MCCC has therefore had some history of not being financially viable.

While the City has for a number of years supported the MCCC, this has been based on an annual decision by council which does not commit the City to an ongoing sponsorship agreement. Council is then able to monitor the performance of the MCCC against their KPIs and come to an informed decision on an annual basis.

Each year all requests for sponsorship and donations are reviewed in two competitive funding rounds in March and September and are assessed on their individual merit.



It is recommended that the current request from the MCCC to enter into an MOU with the City is not supported.

### **Strategic Plan/Policy Implications**

#### **Leading & Listening**

- Effective advocacy that builds and manages relationships with all stakeholders.
- A responsive, accountable and sustainable organisation.
- Quality customer service that promotes business process improvement and innovation that delivers our strategic goals.
- A skilled and engaged workforce.

#### **A Prosperous City**

- Investment in the local economy to achieve a broad base of services and activities.
- Promotion and support for the growth and sustainability of local businesses and local business centres.

### **Budget/Financial Implications**

Nil

### **Legal Implications**

N/A

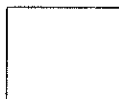
### **Community Consultation**

Nil

### **Attachment(s)**

1. MCCC request for MOU
2. MCCC outline of request for MOU

### **Advice to Proponent(s)/Submissioners**



# Implications of Section 3.18(3) Local Government Act, 1995

Nil

## 9.3 **(MINUTE NO 64) (GAD 18/4/2013) - GRANTS AND DONATIONS COMMITTEE RECOMMENDED ALLOCATIONS 2012/13 (162/003) (R AVARD)**

### **RECOMMENDATION**

That council adopts the revised grants, donations and sponsorship allocations for 2012/13 as attached to the agenda.

### **COMMITTEE RECOMMENDATION**

MOVED Cllr C Reeve-Fowkes SECONDED Mayor L Howlett that that the recommendation for Business Foundations and Melville Cockburn Chamber of Commerce be adopted.

**CARRIED 6/0**

MOVED Mayor L Howlett SECONDED Cr T Romano that council approve the recommended grants, donations and sponsorship allocations for 2012/3 as attached to the agenda subject to the following 6 amendments:

1. An increase in the recommended donation to Training Ship Cockburn Parents Committee Inc. to \$2,000
2. A decrease from \$5,000 to \$4,000 in the recommended sponsorship of Hamilton Senior High School towards the Hamilton Senior High School 50<sup>th</sup> Anniversary
3. A decrease from \$1,000 to \$500 in the recommended sponsorship of Austin Keyte towards his participation on Philippines Immersion
4. A decrease from \$1,000 to \$500 in the recommended sponsorship of Zakary Brown towards his participation on the World Challenge expedition to Sri Lanka
5. A decrease from \$2,000 to \$1,000 in the recommended sponsorship of Suzanne Marsella towards Fundraising for Medical supplies for her Clinical Placement
6. An increase from \$2,460 to \$3,000 in the recommended sponsorship of Atwell College towards their 2013 Canberra Tour
7. An increase from \$1,700 to \$2,160 in the recommended donation to Friends of the Community Inc

**AMENDMENT CARRIED 7/0**

## COUNCIL DECISION

CLR T ROMANO returned to the meeting, the time being 5:42pm

### **Reason for Decision**

Recommended allocation to Training Ship Cockburn Parents Committee Inc. was increased to \$2,000 to assist them in setting up in their new venue to increase their appeal and visibility to the community.

Recommended allocation to Atwell College was increased from the \$2,460 officer's recommendation to \$3,000 to reflect previous year's allocations.

Friends of the Community Inc are a not-for profit group entirely made up of volunteers. The increased recommended donation to the Centre from \$1,700 to \$2,160 will support the operational costs of the group.

Recommended allocations to Hamilton Senior High School, Austin Keyte, Zakary Brown and Suzanne Marsella were decreased to allow for the increase in allocation in other areas.

### **Background**

Council approved a budget for Grants and Donations for 2012/13 of \$1,010,000. The Grants and Donations Committee is empowered to recommend to Council how these funds are to be distributed. At its meeting of 16 July 2012 The Committee recommended a range of allocations which were duly adopted by Council on 9 August 2012.

Following the September 2012 round of grants, donations and sponsorship funding opportunities, the Committee, at its meeting of 16 October 2012, recommended a revised range of allocations which were duly adopted by Council on 25 October 2012.

The March funding round was advertised closing 28 April 2013. A total of 52 applications were received including 26 applications for Community Grants and one application for a sustainable event grant which will be reviewed under delegated authority of the Manager of Community Services. The Committee is to consider the remaining 25



applications for donations and sponsorship, as well as revised allocations for the 2012/13 grants and donations budget.

### **Submission**

N/A

### **Report**

The March 2013 round of grants, donations and sponsorship funding opportunities was advertised to close on 28 March 2013. The applications for donations and sponsorship are described in brief below.

### **Committed and Contractual**

#### **Cockburn Basketball Association Inc**

As a result of the Council decision made on 14 February 2013 there is a new allocation of \$82,000 to the Cockburn Basketball Association Incorporated towards the completion of the re-flooring of Wally Hagan Stadium.

In the Summary of Grants, Donations & Sponsorship Recommended Allocations 2012/13, attached to the agenda for the Committee to consider, there is/are:

- 1 proposed adjustment to the committed/contractual donations
- 9 proposed adjustments to grant funding allocations
- 9 applications for donations
- 15 applications for sponsorship.

### **Committed and Contractual**

#### **Volunteer Home Support Inc**

Volunteer Home Support Inc provide Government subsidized domestic cleaning, transport, handyman, lawn and gardening services to around 1200 frail aged and disabled clients.

Volunteer Home Support Inc had been able to dump waste material from their gardening activities at Henderson Waste Management Site free of charge for many years, until May 2012 when tip fees were applied to them. The City provided a \$5,000 donation to cover some of the cost towards their waste disposal from Cockburn homes that they service.



Volunteer Home Support Inc has again requested a \$5,000 donation to help with the cost of waste removal. It is recommended to support this application.

Murdoch University – Coastal and Estuarine Dolphin Project

Council has received a request from Murdoch University to support their Coastal and Estuarine Dolphin Project (CEDP). The University have requested a contribution of \$10,000 per year for four years.

Native wildlife is the responsibility of the State Government (The Department of Environment and Conservation (DEC)). The City only has jurisdiction within 200 metres of the low water mark and hence no management responsibility for Owen Anchorage or the flora and fauna that resides off our shores. It is recommended not to support this proposal.

**Grants**

Amendment of allocations to March demand across Funding Programs for 2012/13:

- A decrease in funds allocated to the following programs to account for an increase in applications in other grant areas:
  - Community Grants Program from \$139,688 to \$59,005.
  - Sponsorship from \$82,000 to \$47,460.
  - Community Associations Hall Hire Subsidy from \$2,500 to \$1,500.
  - Sustainable Events Grants Program from \$14,500 to \$4,000.
  - Cost of Health Permits for Events from \$1,500 to \$500.
  - Toby Play Pad Subsidy from \$5,000 to \$2,500.
- An increase in funds allocated to the following programs to account for an increase in applications in these grant areas:
  - Donations from \$120,000 to \$153,000.
  - Committed/Contractual Donations from \$392,756 to \$472,863.
  - Cultural Grants from \$18,000 to \$18,510.

**Donations**

Applicant: Business Foundations Inc.

Requested: \$10,000

Recommended: \$10,000



Business Foundations Inc. is a not-for-profit provider of enterprise development services to the community of Cockburn. Clients range from people wanting to start a small business or become self-employed, to existing small to medium sized business owners that require business management skills, to people wanting to exit from business. Services range from one-on-one advisory sessions to group training, mentoring and business incubation.

In the 11/12 financial year, Business Foundations supported 40 new businesses in Cockburn to start up representing approximately \$10.9 million of economic value to the area. They also assisted over 280 people and businesses in Cockburn through their variety of services such as training and networking sessions.

The organisation has received funding from the City in previous years:

October 2007	-	\$10,000
March 2009	-	\$10,000
March 2010	-	\$10,000
March 2011	-	\$10,000
March 2012	-	\$10,000

This year the organisation has again requested a donation of \$10,000 to contribute to operating costs to ensure the one-on-one advisory service is provided to Cockburn residents and for a staff member to maintain a presence at the Melville-Cockburn Chamber of Commerce meetings, promoting services available to Cockburn business people. It is recommended to support this application.

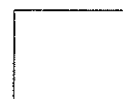
Applicant: City of Cockburn Pipe Band

Requested: \$9,000

Recommended: \$9,000

The City of Cockburn Pipe Band is seeking a donation to assist with ongoing expenses of the band such as kilts and glengarries. In 2012 they were the first Australian Band to attend the St Patricks Parade in New York, which they raised funds to attend. They are also travelling to Scotland in August this year to compete in the Worlds competition.

The band drew for first place at the State Championships and they perform freely at a number of events to support other Cockburn





community organisations including the Cooby Festival, the Spring Fair and in nursing homes.

Previous funding from the City in the form of donations and community grants are as follows:

October 2004	-	\$6,000
October 2006	-	\$4,000
October 2007	-	\$8,000
March 2009	-	\$8,000
March 2010	-	\$8,000
March 2011	-	\$8,000
March 2012	-	\$8,000

This year, the Band is seeking a donation of \$9,000. It is recommended to support this application for \$9,000.

Applicant: Second Harvest

Requested: \$10,000.00

Recommended: \$10,000.00

Second Harvest is a not-for profit community enterprise which supports a number of low cost food outlets run by public welfare organisations and various church bodies. Second Harvest's role is to obtain low cost, and discounted foodstuffs, and household and personal items for the food centres servicing holders of Commonwealth Health Care cards, including pensioners, the disabled and the unemployed, together with other low income families and individuals. Second Harvest also dispenses much needed emergency food relief through the head office and six community food centres.

Second Harvest moved into new premises in Cockburn Central at the beginning of 2012. Second Harvest received a Community Grant of \$8,850 in the 2012 March funding round to purchase a new fridge and freezer.

Second Harvest has requested a donation of \$10,000 to assist with their ongoing costs. Second Harvest prepared and dispensed 250 food hampers in the first 12 months at Cockburn Central (960 individuals). Second Harvest is supported by Lotterywest, members of South Metropolitan Area Health Service and Department of Health. It is recommended to support this application.



Applicant: Hamilton Hill YouthCARE Council

Requested: \$9,000.00

Recommended: \$9,000.00

The Hamilton Hill YouthCARE Council supports a full time chaplain at the Hamilton Hill Senior High School and also the chaplains at two other schools including two days at Coolbellup Community School.

The City has provided annual donations for this program for a number of years:

October 2006	-	\$9,000
March 2008	-	\$9,000
March 2009	-	\$9,000
March 2010	-	\$9,000
March 2011	-	\$9,000
March 2012	-	\$9,000

The Youth CARE mission is to provide pastoral care, teach Christian Religious Education, and provide personnel and professional development for staff and volunteers.

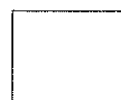
Hamilton Hill YouthCARE Council has requested a donation of \$9,000 to assist with their aim to serve the school community and provide positive benefits for the whole community. It is recommended to support this application.

Applicant: South Lake Ottey Family and Neighbourhood Centre Inc.

Requested: \$12,000.00

Recommended: \$7,000.00

The South Lake Ottey Family and Neighbourhood Centre provide diverse programs and outreach activities in the community in collaboration with solid partnerships and networks. The Centre works closely with City of Cockburn, St. John of God Murdoch, Strong Families and Medicare Local. The Centre is available to individuals and families in Cockburn-Central and surrounds, offering support and activities in response to demonstrated community needs. The Centre aims to provide an integrated service and is seen as a 'one stop shop'



by centre users and external stakeholders. In responding to needs, the Centre sees 'the whole person' and seeks to provide a mix of formal and informal programs and interactions.

The group has previously received a number of donations for operating costs and community grants for specific projects from the City:

Donations:

March 2007	-	\$5,000
March 2008	-	\$5,000
March 2009	-	\$5,000
March 2010	-	\$5,000
March 2011	-	\$5,000
March 2012	-	\$10,000

Community Grants:

October 2001	-	\$1,000
March 2003	-	\$1,000
March 2008	-	\$1,500
September 2009	-	\$1,260

This round, the group has requested a donation of \$12,000 towards general operating costs. This group is well supported by Department for Communities and Lotterywest Grants. Donations to this Centre and to the Yangebup Family Centre had traditionally remained equal at \$5,000 until 2012, when Yangebup Family Centre received \$7000 and South Lake Ottey Family and Neighbourhood Centre received \$10,000. It is recommended to fund both centres equally; therefore it is recommended to support this application for \$7,000.

Applicant: Cockburn Central YouthCARE Council

Requested: \$33,600.00

Recommended: \$24,000

YouthCARE seeks to provide a sensitive Christian presence in WA public schools by providing Chaplaincy and mentoring services and Christian Religious Education. YouthCARE works in partnerships with school communities and the Department of Education to foster the psychological, social, intellectual, emotional, spiritual and physical development of students.



The Cockburn Central YouthCARE Council has requested a donation of \$33,600 to supplement the 2 days per week generally available to each school presently, which is funded under the Commonwealth's National School Chaplaincy & Student Welfare Program, with an additional day in each of four needy primary schools (for example, Atwell Primary School, Bibra Lake Primary School, Success Primary School and Yangebup Primary School) – though the particular schools are yet to be finally identified. The donation will only be used in schools that agree to contribute 30% of the extra funding needed. This application seeks the City of Cockburn to provide the balance of the extra funding needed for these supplementary services.

In previous years, the City has provided the Cockburn Central YouthCARE Council the following:

October 2006	-	\$9,000
October 2007	-	\$9,000
September 2008	-	\$9,000
March 2010	-	\$11,600 (\$9,000 for Lakeland Senior High School and \$2,600 towards Chaplaincy at Atwell College)
March 2011	-	\$9000
March 2012	-	\$9,450

The Council is supported by Atwell Primary School, Success Primary School, Harmony Primary School, Jandakot Primary School, Lakeside Baptist Church, Anglican Parish of Lakelands, Atwell Community Church, Gateway Community Church and Yangebup Baptist Church.

The Cockburn Central YouthCARE Council has requested \$33,600 this year. It is recommended to support this application for the lesser amount of \$24,000, contributing to 50% of the extra funding needed per school on the following condition:

- Cockburn Central YouthCARE Council work with the City to identify which schools are funded.

Applicant: Friends of the Community Inc

Requested: \$1,700.00

Recommended: \$1,700.00



Friends of the Community Inc is a not for profit group entirely made up of volunteers. The group has a small food van which sells sandwiches, tea and coffee, ice creams and cool drinks; however their main income comes from running sausage sizzles. The group's profits are all returned to the community through youth, aged, disabled and disadvantaged applications for funds.

Friends of the Community Inc have previously received the following community grants for specific projects from the City:

March 2007	-	\$4,000
March 2010	-	\$1,799
September 2011	-	\$1,300

Friends of the Community Inc have requested \$1,700 to put towards a new computer and computer hardware to enable their office volunteers to store documentation required to run an efficient volunteer group. It is recommended to support this application.

Applicant: Constable Care Child Safety Foundation (Inc)

Requested: \$10,000.00

Recommended: \$10,000.00

The purpose of Constable Care Child Safety Foundation (CCCSF) is to effectively communicate key safety, crime prevention and citizenship messages to children through best-practice evidence-based theatre-in-education programs.

The foundation delivers these programs across primary schools and early learning centres in the Cockburn area and has received an annual donation for a number of years.

October 2006	-	\$18,045
October 2007	-	\$18,780
September 2008	-	\$19,531
September 2009	-	\$20,495
September 2010	-	\$20,950
March 2012	-	\$10,000

The foundation receives grants and donations through State and Federal Government and thus requested a reduced donation of



\$10,000 toward programs run within the City of Cockburn for 2011/12. CCCSF has again requested a donation of \$10,000 and has proposed to work with the City's Crime Prevention Liaison Officer to ensure relevant messages are relayed to Cockburn children in line with the City's Crime Prevention Strategy. This application is supported by the City's Crime Prevention Liaison Officer. It is recommended to support this application.

Applicant: Training Ship Cockburn Parents Committee Inc

Requested: \$5,000.00

Recommended: \$0

Training Ship Cockburn's purpose is to provide youth with an alternative and challenging program at a reduced rate and with the same values, attitudes and behaviours instilled in serving officers and sailors.

Training Ship Cockburn offers cadets the opportunity to attain the Recreational Slippers ticket, Basic Sailing qualifications, and Bronze Medallion and Senior First Aid qualifications. All of these are led by instructors who donate their time in order to keep costs at a minimum. The organisations fees are kept as low as possible to encourage those in lower socio-economic and demographic situations the opportunity to participate. Training Ship Cockburn has recently been approved to move into the old SES Building, Kent St, Spearwood of which there is no rent payable by this group.

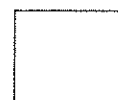
Training Ship Cockburn Parents Committee Inc has requested a donation of \$5,000 to assist them in setting up in their new venue to increase their appeal and visibility to the community. The Committee conducts fundraising and are well supported by the Royal Australian Navy. Due to their rental support and funding received by the Royal Australian Navy it is not recommended to support this application.

### Sponsorship

Applicant: Kaden Sargent

Proposal: 2013 BMX National Championship

Requested: \$2,000.00



Recommended: \$0.00

Kaden Sargent is a Success resident and currently rides for the Cockburn BMX Club. Kaden competed in the local BMX state titles in March 2012 where he finished 2nd overall, qualifying him for the 2013 BMX National Championships in the 11 year boy's category, which will be held in Brisbane, Queensland. Kaden hopes to finish in the top 8 so that he receives a National Plate. If he finishes in the top 16, he will qualify to race at the World Titles in New Zealand in July 2013.

In 2011, Kaden received \$350 funding from the Junior Travel Assistance Program to subsidise his travel to Cairns to represent the state in BMX at the national championships.

Kaden has requested \$2000 towards attending the 2013 BMX National Championships, held in Brisbane, Queensland. If he is successful, Kaden has proposed he will promote his sponsorship by wearing a race shirt with 'supported by the City of Cockburn' on the back. He will also publish this support on the Cockburn BMX Club Website and on Facebook, both his personal page and the Cockburn BMX Clubs page.

It is recommended that the application is not supported as this event is eligible for funding from the Junior Sport Travel Assistance Program. Two similar cases in the past have not been supported due to their eligibility to apply under the Junior Sport Travel Assistance Program, namely Tyson Bryden in March 2009 and Tia Stonehill in March 2012.

Applicant: Tyler TranquilleProposal: To travel interstate for high level Tennis TournamentsRequested: \$2,000.00Recommended: \$0.00

Tyler Tranquille is a 15 year old Success resident and tennis player who is seeking financial assistance to travel interstate for high level tennis tournaments. Tyler needs to travel in order to improve and maintain his Australian Ranking. Tyler is currently the highest ranked junior in the City of Cockburn.

Tyler's proposal is to travel interstate 4 times to National and ITF tournaments, which attract international and national interest. Tyler has requested a sponsorship of \$2,000 to contribute to his total travelling



costs of \$10,000 for 2013. Tyler also receives in-kind contributions from a Chiropractor, a Psychologist, The Cove Health Club and Rudy Project, who provided sunglasses.

Tyler trains 5 days a week, sees a Chiropractor and attends Psychology sessions. Tyler recently won the Esperance 16's Singles and Doubles titles.

Tyler has his own website and if successful with his sponsorship application he would be proud to include the City of Cockburn on his sponsor's page and will wear logos on his tennis clothing. Tyler is also willing to assist the City of Cockburn in whatever way possible, he is willing to be a role model to local teens or make presentations.

It is recommended that the application is not supported as this event is eligible for funding from the Junior Sport Travel Assistance Program. Two similar cases in the past have not been supported due to their eligibility to apply under the Junior Sport Travel Assistance Program, namely Tyson Bryden in March 2009 and Tia Stonehill in March 2012.

Applicant: Hamilton Senior High School

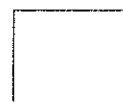
Proposal: Hamilton SHS 50th Anniversary

Requested: \$19,000.00

Recommended: \$5,000.00

Hamilton Senior High School is a comprehensive Year 8 -12 public School with 70 staff members and 600 students. This year Hamilton SHS will be celebrating its 50<sup>th</sup> Anniversary over a one week period on the Hamilton Hill Site with current and past students and staff. A working party has been established and the range of community events planned to culminate in the week commencing 16<sup>th</sup> September 2013 includes:

- Hamilton Jubilation Art Exhibition Opening – School Library.
- Hami Swings by the Fly by Night Club in Fremantle – past music students.
- Hamilton SHS Open Day – 50 years of History.
- Reunion Gala Ball at the Esplanade Hotel in Fremantle – all past students and staff.
- Photo book – Iconic Hamilton Senior High School pictures and School Histories for each decade of the past 50 years.





Hamilton SHS has requested a sponsorship of \$19,000 towards their total costs of \$38,000. If they are successful Hamilton SHS will promote the City's sponsorship through social media, advertising in their newsletter and through articles in the local paper. The City will also have the opportunity to display signage at events and have their logo on event tickets.

It is recommended to support this sponsorship proposal for the lesser amount of \$5,000. This amount is viewed to be an appropriate amount of funding towards this type of event and takes into consideration the budget limitations of the current funding round.

Applicant: Melville Cockburn Chamber of Commerce

Proposal: Partnership Sponsorship

Requested: \$20,000.00

Recommended: \$20,000.00

The City has provided funding to the MCCC for several years to assist with the operational cost of delivering timely information, events and services to the business community of the City of Cockburn. Previous funding includes:

October 2006	-	\$20,000
October 2007	-	\$20,000
September 2008	-	\$20,000
September 2009	-	\$20,000
August 2010	-	\$10,000 (interim funding)
September 2011	-	\$20,000
March 2012	-	\$20,000

The MCCC has applied for \$20,000 for a Partnership Sponsorship with the City of Melville for 2012/13. The MCCC intends to use these funds to promote and foster building of relationships, exchange of business and social contacts and exchange of business knowledge between members.

Sponsorship and branding benefits include organisational naming rights, City logo on Partnership Position on MCCC website, bimonthly newsletter, mail outs, big screen at events and public acknowledgement at all MCCC events. The City will be promoted as



'Partner'. It is recommended to support this sponsorship proposal of \$20,000.

Applicant: Austin Keyte

Proposal: Philippines Immersion

Requested: \$1,000.00

Recommended: \$1,000.00

Austin Keyte is a 15 year old Hamilton Hill resident and a student at Christian Brothers College (CBC) Fremantle. Austin has been selected to participate in the Philippines Immersion for 2013. The CBC Immersion to the Philippines is an 8 day group activity and students learn the "give and take" of the community living experience. The Immersion aims to raise awareness of the power of education in poor remote communities, where students will work with local Christian Brothers students to plant seedlings, assist with a clinic for hearing impaired children and support basic literacy programs.

Austin already helps out with the Schools Open Day and assists younger students with sports coaching. On his return from the Immersion program Austin will complete voluntary work outside of school through programs such as the Good Start Breakfast at East Hamilton Hill Primary School and Fremantle Soup kitchen with the van in the evenings.

Austin has applied for a sponsorship of \$1,000 to subsidise the total cost of \$2,900. If he is successful, Austin will promote his sponsorship through local media.

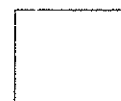
It is recommended to support this sponsorship proposal of \$1,000.

Applicant: Jordan Gladwell

Proposal: Kent St UK Tour 2013

Requested: \$2,000.00

Recommended: \$0.00



Jordan Gladwell is an Atwell resident and a student at Kent St Senior High School. Jordan has been chosen to attend the schools Kent St UK Tour to play State Country Cricket.

Jordan is seeking a \$2,000 sponsorship to help fund his trip to England to play Cricket against teams from the UK, USA and South Africa.

There is limited opportunity for the City to be recognised through this sponsorship and Jordan is not representing the country or state. It is not recommended to support this sponsorship proposal.

Applicant: Zakary Brown

Proposal: World Challenge expedition to Sri Lanka

Requested: \$1,000.00

Recommended: \$1,000.00

Zakary Brown is an Atwell resident seeking sponsorship to attend a World Challenge expedition to Sri Lanka. The expedition will be for 3 weeks and will include a physical challenge, a project (e.g. helping in a village) and adjusting to a new culture. Zakary is getting a job and fund raising to help fund the trip. A support letter from Zakary's school says he is an excellent student, cooperative and attentive at all times.

The City has supported four previous requests for sponsorship to participate in the World Challenge Expedition, two in 2011 and one in 2010 and one in 2012. All were provided \$2,000 in sponsorship each.

It is recommended to support this sponsorship proposal of \$1,000.

Applicant: Jessica Watts

Proposal: Australian Country Hockey Tour 2013 - Athlete Assistance

Requested: \$2,000.00

Recommended: \$0.00

Jessica Watts is a Hamilton Hill resident and has been selected as a member of the Australian Country Women's Hockey team for the 2013 Asia Tour. This year the calibre of the team is of an exemplary level and Jessica believes the team will be extremely competitive against the



national teams. Jessica also believes her participation will increase the profile of hockey within the City of Cockburn.

Jessica's achievements so far include being selected in the Victorian Country team in 2007, 2008, 2009 and 2010 and the WA Country team in 2012.

Jessica trains individually 5 times a week and 3 times with her local club. Jessica is supported by the Western Australian Hockey community, her past Hockey club (Peel Hockey Club), Fremantle Hockey Club and Hockey Australia.

The total cost for the tour is \$4,750 and Jessica has requested a sponsorship of \$2,000 towards this cost. If successful, Jessica will promote the City of Cockburn sponsorship through local media (in particular the Fremantle Cockburn Gazette), Fremantle Hockey Club and Hockey WA. She will also utilise social media and promote the sponsorship through her own networks as she is employed as a Club Development Officer. Jessica works as a volunteer at the Fremantle Hockey Club, where she will be sharing her experiences with the younger Hockey community as well as assisting the club with grant applications and developing greater governance and volunteer management practices.

Unfortunately the tournament dates fall before Jessica will have received a decision regarding her sponsorship which means this application is not eligible for funding as this would be retrospective. It is not recommended to support this application.

Applicant: Suzanne Marsella

Proposal: Fundraising for Medical Supplies for Clinical Placement

Requested: \$2,000.00

Recommended: \$2,000.00

Suzanne Marsella is a Beeliar resident studying at Murdoch University. Suzanne is part of a group of 11 students in third year nursing travelling to Northern Thailand to provide primary healthcare and medical provisions. The trip is a voluntary clinical placement to utilise their nursing skills and assist disadvantaged villagers. The trip is organised by Antipodeans Abroad in conjunction with Murdoch University and this



will be the first time health care will be provided to the particular village of Om Goi, Thailand.

Suzanne will be working in conjunction with the Om Goi district, which is administered by the regional hospital. This hospital serves over 95 villages, of which there are 6 sub-districts with a total of approximately 61,000 people. Limited resources and transportation are key issues and necessary equipment and supplies are often unavailable or unobtainable. Suzanne's group is hoping to raise sufficient funds to purchase a reasonable amount of medical supplies to contribute to the regional hospital to benefit these villages.

Suzanne is supported by Murdoch University.

Suzanne has requested \$2,000 towards her travel costs for the trip, of which the total cost of \$5,500. If successful, Suzanne will promote the sponsorship through Murdoch University, Antipodeans Abroad and the Local Media.

It is recommended to support this application of \$2,000.

Applicant: Sarena Hyland

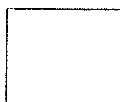
Proposal: European Architecture Students Assembly

Requested: \$2,000.00

Recommended: \$0.00

Sarena Hyland is a Spearwood resident who has recently completed their final year in the Master of Architecture course at the University of Western Australia. Through her studies and profession Serena has been selected as one of two members in the global architecture student network to be a team leader known as an International Contact for a group of architecture students attending a prestigious annual event titled the "European Architecture Students Assembly" to be held in Slovenia from 4<sup>th</sup> – 18<sup>th</sup> August 2013.

Sarena has been a member of St John Ambulance for the past 14 years, providing volunteer first aid services at public events throughout Perth. She has also devoted her time in developing young cadets and adults with first aid and leadership skills on a local, state and national level as a Cadet leader at Fremantle Cadet Division, along with her role within the organisation as the WA Youth Council Chair.



In 2011 Serena was awarded the Walter Winterton Award for most outstanding young volunteer with St John in Western Australia and has recently won the National First Aid Championships for the Cadet Leader category held in Tasmania earlier this year.

Serena is requesting \$2000 to assist her to embark on this endeavour to Slovenia.

As Serena is no longer a student, it is not recommended to support this application.

Applicant: Katherine Ruth Claire James

Proposal: Funding to participate in the school Art & Photography Tour in New York

Requested: \$2,000.00

Recommended: \$0.00

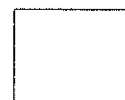
Katherine James is a Bibra Lake resident and a Year 12 student at Penrhos College where she has been taking photography classes since Year 9. Katherine is attending the School Art and Photography tour in New York which is organised for Year 12 photography students. The purpose of the tour is to provide an opportunity for students to enhance the skills learnt over the years of the course and to gain experience in professional photography and develop and learn more skills.

Katherine is supported by the Head of Photography of Penrhos College and she has also been recognised at school with a badge for the number of hours of community work she has completed.

Katherine has been fundraising for her trip for over a year and has also taken a part time job to help fund her trip. Katherine has requested a sponsorship of \$2,000 to assist her to reach her goal of attending the schools photography tour.

If successful Katherine will promote her sponsorship through social media, throughout her school and through a story in the local paper, something she has done previously.

It is recommended that the application is not supported as this event is eligible for funding from the Youth Art Scholarship Program.



Applicant: Southern Lions Rugby Club

Proposal: 2013 Rugby Season

Requested: \$5,000.00

Recommended: \$0.00

Southern Lions Rugby Club hosts teams from U6 age group up to men's opens and ladies. The Club aims to provide a fun, competitive and healthy environment for all members to enable them to thrive achieve success.

Southern Lions Rugby Club have requested a \$5,000 sponsorship to assist with 2013 rugby season costs for their 11 junior and 6 senior teams. If successful, the sponsorship will be promoted on the club website, facebook page, at club events and home matches where the City would also be invited to advertise.

The City of Cockburn has approximately 70 junior and senior sporting clubs. Approving this application will set a precedent for future applications. It is recommended that this application is not supported.

Applicant: South Coogee Junior Football Club

Proposal: South Coogee Y7 team to travel to Victoria to represent South Fremantle Football Club Junior Council

Requested: \$11,400.00

Recommended: \$0.00

South Coogee Junior Football Club (JFC) Year 7 team has been selected to represent the District in Melbourne to play against a Victorian team at the Melbourne Cricket Ground at half time of an AFL game on 9th June 2013 in front of approximately 60,000 people. They will also play a second full game against a Victorian Year 7 team on the following day.

South Coogee JFC has requested a sponsorship of \$11,400 towards their total costs of \$20,000 to cover the cost of travel and accommodation for the 20 players.

If they are successful, the City of Cockburn will be recognised as an official sponsor for the 2013 season. The South Coogee JFC will also



look at placing the City's logos on the teams travel wear or bags. The City will be recognised through any media coverage, on the clubs website and through the clubs newsletter.

The City of Cockburn has approximately 70 junior and senior sporting clubs. Approving this application will set a precedent for future applications. It is recommended that this application is not supported.

Applicant: Atwell College

Proposal: 2013 Canberra Tour

Requested: \$3,000

Recommended: \$2,460

The Atwell College Canberra Tour is an annual tour which aims to offer students with the opportunity to gain a deeper knowledge of Australian History, scientific understandings and the cultural elements of Australia. Thirty-seven students will participate in the tour.

In previous years the City has provided Atwell College with the following funding:

Mar 2012 - \$2000

Sep 2011 - \$5000

Mar 2010 - \$5000

Mar 2009 - \$13,000

Atwell College have requested a \$3,000 subsidy towards the total cost of \$61,698. It is recommended to support this application for the lesser amount of \$2,460 with the following conditions:

- Each student will have the City of Cockburn logo on their jacket for the tour.
- Atwell College supports the City's Youth Advisory Council through the nomination of two students to join the Council.

### **Strategic Plan/Policy Implications**

#### **Community & Lifestyle**

- Community environments that are socially cohesive and embrace diversity.





- Communities that take pride and aspire to a greater sense of community.
- Promotion of active and healthy communities.

### **Leading and Listening**

- A responsive, accountable and sustainable organisation

### **Budget/Financial Implications**

Council approved a budget for grants and donations for 2012/13 of \$1,010,000. Following is a summary of the proposed grants, donations and sponsorship allocations.

Committed/Contractual Donations	\$472,863
Specific Grant Programs	\$335,977
Donations	\$153,700
<u>Sponsorship</u>	<u>\$ 47,460</u>
Total	\$1,010,000

### **Legal Implications**

N/A

### **Community Consultation**

In the lead up to the March 2013 round, grants, donations and sponsorship funding opportunities were promoted through the local media and Council networks. The promotional campaign has comprised:

- Three advertisements running fortnightly in the Cockburn Gazette's City Update on 19/02/13, 5/03/13 and 19/03/13.
- One quarter page advertisement in the Cockburn Gazette's wrap on 19/03/2013
- Four advertisements running fortnightly in the City of Cockburn Email Newsletter.
- Advertisement in the February Cockburn Soundings.
- All members of the Cockburn Community Development Group, Regional Parents Group and Regional Seniors Group have been encouraged to participate in the City's grants program.
- Additional Advertising through Community Development Promotional Channels:



- Gazette part of Full page Ad 12 Feb 2013, 12 March 2012
- Community Development Calender distributed to all NFP groups in Cockburn
- Community Development ENews: 13/3/13,28/2/13,15/2/13, 16/1/13

#### **Attachment(s)**

1. Murdoch University Sponsorship Proposal - Coastal and Estuarine Dolphin Project
2. Murdoch University Addendum to CEDP Proposal Budget
3. Revised Summary of Grants, Donations & Sponsorship Recommended Allocations 2012-13

#### **Advice to Proponent(s)/Submissioners**

N/A

#### **Implications of Section 3.18(3) Local Government Act, 1995**

Nil

#### **10. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**

Nil

#### **11. NOTICES OF MOTION GIVEN AT THE MEETING FOR CONSIDERATION AT NEXT MEETING**

Nil

#### **12. NEW BUSINESS OF AN URGENT NATURE INTRODUCED BY DECISION OF MEETING BY COUNCILLORS OR OFFICERS**

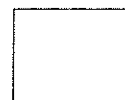
Nil

#### **13. MATTERS TO BE NOTED FOR INVESTIGATION, WITHOUT DEBATE**

Nil

#### **14. CONFIDENTIAL BUSINESS**

Nil



**15. CLOSURE OF MEETING**

There being no further business the meeting was declared closed at 6:20pm.

**CONFIRMATION OF MINUTES**

I, ..... (Presiding Member) declare that these minutes have been confirmed as a true and accurate record of the meeting.

Signed: ..... Date: ...../...../.....





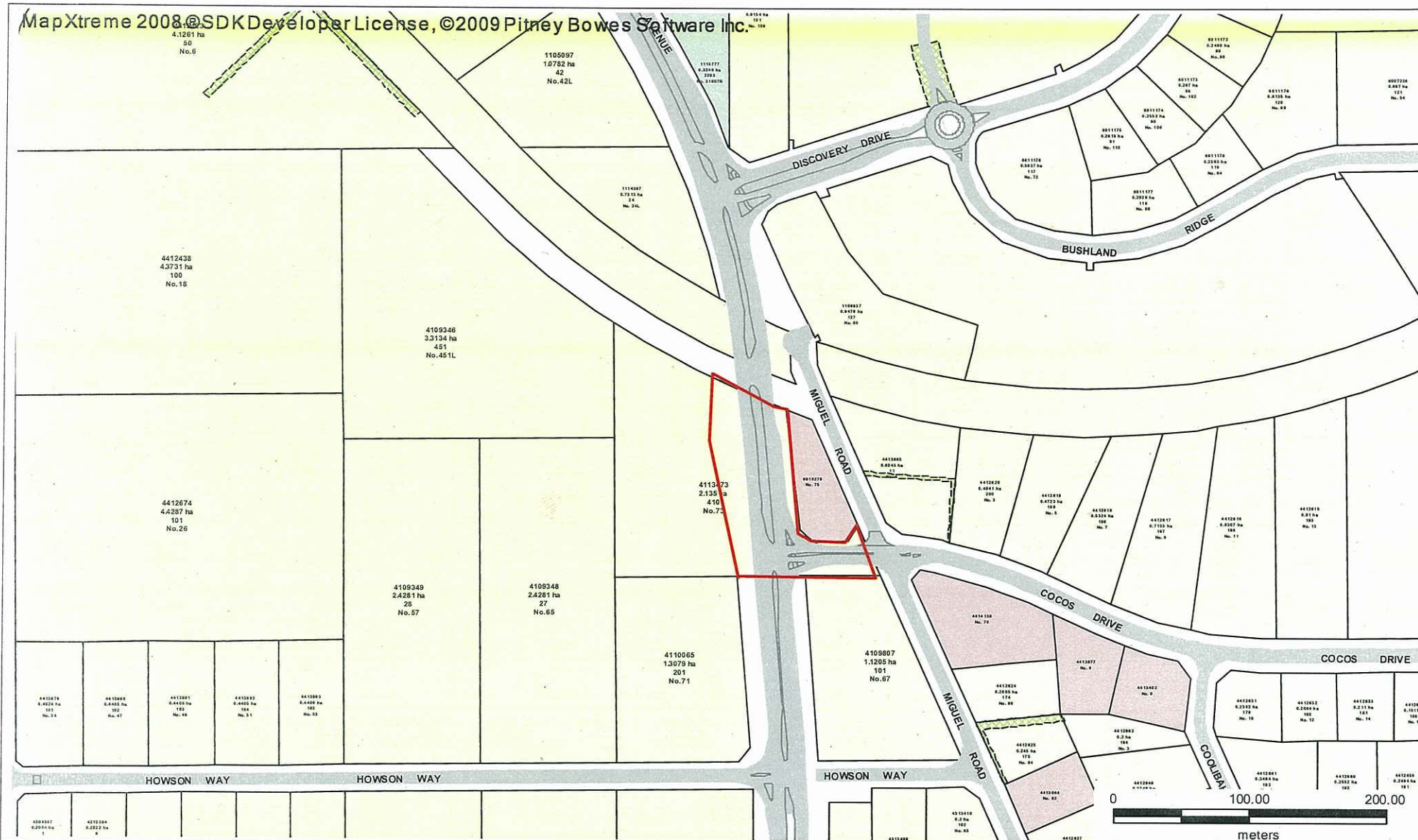
GRANTS, DONATIONS & SPONSORSHIP COMMITTEE RECOMMENDATION BUDGET 2012/2013 (Revised APRIL 2013)

Activity OP 315 Natural Account 6810	Description	Allocated 2012- 2013	Actual as of April	April Actuals/Adjustme nts 2012/2013	Comments
	<b>Committed/Contractual</b>				
9310	Wetlands Education Centre Management Fee	75,569	75,569	75,569	Renewal of Management Agreement for three years to 30 June 2013 and Management Fee in accordance with Council decision on 13 May 2010 (plus Perth CPI)
9317	Pineview Preschool Maintenance Contribution	6,394	6,312	6,394	Annual contribution for maintenance of grounds and building (plus CPI)* lease revised budget should increase to \$6600.00
9398	Cockburn Senior Citizens Building Donation	8,684	8,684	8,684	Assists with maintenance costs as per agreement (plus CPI)
9559	Cockburn Cricket Club Insurance	1,500	0	1,500	Commitment included in the lease
9574	Spearwood Dalmatinac Club	10,000	9,924	9,924	Reimbursement of 50% of the annual rates payable by Spearwood Dalmatinac Club for 41 Azelia Road, Spearwood as to Council decision on 14 May 2009
9242	Burdiya Aboriginal Corporation - Property Insurance	1,200	0	1,200	Property Insurance for 8 Caffrey Place, Hamilton Hill leased from the City by Burdiya Aboriginal Corporation
9237	Cockburn Basketball Association Building Insurance	7,745	6,318	6,318	50% contribution to the annual building insurance premium of the Wally Hagen Basketball Stadium
9239	Native ARC	50,000	50,000	50,000	Donation to progress the Native ARC to cover administrative employment, insurance, legal, accounting, auditing, corporate governance and utilities to bring them up to good
9245	Old Jandakot School Management Committee	3,000	3,000	3,000	Annual contribution to water and electricity charges included in the lease agreement and as to Council decision 8 December 2009
9109	Fremantle Australia Day Celebrations & Cracker	25,000	25,000	25,000	One-off donation towards the City of Fremantle Australia Day Celebrations and Cracker Night (Co-ownership of event to be negotiated)
9179	Senior Citizen Bus	10,000	10,000	10,000	Grant of \$10,000 to the Cockburn Senior Citizen's Association for the purchase bus, subject to a successful application to Lotterywest as per Council decision 14 June
9178	Spearwood Dalmatinac Club	59,300	53,909	53,909	Grant allocation to the Spearwood Dalmatinac Club for an amount of \$59,300 for the air conditioning upgrade as per Council decision 14 June 2012
9322	South Lake Leisure Fee Subsidy	104,364	91,872	104,364	Subsidised fees for swimming club
9188	Cockburn-Fremantle Pistol Club Upgrades	10,000	10,000	10,000	Contribution towards the clubs upgrades of the rifle ranges to meet OSH requirements
9189	Australian Marine Complex Owners Study	20,000	20,000	20,000	Contribution Australian Marine Complex Owners Study
	Cockburn Basketball Association		82,000	82,000	Council decision February 2013 funds for flooring of Cockburn Basketball Association
	Volunteer Home Support Inc			5,000	Request for funding to support Volunteer Home Support Inc
	<b>Sub Total</b>	<b>392,756</b>	<b>452,588</b>	<b>472,863</b>	
	<b>Donations</b>				
	Donations	120,000	0		Funds remaining for March donation allocations (9495)
9249	Cockburn Community & Cultural Council		9000	9,000	Request for funding to support operation of CCCC
9120	Yangebup Family Centre		7000	7,000	Request for funding to support Yangebup Family Centre Crèche
9250	Port School		15000	15,000	Request for funding for chaplaincy program
9121	Returned Services League - City of Cockburn		9000	9,000	Request for funding to support operations of RSL
9195	Portuguese Cultural and Welfare Centre		2000	5,000	Request for funding to support operations of centre
9196	Lions Club of Atwell		1000	1,000	Request for funds to assist with start up costs of new lions club
9252	Fremantle Hospital Medical Research Foundation		15000	15,000	Request for funding to support research project
9119	Cockburn Toy Library		4000	4,000	Request for funding to support operation of the toy library
9251	Halo Leadership Development College Inc		8,000	8,000	Request for funding to support operations of Halo
	Business Foundations			10,000	Request for funding to support Business Foundations
	City of Cockburn Pipe Band			9,000	Request for funding to support City of Cockburn Pipe band
	Second Harvest			10,000	Request for funding to support Second Harvest
	Hamilton Hill YouthCARE			9,000	Request for funding to support Hamilton Hill YouthCARE
	South Lake Ottey Family Centre			7,000	Request for funding to support South Lake Ottey Family Centre
	Cockburn Central YouthCARE Council			24,000	Request for funding to support Cockburn Central YouthCARE Council
	Friends of the Community			1,700	Request for funding to support Friends of the Community
	Constable Care Child Safety Foundation Inc			10,000	Request for funding to support Constable Care Child Safety Foundation Inc
	Training Ship Cockburn Parents Committee Inc			0	Request for funding to support Training Ship Cockburn Parents Committee Inc
	<b>Sub Total</b>	<b>120,000</b>	<b>70,000</b>	<b>153,700</b>	
	<b>Sponsorship</b>				
	Sponsorship	82,000	0		Funds remaining for March sponsorship allocations(draw from 9495)
9197	Phoebe Wilcox- World Challenge Expedition		2,000	2,000	Request for funds for Phoebe Wilcox to attend World Challenge Expedition
9198	(Cooby Now) Coolbellup Community Association		6,000	6,000	Request for funds to support Cooby Fest
9199	Daniella Palazzo- Student Exchange Italy		2,000	2,000	Request for Daniella Palazzo to attend Student Exchange in Italy
9200	Natasha Read- World Challenge Expedition		2,000	2,000	Request for funds for Natasha Read to attend World Challenge Expedition
9131	Bryan Williams - World Transplant Games		2,000	2,000	Request for funds for to attend World Transplant Games
	Kaden Sargeant			0	Request for funds for Kaden to attend the 2013 BMX National Championship - Eligible for Junior Travel Assistance
	Tyler Tranquille			0	Request for funds for Tyler to travel interstate for high level Tennis Tournament - Eligible for Junior Travel Assistance
	Jordan Gladwell			0	Request for funds for Jordan to attend the Kent St UK Tour 2013
	Austin Keyte			1,000	Request for funds for Austin to attend Philippines Immersion
	Zakary Brown			1,000	Request for funds for Zakary to attend World Challenge Expedition
	Jessica Watts			2,000	Request for funds for Jessica to attend Australian Country Hockey Tour 2013
	Suzanne Mersella			2,000	Request for funds for Suzanna to attend Nursing Clinical Placement in Omi Goi region, Thailand
	Sarena Hyland			0	Request for funds for Sarena to attend the European Architecture Students Assembly
	Katherine Ruth Claire James			0	Request for funds for Katherine to participate in the school Art and Photography Tour in New York - Eligible for Youth Art Scholarship
	Hamilton Senior High School			5,000	Request for funds to support Hamilton Senior High School 50th Anniversay
	South Coogee Junior Football Club			0	Request for funds for the South Coogee Y7 team-travel to Victoria to represent the South Fremantle Footbal Club Junior Council
	Melville Chamber of Commerce			20,000	Request for funds to sponsor Melville Chamber of Commerce
	Southern Lions Rugby Club			0	Request for funds to support the Southern Lions 2013 Rugby Season - 1 of approximately 70 sporting clubs in Cockburn
	Atwell College			2,460	Request for funds to sponsor 2013 Canberra Tour
	<b>Sub Total</b>	<b>82,000</b>	<b>14,000</b>	<b>47,460</b>	
	<b>Grants</b>				
9004	Emergency Severe Personal Hardship Fund	20,000	16,070	20,000	For one off emergency and disaster situations (revised as per Council Decision 10 February 2011 and increased by \$10,000)
9015	Youth Academic Grants	1,500	0	1,500	Assists young people to attend academic programs as per DA ACS11
9031	Junior Travel Assistance - Sports	40,000	24,000	40,000	Assists young people in Cockburn representing WA or Australia in interstate or international team or individual sports by providing assistance for travel to competitions
9674	Grants to Schools	8,950	7,198	8,950	For small donations to schools for minor items as per DA ACS7 (increase by \$1950 for Graduation Awards specifically for Indigenous Students (\$600 for 6 high schools,
9312	Community Grants Program	139,688	31,839	59,006	Formal grant process for local organisations as per DA ACS2
9314	Provide Bins Sporting Events	1,500	1,204	1,500	Provide bins to schools for sports carnivals etc



Activity OP 315 Natural Account 6810	Description	Allocated 2012- 2013	Actual as of April	April Actuals/Adjustme nts 2012/2013	Comments
9327	Community Associations Hall Hire Subsidy	2,500	774	1,500	Assists community groups to conduct monthly meetings and events
9329	Cultural Grants Program	18,000	4,455	18,510	Provide small grants to cultural and artistic groups
9331	Bus Hire Subsidy	3,000	550	3,000	Provides a small allocation towards the bus hire for community organisations
9335	Grants Welfare General	5,500	2,880	5,500	Miscellaneous requests for small donations
9341	Community Group Newsletter Subsidy	5,000	2,536	5,000	Assists community groups to disseminate information
9373	Sustainable Events Grants Program	14,500	1,500	4,000	Grants for community organisations to have events on the understanding that the event will become financially self sustainable over four years
9396	U Fund	1,200	0	1,200	Grants up to \$600 to youth for cultural/arts initiatives and events
9399	Youth Arts Scholarships	8,000	2,300	8,000	Assist young people to travel in order to participate in performing/arts events and also for further study
9475	Alcoa Cockburn Community Projects Fund	21,812	0	21,812	A partnership fund with Alcoa delivering community-driven projects (\$10,906 carry forward form 2011/2012)
9490	Environmental Education Initiatives Program	12,000	0	12,000	Support for Environmental Services to assist schools to facilitate environmental education
9517	P & I Volunteer Accident Insurance	7,000	5,801	7,000	Cockburn Community Group Insurance Program
9535	Council/Staff Match Donation	5,000	2,619	5,000	Council to match staff fund raising effort
9536	Cost of Health Permits for Events	1,500	0	500	To provide free health permits for not-for-profit groups
9617	Youth Incentive Program	1,000	0	1,000	Awards for youth who report crime and anti social behaviour
9649	Safety House/Walk to School Program	1,500	455	1,500	Support to schools for safety programs for children getting to school
9688	Security Subsidy for Seniors	25,000	14,164	25,000	Subsidy for security devices for seniors
9240	Sustainability Grants Program	40,000	0	40,000	Grants program to replace Sustainability Awards, in accordance with Council decision on 13 May 2010
9241	Burdiya Hall Subsidy	3,000	454	3,000	Subsidy program that will allow indigenous Cockburn families to access funds to assist with hall hire costs for hosting funerals and memorials
9596	SLLC Subsidy for Emergency Services Volunteer	1,000	1,000	1,000	South Lake Leisure Centre gym subsidy for Jandakot BFB, South Coogee BFB and SES volunteers
9673	Sport and Recreation Club Grant	38,000	22,707	38,000	Grants matched by local sporting clubs to engage in minor capital works on Council owned facilities and to purchase sporting equipment. New allocation made up from left
9100	Toby Play Pad Subsidy		200	2,500	Allocation to establish Toby Playpad Subsidy
9495	Donation and Grants General Account				
	<b>Sub Total</b>	<b>426,150</b>	<b>142,705</b>	<b>335,978</b>	
	<b>Totals</b>	<b>1,020,906</b>		<b>1,010,000</b>	Includes \$10,906 of Alcoa funds carried forward
	<b>Budget</b>	<b>1,010,000</b>		<b>1,010,000</b>	
	<b>Balance</b>			<b>- 0</b>	





**LOCATION PLAN  
ROAD DEDICATION SPEARWOOD AVENUE  
EX. LOT 410  
BIBRA LAKE**

PRINTED ON:  
Monday, 25 March 2013

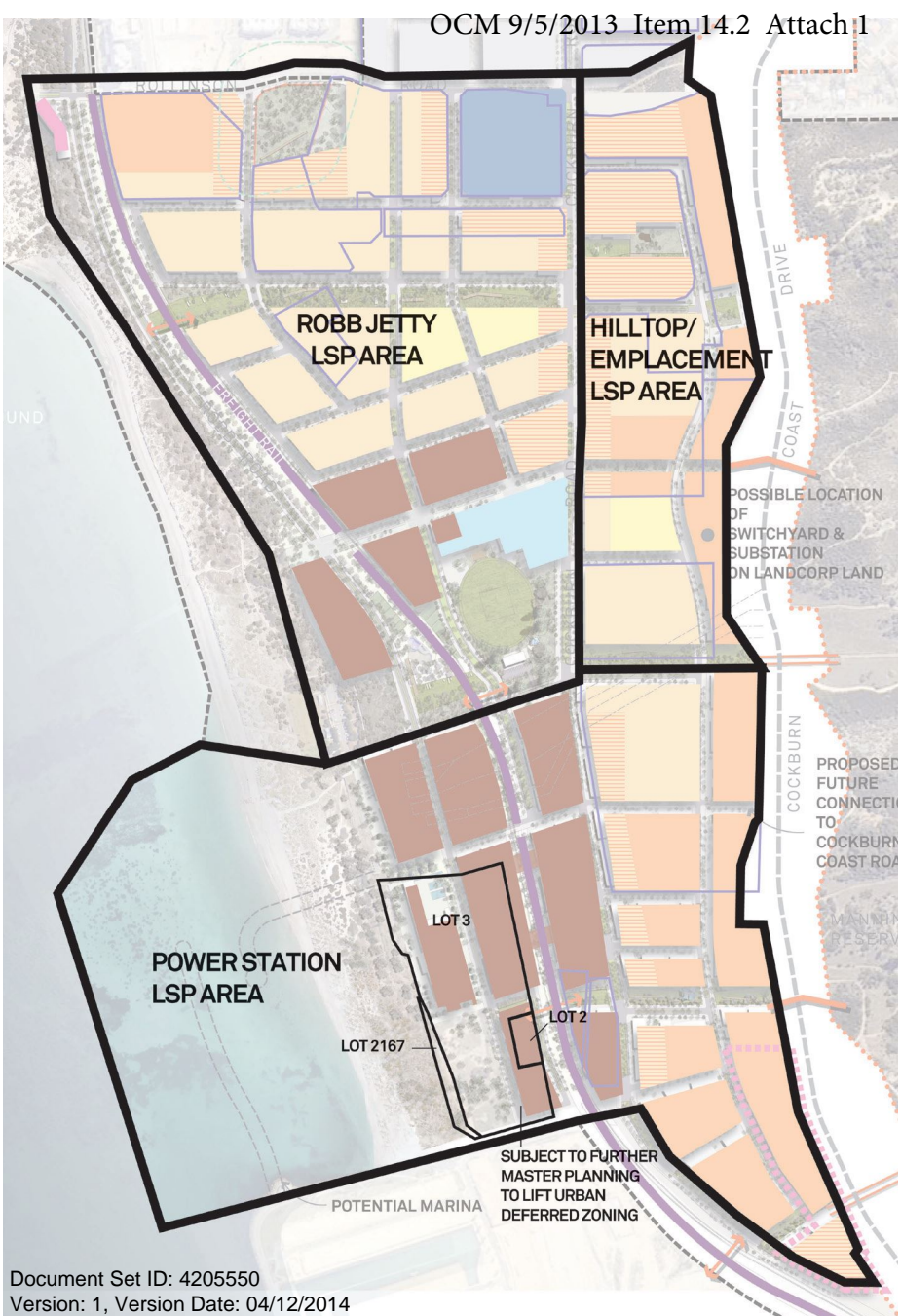
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**City of Cockburn**  
GIS Services Department

VERSION	AMENDMENT	AUTHORISED BY	DATE	<b>LOT 3001 TO BE ACQUIRED FOR A PUBLIC PURPOSE (SPEARWOOD AVENUE)</b>		TYPE	FREEHOLD		<b>HELD BY LANDGATE IN DIGITAL FORM ONLY</b>
						PURPOSE	ACQUISITION		
						PLAN OF	<b>LOTS 202 &amp; 3001</b>		
						DISTRICT	COCKBURN SOUND		FILE
						TOWNSITE			
						LOCAL AUTHORITY	CITY OF COCKBURN		
						LOCALITY	BIBRA LAKE		
						FORMER TENURE	ON	FIELD BOOK	
						LOT 410 ON P 3699 (1) C/T: 1947 - 919	INDEX BG 34 (2) 10.07	121452	
						Scale: @ A3	1:1250		
						ALL DISTANCE ARE IN METRES			





<b>POL</b>	<b>COCKBURN COAST DESIGN GUIDELINES FOR ROBB JETTY AND EMPLACEMENT PRECINCTS</b>	<b>APD73</b>
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<b>POLICY CODE:</b>	APD73
<b>DIRECTORATE:</b>	Planning & Development
<b>BUSINESS UNIT:</b>	Planning and Development
<b>SERVICE UNIT:</b>	Statutory Planning
<b>RESPONSIBLE OFFICER:</b>	Manager of Statutory Planning
<b>FILE NO.:</b>	PS/P/001
<b>DATE FIRST ADOPTED:</b>	
<b>DATE LAST REVIEWED:</b>	
<b>ATTACHMENTS:</b>	
<b>DELEGATED AUTHORITY REF.:</b>	APD54
<b>VERSION NO.</b>	1

<b>Dates of Amendments / Reviews:</b>	
DAPPS Meeting:	
OCM:	

## **BACKGROUND:**

The Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement Precinct have been prepared to guide the development and urban form (including subdivision) of Robb Jetty Local Structure Plan and Emplacement Local Structure Plan (LSP) areas.

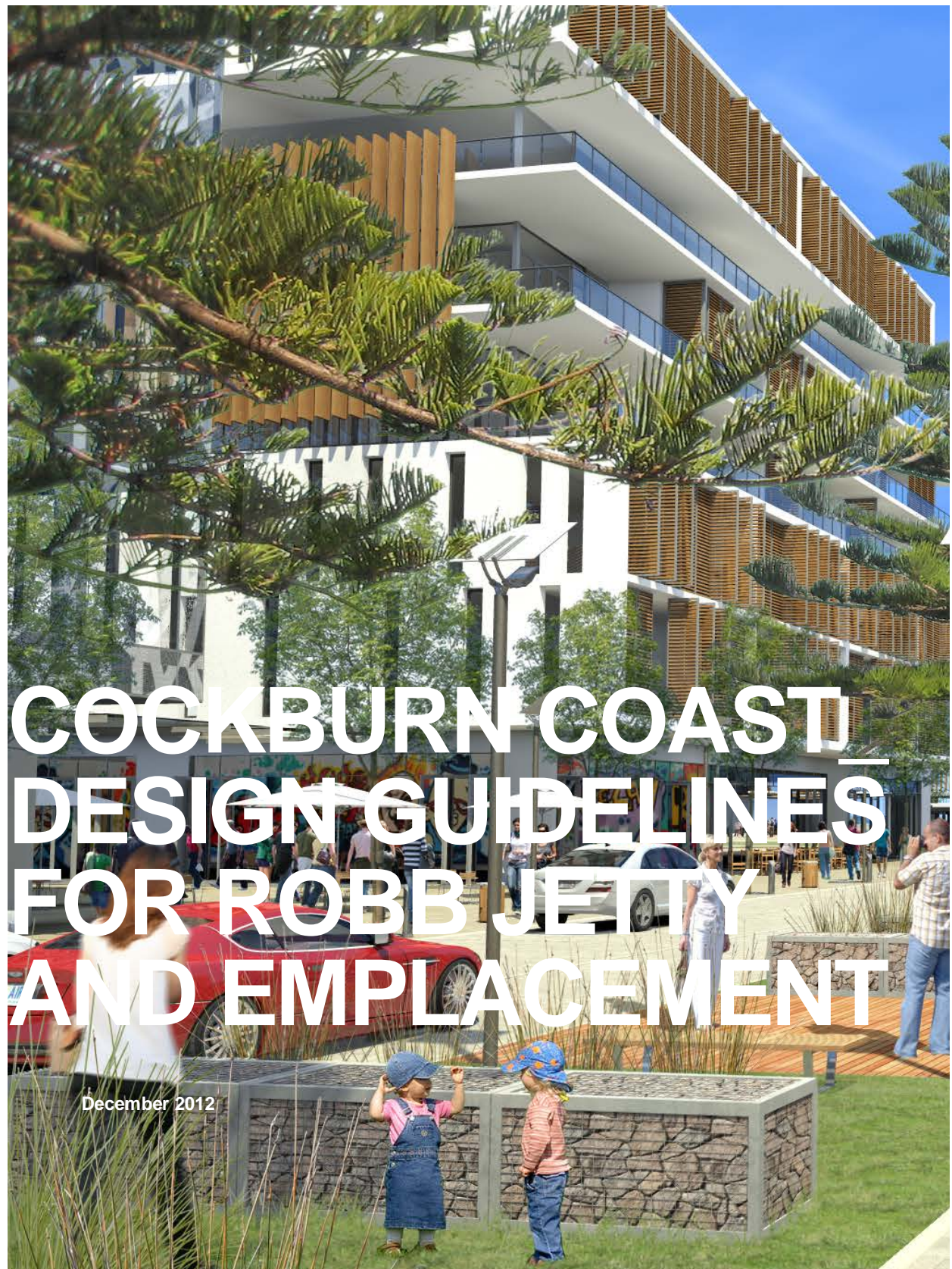
## **PURPOSE:**

The Design Guidelines will guide the creation of a quality development that ensures the design principles of the Robb Jetty and Emplacement LSP's are achieved.

## **POLICY:**

Appendix 1 contains the Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement Precinct.

Development applications will be assessed under the Design Guidelines in conjunction with the Residential Design Codes of Western Australian, the approved structure plan and any other relevant local planning policy.



# COCKBURN COAST DESIGN GUIDELINES FOR ROBB JETTY AND EMPLACEMENT

December 2012

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## **Background**

### **I Introduction**

The Cockburn Coast Design Guidelines for the Robb Jetty and Emplacement LSP areas (henceforth referred to as the Design Guidelines) have been prepared to guide the development ~~(including subdivision)~~ and urban form ~~(including subdivision)~~ of Robb Jetty Local Structure Plan (Robb Jetty LSP) and Emplacement Local Structure Plan (Emplacement LSP) areas. The design guidelines are focused on the creation of a quality development that ensures the design principles of the Robb Jetty and Emplacement LSP's are achieved.

The design guidelines will bring to fruition a lively and sustainable urban centre set amongst dense residential development. The design guidelines introduce standards for development to create the intended character and amenity within the Robb Jetty LSP and Emplacement LSP areas. Although some of the criteria are mandatory, the general approach is to provide a series of broad principles for development to follow while allowing flexibility in design outcomes over the project life span.

The design guidelines are a performance orientated assessment tool. Each design element is expressed as a design objective and one or more assessment criteria. Where a stated assessment criterion is proposed to be varied, development must demonstrate that it meets the related design objective. In this way a performance approach to design and assessment is facilitated.

The design guidelines are divided into two main sections:

#### **Part 1\_Private Realm**

##### *Typology Specific Guidelines*

A series of built form typologies are established in defined areas where specific guideline provisions apply that may expand on or vary the general provisions.

##### *General Provisions*

Contains the design guideline general provisions which are applicable to all development.

#### **Part 2\_Public Realm**

~~Contains development provisions for the public realm to guide the creation of streets and public urban places.~~

### **II Vision for Cockburn Coast**

Capitalising on a rare opportunity, these design guidelines set out to inform the development of an exciting mixed use community that celebrates the best of the Western Australian coastal lifestyle.

Cockburn Coast will be different from its neighbouring suburbs; it will be a place that offers choice and variety of living, recreation and working opportunities. Core to the success of the redevelopment is a well connected Bus Rapid Transit (BRT) system which is intended to link the development to its surrounding areas. As well as connecting the design guideline area to its surrounds, this system will provide an internal system of movement which encourages more sustainable personal transportation choices.

The City of Cockburn's *Cockburn Coast District Structure Plan* (DSP) and *Cockburn Coast District Structure Plan Part 2* (DSP2) nominates three local structure plan areas being Robb Jetty, Emplacement and Power Station. Each of these areas is distinct in character and function. These design guidelines introduce standards for development to create the intended character and amenity within the Robb Jetty and Emplacement LSP areas following a detailed local structure planning process.

##### *Robb Jetty LSP Area*

The Robb Jetty LSP area forms the north-western portion of the site and stretches from Rollinson Road in the north, to the Parkland Corridor in the south and Cockburn Road in the east. The area stretches west of Robb Road but excludes the beach.

The Robb Jetty LSP area will contain elements of mixed use development along significant road links including Cockburn Road but is otherwise set aside for medium to high density residential development. The area will also house supporting community facilities in the form of the two storey urban primary school and the area's key active

playing field. A coastal character is proposed to complement the adjacent foreshore and areas of open space contained within it.

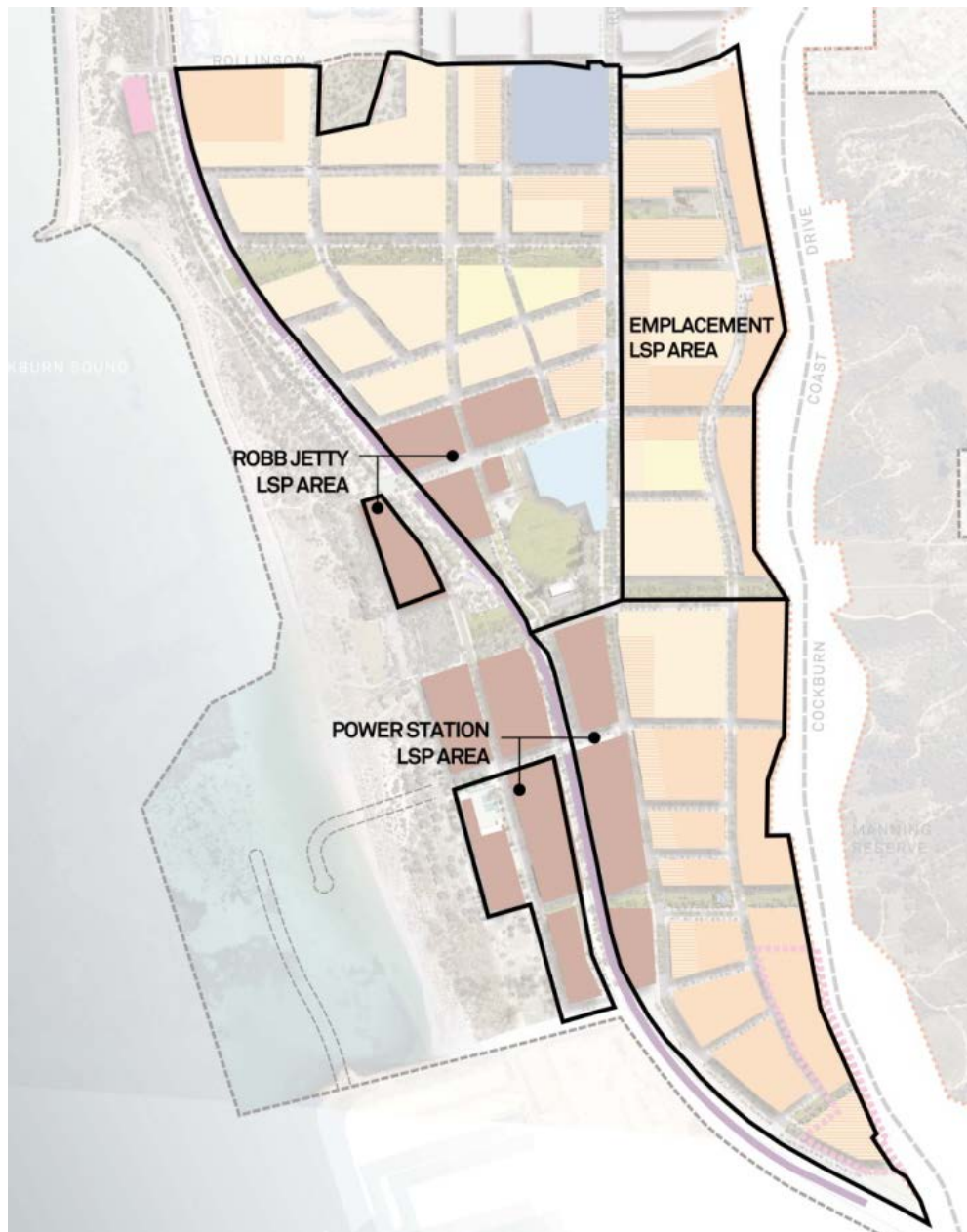
The BRT public transport alignment is set to pass through the heart of the area and be well connected to Fremantle and the rapidly emerging Cockburn Central. A variety of small but connected public spaces will offer a range of experiences from the quiet to the communal, the sheltered to the open and the organic to the formal.

#### *Emplacement LSP Area*

The Emplacement LSP area forms the north-east portion of the project area and stretches from the northern boundary of the master plan area, to the middle parkland corridor to the south, to Cockburn Coast Drive in the east, and Cockburn Road in the west.

The distinct character of the Emplacement LSP area is a product of its elevated topography and this landform influences how it shall be treated. Development will be responsive to the topography and shall aim to retain as much of the existing natural character of the site as possible. The Emplacement LSP area will be predominantly mixed use in its north, residential in its south, and contain the east-west linear parks, providing strong connections from Beeliar Park and through Robb Jetty LSP area to the coastal foreshore.

The Emplacement LSP area will be the new highpoint, a manufactured horizon line that offers the opportunity for a new architectural topography and an integrated landscape of nature and built form.



The design guidelines complete a complex process of strategic planning to capitalise on the opportunity for redeveloping Cockburn Coast identified in the Western Australian Planning Commission's strategic planning document *'Directions 2031 and Beyond'*. The adoption of the DSP and later DSP2 2012 served to solidify the recognition of the Cockburn Coast's potential and identifies a number of key drivers and opportunities that underpin the vision and intent of the DSP and DSP2. Following an amendment (Amendment 89) to the *City of Cockburn Town Planning Scheme No. 3* (The Scheme), which aligns the City of Cockburn's (the City) planning framework with that as proposed in the DSP and DSP2, local structure plans were produced for the Robb Jetty LSP and Emplacement LSP areas which establishes a development agenda and expands on the foundations of the DSP and DSP2.

These design guidelines bring to fruition a vision established and carried forward through a number of strategic planning documents and processes.

#### IV Approach

The DSP established a vision which remains relevant to the ongoing planning of Cockburn Coast:

***"To create a vibrant, landmark destination that is connected, integrated, diverse and accessible."***

The vision seeks to create a place that offers new and exciting living, employment and recreation opportunities, whilst providing an appropriate level of compatibility and support for adjoining residents and existing enterprises in the area. These design guidelines are set to establish this vision by creating a sustainable community that celebrates the area's past as well as taking on creative ideas, innovation and development. Cockburn Coast will be an easily accessible place, with an integrated transit system offering contemporary lively cafes, restaurants, shops, residential and commercial areas, tourism, cultural and recreation activities.

Integral to the vision of Cockburn Coast is the intention to establish a new benchmark for sustainable urban development. This means creating a place where people not only want to live and work today, but also in the future. Sustainable communities cater to the different needs of all its residents; they are safe and inclusive and offer equality of opportunity, they are sensitive to their environment and contribute to a high quality of life.

#### V Objectives

The development of Cockburn Coast is guided by a number of key objectives or drivers which will bring to fruition the vision of a sustainable landmark destination. These objectives have influenced the preparation of the design guidelines and underpin the purpose of the design guidelines, being to:

- \_create a hierarchy of coastal nodes providing for the needs of local residents and visitors alike;
- \_create physical and emotional links between the urban environment and the coast allowing the coastal experience to translate into the urban setting;
- \_provide attractive, pedestrian-oriented streets and public spaces that create an environment for positive community engagement and business exchange;
- \_enable buildings and public realm to engage with pedestrians and facilitate a comfortable and safe urban environment;
- \_allow for activation at ground level by retail and hospitality uses in key streets identified by the Local Structure Plans;
- \_optimise residential development potential whilst maintaining the intended character of the Cockburn Coast;
- \_minimise the impact of car parking on the pedestrian experience and quality of the public realm;
- \_create a sustainable environment that allows for the implementation of green infrastructure; and
- \_promote the use of sustainable modes of transport and a health way of living through active engagement with the urban environment.

#### VI Purpose

These design guidelines have been prepared to guide development within the Robb Jetty LSP and Emplacement LSP areas under the Scheme. Implementation of the guidelines will ensure the realisation of Cockburn Coast as an urban environment providing both local and district centre activity centres.

## VII Design Guideline Policy Area

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These design guidelines apply to the area of land within the Robb Jetty LSP and Emplacement LSP, henceforth referred to as the policy area. The policy area is bound by:

- \_Rollinson Road to the north,
- \_South Fremantle Power Station and the Western Power Switchyard to the south,
- \_Beeliar Regional Park to the east, and
- \_The foreshore reserve to the west.



Figure 02\_Design Guidelines Policy Area

## VIII Relationship to Relevant Planning Documents

The design guidelines are adopted under the provisions of section 2.5 Procedures for Making or Amending a Local Planning Policy of the Scheme. The provisions of these design guidelines vary the requirements of the State Planning Policy 3.1 Residential Design Codes (R-Codes). Where these design guidelines are silent the provisions of the R-Codes and relevant local planning policies apply.

These design guidelines should be read in conjunction with the Scheme, the Robb Jetty LSP, the Emplacement LSP and the R-Codes. In determining any application for development approval, the City will utilise these design guidelines in conjunction with the Scheme and policies.



## IX Relationship to the Robb Jetty LSP and Emplacement LSP

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The Robb Jetty LSP and Emplacement LSP set out a number of development objectives relating to the DSP2 redevelopment area. In particular they establish land use, movement, activity, urban form and resource enhancement development standards to ensure Cockburn Coast operates as an effective urban environment.

These design guidelines build upon both LSPs and provide more detailed guidance on development standards in the form of an adopted local planning policy.

## X Guideline Framework

The detailed design guidelines contained in the General Provisions section are set out with the following framework:

**Design Objective:** A statement outlining the design philosophy and intent of the assessment criteria. It is mandatory for development to meet the design objective.

**Assessment Criteria:** Standards setting out specific criteria which will satisfy the related design objective. Compliance with the applicable assessment criteria will achieve the design objective. However individual criteria are not mandatory and alternative solutions for complying with the design objective will be considered on a performance basis subject to supporting evidence.

The typology specific section of the design guidelines contains character statements. The character statements guide both the design objective and assessment criteria and as such, all development shall be consistent with the relevant character statement.

## XI Discretion

An important provision within the design guidelines is the opportunity for the applicant or owner to meet the design objective through an alternative solution.

The City may approve a development application ~~(DA)~~ where the applicant or owner has departed from the recommended assessment criteria. Variations may be considered where, in the City's opinion, the applicant or owner has demonstrated that the alternative solution is consistent with the Robb Jetty LSP or Emplacement LSP where relevant and meets the design objective. Variations will be considered where a proposal does not include an affordable housing component, but will be considered more favourably where it does.

Where a development proposal is determined to be inconsistent with a design objective in a manner that may impact on the public realm or adjoining properties then the proposal may be refused or referred to Council for determination.

Where the applicant or owner has provided a sufficient affordable housing component, a relaxation of the assessment criteria may be considered where the alternative solution is consistent with the relevant LSP and meets the design objective. These design guidelines provides further guidance on those criteria considered suitable for variation.

Each application for development approval will be assessed on an individual basis and the approval of an alternative solution will not set a precedent for other developments.

## XII Definitions

**Noise Sensitive Premises** (as defined in the Environmental Protection (Noise) Regulations 1997) includes premises occupied solely or mainly for residential or accommodation purposes, and premises used for the purpose of a hospital, sanatorium, educational establishment, public worship, aged care or child care.

**Commercial Laneway** includes any laneway within the mixed use or activity centre typology areas as set out by these design guidelines.

All definitions included in the R-Codes are applicable to land affected by these Design Guidelines.



### XIII Development Process

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Owners, developers and/or agents are encouraged to arrange pre-application meetings with the City's Planning Department prior to lodgement of a formal development application~~DA~~. Once a development application~~DA~~ is lodged, it will be assessed by the City to verify it meets all applicable design objectives and assessment criteria.



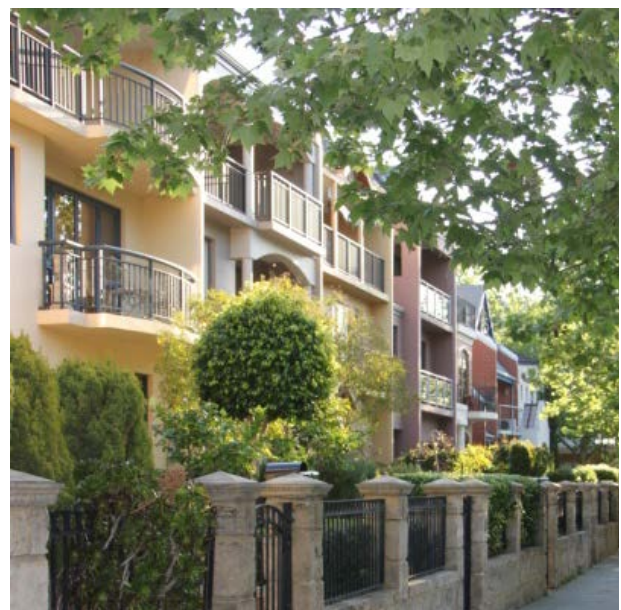
Activity Centre – Main Street Typology



Mixed Use – Cockburn Road Typology



High Density Residential Typology



Medium Density Residential Typology



## Part One\_Private Realm

### Built Form Typologies Introduction

The policy area is divided into a number of built form typologies each with their own distinct character and function. There are also a number of landmark and gateway sites identified by the built form typology location plan. These sites are to be developed with a diverse and active facade to facilitate way finding and reflect the natural hierarchy and land use of the area.

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#### *Activity Centre - Main Street Typology*

Development in this area addresses and activates the identified pedestrian oriented “main” street whilst a high quality public realm creates a comfortable place in which locals meet and conduct business. This area provides a key link between the ocean and urban environment as well as providing for the retail and local service needs of the local community.

#### *Mixed Use - Cockburn Road Typology*

A range of retail and commercial functions complemented by residential development are to be accommodated within this mixed use area. The presence of Cockburn Road informs the scale and built form of development and necessitates the promotion of an active ground floor.

#### *High Density Residential Typology*

The most intensely developed residential typology to afford the greatest access to the proposed bus rapid transit system. High density residential development is to create a new skyline in Cockburn Coast.

#### *Medium Density Residential Typology*

Providing a mix of housing opportunities near the Activity Centre, this typology will feature soft landscape public realm and contemporary urban development ranging from terrace housing to medium scale apartment style buildings.

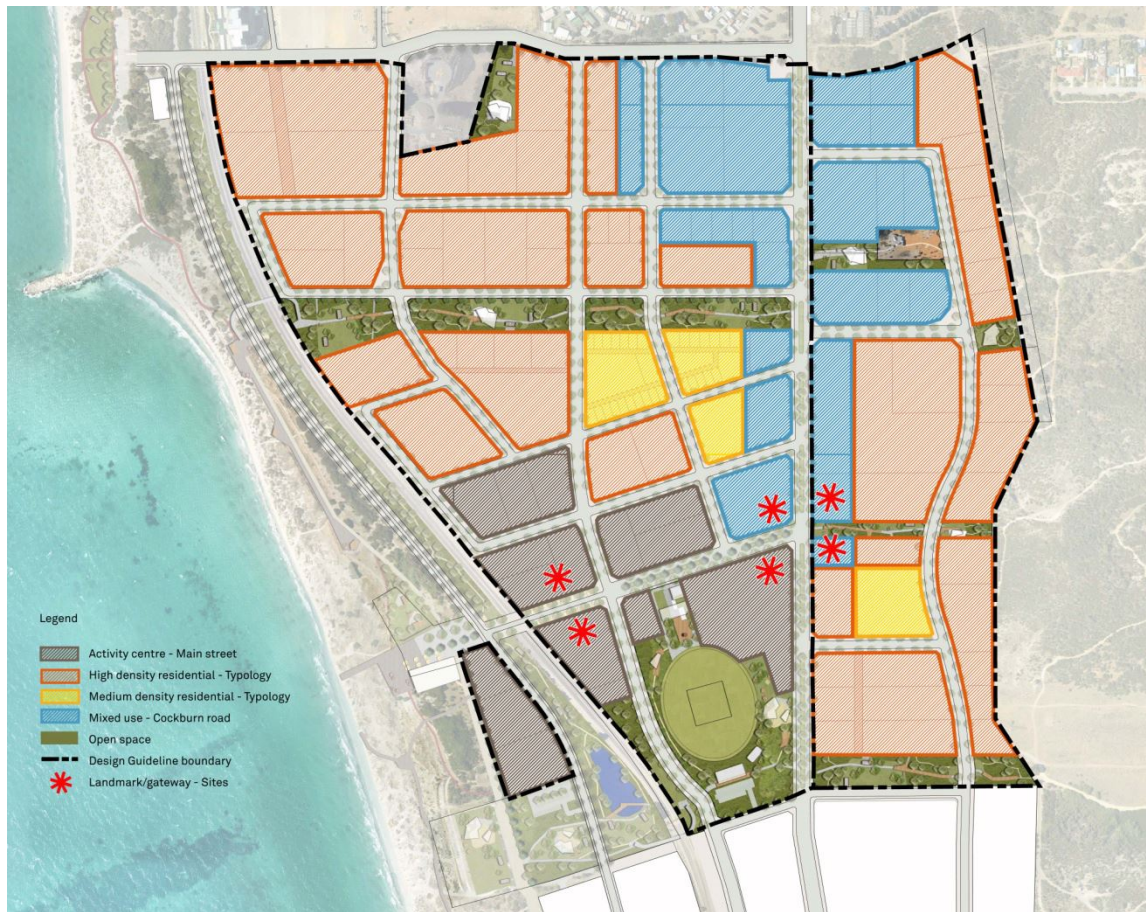


Figure 03\_Built Form Typologies

## 1. Typology Specific Guidelines

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### 1.1 Activity Centre – Main Street Typology

The activity centre typology is primarily a place for local residents and businesses, a walkable village that is intimate in scale and 'soft' in character. The beach comes to the main street and a variety of small but connected public spaces offer a range of experiences from the quiet to the communal. Buildings and land use will facilitate the creation of a central shopping and activity zone resulting in a walkable community hub.

The Main Street provides a convenient and inviting local shopping experience intended to be serviced by a rapid bus transit system. Street trading and active retail is concentrated in the western portion of the area creating a vibrant community hub. A diverse and contiguous streetscape will be developed ~~homing~~ civic, business and retail services to ensuring a suitable business mix. The oval and park within the activity centre typology represents the traditional village green and is therefore the focus of active recreation at Cockburn Coast. It is a place to be shared harmoniously by many for diverse purposes.

The built form is encouraged to take advantage of the abundant natural assets and create a comfortable outdoor environment that encourages social interactions in a relaxed and personal environment. Future built form should embody the feeling of seamless transition, from indoor to outdoor, from formal to informal, from exposed to protected. Respectful of nature, built form should reflect the natural characteristic of the vegetation and landscape.

Buildings generally of 5 to 8 storeys in height will promote a pedestrian friendly place through podium style built form and a focus on ground floor activation. Development embodies a warm architectural finish through the use of natural materials, whilst street awnings, wide footpaths and soft landscaped edges create a sense of intimacy and shelter pedestrians.

Opportunities for laneway development enhance and celebrate the distinctive environment by reflecting the neighbourhood character whilst allowing for it to be developed as a secondary small street. Laneways containing commercial uses will be characterised by small scale tenancies, evolving over time to provide an intimate and unique experience.

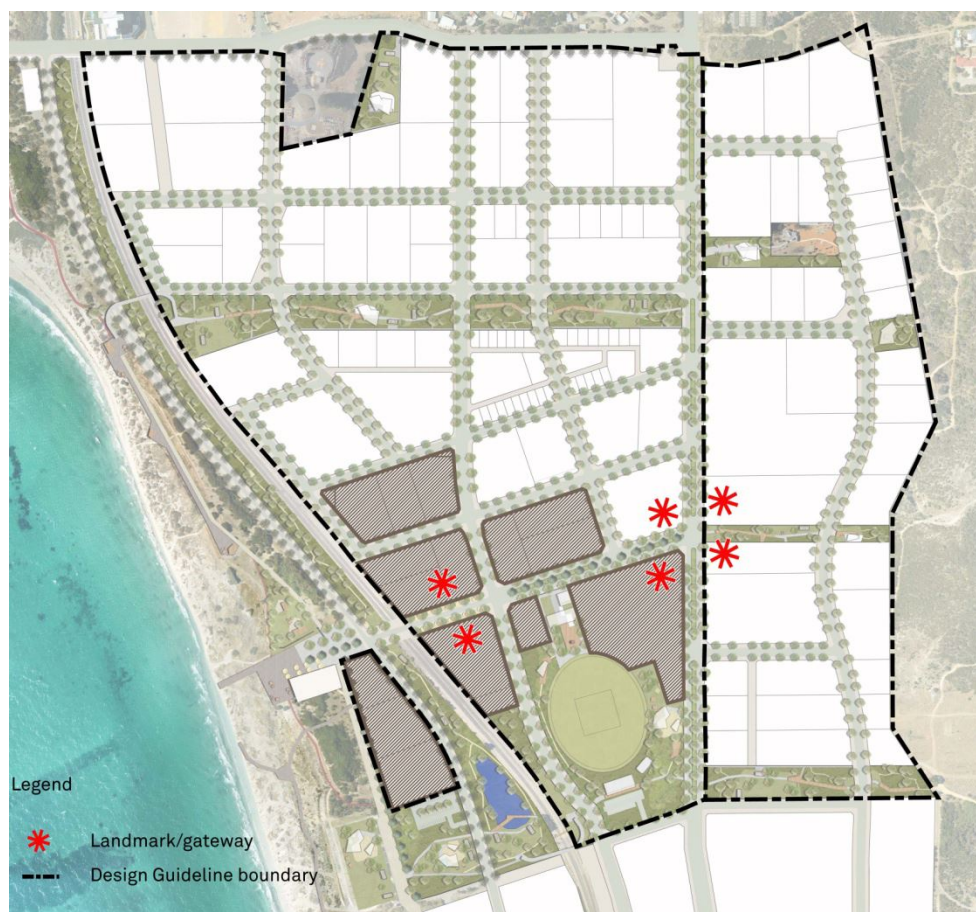


Figure 04\_Activity Centre Built Form Typology



### 1.1.1 Building Setbacks

#### Design Objective

- I. Building setbacks create tightly framed streetscapes and public open spaces.
- II. Building setbacks help create highly urban streetscapes.
- ~~I. are related to the intended character and function of an area and its streetscape quality. Setbacks shall create a pedestrian scaled environment and consistent streetscape rhythm.~~

#### Assessment Criteria

- i. Building setbacks are to be in accordance with the following table.

Setbacks for	Street Setback (minimum and maximum)	Side Setback (minimum)	Rear Setback (minimum)
Levels 1-5	Nil	Nil	Nil
Levels 6+	5.0 metres to wall and 2.0 metres to balconies	3.0 metres	3.0 metres

Table 01\_ Building Setbacks for Activity Centre

- i. Buildings shall be setback 4.0 metres from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area).
- ii. Projections are permitted within the 4.0 metre setback to public parkland to maximum of 2 metres into the setback area.
- iii. Balconies will be supported within the nil setback on levels 1-5 where a substantial facade is provided to ensure a continuous built form.
- ~~ii.~~

### 1.1.2 Building Articulation

#### Design Objective

- I. To ensure that building facades add significantly positively to the public realm and its interest. Building articulation will~~A vibrant and modern design aesthetic will~~ encourage interaction with the street and passive surveillance of adjacent spaces.
- II. To promote a pedestrian scale of buildings at street level.
- III. The building design shall demonstrate an appropriate level of articulation to avoid excessive building massing and bulk appearing excessive.
- ~~III.~~ Building articulation will express a vibrant and modern design aesthetic.

#### Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm.
- ii. For commercial street level frontages a minimum of 80% of the frontage shall be glazed. For the street frontage for all upper floors a minimum of 40% of the frontage shall be glazed.
- iii. Mixed use buildings should provide separate entries for non-residential and residential uses for legibility of pedestrian access.
- iv. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas.
- v. Corner buildings are to address both frontages through the provision of:
  - a) distinct roof form at corners;
  - b) variation in materials and colours; and
  - c) varied balcony treatments.

### 1.1.3 Building Levels

#### Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised.
- ~~II.~~ To allow for the safe use of ceiling fans for cooling.

#### Assessment Criteria

- i. Floor to floor heights on the ground floor should be 4.5 metres to allow for commercial use of the ground floor.
- ii. All other floors shall maintain a 3.1 metre floor to floor height for residential use and a 3.6 metre floor to floor height for commercial use.
- iii. The ground floor should be flush with the adjacent footpath at the boundary.
- iv. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above.

#### 1.1.4 Awnings

##### Design Objective

- I. To encourage a pedestrian scale of development.
- II. To provide shelter from environmental conditions.
- III. To encourage a seamless flow of the use and function of a building from internal to external.
- ~~III, IV.~~ To maintain a safe separation between passing traffic and awnings.

#### Assessment Criteria

- i. Awnings over footpaths are to be provided for no less than 80% of the primary and secondary street frontage. This requirement does not apply to laneways.
- ii. The vertical clearance of awnings shall be consistent and generally 3.2 metres from pavement level.
- iii. Awnings shall project ~~up to~~ 3.5 metres from the building line except where this resulting in a setback between but not closer than 0.6 metres to the awning and the outer edge of the road pavement of less than .06 metres.
- iv. Adjoining awnings are to form continuous coverage over the footpath.
- v. Awnings are to be provided with non structural veranda posts along the Robb Jetty Main street. In this respect awnings are to be suspended by cantilevered construction and not use load bearing posts.

#### 1.1.5 Building Height

##### Design Objective

- I. Building heights help create a compact urban built environment.
- II. Consistent building heights create a recognisable urban character.
- III. Building heights mean the Activity Centre Typology area is highly visible from a distance.
- IV. Building heights do not visually overwhelm the streetscape.
- V. Building heights avoids continual overshadowing of the streetscape.
- ~~I. will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area.~~
- ~~II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment.~~
- ~~III. The built form shall minimise overshadowing to adjacent streets and public spaces.~~

#### Assessment Criteria

- i. Building heights shall be in accordance with the Building Height Plan. (Figure 16)
- ii. Development shall be a minimum of three storeys.

#### 1.1.6 Building Materials

##### Design Objective

- I. To encourage a style of development that is consistent with the coastal location.
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast.
- III. To foster a sense of place through an identifiable character and style of development.

#### Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete).
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged.
- iii. Painted finishes and rendered textures over concrete panels are not permitted.
- iv. The use of natural materials such as stone, timber, and other such natural products is encouraged in both interior and exterior finishes.

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#### 1.1.7 Open Space

##### Design Objective

- I. To ensure that development provides a ~~an appropriate attractive and engaging~~ interface with the public open space.
- II. To maximise the potential for passive surveillance

#### Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment.
- ii. The interface between private lots and the public open space may be fenced to a maximum height of 1.2 metres from natural ground level, but must be visually permeable above a height of 1.0 metres above natural ground level.

### 1.1.8 Landmark Sites

#### Design Objective

- I. To encourage a sense of place and identity.
- II. To increase the legibility of place.
- III. To marcate the natural hierarchy of an area by identifying those places which are of significance.

#### Assessment Criteria

- i. Sites in key locations have been nominated as landmark sites as shown in Figure 04 Built Form Typologies shall:
  - a. Promote prominent architectural form on corner elements to provide a reference point in the built form and landscape.
  - b. Encourage additional height elements where appropriate to create a point of difference with the balance of the development area and demarcate points of entry and prominence.
  - c. Variations to setback requirements will be considered in order to create prominent feature elements.

### 1.1.9 Fencing

#### Design Objective

- I. To ensure that fencing does not detract from the function and appearance of the streetscape.

#### Assessment Criteria

- i. Fencing is not permitted forward of the building line.

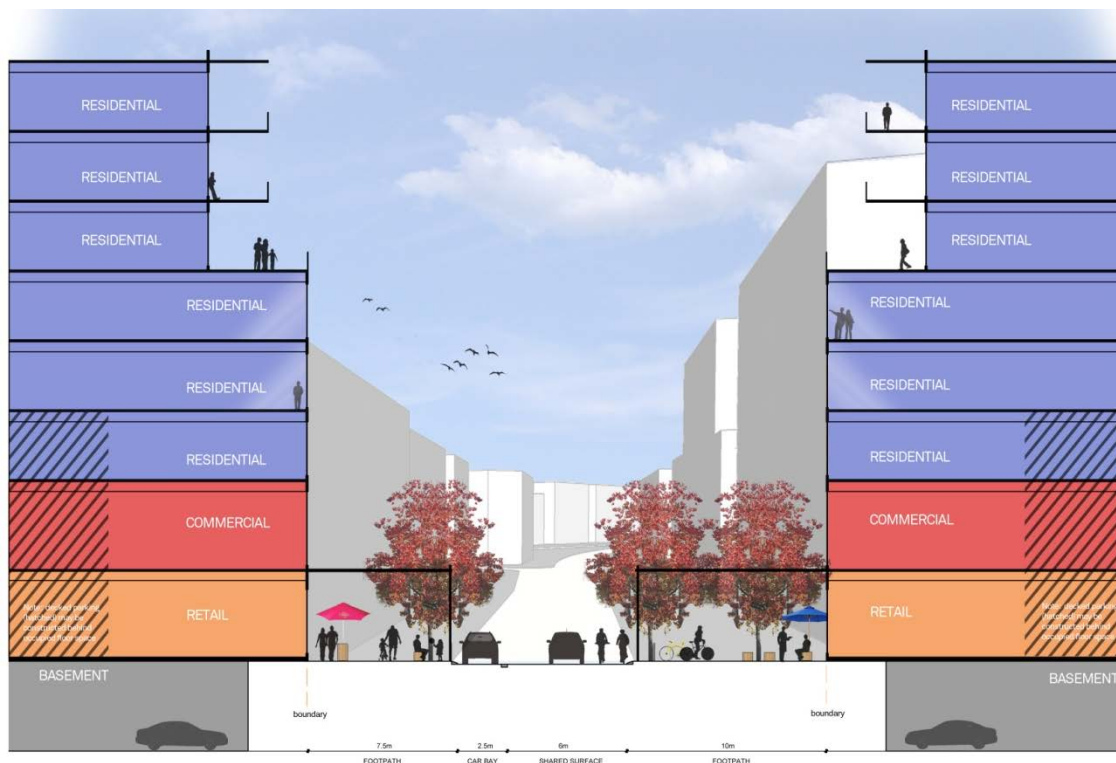


Figure 05\_Typical cross section for activity centre development

## 1.2 Mixed Use – Cockburn Road Typology

14

Cockburn Road is the main arterial road through Cockburn Coast and policy area. Cockburn Road will be the focus of a mixed use form of development allowing for commercial, residential and retail uses. An active ground floor through retail and commercial uses will be encouraged with primarily residential development occupying the upper levels. The impact of the busy Cockburn Road will be softened by landscaping and an active footpath. Alfresco dining opportunities will be encouraged and facilitated by the built forms and land uses.

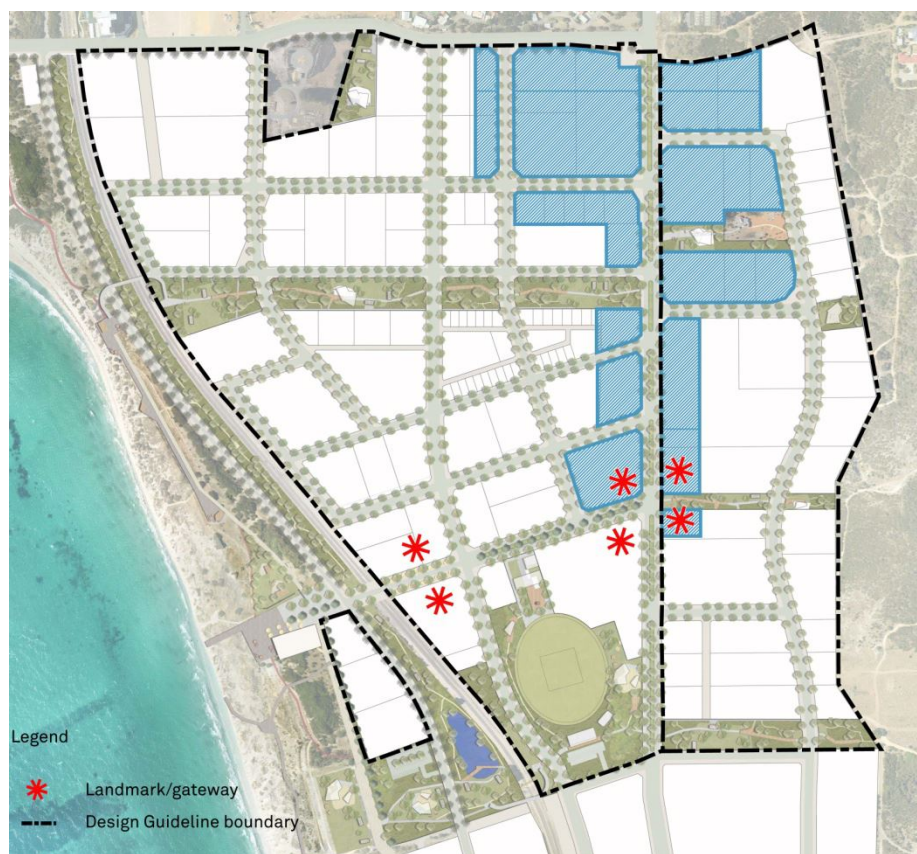


Figure 06\_Mixed Use built form typology

### 1.2.1 Building Setbacks

Design Objective

~~III. Building setbacks tightly framed streetscapes and public open spaces.~~

~~IV. Building setbacks help create highly urban streetscapes.~~

~~I. are related to the intended character and function of an area and its streetscape quality. Setbacks shall create a pedestrian-scaled environment and consistent streetscape rhythm.~~

Assessment Criteria

- i. Building setbacks are to be in accordance with the following table.

Setbacks for	Street Setback (minimum and maximum)	Side Setback (minimum)	Rear Setback (minimum)
Levels 1-3	Nil	Nil	Nil
Levels 4+	5.0 metres to wall and 2.0 metres to balconies	3.0 metres	3.0 metres

Table 02\_ Building Setbacks for Mixed Use development



- ii. Buildings shall be setback 4.0 metres from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e building shall cantilever over the outdoor living area).

iii. Setbacks-Projections are permitted within the 4.0 metre setback to public parkland to maximum of 2.0 metres into the setback area.

iv. Balconies will be supported within the nil setback on levels 1-5 where a substantial facade is provided to ensure a continuous built form.

iii-v.

### 1.2.2 Building Articulation

#### Design Objective

- I. To ensure that building facades add positively significantly to the public realm and its interest. Building articulation will A vibrant and modern design aesthetic will encourage interaction with the street and passive surveillance of adjacent spaces.

- II. To promote a pedestrian scale of buildings at street level.

III. The building design shall demonstrate an appropriate level of articulation to avoid excessive building massing and bulk appearing excessive-

III-IV. Building articulation will express a vibrant and modern design aesthetic

#### Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm.
- ii. For commercial street level frontages a minimum of 80% of the frontage shall be glazed. For the street frontage for all upper floors a minimum of 40% of the frontage shall be glazed.
- iii. Mixed use buildings should provide separate entries for non-residential and residential uses for legibility of pedestrian access.
- iv. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas.
- v. Corner buildings are to address both frontages through the provision of:
  - d) distinct roof form at corners;
  - e) variation in materials and colours; and
  - f) varied balcony treatments.

### 1.2.3 Building Levels

#### Design Objective

- I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised.

I-II. To allow for the safe use of ceiling fans for cooling.

#### Assessment Criteria

- i. Floor to floor heights on the ground floor should be 4.5 metres to allow for commercial use of the ground floor.
- ii. All other floors shall maintain a 3.1 metre floor to floor height for residential use and a 3.6 metre floor to floor height for commercial use.
- iii. The ground floor should be flush with the adjacent footpath at the boundary.
- iv. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above.

### 1.2.4 Awnings

#### Design Objective

- I. To encourage a human scale of development.

- II. To provide shelter from environmental conditions.

III. To encourage a seamless flow of the use and function of a building from internal to external.

### III-IV. To maintain a safe separation between passing traffic and awnings.

#### Assessment Criteria

- i. Awnings over footpaths are to be provided for no less than 80% of the primary and secondary street frontages. This requirement does not apply to laneways.
- ii. The vertical clearance of awnings shall be consistent and generally 3.2 metres from pavement level
- iii. Awnings shall project 3.5 metres from the building line except where this resulting in a setback between to the awning and the outer edge of the road pavement of less than .06 metres.
- ~~iii-iv. Awnings shall project up to 3.5 metres but not closer than 600mm to the outer edge of the road pavement.~~
- ~~iv-v. Adjoining awnings are to form continuous coverage over the footpath.~~
- ~~v-vi. Any veranda post provided to an awning shall be non structural. In this respect awnings are to be suspended by cantilevered construction and not use load bearing posts.~~

### 1.2.5 Building Height

#### Design Objective

- I. Building heights help create a compact urban built environment.
- II. Consistent building heights create a recognisable urban character.
- III. Building heights do not visually overwhelm the streetscape.
- IV. Building heights avoids continual overshadowing of the streetscape.
- ~~I. will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area.~~
- ~~II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment.~~
- ~~III. The built form shall minimise overshadowing to adjacent streets and public spaces.~~

#### Assessment Criteria

- i. Building shall be in accordance with the Building Height Plan.(Figure 16)
- ii. Development shall be a minimum of three storeys.

### 1.2.6 Building Materials

#### Design Objective

- I. To encourage a style of development that is consistent with the coastal location.
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast.
- III. To foster a sense of place through an identifiable character and style of development.

#### Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete).
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged.
- iii. Painted finishes and rendered textures over concrete panels are not permitted.
- iv. The use of natural materials such as stone, timber, and other such natural products is encouraged in both interior and exterior finishes.

### 1.2.7 Open Space

#### Design Objective

- I. To ensure that development provides an attractive and engaging appropriate interface with the public open space.
- II. To maximise the potential for passive surveillance.

#### Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment.
- ii. The interface between residential development and the public open space may be fenced to a maximum height of 1.2 metres from natural ground level, but must be visually permeable above a height of 1.0 metres above natural ground level.

### 1.2.8 Landmark Sites

#### Design Objective

- I. To encourage a sense of place and identity.
- II. To increase the legibility of place.
- III. To marcate the natural hierarchy of an area by identifying those places which are of significance.

#### Assessment Criteria

- i. Sites in key locations have been nominated as landmark sites as shown in Figure 04 Built Form Typologies.  
Development on Landmark Sites shall:
  - a. Promote prominent architectural form on corner elements to provide a reference point in the built form and landscape.
  - b. Encourage additional height elements where appropriate to create a point of difference with the balance of the development area and demarcate points of entry and prominence.
  - c. Variations to setback requirements will be considered in order to create prominent feature elements.

### 1.2.9 Fencing

#### Design Objective

- I. To ensure that fencing does not detract from the function and appearance of the streetscape.

#### Assessment Criteria

- i. Fencing is not permitted forward of the building line.

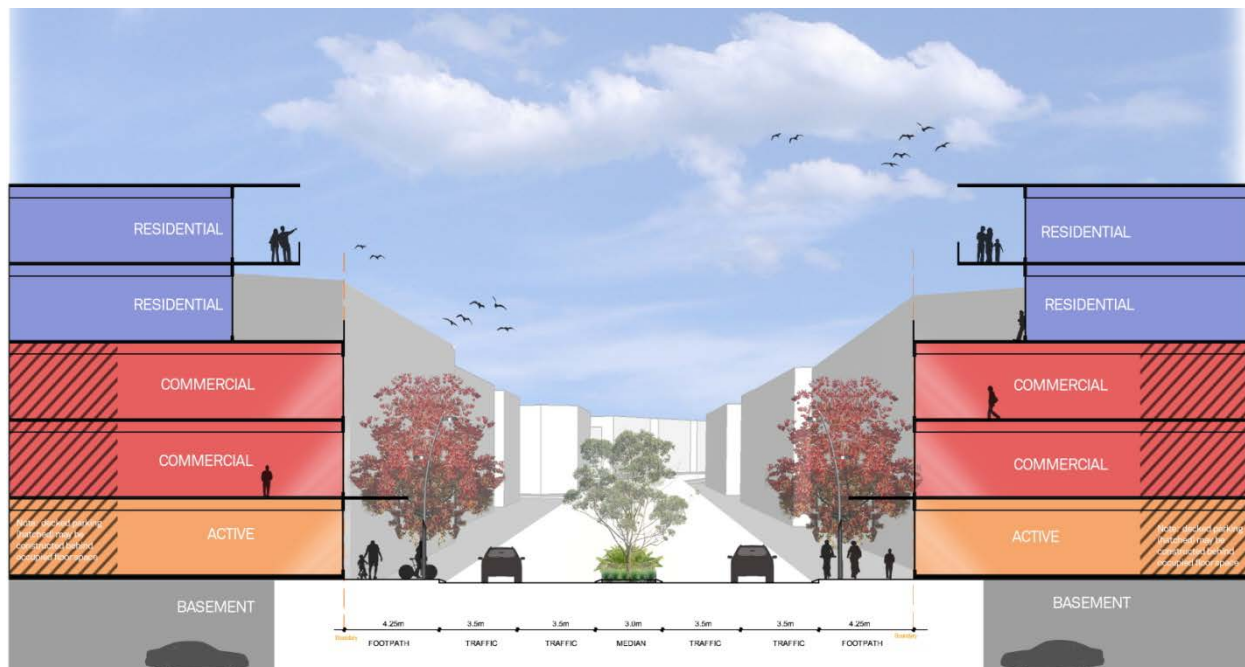


Figure 07\_ Typical cross section for mixed use development



Mixed use will encompass active street edges that create a comfortable pedestrian environment

### **1.3 High Density Residential Typology**

High density housing opportunities along the Emplacement escarpment and within the Robb Jetty LSP area alike will create a new skyline for the Cockburn Coast. A manufactured horizon line of apartment buildings six to eight storeys in height will offer the opportunity for a new architectural topography and an integrated landscape of nature and built form. Residents will enjoy the expansive views but also the sense of containment and grounding in the environment. Facades and balconies shade and veil occupants whilst the ground level public realm is internalised and places focus on the residential communities' common interest.

Landscaped front setbacks and tree lined verges will combine to create a soft and comfortable urban setting for apartment buildings. Pocket parks and integrated greenery with built form create a calming natural feel throughout the area despite the intensity of development, acting as a backyard space and providing a link to the coast.



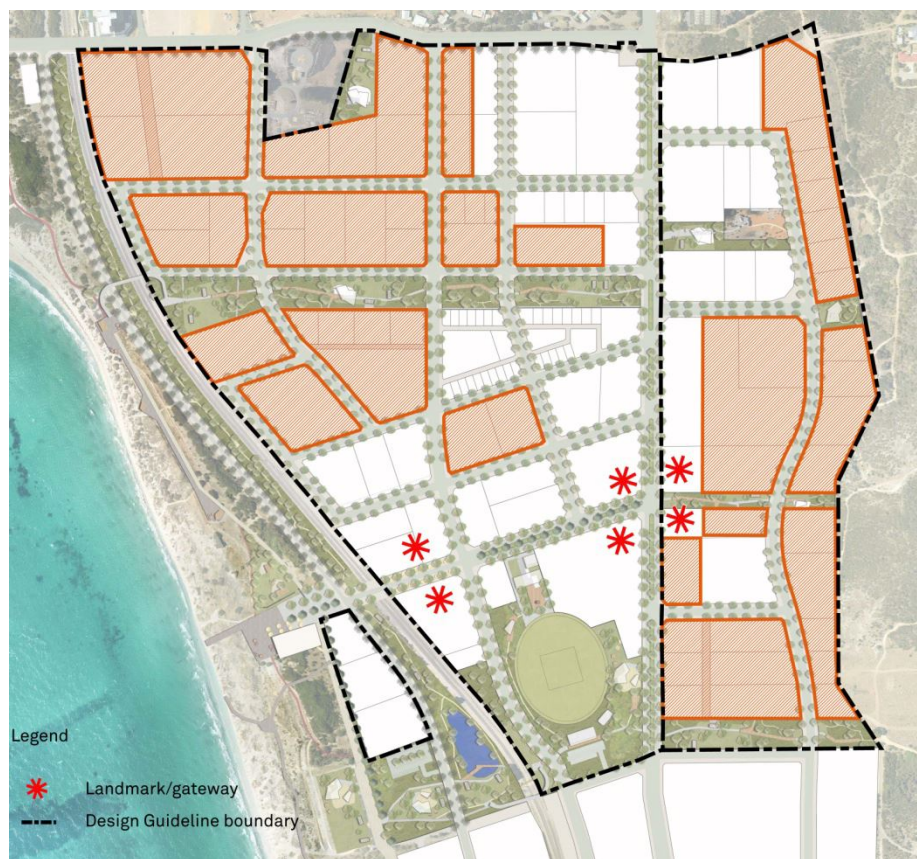


Figure 8\_High Density built form typology

### 1.3.1 Building Setbacks

#### Design Objective

- I. Building setbacks- frame streetscapes and public open spaces.  
~~Building setbacks accommodate landscaping which slightly widen and softens the streetscape, are related to the intended character and function of an area and its streetscape quality. Setbacks shall create a pedestrian scaled environment and consistent streetscape rhythm.~~

#### Assessment Criteria

- i. Building setbacks are to be in accordance with the following table.

Setbacks for	Street Setback (minimum and maximum)	Side Setback (minimum)	Rear Setback (minimum)
Levels 1-3	3.0 metres	Nil	Nil
Levels 4+	5.0 metres to wall Balconies may project into the front setback area.	3.0 metres	3.0 metres

Table 03\_ Building Setbacks for high density residential development

- ii. Buildings shall be setback 4.0 metres from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area).
- iii. Setbacks Projections are permitted within the 4.0 metre setback to public parkland to maximum of 2.0 metres into the setback area.

### 1.3.2 Building Articulation

#### Design Objective

- I. To ensure that building facades add significantly positively to the public realm and its interest. Building articulation will ~~A vibrant and modern design aesthetic will~~ encourage interaction with the street and passive surveillance of adjacent spaces.
- II. To promote a pedestrian scale of buildings at street level.
- III. The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive. ~~excessive building massing and bulk.~~
- III-IV. Building articulation will express a vibrant and modern design aesthetic

#### Assessment Criteria

- i. Permanent blank walls are not permitted to any street frontage. Major openings are required to provide for surveillance and interaction with the public realm.
- ii. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas.
- iii. Built form is to address parks, pedestrian access ways and in particular laneways by providing windows, balconies and suitable facade articulation facing these areas. ~~Built form is to address parks, pedestrian access ways and in particular laneways by providing windows and elevations elements facing these areas.~~  
These elevations are to match the design quality of the dwellings primary street elevation.
- iv. Corner buildings are to address both frontages through the provision of:
  - a. distinct roof form at corners;
  - b. variation in materials and colours; and
  - c. varied balcony treatments.

### 1.3.3 Building Levels

#### Design Objective

I. To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised.

II. To allow for the safe use of ceiling fans for cooling.

#### Assessment Criteria

- i. All development shall maintain a minimum floor to floor height of 3.1 metres.
- ii. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above.
- iii. Where residential dwellings are proposed at on the ground floor adjacent to a street or public open space, a grade separation ~~of from~~ 0.5 metres ~~to and~~ 1.2 metres between the finished floor level of the ground floor and the adjacent street or public open space is encouraged in order to create a visual distinction between the public and private space.

### 1.3.4 Building Height

#### Design Objective

- I. Building heights will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area.
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment.
- III. The built form shall minimise overshadowing to adjacent streets and public spaces.

#### Assessment Criteria

- i. Building shall be in accordance with the Building Height Plan. (Figure 16)
- ii. Development shall be a minimum of three storeys.

### 1.3.5 Building Materials

#### Design Objective

- I. To encourage a style of development that is consistent with the coastal location.
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast.
- III. To foster a sense of place through an identifiable character and style of development.

#### Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete).
- ii. Moulded textures imprinted in the external surfaces of any concrete panels are encouraged.
- iii. Painted finishes and rendered textures over concrete panels are not permitted.
- iv. The use of natural materials such as stone, timber, and other such natural products is encouraged in both interior and exterior finishes.

### 1.3.6 Open Space

#### Design Objective

- I. To ensure that development provides an appropriate interface with the public open space.
- II. To maximise the potential for passive surveillance

#### Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable pedestrian environment.

### 1.3.7 Fencing

#### Design Objective

- I. To ensure that the provision of fencing does not detract from the function and appearance of the streetscape.

#### Assessment Criteria

- i. The interface between private lots and the public open space may be fenced to a maximum height of 1.2 metres from natural ground level, but must be visually permeable above a height of 1.0 metre above natural ground level.

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### 1.3.8 Landscaping

23

#### Design Objective

- I. To ensure an attractive streetscape environment.
- II. To aid the sustainability of a building through the provision permeable surface.

#### Assessment Criteria

- i. The front setback area shall include provision for elements of ~~consist of a minimum of 50%~~ soft landscaping.
- ii. In ground landscaping is preferred over shallow landscaping above basements.
- iii. Paving that is contiguous with foot paths and other paving in the public realm shall be of the same style and materials, matching exactly wherever possible.

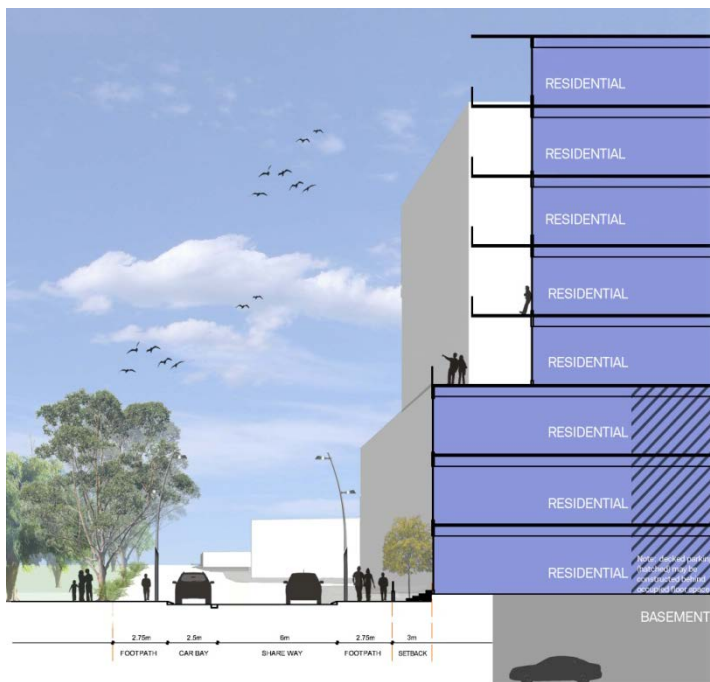


Figure 9\_Typical cross section of ~~high-density residential development abutting public open space~~ (relabel correctly)



Figure 10\_ Typical cross section of high density residential directly adjoining public open space

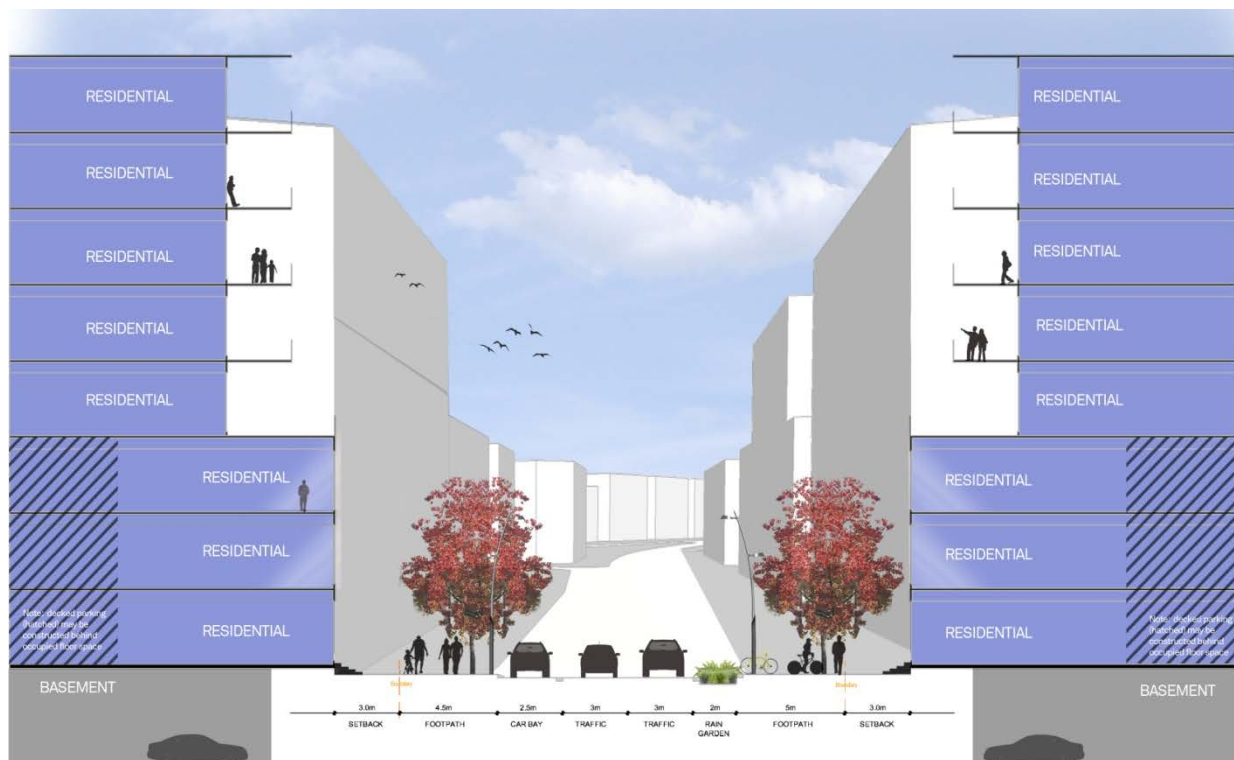


Figure 11\_ Typical cross section for high density residential development



High density Residential Development showing the use of natural materials in the facade and a provision of high quality building articulation in keeping with the objectives of these design guidelines

#### 1.4 Medium Density Residential Typology

The Robb Jetty area provides an important medium density housing area. Leafy streets and small softly landscaped front setbacks will combine to create a comfortable urban setting for contemporary apartment buildings. Future built form will embody a seamless transition from indoor to outdoor, from formal to informal, from exposed to protected. Built form will be respectful of nature and reflect the natural characteristics of the vegetation and landscape within Cockburn Coast.

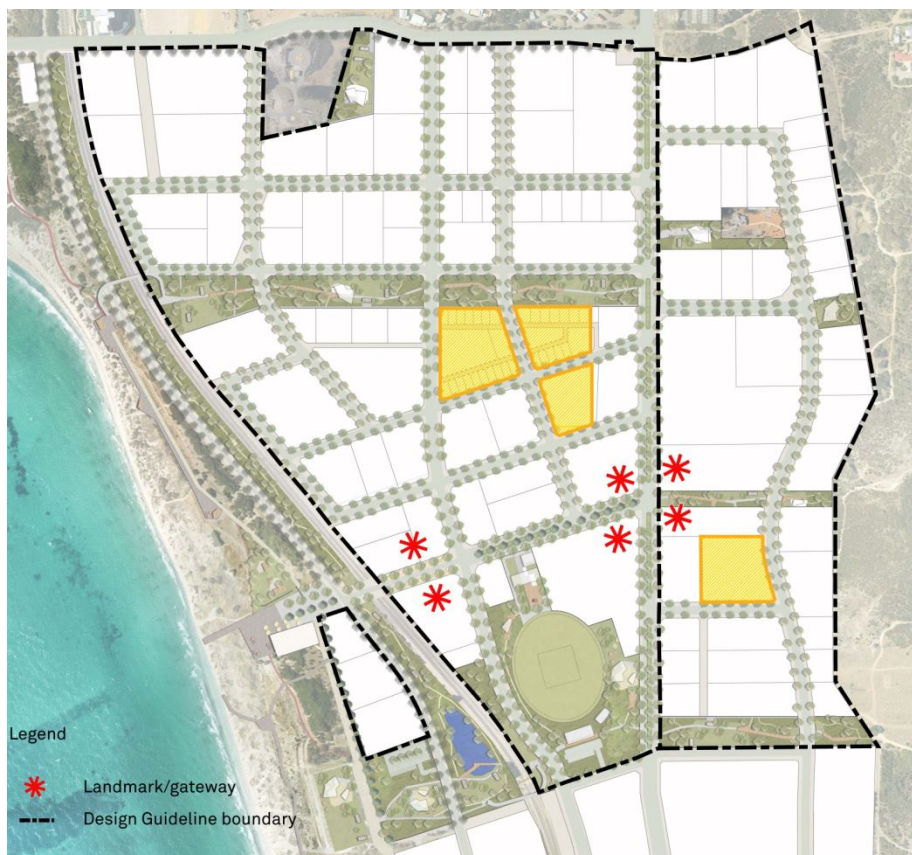


Figure 12\_Medium Density built form typology

##### 1.4.1 Building Setbacks

## Design Objective

- ~~I. Building setbacks are related to the intended character and function of an area and its streetscape quality create intimate streetscapes.~~
- ~~II. Building setbacks accommodate landscaping which slightly widen and softens the streetscape~~
- ~~I. Setbacks shall create a pedestrian scaled environment and consistent streetscape rhythm.~~

## Assessment Criteria

- i. Building setbacks are to be in accordance with the following table.

<b>Setbacks for</b>	<b>Street Setback</b> (minimum and maximum)	<b>Side Setback</b> (minimum and maximum)	<b>Rear Setback</b> (minimum)
Levels 1-3	2.0 metres	Nil for the first 10.0 metres of development	Nil
Levels 4+	5.0 metres to wall and 2.0 metres to balconies		

Table 04\_ Building Setbacks for medium density residential development



- ii. Buildings shall be setback 4.0 metres from any boundary adjoining public parkland. This setback area shall include space for landscaping and if necessary an outdoor living area. Where additional outdoor living area is to be provided, the additional outdoor living area shall be absorbed into the building space (i.e. building shall cantilever over the outdoor living area).
- iii. ~~Setbacks-Projections~~ are permitted within the 4.0 metre setback to public parkland to maximum of 2.0 metres into the setback area.

#### 1.4.2 Building Articulation

##### Design Objective

- I. To ensure that building facades add ~~positively~~significantly to the public realm and its interest. Building articulation. A vibrant and modern design aesthetic will encourage interaction with the street and passive surveillance of adjacent spaces.
- II. To promote a pedestrian scale of buildings at street level.
- ~~III.~~ The building design shall demonstrate an appropriate level of articulation to avoid building bulk appearing excessive. ~~excessive building massing and bulk.~~
- ~~III.~~ Building articulation will express a vibrant and modern design aesthetic.

##### Assessment Criteria

- i. The facade detail may be simplified on loading areas, parapet walls and walls to 'back of house' areas.
- ii. Built form is to address parks, pedestrian access ways and in particular laneways by providing windows, balconies and suitable facade articulation facing these areas. ~~Built form is to address parks, pedestrian access ways and in particular laneways by providing windows and elevations elements facing these areas.~~ These elevations are to match the design quality of the dwellings primary street elevation.
- iii. Balconies are encouraged but shall not run continuously along the facade. Separate individual balconies are appropriate.
- iv. Corner buildings are to address both frontages through the provision of:
  - a. distinct roof form at corners;
  - b. variation in materials and colours; and
  - c. varied balcony treatments.

#### 1.4.3 Roof Form

##### Design Objective

- I. The roof form should be designed as a contemporary and integrated architectural structure as befits this unique metropolitan coastal location

##### Assessment Criteria

- i. Use of skillion roofs and modern materials is actively promoted;
- ii. Use of pitched roofs and dark tiles is discouraged;
- iii. Lighting or similar features may be used to accentuate the roofscape and provide a positive architectural feature at night; and
- iv. Flat roofs are acceptable where concealed behind a building parapet.

#### 1.4.4 Building Levels

##### Design Objective

- ~~I.~~ To ensure development maintains a positive relationship with the street such that pedestrian movement, sight lines and streetscape character are maximised.
- ~~II.~~ To allow for the safe use of ceiling fans for cooling.
- ~~±.III.~~

##### Assessment Criteria

- i. All development shall maintain a minimum floor to floor height of 3.1 metres.
- ii. All development is to achieve a minimum finished floor level of +3.8AHD to ensure development takes into account coastal erosion and accretion patterns. Non habitable rooms and the provision of basement parking are exempt from the finished floor level stated above.
- iii. Where residential dwellings are proposed at on the ground floor adjacent to a street or public open space, a grade separation ~~from~~ 0.5 metres ~~to~~ 1.2 metres between the finished floor level of the ground floor and

the adjacent street or public open space is encouraged in order to create a visual distinction between the public and private space.

#### 1.4.5 Building Height

##### Design Objective

- I. Building heights will respond to the pedestrian scale, urban character, intended dwelling density, land use mix as well as the natural topography of the area.
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment.
- III. The built form shall minimise overshadowing to adjacent streets and public spaces.

##### Assessment Criteria

- i. Building shall be in accordance with the Building Height Plan (Figure 16).
- ii. Development shall be a minimum of three storeys.

#### 1.4.6 Building Materials

##### Design Objective

- I. To encourage a style of development that is consistent with the coastal location.
- II. To provide for a consistency in the standard of finish and materials throughout Cockburn Coast.
- III. To foster a sense of place through an identifiable character and style of development.

##### Assessment Criteria

- i. Extensive use of concrete tilt panels is discouraged. Where concrete tilt panels are used, they shall be integrally coloured (colour tinted concrete).
- ii. Moulded textures imprinted in the external surfaces of any concrete panels should also be applied.
- iii. Painted finishes and rendered textures over concrete panels are not permitted.
- iv. Warm exterior finishes are encouraged through the use of natural materials such as stone, timber, and other such natural products.

#### 1.4.7 Open Space

##### Design Objective

- I. To ensure that development provides an appropriate interface with the public open space.
- II. To maximise the potential for passive surveillance

##### Assessment Criteria

- i. Where an area of public open space is provided the surrounding development must address the open space by maximising passive surveillance from habitable rooms; buildings must front onto the open space through placement of doors, windows and balconies to create a safe and comfortable relationship to the public open space.

### 1.4.8 Fencing

#### Design Objective

- I. To ensure that fencing does not detract from the function and appearance of the streetscape.

#### Assessment Criteria

- i. The interface between private lots and the public open space may be fenced to a maximum height of 1.2 metres from natural ground level, but must be visually permeable above a height of 1m above natural ground level.

### 1.4.9 Landscaping

#### Design Objective

- I. To ensure an attractive streetscape environment.
- II. To aid the sustainability of a building through the provision permeable surface.

#### Assessment Criteria

- i. The front setback area shall include provision for elements of ~~must consist of a minimum of 50%~~ soft landscaping.
- ii. In ground landscaping is preferred over shallow landscaping above basements, particularly in front setback areas which provides the opportunity for tree planting.

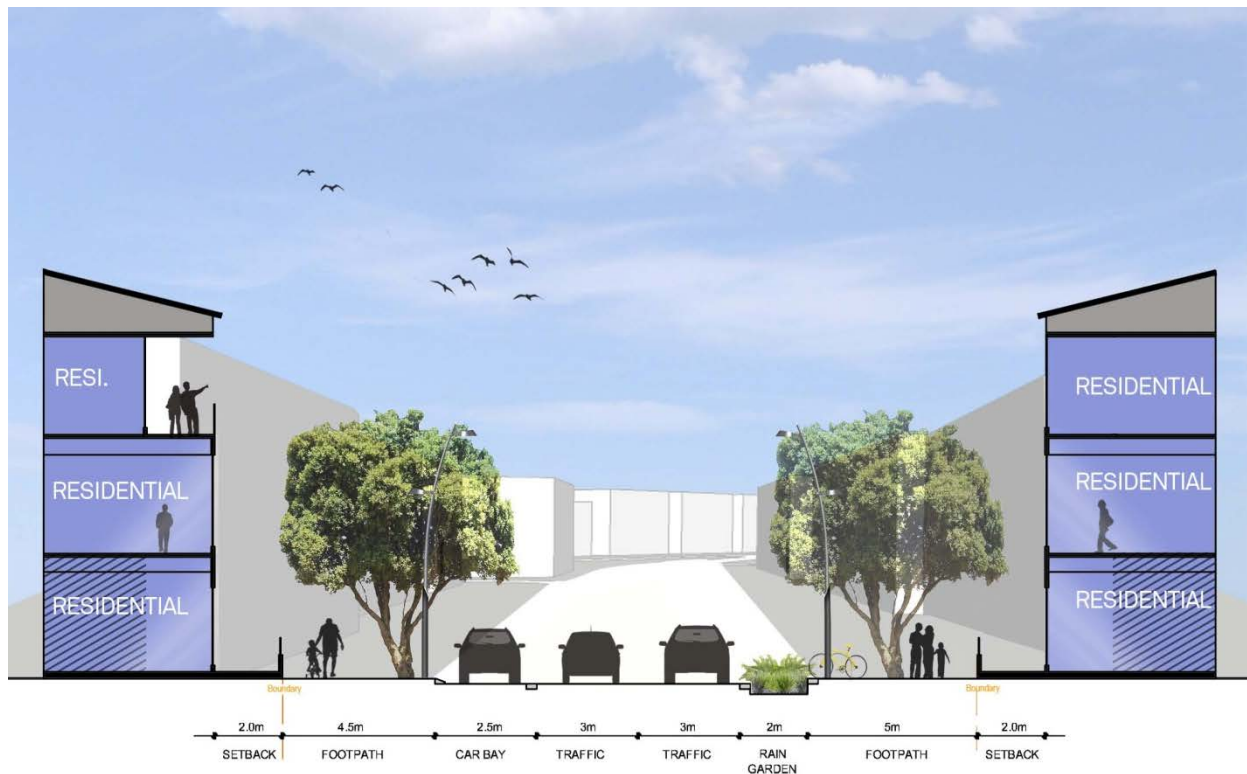




Figure 13\_Typical cross section for Medium Density built form typology

#### 1.4.10 Ancillary Accommodation

##### Objective

- I. Ancillary accommodation which positively addresses the laneway is actively encouraged.
- II. To provide flexibility for family living arrangements.
- III. Increase the diversity of dwelling types and tenure arrangements in the area.
- IV. To encourage activation and increased safety of residential laneways.

##### Assessment Criteria

- i. Ancillary accommodation units should have:
  - a) Nil setback to the laneway boundary at the upper levels only;
  - b) Balcony or a private courtyard with a minimum area of 7m<sup>2</sup>;
  - c) Front (entry) door which addresses the laneway or is accessed via its own pedestrian access gate;
  - d) One car bay specifically for this dwelling with its own hardstand, carport or separately operable garage door. Three door garages will not be considered;
  - e) Front door which addresses the laneway or is accessed via its own pedestrian access gate; and
  - f) Maximum floor area of 60m<sup>2</sup>.
- ii. Any large and visible elevations should be designed to include windows, architectural detailing and quality materials similar to that of the main dwelling.

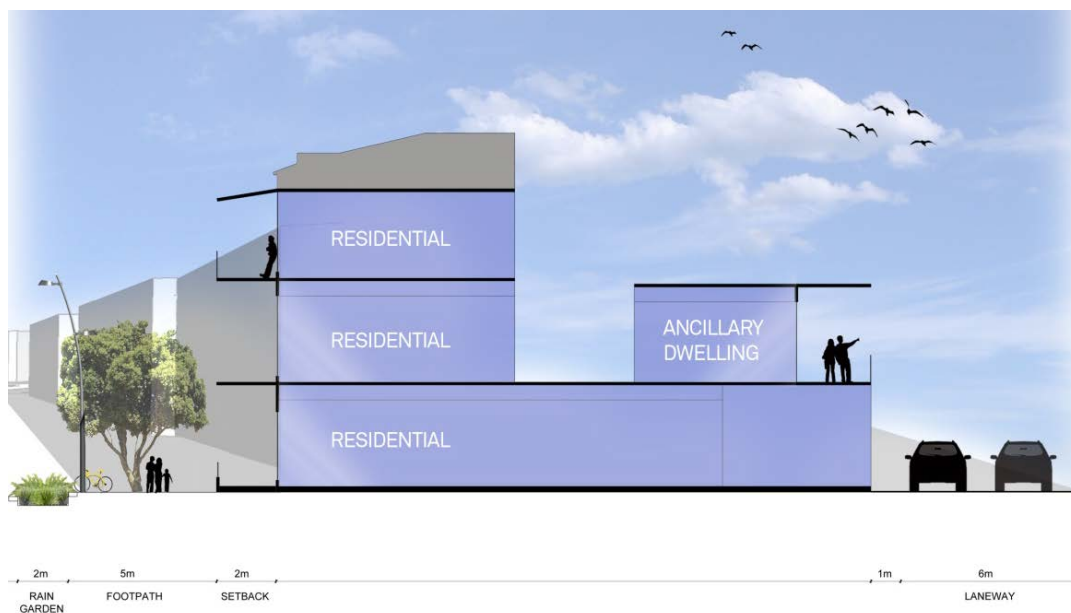


Figure 14\_Typical Cross section for residential laneway development



Figure 15\_Typical elevation for dwelling including ancillary accommodation within a residential laneway development

## 2. General Provisions

### 2.1 Built Form Requirements

Built form should provide a pedestrian scale and define streets and public spaces whilst contributing towards creating an urban presence. The built form will contribute towards the intended streetscape character and typology. Taking cues from the natural assets of the site building height responds to site topography, maximising views to the ocean particularly for residential development.

For private open space, visual privacy, storage for dwellings requirements refers to the relevant section of the R-Codes.

#### 2.1.1 Building Height

##### Design Objective

- I. Building heights will respond to the pedestrian scale and urban character of Cockburn Coast, intended dwelling density and land use mix as well as the natural topography.
- II. The built form of an area shall provide a pedestrian scaled street interface with taller upper floors setback from the street alignment.
- III. The built form shall minimise overshadowing to adjacent streets and public spaces.

##### Assessment Criteria

- i. Heights to be in accordance with the typology specific built form requirements and the Building Height Plan.

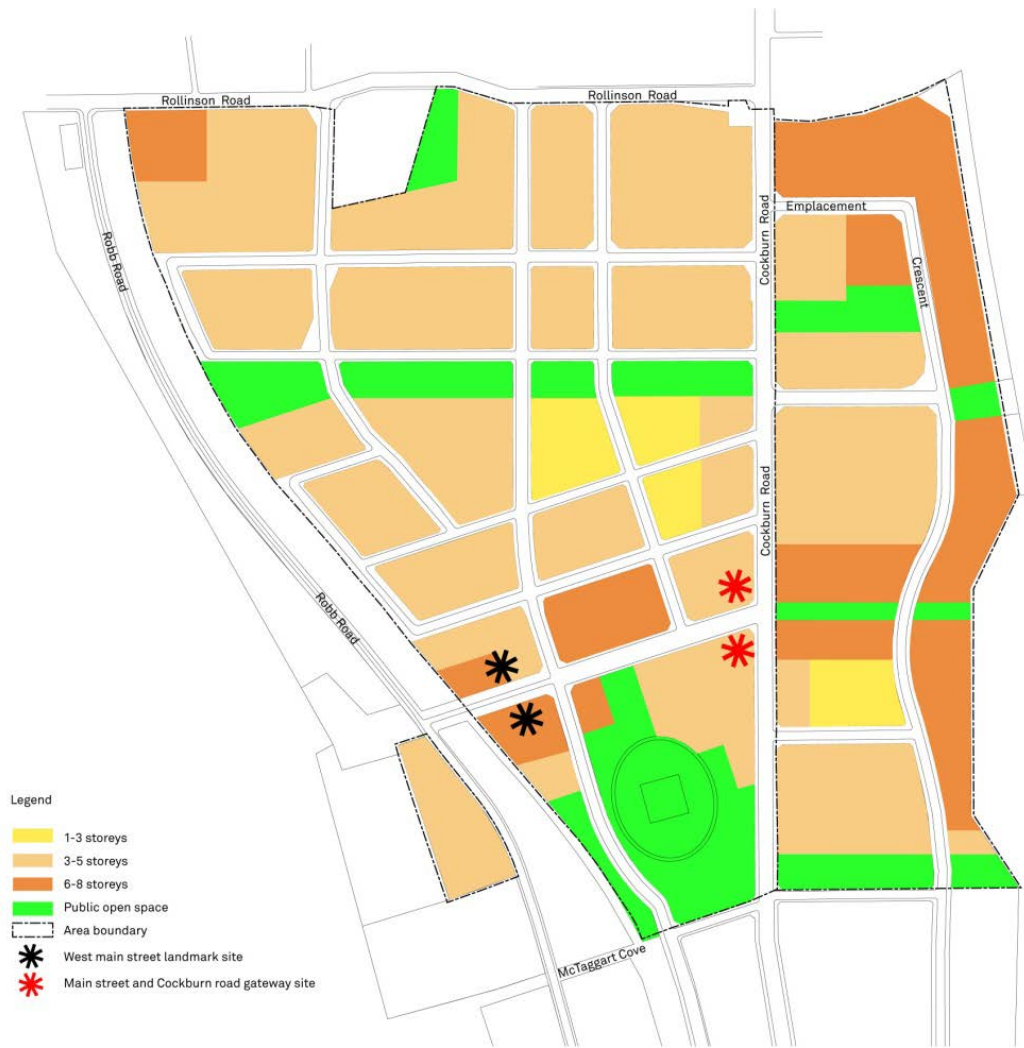


Figure 16\_Building Height Plan

## 2.1.2 Facades

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### Design Objective

- i. Building facades add significantly to the public realm and its interest. A vibrant and modern design aesthetic for Cockburn Coast will require the provision of visually engaging building exteriors which encourage interaction with the street and passive surveillance of adjacent spaces.

### Assessment Criteria

- i. Fenestration, entrances, balconies and awnings shall be provided in a manner that creates visual cohesiveness, interest and interaction with the public realm.
- ii. An exposed parapet or boundary wall must have the same standard of finish as the primary facade. Detailing for permanently exposed blank walls shall include texture, patterns or suitable alternatives to the finish of the wall to address the objective.
- iii. External ducting, air conditioners, plants, pipes, lift over-runs, service doors and similar building services must be screened from public view or adjacent property and incorporated into the building at the initial design stage.
- ~~iv. Apartments sleeving the public car park are to shallow and wide to maximise frontage to the external environment for improved solar access and ventilation.~~
- ~~iv.~~ Ground floor lobbies shall be clearly delineated, well lit and safe to access.
- ~~vi-v.~~ Facade design shall address crime prevention through environmental design (CPTED) principles.



Buildings shall provide a break up of bulk and scale through articulated facades



Building facades are to be finished with fine grain architectural elements



### 2.1.3 Roof Form

#### Design Objective

- i. The roof form as seen from the street or adjoining sites should be designed to make a contemporary and positive architectural contribution to the streetscape and skyline. Where appropriate the roof form can be designed to enhance the architecture and contribute to creating local landmarks through the use of integrated architectural form and detailing.

#### Assessment Criteria

- i. Roof designs must conceal roof plant and equipment including lift over run structures from view from the public realm and street level.
- ii. Lighting or similar features may be used to accentuate the roofscape to provide a feature at night.
- iii. Flat roofs are acceptable where concealed behind a building parapet.

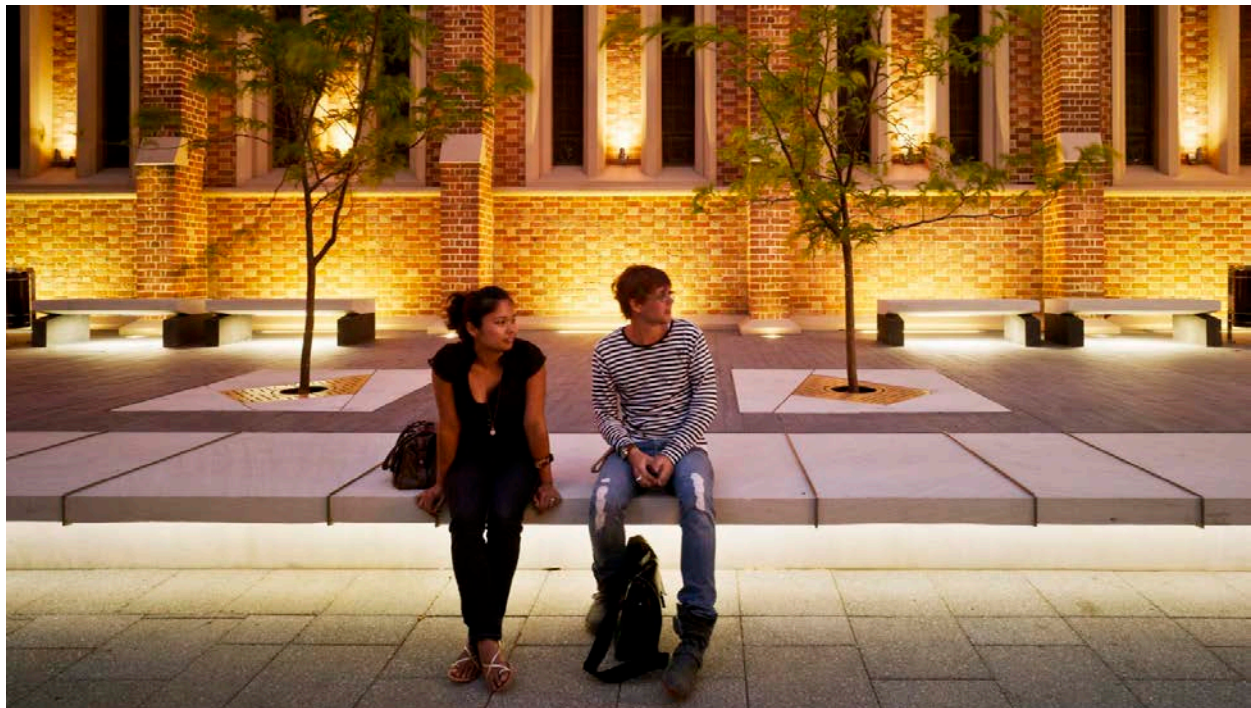
### 2.1.4 Lighting

#### Design Objective

- i. To ensure perceived and actual safety for all users of the area is achieved by providing lighting around public spaces that allows for a high degree of visibility of pedestrians at all times.

#### Assessment Criteria

- i. Lighting to be integrated into built form to highlight architectural features.
- ii. Ensure inset spaces, access, egress and signage is well lit.
- iii. Lighting is to be incorporated into building awnings over the footpath and building entrances.



Innovative lighting built into the facade of a building can contribute to an activated and interesting facade

Design Objective

- I. To facilitate a sustainable mixed use environment where a variety of land uses can co-exist.
- II. To ensure appropriate noise intrusion and noise emission mitigation measures are incorporated into building design and construction and where necessary, building refurbishment.

Assessment Criteria

- i. Design of Noise sensitive premises must be give consideration to the following:
  - a. the identification of existing/potential environmental noise sources;
  - b. development orientation and layout taking into account the location of existing/potential environmental noise sources;
  - c. the location of bedrooms away from noise sources;
  - d. the location of balconies and windows away from noise sources;
  - e. the use of built form (blade walls, etc) to screen noise sources; and
  - f. the use of building design elements (balcony balustrades, decorative screens, etc) to provide some reduction in noise impact on windows.
- ii. Notifications are required to be applied to the created land title and any subsequent strata titles of any noise sensitive premises pursuant to section 70A of the *Transfer of Land Act 1893*, together with section 165 of the *Planning and Development Act 2005* to inform prospective land owners and residents of the likelihood of higher noise levels associated within the inner city environment.
- iii. An acoustic and vibration (as deemed required in the local structure plan) report and associated plans are required detailing compliance with the above design objectives and assessment criteria for noise sensitive and commercial developments. The report is to be prepared by a qualified and experienced acoustic consultant and submitted as part of a DA and should address the requirements of State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning (and associated guidelines) and Quiet House Design Principles. ~~The report is to include:~~
  - ~~a) The identification of all environmental noise sources.~~
  - ~~b) The measurement of all identified noise sources, including adequate sampling to enable the establishment of reliable design noise levels. For traffic noise measurements at different times such as during peak traffic times, and for background noise a day time measurement between 3pm – 5pm Monday to Friday and a night time measurement between 10pm – 12pm Friday or Saturday night.~~
  - ~~c) The character of noise sources is to be adequately described in terms of frequency analysis (minimum of octave bands).~~
  - ~~d) The establishment of appropriate interior design sound levels for various areas of occupancy in accordance with the Performance Standards.~~
  - ~~e) A detailed description of the construction measures that are required to be included, or which have been included, in the proposed development to achieve the noise levels prescribed in accordance with point (ii) above. Calculations shall be based on octave band noise source data and octave band sound reduction performance for construction elements.~~
- vii. ~~Noise attenuation measures that should be addressed in the acoustic report and associated plans include but are not limited to the following:~~
  - ~~a) Windows:~~
    - ~~a. heavyweight / thicker glass~~
    - ~~b. double glazing~~
    - ~~c. special acoustic requirements for window frames.~~
  - ~~b) Walls:~~
    - ~~a. stud frame walls may require acoustic upgrading~~
    - ~~b. acoustic attenuation for exhaust vents through walls~~
    - ~~c. specific acoustic requirements for external doors.~~
  - ~~c) Roof / Ceiling:~~
    - ~~a. specific acoustic requirements for sealing roofs~~
    - ~~b. upgraded acoustic performance for ceilings~~
    - ~~c. closing / sealing of eaves~~
    - ~~d. insulation of ceiling void~~
    - ~~e. acoustic attenuation for vents through roofs.~~

### 2.1.6 Active Edges and Street Relationship

#### Design Objective

- I. The activation of streets and other publicly accessible spaces are fundamental to Cockburn Coast to providing an attractive and safe pedestrian environment.
- II. All development must be designed to activate streets and laneways. This can be achieved by utilising major openings to residential and commercial land uses, alfresco dining areas, pedestrian shelters and legible building entries to create a vibrant, diverse and safe environment.

#### Assessment Criteria

- i. Passive surveillance of communal areas and public spaces are to be integrated into building design, providing for overlooking of the street, public space or communal open space.
- ii. Pedestrian entrances are to be highly visible.
- iii. Ground floor non-residential frontages should be designed as shop fronts with no less than 80% of the shop front glazed with clear glass.
- iv. Car park entries are to be located appropriately to avoid disruption of the pedestrian experience.
- v. Inactive ground floor uses are to be avoided within the Activity Centre and Mixed Use areas particularly on the Robb Jetty Main Street and surrounding the identified landmark development sites.

### 2.1.7 Heritage Considerations

#### Design Objective

- I. Development of site adjacent to a heritage place shall be respectful of the recognised cultural heritage significance; and should not adversely affect the heritage significance.

#### Assessment Criteria

- i. New buildings adjacent to a Heritage Place should conform with the provisions of the City's Heritage Conservation Guidelines policy to ensure that they respect the heritage significance of the place.
- ii. Any new work adjacent to a significant tree should not affect the appearance or health of the tree.



Ground floor commercial land uses will provide active street edges

## 2.2 Service Infrastructure and Access

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Service infrastructure ~~and access arrangements is an~~ important part of allowing development ~~and the broader centre~~ to function effectively. However, ~~these elements it~~ can often ~~create~~ unsightly ~~urban environments~~ -and therefore appropriate treatment ~~and coordination of these element~~ is required to make ~~it an~~ an integral part of new development.

### 2.2.1 Internal Access

#### Design Objective

- I. Internal access within street blocks to perform as one coordinated and efficient movement network.

#### Assessment Criteria

- I. Internal accessways servicing development to be designed to facilitate adjoining development and where logical allow for reciprocal access arrangements.

### 2.2.2 Parking

#### Design Objective

- I. Development will encourage and support alternative modes of transport to the car by limiting and screening the provision of car parking on site.

#### Assessment Criteria

- i. Vehicle crossovers for non-residential development are required to be built underneath the building or provide design elements above the crossover to reduce the street impact and pedestrian environment.
- ii. Reciprocal use of commercial car parking bays for uses within a comprehensive development with different peak usage requirements (such as restaurants and offices) may be considered approved, provided that bays for residential use are always available.
- ~~iii. Commercial parking is to be provided in accordance with the Scheme with the stated rate of provision being provided both as a minimum and maximum.~~
- ~~iv.~~iii. Residential parking is to be provided in accordance with the relevant Local Structure Plan.

### 2.2.3 Parking Location and Access

#### Design Objective

- I. The number of vehicle crossovers into a development is to be minimised to create a pedestrian friendly environment.
- II. Parking is to be located so as minimise the visual impact on the public realm.

#### Assessment Criteria

- i. All on site car parking facilities are to be concealed from public view to ensure car parking does not dominate streetscapes or create conflict with pedestrian and vehicle movement.
- ii. Car parking entry is to be subservient to pedestrian entries and shall address, street spaces, building returns and recesses.
- iii. Where terrace style or single residential lots are proposed vehicle access must be provided at the rear of the dwellings.
- iv. Car parking is to be concealed from public view by habitable frontages, or high quality landscaping along minor/secondary streets.
- v. Parking facilities should not be visible from public open space.
- vi. Where garage doors service only one dwelling they should be no wider than 6 metres.

### 2.2.4 Sleeved Parking

#### Design Objective

- I. To screen multi storey car parks from the public realm and to provide active frontages to the street.
- ~~II. Multi storey car parking structures can maximise the efficient use of land but have the potential to negatively impact on the public realm.~~



#### Assessment Criteria

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- i. All multi storey car parking structures should be sleeved by development to ensure car parking is screened from view of the public realm.
- ii. Sleeve above ground car parking structures with other uses, such as offices, residential and retail.
- iii. Where it is not possible for car parking structure to be screened any car parking structures that contain three or more levels must be appropriately designed and screened from adjacent or nearby buildings and the street through the use of innovative wall detailing, decorative screening, patterning and vegetation.

## Design Objective

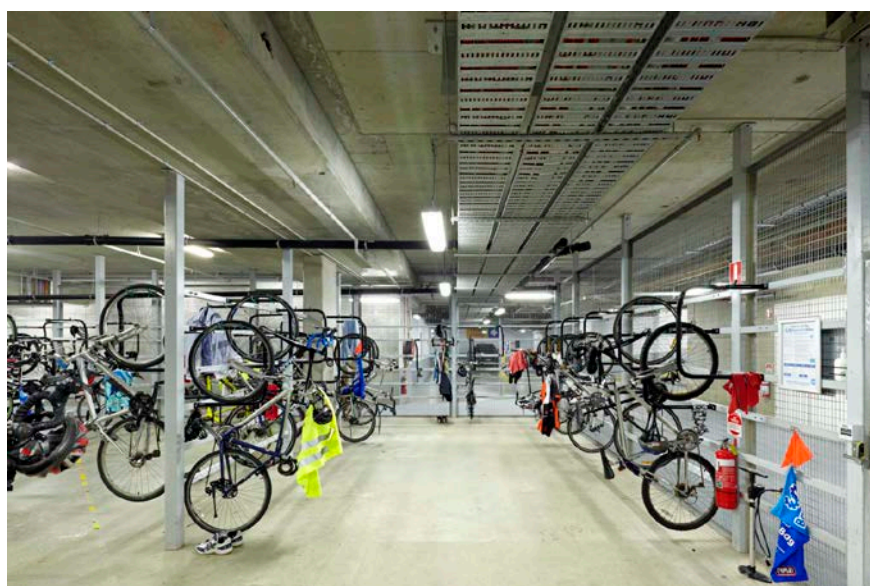
- i. To encourage the use of bicycles, walking and other alternative means of transport to reduce the use of private motor vehicles and contribute to public health.

## Assessment Criteria

- i. Provision of adequate bicycle and change room facilities. Secure lockers, bicycle storage and showers shall be provided within buildings.
- ii. Developments are to be provided with end of trip facilities in accordance with the following table.

<b>Commercial</b>	1 Secure bicycle storage <del>per for 10% of building staff (based on 1 person per 15150m<sup>2</sup></del> of Net Lettable Area (NLA); and
_Accessible showers	There must be a minimum of two female and two male showers, located in separate changing rooms, for the first 10 bicycle parking bays. Additional shower facilities to be provided at a rate of one male and one female shower for every 10 bicycle parking bays <del>or part thereof</del> .
_Changing facilities	Including secure lockers at 1 <del>.5</del> for each bicycle parking bay.
_Visitor Bicycle Storage	A minimum of 1 space per 750m <sup>2</sup> of NLA. Located and signed near the main public entrance to the building.
<b>Residential</b>	Bicycle parking facilities for multiple dwellings, short stay accommodation and serviced apartments shall be provided at a minimum of 1 bay <del>for every three per</del> units.

Table 05\_ End of trip facility provision rates



End of trip facilities



Use of screening can minimise the impact of parking structures

## Design Objective

- I. ~~Services and related elements required for the function of the building shall be appropriately screened or integrated into the building design. The location of building services has the potential to impact visually on the intended building design and adjacent spaces if not appropriately considered.~~
- II. ~~Ensure that services and related elements required for the function of the building are appropriately screened or integrated into the building design.~~

## Assessment Criteria

- i. Air-conditioning units must not be visible from the streets and laneways.
- ii. Service pipes and wired services are to be concealed from public view.
- iii. All meters to be contained within development lots to the requirements of the appropriate authorities.
- iv. Provide secure and accessible facilities for mail delivery.
- v. Commercial utility and waste storage areas are to be screened or located behind buildings and not visible from public view and residential apartments.
- vi. Fire booster cabinets and associated infrastructure are to be discretely designed into development and must not dominate any frontage.

## 2.3 Sustainability Requirements

Integral to the sustainability of the development will be the provision of affordable housing and facilities to encourage alternative modes of transport to the private car. This will promote a healthy lifestyle that encourages people to actively engage with the urban environment and create a robust and diverse community.

### 2.3.1 Sustainable Travel

## Design Objective

- I. To reduce greenhouse gases through the reduction of motorised transport to and from the Cockburn Coast and encourage residents and site visitors to improve their physical health through walking, cycling or other physically active forms of transport either solely or in combination with public transport.

## Assessment Criteria

- i. Demonstrate that pedestrians and cyclists have been prioritised within the development.
- ii. Surface finishes of all driveways and pathways to be safe and comfortable for pedestrians and cyclists.
- iii. Grade changes between private and public spaces to be complementary and accessible.
- iv. ~~All pedestrian areas should be adequately shaded and should include complementary amenities such as drinking fountains and rest points in locations best suited to promote non-vehicular travel.~~

### 2.3.2 Affordable Housing

## Design Objective

- I. ~~To ensure the provision of a diverse range of affordable housing product i.e. mix of sizes, dwelling types.~~
- II. ~~To ensure that affordable housing product is not distinguishable from non-affordable housing within development.~~
- III. ~~To provide guidance on how additional floor space required to accommodate affordable housing product is provided for.~~

## Assessment Criteria

- i. ~~Any discretion applied shall not set precedent for any future development.~~
- ii. ~~Where development provides sufficient affordable housing product (10% – 25% of the dwelling yield) the following variation to assessment criteria may be applied at the Cities discretion.~~
  - a) ~~Floor space bonus~~
    - ~~— a floor space bonus at the following ratio:~~
      - ~~— Affordable yield 10% = 30% floor space bonus~~
      - ~~— Affordable yield 20% = 40% floor space bonus~~
      - ~~— Affordable yield 25% = 45% floor space bonus~~

~~— where a minimum 30% of the affordable yield is provided as family size dwellings (i.e. 3 or more bedrooms) a further 10% floor space bonus may be applied.~~

~~b) Car parking reduction~~

~~Reduced Car Parking requirements at the rates stipulated in the table below:~~

<del>Use Class</del>	<del>Vehicle Parking Provision (expressed as minimum and maximum)</del>
<del>1 Bedroom Residential Dwelling</del>	<del>0.75 bay per dwelling that is affordable housing</del>
<del>2+ Bedroom Residential Dwelling</del>	<del>1 bay per dwelling that is affordable housing</del>

~~Table 06 Car parking reduction~~

~~c) Height~~

~~A variation to the building height may be deemed appropriate where:~~

- ~~— setbacks are not varied;~~
- ~~— the design and finish of the building mitigates the greater visibility of the building. Design elements which can be used to lessen the visibility of the additional height include:~~
  - ~~— Stepping back of building mass;~~
  - ~~— Top floors constructed of lighter weight material and which are less bulky in appearance;~~
  - ~~— More extensive glazing; and~~
  - ~~— Upper floors to be setback from the principle building line.~~

## 2.4 Laneways

### 2.4.1 Residential and Commercial Laneways

#### Design Objective

- I. To create unique and attractive built form and character along laneways through sensitive and innovative design.
- II. To encourage activity and interaction between public laneways and adjacent private uses at the ground level.
- III. To reinforce the primary function of laneways as key service and vehicle access spaces within the development.
- ~~IV. To ensure that laneways maintain a high level of pedestrian amenity and comfort.~~
- ~~V. Promote and create the opportunity for the inclusion of art, landscaping, street furniture, and activity spaces.~~
- ~~VI. Maintain and enhance the intimate environment of lanes by ensuring that higher tower forms are set back to ensure a sense of openness that reinforces a human scale.~~

~~VII-IV.~~ Encourage development to provide highly articulated and well detailed facades that create visual interest, particularly at the lower levels.

~~VIII-V.~~ Encourage development to orientate windows and balconies to overlook lane ways.

#### Assessment Criteria

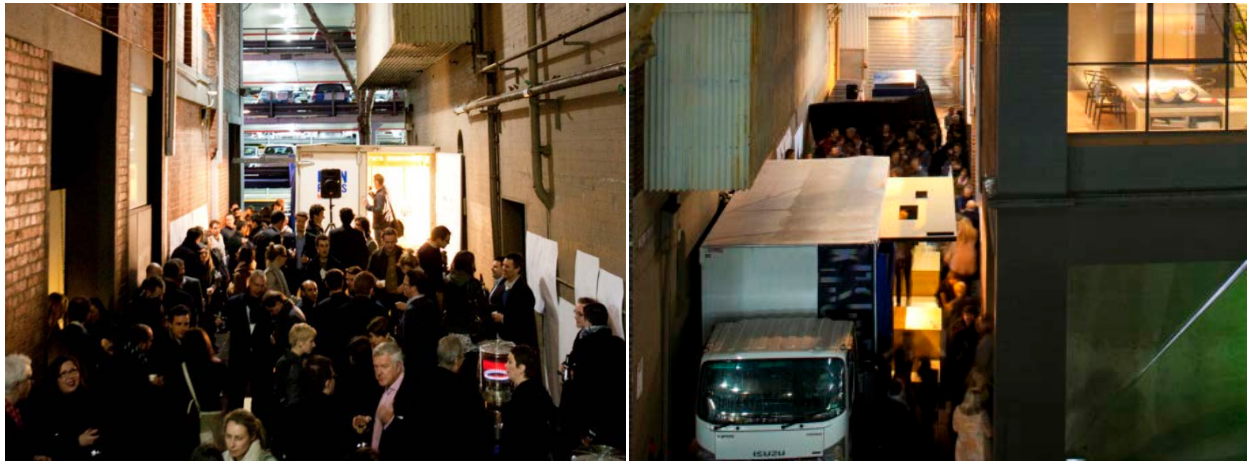
##### i. Residential Laneways

- a) For lots with a laneway frontage of 8 metres or greater, pedestrian access to the laneway from the lot should be provided.
- ~~b) For lots with a laneway frontage 8 metres or greater, the built form should not exceed 85% of the laneway boundary length. This is to provide relief from built edges to the laneway with opportunity for planting, wall returns and pedestrian access.~~
- ~~c) All buildings shall provide a one metre setback to the laneway boundary with the setback area being softly landscaped.~~
- ~~d)b)~~ Buildings are to provide an elevation to the laneway that is articulated and similarly detailed to the front facade.
- ~~e) Development should contain a front (entry) door which addresses the laneway or is accessed via its own pedestrian access, gate, etc.~~
- ~~f) Lighting to illuminate that portion of the laneway adjacent the subject land shall be provided at entry points for vehicles and pedestrians. The lighting structure shall not encroach into the right of way.~~

- ~~g) Gardens that extend over the rear fence and enhance the laneway are encouraged providing they don't create obstructions to vehicular movement.~~
- ~~h) All laneways shall be a minimum of 6 metres in width.~~
- ~~i) Laneways should provide 24 hour public access.~~

i. Commercial Laneways

- a) Laneways within the activity centre and mixed used zones are encouraged to be activated at ground floor level, but shall not be done so to the detriment to the activation of the primary or streetscape facade of the building.
- b) Buildings shall maintain a nil setback to the laneway for the first three storeys.
- c) The minimum setback above 3 storeys should be a distance equivalent to the width of the lane, unless it can be demonstrated that a lesser setback protects the quality of the pedestrian space at ground level including:
  - ~~d)a.~~ by maintaining or providing greater access to sunlight;
  - ~~e)b.~~ by maintaining or providing greater wind protection; and
  - ~~f)c.~~ by avoiding a sense of enclosed space.
- ~~g)d)~~ Buildings are to provide an elevation to the laneway that is articulated and similarly detailed to the front facade.
- ~~h)e)~~ Development should contain a ~~front (entry)~~ door which addresses the laneway or is accessed via its own pedestrian access, gate, etc.
- ~~i) Lighting to illuminate that portion of the laneway adjacent the subject land shall be provided at entry points for vehicles and pedestrians. The lighting structure shall not encroach into the right-of-way.~~
- ~~j) All laneways shall be a minimum of 6 metres in width.~~
- ~~k) Laneways should provide 24 hour public access.~~



Activated laneways encourage vitality and interaction between public laneways and adjacent private uses



## Part Two Public Realm

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### 3. Public Realm

The public realm is an important part of the urban environment that people see, access and interact with. A high quality public realm is vital to the success and activity of a city and determines how people experience a place. It allows for community development, social interaction, physical well being and private contemplation.

#### 3.1 Street Infrastructure

##### Design Objective

- i. To create a pedestrian focussed, comfortable and safe environment that encourages social interaction and activation by providing pedestrian amenities.

##### Assessment Criteria

- i. Ensure that pedestrian networks are uninterrupted, continuous paths of movement that do not exclude people with disabilities from accessing all services and amenities available.
- ii. Link pedestrian pathways to all entry and egress points of adjacent buildings.
- iii. Provide on-street visitor parking bays that are dispersed by street tree planting to ensure parking does not visually dominate the streetscape. Street tree planting is to be provided within the footpath zone at a rate of one tree every 10 metres.
- iv. Minimise surface run-off by providing permeable surfaces and infiltration/bio-retention opportunities within the streetscape design.
- v. Appropriate lighting is provided under pedestrian awnings, along streets and within parks and open spaces.
- vi. Within the Activity Centre and Mixed Use typology areas, pedestrian awnings are provided at a minimum width of 2.5 metres and a minimum height above the footpath of 3 metres.

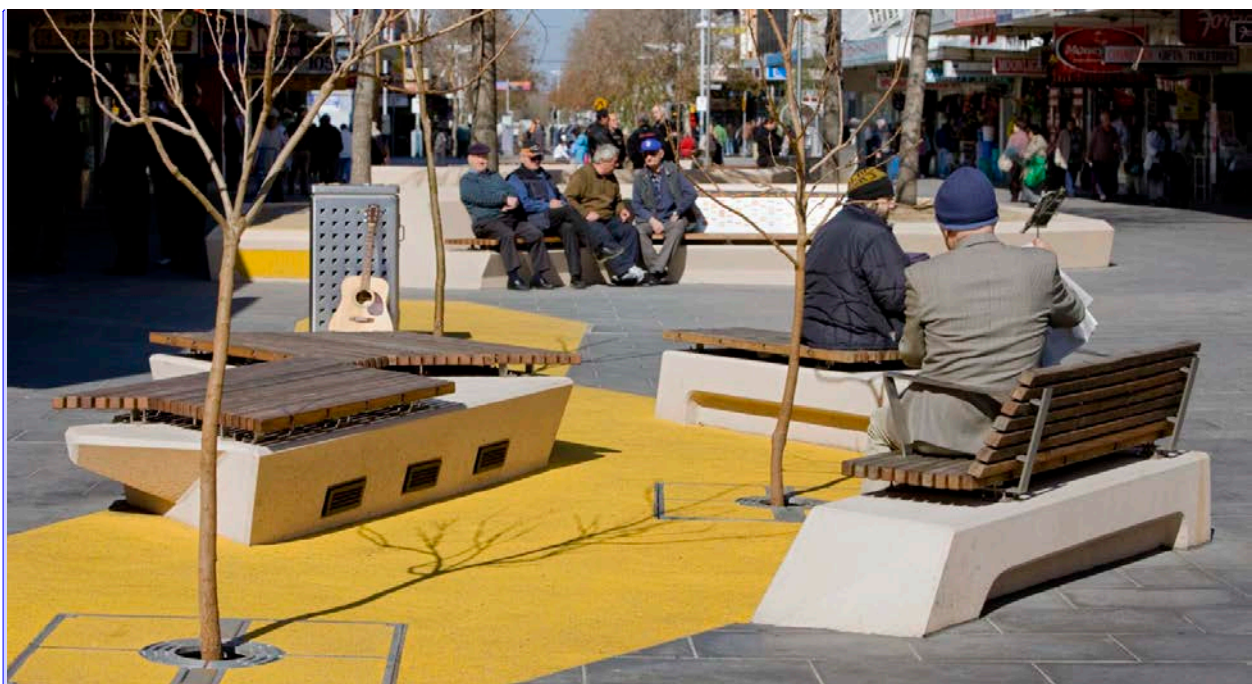
#### 3.2 Lighting

##### Design Objective

- i. To ensure perceived and actual safety for all users of the area is achieved by providing lighting in public spaces that allows for a high degree of visibility of pedestrians at all times.

##### Assessment Criteria

- i. Light pole and fitting selection to align with the City's standards.
- ii. Lighting design should minimise light spill into residential dwellings.
- iii. Light poles should be appropriately placed, preferably located in the same alignment as street trees.
- iv. Ensure inset spaces, access, egress and signage is well lit.



Innovative street infrastructure will help to provide a pedestrian focussed, comfortable and safe environment[c4]

**SCHEDULE OF SUBMISSIONS**

Design Guideline for Robb Jetty and Emplacement areas (Cockburn Coast)

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1.	Fremantle Ports PO Box 95 Fremantle WA 6959	<p>Thank you for referring the draft design guidelines for Cockburn Coast to Fremantle Ports.</p> <p>Fremantle Ports' interest with these guidelines is that the only freight rail route that services the Inner Harbour at Fremantle is located within the area subject to the draft design guidelines. This freight rail line plays a critical role in supporting Fremantle Inner Harbour as a working port allowing it to operate at optimal capacity and efficiency, whilst continuing to contribute to the State economy. It is crucial that the operation of the freight rail line is adequately protected from competing uses and incompatible urban development.</p> <p>The need to protect strategic freight routes from urban encroachment is well recognised by the Western Australia planning system. Fremantle Ports has previously raised its concerns with ensuring adequate measures are implemented in the Cockburn Coast development to mitigate noise and vibration impacts of the freight rail line. These can be managed by providing sufficient buffer distance between the rail/road and the noise sensitive buildings, as well as the acoustic and vibration treatment of the buildings. The Local Structure Plan indicated external noise criteria would be exceeded up to approximately 50m of the railway line and vibration criteria up to approximately 80m (using DEC criteria). As such it is requested that all new developments within the Cockburn Coast area should be located a minimum 80 metres away from the freight rail line. This stipulation is not currently in the draft design guidelines, and Fremantle Ports believes it is crucial to the long term sustainability of this project that such clause be included.</p> <p>Whilst vibration has been identified in the guidelines, Fremantle Ports does not believe that it has been adequately covered. Vibration suppression means are available, yet they have not been adequately covered in the draft guidelines. This is a crucial consideration given the close proximity of development to the freight rail line.</p>	<p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints. It is not reasonable to expect the development to be further set back from the railway line.</p> <p>Opportunities for setting back of development lots further from the railway line has effectively been lost. Planning since the CCDSP 2009 has indicated urban development abutting the railway line. This situation was compounded by the rezoning to 'urban' under the MRS and there is very little scope to see a different land use response to that of a built form response on a lot by lot basis.</p> <p>Agree, there is was discussion in the Part 2 of the local structure plan for Robb Jetty , however this needed to also be included in Part 1 to have statutory effect.</p> <p>A recommendation has already been noted to include</p>



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		<p>Level crossings are planned as part of the previously released local structure plans. With these crossings there are warning bells that sound as trains pass through. There is no evidence that this additional noise source has been accounted for in the noise sensitive section of the draft guidelines.</p>	<p>the issue of vibration to the Design Guidelines. The level of detail as to what suppression measures should be covered is too detailed in the Design Guidelines. A recommendation has been already included to remove some of this detail and simply refer back to the SPP.</p> <p>Measurement of noise was from a location near tot he existing rail crossing at Rollinson Rd. The level crossing is currently fitted with warning bells and therefore would have been captured in the reading taken.</p>
2.	The Freight and Logistics Council Ground Floor , 1 Essex Street, Fremantle WA 6160	<p>The Freight and Logistics Council (the Council) has had a number of communications with the City of Cockburn about the Cockburn Coast development, with a particular focus on highlighting our primary concern which is to ensure that the heavy freight railway line, which is located on the western boundary of the Cockburn Coast redevelopment precinct, is not compromised by future development.</p> <p>Against that background, the Council thanks the City for the opportunity to comment on the draft Design Guidelines for the Robb Jetty and Emplacement areas of Cockburn Coast. We take this opportunity to again reaffirm that Fremantle Port is a key element in the economic well-being of Western Australia. This \$1 billion pa business is, in turn, dependent on a highly efficient and sustainable freight rail service. The railway line traversing the western portion of the Cockburn Coast development cell and the associated rail transport service currently removes 100,000 truck movements from the metropolitan road network, annually thereby reducing road congestion and transport noise to the benefit of the wider community.</p> <p>This figure will grow rapidly as trade through the Port increases and rail's share of the market grows. State Government policies, both current and future, will continue to support this growth to the benefit of the entire State of Western Australia.</p> <p><b>1. COCKBURN COAST STATUTORY FRAMEWORK</b></p> <p>We understand that under the draft provisions of the City's Town</p>	<p>No response seems necessary to the first portion of this submission where the submission outlines some background information.</p> <p>The summary of what is required by the local structure plans is not quite right Cockburn Rd is the first row of buildings which is affected. For the freight rail, within 150m of the railway line, noise will need to be further assessed as an issue and a built form response is required. Within 50-80m of the railway line, the issue of vibration will also need to be further assessed. There has been changes recommended already to the Robb Jetty Local Structure Plan to ensure the matter of vibration is made clear in Part 1 (statutory) section of the local structure plan.</p> <p>Recommendations have also been included to ensure the Design Guidelines refer back to the SPP (also see comments to submission above).</p>

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		<p>Planning Scheme (Amendment No. 89) relating to Cockburn Coast, the recently advertised local structure plans must have associated Design Guidelines to guide development and urban form within each of the precincts referred to as Robb Jetty and Emplacement. Furthermore, we understand that once adopted, the Design Guidelines will be a Local Planning Policy under the City's Town Planning Scheme. However, until such time as the City of Cockburn Town Planning Scheme No. 3 Amendment No. 89 is gazetted and the Robb Jetty and Emplacement Local Structure Plans and the Noise and Vibration Management Plan are adopted/endorsed by the relevant decision making bodies, it is considered premature to comment on the draft Design Guidelines in the context of how development will be required to respond to road and rail noise and vibration.</p> <p>The amendment to 1.0 Objectives of the Cockburn Coast Development Area (k) of the draft Schedule 11 provisions made at the City of Cockburn Council meeting on 9 February 2012 to make specific reference to <i>"Where residential or noise-sensitive development is proposed in a situation where it may be exposed to noise impacts ..... any noise or vibration studies shall be undertaken by appropriately qualified professionals, at the developer's cost, to the satisfaction of the Local Government."</i> is encouraging. However, it is unclear as to what is required to be submitted with applications at each stage of development, particularly in regard to vibration.</p> <p>As a basis for providing comments on the Design Guidelines, it is important to outline the statutory framework for the Cockburn Coast development as we understand it, to understand how road and rail noise and vibration is addressed at all stages of the planning process.</p> <p><b>2. TRANSPORT NOISE STATUTORY FRAMEWORK</b></p> <p>We understand that proposed Amendment No. 89 requires the preparation and submission of a Noise and Vibration Management Plan to support a Local Structure Plan. However, it is unclear if the Local Structure Plan report and/or the Herring Storer studies fulfil that role.</p> <p>The Robb Jetty and Emplacement Local Structure Plans propose the</p>	

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		<p>following requirements in relation to Noise Attenuation, which are drawn from the Herring Storer Train Noise and Vibration Study and the Road Noise Assessment:</p> <p><b>Cockburn Road</b></p> <ul style="list-style-type: none"> <li>• Preparation of an acoustic assessment to inform the design process and to be submitted with a development approval application.</li> <li>• A notification to be placed on the Certificate of Title where residences are exposed to transport noise that exceeds the "Noise Target" identified as 62dB(A) for development facing Cockburn Road and 59dB(A) for development perpendicular to Cockburn Road. It is unclear why the Local Structure Plan refers to the noise levels, outlined in the second point above, referred to in the Herring Storer Road Noise Assessment (October 2011) as Noise Targets. The Local Structure Plan should refer to the need for notifications to be placed on the Certificate of Title of lots that are exposed to noise in excess of the Noise Targets outlined in State Planning Policy 5.4.</li> </ul> <p><b>Freight Rail</b></p> <ul style="list-style-type: none"> <li>• Any development proposed within 150m of the freight rail shall prepare an acoustic assessment to inform the design process, which shall also be submitted with a development approval application.</li> <li>• A notification to be placed on the Certificate of Title where residences are exposed to transport noise that exceeds the "Noise Target". It is assumed that the Noise Target referred to in respect of noise associated with the freight rail is as per the Noise Targets outlined in State Planning Policy 5.4.</li> </ul> <p><b>2.1 Design Guidelines</b></p> <p>The inclusion of Section 2.1.5 Acoustics and the associated assessment criteria within the Design Guidelines is supported. However, we wish to make the following comments:</p> <ul style="list-style-type: none"> <li>• further guidance should be provided to clearly identify areas that are subject to compliance with these criteria, with</li> </ul>	

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		<p>particular reference to Cockburn Road and the freight rail; the criteria should refer to the design of the development being informed by the acoustic assessment that is required to be prepared and submitted with an application for development approval, as per the Local Structure Plan;</p> <ul style="list-style-type: none"> <li>• Point (ii) should also make reference to a notification on the Certificate of Title required in accordance with the requirements of the Local Structure Plan and/or acoustic assessment prepared and submitted with an application for development approval;</li> <li>• Point (ii) should also make specific reference to "road and rail noise" in addition to <i>"higher noise levels associated within the inner city environment"</i>;</li> <li>• Point (iii) should refer to the acoustic report and associated plans being prepared in accordance with State Planning Policy 5.4;</li> <li>• Point (iii) (d) it is unclear what is meant by "Performance Standards"; and</li> <li>• Point (iii) (e) refers to <i>"noise levels prescribed in accordance with point (ii) above"</i>, however Point (ii) does not refer to noise levels.</li> </ul> <p>In addition to the above points, it is unclear as to how the City of Cockburn will assess the acoustic report and ensure that the design of the development responds accordingly. Will the City refer the acoustic report and proposed development to the Department of Environment and Conservation for assessment and comment or will the City engage a qualified acoustic consultant to undertake the assessment?</p> <p><b>3. TRANSPORT VIBRATION STATUTORY FRAMEWORK</b></p> <p>We understand that proposed Amendment No. 89 requires the preparation and submission of a Noise and Vibration Management Plan to support a Local Structure Plan. However, it is unclear whether the Local Structure Plan report and/or the Herring Storer studies fulfil that role. Nonetheless, of particular concern is the absence of commentary within proposed Scheme provisions, the Local Structure Plan report and supporting Train Noise and Vibration Study as to how and when vibration will be addressed through the statutory process.</p>	

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		<p>We understand that the vibration monitoring undertaken by Herring Storer and outlined in the Rail Noise and Vibration Study concluded that the distance from the freight rail line required to achieve compliance with ground vibration criteria varies from 30 - 80m across the development site and that those distances do not exclude development. However, additional amelioration may be required.</p> <p>Following recent discussions with Landcorp and Herring Storer, we understand that a site specific vibration assessment for land within 80m of the rail line will be required to be prepared and submitted with an application for development approval. However, this is not reflected in the Local Structure Plan or the Design Guidelines.</p> <p>The absence of guidance for prospective purchasers and developers on when and how vibration is required to be addressed through the planning process is of particular concern, particularly in light of the results of the Herring Storer study.</p> <p><b>3.1 Design Guidelines</b></p> <p>The Design Guidelines should be amended to include reference to the design objectives relative to vibration and outline the assessment criteria, including the requirement for further detailed assessment and development design responses. The Freight and Logistics Council would welcome the opportunity to discuss the points made here in further detail to ensure the long term protection of a major component of the freight rail network and ensure the highest level of amenity can be achieved for future residents within the Cockburn Coast development.</p>	
3.	Development Planning Strategies, Mr Ian Ricciardi PO BOX 6697 EAST PERTH WA 6892	<p><b>Objection</b></p> <p>We make this submission on behalf of our client Mr Ian Ricciardi, Executive Director of Big Buoy Pty Ltd, a land and business owner and landlord within the Robb Jetty Precinct. The land owned by Big Buoy Pty Ltd is bound by Rollinson Road, Garston Way and Darkan Avenue, being Lots 4, and 303 as shown on Figure 9 of the Robb Jetty Local Structure Plan (as advertised). This land is currently utilised seafood/ chicken/meat processing, trucking facilities and cold storage, with total employment consisting of approximately 180 people. Mr Ricciardi has been an active participant in the planning</p>	<p>It is not appropriate to include development standards for non-conforming uses in the Design Guidelines for the Cockburn Coast area. These guidelines are intended to guide new development in line with the proposed vision for this area.</p> <p>Responses to comments on the local structure plan comments may be found in the Schedule of Submissions for Robb Jetty Local Structure Plan.</p>

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		<p>stages of the Cockburn Coast area, including membership on the reference committees for the District Structure Plan and the Cockburn Coast Planning Committee.</p> <p>Throughout the planning stages our client has made his long term intentions clear that the current operations on the land holdings remain in its current location for at least the next 15 to 25 years. Both Processing and Cold storage facilities were built of high quality with long term perspective. it is imperative to appreciate that the cost for our client to relocate, first consideration is potential loss of business, finding a location with similar proximity to the Fremantle Port would be a near on impossibility hence service ability no longer a niche for existing client base.</p> <p>Secondly, due to increasing cost of build (not including land purchase costs), replacement is estimated to be in the order of \$25 million plus, for the two buildings. As you are aware, our client is able to continue operations under non-conforming use rights. It is our client's concern; however, that the Rob Jetty Structure Plan and the supporting draft Guidelines as currently proposed will have an adverse impact on the operation of the business. It is imperative that the LSP and accompanying Guidelines recognise and respond to the nature of the current operation and appropriately mitigate any impediments to it.</p> <p><b>Design Guidelines</b></p> <p>We object to the proposed design Guidelines for the Robb Jetty Precinct. The Guidelines as currently proposed do not address our client's concerns and objections raised in relation to the proposed Rob Jetty Structure Plan. Copy of our client's previous submission, dated 11 December 2012, is enclosed as Attachment 1. As outlined previously, it is not our client's intent to relocate his current business operations or re-develop the land holdings within the next 25 years. The Rob Jetty Structure Plan (and therefore the draft Guidelines as currently proposed) will have an adverse impact on the operation of our client's business.</p> <p>The Guidelines address issues of building setbacks, articulation, levels, awnings, height, materials, open space, landmark sites, fencing as they would apply if the land was ever redeveloped for 'Mixed Business'. They do not however, fully appreciate or respond to the nature of the current operation and therefore do not offer</p>	<p>The local structure plans note a process (as outlined in the draft State Planning Policy for State Industrial Buffers) which enables landowners the opportunity to submit a technical analysis of the buffer issues to refine them if necessary. The buffers as set out in the draft SPP are generic only. It is up to the local government to determine the appropriate buffer which should apply. It is noted a landowner will most likely desire the larger buffer.</p> <p>Ultimately non conforming use right are set out in the City's Town Planning Scheme. It is not appropriate to include further provisions in either the local structure plans or the Design Guidelines. A number of the comments in this submission have no relevance to development standards outlined in the Design Guidelines and therefore are not discussed further in this schedule of submissions.</p>

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		<p>adequate protection measures to ensure the existing business is not impacted upon. The highest level of the planning document (District Structure Plan) mentions buffers and states they need to be addressed at local structure planning. The Local Structure Plan, as advertised, did not address the issue of the existing uses adequately. Consequently, the guidelines, which support the Local Structure Plan fail to do so as well.</p> <p>We therefore object to the Guidelines and use the opportunity to raise/reiterate the issue previously raised in by our clients in his submission on the Local Structure Plan. It is our understanding that the LSP will be considered by Council in April.</p> <p><b>Robb Jetty Local Structure Plan</b></p> <p>We re-iterate our client's previous concerns with the Local Structure Plan (LSP) in relation to addressing the issue of existing land uses (which have associated buffers) which intend to remain in the area for a significant period of time (25 years). We do not consider that the LSP accurately reflects the buffer necessary for our client's land holding based on the Environmental Protection Authorities <i>Guidance Statement No.3 - Separation Distances between Industrial and Sensitive Land Uses</i>.</p> <p><b>Buffer</b></p> <p>The Commission's State Planning Policy (4.1) - <i>State Industrial Buffer</i> recognises that industry is critical to local, regional, state and national economies and the main objective of the policy is to protect industry from those land uses that would be sensitive to impacts and adversely impact the efficient operations. Based on the land uses occurring at the site (chicken and seafood processing, trucking facilities and cold storage) the Environmental Protection Authorities (EPA) <i>Guidance Statement No.3 - Separation Distances between Industrial and Sensitive Land Uses</i> requires a 500 metre buffer. However, Figure 25 of the LSP shows a 100 metre buffer only applied to Lot 303. The Council's online mapping system shows no buffer for our clients land holding.</p> <p>We consider that a 500 metre buffer to the site should be shown in the LSP. In addition, the LSP should make clear that a proposal for</p>	

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		<p>sensitive land uses within an existing buffer will not be supported unless technical analysis can show that buffer can be reduced or mitigated, even though the land use complies with the LSP. This will strengthen the LSP and Council's ability to protect existing land uses/businesses and employment generators. This is consistent with the clauses included in the District Structure Plan (3.4 and 3.5) discussed below.</p> <p><b>Protecting Existing Land Uses</b></p> <p>Part 1 of the Cockburn Coast District Structure Plan (Section 3.4 and 3.5) clearly outlines the intent to protect existing industrial uses and outline and confirm the principles of non-conforming use rights and protection of existing uses. Importantly, clause 3.5.1 of the District Structure Plan requires that any proposed change of land use in areas adjoining an existing industrial land use demonstrate that:</p> <ul style="list-style-type: none"> <li>• The proposed use would comply with any buffer requirements associated with the existing adjoining use;</li> <li>• The proposed use/development does not undermine the potential for the existing operation to continue normal operational activity; and</li> <li>• The proposed use/development would not be adversely affected in terms of odour, noise, safety or visual amenity, particularly where the proposed use is to include residential development.</li> </ul> <p>The District Structure Plan outlines that this requirement be founded in the information supplied as part of the local structure plan.</p> <p>The Robb Jetty Local Structure Plan is very brief on this issue and it is our opinion that it does not respond appropriately to the matter of protecting existing industrial land uses or provide details as to how this matter is to be dealt with through the implementation of the LSP.</p> <p><b>Road Networks and Traffic Management</b></p> <p>We also take this opportunity to highlight our concerns regarding road networks and traffic management in the area. The design guidelines appear to grossly underestimate potential vehicle movement within the 3 precincts, especially during anticipated 3</p>	



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		<p>stages (zones) of development. A critical issue which affects the continued efficient operation of our client's businesses is local traffic management and access arrangements to the regional and sub-regional road network. In addition, the local and regional road network must be designed to accommodate the trucking operations necessary for delivering and transporting goods processed at the site.</p> <p>The current operations on this land holding generate significant truck movements onto Rollinson Road and Darkan Ave from Cockburn Road. The traffic congestion on Cockburn Road has dramatically increased in recent years. Heavy haulage vehicles can currently wait up to 10 minutes to safely access Cockburn Road from Rollinson Road, significant oncoming traffic restriction when turning north on Cockburn Road. Development in the area, as per the DSP and LSP, will significantly contribute to the bottle neck situation on Darkan / Rollinson Road and the congestion on Cockburn Road.</p> <p>We understand that the ultimate plan, as per the DSP, was for the Cockburn Coast Drive to be extended northwards from Port Coogee through to Rockingham Road. In addition, it was proposed that a Rollinson Road overpass would link to the new extension of Cockburn Coast Road to relieve traffic congestion in this area. However, we are now led to believe the extension of Cockburn Coast Drive and associated Rollinson Road overpass are no longer in Main Roads budget. We understand that it is now intended to upgrade Cockburn Road to address the traffic congestion and traffic management issues in the area in order that Cockburn Road can take on the function of the previously proposed Cockburn Coast Drive, a primary arterial road (regional road).</p> <p>It is our opinion that it is not appropriate for Cockburn Road to be upgraded to function as primary arterial road (regional road) servicing the area, particularly for heavy freight and truck movements, for the following reasons:</p> <ul style="list-style-type: none"> <li>• The ultimate land use pattern abutting Cockburn Road, as per the approved DSP and proposed LSP's, does not support the increased function and associated increased traffic volumes of Cockburn Road and the use of Cockburn Road by heavy freight and trucks; and</li> <li>• Cockburn Road does not provide direct access to</li> </ul>	

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		<p>Rockingham Road. We also understand that it is proposed that the upgrade of Cockburn Road, to a primary arterial road (regional road), become a Development Contribution Item.</p> <p>We believe this logic is flawed as the responsibility for regional roads lies with Main Roads and is not a development contribution item. In summary, the development of the area as proposed by the DSP and LSP is reliant upon the extension of Cockburn Coast Drive and the Rollinson Road overpass. In the absence of this regional infrastructure, the area will suffer greater traffic congestion and our clients operational efficiency will be negatively affected as the road network will restrict efficient access and truck movements to our businesses.</p> <p>We thank you for the opportunity to make a submission. We recognise we have raised many issues that are beyond the scope of the design guidelines to which submission have been invited, and appreciate your time to consider these issues. We understand that the Robb Jetty Local Structure Plan has not yet been considered by Council and we would greatly appreciate the opportunity to discuss the matters we have raised in relation to the Local Structure Plan and traffic impacts with the City in a meeting forum prior to consideration of the Local Structure Plan by Council.</p>	
4.	Steve Beyer, Department of Transport 140 William Street Perth WA 6000	<p>Thank you for your letter dated 1 March 2013 seeking comment from the Department of Transport on the Design Guidelines (Local Planning Policy) for Cockburn Coast– Robb Jetty and Emplacement LSPs. This is a single Transport Portfolio response incorporating comments from Main Roads WA and, on the matter of acoustic and vibration guidelines, from the Public Transport Authority. The comments are as follows:</p> <p><b>1. Cockburn Road (parking access, clearance from kerb, typical cross section)</b></p> <p>A Working Group has been established with DoT, MRWA, the City and LandCorp for the review of Cockburn Road that will also include the development of a Vehicle Access Plan (VAP) to limit direct vehicle access to individual properties along Cockburn Road. Therefore, the following issues need to be incorporated:</p>	

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		<ul style="list-style-type: none"> <li>The Design Guidelines should minimise direct vehicle access from individual properties along Cockburn Road in accordance with the Western Australian Planning Commission (WAPC) Development Control Policy 5.1 and Liveable Neighbourhood Policy Element 2, P.3, P.22 R8 and Element 3 P.3 R30.</li> <li>The Design Guidelines should adopt a 2.5m clearance from the kerb face on Cockburn Road to any awnings, as required by the Main Roads Code of Practice. This clearance is required to provide for roadside furniture, i.e. lighting poles, traffic signals, directional signage, etc.</li> <li>A typical cross section for Cockburn Road needs to be included in the Design Guidelines to ensure consistency with the outcomes agreed by the Working Group for the review of Cockburn Road.</li> </ul> <p><b>2. Road Noise</b></p> <ul style="list-style-type: none"> <li>The Guidelines should record that a transport noise assessment of proposed developments should be conducted in accordance with the guidelines for the State Planning Policy (SPP) 5.4, and in line with procedures outlined under any Noise Management Plan prepared for these locations, preferably as part of the Local Structure Plan.</li> </ul> <p><b>3. Acoustics</b></p> <p>There are a number of issues with the Design Guidelines in relation to acoustics:</p> <ul style="list-style-type: none"> <li>The Guidelines seek to cover a broad range of acoustic issues but impose only a weak requirement for implementation. For example, Assessment Criterion (i) [p. 30] states that developers must “consider” locating balconies and windows away from noise sources, but this is highly unlikely where dwellings overlook both the ocean and the rail freight line;</li> <li>The Guidelines do not clearly identify the “Performance Standards” for building interiors that they refer to;</li> <li>The Guidelines refer only to noise standards for building</li> </ul>	<p>The City already has a local planning policy to deal with vehicle access onto busier roads. It would be appropriate, once the Vehicle Access Plan is available to update that policy to include the access arrangements for Cockburn Rd.</p> <p>Cross sections of Cockburn Rd can be found in the local structure plan. It is not appropriate to replicate this in the Design Guidelines.</p> <p>Agree, it has already been included in the officer recommendation to simply state the requirements as to be as per SPP 5.4.</p> <p>It has already been included in the officer recommendation to simply state the requirements as to be as per SPP 5.4. The Design Guidelines do not need to replicate this as this will add to confusion for applicants.</p>

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		<p>interiors, rather than for external living areas, which are also required under the SPP 5.4.</p> <p>Therefore, the Design Guidelines should:</p> <ul style="list-style-type: none"> <li>State that a transport (road and rail) noise assessment is required for proposed developments, in compliance with the SPP 5.4 guidelines;</li> <li>Be revised in line with the standards, procedures and design considerations outlined by the Noise Management Plan which is to be prepared for the Local Structure Plan area;</li> <li>Clearly state the outdoor and indoor noise targets adopted by the Noise Management Plan. (These noise levels should at least comply with the noise <u>targets</u> (not the noise limits) in the SPP 5.4).</li> </ul> <p><b>4. Vibration</b></p> <ul style="list-style-type: none"> <li>The Design Guidelines do not discuss vibration, even though it is likely to have a major impact on the development. The treatment of vibration needs to be addressed on a development-wide scale, as part of a DEC-approved Noise (and Vibration) Management Plan.</li> <li>The Design Guidelines should therefore be amended to include the requirements identified by the Noise Management Plan, and should also clearly state an acceptable level of vibration mitigation that developers must adhere to.</li> </ul> <p><b>5. Positive Covenant on Land to Address Noise and Vibration Issues</b></p> <ul style="list-style-type: none"> <li>Given the strategic importance of protecting the transport corridor from noise complaints, as well as the residents themselves from noise impacts, a mechanism is required to ensure ongoing compliance with the acoustic and (proposed) vibration sections of the Design Guidelines.</li> <li>The Design Guidelines should be applied using a positive covenant to land titles requiring that any development must comply with the Guidelines' acoustic and vibration sections.</li> </ul> <p><b>6. Accessibility and Sustainable Travel</b></p>	<p>The matter of a Noise Management Plan was raised in this Department's submission on the local structure plans. It was not considered that a Noise Management Plan was appropriate at this stage. These will be on a lot by lot basis through the subdivision and development process.</p> <p>It has already been included in the officer recommendation to add this requirement to Part 1 of the local structure plan to have statutory effect.</p> <p>See comments above.</p> <p>Covenants are already recommended in the local structure plans.</p>

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		<ul style="list-style-type: none"> <li>The Guidelines do not mention but should specify the width of walking, cycling or shared paths, or the width of roads/cycle lanes. These are to meet the Transport Standards under the Disability Discrimination Act as well as the Access to Premises Standards. In relation to cycling, road/cycle lane widths should comply with cycling aspects of Austroads guides.</li> </ul> <p><b>7. End of Trip Facilities</b></p> <ul style="list-style-type: none"> <li>The number of bicycle parking bays in residential dwellings is clearly insufficient – in line with proposed guidelines for the WA Bicycle Network Plan, there should be at least 1 bay for every unit, not 1 bay for every 3 units.</li> <li>The Guidelines' rate of locker provision in commercial buildings, at 1 per bicycle bay, is insufficient. Given plans to promote the use of active transport in the LSP areas, there should be 1.5 lockers per bicycle bay. The Guidelines also need to specify the provision of a designated space for drying wet items.</li> </ul> <p><b>8. Public Lighting</b></p> <ul style="list-style-type: none"> <li>Design of public lighting should, as a minimum, comply with the requirements of AS1158.3.1 and Main Roads specific requirements.</li> </ul>	<p>These are already specified in the local structure plans.</p> <p>This appears to be a reasonable recommendation and should be included in the Design Guidelines.</p> <p>This appears to be a reasonable recommendation and should be included in the Design Guidelines.</p> <p>It has been recommended the entire public realm section be removed. Nevertheless, the City would be seeking to comply with Australian Standards in the public realm. There will be separate public realm guidance prepared.</p>
5.	<p>Peter Goff , MGA Planners On behalf of Schaffer Corporation</p> <p>PO Box 104 West Perth WA 6872</p>	<p>On behalf of Schaffer Corporation we make the following submission on the proposed Design Guidelines for Robb Jetty and Emplacement within the Cockburn Coast Development.</p> <p>As a general statement, Schaffer Corporation believes that there should be provision for much greater variety in housing styles within the Robb Jetty Local Structure Plan area and over the company's holding in particular. Schaffer Corporation is of the view that multiple dwelling housing styles are very sensitive to market conditions and because of the long construction time represent a particularly risky housing strategy. The opportunity should therefore be available to developers to meet market conditions by providing a greater variety of housing styles. The guidelines should therefore anticipate the</p>	<p>This matter has already been raised in submission on the Robb Jetty Local Structure Plan. Two storey detached dwellings are at odds with the overall vision for the Cockburn Coast development.</p>

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		<p>development of 2-3 storey attached and detached housing.</p> <p>In addition to the above general comment, Schaffer Corporation finds the design guidelines both confused and confusing. For example, the guidelines refer to a variety of housing typologies including Activity Centre - Main Street Typology, Mixed Use - Cockburn Road Typology, High Density Residential Typology and Medium Density Residential Typology. These built form typologies are located on Figure 3 and the Schaffer Corporation and holding is shown to be exclusively within the High Density Residential Typology. The guidelines relating to the High Density Residential Typology advise that in relation to building height, development shall be in accordance with the Building Height Plan at Figure 16. In turn, Figure 16 advises that development over the Schaffer Corporation land shall be 3-5 storeys in height. In fact, 3-5 storeys of height applies to all of the High Density Residential Typology within the Robb Jetty Local Structure Plan area.</p> <p>However, the Figures 9, 10 and 11 demonstrating how building heights and setbacks apply show buildings of 8 storeys in height. Schaffer Corporation is therefore confused and uncertain whether it is constrained to a maximum height of 5 storeys or is able to construct buildings to 8 storeys in height.</p> <p>In this regard, should multiple dwellings be constructed on the land, Schaffer Corporation supports development to 8 storeys in height as shown within the particular diagrams relating to the High Density Residential Typology within the guidelines.</p> <p>There is also confusion in relation to the required building levels. The requirement at Assessment Criteria ii of 1.3.3 to maintain a floor to floor height of 3.1 metres is queried. Ceiling heights are generally allowed to a minimum of 2.4 metres. There is no clear indication as to why a floor to floor height of 3.1 metres is required. There is no indication for example as to why a higher floor to floor height might not be chosen particularly if a building included an expansive foyer. There is no reason provided as to why a lower floor to floor height could not be utilised.</p> <p>Also at Assessment Criteria iii in relation to building levels, it is stated that the ground floor adjacent to a street or public open space</p>	<p>There is a small section of land in the north west corner and land up in the Emplacement area which enables 6-8 storey development. It should also be kept in mind that if affordable housing bonuses were utilised, this may see higher development than indicated in the Building Height Plan occurring.</p> <p>This has been proposed by the applicant. It needs to be kept in mind it is an 'acceptable development' criteria and developers can still propose a reduced floor to floor (as long as minimum ceiling heights are incorporated). This can be assessed as a performance criteria against the objectives.</p> <p>This appears to be a typographical error. The correct wording should read:</p>

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		<p>requires a grade separation of 0.5 metres and 1.2 metres. It is suggested that it can only be one and cannot be both. Possibly the word "respectively" has been omitted in which case, there is no explanation of why a 1.2 metre separation is required to the level of the adjacent open space.</p> <p>The provisions on Building Material will make the use of tilt-up concrete panels difficult, particularly if they are to be painted or spray coated. Notwithstanding the opposition to tilt-up concrete panels, many of the images used in the document are of painted/spray coated tilt-up buildings. It is ironic that these existing buildings are used as images of how the Cockburn Coast should be developed and to then outlaw the very same materials.</p> <p>Tilt-up concrete panels, both painted and spray coated have been successfully used in many upmarket apartment buildings from Burswood to the Perth CBD, Scarborough to Leighton. There is no justification for outlawing these cost effective and attractive building techniques.</p> <p>Figure 9 purports to show a typical cross section of a high density residential development abutting public open space but, no open space is shown and the building cross-section appears to front a public street.</p> <p>With regard to setbacks, the street setback for buildings up to 3 storeys is 3 metres and above 3 storeys is 5 metres. As Council is aware, there is a high tension power line running down the western side of Bennett Avenue. The cost of undergrounding that power line is prohibitive and abutting land owners are not willing to pay the cost of undergrounding this line. It is understood that retention of the above ground high tension line will result in the need for greater setbacks to the western alignment of Bennett Avenue. This requirement is not addressed in the design guidelines and indeed, the design guidelines conflict with the requirement.</p> <p>In Schaffer Corporation's view, the guidelines need to be redrafted and land owners consulted again and given the opportunity of</p>	<p>"...grade separation of <i>from</i> 0.5 metres <i>to</i> 1.2 metres between the finished floor level of the ground level of the ground floor and the adjacent street or public open space...".</p> <p>This can be corrected.</p> <p>This is an 'acceptable development' criteria and developers can still propose use of tilt up. This can be assessed as a performance criteria against the objectives. The imagery used are illustrations, however, where they are indicating building materials contrary to the content of the Design Guidelines, these can be removed.</p> <p>This appears to be a typographical error and should be corrected.</p> <p>The Design Guidelines provide the general overview of development standards. If there is a site specific constraint such as an easement, this will supersede a conflicting development standard set out by the Design Guidelines.</p> <p>A number of changes have been recommended to the Design Guidelines, however, it is not considered</p>

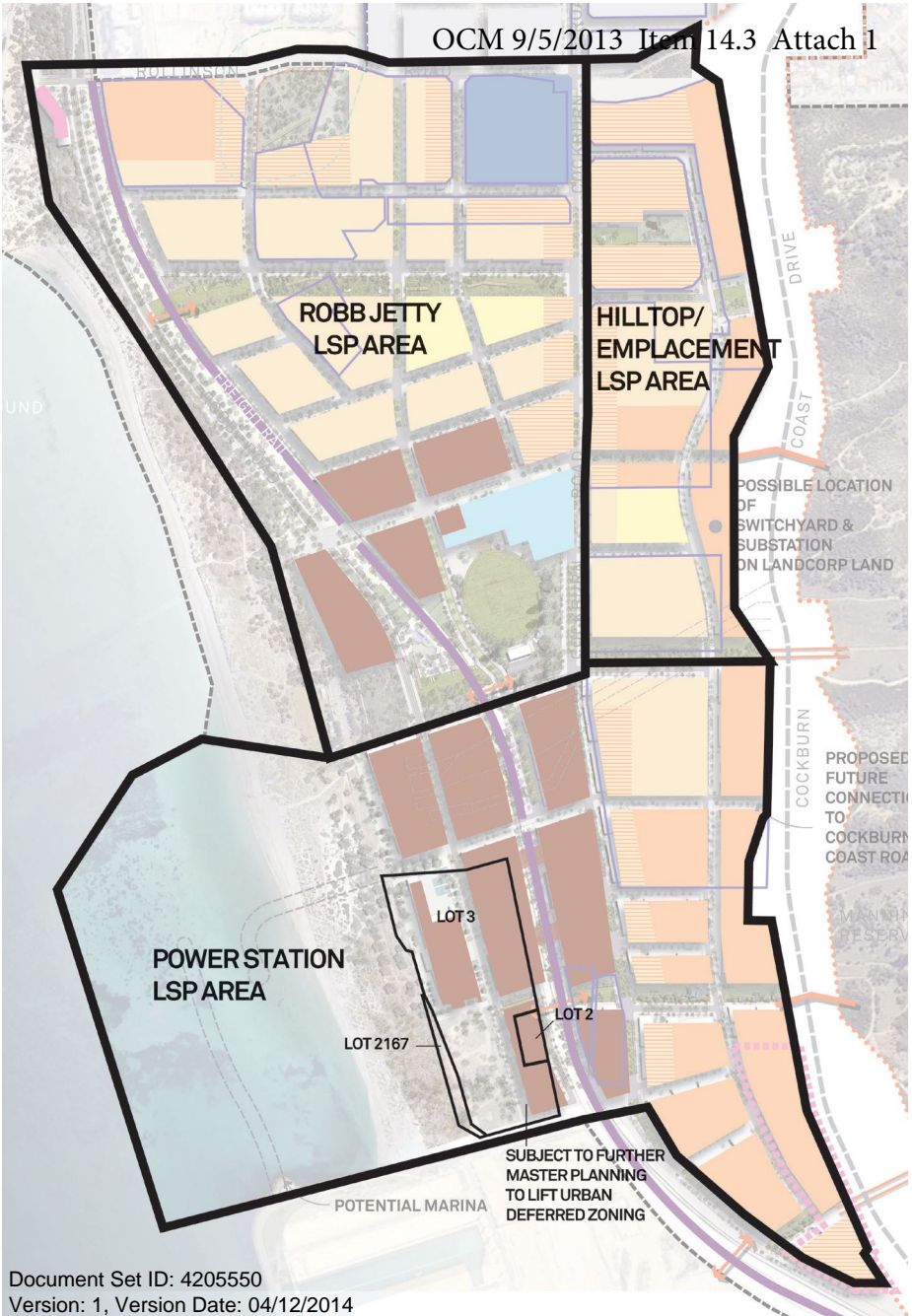
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		providing comment on the revised guidelines. As discussed, it is unclear what the guidelines are in relation to many aspects of the development of the Schaffer Corporation land. Should you require further advice in relation to this submission do not hesitate to contact us.	necessary to readvertise these changes. There is sufficient flexibility in the assessment options provided by the Design Guidelines for applicants to use while still setting broad expectations of the development's objectives.
6.	State Heritage Office PO Box 7479 Cloisters Square Perth WA 6850	<p>The State Heritage Office provided comment on the respective LSPs in November 2012.</p> <p>The following comments regarding the Draft Design Guidelines are made on behalf of the State Heritage Office:</p> <ol style="list-style-type: none"> <li>1. The Draft Design Guidelines could potentially make appropriate reference to the objectives in relation to State and local heritage places (and other sites of historic interest) that were outlined in the respective LSP documents. Furthermore, the manner in which it is intended for public spaces to interface with heritage places (such as the Robb Jetty Chimney or Manning Estate) could potentially be dealt with in Part 2 of the Draft Design Guidelines.</li> </ol>	<p>Noted.</p> <p>A recommendation has been included to remove Part 2 (Public Realm) from the Design Guidelines and include in a separate document. However, this is certainly an issue which can be addressed through the standards for the public realm.</p>
7.	Ashley Palmer, Alba Edible Oils 2 Emplacement Crescent Hamilton Hill WA 6163	<p>This submission is made on behalf of Alba Edible Oils, 2 Emplacement Crescent, Hamilton Hill WA 6163.</p> <p>Alba Edible Oils have been active participants in the planning stages for the Cockburn Coast area. Ashley Palmer CEO of Alba Edible Oils has been on the reference committee for the district structure plan and the Cockburn Coast Planning Committee. He was also active with state government before any of the proposed changes to this industrial area were announced, keeping them informed of his capital spending and obtained assurances that his business could keep investing in this industrial area.</p> <p>Alba Edible Oils now operates the only edible oil refinery in Western Australia, for which it has won a number of awards. It is currently in the middle of a significant investment program on this site with assistance of the federally funded clean technology investment program.</p> <p>Alba has clearly stated that it intends to remain operating at its current site for the longer term and has no intention to relocate.</p>	<p>Noted, however, it is not appropriate to include</p>



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		<p>Alba is able to continue its operations under the non-conforming use rights. Alba has concerns that the Emplacement Local Structure Plan and the supporting draft guidelines as currently proposed will have an adverse impact on the operations of the business.</p> <p>Design Guidelines Alba objects to the proposed design guidelines for the Emplacement Local Structure Plan(LSP). These guidelines do not address our concerns and objections raised in relation to the proposed LSP. A copy of our previous submission is enclosed at the end of this document. I will therefore attempt to limit repeating the points contained in that document here.</p> <p>Emplacement Local Structure Plan We take cause with the proposed buffer for our site. In the Robb Jetty Structure plan section 4.5.1, the premise for only having a 50 metre buffer for the pumping station is based on EPA Guidance statement 3. However this same guideline has been disregarded for Emplacement Crescent, where the EPA guideline is for a 500 metre buffer. We would suggest that this lack of consistency requires explanation. Alba has requested a 500 metre buffer due to the fact that we have already spent significant money and time dealing with complaints from residents after council approved the development of townhouses on Bellion Drive.</p> <p>Protecting Existing land uses Details on how existing Industrial land users are going to be</p>	<p>development standards for non-conforming uses in the Design Guidelines for the Cockburn Coast area. These guidelines are intended to guide new development in line with the proposed vision for this area.</p> <p>Responses to comments on the local structure plan comments may be found in the Schedule of Submissions for Robb Jetty Local Structure Plan.</p> <p>The local structure plans note a process (as outlined in the draft State Planning Policy for State Industrial Buffers) which enables landowners the opportunity to submit a technical analysis of the buffer issues to refine them if necessary. The buffers as set out in the draft SPP are generic only. It is up to the local government to determine the appropriate buffer which should apply. Council officers are familiar with the operations of this business, any historical issues (such as complaints) and where improvements have been made to address any issues. With that knowledge, they are able to make a judgement as to what an acceptable buffer may be, in this case, refining the generic buffer (which could include any such operation in the State) to recognise the site specific factors of this development. It is noted a landowner will most likely desire the larger buffer. Note, the buffer for the waste water pumping station has been now reduced to 25m (measured from the cadastral boundary).</p> <p>Ultimately non conforming use right are set out in the City's Town Planning Scheme. It is not appropriate to</p>

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		<p>protected under the LSP are limited and we would like to see much more detail.</p> <p>Road Network and Traffic Management Alba has consistently expressed its concerns regarding road networks and traffic management in the whole of the Robb Jetty and Emplacement Industrial zone. When we started this process there was to be a new Cockburn Coast Drive and an extension of Rollinson Road so that heavy vehicles and the majority of traffic would be removed from this zone.</p> <p>We have recently been informed that neither of these two roads has either a start date or budget plan from Main Roads.</p> <p>It is our opinion that at a minimum, the extension of Rollinson Road should be completed and that the Cockburn Coast Drive should be approved. Using the existing Cockburn road will cause major issues for both current Industrial businesses and for new residential users. We are already experiencing long delays in exiting Emplacement Crescent and dangerous incidents are being reported when crossing onto Cockburn Road. These have drastically increased since the Spearwood Avenue extension and the development of the Port Coogee estate.</p> <p>It has been proposed that the upgrade of Cockburn Road to a primary arterial road become part of our Development Contribution. We object with this and believe this is the responsibility of Main Roads. I have requested (see attached e-mail) at a number of development meetings that data be provided to show the effects of both the Spearwood Avenue extension and Port Coogee estate development on traffic volume compared with both five and ten years previous. To date I have not received this data, however I am sure that it will show that the traffic increase has been significant from south of the Robb Jetty and Emplacement zones. Despite this, Main Roads investment in this road has been limited.</p> <p>Alba thanks you for the opportunity to make our submission. We</p>	<p>include further provisions in either the local structure plans or the Design Guidelines. A number of the comments in this submission have no relevance to development standards outlined in the Design Guidelines and therefore are not discussed further in this schedule of submissions.</p> <p>See comments on Emplacement Local Structure Plan. Upgrades are proposed to Cockburn Road, including signalisation at Rollinson Road.</p> <p>The matter of Cockburn Road and what proportion may be appropriate to include in the Development Contribution Plan for Cockburn Coast will still need to be considered. This is not a matter related to the Design Guidelines, see the report on the Emplacement Local Structure Plan.</p>

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		<p>recognize there are many different and competing issues with a change to use of any area. We would appreciate the opportunity to meet to discuss the matters raised in both this submission and our previous one before council meet to consider the Local Structure Plan. We will be in contact in the near future to arrange a meeting.</p> <p>Copy of submission made on structure plan enclosed with submission</p>	<p>See responses detailed in Schedule of Submission on Emplacement Local Structure Plan.</p>



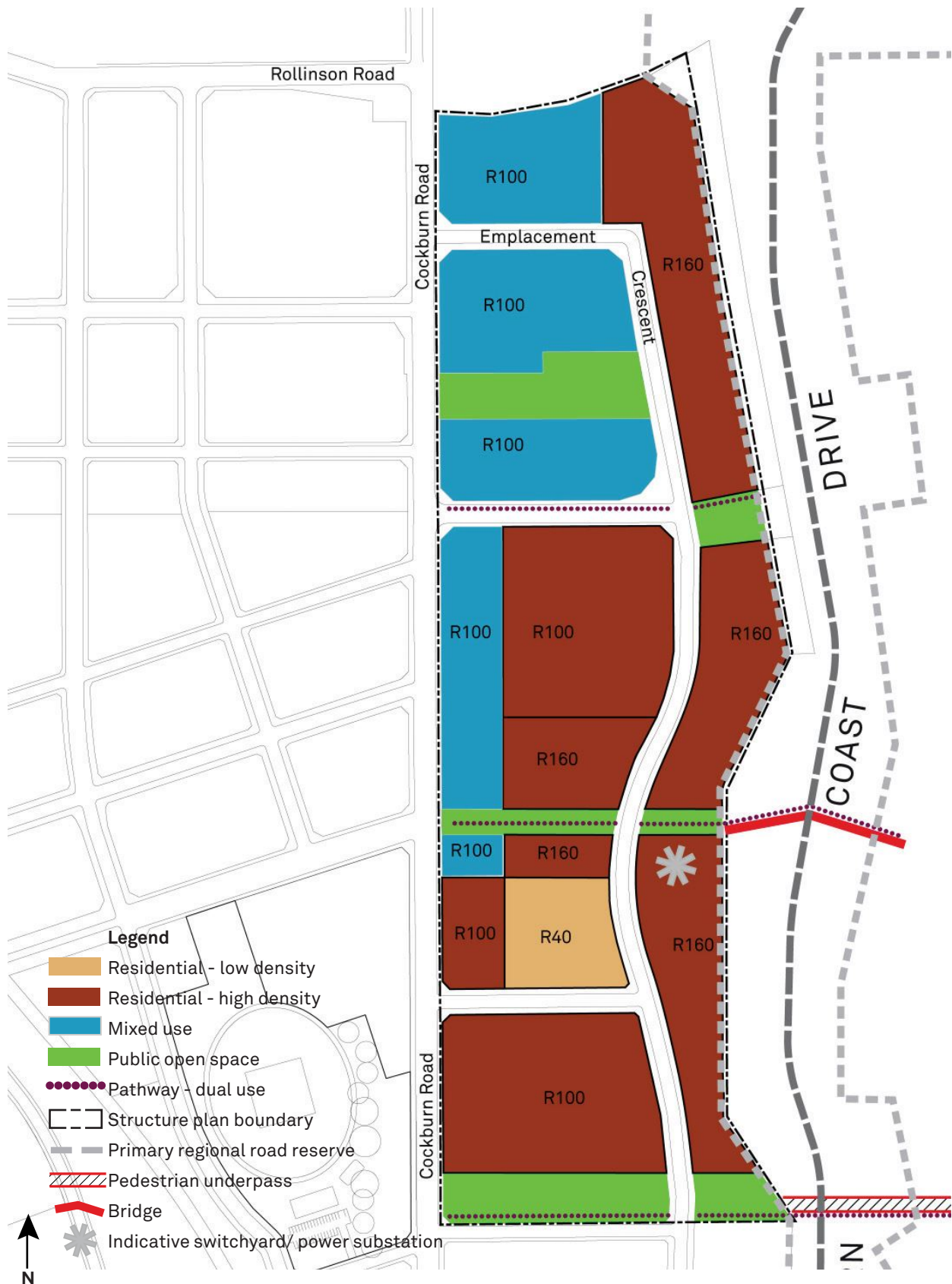


Figure 01\_Local Structure Plan Map

**SCHEDULE OF SUBMISSIONS**  
**PROPOSED LOCAL STRUCTURE PLAN WITHIN COCKBURN COAST– EMBLEMMENT CRESCENT**

No.	Name/address	Submission	Council's recommendation
1	Telstra Forecasting & Area Planning Locked Bag 2525 Perth WA 6001	<p><b>Support</b></p> <p>Thank you for the above advice. At present, Telstra Corporation Limited has no objection. I have recorded it and look forward to further documentation as the development progresses.</p> <p>Any network extension that may be required for any development within the area concerned, the owner/developer will have to submit an application before construction is due to start to NBN Co. or the Telstra Smart Community website: <a href="http://www.telstra.com.au/smart-community/developers/">http://www.telstra.com.au/smart-community/developers/</a> .</p> <p>More information regarding NBN Co. can be found on their website <a href="http://www.nbnco.com.au/">http://www.nbnco.com.au/</a> . I add this information about NBN Co. as it is not known when services will be available from NBNCo. Telstra may provide services if NBN Co. cannot.</p> <p>Please dial 1100 (Dial before You Dig) for location of existing services.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
2.	Department of Education 151 Royal Street EAST PERTH WA 6004	<p><b>Support</b></p> <p>Thank you for your letter dated 19 November 2012 regarding the Robb Jetty and Emblement Local Structure Plans.</p> <p>The Department of Education has reviewed the document and advises that its requirements regarding educational facilities are adequately catered for within the proposed residential developments.</p> <p>Therefore the Department has no objection to the proposed structure plans.</p>	<p><b>Noted.</b></p> <p>No changes are requested/required as a result of this submission</p>
3.	Department of Water PO Box 332 MANDURAH WA 6210	<p><b>Support</b></p> <p>Thank you for the referral of the above Local Structure Plans (LSPs) received with correspondence dated 19 November 2012. The Department of Water (DoW) has reviewed the proposal and wishes to provide the following advice:</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>

No.	Name/address	Submission	Council's recommendation
		<p><b>Better Urban Water Management</b></p> <p>Consistent with the Western Australian Planning Commission's (WAPC) <i>Better Urban Water Management</i> (BUWM) (WAPC, 2008) document and the policy measures outlined in <i>State Planning Policy 2. 9 Water Resources</i>, the proposed LSPs should be supported by a Local Water Management Strategy (LWMS) prior to the approval of the proposed LSPs.</p> <p>The supporting documents, <i>Robb Jetty Local Water Management Strategy</i> (GHD, November 2012) and <i>Hilltop Emplacement Local Water Management Strategy</i> (GHD, November 2012) was deemed satisfactory to the DoW as noted in correspondences dated 21 November 2012. Accordingly, the DoW has no objections to the proposed LSPs. An Urban Water Management Plan (UWMP) will be required as a condition of subdivision in the future, in accordance with BUWM 0/JAPC, 2008) and shall describe and illustrate a greater level of information for storm water design principles and infrastructure to be implemented on site.</p>	
4.	Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	<p><b>Support</b></p> <p>Thank you for your letter of 19 November seeking comment from the Department of Indigenous Affairs (DIA) with respect to the Robb Jetty Local Structure Plan (Robb Jetty LSP) and the Emplacement Local Structure Plan (Emplacement LSP). I reviewed the documents provided and offer the following comment.</p> <p>The area to which the Robb Jetty LSP applies has a slight intersection with Aboriginal heritage site DIA 3707 (Robb Jetty Camp). Accordingly, if any development associated with the Robb Jetty LSP will impact the Aboriginal heritage values of DIA 3707 (Robb Jetty Camp) then the prospective developer is encouraged to contact DIA in order to ascertain the need for prior approval under the <i>Aboriginal Heritage Act 1972</i> (AHA).</p> <p>DIA notes the existence of the Cultural Heritage Strategy and the intention to interpret the heritage values of the Robb Jetty LSP area, including DIA 3707 (Robb Jetty Camp). This is seen as a positive initiative which will assist in public understanding and long term heritage management for the area. Due to the long term association of Noongar people with DIA 3707 (Robb Jetty Camp), and the high level of significance accorded this place by the contemporary Noongar population, it is recommended that consideration is given to consulting with relevant Aboriginal people when developing interpretation for the Robb Jetty Camp.</p>	<p>Noted. The proponent of the local structure plan has been provided with the content of this submission.</p> <p>Noted</p> <p>Noted. This information would be useful for other developers as well. Therefore, the City will add this information to its webpage on</p>

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		<p>DIA is unaware of any Aboriginal heritage values which intersect with the area to which the Emplacement LSP applies. It is also suggested, that prior to development occurring within the areas to which the LSPs relate, that prospective developers have their attention brought to the existence of the State Cultural Heritage Due Diligence Guidelines. The Guidelines are intended to assist prospective developers in assessing the risk that a proposed development may have on impacting Aboriginal heritage values and whether or not consent under the AHA should be sought prior to the development occurring. The guidelines can be found at:</p> <p><a href="http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHA">http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHA</a></p>	<p>Heritage matters.</p> <p>No changes are requested/required as a result of this submission. However, the City's website has been updated to include a link to the Department's Guidelines.</p>
5.	<p>State Heritage Office PO Box 7479 Cloisters Square PO WA850</p>	<p><b>Support</b></p> <p>Thank you for providing the opportunity to provide input to the Robb Jetty and Emplacement Local Structure Plans which were correspondence received on 19 November 2012.</p> <p>The State Heritage Office is supportive of the broad objectives to conserve and retain state and local heritage places within the local structure plans. We are particularly supportive for the retention of the Rob Jetty remnants and confirmation that any future development will be in accordance with State and local heritage policies and procedures.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
6.	<p>Resident Hammond Park</p> <p>Details to be kept confidential</p>	<p><b>Support</b></p> <p>I absolutely support both rob jetty + emplacement project.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
7.	<p>Resident COOGEE</p> <p>Details to be kept confidential</p>	<p><b>Support</b></p> <p>I fully support the Robb Jetty LSP and Emplacement LSP. At the moment, I feel the entire Cockburn Coast area is not being utilised to its full potential. Currently we have the Port Coogee and South Beach redevelopment, but nothing in between.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
8.	<p>Dan Sheikh 9 Modong Nook SUCCESS WA 6164</p>	<p><b>Support</b></p> <p>I absolutely support this plan for the Cockburn Coast. Most of Perth's beaches are full of McMansions and sprawl. This area has the potential to be a vibrant, residential hub on the ocean with shops, cafes, restaurants and bars. It will be vibrant due to the resident population of the area, 10,000 residents (which I think should be double). Also higher</p>	<p>Noted. It is not realistic to double the number of proposed residents at this stage. All the preliminary planning done for Cockburn Coast is predicated on approximately 10,000</p>



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		densities combat urban sprawl. This is an area ppl will be willing to buy into if it is not done in a half hearted manner.	residents.  No changes are recommended as a result of this submission
9.	Hugh Hyland 19 Buchanan Rise COOGEE WA 6166	<p><b>Support</b></p> <p>The switch-yard at the old power station needs to be moved inland as far as possible.</p> <p>Commuters need to be encouraged onto public transport. Robb Jetty and Emplacement will substantially add to the number of residents and local staff in the area. Adequate public transport is essential for Perth's future, and railways are a most essential part of this. Passenger services need to be restored along the rail line from Fremantle to Robb Jetty and on to Spearwood, and then continued to Thornleigh. This would provide a quick service to Fremantle and on to the city, as well as a ring route bypassing the city and linking up with the Mandurah and Armadale lines. Most of the infrastructure is already there, with double tracks almost all the way, with only a small amount to be re-laid as dual gauge each way and an even smaller amount to be duplicated. Electric trains are more efficient than buses, and are quieter than diesel engines. There would be ample capacity for them and goods trains on such a dual line.</p>	<p><b>Noted</b></p> <p>The proposed Emplacement Structure Plan includes an indicative switchyard /power sub-station site located towards the eastern boundary of the subject area.</p> <p><b>Not supported</b></p> <p>While it is agreed that commuters need to be encouraged onto public transport, the proposed use of the heavy rail line and restoration of services from Fremantle to Robb Jetty and on to Spearwood and Thornlie relate to the regional network and are beyond the scope of the Local Structure Plans. The option of potentially using the freight rail for passenger services was evaluated at the Cockburn Coast District Structure Plan (Part 1) stage, and was discounted due to high costs and other constraints.</p>
10.	Nandi Chinna Ommanney Street Hamilton Hill 6163	<p>To whom it may concern Regarding the Cockburn Coastal Development plans; Robb Jetty and Emplacement Structure Plan. I commend the high density aspect of the plans. High density housing connected to public transport nodes is a way of reducing the need for further land clearing on the Swan Coastal Plain and reduces dependence upon cars and road travel.</p> <p>However there are some serious concerns regarding the position of the development and the construction of new roads. It appears that many of the concerns raised in the original community consultation have not been taken into account. The retention in the plan of the construction of a new MRS primary road indicates that environmental and heritage issues have not been given sufficient consideration. Although the alignment of Cockburn Coastal Drive has been revised to reduce the impact on the ridgeline and Beeliar Regional park, the proposed road is still a major arterial road and will impact significantly on the bushland and have considerable detrimental consequences to the</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p> <p><b>Not supported</b></p> <p>The Primary Regional Road Reservation falls outside the Emplacement LSP area, and was dealt with through the district structure planning, and MRS Amendment No. 1180/41 (Cockburn Coast District Structure Plan). MRS Amendment No. 1180/41 included a revision to the alignment of the reservation that has enabled an additional 5.7 ha of</p>

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		<p>sustainability of Beeliar regional Park. The inclusion of Cockburn Coastal Drive negates the professed sustainability of the regional plan. The construction of a major arterial road that promotes the movement of heavy traffic through the area will divide the community and have a destructive impact on protected species of native fauna. As suggested in the original community consultation, Cockburn Road as it currently exists should be upgraded and heavy traffic diverted using existing routes such as Stock road. Freight by rail should be increased and alternative transport systems implemented. Light rail, heavy rail, and a network of bicycle paths would help to reduce dependency on cars. The loss of natural vegetation and the fragmentation of remaining bushland in Beeliar regional Park by building of the arterial road will significantly impact on the area. Many birds and reptile species inhabit the area and these species move through the area to feed on vegetation or to hunt. Endangered species including Carnaby Black Cockatoo, Blue Wrens, and the Black Shouldered Kites nest in the area. The Nankeen Falcon, Australian Small Eagle and Peregrine Falcon- migratory species such as the Bee-eater and priority species like the Lined Burrowing Skink and Black Striped snake are also found in the area. It is imperative that an independent environmental impact study be undertaken before this road is considered. The Department of Environment and Conservation has stated that, 'protected areas are essential to maintain natural and cultural diversity and to foster a sense of place and belonging and contribute to the values of our community.' The EPA claims that native vegetation needs to be protected to preserve biodiversity and as green areas to absorb carbon emissions.</p> <p>The Emplacement Precinct involves the removal of bushland in addition to the negative impact of the road.</p>	<p>former road reservation land to be included within the Beeliar Regional Park Management Plan. In addition, the actual road will be designed to minimise the amount of vegetation to be cleared, supported by further more detailed fauna and flora surveys. The MRS Amendment was referred to the Environmental Protection Authority, who considered the environmental impact of the road reservation, and advised that it did not require formal assessment under Part IV of the Environmental Protection Act 1986.</p> <p><b>Noted</b></p> <p>The Ecological Assessment identifies that there is remnant vegetation within the southern area of the Emplacement LSP area, some of which constitutes habitat for the Carnaby Black Cockatoo. Based on the draft Department of Sustainability, Environment, Water, Population and Communities (DSEWPaC) referral guidelines, clearing the vegetation and development of the Hilltop/Emplacement Crescent Project Site would have a direct impact on Black Cockatoo feeding habitat and given this future referral to DSEWPaC may be required (i.e. prior to subdivision or development).</p> <p>The Ecological Assessment for the Emplacement LSP also notes that Vegetation type 1, which occurs on the limestone ridge on the eastern side of the Project Site, has similarities to a DEC-listed TEC, (<i>Melaleuca huegelii</i> — <i>Melaleuca acerosa</i> [currently <i>M. systema</i>] shrublands on limestone ridges). A vegetation survey in spring (when annual species are present) would be required to confirm this. I</p>

No.	Name/address	Submission	Council's recommendation
			<p>It is therefore recommended that the Emplacement LSP report be amended to include the requirement for a spring survey to be conducted prior to any subdivision or development (that proposes works to the land).</p> <p>The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense urban development that reduces the need for housing on the urban fringe. The City must plan for population growth and Directions 2031 and Beyond sets the spatial framework for how the metropolitan region will grow. It seeks to ensure urban growth is managed, and to make the most efficient use of land available.</p> <p><b>Not supported</b></p> <p>The Emplacement LSP and associated Cockburn Coast Cultural Heritage Strategy will ensure the retention and protection of the remaining gun emplacement. Specifically, the Emplacement LSP includes the gun emplacement within public open space to ensure that this important feature is not subject to development pressure. The two other gun emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the Heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the</p>

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		<p>I am also concerned that WWII historical sites belonging to a complex military infrastructure associated with the gun emplacement sites and which are yet to be studied will be destroyed by the development.</p>	<p>Leighton Battery did not reveal that tunnels were associated with the South Beach Battery site.</p> <p><b>Not supported</b> It is agreed that horse heritage is an important consideration, and that is why it has been considered from the District Structure Planning stage through to the Local Structure Plans</p> <p>The LSP and associated Cockburn Coast Cultural Heritage Strategy identify and recognise the importance and heritage value of the South Beach Horse Exercise Area. The LSP (pg 60) states 'the aim is for horse facilities to remain at McTaggart Cove to provide facilities for horses with a horse float car park, where the dunes are lower and there will be less disturbance to future residential uses, thus minimising potential land use impacts.' A key objective of the Heritage Strategy is that "South Beach should continue to be used for the horse training, a use with which it has had a long association'.</p> <p><b>Not supported</b> The assessment of the coastal vulnerability and the coastal setback to the proposed development has been completed in accordance with the 2012 draft State Coastal Planning Policy. This includes a full assessment of the potential impacts of a rise in sea level of 900 mm over the coming century. This value of sea level rise would accommodate an average rise of 9 mm/year over the coming century. This value was</p>

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		<p>The other issue which I feel as not been given proper consideration is the horse heritage of the area; This is a living heritage which has a long and colourful history in the community. The road potentially will isolate the heritage listed property of Randwick Stables from the heritage listed beach as it will make access difficult. Horses do not go through tunnels or use overpasses. Many members of our community also support keeping the beach as an area for animal exercise and I am pleased that the structure plan supports the ongoing use of the beach for these purposes. I hope that this will not be compromised as the development unfolds.</p> <p>Insufficient consideration has been given to predicted sea level rise. Statistics from the Australian Cities report indicate that sea levels along WA's coast are rising by between 9mm and 10mm per annum, three times the global average! It is going to be an extremely costly exercise to be considering situating the development so close to the coast in this very low lying area. The Insurance council of Australia states that 'the coastal risks of storm surge, coastal erosion and gradual sea level rise are excluded by</p>	<p>adopted by the Department of Transport and the Department of Planning in 2010 after careful consideration of the data and projections.</p> <p><b>Not supported</b></p> <p>With regard to noise emissions from freight trains, under Implementation Guidelines for SPP 5.4 'Road and Rail Transport Noise and Freight Considerations in Land Use Planning', where the number of movements is not defined, 24 train movements per 24 hour day should be used. However, to ensure some "future proofing" the modelling undertaken by the Noise and Vibration Strategy which forms part of the LSP has recommended a higher standard to SPP 5.4 by recommending the assessment of each development be based on that of the highest single train movement rather than an average.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p> <p>The Noise and Vibration Strategy forms an addendum to the LSP's and shows the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The design guidelines will outline the requirements for compliance with noise and vibration for land within the impact zone. The Design Guidelines will also include requirements for Notification on titles.</p>

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		<p>many general insurance policies in Australia.</p> <p>Consumers should ensure they are familiar with their policy and are aware of what risks the policy will not respond to' (<a href="http://www.insurancouncil.com.au/issues-submissions/industry-in-focus/coastal-vulnerability-risks">http://www.insurancouncil.com.au/issues-submissions/industry-in-focus/coastal-vulnerability-risks</a>). On October 30, 2012, ABC news reported that the South Gippsland Shire Council plans to cut its ties with the committee it set up to maintain seawalls along the Corner Inlet coast. By cutting its ties with the group, it can no longer be sued if homes are inundated by sea level rises. Karl Sullivan, from the Insurance Council of Australia stated that residents will be unable to insure their homes against gradual sea level rises. "If it's a single large event, generally you will find a lot of people will have cover for these things but a gradual increase in sea level, over many decades that gradually ... [submerges] the house, is not really contemplated under most policies," he said. "From a residential perspective, there's really no cover available globally to protect yourself [from] a gradual sea level rise and loss of amenity of a property." These scenarios are becoming more common on the east coast of Australia, so why, with sea levels in WA set to rise at a rate three times higher than the global average, is Cockburn ploughing ahead with housing developments so close to the coast. Surely it cannot be ignorant of this kind of data? If not then may I suggest that this development is driven by short term financial gain with little thought of the cost to future generations of flood mitigation and property damage due to sea level rises?</p> <p>The other important issue that has not been duly considered is the proximity of the development to freight rail lines. With more and more freight set to be transported by rail to relieve pressure on congested roads, the freight rail line that runs through the development site needs to be given high priority over housing set close to its trajectory. It is easy to predict that people who buy residences situated along this rail line will soon be complaining of noise and pollution threats to their homes, and will be calling for sanctions to be placed on the movement of freight rail which at present moves along the line at all hours of the day and night. In conclusion I feel that there are many issues that have not been adequately addressed in the plan, in particular the ones I have mentioned in the above submission. I hope that due consideration will be given to these important issues.</p>	
11	N S McNally PO BOX 1000 CANNING BRIDGE WA 6153	<p><b>Objection</b></p> <p><i>With respect, the Cockburn Coast Plan looks as if It has been drafted with no proper vision whatsoever into the future.</i></p> <p>The following notes should be considered seriously before any of the current proposals</p>	

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		<p>are adopted:</p> <p><b>Contamination</b>  The majority of the land involved in the proposal is seriously contaminated. The entirety of the land should be subjected to a well-planned decontamination procedure so that a fragmented approach to the clean-up does not occur. The decontamination plan should also include the land in the South Fremantle Landfill Site within the boundaries of the City of Fremantle. Decontamination of the subject Cockburn Coast land without a parallel consideration of the Fremantle Landfill site will seriously impact on the cost of processing the landfill site in the future. If an overall decontamination program for all of the land under consideration for development is not planned and implemented as a single operation (over time) then the economic viability of the future development of some of the land will be dramatically affected. The effect of this may be that the proposed development will suffer from lack of coordination which may result in the overall project not taking ten to fifteen years but more like forty or fifty years.</p> <p><b>Building Height Plan</b>  The proposed building height plan should be scrapped completely. There should be no height restrictions. Projects should be assessed on a performance based criteria that assesses the overall height of proposal based on what the proposal contributes to the amenity of the area. Other design criteria such as environmental benefits, sustainability etc. etc. affordable housing ratios, etc. Can be associated with height allowances and increases and so on. The overall development of the area will progress as a dynamic development and result in a much more appealing built environment than what can be expected from the proposed homogenous ~ boring development parameters proposed in the current Cockburn Coast Plan.</p>	<p><b>Not Supported</b>  The City has no ability under the <i>Contaminated Sites Act 2003</i> to mandate the landowners to coordinate the decontamination of multiple sites.</p> <p>The Emplacement Local Structure Plan is supported by a Contaminated Sites Study (Appendix H). The study includes a preliminary assessment of all lots within the LSP which identifies known and suspected contaminated.</p> <p>By identifying known and suspected contamination sites and making this information publically available the Contaminated Sites Study will aid adjoining landowners to work with each other when undertaking decontamination.</p> <p><b>Not supported</b>  The application of building height control is a long standing and well established planning convention. Building height controls are driven by design considerations including over shadowing, protection of vistas and important view lines and creating a consistent built form character. In addition, it is noted that proposed building heights have been a recurring theme of interest to the wider community, and inclusion of a building height plan provides a mechanism to address these concerns and provide a level of</p> <p>The building height controls outlined in the Emplacement LSP and the draft Design Guidelines for Emplacement and Robb Jetty</p>

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		<p><b>Residential Zone</b></p> <p>There is too much emphasis on residential areas (on <i>land seriously contaminated with lead</i>.) The whole of the area should have a blanket zone allowing mixed business, commercial, residential projects. Leave the vision to the architects who should have a blank sheet to work with - not a Planning 101 TPS that shrieks of dullness. An openness of planning requirements will attract a much wider variety of developers with a far greater range of plans and visions than that which might result from the current proposals.</p>	<p>Precincts are performance base. Variations to height will be permitted when various design criteria are met which consider urban character, streetscape amenity and overshadowing.</p> <p><b>Not supported</b></p> <p>Concentrating commercial uses in certain areas like adjacent to Cockburn Drive and in Robb Jetty District Centre promotes the creation of lively nodes of activity. Cockburn Coast is not expected to accommodate a significant amount of commercial floor space, due to its limited population demand catchment. This makes concentration of commercial floor space more important. Commercial development also benefits co-location by attracting clients/shoppers who are looking to satisfy multiple needs. Commercial uses adjacent to residential uses can also create amenity issues which are more easily planned and a designed for in certain identified locations. The local structure plan, design guidelines and Development Area Scheme provisions are considered to provide a unique planning framework that has a level of flexibility so as not to stifle innovation, while providing a level of certainty for landowners and the community.</p> <p><b>Noted</b></p> <p>The City supports the development of Cockburn Coast to its maximum potential with significant commercial and entertainment uses in a compact high density urban form. The project has the potential to accommodate 10,000 people in 5,000 dwellings with supporting employment and retail</p>



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		<p><b>Major City Centre Potential</b></p> <p>The overall area has the potential to become a major city centre area with hotels, multi-storey office and residential buildings, substantial retail complexes along with significant social and entertainment facilities. The current plan strangles the potential opportunity of the area. Flexibility in project proposals is critical to ensuring the old power station building is revamped and retained. The old building (very very seriously contaminated along with the adjoining switch station) might then be connected directly to a major marina complex built for the use of the people of the region - not just a select few who happen to reside nearby. Think big!</p>	<p>opportunities. The entire project combined which includes the South Fremantle Power Station in a third LSP area allows for the project to become a key metropolitan sub-regional centre.</p> <p>The Emplacement LSP provides for significant development in comparison to metropolitan Perth outside of the CBD. The South Fremantle Power Station is not included in the Emplacement LSP. It will be part of separately prepared master plan and LSP which will be lodged with the City and advertised to the community in the future.</p> <p><b>Noted</b></p> <p>No changes are recommended as a result of this submission. The DoT, CofC, CofF, PTA, MRWA, DoP and LandCorp through the Transport Planning Working Group have completed investigations into possible public transport links from Fremantle Train Station to Cockburn Coast. A study was carried out to consider the best route to link the two areas and the most appropriate technology with a decision being made in favour of a priority bus route for the short to medium term. The route is consistent with the DoT's draft Public Transport Network Plan for Perth which identifies implementation of Bus Rapid Transit to Cockburn Coast by 2020 extending to Rockingham by 2031. The route investigation also included 'future proofing' that would enable the Bus Rapid Transit system to convert to Light Rail in future.</p> <p><b>Supported</b></p> <p>The Emplacement and Robb Jetty LSP are supported and informed by the Cockburn</p>

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		<p><b>Linkage with City of Fremantle</b>  The area has the potential to become the major business centre south of Fremantle. The pressure to develop within the centre of the old Fremantle Town area will be alleviated. The two areas will complement each other over future years with Cockburn Coast being the vibrant modern business and residential area while Fremantle can retain its historical/cultural port city role.</p> <p><b>Public Transport Systems</b>  Public transport systems must be designed into the area. The ideal plan will link the City</p>	<p>Coast Local Transport and Traffic Management Strategy. The Strategy and the LSPs proposes a rapid transit system through Cockburn Coast which connects up to Fremantle in the north and could be extended through to Cockburn Central as part of wider public transport investments in Perth.</p> <p><b>Not Supported</b>  The City of Cockburn does not support the construction of Roe Highway west of Kwinana Fwy due to the environmental value of the reserve and the negative environmental impact of the extension. It should also be noted that the Fremantle Eastern Bypass was removed from the Metropolitan Region Scheme in 2004, and disposal of the land to private ownership is now well advanced with development already occurring within the former reservation.</p> <p><b>Not Supported</b>  The Emplacement LSP provides for medium and high density development and the Cockburn Coast project as a whole is expected to accommodate 10,000 people. Proposed building heights are primarily between 6-8 storeys (high density) and 3-5 storeys (medium density), and it is not considered that this equates to 'low-rise suburbia'. Only a small pocket of land within the Emplacement LSP area is identified for low density (1-3 storeys), to provide the potential for housing options for families. In addition, the proposed residential codings are supported by proposed Scheme provisions that mandate minimum densities to ensure the vision for Cockburn Coast is achieved.</p>

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		<p>of Fremantle to the Cockburn Coast land with a further linkage to Cockburn Central.</p> <p><b>Roe Highway Linkage</b>  The City of Cockburn and the City of Fremantle should lobby to get the Roe Highway and the Eastern By-pass constructed as soon as possible. The Cockburn Coast land will then have the ability to attract major international and national developers to the site who specialise in major hotel, residential and entertainment projects. The Cockburn Coast and the City of Fremantle will benefit directly from the ability of visitors to virtually drive or be transported directly to the area from the domestic and international airports. (Probably in almost the same time it would take to get from the airport to the City of Perth.) It is imperative that this road connection be constructed- not just for the people of Fremantle and Cockburn - but for all of the future generations of the State.</p> <p><b>Urban Sprawl Vision</b>  The Cockburn Coast area will be a completely lost opportunity if the current plans go ahead. Turning the area into low rise suburbia will be a complete and wanton waste of the potential of the area. Instead of a 'suburban' vision being applied to the land a far greater vision in the form of a major city centre with a much wider variety of land uses and building types should be pursued. The current plan is weak. The plan is just another version of urban sprawl being poured over land that has some of the greatest development potential this State has seen for years.</p> <p><b>Lost Opportunity</b>  To adopt the Cockburn Coast Plan in its current form would be to choke the development potential of the land and create another sector of urban sprawl just for the sake of it. A serious lack of vision is being applied in the current proposal. A lack of vision that if supported will cost this State and future generations hundreds of millions of dollars in lost opportunity. I submit that the entire plan be reviewed and aligned with the true development potential of the land.</p>	<p><b>Not Supported</b>  The Cockburn Coast project is an ambitious urban infill project which envisions an urban form more dense than anywhere outside of the Perth CBD</p>
12	Paul Watson	<b>Objection</b>	

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	56 Davilak Avenue Hamilton Hill 6163	<p><b>SUMMARY OF OBJECTIONS</b></p> <ol style="list-style-type: none"> <li>1. Time allowed for submissions</li> <li>2. Neglect of heritage and environmental values necessary for sense of place</li> <li>3. Contravention of commitment regarding western skyline</li> <li>4. Non-transparency of process for reference group</li> <li>5. Contemptuous treatment of previous submissions by residents in the revision process</li> <li>6. Allowance for one school flawed</li> </ol> <p><b>Time allowed for submissions</b></p> <p>According to the Hon. John Day, Planning Minister (Cockburn Plans Beachside Life Vision for industrial site; "The West Australian", November 21, 2012), the proposed redevelopment will take 15 to 20 years. The majority of Cockburn residents received notice of the proposal when the December edition of Cockburn Soundings was delivered to their mailboxes in early December. It is unreasonable to allow ratepayers less than one month to prepare submissions on a project with such a long disruptive development time and with such long- ranging impacts on the social and environmental fabric of the City of Cockburn. It is only fair to residents and other concerned parties that the WA Planning Commission allows a period of no less than 3 months for such submissions.</p> <p><b>Neglect of heritage and environmental values necessary for sense of place</b></p>	<p>The submission period for local structure plans is guided by the Town Planning Scheme, which are required to follow a format outlined by the State in the 'Model Scheme Text'. The minimum period is 21 days, the City has allowed for 28 days in this case. This is an adequate time period to advertise a document which is a refinement of several other planning documents which have also been advertised over the last nine years:</p> <p>2004: 'Dialogue on Cockburn Coast'  2009: District Structure Plan  2010: Metropolitan Region Scheme amendment to 'Urban'  2011: District Structure Plan (Part 2) and Town Planning Scheme Amendment 89 to 'Development Area'.</p> <p>The LSP and associated Cockburn Coast Cultural Heritage Strategy will not result in the destruction of an important World War II site. There are provisions to protect and retain the remaining Battery. Specifically, in the LPS the area on which the Battery is located has been identified to remain as public open space to</p>

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		<p><b>Heritage</b>  <i>Destruction of heritage sites</i>  The current proposal includes the destruction of significant WWII heritage sites in Emplacement Precinct. This is contrary to claims in the Executive Summary of the Emplacement Local Structure Plan, that it “sets out to establish a sustainable community that celebrates the areas [sic] past”.</p> <p>The Executive Summary describes the history of Emplacement as “...characterised by industrial development including the once pulsating Robb Jetty, Cockburn Coast cattle industry and South Fremantle Power Station”, suggesting that “By recognising and learning from the past”, the Local Structure Plan “lays the foundations for an exciting future”.</p> <p>It is unfortunate that this future will be marred and poorer, due to an examination of the heritage value of the precinct, which has been at best, neglectful and at worst, misleading.</p> <p><i>Cursory attention to heritage in the Local Structure Plan</i>  The Heritage section of the Local Structure Plan comprises eight short paragraphs. Within these, it states that: “The recognition and incorporation of the distinctive heritage of the area is a significant component of the urban renaissance of Cockburn Coast and is integral to creating a distinct and meaningful place. To guide the Local Structure Plans, the Cultural Heritage Strategy includes strategies setting out how to protect and transmit the heritage values of each place, in accordance with relevant legislative requirements”.</p> <p>The Heritage section of the Local Structure Plan acknowledges the military heritage of the area, by identifying the use of the coast for military training during WWII and identifying South Beach Battery (remains) as “a remnant of a larger military complex that has associations with the military defence operations of Western Australia during World War Two”.</p> <p>However, it has omitted to reveal the extent of that larger military complex, which still exists along the ridge and both within the boundaries of areas identified for high-density dwellings, and within the boundaries of the proposed Cockburn Coastal Drive. It's recommendations for the South Beach Battery site advise developers to:</p> <ul style="list-style-type: none"> <li>• Integrate interpretation of the site in the Cockburn Coast project to communicate the tangible and intangible values and history of the place to the community and that</li> </ul>	<p>ensure that this important aspect is not subject to development pressure. The two other emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated/extant with the South Beach Battery site.</p> <p>The Cultural Heritage Strategy does acknowledge that the South Beach Battery was constructed as part of the coastal defence system during World War II for the Fremantle Port. However, the Strategy is not intended to be a full history of the place or of Australian Defence. Rather it identifies strategies for its conservation and interpretation to ensure that it can contribute to the history of the area. As part of any specific interpretation proposal for the site further research would be undertaken.</p> <p>The protection and enhancement of the project area's historical components is also found in the Cockburn Coast Place Making Strategy.</p>

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		<ul style="list-style-type: none"> <li>Consideration should be given to the partial reinstatement of earth embankments to allow an appreciation of its original form</li> </ul> <p>However, it fails to acknowledge the complex infrastructure constructed along the coastal ridge during 1942-1944 to support coastal defence and which still exists today. The Cockburn Coast Cultural heritage Survey, 2012, devotes two and a half pages to Defence heritage, including the area's role as a training ground for the 10th Light horsemen during WWI and in terms of its role in coastal defence during WWII.</p> <p>However, it also fails to identify the coastal infrastructure along the ridgeline, associated with South Beach Battery. I believe it also understates the sense of fear which pervaded the community at that time, by understating the perceived imminence of Japanese invasion by General McArthur, Prime Minister Curtin, and the community in general.</p> <p>Acknowledgement and preservation of the military heritage of this area is essential for the development of a "sense of place", which is seen as intrinsic to effective community development. With invasion at Fremantle of Japanese forces seen as imminent in 1942, real fear was tangible in the community and the Cockburn Coast suddenly became a hive of activity.</p> <p>According to a United States Office of Strategic Services (OSS) report from October 1942, the Japanese were actively planning an invasion of Australia in June or July 1942. The OSS report is based upon information secretly passed to an OSS asset by neutral Spanish diplomatic staff in Tokyo.  <a href="http://australianbunkermilitarymuseum.org/abmm/research-mainmenu-29/14-invasion-threat">http://australianbunkermilitarymuseum.org/abmm/research-mainmenu-29/14-invasion-threat</a></p> <p>In February 1942 after the fall of Singapore, an urgent survey was conducted by the British Admiralty, and Cockburn Sound was selected as an ideal fleet anchorage with its wide expanse of water. Work quickly began on securing the Sound in 1942 and went on 24 hours a day, seven days a week for the two years. Overall the project cost two million pounds.</p> <p>Heavy Artillery was set up along the coast from Swanbourne to Cape Peron, and on Rottnest and Garden Islands, to protect the proposed anchorage and its approaches.  <a href="http://inbox.apana.org.au/?p=210">http://inbox.apana.org.au/?p=210</a> According to the military history website <a href="http://www.ozatwar.com/usnavy/fremantlesubmarinebase.htm">http://www.ozatwar.com/usnavy/fremantlesubmarinebase.htm</a>, "Approximately 170 American, British and Dutch submarines made a total of 416 war patrols out of Fremantle Submarine Base during WW2" and "By the end of 1943, the number of</p>	

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		<p><i>submarines operating out of Fremantle had increased to thirty". According to the Cockburn Coast Cultural heritage Survey, 2012 (pp.22,59), during 1944, "Leighton Battery and Robb's Jetty, Cockburn [also known as South Beach Battery] were the two places earmarked for the location of the new 5.25 inch emplacements and three emplacements were to be constructed at each site. Unlike Robb's Jetty which was built into soil, the limestone at Buckland Hill had to be quarried for the underground tunnel system and the emplacements. In addition, although emplacements were constructed at Robb's Jetty, guns were never installed and the battery was never operational as it was at Leighton".</i></p> <p>According to the military history website <a href="http://www.artillerywa.org.au/RAAHS/history.htm">www.artillerywa.org.au/RAAHS/history.htm</a> , <i>"The Leighton Beach Battery site has been entered in the Register of the National Estate as a significant World War Two Coastal Defence Facility".</i> Leighton Beach Battery was in an advanced state of neglect prior to its recognition as a military heritage site and both State and federal resources have been allocated to its preservation and renovation. It is now a successful tourist attraction and makes a significant contribution to the "sense of place" in the communities of North Fremantle, Mossman Park and Cottesloe.</p> <p>Leighton Beach Battery consists of gun emplacements and the tunnel system associated with them. Both are important components of the heritage site and interpretive tours of the tunnels, together with interpretive signage provide popular educational and recreational activities for young and old, including many school excursions.</p> <p>Although, as mentioned in the Cockburn Coast Cultural heritage Survey, 2012 (pp.22, 59), the South Beach (or Robbs Jetty) Battery was built into sand, the emplacement was not elevated. Consequently, in the event of the guns being fired, artillery fire-spotters were needed to direct the guns' fire to a target. Along the ridge, above, behind and south of the battery, a complex system of tunnels was constructed. Typical of military tunnel systems, fire-spotters had several locations from which they would observe seaward from the coast and tunnels were needed for them to get from one observation point to another without being observed from the sea. The system of tunnels however along the coastal ridge, extends further south than might be anticipated for this purpose. Indeed it has been suggested that a tunnel complex including military bunkers for storage of post-invasion supplies for a resistance exists along the Spearwood Dune System all the way to Kwinana.</p> <p>Evidence of the particular tunnel system in the Emplacement precinct of the</p>	<p>This is incorrect; no land is proposed to be 'removed' from Beeliar Regional Park by the local structure plans. The Emplacement Precinct abuts the current road reserve for Cockburn Coast Drive and sits within the area zoned for 'Urban' purposes under the Metropolitan Region Scheme (MRS). The land which forms Beeliar Regional Park is designated 'Parks and Recreation' under the MRS.</p>

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		<p>development zone exists, which can be identified as heritage sites. These can be identified by map coordinates. According to WA Planning Commission. The Changing Cockburn Coast, Appendices; Coastal Planning Strategy, (p.38), <i>"Creating a sense of place involves a conscious decision to do so. Putting these words into action, the Cockburn coast needs to present itself as a readable story, engaging people in its past, its traditions, its significant places, old buildings and beauty. The future is about being authentic to this story and it begins with fostering sense of place elements in the development framework. Sustainable communities don't happen by accident; they begin by authentic place making and design with a sense of place"</i>.</p> <p>The Cockburn Coast Cultural Heritage Survey, 2012 (p.III) also states that: <i>"This Strategy provides a management framework for the identified heritage sites in each of the three precincts; setting out how to protect and transmit their heritage values"</i>, It is evident that by neglecting to acknowledge, preserve and interpret these significant heritage sites, the WA Planning Commission has been in neglect of its duties. The following questions must be asked: How will the Coastal Development Plan protect and transmit the heritage values of these sites? Without preserving and maintaining this important chapter in Western Australian History, how will the Cockburn Coast present itself as a readable story, engaging people in its past, its traditions, its significant places?</p> <p><b>Neglect of Environment</b>  <i>Encroachment on the existing land area of Beeliar Regional Park</i>  The Emplacement Precinct has removed significant areas of land from Beeliar Regional Park. In addition, the new Cockburn Coast Drive effectively separates the coast from our existing community. This is in contravention of Beeliar Regional Park, Final Management Plan, 2006 (p.1), which is intended to <i>"ensure the Park is managed appropriately and is capable of sustaining its high nature conservation and cultural values as well as use by the community"</i>. These lands were transferred to the Conservation Commission of Western Australia ... <i>"for the maintenance and restoration of the natural environment, and to protect, care for and promote the study of indigenous flora and fauna and to preserve any feature of archaeological, historic or scientific interest"</i>. It is also in contravention of Local Government Planning Policy – Cockburn Sound Catchment Policy, which states one of its objectives as <i>"where appropriate, to maintain or increase native local vegetation in the Cockburn Sound catchment area"</i> (WA Planning Commission. The Changing Cockburn Coast, Appendices; Coastal Planning Strategy, p.7)</p> <p><b>Contravention of commitment regarding western skyline</b></p>	<p>Reference is made to the Port Catherine development. To clarify, that development is now known as 'Port Coogee' which is further south of these proposals. It is not correct to link the report on one development area (Port Coogee) to a different development area (Cockburn Coast).</p> <p>The plan allows greater height (6-8 storeys) on the western side of the ridge subject to submission of a detailed development application and assessment against the design guidelines to ensure through site/visual analysis that buildings will not be overly visible from points some distance east of the ridge.</p>



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		<p><b><i>The integrity of the Limestone ridge – natural value</i></b>  <i>"Located along the ridge line separating the coast from the bush, Emplacement will be the new high point, a manufactured horizon line that offers the opportunity for a new architectural topography, an integrated landscape of nature and built form".</i> (Cockburn Coast Redevelopment Plan: Emplacement LSP)</p> <p>The area immediately west of the planned Cockburn Coast Drive in Emplacement Precinct (extending south from Rollinson Road) and all the way south to the existing railway line, is identified in the Plan as allowing for structures of 6-8 stories. Although allowing for this height for iconic and gateway buildings This is in direct contravention of advice given by Port Catherine Developments that the skyline as seen from the inland (eastern) aspect of the ridge would not be broken by visible structures. (Metropolitan Region Scheme Amendment No. 1010/33 - Port Catherine, 3.8 Visual Amenity. Western Australian Planning Commission, in Environmental Protection Authority Perth, Western Australia Bulletin 1060 [August 2002]).</p> <p>This was confirmed by the WA Planning Commission, when it stated that <i>"The residential development will also not be visible from the east"</i> (Metropolitan Region Scheme Amendment No. 1010/33 - Port Catherine, 3.8 Visual Amenity. P.29). This directly also contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.1 Land use and zoning), which claims that objectives which have driven land use classifications include <i>"the use of natural landform....to create ...built form character precincts"</i>.</p> <p>This also directly contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.4 Public realm and open space), for which the objectives were stated as to <i>"create an urban typology for open spaces, while respecting the natural landform and characteristics of the Cockburn Coast area"</i>.</p> <p>This also directly contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.5 Landscape philosophy), that the aim of structure planning was to develop <i>"a landscape theme and identity for the Cockburn Coast area, based on its historical, cultural, environmental and physical characteristics"</i>.</p> <p>It also contradicts advice in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, p.3), that <i>"the big challenge in returning a forgotten industrial coastal strip back to the community is to engage in coastal recreational and tourism planning that responsibly addresses community needs and aspirations without compromising environmental and cultural values"</i>.</p>	<p>The reference group referred to was set up by the State Government and had input into the 2009 District Structure Plan. This group met between December 2006 and July 2007. It is not appropriate for the City to comment on a group which it did not manage and which ceased nearly six years ago. However, this concern can be raised with the Department of Planning.</p> <p>It is unfortunate if this is the case. However, it is acknowledged that depending on the issue raised, there may be little or no scope to change. An example would be the overall density targets, these are set within higher level planning instruments, thus when it comes to the local structure plans these targets will need to be met.</p> <p>The submissions received by the City of Cockburn for this consultation period have been carefully analysed and responded to. Where possible and appropriate,</p>

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		<p>It also contradicts State Planning Policy No. 2.6 State Coastal Planning Policy, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that <i>"the objectives of this policy are to: protect, conserve and enhance coastal values, particularly in areas of landscape, nature conservation, indigenous and cultural significance"</i>.</p> <p>It also contradicts State Planning Policy No. 2 Environmental and Natural Resources Policy, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that... <i>"the objectives of the policy are: to integrate environment and natural resource management with broader land use planning and decision making; and to protect, conserve and enhance the natural environment"</i>.</p> <p>It also contradicts State Environmental (Cockburn Sound) Policy 2005, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that... <i>"The purposes of this policy are: to declare, protect and maintain the environmental values of Cockburn Sound"</i>.</p> <p>Action Required: It is incumbent on the WA Planning Commission and the Cockburn City Council to ensure that the integrity of this provision is maintained, as has not happened with the Port Coogee Development, where structures have been made clearly visible from Hamilton Road and further east.</p> <p>Furthermore, the residents and ratepayers of Cockburn are still waiting for advice from the Planning Commission regarding penalties which will be imposed on the developer for this transgression. The Planning Commission, in consultation with Cockburn City Council should make this provision binding, with clear identification of penalties to be incurred for non-compliance.</p> <p><b>Non-transparency of process for reference group</b> According to Cockburn Coast Draft District Structure Plan (2.2.1), "the residential mix that gained stakeholder approval was informed by iconic urban coastal locations elsewhere in Australia, such as Manly and St. Kilda". Since this coastal development affects residents of Cockburn City most directly, the Council and the WA Planning Commission have a responsibility to ask residents primarily if they want their section of coast to look like these "icons", rather than allow a majority of "stakeholders" who have no long-term vested social interest in the area. Neither the Council, nor the WA Planning Commission has made it clear which or how many of the stakeholders wanted</p>	<p>modifications to the local structure plans have been required.</p> <p>The comments here relate to a submission process run by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>The comments here relate to a submission process run by the State Government. It is</p>

No.	Name/address	Submission	Council's recommendation
		<p>this type of landscape, but since only nine out of 33 in the reference group can be identified as actually living within Cockburn City limits, it is unlikely that those in favour constituted a majority. The selection process for both the 16 landowners and the 9 community representatives has also not been made transparent.</p> <p><b>Contemptuous treatment of previous submissions by residents in the revision process</b></p> <p>There is a strong feeling among many residents of Cockburn that any call for submissions to respond to Planning Instruments is largely tokenistic and that the developments which are the target of these instruments are a fait accompli. This is perhaps not surprising, when the opportunity to amend the instruments in response to submissions seems to be often ignored.</p> <p>Some examples can be sourced from the Final Public Submissions Report 120809, for the Draft Cockburn Coast District Structure Plan. There were 92 valid (of 95) submissions received regarding the Plan, according to the following categories:</p> <ul style="list-style-type: none"> <li>Community - 53;</li> <li>Community groups - 9;</li> <li>Landowners - 12;</li> <li>Federal Government agency - 1;</li> <li>State Government agencies - 15;</li> <li>Local Government Authorities - 2.</li> </ul> <p>The following provides some critical commentary of the proponent's responses to submissions for a number of items in the Final Public Submissions Report.</p> <p><b><i>Item 5.1 Necessity and alignment of Cockburn Coast Drive</i></b></p> <p>Submission No.: 2, 33, 34, 35, 38, 40, 42, 44, 45, 46, 51, 52, 53, 60, 61, 62, 63, 67, 72, 78, 79, 84, 85, 87, 89, 94 (28 submissions)</p> <p>According to the report, of the 9 central concerns from submissions, 5 related to perceived negative environmental and social impacts of the proposed Cockburn Coast Drive. These were:</p> <ul style="list-style-type: none"> <li>• impact on remnant vegetation and biodiversity within Beeliar Regional Park</li> <li>• impact on the topography of the Beeliar Regional Park ridgeline</li> <li>• impact on properties east of Beeliar Regional Park in relation to noise and visual amenity</li> <li>• reduced accessibility to coastal area from east of Beeliar Regional Park</li> <li>• impact on heritage listed properties - Randwick Stables, Marks House (Davilak</li> </ul>	<p>not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>The 'as advertised' version of the Development Area provisions included a requirement for visual assessment modelling from the landward side of Beeliar Regional Park. However, this provision was required to be deleted by the Minister for Planning.</p> <p>The plan allows greater height (6-8 storeys) on the western side of the ridge subject to submission of a detailed development application and assessment against the design guidelines to ensure through site/visual analysis that buildings will not be overly visible from points some distance east of the ridge.</p> <p>The assessment of these applications will be critical as if a development proposal is built in accordance with the plans approved and</p>

No.	Name/address	Submission	Council's recommendation
		<p>Avenue)</p> <p>In addition, concerns were expressed as recommendations in 11 submissions and of the three identified bases of discussion among these submissions, one related to perceived negative environmental and social impacts of the road:</p> <ul style="list-style-type: none"> <li>if road is to be constructed, consider alignment as west as possible to minimise impacts on the aesthetic and environmental values of the ridgeline and Beeliar Regional Park.</li> </ul> <p>The Response given to these concerns is perhaps not surprising, given the wording of the topic. Use of the word "Necessity" gives a strong impression that Coastal Coast Drive and its alignment were indeed a fait accompli. The response claims to have become "more responsive to the topography of the area", while maintaining that the alignment is merely, broadly utilising "the existing Fremantle to Rockingham Controlled Access Highway Primary Regional Road Reservation". What it seems to have chosen to deliberately ignore, are objections from these 28 submissions, to the necessity to have a road there at all.</p> <p>Rather it has chosen to focus on the alignment, since it can claim to have done something to address that aspect. The proponent claims to have "substantially revised" the alignment "to reduce potential impacts on the ridgeline and Beeliar Regional Park," by acceding 57 hectares of land for transfer back to the park. It seems clear here that concerns about maintaining the integrity of the ridgeline have not been responded to adequately. Some concessions have apparently been made, according to some vague reduction in potential impacts, but the fact remains that the proponent clearly has no intention of allowing for the integrity of the ridgeline to be retained.</p> <p>Some further vague statement of intention to "achieve greater aesthetic and safety outcomes " regarding the "built form interface with Cockburn Coast Drive" will give little further comfort to the concerns expressed in these many submissions, which clearly seek a much larger separation between built form and bush, who do not want Cockburn Coastal Drive to be built. And who do not want built form to visibly break the skyline from the east.</p> <p><b>Item 3.8 Visual amenity</b></p> <p>The report claims that a submission from the Department of Indigenous Affairs "commented on" the value of the eastern face of the limestone ridge for its links to an Aboriginal cultural myth. The report refers to public submissions, which "claimed" that the stark nature of this ridge does not significantly lessen its landscape value, and that</p>	<p>complies with the conditions prescribed; there is no recourse for the City to take against a developer.</p> <p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>It can be noted the local structure plans indicate 3-5 storeys in height across the majority of the development area, with 6-8 storeys to the east.</p>

No.	Name/address	Submission	Council's recommendation
		<p>the proposed excavation of the ridgeline would compromise its natural profile. It also suggests that public submissions "raised concern that impacts on visual amenity will reduce the value of Beeliar Regional Park". Firstly, from a critical literacy perspective, the language used here is interesting. By suggesting that the Indigenous Affairs submission merely commented, suggests that both the proponents and Indigenous groups see this cultural myth as something not worthy of consideration. Secondly, use of the word "claiming" attempts to give the impression that these Public submissions, clearly did not really know what they were talking about.</p> <p>In these ways, submissions have been treated with contempt by the proponents; their concerns have been trivialised to justify a lack of significant response to their concerns. This summation of the value of submissions appears to have been accepted verbatim and supported by the EPA, who agreed (not surprisingly, that: "the visual amenity of the areas adjacent to the project to not be unduly affected by the proposed scheme amendment.</p> <p>The EPA recognises that the limestone ridge traversing the site has significant landscape value, as indicated in many of the public submissions. However, it is considered that implementation of the proposed amendment will not significantly alter the integrity of ridge as a regional landscape feature. The proposed residential development on the west of the ridge will replace the current, largely denuded, landscape, but would not significantly compromise the limestone ridge itself. The residential development will also not be visible from the east.</p> <p>The way in which Public submissions have been treated with contempt by this assessment, and by the proponents response to submissions, is: The EPA has confirmed that residential structures will not be visible from the east. That is good, but it is a commitment which has not been adhered to by the Port Coogee development, so residents should have little confidence that it will be so here. What has not been made clear is how the Department of Planning, Cockburn City Council will respond if in fact this commitment is not adhered to.</p> <p>Furthermore, the response and EPA assessment have colluded in little "smokes and mirrors" behaviour. By confirming that the residential structures will not be visible, they have allayed fears by those who prepared submissions that the development will not be visible from the East. In reality, it appears likely that Cockburn Coast Drive, including traffic and street lights, will be visible from the east, so the integrity of the ridgeline will actually NOT have been retained by the development and associated infrastructure.</p>	<p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>Please also see response to submission 11 further above. Horses are currently exercised</p>

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		<p><b>Item 5.2 Height</b>  Submission No.: 4, 5, 17, 28, 34, 25, 36, 38, 40, 42, 45, 51, 57, 58, 59, 65, 67, 69, 72, 74, 84, 85, 91, 81.  According to the report, a number of these submissions suggested <i>"maximum height limits within the project area of between 2, 4 and 6 storeys dependent on distance to the coast, and an emphasis on high density as opposed to high rise"</i>.  Reasons related broadly to concerns about:</p> <ul style="list-style-type: none"> <li>• Changing the skyline and visual appeal of the area</li> <li>• Not appropriate within the regional context which has historically been rural</li> <li>• Detracts from scenic landscape</li> <li>• Potential to undermine sense of place</li> <li>• Desirability and necessity of medium and high rise development called into question For example, one resident objected that the draft plan allowed for "up to 35% of buildings" up to "8 story plus". This resident (not the author of this submission) <i>"strongly disagrees with this proposal it will completely change the skyline and visual appeal of the area. It is not in the character or the culture of the Cockburn coast to have high rise apartments on the foreshore. Leave that for Scarborough. The plan to reserve 20% of the available housing to remain affordable for low income earners is a good idea but this does necessitate high rises. High density can be achieved at low level"</i>.</li> </ul>	<p>in the early morning. It is noted the horses from Randwick Stables currently traverse the road system to access the beach and this will become more difficult over time given the regional road network and the pressures of a growing City. In the interests of safety and also accommodating the broader horse community, the option to use floats will be provided for at the McTaggart Cove Rd parking area.</p> <p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>It is noted there are changes recommended to the public open space for the Robb Jetty Local Structure Plan. This plan will now provide closer to the minimum 10% local public open space, as well as the sports oval required by the City's Sport and Recreation Strategic Plan which will provide for a greater catchment.</p>

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		<p>A number of submissions (generally from or on behalf of landowners) also requested:</p> <ul style="list-style-type: none"> <li>• consideration for the location of landmark or gateway buildings on their landholdings</li> <li>• consistency of heights with other new developments within certain precincts (i.e. Newmarket precinct)</li> <li>• reconsideration of height limits to enable flexibility and economic feasibility</li> </ul> <p>The Response outlined the minimum proportions targeted for various heights of structures in the draft CCDSP as:</p> <ul style="list-style-type: none"> <li>• Min. 3% separate houses</li> <li>• Min. 22% terrace houses</li> <li>• Min. 33% low rise apartments (3-5 storeys)</li> <li>• Min. 31% medium to high rise apartments (6-8 storeys, over 8 storeys respectively)</li> </ul> <p>The response appears to have chosen to disregard completely the concerns and submissions which aimed to reduce planned building heights. merely brushing them off with the inadequate response: <i>"targets were established in conjunction with the Cockburn Coast Reference Group, on the basis that they would support urban consolidation, public transport and sustainability objectives for the area; as well as intensity and diversity of housing stock"</i>.</p> <p><b>5.11 Coastal development (north of McTaggart Cove)</b></p> <p>The report identified <i>"that further clarity is required on the reasoning for the inclusion of this land for development purposes and on the built form provisions for this development area"</i>, based on submissions. Critical literacy analysis of this suggests that there was broad and loud resistance to the development of this area voiced in submissions. The response firstly waffled on about ensuring <i>"that there is a critical mass to support the new town centre within the revitalised Power Station precinct"</i>. What does that mean? Does it mean the precinct needs more residents to provide the necessary population to justify the hugely expensive development of the Power Station, to provide a vibrant community, or to open up more land for speculators? This is not made clear and the report needs to be more responsive to submissions, by doing so.</p> <p>The response secondly waffled on about providing <i>"passive surveillance and activation adjacent to the foreshore area. This activation is required in response to the significant anti- social behaviour present in the location, owing to the lack of adjacent development"</i>. What does that mean? Does it mean that because there have been a</p>	<p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>Schools are provided based on the advice of the Department of Education. A submission has confirmed that they are happy with the school as indicated in the draft Robb Jetty Local Structure Plan.</p> <p>A copy of the content of this submission will be referred to the Department of Planning. The submission indicates frustration with</p>

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		<p>few cars broken into in the area; we need to build a city to stop that? What a load of nonsense, honestly.</p> <p>Submissions also apparently were very concerned about <i>"the potential impact on coastal vegetation"</i> of this area of precinct. , In response, the report admitted that <i>"a small area of vegetation is potentially impacted on by the western development proposal"</i>. This is supposed to be a professional document, so how can it be taken seriously, when the size of the area is not identified. Instead we (the public) are left to interpret <i>"a small area"</i> verbatim. The response goes on to accede that destruction will occur of an area where <i>"vegetation is largely of a good condition,"</i> and <i>"small area of vegetation of very good condition may be impacted"</i>.</p> <p>Their referral to the Environmental Protection Authority for <i>"consideration in determining the need for environmental assessment under the process as outlined in section 5.9"</i> is laudable, but how will the results of that enquiry be communicated to the public and particularly to residents who expressed concerned about this in their submissions. The report does acknowledge that <i>"further consultation with Indigenous elders will be required in more detailed planning for the project area"</i>, but makes no suggestion of how the results of that consultation be communicated to the public and particularly to residents who expressed concerned about this in their submissions.</p> <p><b>Item 5.13.1 Continuation of animal exercise</b>  Submission No.: 22, 41, 44, 46, 50, 55, 56, 57, 67, 71, 79, 81, 84, 87, 93.  The report identified <i>"Strong support was expressed for the continuation of animal (dog and horse) exercise opportunities within the CY O'Connor reserve and the redevelopment area"</i> and stated that <i>"the district structure plan supports the ongoing use of the beach for these purposes, and acknowledges that the continued use of the beach for these activities will need to be carefully managed as development progresses"</i>. However, there are issues, which the report fails to acknowledge from submissions.</p> <p>Critical literacy analysis of the above statement indicates that the proponents have been careful to only mention the horse exercise area within C Y O'Connor reserve, while the issue of "Continuation of Horse Exercise" involves a much larger area of the proposed development. In this way, submissions have been treated with contempt.</p> <p>One submission for example, stated that: <i>"The living horse heritage [sic] in the area is going to be severely inhibited and endangered by the proposed road"</i></p>	<p>previous consultations and reports prepared by the Department and it is appropriate they are afforded the opportunity to respond.</p>



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		<p><i>network...Randwick racing stables are home to several beautiful horses who use the Cockburn coast beaches and regional parks. They are well known and loved characters in the area. The stables themselves are on the, state heritage [sic] register but the planned road network cuts them off from the coast and the parks and surrounds them with busy high traffic roads. Horses will not use overpasses or go through tunnels so they will [sic] be made virtually [sic] homebound. As this area has such a rich culture and history involving horses it should not have such a detrimental [sic] network of roads dividing and separating [sic] the people and horses who live in it and use it".</i></p> <p>How are horses from a number of heritage-listed and continuing horse stables in Hamilton Hill supposed to get to the beach? By allowing for a maintained horse exercise area at CY O'Connor beach, but not allowing for safe riding trails to get horses the proponents are saying that they will accept horses on the beach, but it will only be possible if they are transported there by horse float.</p> <p><b>Item 5.13.5 Public open space</b>  Submission No.: 14, 15, 20, 22, 25, 30, 34, 51, 53, 59, 67, 79, 88, 89, 91, 94.  <i>"The district structure plan outlines a level of public open space above the 10% required by WAPC policy. Further local open space may be identified during local structure planning stages to achieve the minimum 10% contribution required under WAPC policy, and to complement the layout of the open space identified on the district structure plan".</i>  What is this saying? This statement is clearly contradictory, but whether it is intentionally intended to be misleading is unclear.</p> <p><b>Item 5.13.8 Wildlife/vegetation corridors to surrounding areas</b>  Submission No.: 12, 37, 78, 79, 84, 85, 87, 87.  A number of submissions proposed improved vegetation and wildlife linkages between the key natural areas within the vicinity of Cockburn coast, including Beeliar Regional Park, Clontarf Hill and Woodman Point". One submission for example, suggested that <i>"it is just an illogical idea to introduce traffic to an area that is already preserved bushland when there are already roads in place (Hampton Rd, Cockburn Rd, Rockingham Rd) that will be able to service the area. Heavy traffic should be diverted completely using Stock Road. Light rail networks, bus lanes and cycle paths will be a much better investment for sustainable community. For any roads that must be built speed limits should be capped at 60kph within the area because we do not want walls and sound barriers separating the parkland from the community and the coast. This coastal region should not be used as a thoroughfare for traffic between Fremantle and Rockingham".</i></p> <p>The response below appears to be along the lines of "it's too late, there are already</p>	

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		<p>barriers, so bad luck" and makes half-hearted noises about links for vegetation and pedestrians. <i>"These areas are currently largely segregated by existing physical barriers such as the freight rail, roads and urban development. Given these existing constraints, there is limited opportunity to effect this proposal. However, the draft CCDSP encourages the establishment of east-west open space links within the redevelopment area to encourage retention of existing vegetation and pedestrian and cyclist connections back to Beeliar Regional Park; and identifies the desirability of pedestrian and open space connections to Clontarf Hill should the regional road reservation impacting this areas be rationalised or realigned"</i>.</p> <p>The submissions mentioned above clearly were asking for "improvements" in vegetation and wildlife linkages. The response is that current linkages are negligible, therefore can't be improved. This is illogical and treats submissions with contempt.</p> <p><b>Item 6. Key revisions to the draft Cockburn coast district structure plan</b>  <b>6.2 Planning and built environment</b>  The only <i>"Modification of land use areas and associated dwelling and population yields"</i> incorporated into the revised structure plan, were <i>"based on revised alignment of Cockburn Coast Drive"</i>. Clearly submissions from residents have been ignored in this revision. The revised plan apparently gives <i>"Greater clarity on permitted heights within Power Station and Newmarket precincts and appropriateness of height within the broader district structure plan area"</i></p> <p>Clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of altering permissible and appropriate heights for buildings within the city limits from those proposed in the draft structure plan.</p> <p>The revised plan apparently gives <i>"Clarification on public open space contribution"</i>  Again, can I stress that clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of altering the areas allocated for public open space from those proposed in the draft structure plan because they felt strongly that they were inadequate.</p> <p>The revised plan apparently gives <i>"Clarity on anticipated built form interface with Cockburn Coast Drive and adjacent to the foreshore in the Robb Jetty precinct"</i> Again, can I stress that clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of significantly altering the provisions for the built form interface from those proposed in the draft structure plan because they felt strongly that they were inadequate. Revisions listed do not include any which relate to Coastal</p>	

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		<p>development (north of McTaggart Cove), Continuation of animal exercise, or Wildlife/vegetation corridors to surrounding areas, as called for in many residents' submissions.</p> <p><b>Allowance for one school flawed</b> According to Cockburn Coast Draft District Structure Plan (2.2.6), only one primary school will be required in the developed area. This is well below the recommended ratio of schools required for the number of lots in the developed area and two reasons are given.</p> <p>The first is that DET has advised that the extra schoolchildren can be accommodated by the existing capacity of <i>"adjoining primary school catchments"</i>. The second is that apparently, the anticipated demographic of the developed area will not be such that demand for school places will eventuate. This clearly shows that the anticipated residents of the new area is not anticipated to be characterized by families, but more likely by single people and speculators, which does not augur well for developing any real sense of community</p> <p><b>Conclusion</b> The report takes care to note that <i>"the consultation process undertaken for the district structure plan is the first stage in an ongoing liaison with the community, that will be undertaken over the life of the project. In addition to the evolution of the Reference Group process, the community will have the opportunity to provide submissions at the following statutory public comment stages:</i></p> <ul style="list-style-type: none"> <li><i>Metropolitan Region Scheme amendment</i></li> <li><i>Local Planning Scheme amendment</i></li> <li><i>Adoption of local structure plan(s)</i></li> <li><i>Adoption of local planning policies (where applied)"</i></li> </ul> <p>However, that does not excuse an inadequate response at this or any other stage. Clearly submissions from residents have been ignored in this revision. The proponents have chosen to ignore or explain away almost all concerns of residents. Rather than take the opportunity to listen to the voices of the community, they have chosen to merely attempt to justify the decisions already made, and in some cases appear to have been misleading in directing attention away from the absence of significant changes, by highlighting minor changes or by presenting already-made decisions as irrevocable "Necessity". This, I believe describes an attitude of contempt for the residents of the City of Cockburn. Residents expended much effort and emotion into preparing submissions. Residents of the area are passionate about their coastal environment and</p>	<p>No changes are recommended as a result of this submission. Though it is noted the submission raises the issue of public open space and reviewed assessment of this has been now undertaken. This submission expresses very strong concerns with previous submission period conducted by the Department of Planning. To ensure these concerns are directly appropriately (as they are not appropriate for the City to comment upon) these concerns will be forwarded to the Department of Planning.</p>

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		their city. They deserve more respect than the treatment this process has accorded them.	
13	Blandine Halle 73 Healy Road Hamilton Hill WA 6163	<p><b>Objection</b></p> <p>No high density development along the coastline. This land of Robb's Jetty &amp; Emplacement should be transformed into public parkland with native vegetation replantation, cycle path, pedestrian paths, kiosks/cafes. I would be fantastic to have a green corridor of parklands with existing Manning range/park. Residential development should be kept away from coastline. Port Coogee is an example of an environmental disaster - we DON'T WANT a repeat of that.</p>	<p><b>Not Supported</b></p> <p>The project fulfils the State Government's vision and clearly defines objectives to develop a unique dense metropolitan activity centre adjoining the coast. The plan is supported by a Foreshore Management Plan to protect and enhance the community's access to the coastline.</p>
14.	Department of State Development Level 6, 1 Adelaide Terrace EAST PERTH WA 6004	<p><b>Objection</b></p> <p>The Department of State Development (DSD) provides leadership to drive responsible redevelopment for Western Australia, with a focus on:</p> <ul style="list-style-type: none"> <li>Delivering the WA Government's priorities for development in projects of significance to the State.</li> <li>Assisting project proponents and working with stakeholders to develop major resource and industry projects.</li> </ul> <p>The Department's role with regard to industrial land focuses primarily on the development and support of Strategic Industrial Areas (SIA), which are designed to meet the land requirements of Heavy Industry. Notwithstanding, the Department recognises the strategic value of other industry zoned land and is keen to ensure that sufficient land is available to accommodate general and light industry which supports and is synergistic with heavy industry- particularly where such land is within close proximity to SIAs.</p> <p>The Economic and Employment Lands Strategy (EELS): non-heavy industrial; Perth metropolitan and Peel regions, April 2012, identified the impact of unprecedented economic growth during the 1990's and early 2000's upon industrial land values and availability. The Strategy forecasts that the demand for available industrial land within the metropolitan south-west sub-region, the area incorporating the Western Trade Coast (WTC) and proposed Cockburn Coast will exceed the available supply by 278 hectares by 2031.</p>	<p><b>Not Supported</b></p> <p>The Emplacement LSP is currently zoned for urban uses under the Metropolitan Region Scheme and City's Town Planning Scheme No.3. The objection to the zoning for uses other than industrial is not considered relevant to this proposal.</p> <p>The Cockburn Coast project is approximately 5km north of the Australian Marine Complex and Latitude 32 industrial area and 12km north of Kwinana Heavy Industrial area. All these areas have residential development in far closer proximity to them than the Cockburn Coast proposal. Therefore, it is not expected that the project will place any pressures on the operations of these industrial areas.</p>

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		<p>Current budgetary constraint suggests that the implementation of EELS may not take place for some time, increasing the strategic value and scarcity of existing industry zoned land.</p> <p>The development of industrial land is a lengthy and expensive process, due to the requirement for structure planning (including appropriate separation from land for sensitive uses), environmental and other clearances, rezoning and arrangement for transport routes and service infrastructure to meet industry needs. With project ready industrial land becoming scarce in the Perth-Peel region, particularly premium coastal industrial land, the proposed rezoning of existing project ready industrial land on the Coogee Coast would oppose the objectives of EELS. A significant portion of land within the area identified for development under the Cockburn Coast District Structure Plan is currently zoned for industrial purposes. The Department emphasises that the subject land is the only existing industrial land with coastal access outside of the WTC, an area which is subject to increasing pressures through the encroachment of proposed urban development to the boundary of the Kwinana Air Quality Buffer, and to reinforce the significance of industrial land which is close in proximity to ports and appropriate transport corridors.</p> <p>It should be noted that the land developed under the 1993 Coogee Masterplan saw the relocation and establishment of industry proponents to the subject area. This included relocation to the subject area of industry proponents then located south of the rail reserve, to facilitate rezoning and redevelopment of that land for the residential Port Coogee development. The area north of the rail reserve, the area now proposed for rezoning for residential development under the Cockburn Coast District Structure Plan, was for the purpose of establishing a Biotechnology Park which would accommodate Special Industry, such as proponents involved in seafood processing.</p> <p>In summary, the Department of State Development raises that rezoning existing industrial land adjacent to the coast and close to the WTC will exacerbate land use pressures already placed upon the WTC. It also carries a potential risk to ongoing industrial development and economic growth within the metropolitan region. Whilst adverse economic effects may possibly not be obvious within the short-term, this reduction in industry land has potential over the mid to long term to impede delivery of the State Government's goal for economic output and employment within the WTC to ultimately double to \$28.3 billion and 22,000 jobs.</p>	
15.	The Western Trade Coast Industries Committee	<p><b>Objection</b></p> <p>The Western Trade Coast Industries Committee (WTCIC) was established by the State</p>	<b>Noted</b>

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	Unit 2/1 <sup>st</sup> Floor, 18 Civic Boulevard ROCKINGHAM WA 6168	<p>Government in 2011 with the goal of seeing the annual economic output and employment within the Western Trade Coast ultimately double to \$28.3 billion and 22,000 jobs.</p> <p>As such, the WTCIC is concerned only about potential impacts (positive or negative) on the WTC arising from the Cockburn Coast proposals. There are two matters the WTCIC wishes to comment on:</p> <ol style="list-style-type: none"> <li>1. Overall Cockburn Coast proposal</li> <li>2. Impact on freight routes.</li> </ol> <p>1. Overall Cockburn Coast Proposal</p> <p>The State Government's objective for the WTC is to see its economic output and employment double. The Cockburn Coast project will potentially provide both an additional workforce and customers within 5km of the northern boundary of the WTC. Having this additional potential workforce and customers nearby will help meet the State Government's objectives for the WTC. On that basis the broad objectives of the Cockburn Coast proposal are supported.</p> <p>2. Impact on Freight Routes</p> <p>A concern of WTCIC is to ensure movement of freight into and out of WTC is not constrained by the Cockburn Coast project. Freight is only likely to be constrained if sensitive land uses are allowed to abut the rail and road freight routes and, if so, those conflicts are inadequately managed. Residential development, a noise and vibration sensitive land use, is proposed adjoining the freight rail line and Cockburn Road. This does raise the potential for these two freight routes to be adversely impacted.</p> <p>The WTCIC believes the precautionary principle should be the overriding guiding approach and, as such, supports land use decisions that seek to avoid potential land use conflict in preference to allowing the potential conflict to occur and then trying to manage it.</p> <p>If, however, the City proceeds with the existing plans for the Cockburn Coast, then the proposal must be fully compliant with the intent of SPP 5.4 and all possible measures taken to ensure that the conflict is indeed successfully managed and in perpetuity as the freight volume grows. In that regard, the WTCIC notes the noise and vibration study undertaken and endorses the proposal to adopt a precautionary approach and adopt a</p>	<p>Any development will need to comply with the requirements of SPP 5.4 for freight rail, to ensure that the transport of freight by rail to and from Fremantle Port can continue into the future. The modelling undertaken to inform the Noise and Vibration strategy exceeds the requirements of SPP 5.4.</p> <p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p> <p><b>Noted</b></p> <p>Careful consideration has been given to ensuring freight movement is adequately planned for in Cockburn Coast. With regard to noise emissions from freight trains, under Implementation Guidelines for SPP 5.4 'Road and Rail Transport Noise and Freight Considerations in Land Use Planning', where the number of movements is not defined, 24 train movements per 24 hour day should be used. However, to ensure some "future proofing" the modelling undertaken by the Noise and Vibration Strategy which forms part of the LSP has recommended a higher standard to SPP 5.4 by recommending the assessment of each development be based on that of the highest single train movement rather than an average.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p>

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		<p>mandatory noise sensitive design requirement within 150m of the freight rail line. This submission represents the agreed view of the WTCIC and does not necessarily reflect the individual views of each member organisation.</p>	<p>The Noise and Vibration Strategy forms an addendum to the LSPs and shows the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The design guidelines will outline the requirements for compliance with noise and vibration for land within the impact zone. The Design Guidelines will also include requirements for Notification on titles.</p> <p>No changes are considered necessary as a result of this submission.</p>
16.	B & M Gosatti 856 Karnup Road HOPELAND WA 612	<p><b>Support</b></p> <p>We have no objections to the above proposal and think that this proposal is a wonderful idea, but we do think that the City of Cockburn should start thinking about a proposal for similar structure place for the Power Station, now rather further into the future.</p> <p>We wish to be kept informed of any future developments within this area.</p>	<p><b>Noted</b></p> <p>The district structure planning includes the Power Station precinct, and substantial planning has been undertaken for this precinct through this process. No changes are recommended as a result of this submission</p>
17.	Alison Bolas 24 Rockingham Road HAMILTON HILL WA 6163	<p><b>Objection</b></p> <p>I have a number of objections to the Structure Plans and consider that many of the concerns raised in the original community consultation have not been taken into account.</p> <p>I think environmental and heritage issues have not been given sufficient consideration evident from the continued inclusion of the M.R.S primary road. Although the alignment of Cockburn coastal Drive has been revised to reduce the impact on the ridgeline and Beeliar Regional Park, it still is a major arterial road and will impact significantly on the bushland and have considerable detrimental consequences to the sustainability of Beeliar Regional Park.</p>	<p><b>Not supported</b></p> <p>The MRS Primary Regional Road Reservation falls outside of either the Emplacement of Robb Jetty LSP area, and was dealt with through the district structure planning and MRS Amendment No. 1180/41 (Cockburn Coast District Structure Plan).</p> <p>As noted, the MRS Amendment included a</p>

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		<p>It is illogical to promote the sustainability of the regional plan and still include the construction of a major arterial road that would promote the movement of heavy traffic through the area, divide the community and have a destructive impact on protected species of native fauna.</p> <p>I still maintain that Cockburn Road as it currently exists should be upgraded and heavy traffic diverted using existing routes such as Stock road. Freight by rail should be increased and alternative transport created for example light rail, rail and a network of bicycle paths would help to reduce dependency on cars.</p> <p>It seems ludicrous to build roads that only increase traffic when reduction of carbon emissions is so essential to alleviate the effect of climate change. The loss of natural vegetation and the fragmentation of remaining bushland in Beeliar Regional Park by building of the arterial road will significantly impact on the area. Many birds and reptile species inhabit the area and many species move through the area to feed on vegetation or to hunt. Endangered species including Carnaby Black Cockatoo, Blue Wrens and Black Shouldered Kites nest in the area.</p> <p>The Nankeen Falcon, Australian Small Eagle and Peregrine Falcon- migratory species such as the Bee-eater and priority species like the Lined Burrowing Skink and Black Striped snake are found in the area. I still believe that an independent environmental impact study should be undertaken before this road is considered As the Department of Environment and Conservation has stated "protected areas are essential to maintain natural and cultural diversity" and "to foster a sense of place and belonging and contribute to the values of our community"</p> <p>Beeliar Regional Park was primarily created to protect endangered species of natural flora and fauna and as climate change is a major threat to the world's environment and society and is expected to have a profound impact on the unique diversity of Australian wildlife protected areas are essential refuges for species already stressed by the destruction of so much of their habitat. It is stated by the EPA that native vegetation needs to be protected to preserve biodiversity and as green areas to absorb carbon emissions.</p> <p>I am concerned that the Emplacement Precinct involves the removal of bushland in addition to the negative impact of the road.</p>	<p>revision to the alignment of the reservation that has enabled an additional 5.7 ha of former road reservation land to be included within the Beeliar Regional Park Management Plan. In addition, the actual road will be designed to minimise the amount of vegetation to be cleared, supported by further more detailed fauna and flora surveys. The MRS Amendment was referred to the Environmental Protection Authority, who considered the environmental impact of the road reservation, and advised that it did not require formal assessment under Part IV of the Environmental Protection Act 1986. The Integrated Transport Plan provides for measures to reduce car dependency and encourage walking and cycling as an alternative for future Cockburn Coast residents.</p> <p><b>Noted</b></p> <p>The Ecological Assessment identifies that there is remnant vegetation within the southern area of the Emplacement LSP area, some of which constitutes habitat for the Carnaby Black Cockatoo. Based on the draft Department of Sustainability, Environment, Water, Population and Communities (DSEVVPaC) referral guidelines, clearing the vegetation and development of the Hilltop/Emplacement Crescent Project Site would have a direct impact on Black Cockatoo feeding habitat and given this future referral to DSEWPaC may be required (i.e. prior to subdivision or development).</p> <p>The Ecological Assessment for the Emplacement LSP also notes that Vegetation type 1, which occurs on the limestone ridge</p>



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			<p>on the eastern side of the Project Site, has similarities to a DEC-listed TEC, (<i>Melaleuca huegeli</i> — <i>Melaleuca acerosa</i> [currently <i>M. systema</i>] shrublands on limestone ridges). A vegetation survey in spring (when annual species are present) would be required to confirm this. I</p> <p>It is therefore recommended that the Emplacement LSP report be amended to include the requirement for a spring survey to be conducted prior to any subdivision or development (that proposes works to the land).</p> <p><b>Not supported</b></p> <p>The Emplacement LSP and associated Cockburn Coast Cultural Heritage Strategy will ensure the retention and protection of the remaining gun emplacement. Specifically, the Emplacement LSP includes the gun emplacement within public open space to ensure that this important feature is not subject to development pressure. The two other gun emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the Heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated with the South Beach Battery site.</p> <p><b>Not supported</b></p> <p>It is agreed that horse heritage is an important consideration, and that is why it has been considered from the District Structure Planning stage through to the Local Structure</p>

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		<p>I am also concerned that WWII historical sites belonging to a complex military infrastructure associated with the gun emplacement sites and which are yet to be studied will be destroyed by the development.</p> <p>The other issue which I feel has not been given proper consideration is the Horse heritage which is a living heritage and has a long and colourful history in the community. The road potentially will isolate the heritage listed property of Randwick Stables from the heritage listed beach as it will make access difficult. Horses do not go through tunnels or use overpasses. I am also concerned that the horse exercise areas and dog exercise areas that are within the CY O'Connor reserve are maintained. I know many people in the community also support keeping the beach as an area for animal exercise and I am pleased that the structure plan supports the ongoing use of the beach for these purposes. I hope that this will not be compromised as the development unfolds. I</p>	<p>Plans. The LSP and associated Cockburn Coast Cultural Heritage Strategy identify and recognise the importance and heritage value of the South Beach Horse Exercise Area. The LSP (pg 60) states 'the aim is for horse facilities to remain at McTaggart Cove to provide facilities for horses with a horse float car park, where the dunes are lower and there will be less disturbance to future residential uses, thus minimising potential land use impacts.' A key objective of the Heritage Strategy is that "South Beach should continue to be used for the horse training, a use with which it has had a long association".</p> <p><b>Not supported</b></p> <p>The assessment of the coastal vulnerability and the coastal setback to the proposed development has been completed in accordance with the 2012 draft State Coastal Planning Policy. This includes a full assessment of the potential impacts of a rise in sea level of 900 mm over the coming century. This value of sea level rise would accommodate an average rise of 9 mm/year over the coming century. This value was adopted by the Department of Transport and the Department of Planning in 2010 after careful consideration of the data and projections.</p> <p><b>Not supported</b></p> <p>The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense urban development that reduces the need for housing on the urban fringe, and</p>

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		<p>also question the non-transparency of the reference group who considered that" iconic coastal locations such as Manly and St Kilda "should inform the nature of the development. I understand that the majority of these people were not residents of the City of Cockburn and have therefore no long term social interest in the area.</p> <p>With the likelihood of sea levels rising I don't think that consideration has been given to allow sufficient set back of development from the coast. I think that more effort should be given to the protection of our coastline and beaches from increased erosion caused by developments such as Port Coogee.</p> <p>In conclusion, although some of the issues that were raised in the process of community consultation have been addressed to a degree, I don't think the environmental and social concerns have been given enough importance. The values and needs of the community should not be overlooked in favour of vested interests.</p>	<p>provides for well-located affordable housing. The City must plan for population growth, and make the most efficient use of land available.</p>
18.	Department of Health PO Box 8172	<p><b>Support</b></p> <p>Thank you for your letter dated 19 November 2012 requesting comment from the</p>	

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	Perth Business Centre, WA 6849	<p>Department of Health (DOH) on the above proposal.</p> <p><b>1. Water and Sewerage</b> All developments must connect to scheme water and reticulated sewerage so as to comply with the <i>Government Sewerage Policy- Perth Metropolitan Region</i>.</p> <p><b>2. Mosquito-borne Disease Control Programs and Services</b> Mosquito populations and the types of mosquito-borne diseases vary across WA. Existing habitats such as wetlands can support extensive mosquito populations and can cause serious nuisances to humans who may reside within these areas, as well as increase the chance of people contracting debilitating or potentially life threatening mosquito-borne diseases.</p> <p>To minimise the risk of mosquito-borne disease and breeding sites, a proponent needs to provide written evidence of the following:</p> <ul style="list-style-type: none"> <li>• The identification of existing breeding locations within close proximity to the proposed development, and the extent of known mosquito-borne disease risk and nuisance levels from biting insects.</li> <li>• Commitment to develop and implement a mosquito management plan that provides strategies for managing mosquito breeding sites during construction and ongoing operational phases of the development and minimising the exposure of future residents to adult mosquitoes.</li> <li>• Commitment to locate, design and maintain any proposed man-made water bodies (e.g. constructed wetlands, vegetated swales and other stormwater infiltration infrastructure) in accordance with the <i>Chironomid midge and mosquito risk assessment guide for constructed water bodies</i> (Midge Research Group, 2007).</li> </ul> <p><b>3. Health Impact Assessment</b> You should also consider incorporating Health Impact Assessment (HIA) and/or Public Health Assessment (PHA) principles in your decision making process. The City of Cockburn should use this opportunity to minimise potential negative impacts of</p>	<p><b>Noted</b> No changes are proposed as a result of this submission.</p> <p><b>Not supported</b> The approved Local Water Management Strategies ("LWMS") prepared for Robb Jetty and Emplacement do not allow for the construction of any man made water bodies. The LWMSs state that any retention or detention structures must be completely infiltrated within 96 hours to minimise mosquito breeding.</p> <p>The nearest open water bodies are the Indian Ocean or Manning Lake approximately 800m away. As a result it is not felt that developing a mosquito management plan is necessary. During construction all necessary measures will be undertaken to ensure that any temporary retention or detention structures will be completely infiltrated within 96 hours.</p> <p><b>Noted</b> Such public health principles have been incorporated into the decision making processes for the Cockburn Coast project from its early stages so that implications of development on current and future communities living in or near the development are considered as a priority. For example, studies and resulting actions which have informed the decision making process (as referred to in DSP Part 2 and LSP submissions) include:</p> <ul style="list-style-type: none"> <li>• Assessments of potential air quality,</li> </ul>

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		<p>increased density development such as noise, odour, light and other lifestyle activities. Public health impacts draw attention to those issues and they should be appropriately and adequately addressed at this stage.</p> <p>For your information and guidance, you may access the relevant information at the following sites:  HIA- <a href="http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm">http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm</a>  PHA- <a href="http://www.public.health.wa.gov.au/2/1399/2/public_health_assessment.pm">http://www.public.health.wa.gov.au/2/1399/2/public_health_assessment.pm</a></p>	<p>noise and vibration issues (road and rail related)</p> <ul style="list-style-type: none"> <li>• Odour impact assessment for Bennett Avenue Pump Station</li> <li>• Master planning consideration of building heights, quality and detailing for the built form with respect to light, visual amenity, safety, integration into the wider area and requirements for appropriate design guideline controls in LSP areas.</li> <li>• Development of an integrated transport plan to provide a comprehensive structure to the future movement network of Cockburn Coast which is sustainable, pedestrian orientated, maximises access to public transport and seeks to minimise possible effects on upon safety and health.</li> <li>• Consideration of site characteristics cultural heritage, natural features and amenity, and resulting actions to maximise amenity such as creation of key physical links for safe community access and public open space.</li> </ul> <p>Early adoption of such principles has allowed potential effects of increased density development to be recognised and the master planning process optimised so that the form of development presented in the district and local structure plans minimises potential for negative effects to occur.</p> <p>Negative impacts associated with mixed use development can be adequately dealt with at the Development Application stage. The</p>

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			Emplacement LSP provides additional guidance on how noise attenuation should be dealt with (Sections 8) and other non-planning legislation is available to control light and odour emissions (including the Health Act 1911 and City of Cockburn (Local Government Act) Amended Local Law 20123).
19.	Fremantle Ports 1 Cliff Street Fremantle WA 6160	<p>The planning process for the Cockburn Coast project area has been underway for over ten years. During this time Fremantle Ports has provided many submissions to local government, the Department of Planning and Western Australian Planning Commission highlighting its concerns. The primary concern that remains to be adequately addressed and that has an increasing potential adverse impact on the port, the community and the economy, is urban encroachment and the threat this presents to the continued, unimpeded operation of the freight rail line and road links that transect the area and connect with the Inner Harbour at Fremantle.</p> <p>The response by the approval and assessing bodies over the last decade has been inconsistent and in the main disappointing. The project is now at the point where land is being developed with people living too close to freight corridors. It is our view that such a result reflects poor planning that shows little regard for the freight corridor users or the future residents who will be living next to these corridors. Whilst maximising developable land for urban uses may allow some short term goals to be achieved for certain stakeholders (for example land developers), often longer term problems are created and the cost of addressing these problems is shifted to and left to be borne by other stakeholders or sectors of the community. This could hardly be described as a desirable outcome from a proper and robust planning process.</p> <p>In 2004 Fremantle Ports wrote to the Western Australian Planning Commission suggesting that a Working Group be established to help progress the project. The aim being that bringing together key stakeholders via a Working Group would allow for critical issues, such as the rail, to be integrated into the project at the earliest possible stage in an appropriate manner. This request was not adopted and whilst Fremantle Ports has been invited to make comment on the project at the statutory consultation periods, we do not believe this has been effective. Once plans have been developed and advertised for comment there has been a pattern of little real change occurring following any of the consultation periods. We believe that many of the planning issues</p>	This comment relates to a process undertaken by the Western Australian Planning Commission. It is not appropriate for the City to respond to this comment. However, the issue can be raised with the Department of Planning.

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		<p>that we perceive with this project could have been resolved or minimised if an effective Working Group with a wide membership had been developed.</p> <p><b>Robb Jetty and Emplacement Local Structure Plans</b>  In reviewing the Robb Jetty and Emplacement Local Structure Plans the comments put forward are within the context of the Western Australian Planning Commission's key planning documents which dictate planning for the Structure Plan areas, transport corridors and infrastructure such as ports.</p> <p>There are several key Western Australian Planning Commission documents which apply and the manner in which these have been addressed needs to be clearly articulated. Notably Statement of Planning Policy No 1, State Planning Framework which states: <i>"planning for land use and development in a manner that allows for the logical and efficient provision and maintenance of infrastructure, including protecting key infrastructure, including ports, airports, roads, railways and service corridors from inappropriate land use and development."</i></p> <p>Similarly the Western Australian Planning Commission's State Planning Strategy and Directions 2031 provide specific support to ports and their transport corridors. Specifically the State Planning Strategy states: <i>"ensure that the transport corridors between the generators of heavy traffic (ports and their strategic industry sites) are protected from uses which could jeopardise their efficiency", and "The operation of transport facilities should be made as effective as possible. Transport needs to be provided with adequate transport corridors and facilities which need to be protected from incompatible land uses. This particularly applies to our sea and airports which are the gateways for our future wealth and are of national and State strategic importance."</i></p> <p>Directions 2031: <i>"Perth, perhaps more than other Australian cities due to its relative isolation and primary economy, depends heavily on the efficient movement of freight in and around the city. It specifically states that its strategies include to "protect freight networks and the movement economy" and to "minimise conflict between land use and key infrastructure assets."</i></p> <p>Urban encroachment of the Inner Harbour and its land transport corridors, including the area south of Fremantle to Cockburn is of increasing concern. On the western border of the Robb Jetty Local Structure Plan area is a Freight Rail Reserve; on the eastern border of the Emplacement Local Structure Plan area is a Primary Arterial Road. Together these are key access routes that form part of a wider network providing freight access around the metropolitan region. The freight rail link is critical and to remain</p>	<p>See comment on Submission 18 further above. The section on relevant State Planning Policies can be expanded to include SPP1.</p> <p>Noted, similar to the issue raised above. The section discussing Directions 2031 can be broadened. and additional discussion as to how the LSP has been developed in line with this can be included.</p> <p>To mitigate industry concerns for development in close proximity to existing freight rail line, the applicant (Landcorp) established a working group which includes the PTA, Brookfield Rail, MRWA, the City and Landcorp. The working group has been</p>

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		<p>effective has to continue to operate 24 hours a day, 7 days a week. If the land use around the Primary Regional Road and freight rail link change to allow sensitive uses including residential there is the real potential for conflict. The key points raised in this submission applicable to the Robb Jetty Local Structure Plan and Emplacement Local Structure Plan are:</p> <ol style="list-style-type: none"> <li>1. Whilst both Local Structure Plans may achieve residential sustainability objectives, there is concern that in their current form they do not adequately address how they will accommodate current and increasing use of the freight rail line and Primary Arterial Road.</li> <li>2. Urban encroachment on the freight rail and road transport corridors is a lose - lose situation. Sensitive uses located near the corridors, as well as freight rail and road operations both suffer; and the primary beneficiary would appear to be the land developer.</li> <li>3. In 2011 the Minister for Transport announced Fremantle Ports' Inner Harbour will be retained as an operating container and general cargo working port in the long-term, and its container trade will double to about 1.2 million TEUs (containers) per annum around 2020 - 2025. It is difficult to forecast what proportion of this will move by rail but the current target is 30%. Rail is currently moving about 100,000 TEU p.a. clearly there is a strong likelihood that rail volumes will increase substantially in the future.</li> <li>4. The Port of Fremantle is the State's single major container port. The container trade has grown by an average of approximately 5.5% per annum over the last decade and with this growth there will be continuing reliance on road freight and increasing use of the freight rail link. In 2002 less than 3% of containers were transported from the port by rail - the proportion is currently about 14%.</li> <li>5. The Structure Plans incorrectly state that freight trains do not operate in the peak periods. Freight trains have in fact operated in the peak periods for over a decade. The challenge is that it is more difficult to manage given that a section of track is shared by both freight and passenger rail. However current restrictions could be removed at some point in the future with the redevelopment of the Fremantle Traffic Bridge. If this occurs, freight train movements will certainly occur at increasing frequency during the morning and afternoon peaks.</li> <li>6. Section 4.1.2 states that "It is envisaged that in order to attain the desired overall volume and percentage shipped by rail, a fourfold increase in train movements is potentially required although these will be limited to non-peak hour periods." Work by Fremantle Ports suggests that a suitable estimate of</li> </ol>	<p>involved in the review of existing and future at-grade and grade separated crossings, across the freight rail line.</p> <p>The working group has discussed maintaining the at-grade pedestrian and vehicle crossing at Rollinson Road and establishing a new at-grade pedestrian and vehicle crossing to support the 'main street' in Robb jetty. This would be established at the expense of the current McTaggart crossing which would be closed once the 'Main Street' crossing is established. Two grade separated pedestrian bridges would also be established to facilitate pedestrian access to the foreshore. These proposals are highlighted in a plan contained within the Robb Jetty LSP.</p> <p>Also see response to submission 18.</p> <p>Noted, the rapid transit route is likely to commence as a bus (with ability to transition to light rail) and will be within road reserve.</p>



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		<p>projected rail freight movements is about 18 per day, which includes 6 trains per day (12 movements) between the Inner Harbour and Forrestfield and 3 "other" trains per day (6 movements), potentially from areas such as Kwinana and Kalgoorlie. However there are many variables that could affect the actual number of future trains movements.</p> <p>7. It is with certainty that future freight rail projections are for it to grow and that freight trains will be longer and be more frequent. Additionally they may carry double stacked containers at some time in the future.</p> <p>8. It is noted that passenger rail is excluded from the Local Structure Plans, this is supported. Currently freight rail is required to share a small portion of the passenger rail line in Fremantle. This results in some limitation on current freight rail operations in morning and afternoons. It is likely that the use of passenger rail on any portion of the existing freight rail line south of Fremantle would create further limitations on current and future freight operations.</p> <p>9. WAPC Statement of Planning Policy 5.4 supports the principle of avoiding land use conflict as the first choice approach rather than creating and then seeking to manage conflict. This is evidenced by the following statement in the Policy: "Zoning and permissible uses of land in areas adjoining primary freight routes or established freight nodes should be reviewed to ensure, as far as practicable, that they are compatible with freight operations." The policy is clear, adjoining land uses should be compatible. However, contrary to the policy, the Local Structure Plans propose sensitive land uses next to the railway line and road corridor and propose to manage rather than avoid this conflict. Land uses immediately abutting the rail and road freight routes should only be developed on the following basis:</p> <ul style="list-style-type: none"> <li>• No residential or other sensitive land uses immediately abutting the road and rail freight routes.</li> <li>• Residential and other sensitive land uses being separated from the road and rail freight routes by other non-sensitive land uses.</li> </ul> <p>10. Section 8.5 acknowledges that noise will impact future residents, however it then goes on to state: <i>"the onus will be on the designers and developers of the new residential development to demonstrate to the satisfaction of the City of Cockburn that potential noise impacts have been considered and addressed."</i> The reports also suggest approval for noise amelioration measures at the</p>	<p>These proposals are for local structure plans, not rezoning. The City has an obligation under section 124 of the Planning and Development Act 2005 to reflect the intent of the Metropolitan Region Scheme. This location was rezoned to Urban in Sept 2011 and the City has reflected this by proposing a Development zone to enable structure planning to occur. District Structure Planning, undertaken by the Department of Planning and Landcorp was used to demonstrate the area was capable of development and supported the request to change the Metropolitan Region Scheme. Residential has been shown adjacent to the railway line in these earlier plans.</p> <p>This is a valid mitigation measure offered by SPP5.4.</p> <p>A noise assessment has been required for all development within 150m of the railway line. Development in accordance with the recommendations of those assessments will be a valid mitigation measure offered by SPP5.4.</p>

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		<p>building permit stage, however in light of previous failures we believe this is too late in the process. We believe the deferral this to a later stage of the planning process does not reflect good planning and all efforts to address this should be occurring now.</p> <p>11. The LSP indicates external noise criteria would be exceeded up to approximately 50m of the railway line and vibration criteria up to approximately 80m (using DEC criteria). As such it is suggested that there be no residential development within at least 80m of the rail line.</p> <p>12. Vibration has been identified by the consultant as an issue, but it is not adequately addressed in the Local Structure Plans. Vibration suppression means are available however they are not mentioned. It is unclear if anything is planned in this regard, though it is considered necessary.</p> <p>13. Level crossings are planned as part of the Local Structure Plans, with these crossings there are warning bells that sound as trains pass through. There is no evidence that this additional noise source has been accounted for.</p> <p>14. It is of concern that the LSP incorporates an additional rail crossing between the existing crossings at Rollinson Road and McTaggart Cove. This new crossing, referred to a Main Street, will be one of the key routes into the Robb Jetty precinct, and is planned to accommodate higher traffic volumes than other</p>	<p>Noted. The Noise and Vibration Study indicates vibration is an issue ranging from 50-80m along the railway line. While vibration is discussed in Part 2 of the structure plan, it does not contain a related statutory requirement in Part 1. This can be modified to also include vibration to be assessed where applicable.</p> <p>The Noise and Vibration Study includes a plan indicating where the readings for both noise and vibration were taken. The locations are near the existing Rollinson Rd crossing and appear to be in accordance with the methodology outlined in the guidelines which accompany SPP5.4. The existing crossing currently has warning signals and therefore this noise source would already be accounted for.</p> <p>This issue was previously raised as part of the submission for the District Structure Plan. As a result, the following annotation was added to that plan:</p> <p><i>"At-grade crossings will need to be designed, constructed and maintained to the satisfaction of the Office of Rail Safety, within the Department of Transport"</i></p> <p>The Department of Transport have also lodged a submission on these local structure plans and requested the Public Transport Authority (PTA) be consulted for new, upgraded or relocated crossings.</p> <p>The applicant (Landcorp) has already commenced the design process for each</p>

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		<p>internal roads. Also of concern is that the western section of Main Street where it crosses the rail lines is planned as a shared zone giving greater priority to pedestrians and cyclists.</p> <p>15. To assist with dealing with noise management there is a strong case for using positive covenants. There is successful precedent for putting these on titles that obligate land owners to incorporate noise amelioration in subsequent construction.</p> <p>16. The issue of emergency and recovery vehicle access in case of train derailments has not been addressed. The fundamental question to be answered as an imperative is if there is enough land around the rail corridor to provide emergency access in the event of a derailment and what this means for the surrounding propose land uses.</p> <p>17. The imagery on the cover of the Local Structure Plan is misleading and uninformative. Specifically it does not accurately detail the existing freight rail line. It has been represented as a minor rail line that an uninformed person could interpret as light rail, not a heavy freight rail line. The artist's impression does not show basic rail safety requirements such as level crossing signals and fencing along both sides of the rail line.</p> <p>18. To give statutory force to the matters raised in this submission it is strongly suggested that where possible that a Scheme Amendment occur incorporating measures to protect the transport corridors.</p> <p>Experience with urban encroachment at the Inner Harbour provides clear evidence that introducing sensitive uses, such as residential, in close proximity to transport and port infrastructure will present compatibility problems. There is concern that both the Local Structure Plans do not adequately address how they will accommodate the current and</p>	<p>crossing with the relevant parties.</p> <p>Memorials on Titles are already required as appropriate, as discussed in Part 1 of the local structure plan.</p> <p>There is no reduction of the existing railway corridor proposed. The corridor is also directly adjacent to the Foreshore Reserve (as it is currently). The land to the east of the corridor is abutted by adjacent roads or reserves for approximately half its length. Detailed design stage will enable appropriate locating of access gates as well.</p> <p>Noted. This has also been raised in another submission and Landcorp have advised they will update the image. A modification has already been noted.</p> <p>A number of the issues are not agreed with. The few which are can be adequately included into the structure plan, some within the statutory section of the plan (Part 1). There is no need to include further transport corridor matters in the Scheme. This has already been adequately catered for by Amendment 89 which included the provisions specific to Cockburn Coast Development Area.</p> <p>See comments above.</p>

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		<p>increasing use of the freight rail line and Primary Regional Road. The Local Structure Plans may achieve residential development sustainability; however they have the potential to impact on the current and future freight rail and road operations, which are critical elements of sustainable freight transport planning for the metropolitan area.</p> <p>The concept of sustainability requires that all elements are considered and that the optimal outcome for all, with a long term view, is sought. To give precedence to residential development around the road and rail linkages to the Inner Harbour is short term in approach, and may be considered inconsistent with the broader long term view required of sustainability.</p>	<p>As outlined above, the City has an obligation to implement the Urban zoning of this land. Land uses as guided by the District Structure Plan have been included as well as due adherence to the various State Planning Policies which are relevant. This proposal is not preventing the existing rail corridor from use; it is not reducing it, or seeking to collocate infrastructure within it. It provides for mitigation measures which are provided for by SPP5.4</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p> <ul style="list-style-type: none"> <li>• Provide clarity to the freight rail movement's information (provided by the operator).</li> <li>• Update perspective image used on cover of Robb Jetty LSP</li> <li>• Include reference to SPP1 State Planning Framework Policy</li> <li>• Broaden reference to Directions 2031</li> <li>• Update Part 1 of the Robb Jetty LSP to include requirement for Vibration Assessment in the 50-80m area adjacent to the rail corridor.</li> </ul>
20.	Ashley Palmer, Alba Edible Oils P.O. Box 385 South Fremantle WA 6162	<p><b>Objection</b></p> <p>Alba Edible Oils (Alba) understands the proposed Cockburn Coast project provides the framework for the redevelopment of the former Robb Jetty industrial area and the South Fremantle power station and is anticipated to take approximately 15 - 20 years to fully develop. As you would be aware, this area was previously the location of the Coogee Special Industrial Area, which housed a number of animal and marine processing industries and was the subject of significant government investment efforts to attract such businesses to the area, including Alba Edible Oils, in 1998.</p>	

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		<p>Alba is providing the comments below in respect of the proposed Emplacement Local Structure Plan (LSP) within the Cockburn Coast development area: The LSP has been preceded by a number of planning initiatives, including the Cockburn Coast District Structure Plan (DSP) Part 1 (2009) and Part 2 (2011) and more recently Scheme Amendment No. 89. Alba's key concern lies in the continued operating ability for the states' only remaining edible oil processor, Alba Edible Oils, which contributes over \$10 million per annum in direct refinery sales, without taking into account the two crushing facilities located in the South West and the many other food processors that rely on edible oil from Alba to continue operating their businesses. Alba's operations support a large part of the food supply chain throughout Western Australia. If Alba was not to remain in the State, a number of other local food manufacturers would close.</p> <p>Alba has been particularly proactive and engaged in the planning activities for the area and is represented on the Cockburn Coast planning committee.</p> <p>Alba's specific comments on the LSP relate to:</p> <ol style="list-style-type: none"> <li>1. Transitional arrangements.</li> <li>2. Statutory planning implementation mechanisms.</li> </ol> <p>Both of these issues were identified as being required to be addressed in the Local Structure Plan in the District Structure Plan Part 1 Section 3.4, Part 2 Section 3.3.4 <i>Local Structure Plans</i>, and Section 5.0 of Amendment No.89 <i>3.20 Statutory Planning Implementation</i>.</p> <p>Section 4.6 of the LSP does identify existing industrial activities and their associated buffers. It does not discuss any specific transitional arrangements and / or management of offsite impacts from existing developments. It is important to reiterate here Section 3.4 of the Cockburn Coast DSP Part 1 (2009) 'Transitional Arrangements', which states:</p> <p><i>Specific consideration needs to be given to transitional arrangements given the potential land use conflict between current and proposed development. This is particularly the case within the Darkan and Emplacement precincts due to the extent and nature of existing developments.</i></p> <p>This section goes on to identify transitional arrangement principles, operation and implementation, non-conforming use rights, including expansion issues, and most importantly identifies the local structure plan at which this level of detail will be provided. Alba does not believe transitional arrangements have been appropriately addressed to</p>	<p><b>Not Supported</b></p> <p>As observed in the submission the LSP proposes to address potential conflict between existing industrial uses and future sensitive land uses. The Emplacement LSP does more than infer how these conflicts will be dealt with. Sections 8 Noise Attenuation (Part 1) and Section 4.7 Industrial Activities (Part 2) outline the requirements for sensitive development proposal within proximity to an existing industrial use. It is not possible at this stage for more explicit transitional arrangements to be specified, as the type of suitable arrangements will be dependent upon the specific location and nature of any future proposals.</p>

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		<p>the intent of the DSP in the current LSP.</p> <p>It may be inferred from the LSP that the proposed transition mechanisms are:</p> <ol style="list-style-type: none"> <li>1. The Mixed Use zone itself which intends a significant proportion of non-residential uses;</li> <li>2. Noise attenuation measures including design response by proponents and notification on titles for those exceeding noise limits on Cockburn Rd;</li> <li>3. Where sensitive land uses are proposed, the provision of a technical analysis by proponent to reduce or mitigate existing industrial buffer zones.</li> </ol> <p>Alba requests that the transition arrangements be explicitly identified, discussed as a per the intent of Section 3.4 of the DSP, added as a subsection itself after Section 4.7 'Industrial Activities' in the LSP, and in addition be expanded to address the below issues (at minimum).</p> <p>It would also be appropriate to identify Alba Oils in the list of current land users within the LSP as detailed in Section 1.2.2. 'Area and Land Use', in particular given they were specifically referred to in the DSP as the most significant of these existing users' (Refer 3.5.5) which also identified that transitional arrangements will be particularly important in the Emplacement Precinct due to their presence.</p> <p>Transport Access. Vehicle access is critical to existing business on Emplacement Crescent. Alba operates 24 hours per day, seven days a week every day of the year. Crude oil is bought in daily using road trains from the South. Local deliveries using semi-trailers are loaded out daily and head North using Rockingham Road and Cockburn Road. Alba requires the current access roads to Port to remain as they freight containers direct to port for export. Business viability would be significantly impacted on if restrictions were placed on the size and number of trucks entering the site. Access to Emplacement Crescent as proposed in 5.5.1 'Movement Network' to better reflect and accommodate these existing transport activities. We would require that we can do a right hand turn from the existing set up in Emplacement Crescent and no medium strip to block this access is applied to Cockburn road.</p>	<p><b>Supported</b> It is recommended that the Emplacement LSP is amended to identify Alba Oils as a current land use in Section 1.2.2.</p> <p><b>Supported</b> It is recommended that Appendix E - Local Transport and Traffic Management Strategy be updated so as to include current and future intersection operations for the two intersections of Emplacement Crescent and Cockburn Road.</p> <p>Currently, only the southern intersection of Emplacement Crescent and Cockburn Road allows for right hand turns from Emplacement Crescent. It is recommended that future intersection of one of the intersections of Emplacement Crescent and Cockburn Road maintain a right hand turn from Emplacement Crescent.</p> <p><b>Not supported</b> Emplacement Crescent is not considered a major road under <i>State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning</i> and therefore not considered to generate significant noise which may affect sensitive land uses. As such no noise attenuation measures will be required as a result of Emplacement Crescent.</p> <p><b>Not Supported</b> Light spillage whether from industrial or residential land uses are required to be</p>

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		<p>Transport Noise. Noise attenuation measures should be considered to be expanded to Emplacement Crescent not only Cockburn Road. This includes the need for design requirements for buildings, including landscaping, facing onto Emplacement Crescent to minimize noise impacts.</p> <p>The intent for modelling to be required to assess potential impacts to sensitive land uses from lighting and light spillage for new proposals.</p>	<p>addressed by the producer of the light emissions under the City of Cockburn (Local Government Act) Amended Local Law 20123). It is impractical for development proposals to mitigate against light emissions from adjoining development as lighting is so easily altered so as to increase or decrease its emissions. Therefore, proposals would never be able to ensure that they completely mitigate light emissions from adjoining development. All new commercial development proposals will be designed to limit their off-site light emissions. It is not considered unreasonable that existing industrial/commercial development operate under those conditions.</p> <p><b>Not supported</b></p> <p>The City undertook a site by site analysis for all industrial land uses to determine a buffer for sensitive land uses that reflects the approved land use undertaken on each site. The EPA's <i>Guidance Note 3 Separation Distances between Industrial and Sensitive Land Use</i> provides "generic distances" and these are not "intended to be absolute separation distances".</p> <p>The City's Environmental Health Section have advised that there is no evidence to suggest that either noise or odour emissions from this facility are excessive or a nuisance. In addition, it is noted that there is no capacity to allow an intensification of the existing approved use nor is the City likely to approve any new use or process that has the capability to cause off site impacts. Under these circumstances it has therefore been considered appropriate to reduce the generic</p>

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		<p>The requirement for proposals for sensitive land uses to undertake a technical analysis to seek to reduce or mitigate existing buffers is identified under the separate 'Transitional Arrangements' section. It is noted Figure 27 of the LSP shows a 200m buffer around Alba Oils, yet the recommended buffer for edible oil processing under the EPA Guidance Statement 3 is 500m. Is there a reason why this has been reduced. Alba requests that the 500 metre buffer is applied.</p>	<p>500m buffer to align with cadastral boundaries so that adjacent blocks may be developed without having to compromise due to buffer lines across the lots. It is considered that the main potential off site impacts are from trucks entering and leaving the site. Given that the access point is not adjacent to potential residential premises it is not anticipated that this will be a problem.</p> <p><b>Not supported</b></p> <p>Bicyclists are required to travel on the road carriageways and not on exclusively on pedestrian only paths. The Emplacement LSP identifies an off-road shared path along running east west from the Beeliar Regional Reserve to Cockburn Road where Bicyclists and pedestrian share one path. These shared paths will 4m to 4.5m wide and are considered adequately wide to avoid conflict between bicyclists and pedestrians.</p> <p>The Emplacement LSP identifies a zebra crossing on Cockburn Road connecting the shared path on either side of Cockburn Road to create a continuous link from Beeliar Regional Reserve to the beach.</p> <p><b>Not supported</b></p> <p>It is noted that the District Structure Plan (2009) refers to 'retention of employment and staging of development' for the Emplacement Precinct – however this statement was made in the context of non-conforming use rights (the section under which this reference is included). It is not considered appropriate to refer to the 'retention of employment' as an objective for the Mixed Use zone because 'Industry' uses are not permitted in the 'Mixed Use' zone.</p>



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		<ul style="list-style-type: none"> <li>Ensure the separation of bikes and pedestrians on Emplacement Crescent to remove the potential for vehicle conflict. The original DSP also showed Emplacement Crescent connected to the beach side by pedestrian walkways over Cockburn road. Removal of this further increases the risk of pedestrian and vehicle conflict across the busy Cockburn Road.</li> </ul>	<p>Alba Oils is an 'Industry – General' use pursuant to the Scheme, and the DSP (2009) specifically identified that industrial uses are not considered appropriate for the 'Mixed Use' area. Therefore it is clear that the 'retention of employment' is to be facilitated by way of non-conforming use rights, and that these rights should not be hindered by development in the surrounding area. The Emplacement LSP identifies a buffer to Alba Oils, and restricts the development of sensitive land uses within that buffer, which is considered to address this matter adequately. To by ensuring Alba Oils can continue operating in accordance with current approvals.</p> <p><b>Noted</b></p> <p>As outlined earlier and stated in the submission the LSP addresses potential conflict between existing industrial uses and future sensitive land uses through noise attenuation requirements (Sections 8) and requirements for sensitive land uses proposal within buffers to industrial uses to demonstrate through technical analysis how impacts from the industrial uses are to be mitigated (Section 4.7 Industrial Activities (Part 2)).</p>

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		<ul style="list-style-type: none"> <li>The retention of employment and staging of development (as per DSP 3.5.5) is included in the objectives (for the Mixed Use Zone), acknowledging the important employment value existing operations bring to the area.</li> </ul>	

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		<ul style="list-style-type: none"> <li>How the City of Cockburn intends as per Section 5.0 3.20, Amendment No.89: <i>Identify and describe how future land use and development in accordance with the LSP will be managed so that areas experiencing offsite impacts from existing lawful development are either avoided or managed.</i></li> </ul> <p>Alba Edible Oils does not support the current Emplacement LSP as advertised until the issues relating to transitional arrangements as identified above are addressed. We look forward to the opportunity to work together with the Shire of Cockburn further.</p>	
21	Ashis Parajuli, Main Roads WA PO Box 6202 EAST PERTH WA 6892	<p><b>Objection</b></p> <p>Thank you for your letter dated 19 November 2012 requesting Main Roads comments on the above proposals.</p> <p>Main Roads has reviewed the proposed local structure plans for Robb Jetty and Emplacement and has no objections in principle subject to the following conditions:</p> <ol style="list-style-type: none"> <li>Development of an agreed planning design concept for Cockburn Road between Rockingham Road and Spearwood Avenue. As you may be aware, Landcorp, City of Cockburn, Department of Transport, Department of Planning and Main Roads are currently developing a revised planning design concept for Cockburn Road between Rockingham Road and Spearwood Road. The proposed concept includes upgrading of Cockburn Road to a four lane divided road with two lanes in each direction. A vehicle access strategy is also required to be developed for Cockburn Road to manage and control vehicular access from Cockburn Road.</li> <li>The proposed upgrade of Cockburn Coast to four lanes will require widening of existing Cockburn Road reserve. The widened road reservation will need to be protected through the local structural plan and subdivision process. Any additional land required shall be ceded at no cost to Main Roads.</li> </ol>	<p>Noted, the City is aware of the work undertaken for a design concept for Cockburn Road and the vehicle access strategy being drafted. Prior to forwarding the local structure plan to the Department of Planning, these draft documents can be required.</p> <p>The local structure plan will be clear as to the width required to Cockburn Road, including any areas where widening may be necessary (such as at intersections). It is noted there is already mention in the local structure plan regarding this potentially being a development</p>

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		<p data-bbox="533 531 1525 619">3. The applicant is required to undertake a transport noise assessment in accordance with the guidelines of the WAPC State Planning Policy 5.4 <i>"Road and Rail Transport Noise and Freight Considerations in Land Use Planning"</i>.</p> <p data-bbox="483 1177 719 1201">Advice to Applicant:</p> <ol data-bbox="533 1241 1525 1449" style="list-style-type: none"> <li>1. Following the development of an agreed planning design concept and reservation for Cockburn Road, Main Roads intends to initiate the removal of the Primary Regional Road (PPR) reservation of the future Cockburn Coast Drive from the MRS and have Cockburn Road up to Rollinson Road included in the MRS as a PPR.</li> <li>2. The structure plans propose a number of traffic signals along the existing</li> </ol>	<p data-bbox="1547 252 2096 619">contribution cost. The development contribution plan will need to elaborate on matters such as need and nexus to determine what proportion is appropriate to be required by the developers and what proportion is not related to the Cockburn Coast development. It is not appropriate for the local structure plan to categorically state that Main Roads have no responsibility for cost. Such a statement will not be included in the local structure plan as it seconds guessing the outcome of the development contribution plan assessment.</p> <p data-bbox="1547 655 2096 802">Noted, refer to comments on submission from the Department of Transport (prepared with the Public Transport Authority and Main Roads). Queries have been raised about the methodology used.</p> <p data-bbox="1547 839 2096 1329">Several interest groups and government agencies have indicated they do not believe the methodology has been followed properly. It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.</p> <p data-bbox="1547 1366 2096 1417">Noted, it is understood this is the intent of Main Roads.</p>

No.	Name/address	Submission	Council's recommendation
		<p>Cockburn Road. Main Roads approval is required for all proposed traffic signals prior to implementation. The applicant needs to provide justification and an evaluation of alternative measures for any proposed traffic signals. Supporting information such as a preliminary design drawing(s), predicted traffic and pedestrian volumes, SIDRA analysis and traffic impact reports will need to be included for any formal assessment.</p> <p>3. The widening/upgrading of Cockburn Road is not in the Main Roads' current 4 year program and as such is considered long term. However, Main Roads is working with Landcorp and other stakeholders develop staging options to facilitate incremental improvement to Cockburn Road.</p>	<p>Noted, the applicant has been advised of this (by provision of the content of this submission).</p> <p>Noted.</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p> <ul style="list-style-type: none"> <li>Recommendation to require the concept design and vehicle access strategy to be provided, prior to the local structure plan being forwarded to the WA Planning Commission.</li> </ul>
22	The Department of Agriculture and Food Western Australia Locked Bag 4 BENTLEY DC 6983	<p>The Department of Agriculture and Food Western Australia (DAFWA) role is to assist the State's Agriculture, Food and Fibre sectors to be sustainable and profitable, with a clear focus on export-led growth. DAFWA understands the proposed Cockburn Coast project provides the framework for the redevelopment of the former Robb Jetty industrial area and the South Fremantle Power station and is anticipated to take approximately 15 - 20 years to fully develop. As you would be aware, this area was previously the location of the Coogee Special Industrial Area which housed a number of animal and marine processing industries and was the subject of significant government investment efforts to attract such businesses to the area in the late 1990's.</p> <p>DAFWA is providing the below comments in respect of the proposed Emplacement Local Structure Plan (LSP) within the Cockburn Coast development area. The LSP has been preceded by a number of planning initiatives, including the Cockburn Coast District Structure Plan (DSP) Part 1 (2009) and Part 2 (2011) and more recently Scheme Amendment No. 89. DAFWA's key concern lies in the continued operating</p>	

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		<p>ability for existing food and seafood processors in Emplacement Crescent such as Alba Edible Oils. Alba Oils are the States only remaining edible oil processor and are an integral part of the food processor supply chain in Western Australia. Comments are also applicable to operators such as Fremantle Cold stores in the Robbs Jetty Precinct. DAFWA understand Alba Oils has been proactive and engaged in the Cockburn Coast planning initiatives, including participating in the Cockburn Coast planning committee on the basis transitional arrangements for existing business would be accommodated in future planning documents. DAFWA specific comments on the LSP relate to:</p> <ol style="list-style-type: none"> <li>1. Transitional arrangements.</li> <li>2. Statutory planning implementation mechanisms.</li> </ol> <p>Both of these issues were identified as being required to be addressed in the Local Structure Plan in the District Structure Plan Part 1 Section 3.4, Part 2 Section 3.3.4 <i>Local Structure Plans</i>, and Section 5.0 of Amendment No.89 <i>3.20 Statutory Planning Implementation</i>.</p> <p>It is important to reiterate here Section 3.4 of the Cockburn Coast DSP Part 1 (2009) Transitional Arrangements', which states:</p> <p><i>Specific consideration needs to be given to transitional arrangements given the potential/and use conflict between current and proposed development. This is particularly the case within the Darkan and Emplacement precincts due to the extent and nature of existing developments.</i></p> <p>This Section goes on to identify transitional arrangement principles, operation and implementation, non-conforming use rights, including expansion issues, and most importantly identifies the local structure plan at which this level of detail will be provided. DAFWA does not believe transitional arrangements have been appropriately addressed to the intent of the DSP in the current LSP. Section 4.6 of the LSP identifies existing industrial activities, their associated buffers and discusses the need for a technical analysis by new proponents to reduce these buffers. Other intended transition mechanisms may assumed to be:</p> <ol style="list-style-type: none"> <li>1. The Mixed Use zone itself which intends to include a significant proportion of non-residential uses; and</li> <li>2. Noise attenuation measures including design response by proponents and notification on titles for those exceeding noise limits on Cockburn Rd.</li> </ol> <p>DAFWA requests transitional arrangements be explicitly discussed as per the intent of</p>	<p><b>Not Supported</b></p> <p>The proposed Emplacement LSP addresses potential conflict between existing industrial uses and future sensitive land uses through noise attenuation requirements in Sections 8 and requirements for sensitive land uses proposal within buffers to industrial uses to</p>

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		<p>Section 3.4 of the DSP, added as a subsection itself after Section 4.7 'Industrial Activities' in the LSP, and be expanded to address the below issues (at a minimum) –</p> <ul style="list-style-type: none"> <li>Identify any existing operators and discuss key issues in Section 4.7, Figure 27 as to why buffers have been required.</li> <li>Vehicle access is critical to existing business on Emplacement Crescent, particularly those such as Alba that require freight container access to the Fremantle port. Access to Emplacement Crescent as proposed in 5.5. 1 'Movement Network' needs to better reflect existing transport activities.</li> </ul>	<p>demonstrate through technical analysis how impacts from the industrial uses are to be mitigated in Section 4.7 Industrial Activities (Part 2).</p> <p><b>Supported</b> It is recommended that Appendix E - Local Transport and Traffic Management Strategy be updated so as to include current and future intersection operations for the two intersections of Emplacement Crescent and Cockburn Road.</p> <p>Currently, only the southern intersection of Emplacement Crescent and Cockburn Road allows for right hand turns from Emplacement Crescent. It is recommended that future intersection of one of the intersections of Emplacement Crescent and Cockburn Road maintain a right hand turn from Emplacement Crescent.</p> <p><b>Not Supported</b> Emplacement Crescent is not considered a major road under <i>State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning</i> and therefore not considered to generate significant noise which may affect sensitive land uses. As such no noise attenuation measures will be required as a result of Emplacement Crescent.</p>

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		<ul style="list-style-type: none"> <li>Noise attenuation measures need to be expanded to Emplacement Crescent not only Cockburn Road. This includes the need for design requirements, landscaping and notifications on titles.</li> <li>Modelling should be required to assess potential impacts on new proposals for sensitive land uses from lighting and light spillage of existing businesses.</li> </ul>	<p><b>Not Supported</b> Light spillage whether from industrial or residential land uses are required to be addressed by the producer of the light emissions under the City of Cockburn (Local Government Act) Amended Local Law 20123). It is impractical for development proposals to mitigate against light emissions from adjoining development as lighting is so easily altered so as to increase or decrease its emissions. Therefore, proposals would never be able to ensure that they completely mitigate light emissions from adjoining development. All new commercial development proposals will be designed to limit their off-site light emissions. It is not considered unreasonable that existing industrial/commercial development operate under those conditions.</p> <p><b>Noted</b></p> <p><b>Not Supported</b> The City undertook a site by site analysis for all industrial land uses to determine a buffer for sensitive land uses that reflects the approved land use undertaken on each site. The EPA's <i>Guidance Note 3 Separation Distances between Industrial and Sensitive Land Use</i> provides "generic distances" and these are not "intended to be absolute separation distances". The City's Environmental Health Section have advised that there is no evidence to suggest</p>



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		<ul style="list-style-type: none"> <li>• The requirement for proposals for sensitive land uses to undertake a technical analysis to seek to reduce or mitigate existing buffers is discussed under the separate 'Transitional Arrangements' section.</li> <li>• It is noted Figure 27 of the LSP shows a 200m buffer around Alba Oils, yet the recommended buffer for edible oil processing under the EPA Guidance Statement 3 is 500m. Please advise why the buffer has been reduced.</li> </ul>	<p>that either noise or odour emissions from this facility are excessive or a nuisance. In addition, it is noted that there is no capacity to allow an intensification of the existing approved use nor is the City likely to approve any new use or process that has the capability to cause off site impacts. Under these circumstances it has therefore been considered appropriate to reduce the generic 500m buffer to align with cadastral boundaries so that adjacent blocks may be developed without having to compromise due to buffer lines across the lots. It is considered that the main potential off site impacts are from trucks entering and leaving the site. Given that the access point is not adjacent to potential residential premises it is not anticipated that this will be a problem.</p> <p><b>Not Supported</b> Bicyclists are required to travel on the road carriageways and not on pedestrian only paths. The Emplacement LSP identifies an off-road shared path along running east west from the Beeliar Regional Reserve to Cockburn Road where Bicyclists and pedestrian share one path. These shared paths will 4m to 4.5m wide and are considered adequately wide to avoid conflict between bicyclists and pedestrians. The Emplacement LSP identifies a zebra crossing on Cockburn Road connecting the shared path on either side of Cockburn Road to create a continuous link from Beeliar Regional Reserve to the beach.</p> <p><b>Not supported</b> The employment value of existing operations is noted; however these are 'industrial' uses,</p>

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		<ul style="list-style-type: none"> <li>Ensure the separation of bikes and pedestrians on Emplacement Crescent to remove the potential for vehicle conflict. The original DSP also showed Emplacement Crescent connected to the beach side by pedestrian walkways over Cockburn road. Removal of this further increases the risk of pedestrian and (large) vehicle conflict across the busy Cockburn road.</li> </ul>	<p>which require buffers to sensitive land uses. They are not uses that are compatible in the long term with urban development.</p> <p><b>Noted</b> The proposed Emplacement LSP addresses potential conflict between existing industrial uses and future sensitive land uses through noise attenuation requirements in Sections 8 and requirements for sensitive land uses proposal within buffers to industrial uses to demonstrate through technical analysis how impacts from the industrial uses are to be mitigated in Section 4.7 Industrial Activities (Part 2).</p> <p><b>Not supported</b> Proposals to expand existing premises are not likely to be supported. The Scheme's non-conforming use rights will apply to existing uses. It is considered that the LSPs adequately deal with transitional arrangements.</p>

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		<ul style="list-style-type: none"> <li data-bbox="533 472 1525 592">• The retention of employment and staging of development (as per DSP 3.5.5) is included in Section 4. 7 and in the objectives for the Mixed Use Zone, acknowledging the important employment value existing operations bring to the area.</li> <li data-bbox="533 842 1525 995">• Address how the City of Cockburn intends as per Section 5.0 3.20, Amendment No.89 to: <i>Identify and describe how future land use and development in accordance with the LSP will be managed so that areas experiencing offsite impacts from existing lawful development are either avoided or managed.</i></li> </ul> <p data-bbox="483 1337 1525 1453">Amendment 89 will be particularly important for providing direction on how Council intends to deal with proposals to expand or modify premises (i.e. expand on DSP Part 1, 3.5.2). DAFWA does not support the current Emplacement LSP as advertised on the basis that matters relating to transitional arrangements have not been addressed. The</p>	

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		<p>lack of appropriate transitional arrangements is a significant issue. DAFWA sees frequently impacting on existing food businesses in redevelopment areas. This issue requires resolution in the Emplacement LSP given the commitment stated in DSP Parts 1 and 2 to address this issue in greater detail at the LSP stage, and the stated 15-20 year timeframe to full development at Cockburn Coast which in the interim to full development effectively leaves existing businesses in a planning limbo.</p>	
23.	<p>Department of Transport Level 8, 140 William Street Perth WA 6000</p>	<p>Reference is made to the City of Cockburn's request for comments on the above noted Local Structure Plans (LSPs). The Department of Transport (DoT) has liaised with the Public Transport Authority (PTA) and Main Roads Western Australia (MRWA) and provides the following comments on each of the LSPs.</p> <p>The DoT is also aware that the Freight and Logistics Council has written to you and shares some of their concerns, some of which are reiterated below. The DoT is prepared to support the two structure plans on the condition that the issues raised in this letter are addressed prior to consideration by the Western Australian Planning Commission (WAPC).</p> <p><b>General Comments</b></p> <p>While the Transport Portfolio agencies generally support intensification of urban development and the creation of employment opportunities through infill development, there are a number of issues associated with development as proposed in the Robb Jetty and Emplacement Local Structure Plans. Additional information and strategies will be required to address such issues as the mitigation of freight rail noise and vibration, provision of level crossings and fencing along the rail lines.</p> <p>The Transport Portfolio agencies compliment the City for undertaking the Cockburn Coast Integrated Transport Plan to ensure all modes are considered in the proposed developments. More detailed traffic modelling needs to be undertaken to determine the traffic implications of the development, particularly the projected traffic volumes for intersections on Cockburn Road and the existing and proposed level crossings over the freight line. This will be particularly relevant at the development application stage as it may affect development setback requirements and access issues.</p> <p>The Cockburn Coast District Structure Plan (CCDSP) recognises the important role that rail will continue to play in the State's freight movement strategy and the likely increase of freight movements along the rail line. The Fremantle freight rail corridor forms the western boundary of this LSP. As such, noise and vibration generated by freight trains</p>	<p>This submission was followed up with the Department of Transport following a meeting arranged by the applicant.</p> <p>The Department of Transport have since advised the only unresolved matter is that of the methodology followed for the assessments of noise and vibration. They also note they believe a Noise Management Plan is required at the Local Structure Plan stage.</p> <p>Several interest groups and government agencies have noted they do not believe the methodology has been followed properly. It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply</p>

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		<p>must be taken into consideration particularly as they operate 24 hours a day, 7 days a week, and by 2031 it is anticipated the current 22 train movements per week will have increased to 126. The WAPC's draft <i>Outer Metropolitan Perth and Peel Sub-regional Strategy</i> notes that corridors with a predominant freight function are identified in <i>State Planning Policy 5. 4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning (SPP.5.4)</i>. This policy aims to ensure that major freight corridors are protected from incompatible urban encroachment. The Cockburn Coast area is included in SPP 5.4. The Transport Portfolio agencies have noted that the noise levels contained within Appendix B of the Structure Plan, the Noise Vibration Study, do not adequately meet policy requirements and are inconsistent with the <i>SPP 5.4</i> based on the following:-</p> <ul style="list-style-type: none"> <li>• The maximum noise levels used in the Cockburn Coast Noise Vibration Study are inconsistent with the SPP 5.4;</li> <li>• The noise modelling should have used Outside Noise Criteria (Table 1 Herring Storer report) rather than Inside Noise Criteria;</li> <li>• The noise modelling has been undertaken for a quieter trains and does not take into account other classes of train in comparison with other studies carried out by the PTA; and</li> <li>• The Study does not depict noise modelling contours for the freight line, hence the extent of noise impacts on the proposed development is not defined or clear. The Robb Jetty LSP also does not adequately address vibration from the existing freight rail. The Noise Vibration Study shows that development is impacted up to 65 - 80 metres from the freight rail but development is indicated well within this distance, and no vibration mitigation measures have been considered or recommended.</li> </ul> <p>With regard to safety and security, the PTA will require an upgrade to the freight line fencing to PTA standard and at no cost to the PTA. The PTA has previously advised that no additional level crossings are to be provided. It is understood that LandCorp are seeking to close McTaggart Cove crossing to enable the proposed Main Street crossing to be provided.</p> <p><b>Rob Jetty Local Structure Plan and Emplacement Local Structure Plans</b></p> <p>Cockburn Road forms the eastern boundary of the Robb Jetty LSP and the western boundary of the Emplacement LSP area. It is understood that discussions are underway</p>	<p>with noise and vibration constraints.</p> <p>The Noise and Vibration Strategy forms an addendum to the local structure plan ("LSP") and indicates the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The Design Guidelines will also outline the requirements for compliance with noise and vibration for land within the impact zone. Both the LSP and the draft Design Guidelines also include requirements for Notification on titles and refer back to SPP5.4 where the specifications for these more detailed assessments reside.</p> <p>With regard to the request for a Noise Management Plan to be done at the local structure plan stage, the applicant has indicated this plan will be done at the development approval stage (i.e. on a lot by lot basis). This appears consistent with the intent of SPP5.4 which does not specify the Noise Management Plan must be done at the local structure plan. Looking at the content of a Noise Management Plan outlined in the guidelines which accompany the SPP5.4, it seems most of this information is already captured via the Noise and Vibration Assessment Study.</p> <p>Spatially, the local structure plan would not change if this Noise Management Plan were undertaken at this early stage. Opportunities for setting back of development lots further from the railway line has effectively been lost. Planning since the CCDSP 2009 has indicated urban development abutting the railway line. This situation was compounded by the rezoning to 'urban' under the MRS and</p>

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		<p>with MRWA, DoT, LandCorp and the City on the future form and function of Cockburn Road, and there may no longer be a need for the inland Cockburn Coast Drive as it appears that future traffic volumes may be able to be accommodated on a 4 lane Cockburn Road. This may have statutory planning implications for the site at the development stage. Further consultation with MRWA will be required, including further traffic analysis at the proposed intersections. To ensure sufficient road capacity through this area, the DoT, MRWA, City of Cockburn and LandCorp have agreed that MRWA will develop a suitable road design concept to accommodate the projected traffic volumes of around 30,000 annual average weekday traffic (AAWT) by 2031. The following parameters should be adopted for development of the design concept:</p> <ul style="list-style-type: none"> <li>• Four-lane divided road, typically with two 3.5 metre traffic lanes in each direction;</li> <li>• A central median varying from 2 metres to 5.5 metres to accommodate right turn pockets at intersections;</li> <li>• 1.5 metre on-road cycle lanes in both directions;</li> <li>• 5.1 m verges to accommodate shared paths (to connect / extend the existing shared path south of McTaggart Cove), street trees and underground service infrastructure;</li> <li>• Adequate chanelisation/turn pockets at intersections; and</li> <li>• Bus priority facilities in accordance with the proposed bus rapid transit requirements. Given the above design elements, a mid block reservation width of up to 32 metres may be required.</li> </ul> <p>The proposed pedestrian and cycling network across both LSP areas indicates that the main internal bicycle network will primarily consist of on-road facilities. Figures 40 and 41 in the Embankment and Robb Jetty LSPs need to be modified to show shared paths on both sides of Cockburn Road. In addition, the LSPs need to show on-road bike lanes on Main St in order to be consistent with the cross section (Figure 34). These bike lanes are to continue through the Emplacement LSP area. Bicycle priority treatments are also required at signalised intersections on designated bicycle routes.</p> <p><b>Recommendations</b></p> <p>The DoT strongly requests that the City incorporate the following comments in its</p>	<p>there is very little scope to see a different land use response to that of a built form response on a lot by lot basis.</p> <p>Given there is no indication otherwise from the DEC and given the apparently reasonable approach to the methodology used in the Noise and Vibration Assessment Study do not recommend withholding endorsement of this local structure plan on this matter. The Department of Transport are welcome to raise their concerns with the Department of Planning prior to their consideration of the plan.</p>

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		<p>determination on both Local Structure Plans.</p> <ol style="list-style-type: none"> <li>1. Existing, relocated and new road traffic and pedestrian at grade and grade separated rail crossings are to be designed and located to PTA's safety and operational requirements.</li> <li>2. Fencing along the freight rail line is to be upgraded at no cost and to the satisfaction of the PTA.</li> <li>3. The Robb Jetty LSP is to clearly show a 150m impact zone on each side of the freight rail line as a support to noise and vibration mitigation measures.</li> <li>4. A revised Noise Vibration Study needs to be undertaken in accordance with SPP 5.4 to indicate the noise and vibration contours in locations along the length of the rail line in order to show where noise and vibration levels exceed the acceptable levels noted in SPP 5.4. The study will also need to provide specific recommendations on appropriate noise and vibration mitigation measures.</li> <li>5. The Robb Jetty LSP is to clearly demonstrate how noise and vibration mitigation will be addressed at subdivision and development application stages, including: <ul style="list-style-type: none"> <li>• Appropriate building materials and noise mitigation treatments are to be incorporated into Building Design Guidelines to address road and rail freight noise and vibration issues.</li> <li>• A moratorium must be included in the Certificate of Title of each development to include the Building Design Guidelines and materials used to adhere to noise attenuation measures identified in SPP 5.4.</li> <li>• The Building Design Guidelines must be included into the City's Town Planning Scheme to ensure that conditions of development are a statutory requirement.</li> </ul> </li> <li>6. Widening/upgrading of Cockburn Road is to be designed to MRWA standards, and additional land to be ceded free of cost to MRWA.</li> <li>7. The need for the traffic signals proposed for Cockburn Road is to be demonstrated and approved by MRWA prior to implementation including a SIDRA intersection analysis to ensure intersection capacity is adequate to meet</li> </ol>	

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		<p>the demands of regional traffic along Cockburn Road.</p> <p>DoT would appreciate receiving advice if the above inclusions cannot be adopted by Council.</p>	
24.	Department of Planning Locked Bag 2506 Perth WA 6001	<p>I refer to the City's letters dated 16 November 2012 (received by the WAPC 19 November 2012) regarding the above local structure plans (LSPs).</p> <p>Please be advised that the WAPC is not prepared to endorse the proposed LSPs until such time as consideration is given to and response provided in respect of the following:</p> <p><b>In respect of the LSP Documents – Emplacement</b> Executive Summary – the POS does not tally with the figures under Section 5.1 table 3</p> <p>Figure 1 (Local Structure Plan Map)</p> <ul style="list-style-type: none"> <li>• A scale should be noted;</li> <li>• low density under Legend should be corrected to medium density,</li> </ul> <p>• query location of R40 area adjacent R100 and R160,</p> <ul style="list-style-type: none"> <li>• location of switch yard is mainly on WAPC site rather than next to POS. This is counter to previous discussion whereby the switch yard was going to be located on LandCorp land. Switch yard should not be zoned but reserved as public purpose and retained as freehold.</li> </ul>	<p><b>Supported</b> It is recommended that the table in the Executive Summary and in section 5.1 (Table 3) and 5.6.1 (Table 9) be revised to accurately reflect the quantity of POS (including the correct amount of restricted POS).</p> <p><b>Supported</b> It is recommended that a scale be added to Figure 1, and the 'low density' label be corrected to 'medium density' within the Emplacement LSP report.</p> <p><b>Not supported</b> The proposed R40 is consistent with that shown on the District Structure Plan Part 2.</p> <p><b>Not supported</b> The indicative location of the switchyard shown in the Infrastructure and Servicing Report aligns with that shown on the DSP Part 2, and is located where it has always been contemplated and discussed in the Switchyard Working Group, which includes the Department of Planning.</p> <p><b>Noted</b></p>



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		<ul style="list-style-type: none"> <li>Note that the pedestrian underpass and bridge are located outside of the LSP area therefore question their deliverability.</li> </ul> <p>Section 2.2 Use Class Permissibility – If this taken from the City of Cockburn Town Planning Scheme question the need to include in this document.</p> <p>Section 3.0 Operation Date – last paragraph, object to WAPC endorsing in sixth year, delete reference to sixth year.</p> <p>6.1 Proposed Residential Density - reference has been made to Activity Centre which is not shown in the LSP area. Delete reference to Activity Centre.</p>	<p>The pedestrian underpass is shown outside of the LSP area, however, it is shown for the purposes of identifying how pedestrian connectivity can be achieved, and would form part of Cockburn Coast Drive.</p> <p><b>Not supported</b> The use class table (Table 2) is not taken from City of Cockburn Town Planning Scheme No. 3, and it includes 'Mixed Use', which is not a zone in the Scheme. It is therefore recommended that the use class table remain in the Emplacement Crescent LSP.</p> <p><b>Supported</b> Delete references to WAPC endorsing the Local Structure Plans in the sixth year, as this sets an inflexible timeframe for review of the Local Structure Plans which may not be appropriate.</p> <p><b>Supported</b> It is recommended that reference to an activity centre zone under Section 6.1 of the Emplacement Local Structure Plan be deleted.</p> <p><b>Supported</b> It is recommended that any references to 'Cockburn Coast Redevelopment Area' be deleted.</p> <p><b>Not Supported</b> Section 8.4 Lot Design Guidance relates to subdivision of land.</p>

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		<p>7.3 Floor space Bonus – reference to Cockburn Coast Redevelopment area new term not defined, delete and just use Cockburn Coast District Structure Plan.</p> <p>8.0 Subdivision and Development Requirements – Section refers to development requirements however there are no subdivision provisions. Is this because there are none and have been dealt with?</p> <p>11.1 Variation to the Residential Design Codes – reference is made to the variations being outlined in the approved design guidelines however variation to the residential design codes are only permissible by the WAPC.</p> <p>Note that there is no Fire Management Plan for this LSP. The LSP is adjacent to Beeliar Regional Park and the Cockburn Coast Drive Primary Regional Road reservation. Whilst it is noted that the reservation is in place it may be several years if at all that the road is developed. In the meantime the area is vegetated and could be a fire hazard. The LSP must respond to this in accordance with the WAPC Planning for Bushfire Protection Guidelines Edition 2 May 2010.</p>	<p><b>Not Supported</b> Section 5.3- Scope of local planning policies, of the <i>Residential Design Codes</i> allows for local planning policies to vary various provisions of the Codes without the approval of the WAPC. The Design Guidelines will be approved as a local planning policy pursuant to City of Cockburn Town Planning Scheme No. 3.</p> <p><b>Supported</b> The Emplacement LSP proposes development within 100m of vegetation which may be considered a 'moderate to extreme' bushfire hazard. Therefore in accordance with <i>Planning for Bushfire Protection</i> the LSP should be supported by a bush fire hazard assessment. It is recommended that a bush fire hazard assessment is prepared and the LSP be amended to consider the outcomes of the assessment.</p>
25.	Department of	I refer to your letter dated 19 November 2012, referring the above proposed local	

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	Environment and Conservation PO Box 1167 Bentley Delivery Centre WA 6983	<p>structure plans to the Department of Environment and Conservation (DEC) for comment. DEC has reviewed the documents for the Robb Jetty and Emplacement local structure plans and this submission relates to both areas.</p> <p><b>General</b> The Emplacement local structure plan (LSP) area is located adjacent to the Manning Lake area, which comprises <i>Bush Forever Site No. 247 "Manning Lake and Adjacent Bushland, Hamilton Hill/Spearwood"</i> and forms part of Beeliar Regional Park, which is managed by the City of Cockburn and DEC for conservation and recreation purposes. A proposed primary regional road (Cockburn Coast Drive) separates the Manning Lake area from the structure plan area. DEC has provided advice in relation to the earlier <i>Metropolitan Regional Scheme amendment No. 1180/41</i> (WAPC Ref. 809-2-23-17 Pt 1), which covers these structure plan areas, and provided advice to the City of Cockburn in relation to the <i>Cockburn Coast District Structure Plan (part 2) and Town Planning Scheme no. 3 - Amendment no. 89 (proposed zoning changes to Cockburn coast industrial area)</i> in a letter dated 30 November 2011. The relevant aspects of the earlier advice and additional information are provided for your consideration.</p> <p><b>Native vegetation management</b> DEC notes an ecological assessment report (Appendix C) has been prepared for the Emplacement LSP area; however a copy of this report has not been provided to DEC. The Emplacement LSP report - Part 2 Explanatory 3.1 Environmental Assets and Constraints: outlines that there are patches of vegetation in good condition with 2.96ha vegetation to be cleared and vegetation type 1 located on the eastern side of the project site has similarities to DEC-listed threatened ecological community [SCP 26a]. DEC therefore recommends that detailed flora and vegetation surveys of all potentially affected areas of native vegetation be conducted by an environmental consultant, in accordance with the Environmental Protection Authority's (EPA's) <i>Guidance Statement 51 - Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia</i>. The survey should determine the presence or otherwise of priority or other significant flora and plant assemblages. If such flora and vegetation is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts.</p> <p>DEC has reviewed the ecological assessment for the Robb Jetty LSP area. The field study was conducted in March 2012, which is not considered the optimal time for flora surveys; therefore not considered conducted in accordance with EPA's Guidance</p>	<p><b>Supported</b> It is recommended that a spring flora and vegetation survey be undertaken within the Emplacement LSP, prior to subdivision or development of the land (where development proposes works to the land). It is recommended that the Emplacement LSP report be modified to reflect this requirement, and that Council advise landowners of the requirement to ensure they can factor it into the timing of any proposals.</p> <p><b>Supported</b> No further changes are recommended as a result of this comment.</p>

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		<p>Statement 51. However, it is noted that the Robb Jetty study site is highly modified and degraded due to a history of multiple disturbances and development. Therefore DEC concurs the site comprises limited environmental value.</p> <p>Regardless of the outcomes of the above recommended flora and vegetation surveys for the Emplacement LSP area, DEC recommends that areas of native vegetation in good or better condition within the structure plan areas be retained and incorporated into future public open space (POS).</p> <p>Additionally, any clearing of native vegetation requires a clearing permit obtained from DEC, unless of a kind that is exempt in accordance with Schedule 6 of the <i>Environmental Protection Act 1986</i> or <i>Regulation 5 of the Environmental Protection</i></p>	<p><b>Not Supported</b> The proposed areas of POS within the Emplacement LSP are consistent with the CCDSP Part 2. Vegetation within the green POS links will be retained where possible to provide a physical and ecological link between the foreshore and Beeliar Regional Park. While it may be possible for some vegetation to be retained within POS, the key function of the proposed POS is to provide a variety of recreational functions for residents and visitors, cognisant of the fact that it will be a high density environment. The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense and diverse urban environment with high levels of accessibility.</p> <p><b>Noted</b> No further changes are recommended as a result of this comment.</p> <p><b>Noted</b> Obligations under the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act) are noted.</p>

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		<p><i>(Clearing of Native Vegetation) Regulations 2004</i>. DEC's Native Vegetation Conservation Branch should be contacted regarding the possible need for a clearing permit.</p> <p><b>Fauna management</b>  The Manning Lake area and adjoining bushland serves as habitat for a variety of native fauna, which the proposed Emplacement LSP area has the potential to impact upon, as development proceeds. The Cockburn Coast District Structure Plan (DSP) area is known to serve as roosting and foraging habitat for the threatened Carnaby's cockatoo (<i>Calyptorhynchus latirostris</i>). In addition, the area is likely to support the common and widespread <i>Lomandra maritima</i> and possibly <i>Lomandra hermaphrodita</i>, either of which can support populations of the threatened Graceful sun-moth (<i>Synemon gratiosa</i>). The Stage 1 <i>Flora and Vegetation Assessment (ENV Australia, 2008)</i> indicates that <i>Lomandra maritima</i> has been identified within the Cockburn Coast DSP area. Both Carnaby's cockatoo and the Graceful sun-moth are listed as 'fauna that is rare or likely to become extinct' under the <i>Wildlife Conservation Act 1950</i>. The Priority 3 Lined Skink (<i>Lerista lineata</i>) has also been recorded in the Manning Lake bushland.</p> <p>DEC notes an ecological assessment report (Appendix C) has been prepared for the Emplacement LSP area; however a copy of this report has not been provided to DEC. The Emplacement LSP report - Part 2 Explanatory 3.1 Environmental Assets and Constraints: outlines that there are patches of vegetation in good condition with 2.96ha of good quality feeding habitat for Carnaby's cockatoo to be cleared. DEC therefore recommends that, prior to structure planning being finalised, the proponent undertake a detailed fauna survey in accordance with EPA <i>Guidance Statement No. 56 - Terrestrial Fauna Surveys for Environmental Impact Assessment in Western Australia</i>. If habitat suitable for conservation significant fauna is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts</p> <p>DEC has reviewed the ecological assessment for the Robb Jetty LSP area. It is noted that the Robb Jetty study site is highly modified and degraded due to a history of multiple disturbances and development. Therefore DEC concurs the site comprises limited fauna habitat value. Both Carnaby's cockatoo and the Graceful sun-moth are protected by the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities (SEWPaC) to determine what responsibilities they have under the EPBC Act.</p>	<p><b>Supported</b>  The limited fauna habitat within the Rob Jetty LSP is noted.</p> <p><b>Not Supported</b>  These comments do not relate to the proposed Emplacement LSP. The DEC is responsible for the management of the Beeliar Regional Park and Main Roads are responsible for the design and construction of any future road within the Primary Regional Road Reserve. Therefore ensuring a sensitive interface between the future road and the Reserve are beyond the control of any landowner within the Emplacement LSP.</p> <p><b>Supported</b>  It is recommended that additional provisions</p>

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		<p><b>Boundary interface treatment (with Beeliar Regional Park)</b></p> <p>It is noted that the Emplacement LSP area abuts the proposed Cockburn Coast Drive road reserve. Cockburn Coast Drive will represent the future western boundary of Beeliar Regional Park at the Manning Lake area and if this road is to proceed, DEC considers it important that a high standard of visual amenity be created and maintained along this parkland interface. In this regard, the design of this road should minimise its visual impact as far as possible, and an emphasis should be placed on vegetating road batters and rehabilitating existing degraded areas with appropriate native plant species of local provenance. It is also recommended that construction of a dual use pathway along/adjacent to Cockburn Coast Drive be considered. DEC would prefer to see such a pathway located on the eastern side of this road (adjacent to Beeliar Regional Park).</p> <p>Until such time that the Cockburn Coast Drive is constructed, the proponent should ensure there is adequate fencing between any development site and areas retained for conservation, and between any development site and Beeliar Regional Park. Additionally, no vegetation, earth spoil or any other debris is to be disposed of within the adjacent regional park.</p>	<p>be included in Section 8 (Part 1) requiring development proposals to ensure adequate interface, including fencing, to the Primary Regional Road Reserve in order to protect the conservation value of the Beeliar Regional Reserve. In regard to dumping on either reserves this is an illegal act and the proposed Emplacement LSP is not the appropriate document to reiterate this.</p> <p><b>Noted</b></p> <p>The location of the dual use paths and the pedestrian underpasses connects to the existing compacted limestone paths that run within the Primary Regional Road Reserved and the Beeliar Regional Reserve. The existing paths generally accord with the <i>Beeliar Regional Park Management Plan 2006</i>. More detailed future designing of the underpass will include liaison with DFES.</p> <p><b>Noted</b></p> <p>Section 4.5 of the LSP reports notes this requirement, and there are no recommended changes as a result of this submission.</p> <p><b>Noted</b></p>

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		<p><b>Pedestrian and cyclist networks</b>  DEC notes the proposed linkages between the structure plan area and Manning Lake area as depicted in the Emplacement LSP map, which illustrates shared pedestrian/cycle connections (including one bridge and one underpass). DEC recommends that planning for pedestrian and cycle trails through the structure plan area considers and is complementary to the <i>Beeliar Regional Park Management Plan 2006</i>. DEC supports the proponent's commitment to maintain connectivity for pedestrians and regional park visitors between both sides of the proposed Cockburn Coast Drive. In regard to the design of the above mentioned underpass, the proponent is requested to liaise with the Department of Fire and Emergency Services (DFES) to ensure adequate height and width specifications to allow access for fire and emergency vehicles.</p> <p><b>Site contamination</b>  Due to previous industrial land uses over a long period of time, there is considerable potential for widespread soil and/or groundwater contamination within the structure plan area. A significant number of lots are shown as Reported Contaminated Sites on DEC's Contaminated Sites Database. Of these, a number are "Awaiting Classification", while others are listed as "Possibly Contaminated - Investigation Required".</p> <p>DEC notes that GHD Pty Ltd has undertaken a Preliminary Assessment of all lots within the Emplacement and Robb Jetty LSP areas. Further to the assessment, site investigations are required for some lots within the LSP areas (e.g. Lot 2108 Bennett Avenue, Lot 123 Cockburn Road and Lot 103 Emplacement Crescent). Investigations for soil and groundwater contamination will therefore need to be carried out in accordance with DEC's <i>Contaminated Sites Management Series</i> guidelines. Where these investigations identify soil and/or groundwater contamination that requires remediation to render the site suitable for the proposed use, such remediation (including validation of remediation) will need to be completed to the specifications and satisfaction of DEC's Contaminated Sites Branch, and in accordance with the requirements of the <i>Contaminated Sites Act 2003</i>. DEC's Contaminated Sites Branch should be contacted regarding any site contamination queries.</p> <p><b>Drainage management</b></p>	<p>The Local Water Management Strategy (LWMS) does not propose any stormwater discharge into Beeliar Regional Reserve.</p> <p><b>Noted</b>  The Department of Water has provided comments on the LWMS.</p> <p><b>Noted</b>  The proposed Emplacement LSP has been prepared in accordance with <i>State Planning Policy 5.4</i> and requires sensitive development in proximity to Cockburn Road and the freight rail to comply with the requirements of SPP5.4.</p> <p><b>Supported</b>  As per the <i>Planning for Bushfire Protection Guidelines</i> because the Emplacement LSP proposes development within 100m of vegetation which may be considered a 'moderate to extreme' bush fire hazard the LSP should be supported by a bush fire hazard assessment. It is therefore recommended that a bush fire hazard assessment is prepared and the LSP be amended to consider the outcomes of the assessment.</p>

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		<p>In planning for future subdivisions, it should be noted that no drainage infrastructure is to be placed within the adjoining Beeliar Regional Park, nor is there to be any direct discharge of drainage waters (including road drainage) into the regional park. This requirement is particularly relevant in relation to the possible future design and construction of proposed Cockburn Coast Drive.</p> <p>DEC notes that Local Water Management Strategies (LWMS) have been prepared for the Emplacement and Robb Jetty LSP areas. These should be submitted to the Department of Water for review and approval.</p> <p><b>Noise management</b> It is noted future development within the Emplacement LSP area is proposed against Cockburn Coast Drive, which could result in potential noise impacts from road traffic. It is noted that a Road Noise Assessment (Herring Storer Acoustics 2011) has been prepared for the Cockburn Coast Project, which incorporates the Emplacement and Robb Jetty LSP areas; DEC has not reviewed this document. However, there is a need to comply with <i>WAPC State Planning Policy 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning (2009)</i>. The Draft <i>EPA Guidance Statement No.14 - Road and Rail Transportation Noise (1998)</i> may also be of assistance.</p> <p><b>Fire management</b> Necessary fire management requirements should be provided for within the structure plan areas, in accordance with the (Interim) <i>Planning for Bushfire Protection Guidelines</i> (Edition 2- Western Australian Planning Commission and Fire and Emergency Services Authority, May 2010) and any other relevant policies, and on the advice of DFES. DEC supports having a perimeter road between residential development and POS, for reasons of public safety, protection of bushland within the POS and fire safety for residents. The perimeter road reserve should accommodate all road, dual use path/footpath and drainage infrastructure.</p>	<p><b>Supported</b> It is acknowledged that the Flora and Vegetation Survey contained within the Ecological Assessment were not undertaken in accordance with EPA Guidance Statement 51. It is therefore recommended that a spring flora and vegetation survey be undertaken prior to any subdivision or development (involving works to the land), and that affected landowners be advised of this requirement.</p> <p><b>Not supported</b> The proposed areas of POS within the Emplacement LSP are consistent with the CCDSP Part 2. Vegetation within the green POS links will be retained where possible to provide a physical and ecological link between the foreshore and Beeliar Regional Park. While it may be possible for some</p>



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		<p><b>Further Comments received 10 January 2013:</b></p> <p>I refer to your letter dated 19 November 2012, referring the above proposed local structure plans to the Department of Environment and Conservation (DEC) for comment. DEC reviewed the documents for the Robb Jetty and Emplacement local structure plans and submitted a submission that related to both areas on 21 December 2012. At the time of DEC submitting the submission, a copy of the ecological assessment report (Appendix C) for the Emplacement local structure plan (LSP) area had not been provided. DEC has since received and reviewed the ecological assessment report (<i>Report for Hilltop/Emplacement Crescent-Ecological Assessment June 2012</i>) prepared by GHD and provides the following additional advice on flora and fauna management for your consideration.</p> <p><b>Flora management</b>  DEC has reviewed the document <i>Report for Hilltop/Emplacement Crescent-Ecological Assessment</i> (GHD 2012) and notes the field study was conducted on 16 May 2012, which is not considered the optimal time for flora surveys within the Swan Coastal Plain Bioregion; therefore not considered to be conducted in accordance with Environmental Protection Authority's (EPA's) <i>Guidance Statement 51 - Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia</i>. GHD (2012) outlines that there are patches of native vegetation in good condition (approximately 2.96ha of vegetation in total) and the identified vegetation type 1 located on the eastern side of the project site (Emplacement LSP area) has similarities to DEC-listed threatened ecological community [SCP 26a]. DEC considers that to accurately determine the floristic community types present at the project site, plots need to be established and scored (typically spring and late spring), and data analysed using appropriate statistical techniques. An appropriately timed flora survey in accordance with <i>Guidance Statement 51</i> with methodology consistent with Gibson <i>et al.</i> (1994) is required to determine the</p>	<p>vegetation to be retained within POS, the key function of the proposed POS is to provide a variety of recreational functions for residents and visitors, cognisant of the fact that it will be a high density environment. The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense and diverse urban environment with high levels of accessibility.</p> <p><b>Noted</b>  Obligations under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) are noted.</p>

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		<p>presence of priority and/or threatened ecological communities within the project site.</p> <p>In addition, GHD (2012) indicates that rare flora (e.g. <i>Caladenia huegefit</i>) and priority flora (e.g. <i>Dodonaea hackettiana</i>) are likely to occur within the Emplacement LSP area. Therefore, DEC recommends that another flora and vegetation survey of all potentially affected areas of native vegetation be conducted by an environmental consultant, in accordance with <i>Guidance Statement 51</i>. The survey should determine the presence of priority flora, rare flora or other significant flora. If such flora and vegetation is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts.</p> <p>Regardless of the outcomes of the above recommended flora and vegetation survey for the Emplacement LSP area, DEC recommends that areas of native vegetation in good or better condition within the LSP area be retained and incorporated into future public open space (POS).</p> <p><b>Fauna management</b></p> <p>The Cockburn Coast District Structure Plan (DSP) area is known to serve as roosting and foraging habitat for the threatened Carnaby's cockatoo (<i>Calyptrorhynchus latirostris</i>). In addition, the Stage 1 <i>Flora and Vegetation Assessment (ENV Australia, 2008)</i> indicates that <i>Lomandra maritima</i> has been identified within the Cockburn Coast DSP area. Therefore, the Emplacement LSP area may support the common and widespread <i>Lomandra maritima</i> which is suitable habitat for the threatened Graceful sun-moth (<i>Synemon gratiosa</i>). GHD (2012) states (Table 4, page 2) "<i>There is 2.96 ha of high quality Black Cockatoo foraging habitat (Banksia sessilis woodland) present within the Project Site. Clearing of the Project Site will adversely affect this foraging habitat. The 2.96 ha of high quality foraging habitat is connected to a larger strip of bushland including Manning Park, which provides foraging habitat for Black Cockatoos. The Project Site has also been mapped by the Department of Planning as potential feeding vegetation for Carnaby's Black Cockatoo on the Swan Coastal Plain (Department of Planning, 2011). Therefore, while the majority of the Project Site is degraded and borders developed areas, due to its linkage to other foraging habitat the 2.96 ha extends the available protected habitat in Beeliar Regional Park. Clearing the 2.96 ha will have an impact on the species' regional feeding resources, but it is unlikely to be critical in terms of the species long term survival. The majority of the foraging habitat is located on the limestone outcrop area in the east of the Project Site, and within a fenced industrial area. It is recommended that clearing of the habitat in these areas be minimized or avoided if possible.</i>"</p>	

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		<p>DEC concurs that clearing of high quality foraging habitat for Carnaby's cockatoo should be minimised or avoided, if possible; and recommends that it is retained and incorporated into future POS.</p> <p>DEC reiterates that both Carnaby's cockatoo and the Graceful sun-moth are protected by the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities (SEWPaC) to determine what responsibilities they have under the EPBC Act.</p>	
26.	Water Corporation PO Box 100 Leederville WA 6902	<p><b>Water Corporation Bennett Avenue Waste Water Pump Station (WWPS) odour buffer</b></p> <p>Section 4.5 of the RJLSP deals with industrial buffers in general and more specifically with the Bennett Avenue WWPS. Figure 25 shows the Bennet Ave WWPS buffer being 50 metres measured from the centre point of the wet well. This 50m buffer setback is at odds with the buffer shown in the <i>Cockburn Coast District Structure Plan</i> adopted by the Western Australian Planning Commission (WAPC), which shows a 50 metre buffer from the boundary of the site. This setback also contradicts the City's <i>Cockburn Coast District Structure Plan</i>, which is consistent with the WAPC plan.</p> <p>The RJSP proposed buffer 50m setback does not take into consideration a previous determination by the Minister for Water that the 50 metre buffer is to be measured from the boundary of the WWPS site. This determination has previously been conveyed to LandCorp, the City of Cockburn and adjoining land owners both verbally and in written communications. However, some landowners adjoining the WWPS site have made separate representations to the Minister in an attempt to further reduce the odour buffer. In response, the Minister has recently instructed the Corporation to accept a reduction of the buffer from 50 metres to a 25 metre buffer measured from the <u>boundary</u> of the site.</p> <p>The RJSP report includes some explanation of the application of the Environmental Protection Authority (EPA) Guidance Statement 3. Other existing industrial land uses in the area have been acknowledged and management measures have been put in place to address buffers from these land uses measured from the cadastral <u>boundaries</u> of these properties. It is not clear why the RJSP has selectively interpreted the EPA Guidance Statement to apply a buffer measured from the centre of the Bennett Ave Pump Station wet well. This approach is prejudicial to the Corporation and does not</p>	<p><b>Supported</b></p> <p>The Odour Report that has been submitted seeks to further analyse and further refine the buffer in accordance with draft State Planning Policy 4.1 State Industrial Buffer ("SPP4.1") and the Environmental Protection Authority's Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses ("GS No.3").</p> <p>GS No. 3 notates the following impacts apply: Odour, Noise, Gas and Risk and the key agency for advice or approvals is the local government.</p> <p>The Technical Analysis submitted is predominantly concerned with the issue of odour and pays very limited attention to the issues of gas, noise and risk.</p> <p>City officers are not comfortable endorsing the technical analysis at this stage given it has not given sufficient regard to three of the four impacts this infrastructure presents. The landowners are welcome to lodge an updated technical analysis which does consider all these issues sufficiently, should they choose to apply for subdivision or development</p>

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		<p>provide any flexibility for the Corporation to utilise other parts of the site for pump station works. Measuring the 50m radius odour buffer from the centre of the wet well as being the only source of potential odour precludes the development of any additional odour emitting assets on the WWPS site in the future.</p> <p>The package of information contained on the CD accompanying the RJSP includes an odour report undertaken by a consultant on behalf of the affected landowners. This report has not been subjected to an independent assessment and appears to conclude that the odour levels from the WWPS are currently low and acceptable. This situation is primarily attributable to the success of an odour scrubbing unit that the Corporation installed at the WWPS in early 2011. The odour report and associated modelling has modelled only the current reduced odour levels and has not accounted for future rises in wastewater flows through this main pump station in the longer term. The report also assumes that future wastewater flow and odour increases at the WWPS will be attenuated by further Water Corporation investment in additional odour controls to manage odours within the proposed 50m radius. This has not been agreed or incorporated into the Corporation capital planning for this WWPS and the long-term success of the current odour scrubbing unit is yet to be evaluated for much larger flows.</p> <p><b>Infrastructure Coordination, Servicing and Staging</b> The updated servicing reports attached to both LSPs are noted. Further discussions and arrangements will need to be made with the Corporation regarding the rationalisation and relocation of existing water and wastewater pipes through the area. However, the information and mapping provided in this report is sufficient for this stage of the planning process.</p> <p><b>Emplacement LSP – water supply planning</b> The findings of the Corporation's recent water supply planning review for the Hamilton</p>	<p>approval. In the interim it is recommended that the local structure plan be modified to reflect the boundary of a 25m buffer as measured from the property boundary, and no sensitive land uses will be deemed acceptable in this area, and it is recommended</p> <p><b>Infrastructure, Servicing and Staging</b> For Robb Jetty they have noted that upon development rationalisation and relocation of existing water and wastewater pipes through the area will be required, however the information and mapping is sufficient at this point in the planning process. We agree with this comment and further design would occur as part of the normal development process.</p> <p><b>Emplacement Crescent LSP – water supply planning</b> Water Corporation has undertaken further water supply modelling for the Hamilton Hill gravity water supply scheme. It is noted that two small R160 sites at the Eastern extremity of the LSP area appear to be affected by the Water Corporation RL33mAHD height contour. Above this level it is noted they may not be able to provide water at a sufficient head (pressure). Development of these sites will involve substantial earthworks due to the steep nature of the existing ground. Therefore, upon final design it may be that these sites are below the RL33mAHD level. Failing that, because they are high density sites and likely multi storey buildings, the design would incorporate water supply pressure boosting systems.</p>

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		<p>Hill gravity water supply scheme and the water mains upgrades relevant to the Cockburn Coast area, have largely been reflected in the LSP and servicing report. However, it should be noted that any land above 33m AHD will not be able to be served off the gravity scheme, even after the successful completion of these water mains upgrades along Forrest Rd (DN500) and Cockburn Rd (DN375). Small portions of the eastern edges of the 'R160' sites could be affected by this supply limit. Developers of land above 33m AHD will need to investigate other measures (e.g. pressure boosters) to provide adequate pressure to developments, particularly for proposed multi-storey buildings in this area. The areas above 33m AHD are shown on the ached plan.</p> <p>Please call me discuss if necessary. I will be sending the formal letter in the mail tomorrow morning.</p> <p><i>(Additional comments received):</i></p> <p>Thank you for your letter of 19 November 2012 inviting comments from the Water Corporation on the <i>Robb Jetty Local Structure Plan</i> and the <i>Emplacement Local Structure Plan</i>. The Corporation offers the following comments in addition to the advice sent to the City via e-mail on 17 December.</p> <p>The Corporation has previously provided advice to Landcorp, the City and the developer's engineering consultants regarding water and wastewater infrastructure planning for this area, and in particular in relation to the existing Bennett Avenue Waste Water Pump Station and its odour buffer. The servicing issues relevant to the Robb Jetty and Emplacement precincts are largely reflected in the LSP report and the accompanying <i>Infrastructure and Servicing Report</i> prepared by Wood and Grieve Engineering Consultants.</p> <p>Over the past 2-3 years the Water Corporation has reviewed its water and wastewater</p>	

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		<p>infrastructure planning for this area taking into account the development yields and indicative development timing for the Cockburn Coast development area. This planning may need to be further refined in consultation with individual land developers, as more detailed planning is progressed for the various development sites. Staging of water and wastewater headworks and upgrades, in particular the timing of major items such as staged extension of a water distribution main along Cockburn Rd (DN375 water main extension) and later along Forrest Rd (DN500 water main), will depend on the progression of development and water demands. The capacity and progressive upgrading of the Bennett Avenue waste water pump station should not be an impediment to the timing of the initial stages of development. The Corporation will undertake upgrades to the WWPS as required and when capital funds have been scheduled. Further comments follow in relation to some outstanding matters and issues that require further detailed consideration.</p> <p><b>Fremantle Sewer District Waste Water Pump Station No.2 - Bennett Avenue</b>  As the City would be aware, the Water Corporation owns and operates a large waste water pumping station (WWPS) at the corner of Bennett Avenue and Rollinson Road. The WWPS is a permanent facility that was built in this location at a time when the surrounding land was used for industrial purposes. The WWPS is the final receival point for wastewater generated from the Fremantle Sewer District and is therefore critical public infrastructure. The WWPS and its associated onsite, underground emergency storage tanks are potential sources of odour.</p> <p>Encroachment of incompatible land uses within close proximity to the WWPS may place residents in situations of unacceptably high odour, undermine significant investment in this infrastructure, decrease the ultimate operating capacity of the pump station, and thereby risk the achievement of the planned ultimate urban densities within the City of Fremantle and the Cockburn Coast area.</p> <p>In October 2011, in response to approaches from adjoining landowners, the Minister for Water requested the Water Corporation to limit the planned ultimate capacity of the WWPS to a maximum of 350 Litres/second, in order to avoid the need to implement a larger 150m radius odour buffer required under EPA Guidance Statement No.3 for pump stations &gt;350l/s. The implication of this is that the projected ultimate wastewater flow from the Fremantle Sewer District will not be able to be accommodated through the Bennett Avenue WWPS and alternative measures will need to be explored to deal with the long-term wastewater flows, including the possibility of diverting wastewater into neighbouring sewer catchments. The Water Corporation currently does not have any</p>	<p><b>Not supported</b>  The City is not responsible for the delivery of wastewater infrastructure and therefore it is not recommended that the City include such an item as a Developer Contribution. Pursuant to Clause 6.3.17 of the Scheme the City is responsible for any shortfall in the total cost contributions when all costs contributions have been made or accounted for. Inclusion of items that the City is not responsible for delivering means that the City is will be left liable for any shortfalls on the delivery of items outside of its control.</p> <p><b>Supported</b>  The Odour Report that has been submitted seeks to further analyse and further refine the buffer in accordance with draft State Planning Policy 4.1 State Industrial Buffer ("SPP4.1") and the Environmental Protection Authority's Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses ("GS No.3").</p> <p>GS No. 3 notates the following impacts apply: Odour, Noise, Gas and Risk and the key agency for advice or approvals is the local government. The Technical Analysis submitted is predominantly concerned with the issue of odour and pays very limited attention to the issues of gas, noise and risk.</p>

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		<p>planning in place or capital works programmed for the infrastructure that will be required to effect such a diversion. Further detailed engineering investigations will be required to explore if and how wastewater can be diverted and to determine the cost of these works. The Council is requested to include this item in the developer contributions scheme for the Cockburn Coast development area.</p> <p>The size and configuration of the required odour buffer around the Bennett Avenue WWPS has been a matter of debate for some time. The Council's last decision on the DSP and the Cockburn Coast Master Plan indicated a buffer of 50m radius measured from the WWPS site boundary, which the Corporation supported. Section 4.5 of the Robb Jetty LSP deals with industrial buffers in general and more specifically with the Bennett Avenue WWPS. Figure 25 shows the Bennett Ave WWPS buffer being measured as 50m from the centre point of the wet well. This buffer setback is at odds with the buffer shown in the <i>Cockburn Coast District Structure Plan</i> and the Masterplan.</p> <p>The 50m buffer proposed in the Robb Jetty LSP does not take into consideration a previous determination by the Minister for Water that the buffer is to be measured from the boundary of the WWPS site. The Minister's determination has previously been conveyed to LandCorp, the City of Cockburn and adjoining land owners both verbally and in writing. However, landowners adjoining the WWPS site have since made representations to the Minister for Water in an effort to further reduce the extent of the buffer. In response, the Minister has recently requested the Corporation to accept a reduction of the buffer from a 50m to 25m radius measured from the boundary of the site. The City is requested to reflect the Minister's decision and the revised odour buffer in the DSP and the Robb Jetty LSP.</p> <p>The Robb Jetty LSP report includes some explanation of the application of the Environmental Protection Authority (EPA) Guidance Statement 3 in relation to industrial buffers. It is noted that other existing industrial land uses in the Cockburn Coast area have been acknowledged and management measures have been put in place to protect these land uses with buffers measured from the cadastral boundaries of these properties. It is not clear why the LSP has selectively interpreted the EPA Guidance Statement No.3 to apply a buffer measured from the centre of the Bennett Ave Pump Station wet well. This approach is prejudicial to the Corporation and does not provide any flexibility for the Corporation to utilise other parts of the site for pump station works.</p> <p>The package of information contained on the CD accompanying the Robb Jetty LSP includes an odour report undertaken by a consultant on behalf of the affected landowners. This report has not been subjected to an independent assessment and</p>	<p>City officers are not comfortable endorsing the technical analysis at this stage given it has not given sufficient regard to three of the four impacts this infrastructure presents. The landowners are welcome to lodge an updated technical analysis which does consider all these issues sufficiently, should they choose to apply for subdivision or development approval. In the interim it is recommended that the local structure plan be modified to reflect the boundary of a 25m buffer as measured from the property boundary, and no sensitive land uses will be deemed acceptable in this area, and it is recommended</p>

No.	Name/address	Submission	Council's recommendation
		<p>appears to conclude that the odour levels from the WWPS are currently low and acceptable. This situation is primarily attributable to the success of an odour scrubbing unit that the Corporation installed at the WWPS in early 2011. The odour report and its conclusions are based on an assessment of the current reduced odour levels and has not accounted for future increases in wastewater flows through this main pump station, and hence potential increases in odour emissions in the longer term. The report also assumes that future wastewater flows and odour increases at the WWPS will be attenuated by further Water Corporation investment in additional odour controls to manage odours within the proposed 50m radius. This has not been agreed to or incorporated into the Corporation's planning for this WWPS and the long-term success of the current odour scrubbing unit is yet to be evaluated for much larger flows.</p>	
27.	<p>Adele Carles MLA Fremantle Shop 1, Queensgate Centre, William Street FREMANTLE WA 6160</p>	<p>Please accept these documents as my submission for the structure plans within the Cockburn Coast development area: Robb Jetty; and Emplacement</p> <p>My views are outlined in the two previous submissions (attached). In addition I would like to raise the matter of new evidence that has arisen in relation to rapid sea level rise in Western Australia – particularly around Perth. The recently released federal report <b>State of Australian Cities 2012</b> (<i>Major Cities Unit, Department of Infrastructure and Transport, Australian Government</i>) details disturbing evidence that sea levels are rising between 9mm and 10mm per year, while the global average is about 3mm per year (sea media release attached). This rapid rate of sea level rise has been attributed to the interaction of factors involving Perth's sinking ground levels due to excessive groundwater abstraction and the rise in sea levels due to climate change.</p> <p>The result is that low lying coastal areas around Fremantle will be subject to more intense sea level intrusion at a more rapid rate than the rest of the country. This new evidence must result in a reappraisal of coastal setbacks for the purposes of planning in the Cockburn Coast Structure Plan. The current prescribed coastal setback for planning are out-dated and will be insufficient to protect built structures from damage and inundation in the near coast areas of the structure plan.</p> <p>I submit that this is the ideal opportunity for planning authorities to review coastal setbacks for planning more generally and to adjust the specific setbacks for the Cockburn Coast structure plan specifically. I would also like to reiterate my opposition to the inclusion of a public marina at the front of the old power station due to the loss of beach it creates and because of the impacts of sea level rise in decades to come.</p>	<p>The applicant has provided a Coastal Vulnerability Assessment (CVA) with their local structure plan. The document has been prepared by an appropriately qualified person and a company who specialise in these assessments.</p> <p>The assumed sea level rise in the CVA is 0.9m to 2110. This is as per the current requirements of the Department of Planning. When the State Planning Policy 2.6 (SPP2.6) State Coastal Planning Policy was gazetted in 2003 a sea level rise of 0.38m needed to be included in assessments. Based on updated data, the Department of Planning issued a new Position Statement in 2010 to increase the sea level rise to be factored into assessments to 0.9m to 2110. In February 2012, the Department advertised a new draft SPP2.6, this reiterates the requirement for 0.9m to 2110. No advice to the contrary has been provided to the City by the Department and therefore it is prudent to apply an</p>



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		<p><i>(Attachment 1 – Media Release on Sea Level Rise – 5 Dec 2012)</i></p> <p>Fremantle Independent MP Adele Carles has demanded an urgent reassessment of the Cockburn Coast Local Structure Plan in light of new scientific evidence about sea levels rising in Perth at three times the global average.</p> <p>Disturbing new statistics from the State of Australian Cities report show readings since 1993 have indicated sea levels are rising by between 9mm and 10mm per year, while the global average is about three millimetres per year.</p> <p>Ms Carles says the new startling information renders the current plan redundant and says planners need to go back to the drawing board.</p> <p>“These new statistics are alarming and must be taken into account while planning for the development of the Cockburn Coast,” Ms Carles said.</p> <p>“Coastal setbacks may need to be increased as current planning regulations for coastal setback and sea level rise are out-dated and don’t reflect the new information that is now available.”</p> <p>“The new warnings also vindicate my opposition to a marina on this part of the coast, which is already overstretched,” Ms Carles said.</p> <p><i>(Attachment 2 - previous submission on Metropolitan Region Scheme Amendment</i></p>	<p>assumed sea level rise of 0.9m to 2110.</p> <p>Neither the Robb Jetty not the Emplacement Local Structure Plans include the power station building. Any proposals for the power station (whether with marina proposals or not) will be the subject of future applications. Council has made it very clear in its proposed town planning scheme provisions, there are a variety of issues which must be discussed should a marina (or similar) coastal feature be proposed, including environmental and social feasibility.</p> <p>Noted the submissioner has included a copy of a media release they have made. This is considered to be provided for information as background the submission above and does not warrant further response. The matter of sea level rise is discussed above.</p>

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		<p><i>1180/41 Cockburn Coast District Structure Plan Area. May 2010).</i></p> <p><b>Executive Summary</b>  This submission outlines a number of concerns and suggestions in relation to the Cockburn Coast District Structure Plan (CCDSP) as it appears in the Metropolitan Region Scheme Amendment 1180/41. Comments are also made in relation to planning issues surrounding the South Fremantle tip site and the Fremantle Chalet Village (Caravan Park). The tip site and the Fremantle Chalet village both fall within the boundaries of the CCDSP and share similar environmental problems, yet the planning considerations of the former tip site are side-stepped in the documentation of the CCDSP. The South Fremantle tip-site is regarded as being subject to separate planning measures by the City of Fremantle and the chosen planning option for the site differs from that which has been made public previously. There is clearly a need to consider sensitive and ecologically sustainable development of this locality given historic use as an industrial area with noxious industry land-use. However the need to remediate or manage contaminated land should not compromise the remaining high conservation values of other land within the structure plan area or resident's health or amenity. With a view to ensuring more sustainable outcomes for the structure plan I have provided comment on;</p> <p>The urgent need to fund dedicated infrastructure for a light rail network to service the structure plan development and alleviate congestion on Hampton Road and throughout Fremantle.</p> <p>Strong community opposition to residential development of the South Fremantle landfill site due to health and safety issues and environmental impacts.</p> <p>The need for greater setbacks between the coastline and the key residential and commercial developments of the CCDSP.</p> <p>Real consideration to be given to the relocation of the Fremantle Chalet Village permanent residents to affordable accommodation within the CCDSP that provides improved amenity and security (the 'village' is currently located on top of a medical and municipal waste dump).</p> <p>The need to integrate renewable energy systems in to the development at district scale where possible and certainly at lot scale for commercial and residential structures.</p> <p><b>Transit: Light rail vs. buses</b> I am disappointed to see that the WAPC is still pursuing the option of road-based transit in the CCDSP. The response below (from the public submissions report) indicates that while the vast majority of respondents (27:3) were encouraging the implementation of light rail to link the CCDSP to Fremantle in</p>	<p>Noted the submissioner has included a copy of a submission they have made on the Metropolitan Region Scheme amendment advertised by the Department of Planning.</p> <p>This is considered to be provided for information as background the submission above and does not warrant further response. The Department of Planning have already responded to the submissions raised as part of that amendment process.</p> <p>A copy of the submissions report on MRS Amendment 1180/41 can be found at:  <a href="http://www.planning.wa.gov.au/publications/941.asp">www.planning.wa.gov.au/publications/941.asp</a></p> <p>The submission is labelled Submission Number 26.</p>

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		<p>preference to a bus system, the WAPC has not adopted their views. <i>"It is considered that Bus Rapid Transit presents the most viable and effective public transport option in the short to medium term, in the absence of the significant Government financial commitment required to implement the alternatives suggested through the public comment period. By securing the public transport priority contiguously to Fremantle from the project area, and ensuring that appropriate transit stops are provided, the opportunity to transition to light rail in the longer term is preserved, should the technology be implemented on a broader scale"</i>.(response to submissions) Draft Cockburn Coast District Structure Plan - Public Submissions Report - August 2009 p.9</p> <p>The argument made is that the bus transit system is cheaper than light rail in the short to medium-term. This is qualified by comments that such viability only occurs in the absence of 'the significant Government financial commitment' that would be required to implement the clearly preferred choice of respondents – which is light rail. Long-term sustainability assessment should be considered in this case and a comparative assessment considered. An electrified light rail system has the benefit of reduced running, maintenance and replacement costs when compared to gas/diesel buses even if the capital investment on infrastructure is considered. It is also clear that light rail can source renewable energy based electricity to reduce or eliminate its inherent carbon footprint unlike gas/diesel buses which will rely on external offsets in order to approach carbon neutrality. If a light rail line were configured parallel to coastal views it would prove attractive to tourists in its own right and increase patronage. It is difficult to see a bus system achieving the same status. The cost for implementing light rail has been estimated at \$15 million per kilometre in high density urban environments (Ludlam 2010), although the majority of track would be installed in a low-constraint environment (the CCDSP itself) until it reached the developed outskirts of Fremantle. Depending on alignment the track may extend 5-7 kilometres with a total cost of \$75-100 million plus rolling stock and maintenance. Light rail vehicle costs are around \$3 million per vehicle and a maintenance facility of around \$4 million.</p> <p>However, construction costs vary dramatically depending on the environment (tunnelling, gradients, dense urban development etc) and many cost assessments from other states and countries are less than those quoted in the CCDSP (Ludlam 2010). Importantly the cost/revenue ratio decreases dramatically with increased patronage and at maximum capacity the light rail system is highly cost effective, carbon efficient and has high rates of congestion reduction. DPI (2008) has acknowledged the superiority of light rail in this regard over buses and also note that light rail gives a sense of <i>permanence</i> to developers who are more likely to invest if government has dedicated capital and infrastructure to a long-term transit system (buses lack this permanence). They also acknowledge that the <i>scale</i> of the transit project can have a significant <i>place-</i></p>	

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		<p><i>making ability</i> as in my earlier comments on tourist potential. In DPI's view "<i>Large-scale (transit) projects with considerable government investment are more likely to generate development/redevelopment opportunities</i>". DPI also acknowledge that buses in Western Australia suffer from stigmatisation (the public view them as uncomfortable or inconvenient) and that this would be a barrier to uptake unless specific marketing plans were put in place at considerable expense. Putting aside the cost merits of buses vs. light rail it is clear that Hampton Road will reach unacceptable levels of congestion in the near future. Indeed the WAPC note in its transport analysis that Hampton Road;</p> <p><i>"... is predicted to be congested, regardless of whether the DSP area is developed or not. Paramics modelling undertaken for Scenario 1 (two lanes for general traffic plus kerbside transit lanes) without any DSP traffic predicts volumes of 32 100 vpd by 2031. The predicted congestion on Hampton Road places an increased emphasis on the need for transit priority in the coastal corridor."</i> (DPI 2008, p.15) Given this assessment by the DPI it is increasingly apparent that any transit option should avoid road use as it will inevitably lead to further congestion.</p> <p><b>Recommendation 1:</b> It is clear there is a need for a light-rail service for the Cockburn DSP to be funded at the next state budget. Although provision is made in the plans for a light rail reserve, the emphasis in the most recent draft CCDSP appears to be on road-based <i>bus</i> transport. I would recommend that the State Government develop an infrastructure investment plan for long term light rail implementation in the CCDSP with a view to extension into surrounding suburbs at a later time.</p> <p><b>Recommendation 2:</b> Traffic congestion on Hampton Road has reached critical levels and with anticipated population increases resulting from the CCDSP, any increase in road based transport (including bus services) will prove unmanageable in the short to medium term. I recommend against road based transit systems as they increase congestion and represent an opportunity cost and investment disincentive for future light rail infrastructure.</p> <p><b>Recommendation 3:</b> Light rail integration with the broader regional transport network is the most sensible and sustainable option to connect Fremantle with the CCDSP development and the suburbs beyond. The state government should establish a transit working group to integrate the heavy rail line at Fremantle station with light rail infrastructure from the CCDSP.</p> <p><b>South Fremantle landfill</b> There is virtually no community support for the development of residential dwellings on</p>	

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		<p>the South Fremantle landfill site. It is noted that the City of Fremantle has been permitted to undertake the planning activities for this site even though it falls within the boundaries of the CCDSP. The City of Fremantle planning process has been augmented by a Stakeholder Advisory Group ('the Group') which includes representation from the community adjacent to the landfill. I was a member of this Group as the spokesperson for the South Fremantle/Hamilton Hill Residents' Assoc Inc. Although this Group has been in abeyance recently, my recollection is that there was no clear consensus on whether Option A (which includes netball courts and a council depot) or Option B (which included more housing) was preferable. I recall that the community representatives on the Group, including myself preferred Option A, whilst the developer representatives preferred Option B, creating an impasse.</p> <p>However, the DSP document available online at DPI states <i>"The structure plan reflects Option B as determined through the advisory group process."</i> I met with the City of Fremantle last month about this matter and I understand that a further meeting of this Group is to be convened with a view to reconciling this and to moving forward. The lack of community support for residential development at the tip site is directly related to the hazards associated with any potential remediation and redevelopment of the site. Historical and anecdotal records confirm that a range of hazardous waste materials are buried within the site and that these include PCBs, quarantine waste, municipal waste, medical waste, sullage and ordnance. There are also serious ongoing issues associated with uncontrolled methane release from the landfill, within the landfill and under the adjoining Chalet village site. Referenced details of the site history and contamination have already been presented to the WAPC in my original submission of the South Fremantle/Hamilton Hill Residents' Association Inc in September 2008 which attached a lengthy scientific report by Kelly Duckworth (Duckworth 2008). I ask that this original submission be included with this current submission.</p> <p>Contaminated site remediation in Western Australia is still in its infancy in terms of methodology and public health protection. The primary means of remediation in WA are dig and dump approaches with poor dust control, no vapour control and inadequate air monitoring techniques. The South Fremantle community were subjected to the remediation of the former lead smelter site in 2005. I represented this community in the Supreme Court in which we attempted to have the WA Government adopt best practice remediation by removing all risks to local residents and beach users. We requested that the Health Department and the Department of Environment and Conservation implement a requirement that the hazardous remediation activity be conducted in an enclosure to prevent the release of lead contamination across Fremantle. Large tent like enclosures operating under negative pressure are used in the US and Europe for</p>	

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		<p>remediation of sites containing harmful dust and vapours in proximity to local communities. Unfortunately the responsible government agencies in WA refused to take this preventative action and approved a plan which did not remove the risk of lead dust being released over South Fremantle. The departments gave public assurances that nothing would go wrong and site specific risk assessments that generated 'tolerable' levels of contaminants.</p> <p>These assessments did not take into account the special sensitivities of the elderly, infants and pregnant women. They also failed to account for the accumulative and synergistic impacts of the hazardous chemicals released and ignore the pre-existing body burdens of likely receptors. Ultimately many families with young children, including mine, made the difficult decision to leave our homes voluntarily while the developer conducted its remediation over an 18 month period. Some families never returned. The unfortunate legacy of this remediation has left a scar on our community.</p> <p>I have relayed this story in the hope that the Government acknowledges that the community adjacent to this tip site has already suffered loss and dislocation due to the recent lead remediation at South Beach. We do not want to find ourselves in this situation again. Specifically we do not want to leave our homes again. Given the high degree of hazard associated with the waste fill at the South Fremantle landfill and the inability of contractors and government agencies to adequately protect local residents, I maintain my strong opposition to residential development of the South Fremantle landfill site. I support the recommendations of the 2008 South Fremantle/Hamilton Hill Residents' Association Inc with respect to this issue and reiterate those recommendations;</p> <p><b>Recommendation 1:</b> Residential redevelopment of the South Fremantle Landfill Site should not be approved due to the many risks associated with the hazardous waste and emission of landfill gases from the site.</p> <p><b>Recommendation 2:</b> A 500m buffer zone, which precludes further residential redevelopment, should be applied around the South Fremantle Landfill Site, in accordance with Victorian EPA guidance statements.</p> <p><b>Recommendation 3:</b> Regular monitoring for landfill gas at the South Fremantle Landfill Site, surrounding residential areas, and commercial areas, already existing within the 500m buffer, should be implemented as a matter of urgency, and in accordance with the highest international standards.</p>	

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		<p><b>Recommendation 4:</b> Comprehensive soil and groundwater testing should be undertaken in the Plan area, to support well informed redevelopment strategies.</p> <p><b>Fremantle Chalet Village</b>  The Fremantle Chalet Village has an uncertain future. It was established in the 1980s as a temporary caravan park to accommodate an influx of temporary visitors to Fremantle during the America's Cup yacht series. In following years, approval was given by the City of Fremantle to integrate long term residents in 'park homes' at the site. Throughout this period little or no regard was given to the health implications of long-term tenancy on a landfill site where waste is regularly exposed during site works and gardening. Methane levels in soil at the site have also been found to be very high at times, raising concerns about asphyxiation risk and explosions. Both the South Fremantle landfill and the Chalet Village share similar problems in terms of waste fill, methane release, inadequate management and remediation.</p> <p>It is doubtful that government authorities would ever again allow a situation where residents were permitted to live for long periods on an unremediated landfill site. This brings into question the future of the residents currently occupying that site. Many long-term residents are elderly pensioners who have invested a large portion of their capital into the 'park homes' that occupy the site. Due to current debate over legislation affecting these types of caravan parks and dwellings, many owners have found themselves in a position where they cannot sell their homes and cannot relocate. The financial situation of many long term residents has been seriously affected as is their security of tenure. The Fremantle Chalet Village requires remediation which cannot be undertaken with the current resident's in-situ. The future development of the site and the intentions of the current owner are not clear, but it appears that the current land-use will change under the CCDSP. Either remediation or re-development will require current long-term residents to relocate.</p> <p>On a positive note there may be an opportunity for the State Government to assist these long-term residents through a relocation program within the CCDSP. The Government has committed to a minimum of 20% affordable housing within the CCDSP which should provide an opportunity to relocate those long term Chalet Village residents who would otherwise have extremely limited options to seek alternative accommodation in close proximity with similar amenity. Many of these residents have family and support networks in adjoining suburbs and cannot afford to move from their current accommodation into surrounding communities. The Government has not yet responded to the Economics and Industry Standing Committee report titled <i>"Provision, Use and Regulation of Caravan Parks (and Camping Grounds) in Western Australia"</i> which</p>	

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		<p>raises many of the issues that disadvantage long-term caravan park residents. However, the Caravan Park/Park Homes Interagency Working Group has been established with a Memorandum of Understanding to assist displaced residents resulting from caravan park closures. The agencies include; Department of Commerce, Department for Communities , Department of Housing &amp; Department of Planning</p> <p><b>Recommendation 1:</b> It would be commendable if the Working Group, in collaboration with the long-term Chalet Village residents could develop a resolution to their current predicament based around secure, affordable housing/accommodation within the CCDSP.</p> <p><b>Coastal setback</b> All development should be setback at least behind the freight rail reserve and Robb Road to protect against the risk of inundation from predicted rising sea levels. The threat of rising sea levels persuaded the Victorian Civil and Administrative Tribunal to refuse a housing development within 1km of the coast recently.</p> <p>Many scientists have predicted sea levels could rise by as much as 2 metres by the end of the century (Allison et al. 2009). Some scientists have predicted the rise could be even greater. This raises the issue of State Government liability. If the precautionary principal were invoked in these circumstances, the State Government would place a moratorium on any further coastal developments within 1km of the coast depending on the slope of the land and potential for inundation.</p> <p><b>Recommendation 1:</b> All development should be setback at least behind the freight rail reserve and Robb Road to prevent inundation of coastal lots.</p> <p><b>Recommendation 2:</b> If the Government accepts current sea level rise predictions, the precautionary principal should be invoked and there should be no coastal development within 1 km of the sea.</p> <p><b>Renewable Energy</b> The implementation of the CCDSP will provide a unique opportunity for government to plan for and integrate renewable energy generation at lot level for domestic and commercial developments within the structure plan. While site orientation and current electricity supply legislation will make it unlikely that district scale renewable systems can easily be implemented (such as terrestrial solar or wind farms) in the short-term, a stronger government commitment to sustainability targets for the development could see wave power and medium scale wind turbines established to</p>	



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		<p>contribute to the carbon neutrality of the project. Geothermal power should be assessed for district power generation for the CCDSP and a feasibility study conducted.</p> <p>Superior site elevation in the north-east corner of the development allows for a very good wind resource and sufficient space is available for around five or six 20kW wind turbines to be installed. This would be sufficient to power 65 six-star energy efficient households. Current electricity legislation would make it very difficult for on-site renewable energy to be used directly as the power supply for CCDSP homes and businesses.</p> <p>I would recommend that the Government consider amendments to legislation to streamline the ability of on-site renewable generation to be used for the power needs of developments such as the CCDSP. It is entirely practical for the Government to encourage (through building codes and developer agreements) the implementation of lot scale renewable energy generation. Commercial buildings could install vertical axis wind turbines and photovoltaic power generation, while households could incorporate solar hot water systems and photovoltaic power generation. There are many other sustainability initiatives that could be considered and many of these have already been outlined in LandCorp's 2009 <i>Alkimos Environmental Sustainability Initiatives Report</i>. All of these initiatives should be considered in the context of the CCDSP.</p> <p><b>Recommendation 1:</b> Assess and implement district scale renewable energy generation for the CCDSP where feasible. Page 10</p> <p><b>Recommendation 2:</b> Require design criteria for domestic and commercial buildings within the CCDSP that maximise passive energy savings and active renewable energy generation wherever possible.</p> <p><b>Recommendation 3:</b> Review legislation to make it easier for larger scale renewable energy generation to be dedicated to local developments to create a level of measurable energy autonomy and carbon footprint reduction for developments such as the CCDSP.</p> <p><i>(Attachment 3 - previous submission on proposed Scheme Amendment No. 89 rezoning the area from Industry to Development zone and Draft Cockburn Coast District Structure Plan Part 2 - November 2011)</i></p> <p><b>Executive Summary</b></p>	<p>Noted the submissioner has included a copy of a submission they have made on the related Town Planning Scheme amendment and District Structure Plan advertised by the City of Cockburn.</p> <p>This is considered to be provided for information as background the submission</p>

No.	Name/address	Submission	Council's recommendation
		<p>This submission outlines a number of suggestions in relation to the Cockburn Coast District Structure Plan 2 (CCDSP2) as it appears in the Proposed Scheme Amendment No. 89. Comments are also made in relation to planning issues surrounding the South Fremantle tip site and the Fremantle Chalet Village (Caravan Park). The tip site and the Fremantle Chalet Village both fall within the boundaries of the CCDSP and share similar environmental problems. The South Fremantle tip-site is regarded as being subject to separate planning measures by the City of Fremantle but I am pleased to see that at least part of the site has been integrated into the strategic planning for public transit for the new development.</p> <p>There is clearly a need to consider sensitive and ecologically sustainable development of this locality given its historic use as an industrial area with noxious industry land-use. However the need to remediate or manage contaminated land should not compromise the remaining high conservation values of other land within the structure plan area or resident's health or amenity.</p> <p>With a view to ensuring more sustainable outcomes for the structure plan I have provided comment on;</p> <ul style="list-style-type: none"> <li>• The urgent need to fund dedicated infrastructure for a light rail network to service the structure plan development and alleviate congestion on Hampton Road and throughout Fremantle.</li> <li>• Strong community opposition to residential development of the South Fremantle landfill site due to health and safety issues and environmental impacts.</li> <li>• The need for greater setbacks between the coastline and the key residential and commercial developments of the CCDSP.</li> <li>• If the owner of the Fremantle Chalet Village decides to sell this site, consideration needs to be given to the relocation of the Fremantle Chalet Village permanent residents to affordable accommodation within the CCDSP.</li> <li>• The need to integrate renewable energy systems into the development at district scale where possible and certainly at lot scale for commercial and residential structures.</li> </ul> <p><b>Transit: Light rail vs. buses</b> I was pleased to see that the WAPC has heard the widespread call for light rail to be</p>	<p>above and does not warrant further response. The City have already responded to the submissions raised as part of that earlier consultation process.</p> <p>A copy of the Submission Schedule on Town Planning Scheme Amendment 89 and the District Structure Plan can be found at:</p> <p><a href="http://www.cockburn.wa.gov.au/meetings_and_minutes">www.cockburn.wa.gov.au/meetings_and_minutes</a></p> <p>It is the attachment to Item 14.2 on the Council meeting agenda for 9 February 2012. The submission is labelled Submission Number 26.</p>

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		<p>established in the structure plan development. The location of the light rail/transit reserve along the 'spine' of the new development makes sense in that it provides for a greater passenger catchment on both sides of the line. This is a benefit that would be lost if the line were to run close to the coast allowing a catchment on one side of the track only.</p> <p>It is important that funding is made available for the early establishment of the reserve at the very beginning of the development to maximize the passenger use of light rail from the outset and to alleviate traffic congestion in the area before an estimated 10,000 new residents take to the roads exclusively in cars.</p> <p>I am concerned that there is still a focus on the use of rapid transit buses initially and then a gradual move to light rail. Any spending on new assets for the bus service will effectively drain funds that could be directed to light rail establishment. Rather than splitting the options, it would be prudent to dedicate funds and planning directly to light rail from the beginning.</p> <p>Hampton Road is already heavily congested. The light rail line will provide an effective antidote to the congestion. I was pleased to see that a proposed light rail station would be based on the site of the former South Fremantle tip site and that plans are included to examine the extension of the line through to the Fremantle train station. I have suggested a similar light rail plan to government and augmented it with a plan to establish a park and ride facility next to the station at the tip site. Capping the land with bitumen for car parking would prevent the ingress of rain water which is a major factor in the spread of groundwater contamination beneath the former tip site. In addition it would allow residents of the new development to park at the tip site and catch the light rail to Fremantle and then heavy rail beyond that into the City, via the Fremantle train station.</p> <p>This would have the effect of removing a sizeable amount of the existing and proposed traffic that clogs Hampton Road and other Fremantle streets while providing a net environmental benefit at the tip site. These are benefits that would not arise from the use of rapid transit buses. DPI (2008) has acknowledged the superiority of light rail in this regard over buses and also notes that light rail gives a sense of <i>permanence</i> to developers who are more likely to invest if government has dedicated capital and infrastructure to a long-term transit system (buses lack this permanence).</p> <p>They also acknowledge that the <i>scale</i> of the transit project can have a significant <i>place-making ability</i>. In DPI's view "<i>Large-scale (transit) projects with considerable</i></p>	

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		<p><i>government investment are more likely to generate development/redevelopment opportunities”.</i></p> <p>DPI also acknowledges that buses in Western Australia suffer from stigmatisation (the public view them as uncomfortable or inconvenient) and that this would be a barrier to uptake unless specific marketing plans were put in place at considerable expense. Putting aside the cost merits of buses vs. light rail, it is clear that Hampton Road will reach unacceptable levels of congestion in the near future. Indeed the WAPC note in its transport analysis that Hampton Road;</p> <p><i>“... is predicted to be congested, regardless of whether the DSP area is developed or not. Paramics modelling undertaken for Scenario 1 (two lanes for general traffic plus kerbside transit lanes) without any DSP traffic predicts volumes of 32 100 vpd by 2031. The predicted congestion on Hampton Road places an increased emphasis on the need for transit priority in the coastal corridor.”</i> (DPI 2008, p.15)</p> <p>Given this assessment by the DPI it is increasingly apparent that any transit option should avoid road use as it will inevitably lead to further congestion.</p> <p><b>Recommendation 1:</b> I recommend that the State Government develop an infrastructure investment plan for long term light rail implementation in the CCDSP with a view to extension into surrounding suburbs (i.e. Fremantle) in the near future.</p> <p><b>Recommendation 2:</b> Traffic congestion on Hampton Road has reached critical levels and with anticipated population increases resulting from the CCDSP, any increase in road based transport (including bus services) will prove unmanageable in the short to medium term. I recommend against road based transit systems as they increase congestion and represent an opportunity cost and investment disincentive for future light rail infrastructure.</p> <p><b>Recommendation 3:</b> Light rail integration with the broader regional transport network is the most sensible and sustainable option to connect Fremantle with the CCDSP development and the suburbs beyond. The State Government should establish a transit working group to integrate the heavy rail line at Fremantle station with light rail infrastructure from the CCDSP.</p> <p><b>South Fremantle landfill</b></p> <p>There is virtually no community support for the development of residential dwellings on the South Fremantle landfill site. It is noted that the City of Fremantle has been</p>	

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		<p>permitted to undertake the planning activities for this site even though it falls within the boundaries of the CCDSP. The City of Fremantle planning process has been augmented by a Stakeholder Advisory Group ('the Group') which includes representation from the community adjacent to the landfill.</p> <p>I was a member of this Group as the spokesperson for the South Fremantle/Hamilton Hill Residents' Association Inc. Although this Group has been in abeyance recently, my recollection is that there was no clear consensus on whether Option A (which includes netball courts and a council depot) or Option B (which included more housing) was preferable. I recall that the community representatives on the Group, including myself preferred Option A, whilst the developer representatives preferred Option B, creating an impasse.</p> <p>However, the DSP document available online at DPI states <i>"The structure plan reflects Option B as determined through the advisory group process."</i> The lack of community support for residential development at the tip site is directly related to the hazards associated with any potential remediation and redevelopment of the site. Historical and anecdotal records confirm that a range of hazardous waste materials are buried within the site and that these include PCBs, quarantine waste, municipal waste, medical waste, sullage and ordnance. There are also serious ongoing issues associated with uncontrolled methane release from the landfill, within the landfill and under the adjoining Fremantle Chalet Village site.</p> <p>Referenced details of the site history and contamination have already been presented to the WAPC in my original submission of the South Fremantle/Hamilton Hill Residents' Association Inc in September 2008 which attached a lengthy scientific report by Kelly Duckworth (Duckworth 2008). I ask that this original submission be included with this current submission.</p> <p>Contaminated site remediation in Western Australia is still in its infancy in terms of methodology and public health protection. The primary means of remediation in WA are dig and dump approaches with poor dust control, no vapour control and inadequate air monitoring techniques. The South Fremantle community were subjected to the remediation of the former lead smelter site in 2005. I represented this community in the Supreme Court in which we attempted to have the WA Government adopt best practice remediation by removing all risks to local residents and beach users. We requested that the Health Department and the Department of Environment and Conservation implement a requirement that the hazardous remediation activity be conducted in an enclosure to prevent the release of lead contamination across Fremantle. Large tent-</p>	

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		<p>like enclosures operating under negative pressure are used in the US and Europe for remediation of sites containing harmful dust and vapours in proximity to local communities.</p> <p>Unfortunately the responsible government agencies in WA refused to take this preventative action and approved a plan which did not remove the risk of lead dust being released over South Fremantle. The departments gave public assurances that nothing would go wrong and site specific risk assessments that generated 'tolerable' levels of contaminants. These assessments did not take into account the special sensitivities of the elderly, infants and pregnant women. They also failed to account for the accumulative and synergistic impacts of the hazardous chemicals released and ignored the pre-existing body burdens of likely receptors. Ultimately many families with young children, including mine, made the difficult decision to leave our homes voluntarily while the developer conducted its remediation over an 18 month period. Some families never returned. The unfortunate legacy of this remediation has left a scar on our community.</p> <p>I have relayed this story in the hope that the Government acknowledges that the community adjacent to this tip site has already suffered loss and dislocation due to the recent lead remediation at South Beach. We do not want to find ourselves in this situation again. Specifically we do not want to leave our homes again. Given the high degree of hazard associated with the waste fill at the South Fremantle landfill and the inability of contractors and government agencies to adequately protect local residents, I maintain my strong opposition to residential development of the South Fremantle landfill site.</p> <p>I support the recommendations of the 2008 South Fremantle/Hamilton Hill Residents' Association Inc with respect to this issue and reiterate those recommendations;</p> <p><b>Recommendation 1:</b> Residential redevelopment of the South Fremantle Landfill Site should not be approved due to the many risks associated with the hazardous waste and emission of landfill gases from the site.</p> <p><b>Recommendation 2:</b> A 500m buffer zone, which precludes further residential redevelopment, should be applied around the South Fremantle Landfill Site, in accordance with Victorian EPA guidance statements.</p> <p><b>Recommendation 3:</b> Regular monitoring for landfill gas at the South Fremantle Landfill Site, surrounding residential areas, and commercial areas, already existing within the</p>	

No.	Name/address	Submission	Council's recommendation
		<p>500m buffer, should be implemented as a matter of urgency, and in accordance with the highest international standards.</p> <p><b>Recommendation 4:</b> Comprehensive soil and groundwater testing should be undertaken in the Plan area, to support well informed redevelopment strategies.</p> <p><b>Recommendation 5:</b> Locate a park and ride facility on the former tip site integrated with the light rail station noted in the current plans. The bitumen capping will have a positive effect on groundwater contamination and represents best use of site with highly limited land use options.</p> <p><b>Fremantle Chalet Village</b>  The Fremantle Chalet Village has an uncertain future. It was established in the 1980s as a temporary caravan park to accommodate an influx of temporary visitors to Fremantle during the America's Cup yacht series. In following years, approval was given by the City of Fremantle to integrate long term residents in 'park homes' at the site. Throughout this period little or no regard was given to the health implications of long-term tenancy on a landfill site where waste is regularly exposed during site works and gardening. Methane levels in soil at the site have also been found to be very high at times, raising concerns about asphyxiation risk and explosions. Both the South Fremantle landfill and the Chalet Village share similar problems in terms of waste fill, methane release, inadequate management and remediation. It is doubtful that government authorities would ever again allow a situation where residents were permitted to live for long periods on an unremediated landfill site.</p> <p>This brings into question the future of the residents currently occupying that site. Many long-term residents are elderly pensioners who have invested a large portion of their capital into the 'park homes' that occupy the site. Due to current debate over legislation affecting these types of caravan parks and dwellings, many owners have found themselves in a position where they cannot sell their homes and cannot relocate. The financial situation of many long term residents has been seriously affected as is their security of tenure.</p> <p>The Fremantle Chalet Village requires remediation which cannot be undertaken with the current resident's in-situ. The future development of the site and the intentions of the current owner are not clear, but it appears that the current land-use will change under the CCDSP. Either remediation or re-development will require current long-term residents to relocate.</p>	

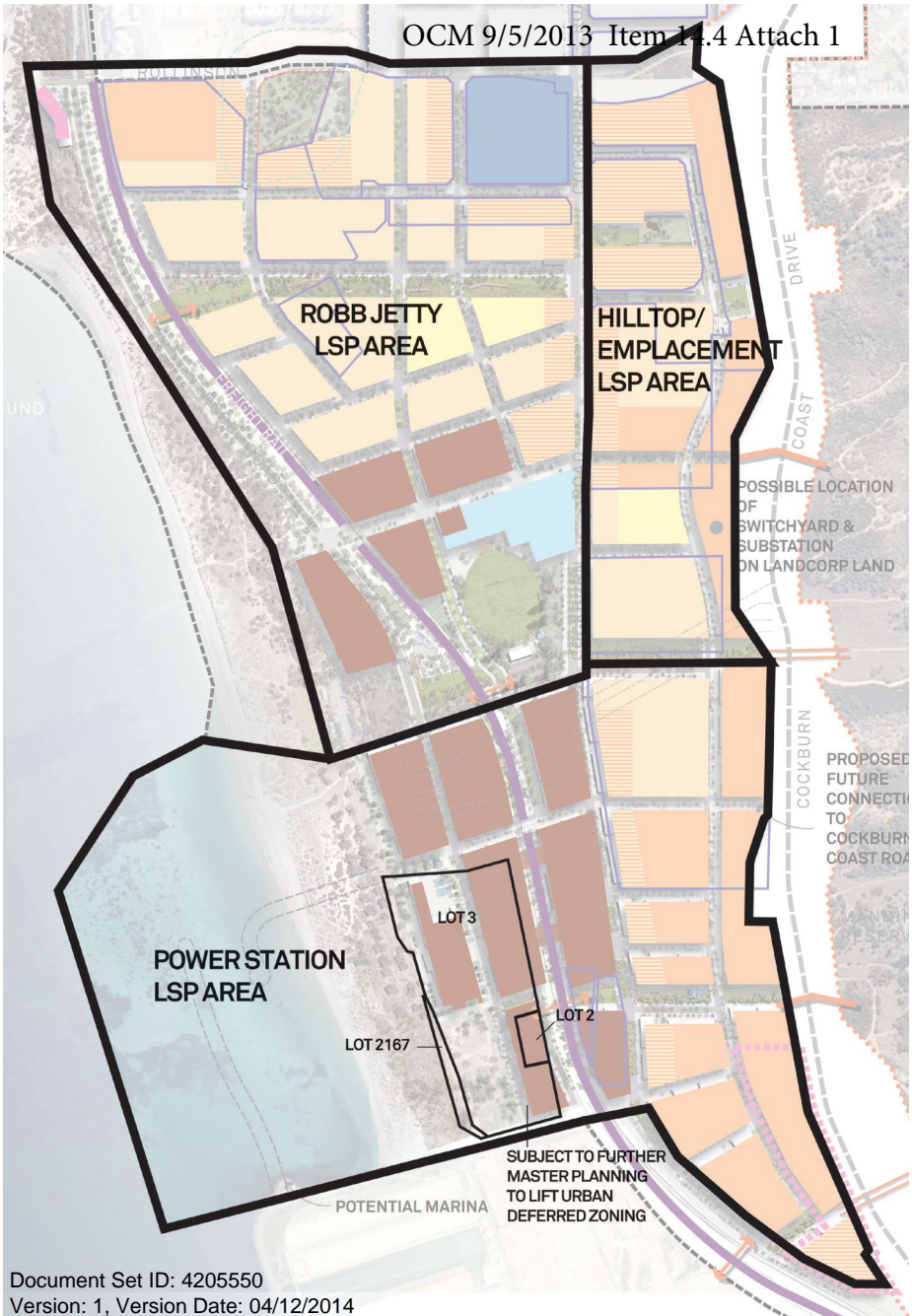
No.	Name/address	Submission	Council's recommendation
		<p>On a positive note there may be an opportunity for the State Government to assist these long-term residents through a relocation program within the CCDSP. The Government has committed to a minimum of 20% affordable housing within the CCDSP which should provide an opportunity to relocate those long term Chalet Village residents who would otherwise have extremely limited options to seek alternative accommodation in close proximity with similar amenity. Many of these residents have family and support networks in adjoining suburbs and cannot afford to move from their current accommodation into surrounding communities.</p> <p>I strongly support the relocation of the Chalet Village residents in the event that their current location is sold for development. I believe that the government should find suitable accommodation for these residents close by within the new development.</p> <p>The Caravan Park/Park Homes Interagency Working Group has been established with a Memorandum of Understanding to assist displaced residents resulting from caravan park closures. The agencies include; Department of Commerce, Department for Communities, Department of Housing &amp; Department of Planning</p> <p><b>Recommendation 1:</b> It would be commendable if the Working Group, in collaboration with the long-term Chalet Village residents could develop a resolution to their current predicament based around secure, affordable housing/accommodation within the CCDSP.</p> <p><b>Coastal setback – at least 100 metres</b>  All development should be setback at least behind the freight rail reserve and Robb Road to protect against the risk of inundation from predicted rising sea levels. The threat of rising sea levels persuaded the Victorian Civil and Administrative Tribunal to refuse a housing development within 1km of the coast recently.</p> <p>Many scientists have predicted sea levels could rise by as much as 2 metres by the end of the century (Allison et al. 2009). Some scientists have predicted the rise could be even greater. This raises the issue of State Government liability. If the precautionary principle was invoked in these circumstances, the State Government would place a moratorium on any further coastal developments within 1km of the coast depending on the slope of the land and potential for inundation.</p> <p><b>Recommendation 1:</b> All development should be setback at least behind the freight rail reserve and Robb Road to prevent inundation of coastal lots.</p> <p><b>Recommendation 2:</b> If the Government accepts current sea level rise predictions, the</p>	



No.	Name/address	Submission	Council's recommendation
		<p>precautionary principle should be invoked and there should be no coastal development within at least 100 metres of the sea.</p> <p><b>Public Marina at the Power Station</b>  I note that the current CCDSP2 has plans sketched on a map for a potential 'public' marina on the foreshore near the old South Fremantle Power Station. I have concerns that the public were not aware of this on the basis of the documentation in CCDSP1. The documentation indicates that any proposal for a public marina would be subject to a separate public consultation process to gauge community support or otherwise for this option.</p> <p>My concern is that there are very few public access beaches between Fremantle and Rockingham and those that do exist are coming under much greater pressure. If this section of the coast included an additional marina it would come at the cost of public access to the beach. People may decide that this is a fair trade off for a marina that perhaps will host public facilities and become a site of social activity. This issue should be considered very carefully and any consultation should be timely and broadly focused as the beach is used by many people along the coast.</p> <p><b>Renewable Energy</b>  The implementation of the CCDSP will provide a unique opportunity for government to plan for and integrate renewable energy generation at lot level for domestic and commercial developments within the structure plan.</p> <p>While site orientation and current electricity supply legislation will make it unlikely that district scale renewable systems can easily be implemented (such as terrestrial solar or wind farms) in the short-term, a stronger government commitment to sustainability targets for the development could see wave power and medium scale wind turbines established to contribute to the carbon neutrality of the project.</p> <p>Superior site elevation in the north-east corner of the development allows for a very good wind resource and sufficient space is available for around five or six 20kW wind turbines to be installed. This would be sufficient to power 65 six-star energy efficient households.</p> <p>Current electricity legislation would make it very difficult for on-site renewable energy to be used directly as the power supply for CCDSP homes and businesses. I would recommend that the Government consider amendments to legislation to streamline the</p>	

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		<p>ability of on-site renewable generation to be used for the power needs of developments such as the CCDSP. It is entirely practical for the Government to encourage (through building codes and developer agreements) the implementation of lot scale renewable energy generation. Commercial buildings could install vertical axis wind turbines and photovoltaic power generation, while households could incorporate solar hot water systems and photovoltaic power generation.</p> <p>There are many other sustainability initiatives that could be considered and many of these have already been outlined in LandCorp's 2009 <i>Alkimos Environmental Sustainability Initiatives Report</i>. All of these initiatives should be considered in the context of the CCDSP. Solar or wind power facilities at the tip site may be able to supplement power supplies for an electrified light rail line further reducing the carbon footprint of the development and its infrastructure.</p> <p><b>Recommendation 1:</b> Assess and implement district scale renewable energy generation for the CCDSP where feasible.</p> <p><b>Recommendation 2:</b> Require design criteria for domestic and commercial buildings within the CCDSP that maximise passive energy savings and active renewable energy generation wherever possible.</p> <p><b>Recommendation 3:</b> Review legislation to make it easier for larger scale renewable energy generation to be dedicated to local developments to create a level of measurable energy autonomy and carbon footprint reduction for developments such as the CCDSP.</p> <p><b>Recommendation 4:</b> Investigate the ability of renewable energy sources to supplement the energy needs of the light rail line.</p>	
28.	Western Power 363 Wellington Street Perth WA 6000	<p>Western Power generally only objects if alignments, easements or clearances are encroached or breached however there is no land here owned by Western Power and the Power Station is owned by Verve</p> <p>However as there are overhead power lines and/or underground cables, adjacent to or traversing the property, the following should be considered, prior to any works commencing at the above site/development/property.</p> <p>Working in proximity to Western Power Distribution Lines All work must comply with Work safe Regulation 3.64 - Guidelines for Work in the</p>	<p>Noted.</p> <p>Noted, this is a more detailed issue. Western Power should raise this at the subdivision and development stages.</p> <p>No changes are recommended based on this submission.</p>

No.	Name/address	Submission	Council's recommendation
		<p>Vicinity of Overhead Power Lines.            If any work is to breach the minimum safe working distances a Request to Work in Vicinity of Power lines form must be submitted.            For more information on this please visit the Western Power Website links below:</p> <p><a href="http://www.westernpower.com.au/safety/WorkingAroundPowerLines/working_near_electricity.html">www.westernpower.com.au/safety/WorkingAroundPowerLines/working_near_electricity.html</a></p> <p><a href="http://www.westernpower.com.au/safety/DialBeforeYouDig.html">www.westernpower.com.au/safety/DialBeforeYouDig.html</a></p> <p><a href="http://www.1100.com.au">www.1100.com.au</a></p> <p><a href="http://www.commerce.wa.gov.au/WorkSafe/">www.commerce.wa.gov.au/WorkSafe/</a></p> <p>If you require further information on our infrastructure including plans, please complete a request for Digital Data</p> <p>Please note:            Western Power must be contacted on 13 10 87, or complete the attached DQA form, if your proposed works involve:</p> <ul style="list-style-type: none"> <li>A) Any changes to existing ground levels around poles and structures.</li> <li>B) Working under overhead power lines and/or over underground cables.</li> </ul> <p>Western Power is obliged to point out that any change to the existing (power) system; if required, is the responsibility of the individual developer.</p>	



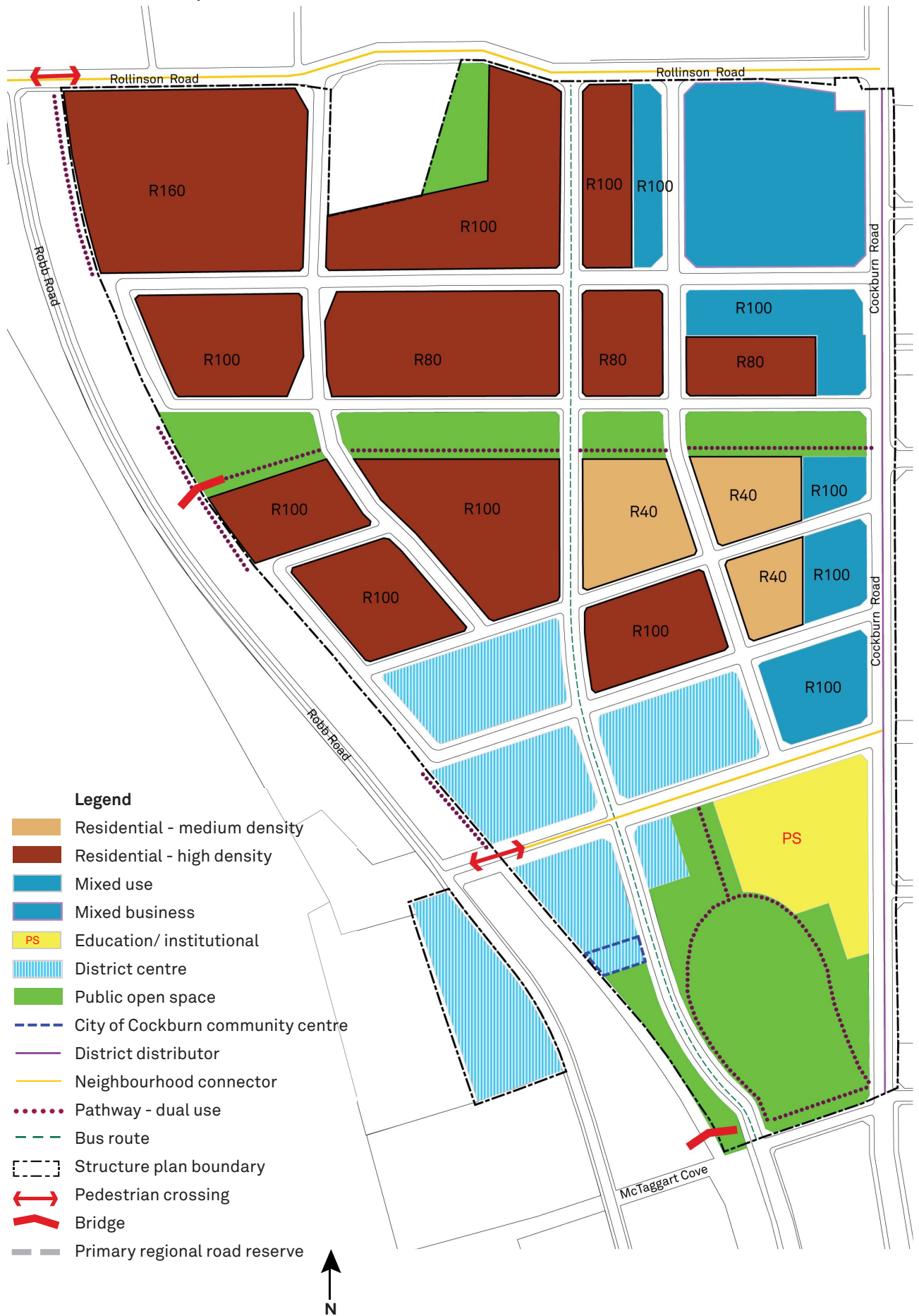


Figure 01\_Local Structure Plan Map

**SCHEDULE OF SUBMISSIONS**  
**PROPOSED LOCAL STRUCTURE PLAN WITHIN COCKBURN COAST- ROBB JETTY**

No.	Name/address	Submission	Council's recommendation
1	Telstra Forecasting & Area Planning Locked Bag 2525 Perth WA 6001	<p><b>Support</b></p> <p>Thank you for the above advice. At present, Telstra Corporation Limited has no objection. I have recorded it and look forward to further documentation as the development progresses.</p> <p>Any network extension that may be required for any development within the area concerned, the owner/developer will have to submit an application before construction is due to start to NBN Co. or the Telstra Smart Community website: <a href="http://www.telstra.com.au/smart-community/developers/">http://www.telstra.com.au/smart-community/developers/</a> .</p> <p>More information regarding NBN Co. can be found on their website <a href="http://www.nbnco.com.au/">http://www.nbnco.com.au/</a> . I add this information about NBN Co. as it is not known when services will be available from NBNCo. Telstra may provide services if NBN Co. cannot.</p> <p>Please dial 1100 (Dial before You Dig) for location of existing services.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
2.	Department of Education 151 Royal Street EAST PERTH WA 6004	<p><b>Support</b></p> <p>Thank you for your letter dated 19 November 2012 regarding the Robb Jetty and Emplacement Local Structure Plans.</p> <p>The Department of Education has reviewed the document and advises that its requirements regarding educational facilities are adequately catered for within the proposed residential developments.</p> <p>Therefore the Department has no objection to the proposed structure plans.</p>	<p><b>Noted</b></p> <p>No changes are requested/required as a result of this submission</p>
3.	Department of Water PO Box 332 MANDURAH WA 6210	<p><b>Support</b></p> <p>Thank you for the referral of the above Local Structure Plans (LSPs) received with correspondence dated 19 November 2012. The Department of Water (DoW) has reviewed the proposal and wishes to provide the following advice:</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission</p>

No.	Name/address	Submission	Council's recommendation
		<p><b>Better Urban Water Management</b></p> <p>Consistent with the Western Australian Planning Commission's (WAPC) <i>Better Urban Water Management</i> (BUWM) (WAPC, 2008) document and the policy measures outlined in <i>State Planning Policy 2. 9 Water Resources</i>, the proposed LSPs should be supported by a Local Water Management Strategy (LWMS) prior to the approval of the proposed LSPs.</p> <p>The supporting documents, <i>Robb Jetty Local Water Management Strategy</i> (GHD, November 2012) and <i>Hilltop Emplacement Local Water Management Strategy</i> (GHD, November 2012) was deemed satisfactory to the DoW as noted in correspondences dated 21 November 2012. Accordingly, the DoW has no objections to the proposed LSPs. An Urban Water Management Plan (UWMP) will be required as a condition of subdivision in the future, in accordance with BUWM 0/JAPC, 2008) and shall describe and illustrate a greater level of information for storm water design principles and infrastructure to be implemented on site.</p>	
4.	Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	<p><b>Support</b></p> <p>Thank you for your letter of 19 November seeking comment from the Department of Indigenous Affairs (DIA) with respect to the Robb Jetty Local Structure Plan (Robb Jetty LSP) and the Emplacement Local Structure Plan (Emplacement LSP). I reviewed the documents provided and offer the following comment.</p> <p>The area to which the Robb Jetty LSP applies has a slight intersection with Aboriginal heritage site DIA 3707 (Robb Jetty Camp). Accordingly, if any development associated with the Robb Jetty LSP will impact the Aboriginal heritage values of DIA 3707 (Robb Jetty Camp) then the prospective developer is encouraged to contact DIA in order to ascertain the need for prior approval under the <i>Aboriginal Heritage Act 1972</i> (AHA).</p> <p>DIA notes the existence of the Cultural Heritage Strategy and the intention to interpret the heritage values of the Robb Jetty LSP area, including DIA 3707 (Robb Jetty Camp). This is seen as a positive initiative which will assist in public understanding and long term heritage management for the area. Due to the long term association of Noongar people with DIA 3707 (Robb Jetty Camp), and the high level of significance accorded this place by the contemporary Noongar population, it is recommended that consideration is given to consulting with relevant Aboriginal people when developing interpretation for the Robb Jetty Camp.</p>	<p>Noted. The proponent of the local structure plan has been provided with the content of this submission.</p> <p>Noted</p>



No.	Name/address	Submission	Council's recommendation
		<p>DIA is unaware of any Aboriginal heritage values which intersect with the area to which the Emplacement LSP applies. It is also suggested, that prior to development occurring within the areas to which the LSPs relate, that prospective developers have their attention brought to the existence of the State Cultural Heritage Due Diligence Guidelines. The Guidelines are intended to assist prospective developers in assessing the risk that a proposed development may have on impacting Aboriginal heritage values and whether or not consent under the AHA should be sought prior to the development occurring. The guidelines can be found at:</p> <p><a href="http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHADueDiligenceGuidelines.pdf">http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHADue Diligence Guidelines.pdf</a></p>	<p>Noted. This information would be useful for other developers as well. Therefore, the City will add this information to its webpage on Heritage matters.</p> <p>No changes are requested/required as a result of this submission. However, the City's website has been updated to include a link to the Department's Guidelines.</p>
5.	State Heritage Office PO Box 7479 Cloisters Square PO WA850	<p><b>Support</b></p> <p>Thank you for providing the opportunity to provide input to the Robb Jetty and Emplacement Local Structure Plans which were correspondence received on 19 November 2012.</p> <p>The State Heritage Office is supportive of the broad objectives to conserve and retain state and local heritage places within the local structure plans. We are particularly supportive for the retention of the Rob Jetty remnants and confirmation that any future development will be in accordance with State and local heritage policies and procedures</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p>
6.	Resident Hammond Park  Details to be kept confidential	<p><b>Support</b></p> <p>I absolutely support both robb jetty and emplacement project.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission</p>
7.	Resident COOGEE  Details to be kept confidential	<p><b>Support</b></p> <p>I fully support the Robb Jetty LSP and Emplacement LSP. At the moment, I feel the entire Cockburn Coast area is not being utilised to its full potential. Currently we have the Port Coogee and South Beach redevelopment, but nothing in between.</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission</p>
8.	Dan Sheikh 9 Modong Nook SUCCESS WA 6164	<p><b>Support</b></p> <p>I absolutely support this plan for the Cockburn Coast. Most of Perth's beaches are full of McMansions and sprawl. This area has the potential to be a vibrant, residential hub on the ocean with shops, cafes, restaurants and bars. It will be</p>	<p><b>Noted</b></p> <p>It is not realistic to double the number of proposed residents at this stage. All the preliminary planning done for Cockburn</p>



No.	Name/address	Submission	Council's recommendation
		vibrant due to the resident population of the area, 10,000 residents (which I think should be double). Also higher densities combat urban sprawl. This is an area people will be willing to buy into if it is not done in a half-hearted manner.	Coast is predicated on approximately 10,000 residents.  No changes are recommended as a result of this submission
9.	Hugh Hyland 19 Buchanan Rise COOGEE WA 6166	<p><b>Support</b></p> <p>The switch-yard at the old power station needs to be moved inland as far as possible. Commuters need to be encouraged onto public transport. Robb Jetty and Emplacement will substantially add to the number of residents and local staff in the area.</p> <p>Adequate public transport is essential for Perth's future, and railways are a most essential part of this. Passenger services need to be restored along the rail line from Fremantle to Robb Jetty and on to Spearwood, then continued to Thornleigh. This would provide a quick service to Fremantle and on to the city, as well as a ring route bypassing the city and linking up with the Mandurah and Armadale lines. Most of the infrastructure is already there, with double tracks almost all the way, with only a small amount to be relaid as dual gauge each way and an even smaller amount to be duplicated. Electric trains are more efficient than buses, and are quieter than diesel engines.</p> <p>There would be ample capacity for them and goods trains on such a dual line.</p>	<p><b>Noted</b></p> <p>The proposed Emplacement Structure Plan includes an indicative switchyard /power sub-station site located towards the eastern boundary of the subject area.</p> <p><b>Not supported</b></p> <p>While it is agreed that commuters need to be encouraged onto public transport, the proposed use of the heavy rail line and restoration of services from Fremantle to Robb Jetty and on to Spearwood and Thornlie relate to the regional network and are beyond the scope of the Local Structure Plans. The option of potentially using the freight rail for passenger services was evaluated at the Cockburn Coast District Structure Plan (Part 1) stage, and was discounted due to high costs and other constraints.</p>
10.	Nandi Chinna Ommanney Street Hamilton Hill 6163	<p>To whom it may concern Regarding the Cockburn Coastal Development plans; Robb Jetty and Emplacement Structure Plan. I commend the high density aspect of the plans. High density housing connected to public transport nodes is a way of reducing the need for further land clearing on the Swan Coastal Plain and reduces dependence upon cars and road travel.</p> <p>However there are some serious concerns regarding the position of the development and the construction of new roads. It appears that many of the concerns raised in the original community consultation have not been taken into account. The retention in the plan of the construction of a new MRS primary road indicates that environmental and heritage issues have not been given sufficient consideration. Although the alignment of Cockburn Coastal Drive has been revised to reduce the impact on the ridgeline and Beeliar Regional park, the proposed road is still a major arterial road and will impact significantly on the</p>	<p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p> <p><b>Not supported</b></p> <p>The Primary Regional Road Reservation falls outside the Emplacement LSP area, and was dealt with through the district structure planning, and MRS Amendment No. 1180/41 (Cockburn Coast District Structure Plan). MRS Amendment No. 1180/41 included a revision to the alignment of the reservation</p>

No.	Name/address	Submission	Council's recommendation
		<p>bushland and have considerable detrimental consequences to the sustainability of Beeliar regional Park. The inclusion of Cockburn Coastal Drive negates the professed sustainability of the regional plan. The construction of a major arterial road that promotes the movement of heavy traffic through the area will divide the community and have a destructive impact on protected species of native fauna.</p> <p>As suggested in the original community consultation, Cockburn Road as it currently exists should be upgraded and heavy traffic diverted using existing routes such as Stock road. Freight by rail should be increased and alternative transport systems implemented. Light rail, heavy rail, and a network of bicycle paths would help to reduce dependency on cars.</p> <p>The loss of natural vegetation and the fragmentation of remaining bushland in Beeliar regional Park by building of the arterial road will significantly impact on the area. Many birds and reptile species inhabit the area and these species move through the area to feed on vegetation or to hunt. Endangered species including Carnaby Black Cockatoo, Blue Wrens, and the Black Shouldered Kites nest in the area. The Nankeen Falcon, Australian Small Eagle and Peregrine Falcon-migratory species such as the Bee-eater and priority species like the Lined Burrowing Skink and Black Striped snake are also found in the area. It is imperative that an independent environmental impact study be undertaken before this road is considered.</p> <p>The Department of Environment and Conservation has stated that, 'protected areas are essential to maintain natural and cultural diversity and to foster a sense of place and belonging and contribute to the values of our community.' The EPA claims that native vegetation needs to be protected to preserve biodiversity and as green areas to absorb carbon emissions.</p>	<p>that has enabled an additional 5.7 ha of former road reservation land to be included within the Beeliar Regional Park Management Plan. In addition, the actual road will be designed to minimise the amount of vegetation to be cleared, supported by further more detailed fauna and flora surveys. The MRS Amendment was referred to the Environmental Protection Authority, who considered the environmental impact of the road reservation, and advised that it did not require formal assessment under Part IV of the Environmental Protection Act 1986.</p> <p><b>Noted</b></p> <p>The Ecological Assessment identifies that there is remnant vegetation within the southern area of the Emplacement LSP area, some of which constitutes habitat for the Carnaby Black Cockatoo. Based on the draft Department of Sustainability, Environment, Water, Population and Communities (DSEWPaC) referral guidelines, clearing the vegetation and development of the Hilltop/Emplacement Crescent Project Site would have a direct impact on Black Cockatoo feeding habitat and given this future referral to DSEWPaC may be required (ie. prior to subdivision or development).</p> <p>The Ecological Assessment for the Emplacement LSP also notes that Vegetation type 1, which occurs on the limestone ridge on the eastern side of the Project Site, has similarities to a DEC-listed TEC, (<i>Melaleuca huegelii</i> — <i>Melaleuca acerosa</i> [currently <i>M. systema</i>] shrublands on limestone ridges). A vegetation survey in spring (when annual species are present) would be required to confirm this.</p>

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		<p>The Emplacement Precinct involves the removal of bushland in addition to the negative impact of the road.</p> <p>I am also concerned that WWII historical sites belonging to a complex military infrastructure associated with the gun emplacement sites and which are yet to be studied will be destroyed by the development.</p>	<p>It is therefore recommended that the Emplacement LSP report be amended to include the requirement for a spring survey to be conducted prior to any subdivision or development (that proposes works to the land).</p> <p>The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense urban development that reduces the need for housing on the urban fringe. The City must plan for population growth, and Directions 2031 and Beyond sets the spatial framework for how the metropolitan region will grow. It seeks to ensure urban growth is managed, and to make the most efficient use of land available.</p> <p><b>Not supported</b></p> <p>The Emplacement LSP and associated Cockburn Coast Cultural Heritage Strategy will ensure the retention and protection of the remaining gun emplacement. Specifically, the Emplacement LSP includes the gun emplacement within public open space to ensure that this important feature is not subject to development pressure. The two other gun emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the Heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated with the South Beach Battery site.</p>

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		<p>The other issue which I feel as not been given proper consideration is the horse heritage of the area, This is a living heritage which has a long and colourful history in the community. The road potentially will isolate the heritage listed property of Randwick Stables from the heritage listed beach as it will make access difficult. Horses do not go through tunnels or use overpasses. Many members of our community also support keeping the beach as an area for animal exercise and I am pleased that the structure plan supports the ongoing use of the beach for these purposes. I hope that this will not be compromised as the development unfolds.</p> <p>Insufficient consideration has been given to predicted sea level rise. Statistics from the Australian Cities report indicate that sea levels along WA's coast are rising by between 9mm and 10mm per annum, three times the global average! It is going to be an extremely costly exercise to be considering situating the development so close to the coast in this very low lying area. The Insurance council of Australia states that 'the coastal risks of storm surge, coastal erosion and gradual sea level rise are excluded by many general insurance policies in Australia.</p> <p>Consumers should ensure they are familiar with their policy and are aware of what risks the policy will not respond to' (<a href="http://www.insurancecouncil.com.au/issues-submissions/industry-in-focus/coastal-vulnerability-risks">http://www.insurancecouncil.com.au/issues-submissions/industry-in-focus/coastal-vulnerability-risks</a>). On October 30, 2012, ABC news reported that the South Gippsland Shire Council plans to cut its ties with the committee it set up to maintain seawalls along the Corner Inlet coast. By cutting its ties with the group, it can no longer be sued if homes are inundated by sea level rises. Karl Sullivan, from the Insurance Council of Australia stated that residents will be unable to insure their homes against gradual sea level rises. "If it's a single large event, generally you will find a lot of people will have cover for these things but a gradual increase in sea level, over many decades that gradually</p>	<p><b>Not supported</b> It is agreed that horse heritage is an important consideration, and that is why it has been considered from the District Structure Planning stage through to the Local Structure Plans</p> <p>The LSP and associated Cockburn Coast Cultural Heritage Strategy identify and recognise the importance and heritage value of the South Beach Horse Exercise Area. The LSP (pg 60) states 'the aim is for horse facilities to remain at McTaggart Cove to provide facilities for horses with a horse float car park, where the dunes are lower and there will be less disturbance to future residential uses, thus minimising potential land use impacts.' A key objective of the Heritage Strategy is that "South Beach should continue to be used for the horse training, a use with which it has had a long association'.</p> <p><b>Not supported</b> The assessment of the coastal vulnerability and the coastal setback to the proposed development has been completed in accordance with the 2012 draft State Coastal Planning Policy. This includes a full assessment of the potential impacts of a rise in sea level of 900 mm over the coming century. This value of sea level rise would accommodate an average rise of 9 mm/year over the coming century. This value was adopted by the Department of Transport and the Department of Planning in 2010 after careful consideration of the data and projections.</p>

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		<p>... [submerges] the house, is not really contemplated under most policies," he said. "From a residential perspective, there's really no cover available globally to protect yourself [from] a gradual sea level rise and loss of amenity of a property." These scenarios are becoming more common on the east coast of Australia, so why, with sea levels in WA set to rise at a rate three times higher than the global average, is Cockburn ploughing ahead with housing developments so close to the coast. Surely it cannot be ignorant of this kind of data? If not then may I suggest that this development is driven by short term financial gain with little thought of the cost to future generations of flood mitigation and property damage due to sea level rises.</p> <p>The other important issue that has not been duly considered is the proximity of the development to freight rail lines. With more and more freight set to be transported by rail to relieve pressure on congested roads, the freight rail line that runs through the development site needs to be given high priority over housing set close to its trajectory. It is easy to predict that people who buy residences situated along this rail line will soon be complaining of noise and pollution threats to their homes, and will be calling for sanctions to be placed on the movement of freight rail which at present moves along the line at all hours of the day and night. In conclusion I feel that there are many issues that have not been adequately addressed in the plan, in particular the ones I have mentioned in the above submission. I hope that due consideration will be given to these important issues.</p>	<p><b>Not supported</b></p> <p>With regard to noise emissions from freight trains, under Implementation Guidelines for SPP 5.4 'Road and Rail Transport Noise and Freight Considerations in Land Use Planning', where the number of movements is not defined, 24 train movements per 24 hour day should be used. However, to ensure some "future proofing" the modelling undertaken by the Noise and Vibration Strategy which forms part of the LSP has recommended a higher standard to SPP 5.4 by recommending the assessment of each development be based on that of the highest single train movement rather than an average.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p> <p>The Noise and Vibration Strategy forms an addendum to the LSP's and shows the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The design guidelines will outline the requirements for compliance with noise and vibration for land within the impact zone. The Design Guidelines will also include requirements for Notification on titles.</p>

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11.	Resident NORTH COOGEE  Details to be kept confidential	<p><b>Objection</b></p> <p>Generally, I am happy with the plan and the proposed changes made, however I am aware that there are a number of knowledgeable environmentalist that are concerned with the predictions for sea water rise, and the management plans for this rise; that rises in sea water levels have been underestimated and the management plans are inappropriate. I would like to see more research to verify the data.</p> <p>Should realistic conditions be taken into consideration, I would be supportive of the plan, pending clarification of a number of aspects as detailed below.</p> <p>I am not comfortable with restrictions on dog access hours to the beach area. Unlimited dog beach access is one of the key reasons we are building in the area; any changes to current access arrangements are not welcomed; I would like it in writing that dog access, as per horse access, will not be limited.</p>	<p>The applicant has provided a Coastal Vulnerability Assessment (CVA) with their local structure plan. The document has been prepared by an appropriately qualified person and a company who specialise in these assessments.</p> <p>The assumed sea level rise in the CVA is 0.9m to 2110. This is as per the current requirements of the Department of Planning. When the State Planning Policy 2.6 (SPP2.6) State Coastal Planning Policy was gazetted in 2003 a sea level rise of 0.38m needed to be included in assessments. Based on updated data, the Department of Planning issued a new Position Statement in 2010 to increase the sea level rise to be factored into assessments to 0.9m to 2110. In February 2012, the Department advertised a new draft SPP2.6, this reiterates the requirement for 0.9m to 2110. No advice to the contrary has been provided to the City by the Department and therefore it is prudent to apply an assumed sea level rise of 0.9m to 2110.</p> <p>To clarify, there are already existing time limitations on horse access. It is not realistic to expect there will be no changes to either dog or horse access over time.</p> <p>The broader Perth Metropolitan Area is facing growth of half a million people over the next two decades. Within the City of Cockburn, it is expected the population will grow by approximately 30,000 people in that time. This development will be able to</p>

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		<p>Additionally, related to specific details within the report, clarification is required on the following:</p> <p>1. Page 74 shows you can expect above 8 stories for High Rise apartments in a defined zone. Page 75 shows High Rise density is allowed on land zoned R160. Page 76 map indicates that the R160 zone extends for the whole land parcel rather than the small area on the northwest proposed for high density development. I would like this lot to be split into two zonings - R160 for the smaller area of the lot; R100 for the remaining area to more accurately reflect the building types proposed. This ensures that building types will be developed as defined within the plan. Currently there is some potential for variation which I am not comfortable with, given our proximity to the lot in question.</p> <p>2. Page 82 (5.5.1) says there will be 3 main access streets in to Cockburn Coast</p>	<p>provide for 10,000 people. This growth will place additional pressure onto the CY O'Connor Beach.</p> <p>The current extent of the Dog Exercise Area is nearly two kilometres in length. The Coastal Vulnerability Assessment indicates the area just south of the Point Catherine groyne (in line with Rollinson Rd) is likely to erode over time and is not expected to remain as a continuous sand beach in the longer term.</p> <p>The beach is also important historically given the long term use of this beach to exercise horses. It should also be remembered that while some people have no issue with dogs being on the beach, there are people who do and want access to beaches where there is no dog access.</p> <p>Council must be cognisant of all of these issues and the need to balance expectations. It is not possible to provide an 'in writing' guarantee that use of the beach will be unchanged over time.</p> <p>The section of the document referred to is Part 2 which contains explanatory information. It needs to be read in conjunction with Part 1 of the document which contains the statutory provisions. Within this section is a Building Height Plan (Figure 3) which provides height limitations. These are 6-8 storeys in a small section of the northwest corner and the remainder of the northern area being limited to 3-5 storeys. This should alleviate the concern expressed.</p> <p>On speaking to the submissioner, it was noted the difficulty in defining the area</p>

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		<p>being Rollinson, McTaggart Cove, and Main St. Page 82 (5.5.2) says the possible "Sections through Main St" shown in Figures 34 and 35 "would be expected for Rollinson Road". Page 84 shows 4 stories in Figure 34 and 5 stories in Figure 35, both sides of Main St therefore this could be expected for Rollinson Rd as well. Our block backs onto houses located on Rollinson Rd. Current zoning in these areas only allows 3 stories. I would like figures developed to reflect that Rollinson Rd in some areas will only have lower density living. At this stage I am not comfortable with the portrayed idea that 4 - 5 story dwelling would be an acceptable development immediately adjacent to our residence.</p>	<p>properly – it is suggested this can be addressed by requiring the maps to all be to scale and therefore less open to variation through interpretation.</p> <p>The Robb Jetty Local Structure Plan will not supersede the requirements already in place in the adjacent South Beach Local Structure Plan, north of Rollinson Rd. However, it is understood the perception that may be created by the discussion and cross sections in this document. A change to this will be required to make it clear the cross sections do not remove or change the current development standards within the South Beach development.</p> <p>Based on this submission, the Movement Network section of the document will be required to be clarified as discussed further above. Also the maps included will be required to be updated to be to scale.</p> <p>No other changes are recommended based on this submission.</p>
12.	N S McNally PO BOX 1000 CANNING BRIDGE WA 6153	<p><b>Objection</b></p> <p><i>With respect, the Cockburn Coast Plan looks as if It has been drafted with no proper vision whatsoever into the future.</i></p> <p>The following notes should be considered seriously before any of the current proposals are adopted:</p> <p><b>Contamination</b></p> <p>The majority of the land involved in the proposal is seriously contaminated. The entirety of the land should be subjected to a well-planned decontamination procedure so that a fragmented approach to the clean up does not occur. The decontamination plan should also include the land in the South Fremantle Landfill Site within the boundaries of the City of Fremantle. Decontamination of the subject Cockburn Coast land without a parallel consideration of the Fremantle Landfill site</p>	<p><b>Not Supported</b></p> <p>The City has no ability under the <i>Contaminated Sites Act 2003</i> to mandate the landowners to coordinate the decontamination of multiple sites.</p> <p>The Emplacement Local Structure Plan is</p>



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		<p>will seriously impact on the cost of processing the landfill site in the future. If an overall decontamination program for all of the land under consideration for development is not planned and implemented as a single operation (over time) then the economic viability of the future development of some of the land will be dramatically affected. The effect of this may be that the proposed development will suffer from lack of coordination which may result in the overall project not taking ten to fifteen years but more like forty or fifty years.</p> <p><b>Building Height Plan</b> The proposed building height plan should be scrapped completely. There should be no height restrictions. Projects should be assessed on a performance based criteria that assesses the overall height of proposal based on what the proposal contributes to the amenity of the area. Other design criteria such as environmental benefits, sustainability etc. etc. affordable housing ratios, etc. Can be associated with height allowances and increases and so on. The overall development of the area will progress as a dynamic development and result in a much more appealing built environment than what can be expected from the proposed homogenous ~ boring development parameters proposed in the current Cockburn Coast Plan.</p> <p><b>Residential Zone</b> There is too much emphasis on residential areas (<i>on land seriously contaminated with lead.</i>) The whole of the area should have a blanket zone allowing mixed business, commercial, residential projects. Leave the vision to the architects who should have a blank sheet to work with - not a Planning 101 TPS that shrieks of</p>	<p>supported by a Contaminated Sites Study (Appendix H). The study includes a preliminary assessment of all lots within the LSP which identifies known and suspected contaminated.</p> <p>By identifying known and suspected contamination sites and making this information publically available the Contaminated Sites Study will aid adjoining landowners to work with each other when undertaking decontamination.</p> <p><b>Not supported</b> The application of building height control is a long standing and well established planning convention. Building height controls are driven by design considerations including over shadowing, protection of vistas and important view lines and creating a consistent built form character. In addition, it is noted that proposed building heights have been a recurring theme of interest to the wider community, and inclusion of a building height plan provides a mechanism to address these concerns and provide a level of</p> <p>The building height controls outlined in the Emplacement LSP and the draft Design Guidelines for Emplacement and Robb Jetty Precincts are performance base. Variations to height will be permitted when various design criteria are met which consider urban character, streetscape amenity and overshadowing.</p> <p><b>Not supported</b> Concentrating commercial uses in certain areas like adjacent to Cockburn Drive and in Robb Jetty District Centre promotes the creation of lively nodes of activity. Cockburn</p>

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		<p>dullness. An openness of planning requirements will attract a much wider variety of developers with a far greater range of plans and visions than that which might result from the current proposals.</p> <p><b>Major City Centre Potential</b>  The overall area has the potential to become a major city centre area with hotels, multi-storey office and residential buildings, substantial retail complexes along with significant social and entertainment facilities. The current plan strangles the potential opportunity of the area. Flexibility in project proposals is critical to ensuring the old power station building is revamped and retained. The old building (very seriously contaminated along with the adjoining switch station) might then be connected directly to a major marina complex built for the use of the people of the region - not just a select few who happen to reside nearby. Think big!</p>	<p>Coast is not expected to accommodate a significant amount of commercial floorspace, due to its limited population demand catchment. This makes concentration of commercial floorspace more important. Commercial development also benefits co-location by attracting clients/shoppers who are looking to satisfy multiple needs. Commercial uses adjacent to residential uses can also create amenity issues which are more easily planned and a designed for in certain identified locations. The local structure plan, design guidelines and Development Area Scheme provisions are considered to provide a unique planning framework that has a level of flexibility so as not to stifle innovation, while providing a level of certainty for landowners and the community.</p> <p><b>Noted</b>  The City supports the development of Cockburn Coast to its maximum potential with significant commercial and entertainment uses in a compact high density urban form. The project has the potential to accommodate 10,000 people in 5,000 dwellings with supporting employment and retail opportunities. The entire project combined which includes the South Fremantle Power Station in a third LSP area allows for the project to become a key metropolitan sub-regional centre.</p> <p>The Emplacement LSP provides for significant development in comparison to metropolitan Perth outside of the CBD. The South Fremantle Power Station is not included in the Emplacement LSP. It will be part of separately prepared masterplan and LSP which will be lodged with the City and</p>

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		<p><b>Linkage with City of Fremantle</b> The area has the potential to become the major business centre south of Fremantle. The pressure to develop within the centre of the old Fremantle Town area will be alleviated. The two areas will complement each other over future years with Cockburn Coast being the vibrant modern business and residential area while Fremantle can retain its historical/cultural port city role.</p> <p><b>Public Transport Systems</b> Public transport systems must be designed into the area. The ideal plan will link the City of Fremantle to the Cockburn Coast land with a further linkage to Cockburn Central.</p> <p><b>Roe Highway Linkage</b> The City of Cockburn and the City of Fremantle should lobby to get the Roe Highway and the Eastern By-pass constructed as soon as possible. The Cockburn Coast land will then have the ability to attract major international and national developers to the site who specialise in major hotel, residential and entertainment projects. The Cockburn Coast and the City of Fremantle will benefit directly from the ability of visitors to virtually drive or be transported directly to the area from the</p>	<p>advertised to the community in the future.</p> <p><b>Noted</b> No changes are recommended as a result of this submission. The DoT, CofC, CofF, PTA, MRWA, DoP and LandCorp through the Transport Planning Working Group have completed investigations into possible public transport links from Fremantle Train Station to Cockburn Coast. A study was carried out to consider the best route to link the two areas and the most appropriate technology with a decision being made in favour of a priority bus route for the short to medium term. The route is consistent with the DoT's draft Public Transport Network Plan for Perth which identifies implementation of Bus Rapid Transit to Cockburn Coast by 2020 extending to Rockingham by 2031. The route investigation also included 'future proofing' that would enable the Bus Rapid Transit system to convert to Light Rail in future.</p> <p><b>Supported</b> The Emplacement and Robb Jetty LSP are supported and informed by the Cockburn Coast Local Transport and Traffic Management Strategy. The Strategy and the LSPs proposes a rapid transit system through Cockburn Coast which connects up to Fremantle in the north and could be extended through to Cockburn Central as part of wider public transport investments in Perth.</p> <p><b>Not Supported</b> The City of Cockburn does not support the construction of Roe Highway west of Kwinana Fwy due to the environmental value of the reserve and the negative environmental impact of the extension. It</p>

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		<p>domestic and international airports. (Probably in almost the same time it would take to get from the airport to the City of Perth.) It is imperative that this road connection be constructed- not just for the people of Fremantle and Cockburn - but for all of the future generations of the State.</p> <p><b>Urban Sprawl Vision</b> The Cockburn Coast area will be a completely lost opportunity if the current plans go ahead. Turning the area into low rise suburbia will be a complete and wanton waste of the potential of the area. Instead of a 'suburban' vision being applied to the land a far greater vision in the form of a major city centre with a much wider variety of land uses and building types should be pursued. The current plan is weak. The plan is just another version of urban sprawl being poured over land that has some of the greatest development potential this State has seen for years.</p> <p><b>Lost Opportunity</b> To adopt the Cockburn Coast Plan in its current form would be to choke the development potential of the land and create another sector of urban sprawl just for the sake of it. A serious lack of vision is being applied in the current proposal. A lack of vision that if supported will cost this State and future generations hundreds of millions of dollars in lost opportunity. I submit that the entire plan be reviewed and aligned with the true development potential of the land.</p>	<p>should also be noted that the Fremantle Eastern Bypass was removed from the Metropolitan Region Scheme in 2004, and disposal of the land to private ownership is now well advanced with development already occurring within the former reservation.</p> <p><b>Not Supported</b> The Emplacement LSP provides for medium and high density development and the Cockburn Coast project as a whole is expected to accommodate 10,000 people. Proposed building heights are primarily between 6-8 storeys (high density) and 3-5 storeys (medium density), and it is not considered that this equates to 'low-rise suburbia'. Only a small pocket of land within the Emplacement LSP area is identified for low density (1-3 storeys), to provide the potential for housing options for families. In addition, the proposed residential codings are supported by proposed Scheme provisions that mandate minimum densities to ensure the vision for Cockburn Coast is achieved.</p> <p><b>Not Supported</b> The Cockburn Coast project is an ambitious urban infill project which envisions an urban form more dense than anywhere outside of the Perth CBD</p>
13.	Paul Watson 56 Davilak Avenue Hamilton Hill 6163	<p><b>Objection</b></p> <p><b>SUMMARY OF OBJECTIONS</b></p> <ol style="list-style-type: none"> <li>1. Time allowed for submissions</li> <li>2. Neglect of heritage and environmental values necessary for sense of place</li> <li>3. Contravention of commitment regarding western skyline</li> <li>4. Non-transparency of process for reference group</li> <li>5. Contemptuous treatment of previous submissions by residents in the revision process</li> <li>6. Allowance for one school flawed</li> </ol>	<p>The submission period for local structure plans is guided by the Town Planning Scheme, which are required to follow a format outlined by the State in the 'Model Scheme Text'. The minimum period is 21 days, the City has allowed for 28 days in this case. This is an adequate time period to advertise a document which is a refinement</p>

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		<p><b>Time allowed for submissions</b></p> <p>According to the Hon. John Day, Planning Minister (Cockburn Plans Beachside Life Vision for industrial site; "The West Australian", November 21, 2012), the proposed redevelopment will take 15 to 20 years. The majority of Cockburn residents received notice of the proposal when the December edition of Cockburn Soundings was delivered to their mailboxes in early December. It is unreasonable to allow ratepayers less than one month to prepare submissions on a project with such a long disruptive development time and with such long- ranging impacts on the social and environmental fabric of the City of Cockburn. It is only fair to residents and other concerned parties that the WA Planning Commission allows a period of no less than 3 months for such submissions.</p> <p><b>Neglect of heritage and environmental values necessary for sense of place</b></p> <p><b>Heritage</b></p> <p><i>Destruction of heritage sites</i></p> <p>The current proposal includes the destruction of significant WWII heritage sites in Emplacement Precinct. This is contrary to claims in the Executive Summary of the Emplacement Local Structure Plan, that it <i>"sets out to establish a sustainable community that celebrates the areas [sic] past"</i>.</p> <p>The Executive Summary describes the history of Emplacement as <i>"...characterised by industrial development including the once pulsating Robb Jetty, Cockburn Coast cattle industry and South Fremantle Power Station"</i>, suggesting that <i>"By recognising and learning from the past"</i>, the Local Structure Plan <i>"lays the foundations for an exciting future"</i>.</p> <p>It is unfortunate that this future will be marred and poorer, due to an examination of the heritage value of the precinct, which has been at best, neglectful and at worst, misleading.</p> <p><i>Cursory attention to heritage in the Local Structure Plan</i></p> <p>The Heritage section of the Local Structure Plan comprises eight short paragraphs. Within these, it states that: <i>"The recognition and incorporation of the distinctive heritage of the area is a significant component of the urban renaissance of Cockburn Coast and is integral to creating a distinct and meaningful place. To guide the Local Structure Plans, the Cultural Heritage Strategy includes strategies setting out how to protect and transmit the heritage values of each place, in accordance with relevant legislative requirements"</i>.</p> <p>The Heritage section of the Local Structure Plan acknowledges the military</p>	<p>of several other planning documents which have also been advertised over the last nine years:</p> <p>2004: 'Dialogue on Cockburn Coast'</p> <p>2009: District Structure Plan</p> <p>2010: Metropolitan Region Scheme amendment to 'Urban'</p> <p>2011: District Structure Plan (Part 2) and Town Planning Scheme Amendment 89 to 'Development Area'.</p> <p>The LSP and associated Cockburn Coast Cultural Heritage Strategy will not result in the destruction of an important World War II site. There are provisions to protect and retain the remaining Battery. Specifically, in the LPS the area on which the Battery is located has been identified to remain as public open space to ensure that this important aspect is not subject to development pressure. The two other emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated/extant with the South Beach Battery site.</p> <p>The Cultural Heritage Strategy does acknowledge that the South Beach Battery was constructed as part of the coastal defence system during World War II for the Fremantle Port. However, the Strategy is not intended to be a full history of the place or of Australian Defence. Rather it identifies strategies for its conservation and</p>

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		<p>heritage of the area, by identifying the use of the coast for military training during WWII and identifying South Beach Battery (remains) as <i>“a remnant of a larger military complex that has associations with the military defence operations of Western Australia during World War Two”</i>.</p> <p>However, it has omitted to reveal the extent of that larger military complex, which still exists along the ridge and both within the boundaries of areas identified for high-density dwellings, and within the boundaries of the proposed Cockburn Coastal Drive. It's recommendations for the South Beach Battery site advise developers to:</p> <ul style="list-style-type: none"> <li>• Integrate interpretation of the site in the Cockburn Coast project to communicate the tangible and intangible values and history of the place to the community and that</li> <li>• Consideration should be given to the partial reinstatement of earth embankments to allow an appreciation of its original form</li> </ul> <p>However, it fails to acknowledge the complex infrastructure constructed along the coastal ridge during 1942-1944 to support coastal defence and which still exists today. The Cockburn Coast Cultural heritage Survey, 2012, devotes two and a half pages to Defence heritage, including the area's role as a training ground for the 10th Light horsemen during WWI and in terms of its role in coastal defence during WWII.</p> <p>However, it also fails to identify the coastal infrastructure along the ridgeline, associated with South Beach Battery. I believe it also understates the sense of fear which pervaded the community at that time, by understating the perceived imminence of Japanese invasion by General McArthur, Prime Minister Curtin, and the community in general.</p> <p>Acknowledgement and preservation of the military heritage of this area is essential for the development of a “sense of place”, which is seen as intrinsic to effective community development. With invasion at Fremantle of Japanese forces seen as imminent in 1942, real fear was tangible in the community and the Cockburn Coast suddenly became a hive of activity.</p> <p>According to a United States Office of Strategic Services (OSS) report from October 1942, the Japanese were actively planning an invasion of Australia in June or July 1942. The OSS report is based upon information secretly passed to an OSS asset by neutral Spanish diplomatic staff in Tokyo.  <a href="http://australianbunkermilitarymuseum.org/abmm/research-mainmenu-29/14-invasion-threat">http://australianbunkermilitarymuseum.org/abmm/research-mainmenu-29/14-invasion- threat</a></p>	<p>interpretation to ensure that it can contribute to the history of the area. As part of any specific interpretation proposal for the site further research would be undertaken.</p> <p>The protection and enhancement of the project area's historical components is also found in the Cockburn Coast Place Making Strategy.</p>

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		<p>In February 1942 after the fall of Singapore, an urgent survey was conducted by the British Admiralty, and Cockburn Sound was selected as an ideal fleet anchorage with its wide expanse of water. Work quickly began on securing the Sound in 1942 and went on 24 hours a day, seven days a week for the two years. Overall the project cost two million pounds.</p> <p>Heavy Artillery was set up along the coast from Swanbourne to Cape Peron, and on Rottnest and Garden Islands, to protect the proposed anchorage and its approaches. <a href="http://inbox.apana.org.au/?p=210">http://inbox.apana.org.au/?p=210</a> According to the military history website <a href="http://www.ozatwar.com/usnavy/fremantlesubmarinebase.htm">http://www.ozatwar.com/usnavy/fremantlesubmarinebase.htm</a>, "Approximately 170 American, British and Dutch submarines made a total of 416 war patrols out of Fremantle Submarine Base during WW2" and "By the end of 1943, the number of submarines operating out of Fremantle had increased to thirty". According to the Cockburn Coast Cultural heritage Survey, 2012 (pp.22,59), during 1944, "Leighton Battery and Robb's Jetty, Cockburn [also known as South Beach Battery] were the two places earmarked for the location of the new 5.25 inch emplacements and three emplacements were to be constructed at each site. Unlike Robb's Jetty which was built into soil, the limestone at Buckland Hill had to be quarried for the underground tunnel system and the emplacements. In addition, although emplacements were constructed at Robb's Jetty, guns were never installed and the battery was never operational as it was at Leighton".</p> <p>According to the military history website <a href="http://www.artillerywa.org.au/RAAHS/history.htm">www.artillerywa.org.au/RAAHS/history.htm</a>, "The Leighton Beach Battery site has been entered in the Register of the National Estate as a significant World War Two Coastal Defence Facility". Leighton Beach Battery was in an advanced state of neglect prior to its recognition as a military heritage site and both State and federal resources have been allocated to its preservation and renovation. It is now a successful tourist attraction and makes a significant contribution to the "sense of place" in the communities of North Fremantle, Mossman Park and Cottesloe.</p> <p>Leighton Beach Battery consists of gun emplacements and the tunnel system associated with them. Both are important components of the heritage site and interpretive tours of the tunnels, together with interpretive signage provide popular educational and recreational activities for young and old, including many school excursions.</p> <p>Although, as mentioned in the Cockburn Coast Cultural heritage Survey, 2012 (pp.22,59), the South Beach (or Robbs Jetty) Battery was built into sand, the</p>	

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		<p>emplacement was not elevated. Consequently, in the event of the guns being fired, artillery fire-spotters were needed to direct the guns' fire to a target. Along the ridge, above, behind and south of the battery, a complex system of tunnels was constructed. Typical of military tunnel systems, fire-spotters had several locations from which they would observe seaward from the coast and tunnels were needed for them to get from one observation point to another without being observed from the sea. The system of tunnels however along the coastal ridge, extends further south than might be anticipated for this purpose. Indeed it has been suggested that a tunnel complex including military bunkers for storage of post-invasion supplies for a resistance exists along the Spearwood Dune System all the way to Kwinana.</p> <p>Evidence of the particular tunnel system in the Emplacement precinct of the development zone exists, which can be identified as heritage sites. These can be identified by map coordinates. According to WA Planning Commission. The Changing Cockburn Coast, Appendices; Coastal Planning Strategy, (p.38), <i>"Creating a sense of place involves a conscious decision to do so. Putting these words into action, the Cockburn coast needs to present itself as a readable story, engaging people in its past, its traditions, its significant places, old buildings and beauty. The future is about being authentic to this story and it begins with fostering sense of place elements in the development framework. Sustainable communities don't happen by accident; they begin by authentic placemaking and design with a sense of place"</i>.</p> <p>The Cockburn Coast Cultural Heritage Survey, 2012 (p.III) also states that: <i>"This Strategy provides a management framework for the identified heritage sites in each of the three precincts; setting out how to protect and transmit their heritage values"</i>, It is evident that by neglecting to acknowledge, preserve and interpret these significant heritage sites, the WA Planning Commission has been in neglect of its duties. The following questions must be asked: How will the Coastal Development Plan protect and transmit the heritage values of these sites? Without preserving and maintaining this important chapter in Western Australian History, how will the Cockburn Coast present itself as a readable story, engaging people in its past, its traditions, its significant places?</p> <p><b>Neglect of Environment</b>  <i>Encroachment on the existing land area of Beeliar Regional Park</i>  The Emplacement Precinct has removed significant areas of land from Beeliar Regional Park. In addition, the new Cockburn Coast Drive effectively separates the coast from our existing community. This is in contravention of Beeliar Regional Park, Final Management Plan, 2006 (p.1), which is intended to <i>"ensure the Park is</i></p>	<p>This is incorrect, no land is proposed to be 'removed' from Beeliar Regional Park by the local structure plans. The Emplacement Precinct abuts the current road reserve for Cockburn Coast Drive and sits within the</p>



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		<p><i>managed appropriately and is capable of sustaining its high nature conservation and cultural values as well as use by the community". These lands were transferred to the Conservation Commission of Western Australia ... "for the maintenance and restoration of the natural environment, and to protect, care for and promote the study of indigenous flora and fauna and to preserve any feature of archaeological, historic or scientific interest". It is also in contravention of Local Government Planning Policy – Cockburn Sound Catchment Policy, which states one of its objectives as "where appropriate, to maintain or increase native local vegetation in the Cockburn Sound catchment area" (WA Planning Commission. The Changing Cockburn Coast, Appendices; Coastal Planning Strategy, p.7)</i></p> <p><b>Contravention of commitment regarding western skyline</b>  <b><i>The integrity of the Limestone ridge – natural value</i></b>  <i>"Located along the ridge line separating the coast from the bush, Emplacement will be the new high point, a manufactured horizon line that offers the opportunity for a new architectural topography, an integrated landscape of nature and built form". (Cockburn Coast Redevelopment Plan: Emplacement LSP)</i></p> <p>The area immediately west of the planned Cockburn Coast Drive in Emplacement Precinct (extending south from Rollinson Road) and all the way south to the existing railway line, is identified in the Plan as allowing for structures of 6-8 stories. Although allowing for this height for iconic and gateway buildings This is in direct contravention of advice given by Port Catherine Developments that the skyline as seen from the inland (eastern) aspect of the ridge would not be broken by visible structures. (Metropolitan Region Scheme Amendment No. 1010/33 - Port Catherine, 3.8 Visual Amenity. Western Australian Planning Commission, in Environmental Protection Authority Perth, Western Australia Bulletin 1060 [August 2002]).</p> <p>This was confirmed by the WA Planning Commission, when it stated that ". <i>The residential development will also not be visible from the east</i>" (Metropolitan Region Scheme Amendment No. 1010/33 - Port Catherine, 3.8 Visual Amenity. P.29). This directly also contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.1 Land use and zoning), which claims that objectives which have driven land use classifications include <i>"the use of natural landform....to create ...built form character precincts"</i>.</p> <p>This also directly contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.4 Public realm and open space), for which the objectives were stated as to <i>"create an urban typology for open spaces, while respecting the</i></p>	<p>area zoned for 'Urban' purposes under the Metropolitan Region Scheme (MRS). The land which forms Beeliar Regional Park is designated 'Parks and Recreation' under the MRS.</p> <p>Reference is made to the Port Catherine development. To clarify, that development is now known as 'Port Coogee' which is further south of these proposals. It is not correct to link the report on one development area (Port Coogee) to a different development area (Cockburn Coast).</p> <p>The plan allows greater height (6-8 storeys) on the western side of the ridge subject to submission of a detailed development application and assessment against the design guidelines to ensure through site/visual analysis that buildings will not be overly visible from points some distance east of the ridge.</p>

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		<p><i>natural landform and characteristics of the Cockburn Coast area</i>".</p> <p>This also directly contradicts the advice in the Cockburn Coast Draft District Structure Plan (2.1.3.5 Landscape philosophy), that the aim of structure planning was to develop <i>"a landscape theme and identity for the Cockburn Coast area, based on its historical, cultural, environmental and physical characteristics"</i>.</p> <p>It also contradicts advice in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, p.3), that <i>"the big challenge in returning a forgotten industrial coastal strip back to the community is to engage in coastal recreational and tourism planning that responsibly addresses community needs and aspirations without compromising environmental and cultural values"</i>.</p> <p>It also contradicts State Planning Policy No. 2.6 State Coastal Planning Policy, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that <i>"the objectives of this policy are to: protect, conserve and enhance coastal values, particularly in areas of landscape, nature conservation, indigenous and cultural significance"</i>.</p> <p>It also contradicts State Planning Policy No. 2 Environmental and Natural Resources Policy, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that... <i>"the objectives of the policy are: to integrate environment and natural resource management with broader land use planning and decision making; and to protect, conserve and enhance the natural environment"</i>.</p> <p>It also contradicts State Environmental (Cockburn Sound) Policy 2005, as noted in the WA Planning Commission document The Changing Cockburn Coast, (Appendices; Coastal Planning Strategy, pp.3-5), that... <i>"The purposes of this policy are: to declare, protect and maintain the environmental values of Cockburn Sound"</i>.</p> <p>Action Required: It is incumbent on the WA Planning Commission and the Cockburn City Council to ensure that the integrity of this provision is maintained, as has not happened with the Port Coogee Development, where structures have been made clearly visible from Hamilton Road and further east.</p> <p>Furthermore, the residents and ratepayers of Cockburn are still waiting for advice from the Planning Commission regarding penalties which will be imposed on the developer for this transgression. The Planning Commission, in consultation with</p>	

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		<p>Cockburn City Council should make this provision binding, with clear identification of penalties to be incurred for non-compliance.</p> <p><b>Non-transparency of process for reference group</b>  According to Cockburn Coast Draft District Structure Plan (2.2.1), "the residential mix that gained stakeholder approval was informed by iconic urban coastal locations elsewhere in Australia, such as Manly and St. Kilda". Since this coastal development affects residents of Cockburn City most directly, the Council and the WA Planning Commission have a responsibility to ask residents primarily if they want their section of coast to look like these "icons", rather than allow a majority of "stakeholders" who have no long-term vested social interest in the area. Neither the Council, nor the WA Planning Commission has made it clear which or how many of the stakeholders wanted this type of landscape, but since only nine out of 33 in the reference group can be identified as actually living within Cockburn City limits, it is unlikely that those in favour constituted a majority. The selection process for both the 16 landowners and the 9 community representatives has also not been made transparent.</p> <p><b>Contemptuous treatment of previous submissions by residents in the revision process</b>  There is a strong feeling among many residents of Cockburn that any call for submissions to respond to Planning Instruments is largely tokenistic and that the developments which are the target of these instruments are a fait accompli. This is perhaps not surprising, when the opportunity to amend the instruments in response to submissions seems to be often ignored.</p> <p>Some examples can be sourced from the Final Public Submissions Report 120809, for the Draft Cockburn Coast District Structure Plan. There were 92 valid (of 95) submissions received regarding the Plan, according to the following categories:</p> <ul style="list-style-type: none"> <li>Community - 53;</li> <li>Community groups - 9;</li> <li>Landowners - 12;</li> <li>Federal Government agency - 1;</li> <li>State Government agencies - 15;</li> <li>Local Government Authorities - 2.</li> </ul> <p>The following provides some critical commentary of the proponent's responses to</p>	<p>The reference group referred to was set up by the State Government and had input into the 2009 District Structure Plan. This group met between December 2006 and July 2007. It is not appropriate for the City to comment on a group which it did not manage and which ceased nearly six years ago. However, this concern can be raised with the Department of Planning.</p> <p>It is unfortunate if this is the case. However, it is acknowledged that depending on the issue raised, there may be little or no scope to change. An example would be the overall density targets, these are set within higher level planning instruments, thus when it comes to the local structure plans these targets will need to be met.</p> <p>The submissions received by the City of Cockburn for this consultation period have been carefully analysed and responded to. Where possible and appropriate, modifications to the local structure plans have been required.</p> <p>The comments here relate to a submission</p>



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		<p>remains that the proponent clearly has no intention of allowing for the integrity of the ridge line to be retained.</p> <p>Some further vague statement of intention to “achieve greater aesthetic and safety outcomes “ regarding the “built form interface with Cockburn Coast Drive” will give little further comfort to the concerns expressed in these many submissions, which clearly seek a much larger separation between built form and bush, who do not want Cockburn Coastal Drive to be built. And who do not want built form to visibly break the skyline from the east.</p> <p><b>Item 3.8 Visual amenity</b></p> <p>The report claims that a submission from the Department of Indigenous Affairs “commented on” the value of the eastern face of the limestone ridge for its links to an Aboriginal cultural myth. The report refers to public submissions, which “claimed” that the stark nature of this ridge does not significantly lessen its landscape value, and that the proposed excavation of the ridgeline would compromise its natural profile. It also suggests that public submissions “raised concern that impacts on visual amenity will reduce the value of Beeliar Regional Park”. Firstly, from a critical literacy perspective, the language used here is interesting. By suggesting that the Indigenous Affairs submission merely commented, suggests that both the proponents and Indigenous groups see this cultural myth as something not worthy of consideration. Secondly, use of the word “claiming” attempts to give the impression that these Public submissions, clearly did not really know what they were talking about.</p> <p>In these ways, submissions have been treated with contempt by the proponents; their concerns have been trivialised to justify a lack of significant response to their concerns. This summation of the value of submissions appears to have been accepted verbatim and supported by the EPA, who agreed not surprisingly, that: “the visual amenity of the areas adjacent to the project to not be unduly affected by the proposed scheme amendment.</p> <p>The EPA recognises that the limestone ridge traversing the site has significant landscape value, as indicated in many of the public submissions. However, it is considered that implementation of the proposed amendment will not significantly alter the integrity of ridge as a regional landscape feature. The proposed residential development on the west of the ridge will replace the current, largely denuded, landscape, but would not significantly compromise the limestone ridge itself. The residential development will also not be visible from the east.</p> <p>The way in which Public submissions have been treated with contempt by this</p>	<p>The ‘as advertised’ version of the Development Area provisions included a requirement for visual assessment modelling from the landward side of Beeliar Regional Park. However, this provision was required to be deleted by the Minister for Planning.</p> <p>The plan allows greater height (6-8 storeys) on the western side of the ridge subject to submission of a detailed development application and assessment against the design guidelines to ensure through site/visual analysis that buildings will not be overly visible from points some distance east of the ridge.</p> <p>The assessment of these applications will be critical as if a development proposal is built in accordance with the plans approved and complies with the conditions prescribed, there is no recourse for the City to take against a developer.</p> <p>The comments here relate to a submission</p>

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		<p>assessment, and by the proponents response to submissions, is: The EPA has confirmed that residential structures will not be visible from the east. That is good, but it is a commitment which has not been adhered to by the Port Coogee development, so residents should have little confidence that it will be so here. What has not been made clear is how the Department of Planning, Cockburn City Council will respond if in fact this commitment is not adhered to.</p> <p>Furthermore, the response and EPA assessment have colluded in a little “smokes and mirrors” behaviour. By confirming that the residential structures will not be visible, they have allayed fears by those who prepared submissions that the development will not be visible from the East. In reality, it appears likely that Cockburn Coast Drive, including traffic and street lights, will be visible from the east, so the integrity of the ridgeline will actually NOT have been retained by the development and associated infrastructure.</p> <p><b>Item 5.2 Height</b>  Submission No.: 4, 5, 17, 28, 34, 25, 36, 38, 40, 42, 45, 51, 57, 58, 59, 65, 67, 69, 72, 74, 84, 85, 91, 81.  According to the report, a number of these submissions suggested “<i>maximum height limits within the project area of between 2, 4 and 6 storeys dependent on distance to the coast, and an emphasis on high density as opposed to high rise</i>”.  Reasons related broadly to concerns about:</p>	<p>process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>It can be noted the local structure plans indicate 3-5 storeys in height across the majority of the development area, with 6-8 storeys to the east.</p> <p>The comments here relate to a submission</p>

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		<ul style="list-style-type: none"> <li>• Changing the skyline and visual appeal of the area</li> <li>• Not appropriate within the regional context which has historically been rural</li> <li>• Detracts from scenic landscape</li> <li>• Potential to undermine sense of place</li> <li>• Desirability and necessity of medium and high rise development called into question For example, one resident objected that the draft plan allowed for “up to 35% of buildings” up to “8 story plus”. This resident (not the author of this submission) <i>“strongly disagrees with this proposal it will completely change the skyline and visual appeal of the area. It is not in the character or the culture of the Cockburn coast to have high rise apartments on the foreshore. Leave that for Scarborough. The plan to reserve 20% of the available housing to remain affordable for low income earners is a good idea but this does necessitate high rises. High density can be achieved at low level”</i>.</li> </ul> <p>A number of submissions (generally from or on behalf of landowners) also requested:</p> <ul style="list-style-type: none"> <li>• consideration for the location of landmark or gateway buildings on their landholdings</li> <li>• consistency of heights with other new developments within certain precincts (i.e. Newmarket precinct)</li> <li>• reconsideration of height limits to enable flexibility and economic feasibility</li> </ul> <p>The Response outlined the minimum proportions targeted for various heights of structures in the draft CCDSP as:</p> <ul style="list-style-type: none"> <li>• Min. 3% separate houses</li> <li>• Min. 22% terrace houses</li> <li>• Min. 33% low rise apartments (3-5 storeys)</li> <li>• Min. 31% medium to high rise apartments (6-8 storeys, over 8 storeys respectively)</li> </ul> <p>The response appears to have chosen to disregard completely the concerns and submissions which aimed to reduce planned building heights. merely brushing them off with the inadequate response: <i>“targets were established in conjunction with the Cockburn Coast Reference Group, on the basis that they would support urban consolidation, public transport and sustainability objectives for the area; as well as intensity and diversity of housing stock”</i>.</p> <p><b>5.11 Coastal development (north of McTaggart Cove)</b>  The report identified <i>“that further clarity is required on the reasoning for the</i></p>	<p>process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p>

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		<p><i>inclusion of this land for development purposes and on the built form provisions for this development area</i>", based on submissions. Critical literacy analysis of this suggests that there was broad and loud resistance to the development of this area voiced in submissions. The response firstly waffled on about ensuring <i>"that there is a critical mass to support the new town centre within the revitalised Power Station precinct"</i>. What does that mean? Does it mean the precinct needs more residents to provide the necessary population to justify the hugely expensive development of the Power Station, to provide a vibrant community, or to open up more land for speculators? This is not made clear and the report needs to be more responsive to submissions, by doing so.</p> <p>The response secondly waffled on about providing <i>"passive surveillance and activation adjacent to the foreshore area. This activation is required in response to the significant anti- social behaviour present in the location, owing to the lack of adjacent development"</i>. What does that mean? Does it mean that because there have been a few cars broken into in the area, we need to build a city to stop that? What a load of nonsense, honestly.</p> <p>Submissions also apparently were very concerned about <i>"the potential impact on coastal vegetation"</i> of this area of precinct. ,In response, the report admitted that <i>"a small area of vegetation is potentially impacted on by the western development proposal"</i>. This is supposed to be a professional document, so how can it be taken seriously, when the size of the area is not identified. Instead we (the public) are left to interpret <i>"a small area"</i> verbatim. The response goes on to accede that destruction will occur of an area where <i>"vegetation is largely of a good condition,"</i> and <i>"small area of vegetation of very good condition may be impacted"</i> .</p> <p>Their referral to the Environmental Protection Authority for <i>"consideration in determining the need for environmental assessment under the process as outlined in section 5.9"</i> is laudable, but how will the results of that enquiry be communicated to the public and particularly to residents who expressed concerned about this in their submissions. The report does acknowledge that <i>"further consultation with Indigenous elders will be required in more detailed planning for the project area"</i>, but makes no suggestion of how the results of that consultation be communicated to the public and particularly to residents who expressed concerned about this in their submissions.</p> <p><b>Item 5.13.1 Continuation of animal exercise</b>  Submission No.: 22, 41, 44, 46, 50, 55, 56, 57, 67, 71, 79, 81, 84, 87, 93.  The report identified <i>"Strong support was expressed for the continuation of animal (dog and horse) exercise opportunities within the CY O'Connor reserve and the</i></p>	<p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>Please also see response to submission 11 further above. Horses are currently exercised in the early morning. It is noted the</p>



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		<p><i>redevelopment area</i>" and stated that <i>"the district structure plan supports the ongoing use of the beach for these purposes, and acknowledges that the continued use of the beach for these activities will need to be carefully managed as development progresses"</i>. However, there are issues, which the report fails to acknowledge from submissions.</p> <p>Critical literacy analysis of the above statement indicates that the proponents have been careful to only mention the horse exercise area within C Y O'Connor reserve, while the issue of "Continuation of Horse Exercise" involves a much larger area of the proposed development. In this way, submissions have been treated with contempt.</p> <p>One submission for example, stated that : <i>"The living horse heritage [sic] in the area is going to be severely inhibited and endangered by the proposed road network...Randwick racing stables are home to several beautiful horses who use the Cockburn coast beaches and regional parks. They are well known and loved characters in the area. The stables themselves are on the, state heritage [sic] register but the planned road network cuts them off from the coast and the parks and surrounds them with busy high traffic roads. Horses will not use overpasses or go through tunnels so they will [sic] be made virtually [sic] homebound. As this area has such a rich culture and history involving horses it should not have such a detrimental [sic] network of roads dividing and separating [sic] the people and horses who live in it and use it"</i>.</p> <p>How are horses from a number of heritage-listed and continuing horse stables in Hamilton Hill supposed to get to the beach? By allowing for a maintained horse exercise area at CY O'Connor beach, but not allowing for safe riding trails to get horses the proponents are saying that they will accept horses on the beach, but it will only be possible if they are transported there by horse float.</p> <p><b>Item 5.13.5 Public open space</b>  Submission No.: 14, 15, 20, 22, 25, 30, 34, 51, 53, 59, 67, 79, 88, 89, 91, 94.  <i>"The district structure plan outlines a level of public open space above the 10% required by WAPC policy. Further local open space may be identified during local structure planning stages to achieve the minimum 10% contribution required under WAPC policy, and to complement the layout of the open space identified on the district structure plan"</i>. What is this saying? This statement is clearly contradictory, but whether it is intentionally intended to be misleading is unclear.</p>	<p>horses from Randwick Stables currently traverse the road system to access the beach and this will become more difficult over time given the regional road network and the pressures of a growing City. In the interests of safety and also accommodating the broader horse community, the option to use floats will be provided for at the McTaggart Cove Rd parking area.</p> <p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p> <p>It is noted there are changes recommended to the public open space for the Robb Jetty Local Structure Plan. This plan will now</p>

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		<p><b>Item 5.13.8 Wildlife/vegetation corridors to surrounding areas</b>  Submission No.: 12, 37,78, 79, 84, 85, 87, 87.  A number of submissions proposed improved vegetation and wildlife linkages between the key natural areas within the vicinity of Cockburn coast, including Beeliar Regional Park, Clontarf Hill and Woodman Point". One submission for example, suggested that <i>"it is just an illogical idea to introduce traffic to an area that is already preserved bushland when there are already roads in place (Hampton Rd, Cockburn Rd, Rockingham Rd) that will be able to service the area. Heavy traffic should be diverted completely using Stock Road. Light rail networks, bus lanes and cycle paths will be a much better investment for sustainable community. For any roads that must be built speed limits should be capped at 60kph within the area because we do not want walls and sound barriers separating the parkland from the community and the coast. This coastal region should not be used as a thoroughfare for traffic between Fremantle and Rockingham"</i>.</p> <p>The response below appears to be along the lines of "it's too late, there are already barriers, so bad luck" and makes half-hearted noises about links for vegetation and pedestrians. <i>"These areas are currently largely segregated by existing physical barriers such as the freight rail, roads and urban development. Given these existing constraints, there is limited opportunity to effect this proposal. However, the draft CCDSP encourages the establishment of east-west open space links within the redevelopment area to encourage retention of existing vegetation and pedestrian and cyclist connections back to Beeliar Regional Park; and identifies the desirability of pedestrian and open space connections to Clontarf Hill should the regional road reservation impacting this areas be rationalised or realigned"</i>.</p> <p>The submissions mentioned above clearly were asking for "improvements" in vegetation and wildlife linkages. The response is that current linkages are negligible, therefore can't be improved. This is illogical and treats submissions with contempt.</p> <p><b>Item 6. Key revisions to the draft Cockburn coast district structure plan</b>  6.2 Planning and built environment</p>	<p>provide closer to the minimum 10% local public open space, as well as the sports oval required by the City's Sport and Recreation Strategic Plan which will provide for a greater catchment.</p> <p>The comments here relate to a submission process/report by the State Government. It is not appropriate for the City to comment on how those submissions were treated. However, this concern can be raised with the Department of Planning.</p>

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		<p>The only <i>“Modification of land use areas and associated dwelling and population yields”</i> incorporated into the revised structure plan, were <i>“based on revised alignment of Cockburn Coast Drive”</i>. Clearly submissions from residents have been ignored in this revision. The revised plan apparently gives <i>“Greater clarity on permitted heights within Power Station and Newmarket precincts and appropriateness of height within the broader district structure plan area”</i></p> <p>Clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of altering permissible and appropriate heights for buildings within the city limits from those proposed in the draft structure plan.:</p> <p>The revised plan apparently gives <i>“Clarification on public open space contribution”</i>. Again, can I stress that clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of altering the areas allocated for public open space from those proposed in the draft structure plan because they felt strongly that they were inadequate.</p> <p>The revised plan apparently gives <i>“Clarity on anticipated built form interface with Cockburn Coast Drive and adjacent to the foreshore in the Robb Jetty precinct”</i>. Again, can I stress that clarity was not what submissions from residents called for; they called for modifications to the plan, in terms of significantly altering the provisions for the built form interface from those proposed in the draft structure plan because they felt strongly that they were inadequate. Revisions listed do not include any which relate to Coastal development (north of McTaggart Cove), Continuation of animal exercise, or Wildlife/vegetation corridors to surrounding areas, as called for in many residents' submissions.</p> <p><b>Allowance for one school flawed</b> According to Cockburn Coast Draft District Structure Plan (2.2.6), only one primary school will be required in the developed area. This is well below the recommended ratio of schools required for the number of lots in the developed area and two reasons are given.</p> <p>The first is that DET has advised that the extra schoolchildren can be accommodated by the existing capacity of <i>“adjoining primary school catchments”</i>. The second is that apparently, the anticipated demographic of the developed area will not be such that demand for school places will eventuate. This clearly shows that the anticipated residents of the new area is not anticipated to be characterized by families, but more likely by single people and speculators, which does not augur well for developing any real sense of community</p>	<p>Schools are provided based on the advice of the Department of Education. A submission has confirmed that they are happy with the school as indicated in the draft Robb Jetty Local Structure Plan.</p>

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		<p><b>Conclusion</b></p> <p>The report takes care to note that <i>“the consultation process undertaken for the district structure plan is the first stage in an ongoing liaison with the community, that will be undertaken over the life of the project. In addition to the evolution of the Reference Group process, the community will have the opportunity to provide submissions at the following statutory public comment stages:</i></p> <ul style="list-style-type: none"> <li>• <i>Metropolitan Region Scheme amendment</i></li> <li>• <i>Local Planning Scheme amendment</i></li> <li>• <i>Adoption of local structure plan(s)</i></li> <li>• <i>Adoption of local planning policies (where applied)”</i></li> </ul> <p>However, that does not excuse an inadequate response at this or any other stage. Clearly submissions from residents have been ignored in this revision. The proponents have chosen to ignore or explain away almost all concerns of residents. Rather than take the opportunity to listen to the voices of the community, they have chosen to merely attempt to justify the decisions already made, and in some cases appear to have been misleading in directing attention away from the absence of significant changes, by highlighting minor changes or by presenting already-made decisions as irrevocable “Necessity”. This, I believe describes an attitude of contempt for the residents of the City of Cockburn. Residents expended much effort and emotion into preparing submissions. Residents of the area are passionate about their coastal environment and their city. They deserve more respect than the treatment this process has accorded them.</p>	<p>A copy of the content of this submission will be referred to the Department of Planning. The submission indicates frustration with previous consultations and reports prepared by the Department and it is appropriate they are afforded the opportunity to respond.</p> <p>No changes are recommended as a result of this submission. Though it is noted the submission raises the issue of public open space and reviewed assessment of this has been now undertaken. This submission expresses very strong concerns with previous submission period conducted by the Department of Planning. To ensure these concerns are directed appropriately (as they are not appropriate for the City to comment upon) these concerns will be forwarded to the Department of Planning.</p>
14.	Blandine Halle 73 Healy Road Hamilton Hill WA 6163	<p><b>Objection</b></p> <p>No high density development along the coastline. This land of Robb's Jetty &amp; Emplacement should be transformed into public parkland with native vegetation replantation, cycle path, pedestrian paths, kiosks/cafes. I would be fantastic to have a green corridor of parklands with existing Manning range/park. Residential development should be kept away from coastline. Port Coogee is an example of an environmental disaster - we DON'T WANT a repeat of that.</p>	<p><b>Not Supported</b></p> <p>The project fulfils the State Government's vision and clearly defines objectives to develop a unique dense metropolitan activity centre adjoining the coast. The plan is supported by a Foreshore Management Plan to protect and enhance the community's access to the coastline.</p>
15.	Michael Fineberg <a href="mailto:mmfineberg@gmail.com">mmfineberg@gmail.com</a>	<p><b>Neutral</b></p> <p>My concerns about this project relate to one area only: as a recreational scuba diver, I am concerned about the protection of the marine life and environment of the area around Robb's Jetty. It's a brilliant place for all the local diving clubs to go to, day or night; you can find people diving there most every week in summer. If heavy construction is built up nearby, storm water drains should funnel water as</p>	<p>Agree, every effort will be made to ensure the pristine marine life is preserved during construction with all proposed development will be subject to stringent Building Code of Australia standards. The Foreshore</p>

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		<p>far away as possible. The construction should have minimal to negligible impact on the marine life. Perth's best shore diving is all south of the river; Robb's Jetty is a brilliant place, easy to access, and we all look forward to being able to continue to use it long into the future.</p>	<p>Management Plan for the project has been carefully prepared to ensure there is improved access and amenity to the beach.</p> <p>No changes are recommended as a result of this submission</p>
16.	<p>Department of State Development Level 6, 1 Adelaide Terrace EAST PERTH WA 6004</p>	<p><b>Objection</b></p> <p>The Department of State Development (DSD) provides leadership to drive responsible redevelopment for Western Australia, with a focus on:</p> <ul style="list-style-type: none"> <li>• Delivering the WA Government's priorities for development in projects of significance to the State.</li> <li>• Assisting project proponents and working with stakeholders to develop major resource and industry projects.</li> </ul> <p>The Department's role with regard to industrial land focuses primarily on the development and support of Strategic Industrial Areas (SIA), which are designed to meet the land requirements of Heavy Industry. Notwithstanding, the Department recognises the strategic value of other industry zoned land and is keen to ensure that sufficient land is available to accommodate general and light industry which supports and is synergistic with heavy industry- particularly where such land is within close proximity to SIAs.</p> <p>The Economic and Employment Lands Strategy (EELS): non-heavy industrial; Perth metropolitan and Peel regions, April 2012, identified the impact of unprecedented economic growth during the 1990's and early 2000's upon industrial land values and availability. The Strategy forecasts that the demand for available industrial land within the metropolitan south-west sub-region, the area incorporating the Western Trade Coast (WTC) and proposed Cockburn Coast, will exceed the available supply by 278 hectares by 2031.</p> <p>Current budgetary constraint suggests that the implementation of EELS may not take place for some time, increasing the strategic value and scarcity of existing industry zoned land.</p> <p>The development of industrial land is a lengthy and expensive process, due to the requirement for structure planning (including appropriate separation from land for sensitive uses), environmental and other clearances, rezoning and arrangement for transport routes and service infrastructure to meet industry needs. With project ready industrial land becoming scarce in the Perth-Peel region, particularly</p>	<p><b>Not Supported</b></p> <p>The Emplacement LSP is currently zoned for urban uses under the Metropolitan Region Scheme and City's Town Planning Scheme No.3. The objection to the zoning for uses other than industrial is not considered relevant to this proposal.</p> <p>The Cockburn Coast project is approximately 5km north of the Australian Marine Complex and Latitude 32 industrial area and 12km north of Kwinana Heavy Industrial area. All these areas have residential development in far closer proximity to them than the Cockburn Coast proposal. Therefore, it is not expected that the project will place any pressures on the operations of these industrial areas.</p>

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		<p>premium coastal industrial land, the proposed rezoning of existing project ready industrial land on the Coogee Coast would oppose the objectives of EELS. A significant portion of land within the area identified for development under the Cockburn Coast District Structure Plan is currently zoned for industrial purposes. The Department emphasises that the subject land is the only existing industrial land with coastal access outside of the WTC, an area which is subject to increasing pressures through the encroachment of proposed urban development to the boundary of the Kwinana Air Quality Buffer, and to reinforce the significance of industrial land which is close in proximity to ports and appropriate transport corridors.</p> <p>It should be noted that the land developed under the 1993 Coogee Masterplan saw the relocation and establishment of industry proponents to the subject area. This included relocation to the subject area of industry proponents then located south of the rail reserve, to facilitate rezoning and redevelopment of that land for the residential Port Coogee development. The area north of the rail reserve, the area now proposed for rezoning for residential development under the Cockburn Coast District Structure Plan, was for the purpose of establishing a Biotechnology Park which would accommodate Special Industry, such as proponents involved in seafood processing.</p> <p>In summary, the Department of State Development raises that rezoning existing industrial land adjacent to the coast and close to the WTC will exacerbate land use pressures already placed upon the WTC. It also carries a potential risk to ongoing industrial development and economic growth within the metropolitan region. Whilst adverse economic effects may possibly not be obvious within the short-term, this reduction in industry land has potential over the mid to long term to impede delivery of the State Government's goal for economic output and employment within the WTC to ultimately double to \$28.3 billion and 22,000 jobs.</p>	
17.	<p>The Western Trade Coast Industries Committee Unit 2/1<sup>st</sup> Floor, 18 Civic Boulevard ROCKINGHAM WA 6168</p>	<p><b>Objection</b></p> <p>The Western Trade Coast Industries Committee (WTCIC) was established by the State Government in 2011 with the goal of seeing the annual economic output and employment within the Western Trade Coast ultimately double to \$28.3 billion and 22,000 jobs.</p> <p>As such, the WTCIC is concerned only about potential impacts (positive or negative) on the WTC arising from the Cockburn Coast proposals. There are two matters the WTCIC wishes to comment on:</p> <p>1 . Overall Cockburn Coast proposal.</p>	

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		<p>2. Impact on freight routes.</p> <p>1. Overall Cockburn Coast Proposal</p> <p>The State Government's objective for the WTC is to see its economic output and employment double. The Cockburn Coast project will potentially provide both an additional workforce and customers within 5km of the northern boundary of the WTC. Having this additional potential workforce and customers nearby will help meet the State Government's objectives for the WTC. On that basis the broad objectives of the Cockburn Coast proposal are supported.</p> <p>2. Impact on Freight Routes</p> <p>A concern of WTCIC is to ensure movement of freight into and out of WTC is not constrained by the Cockburn Coast project. Freight is only likely to be constrained if sensitive land uses are allowed to abut the rail and road freight routes and, if so, those conflicts are inadequately managed. Residential development, a noise and vibration sensitive land use, is proposed adjoining the freight rail line and Cockburn Road. This does raise the potential for these two freight routes to be adversely impacted.</p> <p>The WTCIC believes the precautionary principle should be the overriding guiding approach and, as such, supports land use decisions that seek to avoid potential land use conflict in preference to allowing the potential conflict to occur and then trying to manage it.</p> <p>If, however, the City proceeds with the existing plans for the Cockburn Coast, then the proposal must be fully compliant with the intent of SPP 5.4 and all possible measures taken to ensure that the conflict is indeed successfully managed and in perpetuity as the freight volume grows. In that regard, the WTCIC notes the noise and vibration study undertaken and endorses the proposal to adopt a precautionary approach and adopt a mandatory noise sensitive design requirement within 150m of the freight rail line. This submission represents the agreed view of the WTCIC and does not necessarily reflect the individual views of each member organisation.</p>	<p><b>Noted</b></p> <p>Any development will need to comply with the requirements of SPP 5.4 for freight rail, to ensure that the transport of freight by rail to and from Fremantle Port can continue into the future. The modelling undertaken to inform the Noise and Vibration strategy exceeds the requirements of SPP 5.4.</p> <p><b>Noted</b></p> <p>No changes are recommended as a result of this submission.</p> <p><b>Noted</b></p> <p>Careful consideration has been given to ensuring freight movement is adequately planned for in Cockburn Coast. With regard to noise emissions from freight trains, under Implementation Guidelines for SPP 5.4 'Road and Rail Transport Noise and Freight Considerations in Land Use Planning', where the number of movements is not defined, 24 train movements per 24 hour day should be used. However, to ensure some "future proofing" the modelling undertaken by the Noise and Vibration Strategy which forms part of the LSP has recommended a higher standard to SPP 5.4 by recommending the</p>

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			<p>assessment of each development be based on that of the highest single train movement rather than an average.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p> <p>The Noise and Vibration Strategy forms an addendum to the LSPs and shows the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The design guidelines will outline the requirements for compliance with noise and vibration for land within the impact zone. The Design Guidelines will also include requirements for Notification on titles.</p> <p>No changes are considered necessary as a result of this submission.</p>
18.	<p>The Freight and Logistics Council of Western Australia 1 Essex Street (Marine House) Fremantle WA 6160</p>	<p>The Freight and Logistics Council has had a number of communications with the City of Cockburn about the Cockburn Coast development, our primary concern being to ensure that the heavy rail freight operation to Fremantle Port, which passes through the area, is not compromised by future development.</p> <p>Against that background, the Council would like to comment on the Robb Jetty Local Structure Plan (the Plan). It should be confirmed at the outset that Fremantle Port is a key element in the economic well-being of Western Australia. This \$1 billion pa business is, in turn, dependent on a highly efficient and sustainable freight rail service. The service currently removes 100,000 truck movements from the road network. This figure will grow rapidly as trade through the Port increases and rail's share of the market grows. State Government policies will continue to support this growth. It would seem appropriate, therefore, that the Plan refers to State Planning Policy 1, whose focus is the balance of economic and community interests in Western Australia.</p> <p>The Policy provides clear direction in this respect, for example, <i>"planning for land use and development in a manner that allows for the logical and efficient provision</i></p>	<p>Part 2 of the local structure plans include discussion on a number of relevant State Planning Policies. State Planning Policy 1 (SPP1) - State Planning Framework Policy unites existing State and regional policies, strategies and guidelines within a central framework which provides a context for decision-making on land use and development in Western Australia.</p> <p>Inclusion of a broad reference to the role of SPP1 and how the LSP has been developed in line with this can be included. Reference to SPP1 should acknowledge the broader variety of principles it sets out (this submission only notes two):</p> <ul style="list-style-type: none"> <li>• Environment;</li> <li>• Community;</li> </ul>



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		<p><i>and maintenance of infrastructure protecting key infrastructure, including ports, airports, roads, railways and service corridors from inappropriate land use and development."</i></p> <p>Turning to the specific issue of how the Plan deals with impacts associated with rail freight operations in the area, we would like to make a number of points:</p> <ul style="list-style-type: none"> <li>• The imagery in the Plan is grossly misleading, particularly the artist's impression on the Plan's cover. The lack of fencing, level crossings and general controls of pedestrians/motor cars would, in fact, render the service inoperable on safety grounds. (We have brought the document to the attention of the Office of the Rail Safety Regulator.)</li> <li>• In addition to changing this sort of misleading imagery, the Plan should also include for rail, the sort of cross section analysis provided for road (pages 84-86). Moreover, it should provide noise contour maps for rail to facilitate appropriate spatial planning in the area.</li> <li>• While we do not take issue with the Plan's forecast average train movements of 24 per day, we would point out that State Planning Policy 5.4 requires a transport planning horizon of 15-20 years be used. The Plan refers to movements in 2017 as a planning horizon.</li> <li>• Moreover, it is inaccurate to state (page 54) that train movements will be limited to non-peak periods. This is not the case. The rail service will be unrestricted as to the hours it can operate.</li> </ul>	<ul style="list-style-type: none"> <li>• Economy;</li> <li>• Infrastructure; and</li> <li>• Regional Development.</li> </ul> <p>A modification to this effect will be required.</p> <p>This concern has been raised with the applicant. They have undertaken to amend the perspective image referred to. A modification will be required to either update this perspective or remove it from the document.</p> <p>Several interest groups and government agencies have noted they do not believe the methodology has been followed properly. It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.</p> <p>Noted, this point is also raised by the Fremantle Port Authority submission. This has been raised with the applicant who advise this information was provided by Brookfield Rail, who are the current operator. Therefore, it is appropriate to attribute this statement to Brookfield Rail as a description of how they plan to operate. A rewording to this effect can be required. Regardless,</p>

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		<ul style="list-style-type: none"> <li data-bbox="584 288 1514 379">• Rail noise amelioration initiatives are discussed in Section 4.7.2 (page 66). However, it is not clear from this section what precisely is proposed. We suggest that it be reworded to add clarity on this important aspect.</li> <li data-bbox="584 842 1514 1177">• The discussion in respect of vibration associated with train movements (page 67) is also confusing. The document suggests that residential developments within Curve 1.4 will not be permitted without some vibration attenuation. It then goes on to mention barriers as a possible response. However, barriers will offer no protection against vibration (as they will be equally ineffective for noise attenuation in surrounding buildings higher than two stories). Vibration suppressing mechanisms are available and have been used in the vicinity of this rail line. Their use should be discussed. We believe that the protection of the rail freight operation must be secured within statutory planning instruments to the maximum extent possible.</li> </ul>	<p data-bbox="1541 197 2078 252">modelling considers a single train movement as well as one train per hour, day and night.</p> <p data-bbox="1541 288 2078 715">The Department of Planning has introduced Structure Plan Preparation Guidelines. Part 1 of a structure plan must contain the statutory requirements, while Part 2 has the explanatory text. The section this submission refers to is in Part 2 which currently reads: <i>"...it is recommended that any development located within 150 metres of the railway line have an acoustic assessment, based on the noise levels as listed in Table 5.1 of the Herring Storer Train Noise and Vibration Report, as part of the design, with an acoustic assessment submitted with the development application"</i>.</p> <p data-bbox="1541 751 2078 805">This is replicated in Part 1 as a statutory requirement and is sufficiently clear.</p> <p data-bbox="1541 842 2078 1056">There are a number of methods to ameliorate ground vibration and the method used for each development depends on a number of factors. Thus, a discussion of different suppression techniques was not part of the study. This criteria will be determined as part of the approvals process.</p> <p data-bbox="1541 1093 2078 1204">The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p> <p data-bbox="1541 1241 2078 1455">The Noise and Vibration Strategy forms an addendum to the LSP's and indicates the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The design guidelines will also outline the requirements for compliance with noise and vibration for land within the impact</p>

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		<p>That will require:</p> <ul style="list-style-type: none"> <li>formal recognition in the Town Planning Scheme of the proposed buffer along the rail line of 150 metres (page 12) to manage noise and vibration impacts on, and changes to, adjacent land use;</li> <li>any changes to adjacent land use within the defined buffer to take fully into account maximum noise and vibration levels and mandate an established set of design standards/building materials;</li> <li>design standards/building materials to be specified in the Town Planning Scheme;</li> <li>design standards/building materials to be the basis of approval for Development Applications and Building Licences by the local authority; and</li> <li>design standards/building materials to be issued with Certificates of Title, including memorials on titles to alert purchasers to the proximity of the rail freight line. The Freight and Logistics Council would welcome the opportunity to further discuss the points made here. (Please note that this submission need not be treated as confidential.)</li> </ul> <p>In conclusion, we would note that while a valued member of the Council, the Department of Planning would prefer not to take a position on matters discussed here because of its role in subsequent assessment of the Cockburn Coast development imitative.</p>	<p>zone. Both the LSP and the draft Design Guidelines also include requirements for Notification on titles.</p> <p>Council has determined this area is to be a 'Development Area' under the Town Planning Scheme, known as 'Development Area 33'. The following text provision related to freight rail is to apply:</p> <p><i>"Where appropriate the Local Government may require that proposals be accompanied by a report prepared by a qualified acoustical consultant, certifying that the design features of the development will achieve a satisfactory level of noise attenuation to enable the mixing of residential and non-residential uses to occur; and/or demonstrate mitigation of impacts associated with freight noise and vibration."</i></p> <p><i>"The proponent shall submit to the Local Government a Noise and Vibration Management Plan for approval as an additional detail of the Local Structure Plan(s)."</i></p> <p><i>"The Development Guidelines shall include measures to facilitate sustainable mixed land use urban environments where a diverse range of carefully designed and constructed land uses can successfully co- exist with noise sensitive and noise emitting premises. The objective of such guidelines are to:</i></p> <ul style="list-style-type: none"> <li><i>Achieve appropriate acoustic environments within residential and other noise sensitive premises.</i></li> <li><i>Facilitate a diversity of businesses and services including dining,</i></li> </ul>

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			<p><i>entertainment, culture and creativity industries, and ensure appropriate noise emission mitigation for these and other non-residential land uses.</i></p> <ul style="list-style-type: none"> <li><i>Ensure mechanical, industrial and service equipment is appropriately designed, located and installed to minimise noise disturbance."</i></li> </ul> <p>There is an existing provision in the Town Planning Scheme which states:</p> <p>Clause 10.2.1  <i>"the local government in considering an application for planning approval shall have due regard to such of the following matters as are in the opinion of the local government relevant to the use or development subject of the application –  (c) any approved Statement of Planning Policy of the Commission"</i></p> <p>The local structure plan as detailed further above, includes statutory requirements for acoustic assessments and memorials on title. Similarly the draft Design Guidelines will give further guidance as to what the acoustic report should cover. The Design Guidelines are proposed to be adopted as a Local Planning Policy under the Town Planning Scheme.</p> <p>Given the above, there is no need to include additional provisions in the City's Town Planning Scheme. The above requirements achieve the same intent of what this submission proposes.</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p>

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			<ul style="list-style-type: none"> <li>• Provide clarity to the freight rail movements information (provided by the operator).</li> <li>• Update perspective image used on cover of Robb Jetty LSP</li> <li>• Include reference to SPP1 State Planning Framework Policy</li> </ul>
19.	MGA Town Planners, on behalf of Paino & Associates PO Box 104 WEST PERTH WA 6872	<p><b>Objection</b></p> <p><b>1. Submission on Robb Jetty Local Structure Plan (LSP)</b> This submission is made up behalf of Paino &amp; Associates, owner of Lot 70 Bennett Avenue, Lot 66 Darkan Avenue and Lot 67 Garston Way in response to the advertisement of the LSP.</p> <p><b>2. Summary</b> In Summary it is considered that the LSP documents are overburdened with rhetoric with too little precision. In particular, there is an absence of specificity in the statutory provisions of the LSP. It is accordingly difficult to analyse and therefore formulate a view on many of the issues covered in the LSP. Therefore, the fact that this submission may make no comment in relation to particular matters, should not be construed that this lack of comment represents support for any of these matters.</p> <p><b>3. Public Open Space Provision</b> Clause 5 of the LSP statutory provisions simply lists POS areas without making any provision for their reservation. It is apparent from the schedule at paragraph 5.6.1 of the explanatory text that, the 6.5166ha of POS within the LSP area represents 19.3% of the gross subdivisible area. No justification is provided for the provision of more than the policy standard of 10% other than to advise that the provision accords with DSP 2. The over provision in DSP 2 was pointed out in submissions during advertising but, the DSP was not adjusted as a result. In terms of justification, it is pointed out that the LSP area adjoins a large ocean foreshore reserve with the result that the LSP is in fact well-endowed with recreation areas and does not require additional POS. In addition to comments on the justification for additional POS, it should be noted that a DSP would not normally be expected to be precise on details such as the areas and proportions of POS. Even so, the DSP 2 indicated that POS would only be 12.6% of the residential development</p>	<p>Agree, City officers asked for a number of changes prior to advertising but also faced a similar challenge in preliminary assessment of the content. The statutory provisions are to be contained within Part 1 of the local structure plan. There has been several changes recommended to Part 1, these are detailed in this Schedule of Submissions and also the Schedule of Modifications attached to this agenda item.</p> <p>Noted, additional text will be added to Part 1 of the local structure plans to clearly state that subdivision proposals must include public open spaces as indicated on the local structure plans.</p> <p>The area of public open space has been discussed with the applicant (Landcorp). The ceding of 10% of land suitable for subdivision is only a policy of the Commission and is variable according to the assessment of the circumstances of each case. It is not a statutory requirement and the need for public</p>

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		<p>area over the whole Cockburn Coast area.</p> <p>In particular, both Paino and Associates and Basilia Nominees Pty Ltd consider that the POS area east of the sewerage pumping station site is not required as a buffer area (see Figure 25 of the LSP) and is not functional. Shape and size do not allow efficient use or development and in addition the site is landlocked on three sides, only accessible off Rollinson Road in the north and therefore generally inaccessible to the vast majority of the LSP area.</p> <p>The schedule at paragraph 5.6.1 indicates that there is a total 2.0287ha of "restricted" POS without explaining why it is restricted. It is assumed that some of these POS areas may be restricted because they have a drainage function. If this is the case, it is pointed out that there are currently 6 drainage reserves within the LSP area occupying a total of 0.7963ha. These reserves should simply be relocated if the location of the drainage function is to change.</p> <p>It is noted that in discussion on the Development Contribution Plans (DCP's), (paragraph 5.13), Cockburn Coast is to comprise two DCP's. One covers the Robb</p>	<p>open space and drainage will differ from site to site, depending on the characteristics of the land, the availability of open space already existing within the locality and a number of other considerations. The established mechanism to apply a degree of equity between landowners is the Development Contribution Plan (DCA12). This is adequate to deal with issues in an equitable way.</p> <p>A revised POS schedule has been provided and this now indicates the oval (which will service a number of suburbs) as a proposed item to include in Development Contribution Plan 13 (community infrastructure).</p> <p>The presence of the foreshore is not considered to be a reasonable basis to further reduce the POS provision for this development which will have much higher densities than the traditional residential estates.</p> <p>Do not agree, see comments on Department of Planning submission which also raises this issue. The submissioner is not likely to be aware of the plans of the Water Corporation has to allow access to an area of their lot.</p> <p>The restricted areas are due to drainage. Restricted POS is defined in Liveable Neighbourhoods. The existing drainage reserves are to be designed out by this proposal. The new development will now need to provide for its own drainage.</p> <p>The applicant has done a preliminary assessment of the public open space for the</p>

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		<p>Jetty LSP and the Emplacement / Hilltop LSP. While the second DCP covers the Power Station LSP. It is clear that the Power Station precinct contains considerably less than 10% POS. If necessary, the Power Station DCP should contribute to the POS within the Robb Jetty LSP / DCP.</p> <p>In summary, simply listing POS areas within the statutory section of the LSP is hardly a statutory provision. It needs to explain that each development property is to provide 10% of its area as POS, either as land or cash. If a property provides more than 10%, the land owner should be compensated accordingly. There should be no more than 10% POS from the LSP gross residential area. In this regard, no grounds are provided for nearly doubling the POS provision to 19.3%. This excessive amount cannot be justified as the LSP is in close proximity to a large ocean foreshore reserve resulting in the availability of abundant recreation area. If additional POS is required for drainage, this should be achieved by relocating existing drainage reserves totalling 0. 7963ha.</p> <p><b>4. Affordable Housing</b></p> <p>The Cockburn Coast development area is predominately owned by Government such that there is a significant opportunity for the provision of affordable housing on Government land. The provision of affordable housing is a community responsibility and the community is represented by its Government. It follows therefore that any obligation to provide affordable housing should be accommodated on Government land. The provision of affordable housing should not be the responsibility of individuals, that is, private landowners.</p> <p>It is considered that the inclusion of targets and statements on affordable housing and in particular their incorporation into the statutory provisions of the LSP is potentially beyond power (ultra vires). There seems to be nothing in Schedule 7 of the Planning and Development Act 1005 (P &amp; D Act) or indeed, any other part of the Act authorising a scheme and consequently an LSP adopted under a local scheme to deal with the issue of "affordable housing" Section 7.0, Affordable Housing as well Clauses 8.1 and 8.2, should therefore be removed from the statutory provisions of the LSP. As a minimum, there should be a statement in the statutory section of the LSP confirming that the affordable housing targets are not</p>	<p>power station precinct to confirm that 10% can be achieved within that area. The power station precinct is predominantly District Centre (the non-residential portion can be deducted from the gross subdivisible area) and therefore will be able to achieve 10% quite easily.</p> <p>The area indicated as public open space will be an item in the future Cockburn Coast Development Contribution Plan. Some landowners will give up land, this will be factored in as a 'credit' in calculating their development contribution liability. Other landowners will not give up land, but will have to pay a development contribution. Given they will have no 'credit' applied they will be invoiced for a higher figure.</p> <p>The District Structure Plans (both Parts 1 and 2) have outline targets which apply to all land within the development area, regardless of whether it is under government or private ownership. According, there are a serious of incentives included in this plan which all landowners can take advantage of should they choose to develop affordable housing. The Department of Planning has been very clear in its advice to the City these provisions should not be mandatory. No one is forcing any landowner to take up these incentives.</p> <p>The provisions within the local structure plans relate to incentivising development standards in recognition of the voluntary provision of affordable housing. Development standards are an ordinary part of what a local structure plan does. There is nothing ultra vires in the application of development standards via a local structure plan.</p>

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		<p>mandatory.</p> <p>There is also discussion on affordable housing in the explanatory text. While, there would seem to be the potential to have some aspirational targets, there does not appear to be any ability to formalise these targets or to offer plot ratio bonus on this basis as rewards for the provision of affordable housing.</p> <p><b>5 Car Parking Standards</b></p> <p>The Structure Plan proposes limited provision of car parking for residential developments and in some circumstances, less than 1 bay per unit. There is no analysis of market acceptance of apartments with such limitations on available parking. There is no undertaking in relation to when the proposed BRT will be operational either in terms of a date or in terms of a commitment against the development of a certain number of dwelling units.</p> <p>It is unclear whether standards "A" or "B" of the table at A3.1 of SPP 3.1 apply in the case of a "proposed" high frequency bus route (the BRT) or whether it needs to be an "existing" route. If the "A" standards apply in the case of a proposed route then the restrictions on on-site parking provision are even more intolerable given the lack of commitment to a date for the BRT to become operational.</p>	<p>Part 1 of the local structure plans can be clarified to ensure it reflects the intent (i.e. the provisions related to affordable housing are mandatory).</p> <p>Car parking standards will be as per the City's Town Planning Scheme No. 3, which for residential development will need to meet the requirements of the Residential Planning Codes.</p> <p>In discussion with the City's Manager of Statutory Planning, standard 'A' is applied if a high frequency bus route is planned. In this case, this route is indicated in the Department of Transport's draft Public Transport Plan for Perth. The route is a Stage 1 project annotated as 'before 2020'.</p>



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		<p>Clause 8.6 of the LSP statutory provision should therefore be deleted because the matter of car parking is fully covered by SPP 3.1, the parking provisions of SPP 3.1 are not to be varied and the application of the revised parking standards is too <i>vague</i> to be workable. The standard in the R Codes in relation to minimums should at least continue to apply until such time as the BRT is in place and operating with a service in each direction at intervals of no greater than 15 minutes from 7.00am to 7.00pm.</p> <p><b>6. Detailed Area Plan Requirements</b>            Clause 10 of the LSP Statutory Provisions deals with Detailed Area Plans (DAP's) but, is <i>vague</i> and does not specify the circumstances under which DAP's will be required although the clause is headed "Detailed Area Plan Requirements". It becomes quite conceivable that an applicant can submit a development / subdivision proposal only to be informed that a DAP is required as a prerequisite to approval. This situation results in time delays and additional cost which is ironic given the LSP aspirations for the provision of affordable housing.</p> <p>It is also ironic that the explanatory text contains a regulatory requirement at paragraph 3.5.2 whereas there is no such regulatory requirement in the statutory provisions. Paragraph 3.5.2 appears to say that any subdivision creating a lot greater than 1 hectare or any development of a lot greater than 1 hectare requires a DAP as a prerequisite. This statement at paragraph 3.5.2 which is worded as a regulatory requirement also advises that Council approves subdivisions, which is queried.</p>	<p>This clause does need rewriting as it simply needs to refer to the City's Town Planning Scheme No. 3. The current wording is too focussed on residential development and is silent on the matter of commercial uses. As noted above, standard 'A' is applied. It is noted high frequency is only required (by WAPC Development Control Policy 1.6 Planning to Support Transit Use and Transit Oriented Development) to be at 15 minute intervals or less during peak periods.</p> <p>The local structure plan currently details that a detailed area plan may be required at three stages: prior to development approval, endorsing a subdivision plan, or as a condition of subdivision approval. This will be revised to be prior to development approval or as a condition of subdivision approval. Liaison prior to lodgement of a development proposal would clarify application prerequisites. In many circumstances, the adoption of Design Guidelines will negate the need for Detailed Area Plans.</p> <p>The explanatory section referred to is contained in section 5.3.2 Housing Diversity and Lot Sizes (3.5.2 is mentioned in error) and reads:</p> <p><i>"Where an application seeks to create or development on a lot greater than 1ha, a Detailed Area Plan shall be provided and approved by the City of Cockburn prior to issuing any development or subdivision approval".</i></p>

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		<p>Earlier in the same paragraph, reference is made to Design Guidelines "Criteria". It is unclear whether design guidelines exist or are required or whether or not they are additional to or incorporated within DAP's.</p> <p>The situation in relation to DAP's is therefore confused and requires clarification. If DAP's are to be prerequisites of subdivision or development approval then, the circumstances in which DAP's are required should be specifically defined within the statutory provisions of the LSP.</p> <p><b>7. Housing Diversity</b></p> <p>It is claimed at paragraph 5.3.2 of the explanatory text that Cockburn Coast will provide a diversity of housing types. In WA, there are basically 3 housing types, - single houses, grouped dwellings and multiple dwellings. The vast majority of dwellings catered for in the LSP will be multiple dwellings. Rather than providing a diversity of housing types, the LSP provides virtually no variety. Interrogation of the last 2 censuses reveals that, within the whole City of Cockburn, the number of multiple dwellings (apartments) increased by 514 in the 5 years from 2006 to 2011 or around 100 apartments per year. Government has stated that the Cockburn Coast is a 15 to 20 year project, that is, an average production of 250 to 330 apartments per year. The LSP should be reviewed every 5 years to assess the plan and in particular the multiple dwelling targets against production and up-take in the market. If necessary, the dwelling targets should be adjusted to reflect market requirements.</p>	<p>The Detailed Area Plan (DAP) provisions will be reworded to ensure an appropriate level of clarity is included. The intent is to reflect the DAP is approved by the City and must be done prior to a development application being approved by the City, or a subdivision clearance being given by the City.</p> <p>The Design Guidelines are to be a local planning policy. They are in addition to the local structure plans, in some cases, where there is sufficient detail, it had been envisaged the Design Guidelines could negate the need for a Detailed Area Plan.</p> <p>This is considered a simplistic view of diversity. What the local structure plan seeks to incorporate is:</p> <ul style="list-style-type: none"> <li>• Single detached houses;</li> <li>• Terraced housing</li> <li>• Low, Medium and High Rise multiple dwellings (apartments)</li> <li>• Mixed Use developments</li> </ul> <p>Diversity in high of multiple dwellings is one aspect, as well as diversity of household size (i.e. number of bedrooms provided). There are also incentives toward affordable housing provision which would enable diversity in affordability.</p> <p>Cockburn Coast is a long term project (15-20 years) with a vision predicated on the development of medium-high density 3-8 storey living. It represents a paradigm shift from older development areas. Considered in light of the surrounding developments</p>

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		<p>This is not only important from an economic perspective but, from the viewpoint of servicing. An inadequate rate of development will mean road systems, commercial and community services remain incomplete for an inordinate length of time severely disadvantaging early residents.</p> <p>The Statutory provisions of the LSP should therefore include a requirement to re-assess the LSP against KPI's in respect to the speed of development and housing typologies to ensure the development of a rounded and fully serviced community within the 15 to 20 year time horizon.</p> <p>A range of housing types are permitted under the R Codes at all densities. Private landowners should not be constrained from meeting market demands for different housing styles. There should be flexibility across a landowner's site to increase density on one portion to facilitate a different style of housing demanded by the market on another. The requirement to have a minimum of 3 storey development is impractical for single and grouped dwellings and should be reduced to two storeys. As with other issues, the Government owns the majority of land at Cockburn Coast and is welcome to limit the style of development on its holdings but, it should not restrict the development of private land.</p> <p><b>8. Developer Contribution Arrangements</b>  Previous discussion has addressed the part of POS within the Developer Contribution Arrangement. Generally, the concerns are:</p> <ul style="list-style-type: none"> <li>• POS within the LSP is excessive (19.3%);</li> <li>• Drainage reserves should be relocated rather than provided again (double dipping);</li> <li>• The distribution of POS is inequitable with the Robb Jetty LSP providing much more than the Power Station LSP and the Hill Top/Emplacement</li> </ul>	<p>where single residential housing dominates, the Cockburn Coast project also adds to the diversity in the broader region.</p> <p>'Severely disadvantaged' is considered an exaggeration. Cockburn Coast is adjacent to the Port Coogee development and within minutes of the facilities of Spearwood, Hamilton Hill, Coogee, South Fremantle and the Strategic Metropolitan Centre of Fremantle. Some initial inconvenience is normal for the first residents in an area. There needs to be a reasonable quantum of customers to support businesses and services. There is no need to reassess the local structure plans against KPI's.</p> <p>Disagree, strong development and density outcomes are imperative to ensure the vision for Cockburn Coast is achieved. This is even more important on private landholdings to ensure consistency.</p> <p>Response given further above.</p> <p>Also local road reserves on the local structure plan do not reflect all the local road reserves which will eventuate (this will come with subdivision design). It is impossible to provide certainty with the total area of local roads and therefore it could not be reasonably included in the development</p>

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		<p>LSP, With regard to the inclusion of road infrastructure within the DCP, the Cockburn Coast development has some unique features in that many of the roads within the plan are only required to allow people from one area to access another, for example, provide access from eastern parts of Cockburn Coast to the beaches. Because the development typology is overwhelmingly multiple dwellings, it is reasonably possible to configure a subdivision plan over most existing lots which does not require the construction of new roads. It is therefore arguable that all roads have a function well beyond the land immediately fronting them and should therefore be included as items within the DCP to be shared across the whole LSP.</p> <p>Figure 39 of the LSP reveals that Cockburn Road is to be contained within a 25.5m reserve. For the most part, Cockburn Road north of McTaggart Road is contained within a 25m reserve. Small sections are wider and one section is only 20m wide. The widening from 20 to 25m has been obtained as conditions on subdivision or development approval. These landowners were not compensated. However, it appears that under the proposed DCP landowners who have already ceded widening will have to proportionately pay for the widening to be taken from other owner's land. This is inequitable to them and given the argument above about the broad function of most roads, inequitable to all other owners. Similarly, it is inequitable to require cost sharing over the main street. The main street is situated in an area of commercial and R160 development. It is therefore within the most intensely developed part of the LSP and while it may consume a little more land and cost a little more to construct, these costs are compensated by the intensity of the adjacent development.</p> <p>The BRT route is only 0.5m wider than the reserve for a standard local street and the pavement width only 0.5m wider where a bus stop is located. The additional cost of the BRT route is negligible compared to the standard local street and accordingly, if all streets are not included in the DCP then, neither should the BRT.</p> <p>There are some small lots south of Garston Way which are largely consumed by proposed roads. It is totally unrealistic to think that these owners will make the land available for road reserves. Clearly, these land areas will need to be acquired.</p>	<p>contribution plan (DCP) which will be submitted well before subdivision applications. Regardless, it is an ordinary condition of subdivision that a proposal provides the roads fronting it. Only where a road is above an ordinary standard (such as Robb Jetty's main street, or the bus rapid transit route) would it be reasonable to include this in the DCP.</p> <p>If previous areas of Cockburn Rd were given up this is as part of previous approvals that is a separate issue for those affected landowners. Development and subdivision is not 'as a right'. More often than not, developments are issued subject to conditions. If those conditions are thought to be unreasonable there is a process to dispute those conditions. Assumedly, these landowners did not avail themselves of that opportunity and therefore the conditions to their particular development remain valid and undisputed and they have chosen to comply with them in order to undertake their development. In this case, in the interests of advancing the cohesive development of Cockburn Coast, it is clear some costs are appropriate to be shared between landowners. Also see comments in previous paragraph.</p> <p>In addition to above comments on DCP, there is a lot which are almost entirely consumed by the proposed bus route. They have no development potential left, therefore it is appropriate it is acquired. This is not the case with the other lots which still retain an ability to be developed.</p>

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		<p>Paragraph 5.13.1 also lists public realm and environment improvements as items for inclusion in the DCP. The LSP also appends such strategies as a Public Art Strategy. The inclusion of these items in the LSP and potentially within the DCP is queried on the basis that it is not mentioned in Schedule 7 of the P &amp; D Act and therefore may to be ultra vires.</p>	<p>Public Art is not intended for inclusion in a development contribution plan. This is a matter which would need to be the subject of a Percent for Art Policy, which at this stage has not been considered by Council and is a matter considered broader than Cockburn Coast. Public realm and environment improvement relates to some shared paths (where not provided as part of roads) including links to the coast and crossings over the rail reserve. This is considered to be consistent with Appendix 1 – Standard development contribution requirements of State Planning Policy (SPP) 3.6 Development Contributions for Infrastructure. The Development Contribution Plan will need to be the subject of an amendment to the City's Town Planning Scheme No. 3. This will be require the provision of additional detail, justification and assessment by the City and Department of Planning. It is not considered at this stage however to be ultra vires given the clarity provided by the SPP. It is certainly envisaged the foreshore improvements will have a larger catchment and would be treated similarly to the adjacent foreshore proposal (North Coogee Foreshore Management Plan) which is included as a 'Regional' item in the existing Development</p>

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		<p>With regard to other items such as additional foreshore enhancement and coastal protection as well as community/beach parking, the nexus and equity required under SPP 3.6 will need to be clearly established to justify inclusion. Opening up the beach at Cockburn Coast will provide a major asset to the broader Cockburn community with the result that responsibility needs to be shared.</p> <p>With regard to securing the foreshore against erosion with possible groynes and sea walls, this is mostly required to protect the existing railway line. This piece of infrastructure is owned and operated by Government. A significant responsibility to fund these protective works therefore rests with Government. The Cockburn Coast development should not be viewed as a "cash cow" to be used to redirect funding responsibility from Government.</p> <p>There is a high voltage power line generally following the alignment of Bennett</p>	<p>Contribution Plan 13 (community infrastructure). The 'Regional' catchment applies to the whole of the City of Cockburn.</p> <p>Agree, the first response under the State Planning Policy 2.6 State Coastal Planning is for retreat rather than coastal intervention. There are three items of infrastructure which will be subject to erosion risk in a 'do nothing' scenario. These are (in order they become at risk:</p> <ul style="list-style-type: none"> <li>• Dual use path (managed by the City)</li> <li>• Area in foreshore reserve known as 'Robb Road' though not currently gazetted as a formal road (WA Planning Commission)</li> <li>• Freight rail line (managed by the Public Transport Authority and operated by Brookfield Rail).</li> </ul> <p>While it is possible relocate the first two of these, it is less likely the freight rail would be realigned. With that in mind, the driver for coastal intervention measures would be protection of the railway line, though there is a secondary benefit to the dual use path system, 'Robb Road' and a small area of land east of the railway (part of Lot 2108 Bennett Ave) which is controlled by Landcorp.</p> <p>Coastal intervention (groynes or similar) are not intended for inclusion in a Development Contribution Plan for this area or in DCP13 (community infrastructure). The City's Asset Management team will present a report to Council on this issue in the near future with a view to approaching the Department of Transport about their intentions.</p> <p>Agree there is some impact from these</p>

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		<p>Avenue which blights the whole of the LSP. The undergrounding of this power line should be shared cost under the DCP because it impacts a broad area of the LSP and if it is not undergrounded, will change the character of the LSP through the requirement to set development back from the aerial lines.</p>	<p>powerlines though it is not an impact felt by the whole local structure plan area. The supporting report for the DCP (when submitted) will need to analyse the effect to which there is some benefit derived from this undergrounding if it is proposed to be included as a DCP item. It should be noted there is certainly no guarantee Council will support this type of item being included in a DCP (as the works relate to infrastructure which will belong to a separate party i.e. Western Power). This significantly elevates the risk to the City of making up surplus funds if they fall short as the City has no control over the work undertaken. If the item is not included in the DCP, there is nothing to stop landowners coming to a private agreement between themselves which the City will not be a party to.</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p> <ul style="list-style-type: none"> <li>• Additional text to Part 1 concerning public open space; and</li> <li>• Clarity regarding Detailed Area Plans (stages required).</li> </ul>
20.	<p>MGA Town Planners, on behalf of Gosh Leather Pty Ltd PO Box 104 WEST PERTH WA 6872</p>	<p><b>Objection</b></p> <p><b>1. Submission on Robb Jetty Local Structure Plan (LSP)</b> This submission is made up behalf of Gosh Leather Pty Ltd, owner of Lot 1 Bennett Avenue in response to the advertisement of the LSP.</p> <p><b>2. Summary</b> In Summary it is considered that the LSP documents are overburdened with rhetoric with too little precision. In particular, there is an absence of specificity in the statutory provisions of the LSP. It is accordingly difficult to analyse and therefore formulate a view on many of the issues covered in the LSP. Therefore, the fact that this submission may make no comment in relation to particular matters, should not be construed that this lack of comment represents support for</p>	<p>This submission is the same as Submission 19 and 21, albeit on behalf of a different landowner. Please see responses to the points raised in Submission 19 further above.</p>

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		<p>any of these matters.</p> <p><b>3. Public Open Space Provision</b></p> <p>Clause 5 of the LSP statutory provisions simply lists POS areas without making any provision for their reservation. It is apparent from the schedule at paragraph 5.6.1 of the explanatory text that, the 6.5166ha of POS within the LSP area represents 19.3% of the gross subdivisible area. No justification is provided for the provision of more than the policy standard of 10% other than to advise that the provision accords with DSP 2. The over provision in DSP 2 was pointed out in submissions during advertising but, the DSP was not adjusted as a result. In terms of justification, it is pointed out that the LSP area adjoins a large ocean foreshore reserve with the result that the LSP is in fact well-endowed with recreation areas and does not require additional POS. In addition to comments on the justification for additional POS, it should be noted that a DSP would not normally be expected to be precise on details such as the areas and proportions of POS. Even so, the DSP 2 indicated that POS would only be 12.6% of the residential development area over the whole Cockburn Coast area.</p> <p>In particular, both Paino and Associates and Basilia Nominees Pty Ltd consider that the POS area east of the sewerage pumping station site is not required as a buffer area (see Figure 25 of the LSP) and is not functional. Shape and size do not allow efficient use or development and in addition the site is landlocked on three sides, only accessible off Rollinson Road in the north and therefore generally inaccessible to the vast majority of the LSP area.</p> <p>The schedule at paragraph 5.6.1 indicates that there is a total 2.0287ha of "restricted" POS without explaining why it is restricted. It is assumed that some of these POS areas may be restricted because they have a drainage function. If this is the case, it is pointed out that there are currently 6 drainage reserves within the LSP area occupying a total of 0.7963ha. These reserves should simply be relocated if the location of the drainage function is to change.</p> <p>It is noted that in discussion on the Development Contribution Plans (DCP's), (paragraph 5.13), Cockburn Coast is to comprise two DCP's. One covers the Robb Jetty LSP and the Emplacement / Hilltop LSP. While the second DCP covers the Power Station LSP. It is clear that the Power Station precinct contains considerably less than 10% POS. If necessary, the Power Station DCP should contribute to the POS within the Robb Jetty LSP / DCP.</p> <p>In summary, simply listing POS areas within the statutory section of the LSP is hardly a statutory provision. It needs to explain that each development property is</p>	



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		<p>to provide 10% of its area as POS, either as land or cash. If a property provides more than 10%, the land owner should be compensated accordingly. There should be no more than 10% POS from the LSP gross residential area. In this regard, no grounds are provided for nearly doubling the POS provision to 19.3%. This excessive amount cannot be justified as the LSP is in close proximity to a large ocean foreshore reserve resulting in the availability of abundant recreation area. If additional POS is required for drainage, this should be achieved by relocating existing drainage reserves totalling 0. 7963ha.</p> <p><b>4. Affordable Housing</b></p> <p>The Cockburn Coast development area is predominately owned by Government such that there is a significant opportunity for the provision of affordable housing on Government land. The provision of affordable housing is a community responsibility and the community is represented by its Government. It follows therefore that any obligation to provide affordable housing should be accommodated on Government land. The provision of affordable housing should not be the responsibility of individuals, that is, private landowners.</p> <p>It is considered that the inclusion of targets and statements on affordable housing and in particular their incorporation into the statutory provisions of the LSP is potentially beyond power (ultra vires). There seems to be nothing in Schedule 7 of the Planning and Development Act 1005 (P &amp; D Act) or indeed, any other part of the Act authorising a scheme and consequently an LSP adopted under a local scheme to deal with the issue of "affordable housing" Section 7.0, Affordable Housing as well Clauses 8.1 and 8.2, should therefore be removed from the statutory provisions of the LSP. As a minimum, there should be a statement in the statutory section of the LSP confirming that the affordable housing targets are not mandatory.</p> <p>There is also discussion on affordable housing in the explanatory text. While, there would seem to be the potential to have some aspirational targets, there does not appear to be any ability to formalise these targets or to offer plot ratio bonus on this basis as rewards for the provision of affordable housing.</p> <p><b>5 Car Parking Standards</b></p> <p>The Structure Plan proposes limited provision of car parking for residential developments and in some circumstances, less than 1 bay per unit. There is no analysis of market acceptance of apartments with such limitations on available parking. There is no undertaking in relation to when the proposed BRT will be operational either in terms of a date or in terms of a commitment against the development of a certain number of dwelling units.</p>	

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		<p>It is unclear whether standards "A" or "B" of the table at A3.1 of SPP 3.1 apply in the case of a "proposed" high frequency bus route (the BRT) or whether it needs to be an "existing" route.</p> <p>If the "A" standards apply in the case of a proposed route then the restrictions on on-site parking provision are even more intolerable given the lack of commitment to a date for the BRT to become operational.</p> <p>Clause 8.6 of the LSP statutory provision should therefore be deleted because the matter of car parking is fully covered by SPP 3.1, the parking provisions of SPP 3.1 are not to be varied and the application of the revised parking standards is too <i>vague</i> to be workable. The standard in the R Codes in relation to minimums should at least continue to apply until such time as the BRT is in place and operating with a service in each direction at intervals of no greater than 15 minutes from 7.00am to 7.00pm.</p> <p><b>6. Detailed Area Plan Requirements</b></p> <p>Clause 10 of the LSP Statutory Provisions deals with Detailed Area Plans (DAP's) but, is <i>vague</i> and does not specify the circumstances under which DAP's will be required although the clause is headed "Detailed Area Plan Requirements". It becomes quite conceivable that an applicant can submit a development / subdivision proposal only to be informed that a DAP is required as a prerequisite to approval. This situation results in time delays and additional cost which is ironic given the LSP aspirations for the provision of affordable housing.</p> <p>It is also ironic that the explanatory text contains a regulatory requirement at paragraph 3.5.2 whereas there is no such regulatory requirement in the statutory provisions. Paragraph 3.5.2 appears to say that any subdivision creating a lot greater than 1 hectare or any development of a lot greater than 1 hectare requires a DAP as a prerequisite. This statement at paragraph 3.5.2 which is worded as a regulatory requirement also advises that Council approves subdivisions, which is queried.</p> <p>Earlier in the same paragraph, reference is made to Design Guidelines "Criteria". It is unclear whether design guidelines exist or are required or whether or not they are additional to or incorporated within DAP's.</p> <p>The situation in relation to DAP's is therefore confused and requires clarification. If DAP's are to be prerequisites of subdivision or development approval then, the circumstances in which DAP's are required should be specifically defined within</p>	

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		<p>the statutory provisions of the LSP.</p> <p><b>7. Housing Diversity</b>  It is claimed at paragraph 5.3.2 of the explanatory text that Cockburn Coast will provide a diversity of housing types. In WA, there are basically 3 housing types, - single houses, grouped dwellings and multiple dwellings. The vast majority of dwellings catered for in the LSP will be multiple dwellings. Rather than providing a diversity of housing types, the LSP provides virtually no variety.</p> <p>Interrogation of the last 2 censuses reveals that, within the whole City of Cockburn, the number of multiple dwellings (apartments) increased by 514 in the 5 years from 2006 to 2011 or around 100 apartments per year. Government has stated that the Cockburn Coast is a 15 to 20 year project, that is, an average production of 250 to 330 apartments per year. The LSP should be reviewed every 5 years to assess the plan and in particular the multiple dwelling targets against production and up-take in the market. If necessary, the dwelling targets should be adjusted to reflect market requirements.</p> <p>This is not only important from an economic perspective but, from the viewpoint of servicing. An inadequate rate of development will mean road systems, commercial and community services remain incomplete for an inordinate length of time severely disadvantaging early residents.</p> <p>The Statutory provisions of the LSP should therefore include a requirement to re-assess the LSP against KPI's in respect to the speed of development and housing typologies to ensure the development of a rounded and fully serviced community within the 15 to 20 year time horizon.</p> <p>A range of housing types are permitted under the R Codes at all densities. Private landowners should not be constrained from meeting market demands for different housing styles. There should be flexibility across a landowner's site to increase density on one portion to facilitate a different style of housing demanded by the market on another. The requirement to have a minimum of 3 storey development is impractical for single and grouped dwellings and should be reduced to two storeys. As with other issues, the Government owns the majority of land at Cockburn Coast and is welcome to limit the style of development on its holdings but, it should not restrict the development of private land.</p> <p><b>8. Developer Contribution Arrangements</b>  Previous discussion has addressed the part of POS within the Developer</p>	

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		<p>Contribution Arrangement. Generally, the concerns are:</p> <ul style="list-style-type: none"> <li>• POS within the LSP is excessive (19.3%);</li> <li>• Drainage reserves should be relocated rather than provided again (double dipping);</li> <li>• The distribution of POS is inequitable with the Robb Jetty LSP providing much more than the Power Station LSP and the Hill Top/Emplacement LSP, With regard to the inclusion of road infrastructure within the DCP, the Cockburn Coast development has some unique features in that many of the roads within the plan are only required to allow people from one area to access another, for example, provide access from eastern parts of Cockburn Coast to the beaches. Because the development typology is overwhelmingly multiple dwellings, it is reasonably possible to configure a subdivision plan over most existing lots which does not require the construction of new roads. It is therefore arguable that all roads have a function well beyond the land immediately fronting them and should therefore be included as items within the DCP to be shared across the whole LSP.</li> </ul> <p>Figure 39 of the LSP reveals that Cockburn Road is to be contained within a 25.5m reserve. For the most part, Cockburn Road north of McTaggart Road is contained within a 25m reserve. Small sections are wider and one section is only 20m wide. The widening from 20 to 25m has been obtained as conditions on subdivision or development approval. These landowners were not compensated. However, it appears that under the proposed DCP landowners who have already ceded widening will have to proportionately pay for the widening to be taken from other owner's land. This is inequitable to them and given the argument above about the broad function of most roads, inequitable to all other owners. Similarly, it is inequitable to require cost sharing over the main street. The main street is situated in an area of commercial and R160 development. It is therefore within the most intensely developed part of the LSP and while it may consume a little more land and cost a little more to construct, these costs are compensated by the intensity of the adjacent development.</p> <p>The BRT route is only 0.5m wider than the reserve for a standard local street and the pavement width only 0.5m wider where a bus stop is located. The additional cost of the BRT route is negligible compared to the standard local street and accordingly, if all streets are not included in the DCP then, neither should the BRT.</p> <p>There are some small lots south of Garston Way which are largely consumed by proposed roads. It is totally unrealistic to think that these owners will make the</p>	

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		<p>land available for road reserves. Clearly, these land areas will need to be acquired.</p> <p>Paragraph 5.13.1 also lists public realm and environment improvements as items for inclusion in the DCP. The LSP also appends such strategies as a Public Art Strategy. The inclusion of these items in the LSP and potentially within the DCP is queried on the basis that it is not mentioned in Schedule 7 of the P &amp; D Act and therefore may be ultra vires.</p> <p>With regard to other items such as additional foreshore enhancement and coastal protection as well as community/beach parking, the nexus and equity required under SPP 3.6 will need to be clearly established to justify inclusion. Opening up the beach at Cockburn Coast will provide a major asset to the broader Cockburn community with the result that responsibility needs to be shared.</p> <p>With regard to securing the foreshore against erosion with possible groynes and sea walls, this is mostly required to protect the existing railway line. This piece of infrastructure is owned and operated by Government. A significant responsibility to fund these protective works therefore rests with Government. The Cockburn Coast development should not be viewed as a "cash cow" to be used to redirect funding responsibility from Government.</p> <p>There is a high voltage power line generally following the alignment of Bennett Avenue which blights the whole of the LSP. The undergrounding of this power line should be shared cost under the DCP because it impacts a broad area of the LSP and if it is not undergrounded, will change the character of the LSP through the requirement to set development back from the aerial lines.</p>	
21.	MGA Town Planners, on behalf of Basilia Nominees Pty Ltd PO Box 104 WEST PERTH WA 6872	<p><b>Objection</b></p> <p><b>1. Submission on Robb Jetty Local Structure Plan (LSP)</b> This submission is made up behalf of Basilia Nominees Pty Ltd, owner of Lots 65 &amp; 69 Rollinson Road and 68 Garston Way in response to the advertisement of the LSP.</p> <p><b>2. Summary</b> In Summary it is considered that the LSP documents are overburdened with rhetoric with too little precision. In particular, there is an absence of specificity in the statutory provisions of the LSP. It is accordingly difficult to analyse and therefore formulate a view on many of the issues covered in the LSP. Therefore, the fact that this submission may make no comment in relation to particular</p>	This submission is the same as Submission 19 and 20, albeit on behalf of a different landowner. Please see responses to the points raised in Submission 19 further above.

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		<p>matters, should not be construed that this lack of comment represents support for any of these matters.</p> <p><b>3. Public Open Space Provision</b></p> <p>Clause 5 of the LSP statutory provisions simply lists POS areas without making any provision for their reservation. It is apparent from the schedule at paragraph 5.6.1 of the explanatory text that, the 6.5166ha of POS within the LSP area represents 19.3% of the gross subdivisible area. No justification is provided for the provision of more than the policy standard of 10% other than to advise that the provision accords with DSP 2. The over provision in DSP 2 was pointed out in submissions during advertising but, the DSP was not adjusted as a result. In terms of justification, it is pointed out that the LSP area adjoins a large ocean foreshore reserve with the result that the LSP is in fact well-endowed with recreation areas and does not require additional POS. In addition to comments on the justification for additional POS, it should be noted that a DSP would not normally be expected to be precise on details such as the areas and proportions of POS. Even so, the DSP 2 indicated that POS would only be 12.6% of the residential development area over the whole Cockburn Coast area.</p> <p>In particular, both Paino and Associates and Basilia Nominees Pty Ltd consider that the POS area east of the sewerage pumping station site is not required as a buffer area (see Figure 25 of the LSP) and is not functional. Shape and size do not allow efficient use or development and in addition the site is landlocked on three sides, only accessible off Rollinson Road in the north and therefore generally inaccessible to the vast majority of the LSP area.</p> <p>The schedule at paragraph 5.6.1 indicates that there is a total 2.0287ha of "restricted" POS without explaining why it is restricted. It is assumed that some of these POS areas may be restricted because they have a drainage function. If this is the case, it is pointed out that there are currently 6 drainage reserves within the LSP area occupying a total of 0.7963ha. These reserves should simply be relocated if the location of the drainage function is to change.</p> <p>It is noted that in discussion on the Development Contribution Plans (DCP's), (paragraph 5.13), Cockburn Coast is to comprise two DCP's. One covers the Robb Jetty LSP and the Emplacement / Hilltop LSP. While the second DCP covers the Power Station LSP. It is clear that the Power Station precinct contains considerably less than 10% POS. If necessary, the Power Station DCP should contribute to the POS within the Robb Jetty LSP / DCP.</p> <p>In summary, simply listing POS areas within the statutory section of the LSP is</p>	

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		<p>development of a certain number of dwelling units.</p> <p>It is unclear whether standards "A" or "B" of the table at A3.1 of SPP 3.1 apply in the case of a "proposed" high frequency bus route (the BRT) or whether it needs to be an "existing" route.</p> <p>If the "A" standards apply in the case of a proposed route then the restrictions on on-site parking provision are even more intolerable given the lack of commitment to a date for the BRT to become operational.</p> <p>Clause 8.6 of the LSP statutory provision should therefore be deleted because the matter of car parking is fully covered by SPP 3.1, the parking provisions of SPP 3.1 are not to be varied and the application of the revised parking standards is too <i>vague</i> to be workable. The standard in the R Codes in relation to minimums should at least continue to apply until such time as the BRT is in place and operating with a service in each direction at intervals of no greater than 15 minutes from 7.00am to 7.00pm.</p> <p><b>6. Detailed Area Plan Requirements</b></p> <p>Clause 10 of the LSP Statutory Provisions deals with Detailed Area Plans (DAP's) but, is <i>vague</i> and does not specify the circumstances under which DAP's will be required although the clause is headed "Detailed Area Plan Requirements". It becomes quite conceivable that an applicant can submit a development / subdivision proposal only to be informed that a DAP is required as a prerequisite to approval. This situation results in time delays and additional cost which is ironic given the LSP aspirations for the provision of affordable housing.</p> <p>It is also ironic that the explanatory text contains a regulatory requirement at paragraph 3.5.2 whereas there is no such regulatory requirement in the statutory provisions. Paragraph 3.5.2 appears to say that any subdivision creating a lot greater than 1 hectare or any development of a lot greater than 1 hectare requires a DAP as a prerequisite. This statement at paragraph 3.5.2 which is worded as a regulatory requirement also advises that Council approves subdivisions, which is queried.</p> <p>Earlier in the same paragraph, reference is made to Design Guidelines "Criteria". It is unclear whether design guidelines exist or are required or whether or not they are additional to or incorporated within DAP's.</p> <p>The situation in relation to DAP's is therefore confused and requires clarification. If DAP's are to be prerequisites of subdivision or development approval then, the</p>	



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		<p>circumstances in which DAP's are required should be specifically defined within the statutory provisions of the LSP.</p> <p><b>7. Housing Diversity</b>  It is claimed at paragraph 5.3.2 of the explanatory text that Cockburn Coast will provide a diversity of housing types. In WA, there are basically 3 housing types, - single houses, grouped dwellings and multiple dwellings. The vast majority of dwellings catered for in the LSP will be multiple dwellings. Rather than providing a diversity of housing types, the LSP provides virtually no variety.</p> <p>Interrogation of the last 2 censuses reveals that, within the whole City of Cockburn, the number of multiple dwellings (apartments) increased by 514 in the 5 years from 2006 to 2011 or around 100 apartments per year. Government has stated that the Cockburn Coast is a 15 to 20 year project, that is, an average production of 250 to 330 apartments per year. The LSP should be reviewed every 5 years to assess the plan and in particular the multiple dwelling targets against production and up-take in the market. If necessary, the dwelling targets should be adjusted to reflect market requirements.</p> <p>This is not only important from an economic perspective but, from the viewpoint of servicing. An inadequate rate of development will mean road systems, commercial and community services remain incomplete for an inordinate length of time severely disadvantaging early residents.</p> <p>The Statutory provisions of the LSP should therefore include a requirement to re-assess the LSP against KPI's in respect to the speed of development and housing typologies to ensure the development of a rounded and fully serviced community within the 15 to 20 year time horizon.</p> <p>A range of housing types are permitted under the R Codes at all densities. Private landowners should not be constrained from meeting market demands for different housing styles. There should be flexibility across a landowner's site to increase density on one portion to facilitate a different style of housing demanded by the market on another. The requirement to have a minimum of 3 storey development is impractical for single and grouped dwellings and should be reduced to two storeys. As with other issues, the Government owns the majority of land at Cockburn Coast and is welcome to limit the style of development on its holdings but, it should not restrict the development of private land.</p> <p><b>8. Developer Contribution Arrangements</b>  Previous discussion has addressed the part of POS within the Developer</p>	

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		<p>land available for road reserves. Clearly, these land areas will need to be acquired.</p> <p>Paragraph 5.13.1 also lists public realm and environment improvements as items for inclusion in the DCP. The LSP also appends such strategies as a Public Art Strategy. The inclusion of these items in the LSP and potentially within the DCP is queried on the basis that it is not mentioned in Schedule 7 of the P&amp; D Act and therefore may be ultra vires.</p> <p>With regard to other items such as additional foreshore enhancement and coastal protection as well as community/beach parking, the nexus and equity required under SPP 3.6 will need to be clearly established to justify inclusion. Opening up the beach at Cockburn Coast will provide a major asset to the broader Cockburn community with the result that responsibility needs to be shared.</p> <p>With regard to securing the foreshore against erosion with possible groynes and sea walls, this is mostly required to protect the existing railway line. This piece of infrastructure is owned and operated by Government. A significant responsibility to fund these protective works therefore rests with Government. The Cockburn Coast development should not be viewed as a "cash cow" to be used to redirect funding responsibility from Government.</p> <p>There is a high voltage power line generally following the alignment of Bennett Avenue which blights the whole of the LSP. The undergrounding of this power line should be shared cost under the DCP because it impacts a broad area of the LSP and if it is not undergrounded, will change the character of the LSP through the requirement to set development back from the aerial lines.</p>	
22.	B & M Gosatti 856 Karnup Road HOPELAND WA 612	<p><b>Support</b></p> <p>We have no objections to the above proposal and think that this proposal is a wonderful idea, but we do think that the City of Cockburn should start thinking about a proposal for similar structure place for the Power Station, now rather further into the future.</p> <p>We wish to be kept informed of any future developments within this area.</p>	<p><b>Noted</b></p> <p>The district structure planning includes the Power Station precinct, and substantial planning has been undertaken for this precinct through this process. No changes are recommended as a result of this submission</p>
23.	Ricciardi Seafoods & Coldstores PO BOX 1826 FREMANTLE WA 6959	<p>This submission has been made on behalf of Big Buoy Pty Ltd as land owner, business owner and landlord to Mount Barker Chicken, Grand Processors, Buckland Transport and Fremantle City Coldstores. Currently these businesses employ approximately 180 people in the Robb Jetty Precinct.</p> <p>Big Buoy Pty Ltd (represented by Ian Ricciardi) has been an active participant</p>	<p>Noted. Lawful existing uses are able to</p>

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		<p>throughout the planning phases for the Cockburn Coast including involvement on the District Structure Plan (DSP) reference committee and more recently the Cockburn Coast planning committee. Big Buoy Pty Ltd has made it clear throughout the reference group and the Coast planning committee that it intends to remain and operate from its current location for at least the next 15 to 25 years.</p> <p>Big Buoy Pty Ltd's significant concern, as a local employer and business owner, lies in its ability to remain a viable and competitive business and not restricted from its current operations as a direct result of the proposed surrounding residential development.</p> <p>Big Buoy Pty Ltd's originally supported the MRS rezoning as we are told the Cockburn Coast Drive extension and Rollinson Road overpass to Rockingham Road was work in progress by Main Roads. Big Buoy Pty Ltd specific comments on the Rob Jetty LSP relate to:</p> <ul style="list-style-type: none"> <li>• Transitional arrangements</li> <li>• Implementation methodology</li> </ul> <p>Transitional Arrangement</p> <p>The Cockburn Coast DSP specifically identified Darkan and Emplacement as a 'special development areas'. The existing land use circumstances of the area, however, warrant careful consideration of the management of the transition from industrial uses to an urban environment with an employment focus. Redevelopment of this area requires carefully management of the transition so as not to affect the use rights of existing industrial operations in the area (WAPC 2009).</p> <p>The LSP indicative illustrates 'high density' residential typography abutting the existing Cold Store and associated seafood / meat / chicken processing business. The nature of these businesses is dependent upon flexible truck movements to receive and move produce. Trucks currently access the site on a 24 hour basis. As identified, in the LSP the significant access road for trucks is Cockburn Road. This road currently functions as the primary north-south route for road freight and regional traffic.</p> <p>The Robb Jetty LSP does not address how these existing business activities and their associated transport movements will be accommodated in the</p>	<p>continue operating under non-conforming use provisions of the City's Town Planning Scheme.</p> <p>Part 4 of the provisions of the City's Town Planning Scheme detail the situation relating to non-conforming uses. Adherence to the existing planning approvals for these developments will be required.</p> <p>Noted the issue of regional traffic management is of great importance to these existing businesses.</p> <p>As observed in the submission the LSP proposes to address potential conflict between existing industrial uses and future sensitive land uses. The Robb Jetty LSP does discuss how these conflicts will be dealt with. Sections 8 Noise Attenuation (Part 1) and Section 4.5 Industrial Activities (Part 2) outline the requirements for sensitive development proposal within proximity to an existing industrial use. It is not possible at this stage for more explicit transitional arrangements to be specified, as the type of suitable arrangements will be dependent upon the specific location and nature of any future proposals. The statutory requirement spelt out in Part 1 of the Robb Jetty Local Structure Plan reads:</p>

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		<p>implementation of the development. It does not discuss any specific transitional arrangements and / or management of offsite impacts from existing developments.</p> <p>This is of significant concern to Big Buoy Pty Ltd as any impediments to the current operation hours and truck movements' impacts on the efficiency of the businesses and is directly linked to employment.</p> <p>The Cockburn Coast DSP was specific on this issue stating:</p> <p><i>"Existing uses are protected to continue operation under current and valid planning and environmental approvals. Landowners may continue to operate current approved land uses on existing premises within the structure plan area until alternative premises can be secured, the necessary licences gained and construction of infrastructure can be completed, in the event of relocation of the operations. To ensure this is maintained, any proposed change of use adjoining an existing industrial use needs to demonstrate that:</i></p> <ul style="list-style-type: none"> <li><i>the proposed use would comply with any buffer requirements associated with the existing adjoining use;</i></li> <li><i>the proposed use/development does not undermine the potential for the existing operation to continue normal operational activity; and</i></li> <li><i>the proposed use/development would not be adversely affected in terms of odour, safety, noise or visual amenity, particularly where the proposed use is to include residential development.</i></li> </ul> <p><i>This requirement is proposed to be founded in the information supplied as part of the local structure plan, however this is required to be demonstrated at the development stage also" (WAPC 2009).</i></p> <p>The above has not been articulated / demonstrated in the Robb Jetty LSP. Big Buoy Pty Ltd considers the staging of the development within the Robb Jetty precinct without a clear transitional plan a significant risk to the existing businesses and the existing 180 jobs.</p> <p>Specifically Big Buoy Pty Ltd request the transition arrangements be explicitly identified and discussed as a subsection itself after Section 11 'Operation and Implementation' in the LSP, and would address the below issues:</p> <ul style="list-style-type: none"> <li>Vehicle access is critical to existing business operating on a 24 hours per day seven days a week every day of the year. Seafood, cold storage</li> </ul>	<p><b>"8.7 Existing Industrial Buffer Zones"</b></p> <p><i>Where residential, or other sensitive land use is proposed, within an existing land use buffer, applicants shall provide a technical analysis to seek to reduce or mitigate that buffer. Such analysis must be in accordance with the requirements of the relevant State Planning Policy relating to industrial buffers"</i></p> <p>It is also noted Section 5.2.5 (Part 2) acknowledges the existing industrial uses contained within the area proposed as 'Mixed Business' and their ability to continue operation as 'non-conforming uses' while providing for the opportunity for other business opportunities to establish in this area which are more compatible with the surrounding proposed residential uses.</p> <p>The non-conforming use provisions are contained within Part 4 of the City's Town Planning Scheme No. 3. Given this, they do not need to be restated within the local structure plan. It is also not consistent with the Structure Plan Guidelines to include them in the structure plan.</p> <p>The intersection with Rollinson Rd and Cockburn Rd is intended to be significantly</p>

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		<p>produce and chickens are bought in daily using road trains from the north and south. Local deliveries using semi loads are loaded out daily and head north using Rollinson, Cockburn , Hampton and Rockingham roads. The businesses require the current access roads to and from the Port to remain for import / export of frozen and chilled 20' and 40' FCL movements from the premises. The business would not be viable if any changes were made to the trucking operations and / or size of trucks entering our site. The 'Movement Network' identified in the LSP needs to accommodate these existing activities.</p> <ul style="list-style-type: none"> <li>• Transport Noise. Noise attenuation measures should be considered to be expanded to Rollinson Road, Darkan Avenue and Garston Way and not only Cockburn Road.</li> <li>• The retention of employment and staging of development (refer to DSP 3.5.5) is included in the objectives (for the Mixed Use Zone), acknowledging the important employment value existing businesses bring to the area.</li> </ul>	<p>improved with the installation of traffic signals. A semi-trailer is an 'as of right' vehicle (i.e. it does not need a special permit to traverse roads such as Rollinson Rd, Cockburn, Hampton and Rockingham Roads). Therefore the road geometry will accommodate a semi-trailer. There is no need to further articulate this in the document as non-conforming use rights are provided for under the City's Scheme and the road network is already inclusive of this type of vehicle. A Vehicle Access Strategy for Cockburn Road (and design concept) has been drafted by the applicant in liaison with various State agencies. The officer recommendation requires this document to be submitted prior to the City forwarding the local structure plans for the endorsement of the WA Planning Commission.</p> <p>This is not possible. Rollinson Rd, Darkan Ave and Garston Way are not considered a major road under State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning and therefore not considered to generate significant noise which may affect sensitive land uses. As such no noise attenuation measures can be required for these roads.</p> <p>It is noted that the District Structure Plan (2009) refers to 'retention of employment and staging of development' for the area proposed as 'Mixed Use' – however this statement was made in the context of non-conforming use rights (the section under which this reference is included). It is not considered appropriate to refer to the 'retention of employment' as an objective for the Mixed Use zone because 'Industry' uses</p>

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		<ul style="list-style-type: none"> <li>The intent for modelling to be required to assess potential impacts to sensitive land uses from lighting and light spillage.</li> </ul>	<p>are not permitted in the 'Mixed Use' zone.</p> <p>All the current uses this submission refers to are 'Industry – General' use pursuant to the Scheme, and the DSP (2009) specifically identified that industrial uses are not considered appropriate for the 'Mixed Use' area. Therefore it is clear that the 'retention of employment' is to be facilitated by way of non-conforming use rights, and that these rights should not be hindered by development in the surrounding area. The Robb Jetty LSP identifies a buffer to these existing businesses, and restricts the development of sensitive land uses within that buffer (unless that buffer can be refined by further technical analysis). This is considered to address this matter adequately and ensures these businesses can continue lawfully operating in accordance with current approvals.</p> <p>Light spillage whether from industrial or residential land uses are required to be addressed by the producer of the light emissions under the City of Cockburn (Local Government Act) Amended Local Law 20123). It is impractical for development proposals to mitigate against light emissions from adjoining development as lighting is so easily altered so as to increase or decrease its emissions. Therefore, proposals would never be able to ensure that they completely mitigate light emissions from adjoining development. All new commercial development proposals will be designed to limit their off-site light emissions. It is not considered unreasonable that existing industrial/commercial development operate under those conditions.</p>

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		<ul style="list-style-type: none"> <li>How the City of Cockburn intends as per Section 5.0 3.20, Amendment No.89: <i>Identify and describe how future land use and development in accordance with the LSP will be managed so that areas experiencing offsite impacts from existing lawful development are either avoided or managed.</i></li> <li>Discussion as per DSP Sections 3.4 'Transitional Arrangements' and 3.5 'Non-conforming use rights' is repeated in the LSP, in particular regarding protection of existing uses (3.5.1 ). Note the statement in the DSP for detail on transitional arrangements to be provided at LSP stage.</li> </ul> <p>Implementation Methodology</p> <p>Big Buoy Pty Ltd has been clear from the beginning of its intent to remain within the Cockburn Coast area for the long term. It is not stated in the LSP documentation as to why Robb's Jetty and Emplacement Crescent are the first stages of the Cockburn Coast development. Both Robb's Jetty and Emplacement Crescent have high employment generators in the local area based on the concentration of existing businesses and investment capital.</p> <p>The funding model underpinning LandCorp's decision to commence detailed planning and development within Robb's Jetty and Emplacement Crescent should be presented in the LSP. The funding model should account for the opportunity cost to the existing businesses and employment from implementing the Rob Jetty LSP.</p>	<p>As outlined earlier and stated in the submission the LSP addresses potential conflict between existing industrial uses and future sensitive land uses through noise attenuation requirements (Sections 8) and requirements for sensitive land uses proposal within buffers to industrial uses to demonstrate through technical analysis how impacts from the industrial uses are to be mitigated (Section 4.5 Industrial Activities (Part 2)).</p> <p>The rationale for staging is mentioned in the District Structure Plan Part 2. The remaining precinct (known as the Power Station Precinct) is further south than Robb Jetty and Emplacement. It also has the major constraint of the existing substation which would require relocation. Discussions with the infrastructure provider indicate planning and development of an alternative substation site will take in the order of 8 years. In the meantime much of this area could not be redeveloped for sensitive land uses. It must also be remembered the power station site (and adjoining substation) are still zoned 'Urban Deferred' under the Metropolitan Region Scheme, unlike Robb Jetty and Emplacement which are 'Urban'.</p> <p>Inclusion of a funding model detailing why an applicant is seeking to undertake planning in this area first as opposed to another area is not a town planning concern. Notably, there is no provision for this type of information under the Department of Planning's Structure Plan Guidelines. The only relevant discussion required of funding is the mechanism by which development costs are to be shared (in this case a development contribution plan).</p>



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		<p>The LSP also does not detail how traffic (specifically truck movement) will be managed post the LSP with the implementation of subdivision and development. The existing Mount Barker Chicken, Grannd Processors, Buckland Transport, Fremantle City Coldstores and Alba Oils businesses generate significant truck movements on Rollinson Road and Cockburn Road. Trucks currently can wait up to 10 minutes to safely access Cockburn Road from Rollinson Road.</p> <p>The Port Coogee Development as well as the Spearwood Ave extension into Cockburn Road has dramatically increased traffic congestion on Cockburn Road. If all infrastructure roads in the Robb Jetty Precinct are not constructed prior development commencing in this area we envisage a potential bottle neck east bound on Rollinson Road.</p> <p>The impacts to road traffic congestion during the construction and implementation phase has not been evaluated, truck and vehicle movements from Darkan Ave are already being impacted by construction work currently in progress on Rollinson Road (opp MWA), impact to the existing businesses is likely to increase.</p> <p>Furthermore, Big Buoy Pty Ltd fails to understand why the Duoro Road connector has been excluded as the preferred option to minimize traffic congestion on Rollinson Road through to Cockburn and Hampton Roads.</p> <p>Alba Oil and Big Buoy Pty Ltd have given conditional support for the scheme however without preliminary road infrastructure such as the Rollinson Road overpass to Rockingham Road being constructed by Main Roads as part of the initial phase we have concern that both areas of business will be impacted considerably. As part of the LSP Big Buoy Pty Ltd request further detail is provided on:</p> <ul style="list-style-type: none"> <li>• Traffic movement during the development / construction phase</li> <li>• Measures to be undertaken to ensure minimal disruption to existing businesses.</li> </ul>	<p>See comments further above. Access to Cockburn Rd from Rollinson Rd will be significantly improved by the installation of traffic signalisation.</p> <p>Where a proposal for subdivision indicates new roads, these are constructed prior to new lots being formally created. It is however, unrealistic to think there won't be any further increase in traffic from this development (and during its construction phase). However, that traffic will now have the control it currently lacks through the installation of traffic signals at Rollinson Road (and further south at the new 'Main Street' and McTaggart Cove Road). There will also be increased servicing of the area by public transport. This will also have benefits for the current employees in the area who will have an alternative mode of travel available to them.</p> <p>Options to connect to Douro Rd directly (rather than via Hampton/Cockburn Road) would need to traverse the former landfill site owned by the City of Fremantle. The investigations into this option revealed this to be cost prohibitive.</p> <p>It is appreciated the concern these businesses have. However, as noted above it is unrealistic to think there is going to be no impact on traffic, especially during the construction phase. Ultimately there will be signalisation at Rollinson Rd which will be a vast improvement on the current situation.</p> <p>No changes are recommended on the basis</p>

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		<p>Big Buoy Pty Ltd requests these issues be addressed prior to finalisation of the Robb Jetty LSP and looks forward to the opportunity to discuss the matter with the Shire of Cockburn further.</p>	<p>of this submission, though hopefully there is a level of comfort provided by the design concept work and vehicle access strategy for Cockburn Rd which has been required.</p>
24.	<p>Brookfield Rail Pty Ltd GPO Box S1411 PERTH WA 6845</p>	<p>Thank you for your letter dated 19 November 2012 regarding the proposed development of this location.</p> <p>Brookfield Rail advises that this development is alongside an active freight rail line within a state transport corridor which services the Fremantle Port Authority and its customers.</p> <p>Residential development within close proximity to a rail freight line especially that which appears to now be 'high density' as opposed to the 'low density' previously shown in earlier Structure Plans, brings with it many issues such as safety, noise and vibration complaints, trespass and vandalism. Freight rail operations are 24 hours a day 7 days a week all year operation. Present rail movement numbers will likely increase in the future to meet the requirement of freight customers and train paths are scheduled to meet demand.</p> <p>Brookfield Rail together with the Public Transport Authority has met with the City of Cockburn and LandCorp in regard to this proposed Development and agreed</p>	<p>The earlier plans referred to are the District Structure Plans (DSP) Part 1 (2009) and Part 2.</p> <p>DSP Part 1 – plan simply shows 'residential'. The legend for this plan indicates both 'low density residential' and 'residential', though there does not appear to be any 'low density residential' annotated on the plan. Presumably this is a legacy of an earlier version which may have.</p> <p>DSP Part 2 – plan shows both 'medium density residential' and medium to high density residential'.</p> <p>The Robb Jetty Local Structure Plan shows high density. There are requirements in the plan to require noise and vibration assessments. It is considered that a built form response to this constraint may well be easier to achieve with higher density development in this location. These buildings may provide screening to the properties further away from the railway line.</p> <p>The meeting referred to (as far as the City's involvement extended) was a risk</p>

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		<p>the following.</p> <ul style="list-style-type: none"> <li>No new at grade level crossings. However, the existing crossing at McTaggart Cove is to close and be relocated to Main Street. Existing Rollinson Road level crossing to remain (Refer 5.5.6).</li> <li>Any new pedestrian crossings at existing at-grade road level crossings to be fully funded by developers/City of Cockburn.</li> <li>Any new pedestrian crossings not at existing at-grade road level crossings to be grade separated (bridge or underpasses) and to be owned and maintained by developers/City of Cockburn, including graffiti management.</li> <li>Any new Pedestrian bridges to have full height side screens both sides to stop pedestrians throwing objects onto the railway or at trains.</li> </ul>	<p>management workshop for the various rail crossings. No 'agreement' was given to anything by City officers who provided input on planning and engineering matters as they arose during the course of the workshop. The use of the term 'agreed' is very misleading.</p> <p>Noted within the Robb Jetty Local Structure Plan area there are no new crossings proposed; only the existing Rollinson Road crossing and relocation of the existing crossing from McTaggart Cove Rd to 'Main St'. Further south in the Power Station precinct though there is a new proposed crossing (not part of this proposal).</p> <p>It is noted there is already mention in the local structure plan regarding this potentially being a development contribution cost. The development contribution plan will need to elaborate on matters such as need and nexus to determine what proportion is appropriate to be required by the developers and what proportion is not related to the Cockburn Coast development. It is not appropriate for the local structure plan to categorically state that developers and the City are entirely responsible for cost and maintenance. Such a statement will not be included in the local structure plan as it seconds guessing the outcome of the development contribution plan assessment</p> <p>Noted, it is assumed this would be standard development criteria. While it is not relevant for mentioning in the local structure plan, it is important to know for estimating the cost of the item should it deemed to be an acceptable inclusion in the Development Contribution Plan.</p>

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		<ul style="list-style-type: none"> <li>Railway corridor to have security fencing installed to 1.8m height and to be of such standard as to preclude access to the rail line (50mm outside of corridor boundary) fully owned and maintained by City of Cockburn.</li> <li>Brookfield Rail needs to be able to access the railway corridor at certain designated points for maintenance and construction activities. These will necessarily have to be off adjoining roads or reserves and needs to be allowed for in planning and construction of any development works. There will need to be security gates provided at Rollinson Road and Main Street.</li> </ul> <p>There are other conditions which will need to be imposed on the development:</p> <ul style="list-style-type: none"> <li>No runoff from roads buildings or any adjacent land onto the corridor- Stormwater runoff from roads to be contained within road side swales and to be capable of handling 1:100 storm runoff not just a 1:1 or 1 :2 ARI storms. Alternatively, overflows to be directed into City's stormwater system.</li> <li>The report should also make comment on possibilities for noise from Maintenance activities such as rail grinding, tamping or other mechanised plant which could necessarily be at night if train movement density precludes day time operation. Brookfield Rail would normally consult with local authority for any of this activity, however there is still the requirement for allowing this activity as a normal function of servicing an active freight line.</li> <li>Notations on titles (strata or otherwise) Transfer of Land Act 70a that there is noise and vibration generated from the existing railway despite the fact</li> </ul>	<p>The City of Cockburn is not the developer. To be clear, the City will not be providing such a fence. Any conditions on subdivision or development approvals are the responsibility of those developers to meet. The applicant for the local structure plan has indicated they intend to provide the fencing (assumedly this is to the foreshore reserve and where it abuts their own development, not that of other parties). No 'agreements' were made by the City when it attended the meeting Brookfield Rail refers to. This was simply City officers attending a risk management workshop run by Brookfield.</p> <p>Noted, this is a more detailed issue. Brookfield should raise these points at the development and subdivision application stages.</p> <p>No drainage is shown entering the railway reserve.</p> <p>Noted, this is a more detailed issue. Brookfield should raise these points at the development and subdivision application stages.</p> <p>Memorials on title are already mentioned in Part 1 of the local structure plan. They will</p>

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		<p>it may appear to be obvious there is an active freight line within close proximity of the residential buildings.</p> <ul style="list-style-type: none"> <li>• POS &amp; Road reserves adjoining the railway to have 1.8m high colourbond (pool type) railing security fencing with points on top (as used on PTA passenger stations) owned and maintained by the City refer 5.6.2.</li> <li>• Figure 44 -tree/bush plantings not to impinge on level crossing safety - line of sight.</li> <li>• Development to adhere to the WAPC State Noise Policy and its recommendations in regard to noise abatement measures and those in regard to the minimum distance required to abate Vibration.</li> </ul>	<p>be required as appropriate.</p> <p>See comment earlier above. This is also a more detailed issue. Brookfield should raise these points at the development and subdivision application stages.</p> <p>Noted, this is a more detailed issue. Brookfield should raise these points at the development and subdivision application stages.</p> <p>Noted.</p> <p>No changes are recommended based on this submission.</p>
25.	Alison Bolas 24 Rockingham Road HAMILTON HILL WA 6163	<p><b>Objection</b></p> <p>I have a number of objections to the Structure Plans and consider that many of the concerns raised in the original community consultation have not been taken into account.</p> <p>I think environmental and heritage issues have not been given sufficient consideration evident from the continued inclusion of the M.R.S primary road. Although the alignment of Cockburn coastal Drive has been revised to reduce the impact on the ridgeline and Beeliar Regional Park, it still is a major arterial road and will impact significantly on the bushland and have considerable detrimental consequences to the sustainability of Beeliar Regional Park.</p> <p>It is illogical to promote the sustainability of the regional plan and still include the construction of a major arterial road that would promote the movement of heavy traffic through the area, divide the community and have a destructive impact on protected species of native fauna.</p> <p>I still maintain that Cockburn Road as it currently exists should be upgraded and heavy traffic diverted using existing routes such as Stock road. Freight by rail should be increased and alternative transport created for example light rail, rail and a network of bicycle paths would help to reduce dependency on cars.</p>	<p><b>Not supported</b></p> <p>The MRS Primary Regional Road Reservation falls outside of either the Emplacement of Robb Jetty LSP area, and was dealt with through the district structure planning and MRS Amendment No. 1180/41 (Cockburn Coast District Structure Plan). As noted, the MRS Amendment included a revision to the alignment of the reservation that has enabled an additional 5.7 ha of former road reservation land to be included within the Beeliar Regional Park Management Plan. In addition, the actual road will be designed to minimise the amount of vegetation to be cleared, supported by further more detailed fauna and flora surveys. The MRS Amendment was referred to the</p>

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		<p>It seems ludicrous to build roads that only increase traffic when reduction of carbon emissions is so essential to alleviate the effect of climate change. The loss of natural vegetation and the fragmentation of remaining bushland in Beeliar Regional Park by building of the arterial road will significantly impact on the area. Many birds and reptile species inhabit the area and many species move through the area to feed on vegetation or to hunt. Endangered species including Carnaby Black Cockatoo, Blue Wrens and Black Shouldered Kites nest in the area.</p> <p>The Nankeen Falcon, Australian Small Eagle and Peregrine Falcon- migratory species such as the Bee-eater and priority species like the Lined Burrowing Skink and Black Striped snake are found in the area. I still believe that an independent environmental impact study should be undertaken before this road is considered As the Department of Environment and Conservation has stated "protected areas are essential to maintain natural and cultural diversity" and "to foster a sense of place and belonging and contribute to the values of our community"</p> <p>Beeliar Regional Park was primarily created to protect endangered species of natural flora and fauna and as climate change is a major threat to the world's environment and society and is expected to have a profound impact on the unique diversity of Australian wildlife protected areas are essential refuges for species already stressed by the destruction of so much of their habitat. It is stated by the EPA that native vegetation needs to be protected to preserve biodiversity and as green areas to absorb carbon emissions.</p> <p>I am concerned that the Emplacement Precinct involves the removal of bushland in addition to the negative impact of the road.</p>	<p>Environmental Protection Authority, who considered the environmental impact of the road reservation, and advised that it did not require formal assessment under Part IV of the Environmental Protection Act 1986. The Integrated Transport Plan provides for measures to reduce car dependency and encourage walking and cycling as an alternative for future Cockburn Coast residents.</p> <p><b>Noted</b> The Ecological Assessment identifies that there is remnant vegetation within the southern area of the Emplacement LSP area, some of which constitutes habitat for the Carnaby Black Cockatoo. Based on the draft Department of Sustainability, Environment, Water, Population and Communities (DSEWPaC) referral guidelines, clearing the vegetation and development of the Hilltop/Emplacement Crescent Project Site would have a direct impact on Black Cockatoo feeding habitat and given this future referral to DSEWPaC may be required (ie. prior to subdivision or development).</p>

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		<p>I am also concerned that WWII historical sites belonging to a complex military infrastructure associated with the gun emplacement sites and which are yet to be studied will be destroyed by the development.</p> <p>The other issue which I feel has not been given proper consideration is the Horse heritage which is a living heritage and has a long and colourful history in the community. The road potentially will isolate the heritage listed property of</p>	<p>The Ecological Assessment for the Emplacement LSP also notes that Vegetation type 1, which occurs on the limestone ridge on the eastern side of the Project Site, has similarities to a DEC-listed TEC, (<i>Melaleuca huegelii</i> — <i>Melaleuca acerosa</i> [currently <i>M. systema</i>] shrublands on limestone ridges). A vegetation survey in spring (when annual species are present) would be required to confirm this. I</p> <p>It is therefore recommended that the Emplacement LSP report be amended to include the requirement for a spring survey to be conducted prior to any subdivision or development (that proposes works to the land).</p> <p><b>Not supported</b></p> <p>The Emplacement LSP and associated Cockburn Coast Cultural Heritage Strategy will ensure the retention and protection of the remaining gun emplacement. Specifically, the Emplacement LSP includes the gun emplacement within public open space to ensure that this important feature is not subject to development pressure. The two other gun emplacements were dismantled circa 1970 and the area, where these two emplacements were, has been redeveloped. The preparation of the Heritage Strategy included liaison with the Army Museum of Western Australia and a site visit to the Leighton Battery did not reveal that tunnels were associated with the South Beach Battery site.</p> <p><b>Not supported</b></p> <p>It is agreed that horse heritage is an important consideration, and that is why it</p>

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		<p>Randwick Stables from the heritage listed beach as it will make access difficult. Horses do not go through tunnels or use overpasses. I am also concerned that the horse exercise areas and dog exercise areas that are within the CY O'Connor reserve are maintained. I know many people in the community also support keeping the beach as an area for animal exercise and I am pleased that the structure plan supports the ongoing use of the beach for these purposes. I hope that this will not be compromised as the development unfolds. I also question the non-transparency of the reference group who considered that "iconic coastal locations such as Manly and St Kilda "should inform the nature of the development. I understand that the majority of these people were not residents of the City of Cockburn and have therefore no long term social interest in the area.</p> <p>With the likelihood of sea levels rising I don't think that consideration has been given to allow sufficient set back of development from the coast. I think that more effort should be given to the protection of our coastline and beaches from increased erosion caused by developments such as Port Coogee.</p> <p>In conclusion, although some of the issues that were raised in the process of community consultation have been addressed to a degree, I don't think the environmental and social concerns have been given enough importance. The values and needs of the community should not be overlooked in favour of vested interests.</p>	<p>has been considered from the District Structure Planning stage through to the Local Structure Plans. The LSP and associated Cockburn Coast Cultural Heritage Strategy identify and recognise the importance and heritage value of the South Beach Horse Exercise Area. The LSP (pg 60) states 'the aim is for horse facilities to remain at McTaggart Cove to provide facilities for horses with a horse float car park, where the dunes are lower and there will be less disturbance to future residential uses, thus minimising potential land use impacts.' A key objective of the Heritage Strategy is that "South Beach should continue to be used for the horse training, a use with which it has had a long association".</p> <p><b>Not supported</b></p> <p>The assessment of the coastal vulnerability and the coastal setback to the proposed development has been completed in accordance with the 2012 draft State Coastal Planning Policy. This includes a full assessment of the potential impacts of a rise in sea level of 900 mm over the coming century. This value of sea level rise would accommodate an average rise of 9 mm/year over the coming century. This value was adopted by the Department of Transport and the Department of Planning in 2010 after careful consideration of the data and projections.</p> <p><b>Not supported</b></p> <p>The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense urban development that</p>



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			reduces the need for housing on the urban fringe, and provides for well-located affordable housing. The City must plan for population growth, and make the most efficient use of land available.
26.	Department of Health PO Box 8172 Perth Business Centre, WA 6849	<p><b>Support</b></p> <p>Thank you for your letter dated 19 November 2012 requesting comment from the Department of Health (DOH) on the above proposal.</p> <p><b>1. Water and Sewerage</b> All developments must connect to scheme water and reticulated sewerage so as to comply with the <i>Government Sewerage Policy- Perth Metropolitan Region</i>.</p> <p><b>2. Mosquito-borne Disease Control Programs and Services</b> Mosquito populations and the types of mosquito-borne diseases vary across WA. Existing habitats such as wetlands can support extensive mosquito populations and can cause serious nuisances to humans who may reside within these areas, as well as increase the chance of people contracting debilitating or potentially life threatening mosquito-borne diseases.</p> <p>To minimise the risk of mosquito-borne disease and breeding sites, a proponent needs to provide written evidence of the following:</p> <ul style="list-style-type: none"> <li>• The identification of existing breeding locations within close proximity to the proposed development, and the extent of known mosquito-borne disease risk and nuisance levels from biting insects.</li> <li>• Commitment to develop and implement a mosquito management plan that provides strategies for managing mosquito breeding sites during construction and ongoing operational phases of the development and minimising the exposure of future residents to adult mosquitoes.</li> <li>• Commitment to locate, design and maintain any proposed man-made water bodies (e.g. constructed wetlands, vegetated swales and other stormwater infiltration infrastructure) in accordance with the <i>Chironomid midge and mosquito risk assessment guide for constructed water bodies</i> (Midge Research Group, 2007).</li> </ul>	<p><b>Noted</b> No changes are proposed as a result of this submission.</p> <p><b>Not supported</b> The approved Local Water Management Strategies ("LWMS") prepared for Robb Jetty and Emplacement do not allow for the construction of any man made water bodies. The LWMSs state that any retention or detention structures must be completely infiltrated within 96 hours to minimise mosquito breeding.</p> <p>The nearest open water bodies are the Indian Ocean or Manning Lake approximately 800m away. As a result it is not felt that developing a mosquito management plan is necessary. During construction all necessary measures will be undertaken to ensure that any temporary retention or detention structures will be completely infiltrated within 96 hours.</p>

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		<p><b>3. Health Impact Assessment</b></p> <p>You should also consider incorporating Health Impact Assessment (HIA) and/or Public Health Assessment (PHA) principles in your decision making process. The City of Cockburn should use this opportunity to minimise potential negative impacts of increased density development such as noise, odour, light and other lifestyle activities. Public health impacts draw attention to those issues and they should be appropriately and adequately addressed at this stage.</p> <p>For your information and guidance, you may access the relevant information at the following sites:  HIA- <a href="http://www.public.health.wa.gov.au/2/1400/2/health%20risk%20assessment.pm">http://www.public.health.wa.gov.au/2/1400/2/health risk assessment.pm</a>  PHA- <a href="http://www.public.health.wa.gov.au/2/1399/2/public%20health%20assessment.pm">http://www.public.health.wa.gov.au/2/1399/2/public health assessment.pm</a></p>	<p><b>Noted</b></p> <p>Such public health principles have been incorporated into the decision making processes for the Cockburn Coast project from its early stages so that implications of development on current and future communities living in or near the development are considered as a priority. For example, studies and resulting actions which have informed the decision making process (as referred to in DSP Part 2 and LSP submissions) include:</p> <ul style="list-style-type: none"> <li>• Assessments of potential air quality, noise and vibration issues (road and rail related)</li> <li>• Odour impact assessment for Bennett Avenue Pump Station</li> <li>• Master planning consideration of building heights, quality and detailing for the built form with respect to light, visual amenity, safety, integration into the wider area and requirements for appropriate design guideline controls in LSP areas.</li> <li>• Development of an integrated transport plan to provide a comprehensive structure to the future movement network of Cockburn Coast which is sustainable, pedestrian orientated, maximises access to public transport and seeks to minimise possible effects on upon safety and health.</li> <li>• Consideration of site characteristics cultural heritage, natural features and amenity, and resulting actions to maximise amenity such as creation of key physical links for safe community access and public open</li> </ul>

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			<p>space.</p> <p>Early adoption of such principles has allowed potential effects of increased density development to be recognised and the master planning process optimised so that the form of development presented in the district and local structure plans minimises potential for negative effects to occur.</p> <p>Negative impacts associated with mixed use development can be adequately dealt with at the Development Application stage. The Emplacement LSP provides additional guidance on how noise attenuation should be dealt with (Sections 8) and other non-planning legislation is available to control light and odour emissions (including the Health Act 1911 and City of Cockburn (Local Government Act) Amended Local Law 20123).</p>
27.	Fremantle Ports 1 Cliff Street Fremantle WA 6160	<p>The planning process for the Cockburn Coast project area has been underway for over ten years. During this time Fremantle Ports has provided many submissions to local government, the Department of Planning and Western Australian Planning Commission highlighting its concerns. The primary concern that remains to be adequately addressed and that has an increasing potential adverse impact on the port, the community and the economy, is urban encroachment and the threat this presents to the continued, unimpeded operation of the freight rail line and road links that transect the area and connect with the Inner Harbour at Fremantle.</p> <p>The response by the approval and assessing bodies over the last decade has been inconsistent and in the main disappointing. The project is now at the point where land is being developed with people living too close to freight corridors. It is our view that such a result reflects poor planning that shows little regard for the freight corridor users or the future residents who will be living next to these corridors. Whilst maximising developable land for urban uses may allow some short term goals to be achieved for certain stakeholders (for example land developers), often longer term problems are created and the cost of addressing these problems is shifted to and left to be borne by other stakeholders or sectors</p>	<p>This comment relates to a process undertaken by the Western Australian Planning Commission. It is not appropriate for the City to respond to this comment. However, the issue can be raised with the Department of Planning.</p>

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		<p>of the community. This could hardly be described as a desirable outcome from a proper and robust planning process.</p> <p>In 2004 Fremantle Ports wrote to the Western Australian Planning Commission suggesting that a Working Group be established to help progress the project. The aim being that bringing together key stakeholders via a Working Group would allow for critical issues, such as the rail, to be integrated into the project at the earliest possible stage in an appropriate manner. This request was not adopted and whilst Fremantle Ports has been invited to make comment on the project at the statutory consultation periods, we do not believe this has been effective. Once plans have been developed and advertised for comment there has been a pattern of little real change occurring following any of the consultation periods. We believe that many of the planning issues that we perceive with this project could have been resolved or minimised if an effective Working Group with a wide membership had been developed.</p> <p><b>Robb Jetty and Emplacement Local Structure Plans</b></p> <p>In reviewing the Robb Jetty and Emplacement Local Structure Plans the comments put forward are within the context of the Western Australian Planning Commission's key planning documents which dictate planning for the Structure Plan areas, transport corridors and infrastructure such as ports.</p> <p>There are several key Western Australian Planning Commission documents which apply and the manner in which these have been addressed needs to be clearly articulated. Notably Statement of Planning Policy No 1, State Planning Framework which states: <i>"planning for landuse and development in a manner that allows for the logical and efficient provision and maintenance of infrastructure, including protecting key infrastructure, including ports, airports, roads, railways and service corridors from inappropriate land use and development."</i></p> <p>Similarly the Western Australian Planning Commission's State Planning Strategy and Directions 2031 provides specific support to ports and their transport corridors. Specifically the State Planning Strategy states: <i>"ensure that the transport corridors between the generators of heavy traffic (ports and their strategic industry sites) are protected from uses which could jeopardise their efficiency", and "The operation of transport facilities should be made as effective as possible. Transport needs to be provided with adequate transport corridors and facilities which need to be protected from incompatible landuses. This particularly applies to our sea and airports which are the gateways for our future wealth and are of national and State strategic importance."</i></p>	<p>See comment on Submission 18 further above. The section on relevant State Planning Policies can be expanded to include SPP1.</p> <p>Noted, similar to the issue raised above. The section discussing Directions 2031 can be broadened. and additional discussion as to how the LSP has been developed in line with this can be included.</p>

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		<p>Directions 2031: <i>"Perth, perhaps more than other Australian cities due to its relative isolation and primary economy, depends heavily on the efficient movement of freight in and around the city. It specifically states that its strategies include to "protect freight networks and the movement economy" and to "minimise conflict between land use and key infrastructure assets."</i></p> <p>Urban encroachment of the Inner Harbour and its land transport corridors, including the area south of Fremantle to Cockburn is of increasing concern. On the western border of the Robb Jetty Local Structure Plan area is a Freight Rail Reserve, on the eastern border of the Emplacement Local Structure Plan area is a Primary Arterial Road. Together these are key access routes that form part of a wider network providing freight access around the metropolitan region. The freight rail link is critical and to remain effective has to continue to operate 24 hours a day, 7 days a week. If the landuse around the Primary Regional Road and freight rail link change to allow sensitive uses including residential there is the real potential for conflict. The key points raised in this submission applicable to the Robb Jetty Local Structure Plan and Emplacement Local Structure Plan are:</p> <ol style="list-style-type: none"> <li>1. Whilst both Local Structure Plans may achieve residential sustainability objectives, there is concern that in their current form they do not adequately address how they will accommodate current and increasing use of the freight rail line and Primary Arterial Road.</li> <li>2. Urban encroachment on the freight rail and road transport corridors is a lose - lose situation. Sensitive uses located near the corridors, as well as freight rail and road operations both suffer; and the primary beneficiary would appear to be the land developer.</li> <li>3. In 2011 the Minister for Transport announced Fremantle Ports' Inner Harbour will be retained as an operating container and general cargo working port in the long-term, and its container trade will double to about 1.2 million TEUs (containers) per annum around 2020 - 2025. It is difficult to forecast what proportion of this will move by rail but the current target is 30%. Rail is currently moving about 100,000 TEU p.a. clearly there is a strong likelihood that rail volumes will increase substantially in the future.</li> <li>4. The Port of Fremantle is the State's single major container port. The container trade has grown by an average of approximately 5.5% per annum over the last decade and with this growth there will be continuing reliance on road freight and increasing use of the freight rail link. In 2002 less than 3% of containers were transported from the port by rail - the proportion is currently about 14%.</li> <li>5. The Structure Plans incorrectly state that freight trains do not operate in</li> </ol>	<p>To mitigate industry concerns for development in close proximity to existing freight rail line, the applicant (Landcorp) established a working group which includes the PTA, Brookfield Rail, MRWA, the City and Landcorp. The working group has been involved in the review of existing and future at-grade and grade separated crossings, across the freight rail line.</p> <p>The working group has discussed maintaining the at-grade pedestrian and vehicle crossing at Rollinson Road and establishing a new at-grade pedestrian and vehicle crossing to support the 'main street' in Robb jetty. This would be established at the expense of the current McTaggart crossing which would be closed once the 'Main Street' crossing is established. Two grade separated pedestrian bridges would also be established to facilitate pedestrian access to the foreshore. These proposals are highlighted in a plan contained within the Robb Jetty LSP.</p> <p>Also see response to submission 18.</p> <p>Noted, the rapid transit route is likely to commence as a bus (with ability to transition</p>

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		<p>the peak periods. Freight trains have in fact operated in the peak periods for over a decade. The challenge is that it is more difficult to manage given that a section of track is shared by both freight and passenger rail. However current restrictions could be removed at some point in the future with the redevelopment of the Fremantle Traffic Bridge. If this occurs, freight train movements will certainly occur at increasing frequency during the morning and afternoon peaks.</p> <p>6. Section 4.1.2 states that "It is envisaged that in order to attain the desired overall volume and percentage shipped by rail, a fourfold increase in train movements is potentially required although these will be limited to non-peak hour periods." Work by Fremantle Ports suggests that a suitable estimate of projected rail freight movements is about 18 per day, which includes 6 trains per day (12 movements) between the Inner Harbour and Forrestfield and 3 "other" trains per day (6 movements), potentially from areas such as Kwinana and Kalgoorlie. However there are many variables that could affect the actual number of future trains movements.</p> <p>7. It is with certainty that future freight rail projections are for it to grow and that freight trains will be longer and be more frequent. Additionally they may carry double stacked containers at some time in the future.</p> <p>8. It is noted that passenger rail is excluded from the Local Structure Plans, this is supported. Currently freight rail is required to share a small portion of the passenger rail line in Fremantle. This results in some limitation on current freight rail operations in morning and afternoons. It is likely that the use of passenger rail on any portion of the existing freight rail line south of Fremantle would create further limitations on current and future freight operations.</p> <p>9. WAPC Statement of Planning Policy 5.4 supports the principle of avoiding land use conflict as the first choice approach rather than creating and then seeking to manage conflict. This is evidenced by the following statement in the Policy: "Zoning and permissible uses of land in areas adjoining primary freight routes or established freight nodes should be reviewed to ensure, as far as practicable, that they are compatible with freight operations." The policy is clear, adjoining land uses should be compatible. However, contrary to the policy, the Local Structure Plans propose sensitive land uses next to the railway line and road corridor and propose to manage rather than avoid this conflict. Land uses immediately abutting the rail and road freight routes should only be developed on the following basis:</p> <ul style="list-style-type: none"> <li>• No residential or other sensitive land uses immediately abutting the road and rail freight routes.</li> <li>• Residential and other sensitive land uses being separated from</li> </ul>	<p>to light rail) and will be within road reserve.</p> <p>These proposals are for local structure plans, not rezoning. The City has an obligation under section 124 of the Planning and Development Act 2005 to reflect the intent of the Metropolitan Region Scheme. This location was rezoned to Urban in Sept 2011 and the City has reflected this by proposing a Development zone to enable structure planning to occur. District Structure Planning, undertaken by the Department of Planning and Landcorp was used to demonstrate the area was capable of development and supported the request to change the Metropolitan Region Scheme. Residential has been shown adjacent to the railway line in these earlier plans.</p> <p>This is a valid mitigation measure offered by</p>

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		<p>the road and rail freight routes by other non-sensitive land uses.</p> <p>10. Section 8.5 acknowledges that noise will impact future residents, however it then goes on to state: <i>"the onus will be on the designers and developers of the new residential development to demonstrate to the satisfaction of the City of Cockburn that potential noise impacts have been considered and addressed."</i> The reports also suggest approval for noise amelioration measures at the building permit stage, however in light of previous failures we believe this is too late in the process. We believe the deferral this to a later stage of the planning process does not reflect good planning and all efforts to address this should be occurring now.</p> <p>11. The LSP indicates external noise criteria would be exceeded up to approximately 50m of the railway line and vibration criteria up to approximately 80m (using DEC criteria). As such it is suggested that there be no residential development within at least 80m of the rail line.</p> <p>12. Vibration has been identified by the consultant as an issue, but it is not adequately addressed in the Local Structure Plans. Vibration suppression means are available however they are not mentioned. It is unclear if anything is planned in this regard, though it is considered necessary.</p> <p>13. Level crossings are planned as part of the Local Structure Plans, with these crossings there are warning bells that sound as trains pass through. There is no evidence that this additional noise source has been accounted for.</p>	<p>SPP5.4.</p> <p>A noise assessment has been required for all development within 150m of the railway line. Development in accordance with the recommendations of those assessments will be a valid mitigation measure offered by SPP5.4.</p> <p>Noted. The Noise and Vibration Study indicates vibration is an issue ranging from 50-80m along the railway line. While vibration is discussed in Part 2 of the structure plan, it does not contain a related statutory requirement in Part 1. This can be modified to also include vibration to be assessed where applicable.</p> <p>The Noise and Vibration Study includes a plan indicating where the readings for both noise and vibration were taken. The locations are near the existing Rollinson Rd crossing and appear to be in accordance with the methodology outlined in the guidelines which accompany SPP5.4. The existing crossing currently has warning signals and therefore this noise source would already be accounted for.</p> <p>This issue was previously raised as part of the submission for the District Structure Plan. As a result, the following annotation was added to that plan:</p> <p><i>"At-grade crossings will need to be designed,</i></p>



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		<p>14. It is of concern that the LSP incorporates an additional rail crossing between the existing crossings at Rollinson Road and McTaggart Cove. This new crossing, referred to a Main Street, will be one of the key routes into the Robb Jetty precinct, and is planned to accommodate higher traffic volumes than other internal roads. Also of concern is that the western section of Main Street where it crosses the rail lines is planned as a shared zone giving greater priority to pedestrians and cyclists.</p> <p>15. To assist with dealing with noise management there is a strong case for using positive covenants. There is successful precedent for putting these on titles that obligate land owners to incorporate noise amelioration in subsequent construction.</p> <p>16. The issue of emergency and recovery vehicle access in case of train derailments has not been addressed. The fundamental question to be answered as an imperative is if there is enough land around the rail corridor to provide emergency access in the event of a derailment and what this means for the surrounding propose land uses.</p> <p>17. The imagery on the cover of the Local Structure Plan is misleading and uninformative. Specifically it does not accurately detail the existing freight rail line. It has been represented as a minor rail line that an uninformed person could interpret as light rail, not a heavy freight rail line. The artist's impression does not show basic rail safety requirements such as level crossing signals and fencing along both sides of the rail line.</p> <p>18. To give statutory force to the matters raised in this submission it is strongly suggested that where possible that a Scheme Amendment occur incorporating measures to protect the transport corridors.</p>	<p><i>constructed and maintained to the satisfaction of the Office of Rail Safety, within the Department of Transport"</i></p> <p>The Department of Transport have also lodged a submission on these local structure plans and requested the Public Transport Authority (PTA) be consulted for new, upgraded or relocated crossings.</p> <p>The applicant (Landcorp) has already commenced the design process for each crossing with the relevant parties.</p> <p>Memorials on Titles are already required as appropriate, as discussed in Part 1 of the local structure plan.</p> <p>There is no reduction of the existing railway corridor proposed. The corridor is also directly adjacent to the Foreshore Reserve (as it is currently). The land to the east of the corridor is abutted by adjacent roads or reserves for approximately half its length. Detailed design stage will enable appropriate locating of access gates as well.</p> <p>Noted. This has also been raised in another submission and Landcorp have advised they will update the image. A modification has already been noted.</p> <p>A number of the issues are not agreed with. The few which are can be adequately included into the structure plan, some within the statutory section of the plan (Part 1). There is no need to include further transport corridor matters in the Scheme. This has</p>



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		<p>Experience with urban encroachment at the Inner Harbour provides clear evidence that introducing sensitive uses, such as residential, in close proximity to transport and port infrastructure will present compatibility problems. There is concern that both the Local Structure Plans do not adequately address how they will accommodate the current and increasing use of the freight rail line and Primary Regional Road. The Local Structure Plans may achieve residential development sustainability; however they have the potential to impact on the current and future freight rail and road operations, which are critical elements of sustainable freight transport planning for the metropolitan area.</p> <p>The concept of sustainability requires that all elements are considered and that the optimal outcome for all, with a long term view, is sought. To give precedence to residential development around the road and rail linkages to the Inner Harbour is short term in approach, and may be considered inconsistent with the broader long term view required of sustainability.</p>	<p>already been adequately catered for by Amendment 89 which included the provisions specific to Cockburn Coast Development Area.</p> <p>See comments above.</p> <p>As outlined above, the City has an obligation to implement the Urban zoning of this land. Land uses as guided by the District Structure Plan have been included as well as due adherence to the various State Planning Policies which are relevant. This proposal is not preventing the existing rail corridor from use, it is not reducing it, or seeking to collocate infrastructure within it. It provides for mitigation measures which are provided for by SPP5.4</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p> <ul style="list-style-type: none"> <li>• Provide clarity to the freight rail movements information (provided by the operator).</li> <li>• Update perspective image used on cover of Robb Jetty LSP</li> <li>• Include reference to SPP1 State Planning Framework Policy</li> <li>• Broaden reference to Directions 2031</li> <li>• Update Part 1 of the Robb Jetty LSP to include requirement for Vibration Assessment in the 50-80m area adjacent to the rail corridor.</li> </ul>
28.	Main Roads WA PO Box 6202 EAST PERTH WA 6892	<p><b>Objection</b></p> <p>Thank you for your letter dated 19 November 2012 requesting Main Roads comments on the above proposals.</p> <p>Main Roads has reviewed the proposed local structure plans for Robb Jetty and Emplacement and has no objections in principle subject to the following</p>	

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		<p>conditions:</p> <ol style="list-style-type: none"> <li>1. Development of an agreed planning design concept for Cockburn Road between Rockingham Road and Spearwood Avenue. As you may be aware, Landcorp, City of Cockburn, Department of Transport, Department of Planning and Main Roads are currently developing a revised planning design concept for Cockburn Road between Rockingham Road and Spearwood Road. The proposed concept includes upgrading of Cockburn Road to a four lane divided road with two lanes in each direction. A vehicle access strategy is also required to be developed for Cockburn Road to manage and control vehicular access from Cockburn Road.</li> <li>2. The proposed upgrade of Cockburn Coast to four lanes will require widening of existing Cockburn Road reserve. The widened road reservation will need to be protected through the local structural plan and subdivision process. Any additional land required shall be ceded at no cost to Main Roads.</li> <li>3. The applicant is required to undertake a transport noise assessment in accordance with the guidelines of the WAPC State Planning Policy 5.4 <i>"Road and Rail Transport Noise and Freight Considerations in Land Use Planning"</i>.</li> </ol>	<p>Noted, the City is aware of the work undertaken for a design concept for Cockburn Road and the vehicle access strategy being drafted. Prior to forwarding the local structure plan to the Department of Planning, these draft documents can be required.</p> <p>The local structure plan will be clear as to the width required to Cockburn Road, including any areas where widening may be necessary (such as at intersections). It is noted there is already mention in the local structure plan regarding this potentially being a development contribution cost. The development contribution plan will need to elaborate on matters such as need and nexus to determine what proportion is appropriate to be required by the developers and what proportion is not related to the Cockburn Coast development. It is not appropriate for the local structure plan to categorically state that Main Roads have no responsibility for cost. Such a statement will not be included in the local structure plan as it seconds guessing the outcome of the development contribution plan assessment.</p> <p>Noted, refer to comments on submission from the Department of Transport (prepared with the Public Transport Authority and Main Roads). Queries have been raised about the methodology used.</p> <p>Several interest groups and government agencies have indicated they do not believe the methodology has been followed properly.</p>

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		<p>Advice to Applicant:</p> <ol style="list-style-type: none"> <li>1. Following the development of an agreed planning design concept and reservation for Cockburn Road, Main Roads intends to initiate the removal of the Primary Regional Road (PPR) reservation of the future Cockburn Coast Drive from the MRS and have Cockburn Road up to Rollinson Road included in the MRS as a PPR.</li> <li>2. The structure plans propose a number of traffic signals along the existing Cockburn Road. Main Roads approval is required for all proposed traffic signals prior to implementation. The applicant needs to provide justification and an evaluation of alternative measures for any proposed traffic signals. Supporting information such as a preliminary design drawing(s), predicted traffic and pedestrian volumes, SIDRA analysis and traffic impact reports will need to be included for any formal assessment.</li> <li>3. The widening/upgrading of Cockburn Road is not in the Main Roads' current 4 year program and as such is considered long term. However, Main Roads is working with Landcorp and other stakeholders develop staging options to facilitate incremental improvement to Cockburn Road.</li> </ol>	<p>It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.</p> <p>Noted, it is understood this is the intent of Main Roads.</p> <p>Noted, the applicant has been advised of this (by provision of the content of this submission).</p> <p>Noted.</p> <p>On the basis of this submission there have been minor changes recommended. In summary they are:</p> <ul style="list-style-type: none"> <li>• Recommendation to require the concept design and vehicle access strategy to be provided, prior to the local structure plan being forwarded to the WA Planning Commission.</li> </ul>
29.	Department of Transport	Reference is made to the City of Cockburn's request for comments on the above noted Local Structure Plans (LSPs). The Department of Transport (DoT) has	This submission was followed up with the Department of Transport following a meeting

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	Level 8, 140 William Street Perth WA 6000	<p>liaised with the Public Transport Authority (PTA) and Main Roads Western Australia (MRWA) and provides the following comments on each of the LSPs.</p> <p>The DoT is also aware that the Freight and Logistics Council has written to you and shares some of their concerns, some of which are reiterated below. The DoT is prepared to support the two structure plans on the condition that the issues raised in this letter are addressed prior to consideration by the Western Australian Planning Commission (WAPC).</p> <p><b>General Comments</b></p> <p>While the Transport Portfolio agencies generally support intensification of urban development and the creation of employment opportunities through infill development, there are a number of issues associated with development as proposed in the Robb Jetty and Emplacement Local Structure Plans. Additional information and strategies will be required to address such issues as the mitigation of freight rail noise and vibration, provision of level crossings and fencing along the rail lines.</p> <p>The Transport Portfolio agencies compliment the City for undertaking the Cockburn Coast Integrated Transport Plan to ensure all modes are considered in the proposed developments. More detailed traffic modelling needs to be undertaken to determine the traffic implications of the development, particularly the projected traffic volumes for intersections on Cockburn Road and the existing and proposed level crossings over the freight line. This will be particularly relevant at the development application stage as it may affect development setback requirements and access issues.</p> <p>The Cockburn Coast District Structure Plan (CCDSP) recognises the important role that rail will continue to play in the State's freight movement strategy and the likely increase of freight movements along the rail line. The Fremantle freight rail corridor forms the western boundary of this LSP. As such, noise and vibration generated by freight trains must be taken into consideration particularly as they operate 24 hours a day, 7 days a week, and by 2031 it is anticipated the current 22 train movements per week will have increased to 126. The WAPC's draft <i>Outer Metropolitan Perth and Peel Sub-regional Strategy</i> notes that corridors with a predominant freight function are identified in <i>State Planning Policy 5. 4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning (SPP.5.4)</i>. This policy aims to ensure that major freight corridors are protected from incompatible urban encroachment. The Cockburn Coast area is included in SPP 5.4. The Transport Portfolio agencies have noted that the noise levels contained</p>	<p>arranged by the applicant.</p> <p>The Department of Transport have since advised the only unresolved matter is that of the methodology followed for the assessments of noise and vibration. They also note they believe a Noise Management Plan is required at the Local Structure Plan stage.</p> <p>Several interest groups and government agencies have noted they do not believe the methodology has been followed properly. It must be acknowledged that these groups and agencies are not those responsible for the interpretation of the relevant State Planning Policy 5.4 (SPP5.4) Road and Rail Transport Noise and Freight Considerations in Land Use Planning. No issue has been raised by the Department of Environment and Conservation ("DEC"), where appropriate expertise to assess such studies resides. This matter has been followed up with the DEC and at the time of writing this report, still no formal objection to the methodology undertaken had been made by DEC.</p> <p>The approvals process will require each lot located in the nominated distances from the railway line and Cockburn road, to comply with noise and vibration constraints.</p> <p>The Noise and Vibration Strategy forms an addendum to the local structure plan ("LSP") and indicates the impact zone. Text in the LSP also makes reference to the Noise and Vibration Strategy. The Design Guidelines will also outline the requirements for compliance with noise and vibration for land within the impact zone. Both the LSP and</p>

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		<p>within Appendix B of the Structure Plan, the Noise Vibration Study, do not adequately meet policy requirements and are inconsistent with the SPP 5.4 based on the following:-</p> <ul style="list-style-type: none"> <li>• The maximum noise levels used in the Cockburn Coast Noise Vibration Study are inconsistent with the SPP 5.4;</li> <li>• The noise modelling should have used Outside Noise Criteria (Table 1 Herring Storer report) rather than Inside Noise Criteria;</li> <li>• The noise modelling has been undertaken for a quieter trains and does not take into account other classes of train in comparison with other studies carried out by the PTA; and</li> <li>• The Study does not depict noise modelling contours for the freight line, hence the extent of noise impacts on the proposed development is not defined or clear. The Robb Jetty LSP also does not adequately address vibration from the existing freight rail. The Noise Vibration Study shows that development is impacted up to 65 - 80 metres from the freight rail but development is indicated well within this distance, and no vibration mitigation measures have been considered or recommended.</li> </ul> <p>With regard to safety and security, the PTA will require an upgrade to the freight line fencing to PTA standard and at no cost to the PTA. The PTA has previously advised that no additional level crossings are to be provided. It is understood that LandCorp are seeking to close McTaggart Cove crossing to enable the proposed Main Street crossing to be provided.</p> <p><b>Rob Jetty Local Structure Plan and Emplacement Local Structure Plans</b></p> <p>Cockburn Road forms the eastern boundary of the Robb Jetty LSP and the western boundary of the Emplacement LSP area. It is understood that discussions are underway with MRWA, DoT, LandCorp and the City on the future form and function of Cockburn Road, and there may no longer be a need for the inland Cockburn Coast Drive as it appears that future traffic volumes may be able to be accommodated on a 4 lane Cockburn Road. This may have statutory planning implications for the site at the development stage. Further consultation with MRWA will be required, including further traffic analysis at the proposed intersections. To ensure sufficient road capacity through this area, the DoT, MRWA, City of Cockburn and LandCorp have agreed that MRWA will develop a suitable road design concept to accommodate the projected traffic volumes of around 30,000 annual average weekday traffic (AAWT) by 2031. The following</p>	<p>the draft Design Guidelines also include requirements for Notification on titles and refer back to SPP5.4 where the specifications for these more detailed assessments reside.</p> <p>With regard to the request for a Noise Management Plan to be done at the local structure plan stage, the applicant has indicated this plan will be done at the development approval stage (i.e. on a lot by lot basis). This appears consistent with the intent of SPP5.4 which does not specify the Noise Management Plan must be done at the local structure plan. Looking at the content of a Noise Management Plan outlined in the guidelines which accompany the SPP5.4, it seems most of this information is already captured via the Noise and Vibration Assessment Study.</p> <p>Spatially, the local structure plan would not change if this Noise Management Plan were undertaken at this early stage. Opportunities for setting back of development lots further from the railway line has effectively been lost. Planning since the CCDSP 2009 has indicated urban development abutting the railway line. This situation was compounded by the rezoning to 'urban' under the MRS and there is very little scope to see a different land use response to that of a built form response on a lot by lot basis.</p> <p>Given there is no indication otherwise from the DEC and given the apparently reasonable approach to the methodology used in the Noise and Vibration Assessment Study do not recommend withholding endorsement of this local structure plan on this matter. The Department of Transport are welcome to raise their concerns with the</p>

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		<p>parameters should be adopted for development of the design concept:</p> <ul style="list-style-type: none"> <li>• Four-lane divided road, typically with two 3.5 metre traffic lanes in each direction;</li> <li>• A central median varying from 2 metres to 5.5 metres to accommodate right turn pockets at intersections;</li> <li>• 1.5 metre on-road cycle lanes in both directions; <ul style="list-style-type: none"> <li>• 5.1 m verges to accommodate shared paths (to connect / extend the existing shared path south of McTaggart Cove), street trees and underground service infrastructure;</li> <li>• Adequate chanelisation/turn pockets at intersections; and</li> <li>• Bus priority facilities in accordance with the proposed bus rapid transit requirements. Given the above design elements, a mid block reservation width of up to 32 metres may be required.</li> </ul> </li> </ul> <p>The proposed pedestrian and cycling network across both LSP areas indicates that the main internal bicycle network will primarily consist of on-road facilities. Figures 40 and 41 in the Embankment and Robb Jetty LSPs need to be modified to show shared paths on both sides of Cockburn Road. In addition, the LSPs need to show on-road bike lanes on Main St in order to be consistent with the cross section (Figure 34). These bike lanes are to continue through the Emplacement LSP area. Bicycle priority treatments are also required at signalised intersections on designated bicycle routes.</p> <p><b>Recommendations</b></p> <p>The DoT strongly requests that the City incorporate the following comments in its determination on both Local Structure Plans.</p> <ol style="list-style-type: none"> <li>1. Existing, relocated and new road traffic and pedestrian at grade and grade separated rail crossings are to be designed and located to PTA's safety and operational requirements.</li> <li>2. Fencing along the freight rail line is to be upgraded at no cost and to the satisfaction of the PTA.</li> <li>3. The Robb Jetty LSP is to clearly show a 150m impact zone on each side of the freight rail line as a support to noise and vibration mitigation</li> </ol>	<p>Department of Planning prior to their consideration of the plan.</p>

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		<p>measures.</p> <ol style="list-style-type: none"> <li>4. A revised Noise Vibration Study needs to be undertaken in accordance with SPP 5.4 to indicate the noise and vibration contours in locations along the length of the rail line in order to show where noise and vibration levels exceed the acceptable levels noted in SPP 5.4. The study will also need to provide specific recommendations on appropriate noise and vibration mitigation measures.</li> <li>5. The Robb Jetty LSP is to clearly demonstrate how noise and vibration mitigation will be addressed at subdivision and development application stages, including: <ul style="list-style-type: none"> <li>• Appropriate building materials and noise mitigation treatments are to be incorporated into Building Design Guidelines to address road and rail freight noise and vibration issues.</li> <li>• A moratorium must be included in the Certificate of Title of each development to include the Building Design Guidelines and materials used to adhere to noise attenuation measures identified in SPP 5.4.</li> <li>• The Building Design Guidelines must be included into the City's Town Planning Scheme to ensure that conditions of development are a statutory requirement.</li> </ul> </li> <li>6. Widening/upgrading of Cockburn Road is to be designed to MRWA standards, and additional land to be ceded free of cost to MRWA.</li> <li>7. The need for the traffic signals proposed for Cockburn Road is to be demonstrated and approved by MRWA prior to implementation including a SIDRA intersection analysis to ensure intersection capacity is adequate to meet the demands of regional traffic along Cockburn Road.</li> </ol> <p>DoT would appreciate receiving advice if the above inclusions cannot be adopted by Council.</p>	
30.	Department of Planning Locked Bag 2506 Perth WA 6001	<p>I refer to the City's letters dated 16 November 2012 (received by the WAPC 19 November 2012) regarding the above local structure plans (LSP).</p> <p>Please be advised that the WAPC is not prepared to endorse the proposed LSPs until such time as consideration is given to and response provided in respect of</p>	Noted

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		<p>the following:</p> <p><b>In respect of the LSP Documents – Robb Jetty</b></p> <p>Figure 1</p> <ul style="list-style-type: none"> <li>A scale should be noted, mixed use and mixed business colours are not clear enough (suggest the pink boundary to the mixed business lots is expanded),</li> <li>legend refers to Primary Regional Reserve which is not required as there is no PRR,</li> <li>no reference to the rail reserve this should be included,</li> <li>the area to the west of Robb Road identified as District Centre is not the same Urban area as shown on the MRS plan these must match (applies to all figures),</li> <li>truncations on Lot 1 Bennett Avenue are large any reason for this?</li> <li>It was envisaged that Mixed Business would be located around the Cold Stores site rather than mixed use.</li> <li>RA-C requirement coding over top of Mixed use/District Centre.</li> </ul>	<p>Noted. The scale needs to be made clear. This is a modification which should apply to all of the maps within the document. The line widths indicated on the plan accord with the Department of Planning's Digital Data and Mapping Standards, however, the applicant has indicated they can widen them.</p> <p>Noted. This should be corrected.</p> <p>The railway reserve is not contained within the Development Area. However, the plan can be modified to indicate it similar to how Robb Road (also outside the Development Area) is shown.</p> <p>Noted. This should be corrected.</p> <p>This has been queried with the applicant (Landcorp) who has advised the truncations are wider to accommodate infrastructure. This is reflected in the Servicing Strategy which shows a proposed gravity sewer diversion through this corner.</p> <p>There is a slight difference between how this area is depicted in the District Structure Plan Part 1 (2009) and the District Structure Plan Part 2. Both however, have the objective of transitioning land use spatially from the Mixed Business, to Mixed Use and then Residential.</p>



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		<ul style="list-style-type: none"> <li>Query size shape and orientation of northern most POS adjacent WC site.</li> <li>DUP adjacent railway line this is not continuous but broken up is this correct may lead to a poor outcome for pedestrians.</li> </ul> <p>Section 2.2 Use Class Permissibility If this taken from the City of Cockburn Town Planning Scheme question the need to include in this document.</p> <p>Section 3.0 Operation Date Last paragraph, object to WAPC endorsing in sixth year, delete reference to sixth year.</p> <p>6.1 Proposed Residential Density Reference has been made to Activity Centre which is not shown in the LSP area. Change to District Centre.</p> <p>7.3 Floorspace Bonus</p>	<p>A residential coding has already been included for the both these zones, however, this can be amended to RA-C0 with provisions to be specified for built form and setbacks via the Design Guidelines (as a Local Planning Policy). These will be referenced by the local structure plans.</p> <p>The local POS to the north provides a local POS opportunity for some of the northern lots. This is where some of the higher density residential is located and it is appropriate to ensure those lots have good amenity POS. The POS is also adjacent to the existing Water Corporation Pump Station. Water Corporation has recently advised the City they plan to reduce the area which is currently fenced and landscape the area. This will be a welcome addition to the POS and enable access through to Bennett Avenue to the west.</p> <p>Agree, this can be modified so the Dual Use Path is continuous.</p> <p>It is not, the land uses are the same, however the permissibility is different. There is also the introduction of a Mixed Use zone which is not currently provided for in the City's Town Planning Scheme No. 3.</p> <p>Noted. This can be modified to delete reference to sixth year.</p> <p>Noted. This can be modified.</p>

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		<p>Reference to Cockburn Coast Redevelopment area new term not defined, delete and just use Cockburn Coast District Structure Plan.</p> <p>8.0 Subdivision and Development Requirements Section refers to development requirements however there are no subdivision provisions. Is this because there are none and have been dealt with ?</p> <p>8.7 Existing Industrial Buffer Zones These are to be depicted on a plan and identified ie the Water Corporation buffer.</p> <p>11.1 Variation to the Residential Design Codes Reference is made to the variations being outlined in the approved design guidelines however variation to the residential design codes are only permissible by the WAPC.</p> <p><b>A review of the technical appendices. A review of the responses received from government agencies and other submitters.</b></p> <p>It is noted that Amendment 89 is being progressed and that in due course the current 'Industry ' zoning will be amended to 'Development' zoning with the resulting requirement for local structure plans.</p>	<p>Noted. This can be modified.</p> <p>There are subdivision provisions. For example, there is lot size guidance as well as finished floor levels which a subdivider would need to have regard to when submitting a proposal.</p> <p>There is a plan contained in Part 2 of the local structure plans (context and constraint analysis). As these buffers will change over time (for example as businesses close or technical analysis is undertaken to reduce them) it is considered this is the most appropriate place for this plan. As noted in Part 2 of the local structure plans, the City will maintain current mapping via its constraints module in its online mapping system.</p> <p>Do not agree this is correct. Local Governments are able to make variation to the Residential Design Codes also. Examples are Detailed Area Plans and Local Planning Policies.</p> <p>Noted, modifications to Amendment 89 were recently received from the Minister for Planning. These have been made and submitted back for consideration. The local structure plans will not be endorsed unless the gazettal of the amendment has occurred.</p> <p>On the basis of this submission there have been minor changes recommended. They are detailed above.</p>
31.	Department of Environment and	I refer to your letter dated 19 November 2012, referring the above proposed local structure plans to the Department of Environment and Conservation (DEC) for	

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	<p>Conservation PO Box 1167 Bentley Delivery Centre WA 6983</p>	<p>comment. DEC has reviewed the documents for the Robb Jetty and Emplacement local structure plans and this submission relates to both areas.</p> <p><b>General</b> The Emplacement local structure plan (LSP) area is located adjacent to the Manning Lake area, which comprises <i>Bush Forever Site No. 247 "Manning Lake and Adjacent Bushland, Hamilton Hill/Spearwood"</i> and forms part of Beeliar Regional Park, which is managed by the City of Cockburn and DEC for conservation and recreation purposes. A proposed primary regional road (Cockburn Coast Drive) separates the Manning Lake area from the structure plan area. DEC has provided advice in relation to the earlier <i>Metropolitan Regional Scheme amendment No. 1180/41</i> (WAPC Ref. 809-2-23-17 Pt 1), which covers these structure plan areas, and provided advice to the City of Cockburn in relation to the <i>Cockburn Coast District Structure Plan (part 2) and Town Planning Scheme no. 3 - Amendment no. 89 (proposed zoning changes to Cockburn coast industrial area)</i> in a letter dated 30 November 2011. The relevant aspects of the earlier advice and additional information are provided for your consideration.</p> <p><b>Native vegetation management</b> DEC notes an ecological assessment report (Appendix C) has been prepared for the Emplacement LSP area; however a copy of this report has not been provided to DEC. The Emplacement LSP report - Part 2 Explanatory 3.1 Environmental Assets and Constraints: outlines that there are patches of vegetation in good condition with 2.96ha vegetation to be cleared and vegetation type 1 located on the eastern side of the project site has similarities to DEC-listed threatened ecological community [SCP 26a]. DEC therefore recommends that detailed flora and vegetation surveys of all potentially affected areas of native vegetation be conducted by an environmental consultant, in accordance with the Environmental Protection Authority's (EPA's) <i>Guidance Statement 51 - Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia</i>. The survey should determine the presence or otherwise of priority or other significant flora and plant assemblages. If such flora and vegetation is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts.</p> <p>DEC has reviewed the ecological assessment for the Robb Jetty LSP area. The field study was conducted in March 2012, which is not considered the optimal time for flora surveys; therefore not considered conducted in accordance with EPA's Guidance Statement 51. However, it is noted that the Robb Jetty study site is highly modified and degraded due to a history of multiple disturbances and development. Therefore DEC concurs the site comprises limited environmental</p>	<p><b>Supported</b> It is recommended that a spring flora and vegetation survey be undertaken within the Emplacement LSP, prior to subdivision or development of the land (where development proposes works to the land). It is recommended that the Emplacement LSP report be modified to reflect this requirement, and that Council advise landowners of the requirement to ensure they can factor it into the timing of any proposals.</p> <p><b>Supported</b> No further changes are recommended as a result of this comment.</p>

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		<p>value.</p> <p>Regardless of the outcomes of the above recommended flora and vegetation surveys for the Emplacement LSP area, DEC recommends that areas of native vegetation in good or better condition within the structure plan areas be retained and incorporated into future public open space (POS).</p> <p>Additionally, any clearing of native vegetation requires a clearing permit obtained from DEC, unless of a kind that is exempt in accordance with Schedule 6 of the <i>Environmental Protection Act 1986</i> or <i>Regulation 5 of the Environmental Protection (Clearing of Native Vegetation) Regulations 2004</i>. DEC's Native Vegetation Conservation Branch should be contacted regarding the possible need for a clearing permit.</p> <p><b>Fauna management</b> The Manning Lake area and adjoining bushland serves as habitat for a variety of</p>	<p><b>Not Supported</b> The proposed areas of POS within the Emplacement LSP are consistent with the CCDSP Part 2. Vegetation within the green POS links will be retained where possible to provide a physical and ecological link between the foreshore and Beeliar Regional Park. While it may be possible for some vegetation to be retained within POS, the key function of the proposed POS is to provide a variety of recreational functions for residents and visitors, cognisant of the fact that it will be a high density environment. The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense and diverse urban environment with high levels of accessibility.</p> <p><b>Noted</b> No further changes are recommended as a result of this comment.</p> <p><b>Noted</b> Obligations under the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act) are noted.</p> <p><b>Supported</b> The limited fauna habitat within the Rob Jetty</p>

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		<p>native fauna, which the proposed Emplacement LSP area has the potential to impact upon, as development proceeds. The Cockburn Coast District Structure Plan (DSP) area is known to serve as roosting and foraging habitat for the threatened Carnaby's cockatoo (<i>Calyptorhynchus latirostris</i>). In addition, the area is likely to support the common and widespread <i>Lomandra maritima</i> and possibly <i>Lomandra hermaphrodita</i>, either of which can support populations of the threatened Graceful sun-moth (<i>Synemon gratiosa</i>). The Stage 1 <i>Flora and Vegetation Assessment (ENV Australia, 2008)</i> indicates that <i>Lomandra maritima</i> has been identified within the Cockburn Coast DSP area. Both Carnaby's cockatoo and the Graceful sun-moth are listed as 'fauna that is rare or likely to become extinct' under the <i>Wildlife Conservation Act 1950</i>. The Priority 3 Lined Skink (<i>Lerista lineata</i>) has also been recorded in the Manning Lake bushland.</p> <p>DEC notes an ecological assessment report (Appendix C) has been prepared for the Emplacement LSP area; however a copy of this report has not been provided to DEC. The Emplacement LSP report - Part 2 Explanatory 3.1 Environmental Assets and Constraints: outlines that there are patches of vegetation in good condition with 2.96ha of good quality feeding habitat for Carnaby's cockatoo to be cleared. DEC therefore recommends that, prior to structure planning being finalised, the proponent undertake a detailed fauna survey in accordance with EPA <i>Guidance Statement No. 56 - Terrestrial Fauna Surveys for Environmental Impact Assessment in Western Australia</i>. If habitat suitable for conservation significant fauna is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts</p> <p>DEC has reviewed the ecological assessment for the Robb Jetty LSP area. It is noted that the Robb Jetty study site is highly modified and degraded due to a history of multiple disturbances and development. Therefore DEC concurs the site comprises limited fauna habitat value. Both Carnaby's cockatoo and the Graceful sun-moth are protected by the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities (SEWPaC) to determine what responsibilities they have under the EPBC Act.</p> <p><b>Boundary interface treatment (with Beeliar Regional Park)</b> It is noted that the Emplacement LSP area abuts the proposed Cockburn Coast Drive road reserve. Cockburn Coast Drive will represent the future western boundary of Beeliar Regional Park at the Manning Lake area and if this road is to</p>	<p>LSP is noted.</p> <p><b>Not Supported</b> These comments do not relate to the proposed Emplacement LSP. The DEC is responsible for the management of the</p>

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		<p>proceed, DEC considers it important that a high standard of visual amenity be created and maintained along this parkland interface. In this regard, the design of this road should minimise its visual impact as far as possible, and an emphasis should be placed on vegetating road batters and rehabilitating existing degraded areas with appropriate native plant species of local provenance. It is also recommended that construction of a dual use pathway along/adjacent to Cockburn Coast Drive be considered. DEC would prefer to see such a pathway located on the eastern side of this road (adjacent to Beeliar Regional Park).</p> <p>Until such time that the Cockburn Coast Drive is constructed, the proponent should ensure there is adequate fencing between any development site and areas retained for conservation, and between any development site and Beeliar Regional Park. Additionally, no vegetation, earth spoil or any other debris is to be disposed of within the adjacent regional park.</p> <p><b>Pedestrian and cyclist networks</b> DEC notes the proposed linkages between the structure plan area and Manning Lake area as depicted in the Emplacement LSP map, which illustrates shared pedestrian/cycle connections (including one bridge and one underpass). DEC recommends that planning for pedestrian and cycle trails through the structure plan area considers and is complementary to the <i>Beeliar Regional Park Management Plan 2006</i>. DEC supports the proponent's commitment to maintain connectivity for pedestrians and regional park visitors between both sides of the proposed Cockburn Coast Drive. In regard to the design of the above mentioned underpass, the proponent is requested to liaise with the Department of Fire and Emergency Services (DFES) to ensure adequate height and width specifications to allow access for fire and emergency vehicles.</p> <p><b>Site contamination</b> Due to previous industrial land uses over a long period of time, there is considerable potential for widespread soil and/or groundwater contamination within the structure plan area. A significant number of lots are shown as Reported Contaminated Sites on DEC's Contaminated Sites Database. Of these, a number are "Awaiting Classification", while others are listed as "Possibly Contaminated - Investigation Required".</p>	<p>Beeliar Regional Park and Main Roads are responsible for the design and construction of any future road within the Primary Regional Road Reserve. Therefore ensuring a sensitive interface between the future road and the Reserve are beyond the control of any landowner within the Emplacement LSP.</p> <p><b>Supported</b> It is recommended that additional provisions be included in Section 8 (Part 1) requiring development proposals to ensure adequate interface, including fencing, to the Primary Regional Road Reserve in order to protect the conservation value of the Beeliar Regional Reserve. In regard to dumping on either reserves this is an illegal act and the proposed Emplacement LSP is not the appropriate document to reiterate this.</p> <p><b>Noted</b> The location of the dual use paths and the pedestrian underpasses connects to the existing compacted limestone paths that run within the Primary Regional Road Reserved and the Beeliar Regional Reserve. The existing paths generally accord with the <i>Beeliar Regional Park Management Plan 2006</i>. More detailed future designing of the underpass will include liaison with DFES.</p> <p><b>Noted</b> Section 4.5 of the LSP reports notes this requirement, and there are no recommended changes as a result of this submission.</p>

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		<p>DEC notes that GHD Pty Ltd has undertaken a Preliminary Assessment of all lots within the Emplacement and Robb Jetty LSP areas. Further to the assessment, site investigations are required for some lots within the LSP areas (e.g. Lot 2108 Bennett Avenue, Lot 123 Cockburn Road and Lot 103 Emplacement Crescent). Investigations for soil and groundwater contamination will therefore need to be carried out in accordance with DEC's <i>Contaminated Sites Management Series</i> guidelines. Where these investigations identify soil and/or groundwater contamination that requires remediation to render the site suitable for the proposed use, such remediation (including validation of remediation) will need to be completed to the specifications and satisfaction of DEC's Contaminated Sites Branch, and in accordance with the requirements of the <i>Contaminated Sites Act 2003</i>. DEC's Contaminated Sites Branch should be contacted regarding any site contamination queries.</p> <p><b>Drainage management</b> In planning for future subdivisions, it should be noted that no drainage infrastructure is to be placed within the adjoining Beeliar Regional Park, nor is there to be any direct discharge of drainage waters (including road drainage) into the regional park. This requirement is particularly relevant in relation to the possible future design and construction of proposed Cockburn Coast Drive.</p> <p>DEC notes that Local Water Management Strategies (LWMS) have been prepared for the Emplacement and Robb Jetty LSP areas. These should be submitted to the Department of Water for review and approval.</p> <p><b>Noise management</b> It is noted future development within the Emplacement LSP area is proposed against Cockburn Coast Drive, which could result in potential noise impacts from road traffic. It is noted that a Road Noise Assessment (Herring Storer Acoustics 2011) has been prepared for the Cockburn Coast Project, which incorporates the Emplacement and Robb Jetty LSP areas; DEC has not reviewed this document. However, there is a need to comply with <i>WAPC State Planning Policy 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning (2009)</i>. The Draft <i>EPA Guidance Statement No.14 - Road and Rail Transportation</i></p>	<p><b>Noted</b> The Local Water Management Strategy (LWMS) does not propose any stormwater discharge into Beeliar Regional Reserve.</p> <p><b>Noted</b> The Department of Water has provided comments on the LWMS.</p> <p><b>Noted</b> The proposed Emplacement LSP has been prepared in accordance with <i>State Planning Policy 5.4</i> and requires sensitive development in proximity to Cockburn Road and the freight rail to comply with the requirements of SPP5.4.</p>

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		<p>Noise (1998) may also be of assistance.</p> <p><b>Fire management</b> Necessary fire management requirements should be provided for within the structure plan areas, in accordance with the (Interim) <i>Planning for Bushfire Protection Guidelines</i> (Edition 2- Western Australian Planning Commission and Fire and Emergency Services Authority, May 2010) and any other relevant policies, and on the advice of DFES. DEC supports having a perimeter road between residential development and POS, for reasons of public safety, protection of bushland within the POS and fire safety for residents. The perimeter road reserve should accommodate all road, dual use path/footpath and drainage infrastructure.</p> <p><b>Further Comments received 10 January 2013:</b></p> <p>I refer to your letter dated 19 November 2012, referring the above proposed local structure plans to the Department of Environment and Conservation (DEC) for comment. DEC reviewed the documents for the Robb Jetty and Emplacement local structure plans and submitted a submission that related to both areas on 21 December 2012. At the time of DEC submitting the submission, a copy of the ecological assessment report (Appendix C) for the Emplacement local structure plan (LSP) area had not been provided. DEC has since received and reviewed the ecological assessment report (<i>Report for Hilltop/Emplacement Crescent-Ecological Assessment June 2012</i>) prepared by GHD and provides the following additional advice on flora and fauna management for your consideration.</p> <p><b>Flora management</b> DEC has reviewed the document <i>Report for Hilltop/Emplacement Crescent-Ecological Assessment</i> (GHD 2012) and notes the field study was conducted on 16 May 2012, which is not considered the optimal time for flora surveys within the Swan Coastal Plain Bioregion; therefore not considered to be conducted in accordance with Environmental Protection Authority's (EPA's) <i>Guidance Statement 51 - Terrestrial Flora and Vegetation Surveys for Environmental Impact Assessment in Western Australia</i>. GHD (2012) outlines that there are patches of native vegetation in good condition (approximately 2.96ha of vegetation in total) and the identified vegetation type 1 located on the eastern side of the project site (Emplacement LSP area) has similarities to DEC-listed threatened ecological community [SCP 26a]. DEC considers that to accurately determine the floristic community types present at the project site, plots need to be established and scored (typically spring and late spring), and data analysed using appropriate statistical techniques. An appropriately timed flora survey in</p>	<p><b>Supported</b> As per the <i>Planning for Bushfire Protection Guidelines</i> because the Emplacement LSP proposes development within 100m of vegetation which may be considered a 'moderate to extreme' bush fire hazard the LSP should be supported by a bush fire hazard assessment. It is therefore recommended that a bush fire hazard assessment is prepared and the LSP be amended to consider the outcomes of the assessment.</p> <p><b>Supported</b> It is acknowledged that the Flora and Vegetation Survey contained within the Ecological Assessment was not undertaken in accordance with EPA Guidance Statement 51. It is therefore recommended that a spring flora and vegetation survey be undertaken prior to any subdivision or development (involving works to the land), and that affected landowners be advised of this requirement.</p>



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		<p>accordance with <i>Guidance Statement 51</i> with methodology consistent with Gibson <i>et al.</i> (1994) is required to determine the presence of priority and/or threatened ecological communities within the project site.</p> <p>In addition, GHD (2012) indicates that rare flora (e.g. <i>Caladenia huegefit</i>) and priority flora (e.g. <i>Dodonaea hackettiana</i>) are likely to occur within the Emplacement LSP area. Therefore, DEC recommends that another flora and vegetation survey of all potentially affected areas of native vegetation be conducted by an environmental consultant, in accordance with <i>Guidance Statement 51</i>. The survey should determine the presence of priority flora, rare flora or other significant flora. If such flora and vegetation is present on site, appropriate action should be undertaken to protect it, or to mitigate impacts.</p> <p>Regardless of the outcomes of the above recommended flora and vegetation survey for the Emplacement LSP area, DEC recommends that areas of native vegetation in good or better condition within the LSP area be retained and incorporated into future public open space (POS).</p> <p><b>Fauna management</b> The Cockburn Coast District Structure Plan (DSP) area is known to serve as roosting and foraging habitat for the threatened Carnaby's cockatoo (<i>Calyptorhynchus latirostris</i>). In addition, the Stage 1 <i>Flora and Vegetation Assessment (ENV Australia, 2008)</i> indicates that <i>Lomandra maritima</i> has been identified within the Cockburn Coast DSP area. Therefore, the Emplacement LSP area may support the common and widespread <i>Lomandra maritima</i> which is</p>	<p><b>Not supported</b> The proposed areas of POS within the Emplacement LSP are consistent with the CCDSP Part 2. Vegetation within the green POS links will be retained where possible to provide a physical and ecological link between the foreshore and Beeliar Regional Park. While it may be possible for some vegetation to be retained within POS, the key function of the proposed POS is to provide a variety of recreational functions for residents and visitors, cognisant of the fact that it will be a high density environment. The local impact of some clearing of vegetation in the Emplacement Local Structure Plan area must be balanced against the outcomes of the district structure planning for Cockburn Coast, which seek to facilitate a dense and diverse urban environment with high levels of accessibility.</p> <p><b>Noted</b> Obligations under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) are noted.</p>

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		<p>suitable habitat for the threatened Graceful sun-moth (<i>Synemon gratiosa</i>). GHD (2012) states (Table 4, page 2) "There is 2.96 ha of high quality Black Cockatoo foraging habitat (<i>Banksia sessilis</i> woodland) present within the Project Site. Clearing of the Project Site will adversely affect this foraging habitat. The 2.96 ha of high quality foraging habitat is connected to a larger strip of bushland including Manning Park, which provides foraging habitat for Black Cockatoos. The Project Site has also been mapped by the Department of Planning as potential feeding vegetation for Carnaby's Black Cockatoo on the Swan Coastal Plain (Department of Planning, 2011). Therefore, while the majority of the Project Site is degraded and borders developed areas, due to its linkage to other foraging habitat the 2.96 ha extends the available protected habitat in Beeliar Regional Park. Clearing the 2.96 ha will have an impact on the species' regional feeding resources, but it is unlikely to be critical in terms of the species long term survival. The majority of the foraging habitat is located on the limestone outcrop area in the east of the Project Site, and within a fenced industrial area. It is recommended that clearing of the habitat in these areas be minimized or avoided if possible."</p> <p>DEC concurs that clearing of high quality foraging habitat for Carnaby's cockatoo should be minimised or avoided, if possible; and recommends that it is retained and incorporated into future POS.</p> <p>DEC reiterates that both Carnaby's cockatoo and the Graceful sun-moth are protected by the Commonwealth's <i>Environment Protection and Biodiversity Conservation Act 1999</i> (EPBC Act). Therefore, regardless of any decision under Western Australian planning or environmental approvals processes, the proponent should contact the Commonwealth Department of Sustainability, Environment, Water, Population and Communities (SEWPaC) to determine what responsibilities they have under the EPBC Act.</p>	
32.	Water Corporation PO Box 100 Leederville WA 6902	<p><b>Water Corporation Bennett Avenue Waste Water Pump Station (WWPS) odour buffer</b></p> <p>Section 4.5 of the RJLSP deals with industrial buffers in general and more specifically with the Bennett Avenue WWPS. Figure 25 shows the Bennet Ave WWPS buffer being 50 metres measured from the centre point of the wet well. This 50m buffer setback is at odds with the buffer shown in the <i>Cockburn Coast District Structure Plan</i> adopted by the Western Australian Planning Commission (WAPC), which shows a 50 metre buffer from the boundary of the site. This setback also contradicts the City's <i>Cockburn Coast District Structure Plan</i>, which is consistent with the WAPC plan.</p> <p>The RJSP proposed buffer 50m setback does not take into consideration a</p>	<p><b>Supported</b></p> <p>The Odour Report that has been submitted seeks to further analyse and further refine the buffer in accordance with draft State Planning Policy 4.1 State Industrial Buffer ("SPP4.1") and the Environmental Protection Authority's Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses ("GS No.3").</p> <p>GS No. 3 notates the following impacts apply: Odour, Noise, Gas and Risk and the key</p>

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		<p>previous determination by the Minister for Water that the 50 metre buffer is to be measured from the boundary of the WWPS site. This determination has previously been conveyed to LandCorp, the City of Cockburn and adjoining land owners both verbally and in written communications. However, some landowners adjoining the WWPS site have made separate representations to the Minister in an attempt to further reduce the odour buffer. In response, the Minister has recently instructed the Corporation to accept a reduction of the buffer from 50 metres to a 25 metre buffer measured from the <u>boundary</u> of the site.</p> <p>The RJSP report includes some explanation of the application of the Environmental Protection Authority (EPA) Guidance Statement 3. Other existing industrial land uses in the area have been acknowledged and management measures have been put in place to address buffers from these land uses measured from the cadastral <u>boundaries</u> of these properties. It is not clear why the RJSP has selectively interpreted the EPA Guidance Statement to apply a buffer measured from the centre of the Bennett Ave Pump Station wet well. This approach is prejudicial to the Corporation and does not provide any flexibility for the Corporation to utilise other parts of the site for pump station works. Measuring the 50m radius odour buffer from the centre of the wet well as being the only source of potential odour precludes the development of any additional odour emitting assets on the WWPS site in the future.</p> <p>The package of information contained on the CD accompanying the RJSP includes an odour report undertaken by a consultant on behalf of the affected landowners. This report has not been subjected to an independent assessment and appears to conclude that the odour levels from the WWPS are currently low and acceptable. This situation is primarily attributable to the success of an odour scrubbing unit that the Corporation installed at the WWPS in early 2011. The odour report and associated modelling has modelled only the current reduced odour levels and has not accounted for future rises in wastewater flows through this main pump station in the longer term. The report also assumes that future wastewater flow and odour increases at the WWPS will be attenuated by further Water Corporation investment in additional odour controls to manage odours within the proposed 50m radius. This has not been agreed or incorporated into the Corporation capital planning for this WWPS and the long-term success of the current odour scrubbing unit is yet to be evaluated for much larger flows.</p>	<p>agency for advice or approvals is the local government.</p> <p>The Technical Analysis submitted is predominantly concerned with the issue of odour and pays very limited attention to the issues of gas, noise and risk.</p> <p>City officers are not comfortable endorsing the technical analysis at this stage given it has not given sufficient regard to three of the four impacts this infrastructure presents. The landowners are welcome to lodge an updated technical analysis which does consider all these issues sufficiently, should they choose to apply for subdivision or development approval. In the interim it is recommended that the local structure plan be modified to reflect the boundary of a 25m buffer as measured from the property boundary, and no sensitive land uses will be deemed acceptable in this area, and it is recommended</p>

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		<p><b>Infrastructure Coordination, Servicing and Staging</b>  The updated servicing reports attached to both LSPs are noted. Further discussions and arrangements will need to be made with the Corporation regarding the rationalisation and relocation of existing water and wastewater pipes through the area. However, the information and mapping provided in this report is sufficient for this stage of the planning process.</p> <p><b>Emplacement LSP – water supply planning</b>  The findings of the Corporation's recent water supply planning review for the Hamilton Hill gravity water supply scheme and the water mains upgrades relevant to the Cockburn Coast area, have largely been reflected in the LSP and servicing report. However, it should be noted that any land above 33m AHD will not be able to be served off the gravity scheme, even after the successful completion of these water mains upgrades along Forrest Rd (DN500) and Cockburn Rd (DN375). Small portions of the eastern edges of the 'R160' sites could be affected by this supply limit. Developers of land above 33m AHD will need to investigate other measures (e.g. pressure boosters) to provide adequate pressure to developments, particularly for proposed multi-storey buildings in this area. The areas above 33m AHD are shown on the ached plan. Please call me discuss if necessary. I will be sending the formal letter in the mail tomorrow morning.</p> <p><i>(Additional comments received):</i></p> <p>Thank you for your letter of 19 November 2012 inviting comments from the Water Corporation on the <i>Robb Jetty Local Structure Plan</i> and the <i>Emplacement Local Structure Plan</i>. The Corporation offers the following comments in addition to the advice sent to the City via e-mail on 17 December.</p> <p>The Corporation has previously provided advice to Landcorp, the City and the developer's engineering consultants regarding water and wastewater infrastructure planning for this area, and in particular in relation to the existing Bennett Avenue Waste Water Pump Station and its odour buffer. The servicing issues relevant to the Robb Jetty and Emplacement precincts are largely reflected in the LSP report and the accompanying <i>Infrastructure and Servicing Report</i> prepared by Wood and Grieve Engineering Consultants.</p> <p>Over the past 2-3 years the Water Corporation has reviewed its water and</p>	<p><b>Infrastructure, Servicing and Staging</b>  For Robb Jetty they have noted that upon development rationalisation and relocation of existing water and wastewater pipes through the area will be required, however the information and mapping is sufficient at this point in the planning process. We agree with this comment and further design would occur as part of the normal development process.</p> <p><b>Emplacement Crescent LSP – water supply planning</b>  Water Corporation has undertaken further water supply modelling for the Hamilton Hill gravity water supply scheme. It is noted that two small R160 sites at the Eastern extremity of the LSP area appear to be affected by the Water Corporation RL33mAHD height contour. Above this level it is noted they may not be able to provide water at a sufficient head (pressure). Development of these sites will involve substantial earthworks due to the steep nature of the existing ground. Therefore, upon final design it may be that these sites are below the RL33mAHD level. Failing that, because they are high density sites and likely multi storey buildings, the design would incorporate water supply pressure boosting systems.</p>

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		<p>wastewater infrastructure planning for this area taking into account the development yields and indicative development timing for the Cockburn Coast development area. This planning may need to be further refined in consultation with individual land developers, as more detailed planning is progressed for the various development sites. Staging of water and wastewater headworks and upgrades, in particular the timing of major items such as staged extension of a water distribution main along Cockburn Rd (DN375 water main extension) and later along Forrest Rd (DN500 water main), will depend on the progression of development and water demands. The capacity and progressive upgrading of the Bennett Avenue waste water pump station should not be an impediment to the timing of the initial stages of development. The Corporation will undertake upgrades to the WWPS as required and when capital funds have been scheduled. Further comments follow in relation to some outstanding matters and issues that require further detailed consideration.</p> <p><b>Fremantle Sewer District Waste Water Pump Station No.2 - Bennett Avenue</b>  As the City would be aware, the Water Corporation owns and operates a large waste water pumping station (WWPS) at the corner of Bennett Avenue and Rollinson Road. The WWPS is a permanent facility that was built in this location at a time when the surrounding land was used for industrial purposes. The WWPS is the final receival point for wastewater generated from the Fremantle Sewer District and is therefore critical public infrastructure. The WWPS and its associated onsite, underground emergency storage tanks are potential sources of odour.</p> <p>Encroachment of incompatible land uses within close proximity to the WWPS may place residents in situations of unacceptably high odour, undermine significant investment in this infrastructure, decrease the ultimate operating capacity of the pump station, and thereby risk the achievement of the planned ultimate urban densities within the City of Fremantle and the Cockburn Coast area.</p> <p>In October 2011, in response to approaches from adjoining landowners, the Minister for Water requested the Water Corporation to limit the planned ultimate capacity of the WWPS to a maximum of 350 Litres/second, in order to avoid the need to implement a larger 150m radius odour buffer required under EPA Guidance Statement No.3 for pump stations &gt;350l/s. The implication of this is that the projected ultimate wastewater flow from the Fremantle Sewer District will not be able to be accommodated through the Bennett Avenue WWPS and alternative measures will need to be explored to deal with the long-term wastewater flows, including the possibility of diverting wastewater into neighbouring sewer catchments. The Water Corporation currently does not have any planning in place or capital works programmed for the infrastructure that will be required to effect</p>	<p><b>Not supported</b>  The City is not responsible for the delivery of wastewater infrastructure and therefore it is not recommended that the City include such an item as a Developer Contribution. Pursuant to Clause 6.3.17 of the Scheme the City is responsible for any shortfall in the total cost contributions when all costs contributions have been made or accounted for. Inclusion of items that the City is not responsible for delivering means that the City</p>

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		<p>such a diversion. Further detailed engineering investigations will be required to explore if and how wastewater can be diverted and to determine the cost of these works. The Council is requested to include this item in the developer contributions scheme for the Cockburn Coast development area.</p> <p>The size and configuration of the required odour buffer around the Bennett Avenue WWPS has been a matter of debate for some time. The Council's last decision on the DSP and the Cockburn Coast Master Plan indicated a buffer of 50m radius measured from the WWPS site boundary, which the Corporation supported. Section 4.5 of the Robb Jetty LSP deals with industrial buffers in general and more specifically with the Bennett Avenue WWPS. Figure 25 shows the Bennett Ave WWPS buffer being measured as 50m from the centre point of the wet well. This buffer setback is at odds with the buffer shown in the <i>Cockburn Coast District Structure Plan</i> and the Masterplan.</p> <p>The 50m buffer proposed in the Robb Jetty LSP does not take into consideration a previous determination by the Minister for Water that the buffer is to be measured from the boundary of the WWPS site. The Minister's determination has previously been conveyed to LandCorp, the City of Cockburn and adjoining land owners both verbally and in writing. However, landowners adjoining the WWPS site have since made representations to the Minister for Water in an effort to further reduce the extent of the buffer. In response, the Minister has recently requested the Corporation to accept a reduction of the buffer from a 50m to 25m radius measured from the boundary of the site. The City is requested to reflect the Minister's decision and the revised odour buffer in the DSP and the Robb Jetty LSP.</p> <p>The Robb Jetty LSP report includes some explanation of the application of the Environmental Protection Authority (EPA) Guidance Statement 3 in relation to industrial buffers. It is noted that other existing industrial land uses in the Cockburn Coast area have been acknowledged and management measures have been put in place to protect these land uses with buffers measured from the cadastral boundaries of these properties. It is not clear why the LSP has selectively interpreted the EPA Guidance Statement No.3 to apply a buffer measured from the centre of the Bennett Ave Pump Station wet well. This approach is prejudicial to the Corporation and does not provide any flexibility for the Corporation to utilise other parts of the site for pump station works.</p> <p>The package of information contained on the CD accompanying the Robb Jetty LSP includes an odour report undertaken by a consultant on behalf of the affected landowners. This report has not been subjected to an independent assessment</p>	<p>is will be left liable for any shortfalls on the delivery of items outside of its control.</p> <p><b>Supported</b>  The Odour Report that has been submitted seeks to further analyse and further refine the buffer in accordance with draft State Planning Policy 4.1 State Industrial Buffer ("SPP4.1") and the Environmental Protection Authority's Guidance Statement No. 3 Separation Distances between Industrial and Sensitive Land Uses ("GS No.3").</p> <p>GS No. 3 notates the following impacts apply: Odour, Noise, Gas and Risk and the key agency for advice or approvals is the local government.  The Technical Analysis submitted is predominantly concerned with the issue of odour and pays very limited attention to the issues of gas, noise and risk.</p> <p>City officers are not comfortable endorsing the technical analysis at this stage given it has not given sufficient regard to three of the four impacts this infrastructure presents. The landowners are welcome to lodge an updated technical analysis which does consider all these issues sufficiently, should they choose to apply for subdivision or development approval. In the interim it is recommended that the local structure plan be modified to reflect the boundary of a 25m buffer as measured from the property boundary, and no sensitive land uses will be deemed acceptable in this area, and it is recommended</p>

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		<p>and appears to conclude that the odour levels from the WWPS are currently low and acceptable. This situation is primarily attributable to the success of an odour scrubbing unit that the Corporation installed at the WWPS in early 2011. The odour report and its conclusions are based on an assessment of the current reduced odour levels and has not accounted for future increases in wastewater flows through this main pump station, and hence potential increases in odour emissions in the longer term. The report also assumes that future wastewater flows and odour increases at the WWPS will be attenuated by further Water Corporation investment in additional odour controls to manage odours within the proposed 50m radius. This has not been agreed to or incorporated into the Corporation's planning for this WWPS and the long-term success of the current odour scrubbing unit is yet to be evaluated for much larger flows.</p>	
33.	<p>Adele Carles MLA Fremantle Shop 1, Queensgate Centre, William Street FREMANTLE WA 6160</p>	<p>Please accept these documents as my submission for the structure plans within the Cockburn Coast development area: Robb Jetty; and Emplacement</p> <p>My views are outlined in the two previous submissions (attached). In addition I would like to raise the matter of new evidence that has arisen in relation to rapid sea level rise in Western Australia – particularly around Perth. The recently released federal report <b>State of Australian Cities 2012</b> (<i>Major Cities Unit, Department of Infrastructure and Transport, Australian Government</i>) details disturbing evidence that sea levels are rising between 9mm and 10mm per year, while the global average is about 3mm per year (sea media release attached). This rapid rate of sea level rise has been attributed to the interaction of factors involving Perth's sinking ground levels due to excessive groundwater abstraction and the rise in sea levels due to climate change.</p> <p>The result is that low lying coastal areas around Fremantle will be subject to more intense sea level intrusion at a more rapid rate than the rest of the country. This new evidence must result in a reappraisal of coastal setbacks for the purposes of planning in the Cockburn Coast Structure Plan. The current prescribed coastal setback for planning are out-dated and will be insufficient to protect built structures from damage and inundation in the near coast areas of the structure plan.</p> <p>I submit that this is the ideal opportunity for planning authorities to review coastal setbacks for planning more generally and to adjust the specific setbacks for the Cockburn Coast structure plan specifically. I would also like to reiterate my opposition to the inclusion of a public marina at the front of the old power station due to the loss of beach it creates and because of the impacts of sea level rise in</p>	<p>The applicant has provided a Coastal Vulnerability Assessment (CVA) with their local structure plan. The document has been prepared by an appropriately qualified person and a company who specialise in these assessments.</p> <p>The assumed sea level rise in the CVA is 0.9m to 2110. This is as per the current requirements of the Department of Planning. When the State Planning Policy 2.6 (SPP2.6) State Coastal Planning Policy was gazetted in 2003 a sea level rise of 0.38m needed to be included in assessments. Based on updated data, the Department of Planning issued a new Position Statement in 2010 to increase the sea level rise to be factored into assessments to 0.9m to 2110. In February 2012, the Department advertised a new draft SPP2.6, this reiterates the requirement for 0.9m to 2110. No advice to the contrary has been provided to the City by the Department and therefore it is prudent to apply an</p>

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		<p>decades to come.</p> <p><i>(Attachment 1 – Media Release on Sea Level Rise – 5 Dec 2012)</i></p> <p>Fremantle Independent MP Adele Carles has demanded an urgent reassessment of the Cockburn Coast Local Structure Plan in light of new scientific evidence about sea levels rising in Perth at three times the global average.</p> <p>Disturbing new statistics from the State of Australian Cities report show readings since 1993 have indicated sea levels are rising by between 9mm and 10mm per year, while the global average is about three millimetres per year.</p> <p>Ms Carles says the new startling information renders the current plan redundant and says planners need to go back to the drawing board.</p> <p>“These new statistics are alarming and must be taken into account while planning for the development of the Cockburn Coast,” Ms Carles said.</p> <p>“Coastal setbacks may need to be increased as current planning regulations for coastal setback and sea level rise are outdated and don’t reflect the new information that is now available.”</p> <p>“The new warnings also vindicate my opposition to a marina on this part of the coast, which is already overstretched,” Ms Carles said</p> <p><i>(Attachment 2 - previous submission on Metropolitan Region Scheme Amendment)</i></p>	<p>assumed sea level rise of 0.9m to 2110.</p> <p>Neither the Robb Jetty nor the Emplacement Local Structure Plans include the power station building. Any proposals for the power station (whether with marina proposals or not) will be the subject of future applications. Council has made it very clear in its proposed town planning scheme provisions, there are a variety of issues which must be discussed should a marina (or similar) coastal feature be proposed, including environmental and social feasibility.</p> <p>Noted the submissioner has included a copy of a media release they have made. This is considered to be provided for information as background the submission above and does not warrant further response. The matter of sea level rise is discussed above.</p>



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		<p><i>1180/41 Cockburn Coast District Structure Plan Area. May 2010)</i></p> <p><b>Executive Summary</b>  This submission outlines a number of concerns and suggestions in relation to the Cockburn Coast District Structure Plan (CCDSP) as it appears in the Metropolitan Region Scheme Amendment 1180/41. Comments are also made in relation to planning issues surrounding the South Fremantle tip site and the Fremantle Chalet Village (Caravan Park). The tip site and the Fremantle Chalet village both fall within the boundaries of the CCDSP and share similar environmental problems, yet the planning considerations of the former tip site are side-stepped in the documentation of the CCDSP. The South Fremantle tip-site is regarded as being subject to separate planning measures by the City of Fremantle and the chosen planning option for the site differs from that which has been made public previously. There is clearly a need to consider sensitive and ecologically sustainable development of this locality given historic use as an industrial area with noxious industry land-use. However the need to remediate or manage contaminated land should not compromise the remaining high conservation values of other land within the structure plan area or resident's health or amenity. With a view to ensuring more sustainable outcomes for the structure plan I have provided comment on;</p> <p>The urgent need to fund dedicated infrastructure for a light rail network to service the structure plan development and alleviate congestion on Hampton Road and throughout Fremantle.</p> <p>Strong community opposition to residential development of the South Fremantle landfill site due to health and safety issues and environmental impacts.</p> <p>The need for greater setbacks between the coastline and the key residential and commercial developments of the CCDSP.</p> <p>Real consideration to be given to the relocation of the Fremantle Chalet Village permanent residents to affordable accommodation within the CCDSP that provides improved amenity and security (the 'village' is currently located on top of a medical and municipal waste dump).</p> <p>The need to integrate renewable energy systems in to the development at district scale where possible and certainly at lot scale for commercial and residential structures.</p> <p><b>Transit: Light rail vs. buses</b> I am disappointed to see that the WAPC is still pursuing the option of road-based transit in the CCDSP. The response below (from the public submissions report) indicates that while the vast majority of respondents (27:3) were encouraging the implementation of light rail to link the</p>	<p>Noted the submissioner has included a copy of a submission they have made on the Metropolitan Region Scheme amendment advertised by the Department of Planning.</p> <p>This is considered to be provided for information as background the submission above and does not warrant further response. The Department of Planning have already responded to the submissions raised as part of that amendment process.</p> <p>A copy of the submissions report on MRS Amendment 1180/41 can be found at: <a href="http://www.planning.wa.gov.au/publications/941.asp">www.planning.wa.gov.au/publications/941.asp</a></p> <p>The submission is labelled Submission Number 26.</p>

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		<p>CCDSP to Fremantle in preference to a bus system, the WAPC has not adopted their views. <i>"It is considered that Bus Rapid Transit presents the most viable and effective public transport option in the short to medium term, in the absence of the significant Government financial commitment required to implement the alternatives suggested through the public comment period. By securing the public transport priority contiguously to Fremantle from the project area, and ensuring that appropriate transit stops are provided, the opportunity to transition to light rail in the longer term is preserved, should the technology be implemented on a broader scale"</i>.(response to submissions) Draft Cockburn Coast District Structure Plan - Public Submissions Report - August 2009 p.9 The argument made is that the bus transit system is cheaper than light rail in the short to medium-term. This is qualified by comments that such viability only occurs in the absence of 'the significant Government financial commitment' that would be required to implement the clearly preferred choice of respondents – which is light rail. Long-term sustainability assessment should be considered in this case and a comparative assessment considered. An electrified light rail system has the benefit of reduced running, maintenance and replacement costs when compared to gas/diesel buses even if the capital investment on infrastructure is considered. It is also clear that light rail can source renewable energy based electricity to reduce or eliminate its inherent carbon footprint unlike gas/diesel buses which will rely on external offsets in order to approach carbon neutrality. If a light rail line were configured parallel to coastal views it would prove attractive to tourists in its own right and increase patronage. It is difficult to see a bus system achieving the same status. The cost for implementing light rail has been estimated at \$15 million per kilometre in high density urban environments (Ludlam 2010), although the majority of track would be installed in a low-constraint environment (the CCDSP itself) until it reached the developed outskirts of Fremantle. Depending on alignment the track may extend 5-7 kilometres with a total cost of \$75-100 million plus rolling stock and maintenance. Light rail vehicle costs are around \$3 million per vehicle and a maintenance facility of around \$4 million.</p> <p>However, construction costs vary dramatically depending on the environment (tunnelling, gradients, dense urban development etc) and many cost assessments from other states and countries are less than those quoted in the CCDSP (Ludlam 2010). Importantly the cost/revenue ratio decreases dramatically with increased patronage and at maximum capacity the light rail system is highly cost effective, carbon efficient and has high rates of congestion reduction. DPI (2008) has acknowledged the superiority of light rail in this regard over buses and also note that light rail gives a sense of <i>permanence</i> to developers who are more likely to invest if government has dedicated capital and infrastructure to a long-term transit system (buses lack this permanence). They also acknowledge that the <i>scale</i> of the</p>	

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		<p>transit project can have a significant <i>place-making ability</i> as in my earlier comments on tourist potential. In DPI's view "<i>Large-scale (transit) projects with considerable government investment are more likely to generate development/redevelopment opportunities</i>". DPI also acknowledge that buses in Western Australia suffer from stigmatisation (the public view them as uncomfortable or inconvenient) and that this would be a barrier to uptake unless specific marketing plans were put in place at considerable expense. Putting aside the cost merits of buses vs. light rail it is clear that Hampton Road will reach unacceptable levels of congestion in the near future. Indeed the WAPC note in its transport analysis that Hampton Road;</p> <p><i>"... is predicted to be congested, regardless of whether the DSP area is developed or not. Paramics modelling undertaken for Scenario 1 (two lanes for general traffic plus kerbside transit lanes) without any DSP traffic predicts volumes of 32 100 vpd by 2031. The predicted congestion on Hampton Road places an increased emphasis on the need for transit priority in the coastal corridor."</i> (DPI 2008, p.15)</p> <p>Given this assessment by the DPI it is increasingly apparent that any transit option should avoid road use as it will inevitably lead to further congestion.</p> <p><b>Recommendation 1:</b> It is clear there is a need for a light-rail service for the Cockburn DSP to be funded at the next state budget. Although provision is made in the plans for a light rail reserve, the emphasis in the most recent draft CCDSP appears to be on road-based <i>bus</i> transport. I would recommend that the State Government develop an infrastructure investment plan for long term light rail implementation in the CCDSP with a view to extension into surrounding suburbs at a later time.</p> <p><b>Recommendation 2:</b> Traffic congestion on Hampton Road has reached critical levels and with anticipated population increases resulting from the CCDSP, any increase in road based transport (including bus services) will prove unmanageable in the short to medium term. I recommend against road based transit systems as they increase congestion and represent an opportunity cost and investment disincentive for future light rail infrastructure.</p> <p><b>Recommendation 3:</b> Light rail integration with the broader regional transport network is the most sensible and sustainable option to connect Fremantle with the CCDSP development and the suburbs beyond. The state government should establish a transit working group to integrate the heavy rail line at Fremantle station with light rail infrastructure from the CCDSP.</p> <p><b>South Fremantle landfill</b></p>	

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		<p>There is virtually no community support for the development of residential dwellings on the South Fremantle landfill site. It is noted that the City of Fremantle has been permitted to undertake the planning activities for this site even though it falls within the boundaries of the CCDSP. The City of Fremantle planning process has been augmented by a Stakeholder Advisory Group ('the Group') which includes representation from the community adjacent to the landfill. I was a member of this Group as the spokesperson for the South Fremantle/Hamilton Hill Residents' Assoc Inc. Although this Group has been in abeyance recently, my recollection is that there was no clear consensus on whether Option A (which includes netball courts and a council depot) or Option B (which included more housing) was preferable. I recall that the community representatives on the Group, including myself preferred Option A, whilst the developer representatives preferred Option B, creating an impasse.</p> <p>However, the DSP document available online at DPI states <i>"The structure plan reflects Option B as determined through the advisory group process."</i> I met with the City of Fremantle last month about this matter and I understand that a further meeting of this Group is to be convened with a view to reconciling this and to moving forward. The lack of community support for residential development at the tip site is directly related to the hazards associated with any potential remediation and redevelopment of the site. Historical and anecdotal records confirm that a range of hazardous waste materials are buried within the site and that these include PCBs, quarantine waste, municipal waste, medical waste, sullage and ordnance. There are also serious ongoing issues associated with uncontrolled methane release from the landfill, within the landfill and under the adjoining Chalet village site. Referenced details of the site history and contamination have already been presented to the WAPC in my original submission of the South Fremantle/Hamilton Hill Residents' Association Inc in September 2008 which attached a lengthy scientific report by Kelly Duckworth (Duckworth 2008). I ask that this original submission be included with this current submission.</p> <p>Contaminated site remediation in Western Australia is still in its infancy in terms of methodology and public health protection. The primary means of remediation in WA are dig and dump approaches with poor dust control, no vapour control and inadequate air monitoring techniques. The South Fremantle community were subjected to the remediation of the former lead smelter site in 2005. I represented this community in the Supreme Court in which we attempted to have the WA Government adopt best practice remediation by removing all risks to local residents and beach users. We requested that the Health Department and the Department of Environment and Conservation implement a requirement that the hazardous remediation activity be conducted in an enclosure to prevent the</p>	

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		<p>release of lead contamination across Fremantle. Large tent like enclosures operating under negative pressure are used in the US and Europe for remediation of sites containing harmful dust and vapours in proximity to local communities. Unfortunately the responsible government agencies in WA refused to take this preventative action and approved a plan which did not remove the risk of lead dust being released over South Fremantle. The departments gave public assurances that nothing would go wrong and site specific risk assessments that generated 'tolerable' levels of contaminants.</p> <p>These assessments did not take into account the special sensitivities of the elderly, infants and pregnant women. They also failed to account for the accumulative and synergistic impacts of the hazardous chemicals released and ignore the pre-existing body burdens of likely receptors. Ultimately many families with young children, including mine, made the difficult decision to leave our homes voluntarily while the developer conducted its remediation over an 18 month period. Some families never returned. The unfortunate legacy of this remediation has left a scar on our community.</p> <p>I have relayed this story in the hope that the Government acknowledges that the community adjacent to this tip site has already suffered loss and dislocation due to the recent lead remediation at South Beach. We do not want to find ourselves in this situation again. Specifically we do not want to leave our homes again. Given the high degree of hazard associated with the waste fill at the South Fremantle landfill and the inability of contractors and government agencies to adequately protect local residents, I maintain my strong opposition to residential development of the South Fremantle landfill site. I support the recommendations of the 2008 South Fremantle/Hamilton Hill Residents' Association Inc with respect to this issue and reiterate those recommendations;</p> <p><b>Recommendation 1:</b> Residential redevelopment of the South Fremantle Landfill Site should not be approved due to the many risks associated with the hazardous waste and emission of landfill gases from the site.</p> <p><b>Recommendation 2:</b> A 500m buffer zone, which precludes further residential redevelopment, should be applied around the South Fremantle Landfill Site, in accordance with Victorian EPA guidance statements.</p> <p><b>Recommendation 3:</b> Regular monitoring for landfill gas at the South Fremantle Landfill Site, surrounding residential areas, and commercial areas, already existing within the 500m buffer, should be implemented as a matter of urgency, and in accordance with the highest international standards.</p>	

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		<p><b>Recommendation 4:</b> Comprehensive soil and groundwater testing should be undertaken in the Plan area, to support well informed redevelopment strategies.</p> <p><b>Fremantle Chalet Village</b>  The Fremantle Chalet Village has an uncertain future. It was established in the 1980s as a temporary caravan park to accommodate an influx of temporary visitors to Fremantle during the America's Cup yacht series. In following years, approval was given by the City of Fremantle to integrate long term residents in 'park homes' at the site. Throughout this period little or no regard was given to the health implications of long-term tenancy on a landfill site where waste is regularly exposed during site works and gardening. Methane levels in soil at the site have also been found to be very high at times, raising concerns about asphyxiation risk and explosions. Both the South Fremantle landfill and the Chalet Village share similar problems in terms of waste fill, methane release, inadequate management and remediation.</p> <p>It is doubtful that government authorities would ever again allow a situation where residents were permitted to live for long periods on an unremediated landfill site. This brings into question the future of the residents currently occupying that site. Many long-term residents are elderly pensioners who have invested a large portion of their capital into the 'park homes' that occupy the site. Due to current debate over legislation affecting these types of caravan parks and dwellings, many owners have found themselves in a position where they cannot sell their homes and cannot relocate. The financial situation of many long term residents has been seriously affected as is their security of tenure. The Fremantle Chalet Village requires remediation which cannot be undertaken with the current resident's in-situ. The future development of the site and the intentions of the current owner are not clear, but it appears that the current land-use will change under the CCDSP. Either remediation or re-development will require current long-term residents to relocate.</p> <p>On a positive note there may be an opportunity for the State Government to assist these long-term residents through a relocation program within the CCDSP. The Government has committed to a minimum of 20% affordable housing within the CCDSP which should provide an opportunity to relocate those long term Chalet Village residents who would otherwise have extremely limited options to seek alternative accommodation in close proximity with similar amenity. Many of these residents have family and support networks in adjoining suburbs and cannot afford to move from their current accommodation into surrounding communities. The Government has not yet responded to the Economics and Industry Standing</p>	

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		<p>Committee report titled <i>“Provision, Use and Regulation of Caravan Parks (and Camping Grounds) in Western Australia”</i> which raises many of the issues that disadvantage long-term caravan park residents. However, the Caravan Park/Park Homes Interagency Working Group has been established with a Memorandum of Understanding to assist displaced residents resulting from caravan park closures. The agencies include;</p> <p>Department of Commerce, Department for Communities , Department of Housing &amp; Department of Planning</p> <p><b>Recommendation 1:</b> It would be commendable if the Working Group, in collaboration with the long-term Chalet Village residents could develop a resolution to their current predicament based around secure, affordable housing/accommodation within the CCDSP.</p> <p><b>Coastal setback</b> All development should be setback at least behind the freight rail reserve and Robb Road to protect against the risk of inundation from predicted rising sea levels. The threat of rising sea levels persuaded the Victorian Civil and Administrative Tribunal to refuse a housing development within 1km of the coast recently.</p> <p>Many scientists have predicted sea levels could rise by as much as 2 metres by the end of the century (Allison et al. 2009). Some scientists have predicted the rise could be even greater. This raises the issue of State Government liability. If the precautionary principal were invoked in these circumstances, the State Government would place a moratorium on any further coastal developments within 1km of the coast depending on the slope of the land and potential for inundation.</p> <p><b>Recommendation 1:</b> All development should be setback at least behind the freight rail reserve and Robb Road to prevent inundation of coastal lots.</p> <p><b>Recommendation 2:</b> If the Government accepts current sea level rise predictions, the precautionary principal should be invoked and there should be no coastal development within 1 km of the sea.</p> <p><b>Renewable Energy</b> The implementation of the CCDSP will provide a unique opportunity for government to plan for and integrate renewable energy generation at lot level for domestic and commercial developments within the structure plan. While site orientation and current electricity supply legislation will make it unlikely that district scale renewable systems can easily be implemented (such as terrestrial solar or wind farms) in the short-term, a stronger government</p>	

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		<p>commitment to sustainability targets for the development could see wave power and medium scale wind turbines established to contribute to the carbon neutrality of the project. Geothermal power should be assessed for district power generation for the CCDSP and a feasibility study conducted.</p> <p>Superior site elevation in the north-east corner of the development allows for a very good wind resource and sufficient space is available for around five or six 20kW wind turbines to be installed. This would be sufficient to power 65 six-star energy efficient households. Current electricity legislation would make it very difficult for on-site renewable energy to be used directly as the power supply for CCDSP homes and businesses.</p> <p>I would recommend that the Government consider amendments to legislation to streamline the ability of on-site renewable generation to be used for the power needs of developments such as the CCDSP. It is entirely practical for the Government to encourage (through building codes and developer agreements) the implementation of lot scale renewable energy generation. Commercial buildings could install vertical axis wind turbines and photovoltaic power generation, while households could incorporate solar hot water systems and photovoltaic power generation. There are many other sustainability initiatives that could be considered and many of these have already been outlined in LandCorp's 2009 <i>Alkimos Environmental Sustainability Initiatives Report</i>. All of these initiatives should be considered in the context of the CCDSP.</p> <p><b>Recommendation 1:</b> Assess and implement district scale renewable energy generation for the CCDSP where feasible. Page 10</p> <p><b>Recommendation 2:</b> Require design criteria for domestic and commercial buildings within the CCDSP that maximise passive energy savings and active renewable energy generation wherever possible.</p> <p><b>Recommendation 3:</b> Review legislation to make it easier for larger scale renewable energy generation to be dedicated to local developments to create a level of measurable energy autonomy and carbon footprint reduction for developments such as the CCDSP.</p> <p><i>(Attachment 3 - previous submission on proposed Scheme Amendment No. 89 rezoning the area from Industry to Development zone and Draft Cockburn Coast District Structure Plan Part 2 - November 2011)</i></p>	<p>Noted the submissioner has included a copy of a submission they have made on the related Town Planning Scheme amendment and District Structure Plan advertised by the City of Cockburn.</p> <p>This is considered to be provided for information as background the submission above and does not warrant further response. The City has already responded to the submissions raised as part of that earlier consultation process.</p> <p>A copy of the Submission Schedule on Town Planning Scheme Amendment 89 and the District Structure Plan can be found at:  <a href="http://www.cockburn.wa.gov.au/meetings_and_minutes">www.cockburn.wa.gov.au/meetings_and_minutes</a></p> <p>It is the attachment to Item 14.2 on the Council meeting agenda for 9 February 2012. The submission is labelled Submission Number 26.</p>



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		<p><b>Executive Summary</b></p> <p>This submission outlines a number of suggestions in relation to the Cockburn Coast District Structure Plan 2 (CCDSP2) as it appears in the Proposed Scheme Amendment No. 89. Comments are also made in relation to planning issues surrounding the South Fremantle tip site and the Fremantle Chalet Village (Caravan Park). The tip site and the Fremantle Chalet Village both fall within the boundaries of the CCDSP and share similar environmental problems. The South Fremantle tip-site is regarded as being subject to separate planning measures by the City of Fremantle but I am pleased to see that at least part of the site has been integrated into the strategic planning for public transit for the new development.</p> <p>There is clearly a need to consider sensitive and ecologically sustainable development of this locality given its historic use as an industrial area with noxious industry land-use. However the need to remediate or manage contaminated land should not compromise the remaining high conservation values of other land within the structure plan area or resident's health or amenity.</p> <p>With a view to ensuring more sustainable outcomes for the structure plan I have provided comment on;</p> <ul style="list-style-type: none"> <li>• The urgent need to fund dedicated infrastructure for a light rail network to service the structure plan development and alleviate congestion on Hampton Road and throughout Fremantle.</li> <li>• Strong community opposition to residential development of the South Fremantle landfill site due to health and safety issues and environmental impacts.</li> <li>• The need for greater setbacks between the coastline and the key residential and commercial developments of the CCDSP.</li> <li>• If the owner of the Fremantle Chalet Village decides to sell this site, consideration needs to be given to the relocation of the Fremantle Chalet Village permanent residents to affordable accommodation within the CCDSP.</li> <li>• The need to integrate renewable energy systems into the development at district scale where possible and certainly at lot scale for commercial and residential structures.</li> </ul> <p><b>Transit: Light rail vs. buses</b></p> <p>I was pleased to see that the WAPC has heard the widespread call for light rail to</p>	

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		<p>be established in the structure plan development. The location of the light rail/transit reserve along the 'spine' of the new development makes sense in that it provides for a greater passenger catchment on both sides of the line. This is a benefit that would be lost if the line were to run close to the coast allowing a catchment on one side of the track only.</p> <p>It is important that funding is made available for the early establishment of the reserve at the very beginning of the development to maximize the passenger use of light rail from the outset and to alleviate traffic congestion in the area before an estimated 10,000 new residents take to the roads exclusively in cars.</p> <p>I am concerned that there is still a focus on the use of rapid transit buses initially and then a gradual move to light rail. Any spending on new assets for the bus service will effectively drain funds that could be directed to light rail establishment. Rather than splitting the options, it would be prudent to dedicate funds and planning directly to light rail from the beginning.</p> <p>Hampton Road is already heavily congested. The light rail line will provide an effective antidote to the congestion. I was pleased to see that a proposed light rail station would be based on the site of the former South Fremantle tip site and that plans are included to examine the extension of the line through to the Fremantle train station. I have suggested a similar light rail plan to government and augmented it with a plan to establish a park and ride facility next to the station at the tip site. Capping the land with bitumen for car parking would prevent the ingress of rain water which is a major factor in the spread of groundwater contamination beneath the former tip site. In addition it would allow residents of the new development to park at the tip site and catch the light rail to Fremantle and then heavy rail beyond that into the City, via the Fremantle train station.</p> <p>This would have the effect of removing a sizeable amount of the existing and proposed traffic that clogs Hampton Road and other Fremantle streets while providing a net environmental benefit at the tip site. These are benefits that would not arise from the use of rapid transit buses. DPI (2008) has acknowledged the superiority of light rail in this regard over buses and also notes that light rail gives a sense of <i>permanence</i> to developers who are more likely to invest if government has dedicated capital and infrastructure to a long-term transit system (buses lack this permanence).</p> <p>They also acknowledge that the <i>scale</i> of the transit project can have a significant <i>place-making ability</i>. In DPI's view "<i>Large-scale (transit) projects with considerable government investment are more likely to generate development/redevelopment</i></p>	

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		<p><i>opportunities”.</i></p> <p>DPI also acknowledges that buses in Western Australia suffer from stigmatisation (the public view them as uncomfortable or inconvenient) and that this would be a barrier to uptake unless specific marketing plans were put in place at considerable expense. Putting aside the cost merits of buses vs. light rail, it is clear that Hampton Road will reach unacceptable levels of congestion in the near future. Indeed the WAPC note in its transport analysis that Hampton Road;</p> <p><i>“... is predicted to be congested, regardless of whether the DSP area is developed or not. Paramics modelling undertaken for Scenario 1 (two lanes for general traffic plus kerbside transit lanes) without any DSP traffic predicts volumes of 32 100 vpd by 2031. The predicted congestion on Hampton Road places an increased emphasis on the need for transit priority in the coastal corridor.”</i> (DPI 2008, p.15)</p> <p>Given this assessment by the DPI it is increasingly apparent that any transit option should avoid road use as it will inevitably lead to further congestion.</p> <p><b>Recommendation 1:</b> I recommend that the State Government develop an infrastructure investment plan for long term light rail implementation in the CCDSP with a view to extension into surrounding suburbs (ie Fremantle) in the near future.</p> <p><b>Recommendation 2:</b> Traffic congestion on Hampton Road has reached critical levels and with anticipated population increases resulting from the CCDSP, any increase in road based transport (including bus services) will prove unmanageable in the short to medium term. I recommend against road based transit systems as they increase congestion and represent an opportunity cost and investment disincentive for future light rail infrastructure.</p> <p><b>Recommendation 3:</b> Light rail integration with the broader regional transport network is the most sensible and sustainable option to connect Fremantle with the CCDSP development and the suburbs beyond. The State Government should establish a transit working group to integrate the heavy rail line at Fremantle station with light rail infrastructure from the CCDSP.</p> <p><b>South Fremantle landfill</b></p> <p>There is virtually no community support for the development of residential dwellings on the South Fremantle landfill site. It is noted that the City of Fremantle has been permitted to undertake the planning activities for this site even though it falls within the boundaries of the CCDSP. The City of Fremantle planning process has been augmented by a Stakeholder Advisory Group (‘the Group’) which</p>	

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		<p>includes representation from the community adjacent to the landfill.</p> <p>I was a member of this Group as the spokesperson for the South Fremantle/Hamilton Hill Residents' Association Inc. Although this Group has been in abeyance recently, my recollection is that there was no clear consensus on whether Option A (which includes netball courts and a council depot) or Option B (which included more housing) was preferable. I recall that the community representatives on the Group, including myself preferred Option A, whilst the developer representatives preferred Option B, creating an impasse.</p> <p>However, the DSP document available online at DPI states <i>"The structure plan reflects Option B as determined through the advisory group process."</i> The lack of community support for residential development at the tip site is directly related to the hazards associated with any potential remediation and redevelopment of the site. Historical and anecdotal records confirm that a range of hazardous waste materials are buried within the site and that these include PCBs, quarantine waste, municipal waste, medical waste, sullage and ordnance. There are also serious ongoing issues associated with uncontrolled methane release from the landfill, within the landfill and under the adjoining Fremantle Chalet Village site.</p> <p>Referenced details of the site history and contamination have already been presented to the WAPC in my original submission of the South Fremantle/Hamilton Hill Residents' Association Inc in September 2008 which attached a lengthy scientific report by Kelly Duckworth (Duckworth 2008). I ask that this original submission be included with this current submission.</p> <p>Contaminated site remediation in Western Australia is still in its infancy in terms of methodology and public health protection. The primary means of remediation in WA are dig and dump approaches with poor dust control, no vapour control and inadequate air monitoring techniques. The South Fremantle community were subjected to the remediation of the former lead smelter site in 2005. I represented this community in the Supreme Court in which we attempted to have the WA Government adopt best practice remediation by removing all risks to local residents and beach users. We requested that the Health Department and the Department of Environment and Conservation implement a requirement that the hazardous remediation activity be conducted in an enclosure to prevent the release of lead contamination across Fremantle. Large tent-like enclosures operating under negative pressure are used in the US and Europe for remediation of sites containing harmful dust and vapours in proximity to local communities.</p> <p>Unfortunately the responsible government agencies in WA refused to take this</p>	

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		<p>preventative action and approved a plan which did not remove the risk of lead dust being released over South Fremantle. The departments gave public assurances that nothing would go wrong and site specific risk assessments that generated 'tolerable' levels of contaminants. These assessments did not take into account the special sensitivities of the elderly, infants and pregnant women. They also failed to account for the accumulative and synergistic impacts of the hazardous chemicals released and ignored the pre-existing body burdens of likely receptors. Ultimately many families with young children, including mine, made the difficult decision to leave our homes voluntarily while the developer conducted its remediation over an 18 month period. Some families never returned. The unfortunate legacy of this remediation has left a scar on our community.</p> <p>I have relayed this story in the hope that the Government acknowledges that the community adjacent to this tip site has already suffered loss and dislocation due to the recent lead remediation at South Beach. We do not want to find ourselves in this situation again. Specifically we do not want to leave our homes again. Given the high degree of hazard associated with the waste fill at the South Fremantle landfill and the inability of contractors and government agencies to adequately protect local residents, I maintain my strong opposition to residential development of the South Fremantle landfill site.</p> <p>I support the recommendations of the 2008 South Fremantle/Hamilton Hill Residents' Association Inc with respect to this issue and reiterate those recommendations;</p> <p><b>Recommendation 1:</b> Residential redevelopment of the South Fremantle Landfill Site should not be approved due to the many risks associated with the hazardous waste and emission of landfill gases from the site.</p> <p><b>Recommendation 2:</b> A 500m buffer zone, which precludes further residential redevelopment, should be applied around the South Fremantle Landfill Site, in accordance with Victorian EPA guidance statements.</p> <p><b>Recommendation 3:</b> Regular monitoring for landfill gas at the South Fremantle Landfill Site, surrounding residential areas, and commercial areas, already existing within the 500m buffer, should be implemented as a matter of urgency, and in accordance with the highest international standards.</p> <p><b>Recommendation 4:</b> Comprehensive soil and groundwater testing should be undertaken in the Plan area, to support well informed redevelopment strategies.</p>	

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		<p><b>Recommendation 5:</b> Locate a park and ride facility on the former tip site integrated with the light rail station noted in the current plans. The bitumen capping will have a positive effect on groundwater contamination and represents best use of site with highly limited land use options.</p> <p><b>Fremantle Chalet Village</b>  The Fremantle Chalet Village has an uncertain future. It was established in the 1980s as a temporary caravan park to accommodate an influx of temporary visitors to Fremantle during the America's Cup yacht series. In following years, approval was given by the City of Fremantle to integrate long term residents in 'park homes' at the site. Throughout this period little or no regard was given to the health implications of long-term tenancy on a landfill site where waste is regularly exposed during site works and gardening. Methane levels in soil at the site have also been found to be very high at times, raising concerns about asphyxiation risk and explosions. Both the South Fremantle landfill and the Chalet Village share similar problems in terms of waste fill, methane release, inadequate management and remediation. It is doubtful that government authorities would ever again allow a situation where residents were permitted to live for long periods on an unremediated landfill site.</p> <p>This brings into question the future of the residents currently occupying that site. Many long-term residents are elderly pensioners who have invested a large portion of their capital into the 'park homes' that occupy the site. Due to current debate over legislation affecting these types of caravan parks and dwellings, many owners have found themselves in a position where they cannot sell their homes and cannot relocate. The financial situation of many long term residents has been seriously affected as is their security of tenure.</p> <p>The Fremantle Chalet Village requires remediation which cannot be undertaken with the current residents in-situ. The future development of the site and the intentions of the current owner are not clear, but it appears that the current land-use will change under the CCDSP. Either remediation or re-development will require current long-term residents to relocate.</p> <p>On a positive note there may be an opportunity for the State Government to assist these long-term residents through a relocation program within the CCDSP. The Government has committed to a minimum of 20% affordable housing within the CCDSP which should provide an opportunity to relocate those long term Chalet Village residents who would otherwise have extremely limited options to seek alternative accommodation in close proximity with similar amenity. Many of these residents have family and support networks in adjoining suburbs and cannot afford</p>	

No.	Name/address	Submission	Council's recommendation
		<p>to move from their current accommodation into surrounding communities. I strongly support the relocation of the Chalet Village residents in the event that their current location is sold for development. I believe that the government should find suitable accommodation for these residents close by within the new development.</p> <p>The Caravan Park/Park Homes Interagency Working Group has been established with a Memorandum of Understanding to assist displaced residents resulting from caravan park closures. The agencies include; Department of Commerce, Department for Communities, Department of Housing &amp; Department of Planning</p> <p><b>Recommendation 1:</b> It would be commendable if the Working Group, in collaboration with the long-term Chalet Village residents could develop a resolution to their current predicament based around secure, affordable housing/accommodation within the CCDSP.</p> <p><b>Coastal setback – at least 100 metres</b> All development should be setback at least behind the freight rail reserve and Robb Road to protect against the risk of inundation from predicted rising sea levels. The threat of rising sea levels persuaded the Victorian Civil and Administrative Tribunal to refuse a housing development within 1km of the coast recently.</p> <p>Many scientists have predicted sea levels could rise by as much as 2 metres by the end of the century (Allison et al. 2009). Some scientists have predicted the rise could be even greater. This raises the issue of State Government liability. If the precautionary principle was invoked in these circumstances, the State Government would place a moratorium on any further coastal developments within 1km of the coast depending on the slope of the land and potential for inundation.</p> <p><b>Recommendation 1:</b> All development should be setback at least behind the freight rail reserve and Robb Road to prevent inundation of coastal lots.</p> <p><b>Recommendation 2:</b> If the Government accepts current sea level rise predictions, the precautionary principle should be invoked and there should be no coastal development within at least 100 metres of the sea.</p> <p><b>Public Marina at the Power Station</b> I note that the current CCDSP2 has plans sketched on a map for a potential 'public' marina on the foreshore near the old South Fremantle Power Station. I have concerns that the public were not aware of this on the basis of the</p>	

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		<p>documentation in CCDSP1. The documentation indicates that any proposal for a public marina would be subject to a separate public consultation process to gauge community support or otherwise for this option.</p> <p>My concern is that there are very few public access beaches between Fremantle and Rockingham and those that do exist are coming under much greater pressure. If this section of the coast included an additional marina it would come at the cost of public access to the beach. People may decide that this is a fair trade off for a marina that perhaps will host public facilities and become a site of social activity. This issue should be considered very carefully and any consultation should be timely and broadly focused as the beach is used by many people along the coast.</p> <p><b>Renewable Energy</b></p> <p>The implementation of the CCDSP will provide a unique opportunity for government to plan for and integrate renewable energy generation at lot level for domestic and commercial developments within the structure plan.</p> <p>While site orientation and current electricity supply legislation will make it unlikely that district scale renewable systems can easily be implemented (such as terrestrial solar or wind farms) in the short-term, a stronger government commitment to sustainability targets for the development could see wave power and medium scale wind turbines established to contribute to the carbon neutrality of the project.</p> <p>Superior site elevation in the north-east corner of the development allows for a very good wind resource and sufficient space is available for around five or six 20kW wind turbines to be installed. This would be sufficient to power 65 six-star energy efficient households.</p> <p>Current electricity legislation would make it very difficult for on-site renewable energy to be used directly as the power supply for CCDSP homes and businesses. I would recommend that the Government consider amendments to legislation to streamline the ability of on-site renewable generation to be used for the power needs of developments such as the CCDSP. It is entirely practical for the Government to encourage (through building codes and developer agreements) the implementation of lot scale renewable energy generation. Commercial buildings could install vertical axis wind turbines and photovoltaic power generation, while households could incorporate solar hot water systems and photovoltaic power generation.</p> <p>There are many other sustainability initiatives that could be considered and many</p>	



No.	Name/address	Submission	Council's recommendation
		<p>of these have already been outlined in LandCorp's 2009 <i>Alkimos Environmental Sustainability Initiatives Report</i>. All of these initiatives should be considered in the context of the CCDSP. Solar or wind power facilities at the tip site may be able to supplement power supplies for an electrified light rail line further reducing the carbon footprint of the development and its infrastructure.</p> <p><b>Recommendation 1:</b> Assess and implement district scale renewable energy generation for the CCDSP where feasible.</p> <p><b>Recommendation 2:</b> Require design criteria for domestic and commercial buildings within the CCDSP that maximise passive energy savings and active renewable energy generation wherever possible.</p> <p><b>Recommendation 3:</b> Review legislation to make it easier for larger scale renewable energy generation to be dedicated to local developments to create a level of measurable energy autonomy and carbon footprint reduction for developments such as the CCDSP.</p> <p><b>Recommendation 4:</b> Investigate the ability of renewable energy sources to supplement the energy needs of the light rail line.</p>	
	<p>Western Power 363 Wellington Street Perth WA 6000</p>	<p>Western Power generally only objects if alignments, easements or clearances are encroached or breached however there is no land here owned by Western Power and the Power Station is owned by Verve</p> <p>However as there are overhead powerlines and/or underground cables, adjacent to or traversing the property, the following should be considered, prior to any works commencing at the above site/development/property.</p> <p>Working in proximity to Western Power Distribution Lines All work must comply with Worksafe Regulation 3.64 - Guidelines for Work in the Vicinity of Overhead Power Lines. If any work is to breach the minimum safe working distances a Request to Work in Vicinity of Powerlines form must be submitted.</p> <p>For more information on this please visit the Western Power Website links below:</p> <p><a href="http://www.westernpower.com.au/safety/WorkingAroundPowerLines/working_near_electricity.html">www.westernpower.com.au/safety/WorkingAroundPowerLines/working_near_electricity.html</a></p> <p><a href="http://www.westernpower.com.au/safety/DialBeforeYouDig.html">www.westernpower.com.au/safety/DialBeforeYouDig.html</a></p> <p><a href="http://www.1100.com.au">www.1100.com.au</a></p>	<p>Noted.</p> <p>Noted, this is a more detailed issue. Western Power should raise this at the subdivision and development stages.</p> <p>No changes are recommended based on this submission.</p>

No.	Name/address	Submission	Council's recommendation
		<p><a href="http://www.commerce.wa.gov.au/WorkSafe/">www.commerce.wa.gov.au/WorkSafe/</a></p> <p>If you require further information on our infrastructure including plans, please complete a request for Digital Data</p> <p>Please note: Western Power must be contacted on 13 10 87, or complete the attached DQA form, if your proposed works involve:</p> <p>A) Any changes to existing ground levels around poles and structures. B) Working under overhead powerlines and/or over underground cables.</p> <p>Western Power is obliged to point out that any change to the existing (power) system; if required, is the responsibility of the individual developer.</p>	

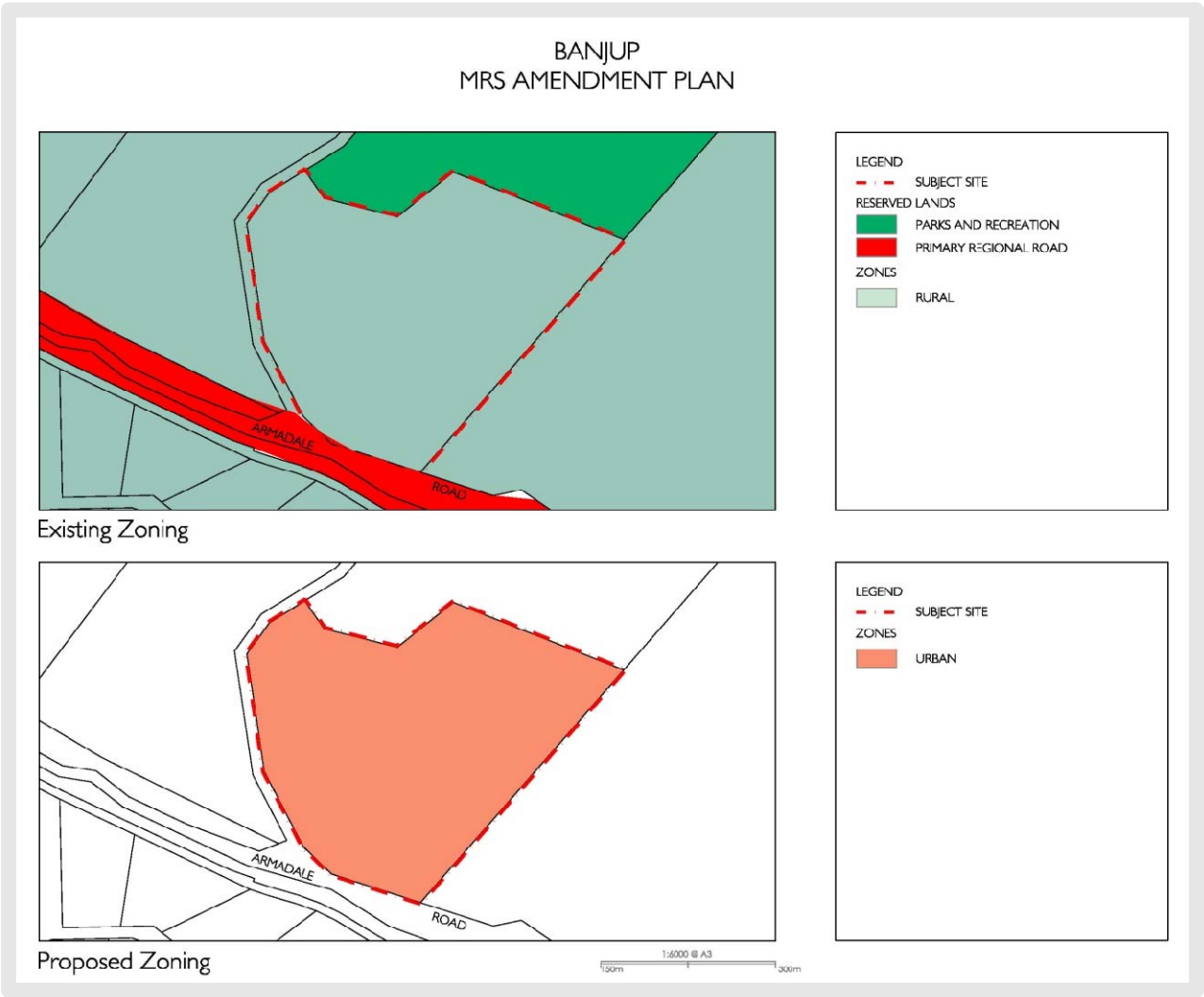
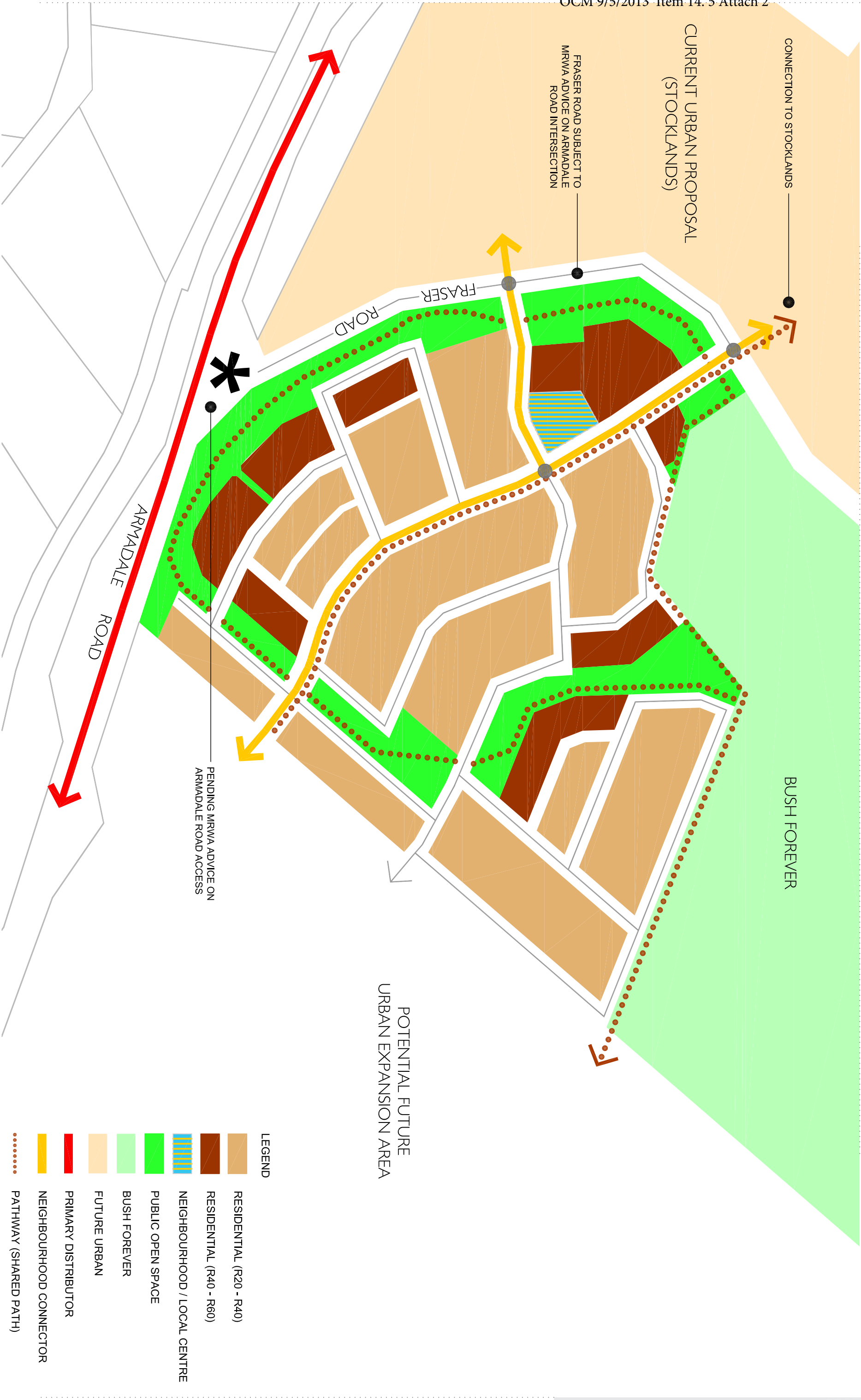


FIGURE 5 PROPOSED MRS ZONING



<p>date 27 February 2013 job no. 7514 scale 1:3000 @ A3</p> <p>designer K Kyle prepared by S Blanchard e-ref 7514_CON01A_20130227</p> <p>client DdH projection MGA50 GDA94</p>		<p>title concept plan</p> <p>draft</p> <p>address lot 821 armadale road</p> <p>bsn/jlp</p>	
<p>Level 3, 369 Newcastle Street, Northbridge, Western Australia, 6003</p> <p>email gni@greg-rowe.com web greg-rowe.com tel +61 8 9221 1991 fax +61 8 9221 1919</p>		<p>PERTH • PEEL / SOUTH WEST • MID WEST • PILBARA</p>	

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**GREG ROWE & associates**  
FOCUSSED ON ACHIEVEMENT

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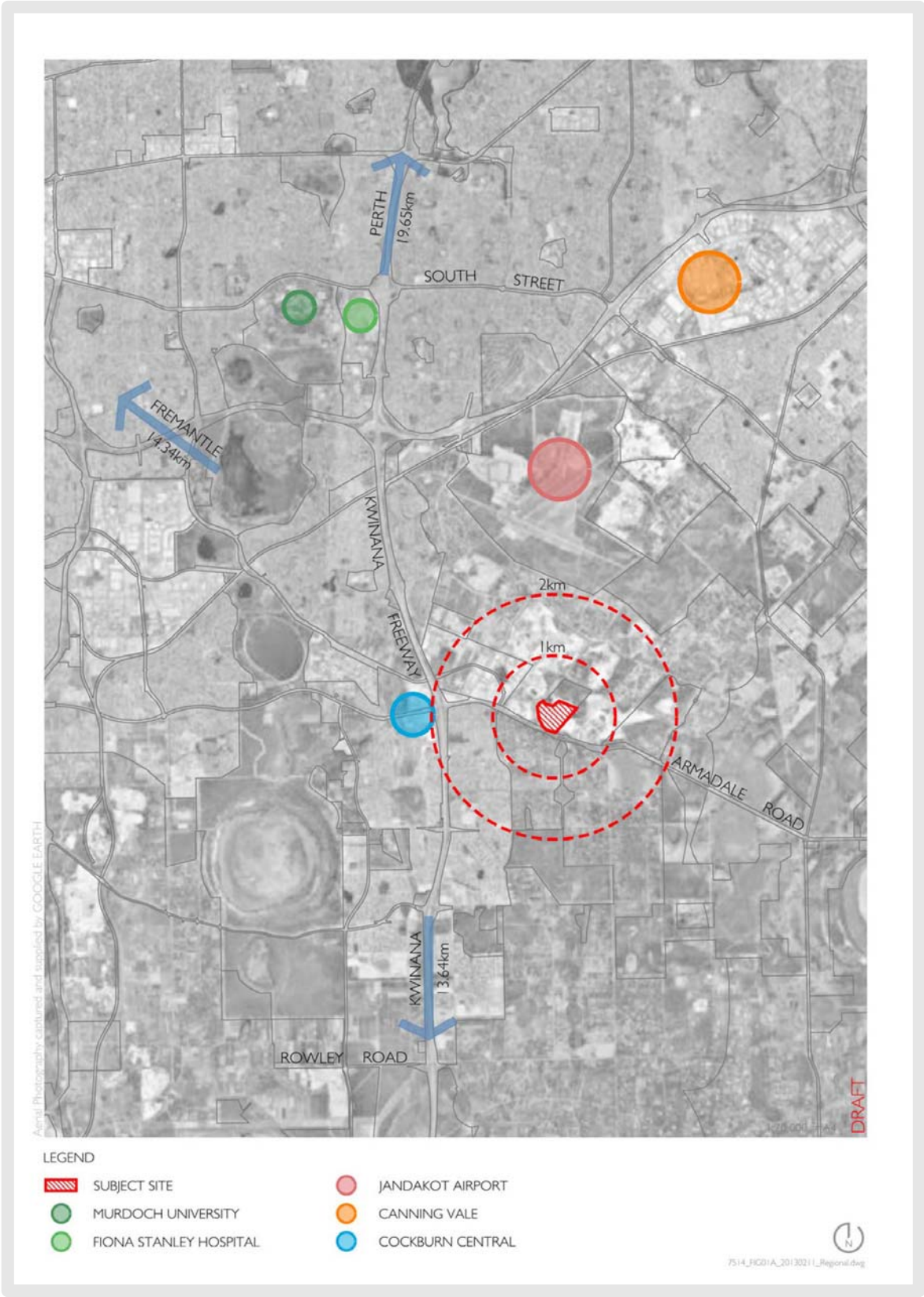
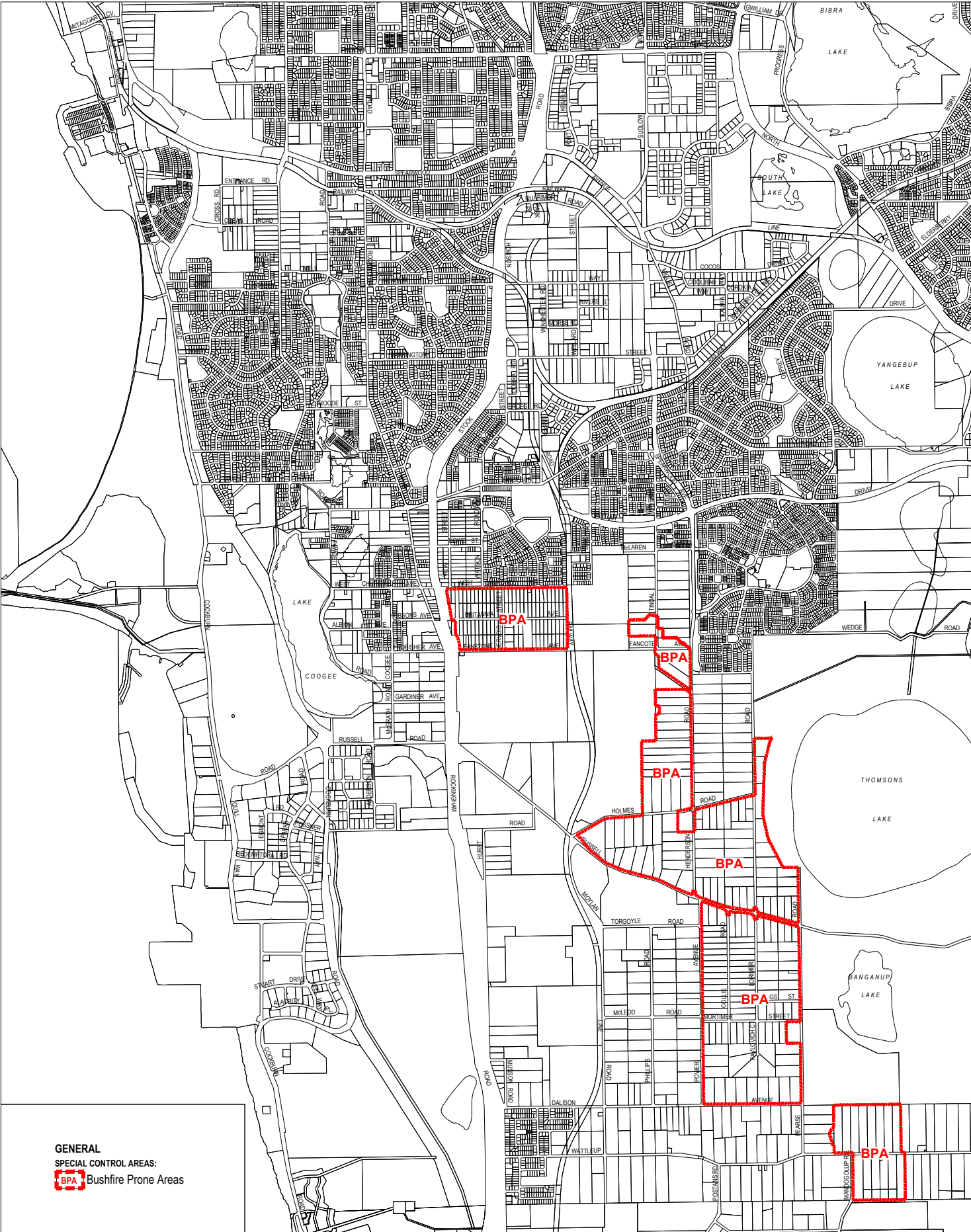


FIGURE I REGIONAL LOCATION



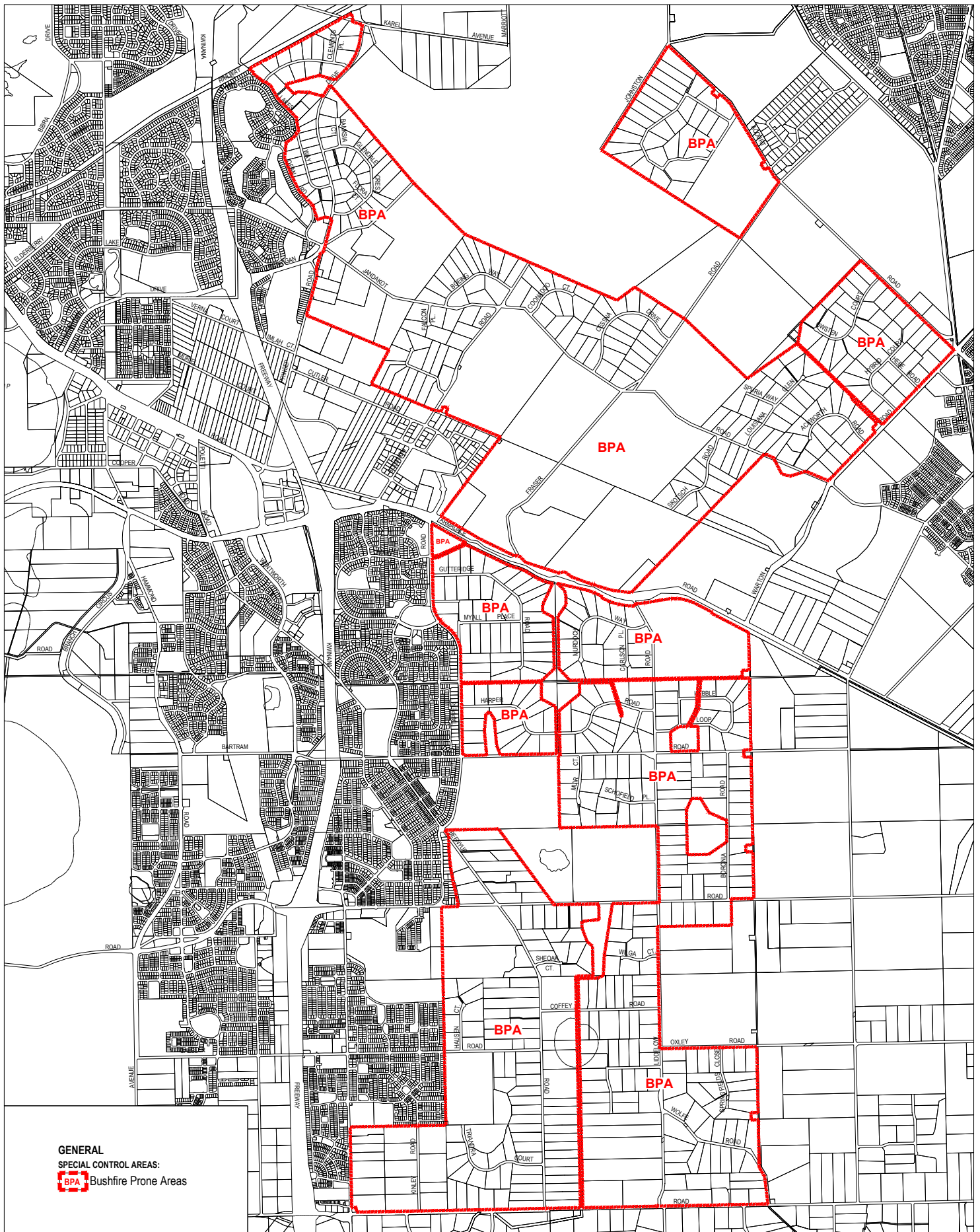


GENERAL  
SPECIAL CONTROL AREAS:  
BPA Bushfire Prone Areas



0 1km  
SCALE

# PROPOSED ZONING TOWN PLANNING SCHEME No.3 AMENDMENT No.92

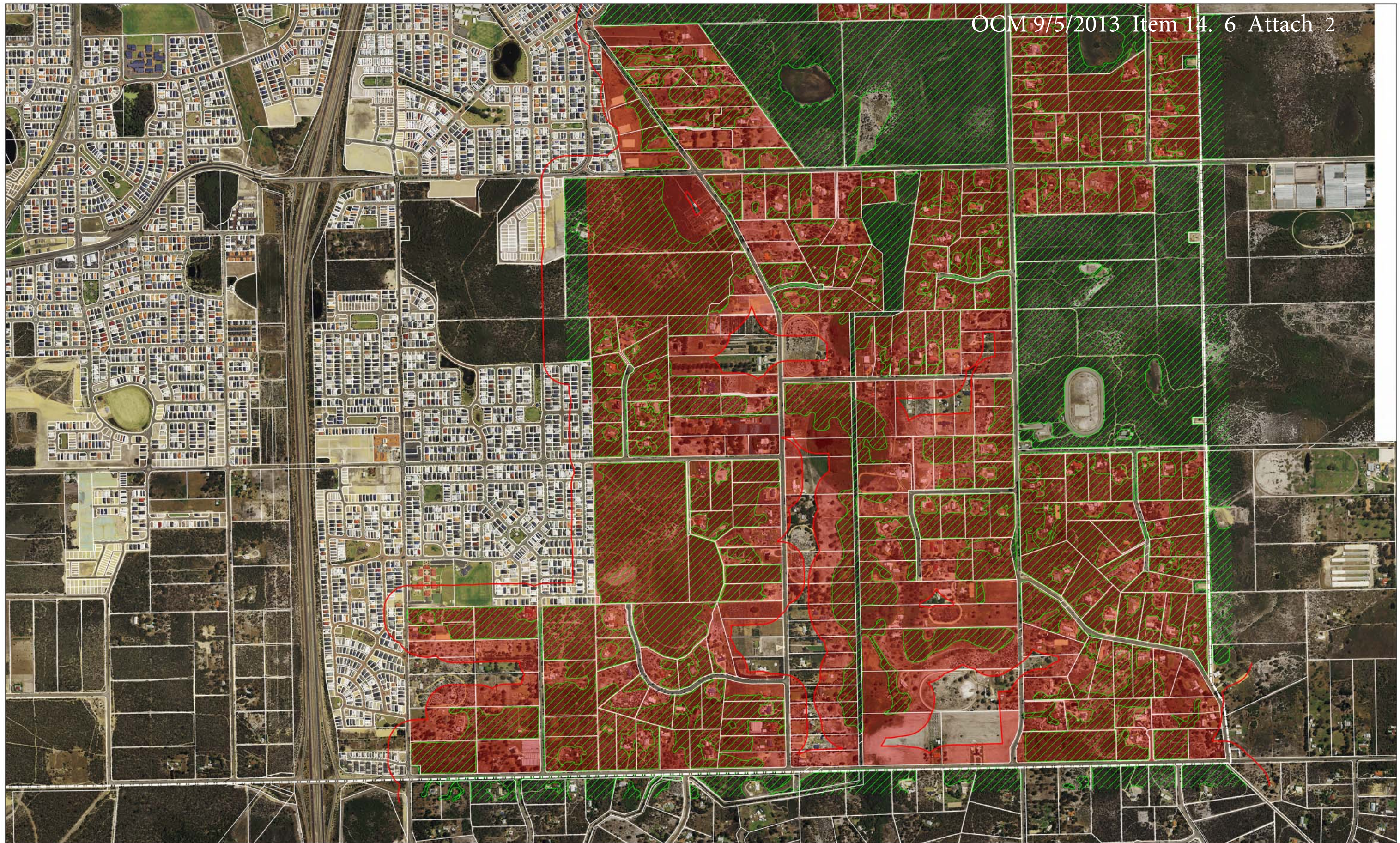


0 1km  
SCALE

# **PROPOSED ZONING TOWN PLANNING SCHEME No.3 AMENDMENT No.92**

MAP 2 of 2





-  Bushland Areas
-  100m Buffer
-  Bushfire Prone Areas  
(Resource/Rural/Rural Living Zones)

**EXAMPLE ONLY**

## BUSHFIRE HAZARD ASSESSMENT MAP WITHIN RESOURCE/RURAL/RURAL LIVING ZONES

**EXAMPLE ONLY**



**SCHEDULE OF SUBMISSIONS****PROPOSED SCHEME AMENDMENT NO 92 TO CITY OF COCKBURN TOWN PLANNING SCHEME NO 3 - CREATION OF SPECIAL CONTROL AREA (BUSHFIRE PRONE AREAS) AND ASSOCIATED AMENDMENTS**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
m1	Cr Steven Portelli (East Ward) <a href="mailto:steve.portelli@iinet.net.au">steve.portelli@iinet.net.au</a> PO Box 1215 BIBRALAKE WA 6965	<p>To propose only Rural Zone, Rural living zone, Resource zone, and Conservation zone areas for Fire Management plans and requiring Fire Risk Assessment [F.R.A.] reports for any proposed construction or renovation is not going far enough.</p> <p>AS3959-2009 advises that any residence/property within 100 metres of a fire risk requires measures to minimise damage. It therefore follows that any residential property adjacent such fire risks need to be measured and have fire prevention controls put in place.</p> <p>FMP as attached for the Lot 204 Lyon Road. This covers a new estate of some 200 plus homes that is circled by Bushlands Forever. We have clearly identified fire risk to homes adjacent Prosperity Loop Aubin Grove. With BAL 19 being applied to at least one proposed home. This is not a unique area. We have examples throughout Cockburn with residents as close if not closer to vegetation which has a high fuel load.</p> <p>Examples;</p> <ul style="list-style-type: none"> <li>• Tapper Road Atwell, Beenyup Road Atwell - Opposite Jandakot regional park</li> <li>• Kurrajong Approach - Opposite Atwell Waters POS</li> <li>• Gibbs Road Atwell - Opposite bushland</li> <li>• Jewel Gardens Hammond Park - Opposite bushland 60110380 Lot 3000</li> <li>• Oakridge Meander Success - Opposite bushland 6004643 Lot 4004</li> <li>• Wentworth Parade success - Opposite bushland 6000263 lot 9015</li> </ul> <p>There are hundreds more in Cockburn that are at risk.</p> <p>We have a duty of care to make all residents aware of the risks. I propose:</p> <ul style="list-style-type: none"> <li>• All new home construction/renovations have a requirement that Fire Risk Assessment Report is submitted for each and every applicant.</li> <li>• Established homes and properties are made aware of any risks. How?</li> </ul> <p>Established properties need to be addressed promptly. We can do a Fire Risk Assessment report for example on one property only in each of the exemplified areas above and then advise all the property owners adjacent of the particular fire risk that could apply to them.</p> <p>Therefore all fire risk areas need to be identified within Urban areas and in lieu of blanket F.R.A. reports being done, do a sample. This will then become an indicative report for guidance of other property owners.</p> <p>Maps of Cockburn could then having ratings based on BAL [Category of bush fire attack] rating from low thru BAL 1 – 12.5 [subject to ember attack] BAL 13 – 19 [Increasing levels of ember</p>	<p>The decision to restrict Scheme Amendment No. 92 to the more rural areas of Cockburn is in response to the general nature of the land being more predisposed to fire risk than an urban area. Moreover, it is also in response to the general nature or urban development being generally cleared, higher access to metropolitan fire services with shorter response times, access to scheme water and availability of hydrants.</p> <p>A number of the areas mentioned are in areas that have undergone structure planning for which fire management plans are often required when appropriate. Through this process appropriate response to the identified risk can be factored in at the early planning stages and can lead to an overall positive outcome in terms of fire mitigation.</p>

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		<p>attack] BAL 41 – FZ direct exposure to flames from fire front in addition to ember attack and heat flux.</p> <p>This would be a snap shot based on a set time and would need to be updated every 5 years. Property owners would need to be cognizant of any changes to their properties vegetation can impact on their risks. Likewise any changes in the adjacent fire risks.</p> <p>A major education campaign will need to address the new levels of information. Examples of action that can be taken to minimise fire risks needs to be provided.</p> <p>Established homes can be captured by having F.R.A. reports as a requirement when sold. Or at the very least have the indicative Fire Risk applied and has the property upgraded to minimise the risk. Who pays for or is responsible for this to be discussed. But it will need to be clearly stated on their Offer and Acceptance on the property. As this could be a beucratic nightmare I would propose that BAL at or under 19 have it recommended and not mandated. Above BAL 19 that some relaxation be allowed but at least some measures be put in place. This action to be determined and discussed further.</p> <p>NOTES.</p> <p>Even doing just indicative FRA reports is going to be a costly burden on Local Government. Do we need assistance from the State?</p> <p>Would it be better to employ directly a qualified Fire Risk Assessor? A standard F.R.A.. report costs a builder for a standard residential lot around \$250 each. User pays, shares the burden, but for established properties we need to lead and organise. We cannot leave it to the owners. The most cost effective process is what I have proposed above.</p> <p>Enclosed in email copy of: Fire Management Plan – Lot 204, Lyon Road - Atwell</p>	<p>Within such areas one must have faith in the robustness of the structure planning process. Additionally educational campaigns to inform people about the risk are continually administered by both the Council and FESA.</p> <p>Bushfire Hazard Assessment Maps will be created to identify the areas within the Special Control Area that are directly impacted by a fire risk, areas within 100m of these areas will also be required to meet higher building and planning requirements. All development within a designated bushfire prone area and identified on a bushfire hazard assessment map will be required to undertake a bushfires assessment as part of any approval. The bushfire hazard assessment map will be reviewed regularly.</p>
2	Richard Bloor Department of Education 151 Royal Street EAST PERTH WA 6004	<p><b>Support</b></p> <p>The Department of Education has reviewed the document and advises that it has no objection to this Proposal.</p>	Submission Noted

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
3		KEEP CONFIDENTIAL. I as the owner of a property in the "special control area" oppose this amendment. Reasons being that in the near future in wish to build a house on the property and this legislation will only further cut into my budget. As a low income earner this legislation will only put further stress and unwanted costs on the project.	<p>The potential for increased cost on new residences is noted and acknowledged in the Scheme Amendment Report.</p> <p>The disincentives of imposing higher building costs thorough bush fire designation; such as lowering property prices, insurance issues and potential developers viewing the imposition of additional construction standards as a disincentive to invest must be carefully weighed against the wider responsibility of Local Government.</p> <p>While Amendment 92 will place additional cost impositions on landowners and developers seeking to undertake development in bushfire prone areas, these are considered not to represent a magnitude which should dissuade this being seen for the broader importance of development being undertaken in a more appropriate manner cognisant of the bushfire risk.</p>
4	Sonja Seal Western Power Locked Bag 2520	<b>Support</b>  There are no objections; however, there are overhead powerlines and/or underground cables,	Submission Noted

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
	PERTH WA 6001	<p>adjacent to or traversing the property. Therefore, the following should be considered, prior to any works commencing at the above site/development/property.</p> <p><b>Working in proximity to Western Power Distribution Lines</b>  All work must comply with Worksafe Regulation 3.64 - Guidelines for Work in the Vicinity of Overhead Power Lines. If any work is to breach the minimum safe working distances a Request to Work in Vicinity of Powerlines form must be submitted. For more information on this please visit the Western Power Website links below:</p> <p><a href="http://www.westernpower.com.au/safety/Electrical_Safety_at_Work.html">http://www.westernpower.com.au/safety/Electrical_Safety_at_Work.html</a></p> <p><a href="http://www.westernpower.com.au/safety/DialBeforeYouDig.html">http://www.westernpower.com.au/safety/DialBeforeYouDig.html</a> or <a href="http://www.1100.com.au">www.1100.com.au</a>  <a href="http://www.commerce.wa.gov.au/WorkSafe/">http://www.commerce.wa.gov.au/WorkSafe/</a></p> <p><b>Working in proximity to Western Power Transmission Lines</b>  All work must comply with Worksafe Regulation 3.64 - Guidelines for Work in the Vicinity of Overhead Power Lines. Our standard conditions for working in close proximity to overhead transmission lines are attached for your information. For more information on this please visit the Western Power Website link below: <a href="http://www.westernpower.com.au/safety/Safety_Transmission_Lines.html">http://www.westernpower.com.au/safety/Safety_Transmission_Lines.html</a></p> <p><b>Please note: Western Power must be contacted on 13 10 87, if your proposed works involve:</b></p> <p>A) Any changes to existing ground levels around poles and structures.  B) Working under overhead powerlines and/or over underground cables.</p> <p><b>Western Power is obliged to point out that any change to the existing(power) system, if required, is the responsibility of the individual developer.</b></p>	
5	Yvonne Ramsey 32 Lakes Way JANDAKOT WA 6164	<p>I believe if the council gave us more opportunities to get rid of our tree waste there would be no need to have our area as a fire risk as our property is between Glen Iris and the Jandakot city development.</p> <p>With a bit of help and support from the council our area could be fire proof especially because we are so close a residential area.</p>	<p>Comments regarding additional green waste pickups noted.</p> <p>Should Amendment No.92 be adopted the need for a Bushfire hazard Assessment Map for all areas noted as SCA Bushfire Prone shall be undertaken</p> <p>At that time site specific scenarios will be taken into</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			consideration when identifying an area as bushfire prone or not.
6	Alec Leatherday 58 Fanstone Avenue BEELIAR WA 6164	<p><b>Objection</b></p> <p>In a “ Bushfire Prone Area ‘ would the best proposal be to remove some of the fuel so that the bushfire risk could be removed. (Surely putting in place extra planning controls for future development do nothing to look after those already living in the area ).</p> <p>If I have excess vegetation that could be posing a fire risk to my family and neighbours. Please identify it and I will have it removed.</p> <p>There is a section of the letter that under the heading “ will my land be affected “, says “ all residents in the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone area are subject to this Amendment 92. As we are considered “ Improved Residential “ maybe it does not include us and the attached map is incorrect.</p>	<p>The definition of “Improved Residential” is a classification utilised to levy rates.</p> <p>The lot in question is correctly identified in the area impacted by Amendment 92. The land is zoned “Rural Living”.</p> <p>The Amendment will place additional planning and building requirements on land identified on a Bushfire Hazard Assessment Map. These requirements will not be retrospective and will only apply to those undertaking new development or major additions to existing structures.</p> <p>When looking at whether or not an area is to be designated Bushfire Prone on a Bushfire Hazard Assessment Map a set methodology, previously endorsed by Council, is utilised.</p>
7.	Palmerino Ronci 37-41 Burlington Street NAVAL BASE WA 6163	We believe each property needs to be assessed under its own merits, we also believe City of Cockburn must be diligent in its assessment of properties within the Bushfire Prone Area, and that given past land used on many site (including Lot 2) (particularly in the resource zone) these are devoid of any natural vegetation and pose no inherit fire risk. As such these sites should be	The identification of a broad area of Cockburn as a Special Control Area – Bushfire Prone will enable the Council to do further

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		<p>excluded.</p> <p>Also the discussed 'Protocol' should be made transparent, clear and applied consistently</p>	<p>more detailed mapping and identification of areas that have a direct bushfire threat. Identification on the Bushfire Hazard Assessment Map will require approval from Council and also community consultation with affected landowners.</p>
8.	<p>Andrew Miller 140 Holmes Road MUNSTER WA 6166</p>	<p><b>Objection</b></p> <p>I do not support the planning amendments in their current form. I feel the neglect the chance of bushfires and do not adequately address the interest within the City of Cockburn. There are a significant number of residents properties that are less than 100m from bushland and the amount of properties could well increase with urbanisation within the City limits. The amendment also appears to neglect the impact of ember attack from bushfires.</p> <p>The threat from dirbane ember entering evaporative air conditioning limits and roof space extend for up to a kilometre from the fire front. This perimeter includes a large number of residentially zoned areas surrounding parkland and rural blocks within the City of Cockburn.</p> <p>I feel it is important to consider inappropriately constructed residential properties and their contribution to propagating a bushfire. As such properties within 2 KM envelope from rural and parkland should be constructed in accordance with the relevant bushfire attack level as detailed in Australian Standard as 3959-2009.</p> <p>Area within this envelope have be highlighted in green in the attached map. The yellow outline indicated properties that are less than 100M from rural or parkland.</p>	<p>Land within new residential estates is primarily subject to the structure planning process. A requirement of this process is the preparation of a fire management plan. Through the Structure Planning process Council is able to have the fire risk of an area mapped and solutions to address any risk managed.</p> <p>Moreover, land in built up establish urban environments have varying conditions to those in a rural setting remove the need for broad scale identification as bushfire prone. The provision of hydrants, scheme water, fire fighter response times and also street design overall lower the risk and increase survivability during a bushfire incident.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
9.	John Fraser 16 Eagle Drive JANDAKOT WA 6164	<p>We note the proposed Bushfire Prone Area (BPA) is close to our property on Berrigan Drive, while we have no issue with additional controls being placed on development within a BPA, we do have a concern about the council's intention to buffer the SPA's by 100m.</p> <p>Our property is almost all within 100m of the proposed BPA. Any additional controls on property development within the 100m "buffer" would be prejudicial and in our opinion unreasonable.</p> <p>We seek clarification on the implications of the proposed 100m "buffer".</p>	<p>Only land Zoned Rural, Rural Living, Resource and Conservation will be subject to the requirements laid down in proposed Clause 6.6 of the Scheme.</p> <p>The 100m buffer refers to the methodology utilised in identifying bushfire prone areas on any future Bushfire Hazard Assessment Map. Although this buffer may in theory extend beyond the boundary of the Special Control Area only and within the Special Control Area can be required to meet higher planning and building controls.</p>
10.	Corinne Franklin, Vice President of the Banjup Residents Group Inc 176 Gibbs Road BANJUP WA 6164	<p><b>Objection</b></p> <p>On behalf of the Banjup Residents Group Inc, in the position of Vice President, Banjup Residents Group Inc is against the proposed scheme amendment No. 92 to city of Cockburn town planning scheme No. 3 – creation of special control area (bushfire prone areas) and associated amendments.</p> <p>The Banjup Residents Group Inc has a membership of over 300 residents in 167 households in Banjup.</p> <p><b>1. BANJUP RESIDENTS' POSITION</b> Banjup residents acknowledge that landowners in the area have duties and obligations to care for and protect their properties and the environment. Not least among these are fire safety obligations. Banjup residents appreciate that the recommendations arising from the two recent bush fire enquiries are sensible and should be implemented, not least for our own protection.</p> <p><b>2. CITY OF COCKBURN HAS UNFULFILLED OBLIGATIONS TO RATEPAYERS</b> A main concern is that the City of Cockburn is continually putting more restrictions on owners of rural blocks but cutting back or not fulfilling its obligations to the ratepayers of Banjup. It is the role</p>	<p>Objection noted and partially supported.</p> <p>Response to 2.1: Comment noted. The location the Banjup volunteer bush fire brigade is not a matter subject to Amendment 92.</p> <p>Response to 2.2: Comment noted. The City has responsibilities, under the Land Administration Act, to maintain its property and land vested in its control, including road verges. However, such matters are not subject to Amendment 92.</p>

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		<p>of the local authorities to provide and maintain sufficient infrastructure to look after its ratepayers. Banjup has a rural setting and therefore has different requirements compared to a compact housing estate. If the City of Cockburn requires Banjup ratepayers to comply with more regulations, in return we request the City of Cockburn to fulfil or adopt these fire safety initiatives in Banjup:</p> <p>1) Maintain our fire services, namely the Banjup volunteer bush fire brigade, which is strategically positioned in the middle of Banjup. Council's and FESA's frequent attempts to move the equipment/water tanker/facilities represents a far greater risk to the safety of Banjup residents than the marginal benefits proposed in this amendment. If Council was serious about fire safety (and not concerned about related cost) why does it keep proposing to directly increase our risks so that it can save money?</p> <p>2) Cut back vegetation on roadside verges, particularly in cui-de-sacs. For example, repeatedly over the past 2 years we have requested the City, through the Mayor and others, to cut back the scrub trees along Scofield Place, yet nothing has been done. We note from the minutes of the Bushfire Reference Group that council officers are pressing for higher penalties with no warnings for fire break infringements and yet those same officers do little to improve the fire safety of the lands for which they are responsible.</p> <p>3) Optimize escape routes from cui de sacs, many of which are severely overgrown on the verge and present a significant fire hazard to those trying to make an escape from adjacent houses. Similarly, most roads in Banjup lack lane markings or reflectors, which could help residents safely evacuate at times of low visibility due to smoke. Remove the requirement for fire permits in the winter months. This restriction is not present in other suburbs and the associated imposition has lead to an increase in fireload in the Banjup area over the last 3-4 years since it was introduced. This is similar to the increasing restrictions on burnbacks in country areas. Your good intent is proving to be counterproductive and unfortunately increasing our risk profile.</p> <p>4) Enforce the maintenance of the firebreaks by owners. However, inspections should occur prior to the fire season commencing, say October, to ensure that the firebreaks are 3m wide and 4m tall, with a friendly reminder to owners that the tracks should be ready for truck access. The current system of issuing notices in the searing heat of summer is not a collaborative way to manage this issue. This will give time for owners to remove obstructing vegetation before the 30 of November.</p> <p>5) Providing/sponsoring/subsidising underground power to Banjup residents. Most residents require electricity to pump water and help combat bush fires. Burying power lines would ensure the availability of electricity in bushfire situations.</p>	<p>Response to 2.3: Comment Noted.</p> <p>Response to 2.4: Comments Noted. The matter of fire breaks inspections their timing and issuing of notices is not a matter subject to Amendment 92.</p> <p>Response to 2.5: The undergrounding of Power is not a matter subject to Amendment 92.</p> <p>Response to 2.6: Comment noted. The City has responsibilities, under the Land Administration Act, to maintain its property and land vested in its control, including road verges. However, such matters are not subject to Amendment 92.</p> <p>Response to 2.7: The Australian Building Codes Board (ABCB) in 2010 released a set of Performance Standards for Private Bushfire Shelters. However, the ABCB notes that the Standard and private bushfire shelters are not a standalone solution to protect people in bushfires. A proposal to</p>



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		<p>6) Reduce the density of vegetation on road verges, particularly under power lines.</p> <p>7) Provide a standard design, Council pre-approved, fire shelter design that residents could install on their block, with Council providing a free on-site approval and free inspection on request, i.e. make it easy for residents to install protection facilities.</p> <p>8) Provide a 'text alert' service, so that residents can be quickly informed of fire threats, location, wind direction &amp; speed, road closure etc. Voluntary subscription.</p> <p>9) Hold education workshops for rural residents, educational material such as template fire plans, with details to be completed by residents, fight or flight, equipment, where to meet, pets etc.</p> <p>10) Let us assess and find our own balance of lifestyle and safety without imposition. Respect our rural lifestyle,</p> <p>3. DEVIL IN THE DETAIL OF IMPLEMENTATION</p> <p>3.1. New and enhanced houses</p> <p>The BRG notes the City's commitment that the proposed changes to the Town Planning Scheme and building regulations applies to new houses built in Bush Fire Prone Areas, such as Banjup, and is not retrospective on existing houses, except when the house is 'expanded'.</p> <p>Representatives of the BRG have met with Cockburn officers and have come to understand that the proposed changes will only apply to existing houses if the expansion is 'major'. We submit that 'major' is too subjective a term to use in what could, in some instances, become an emotional debate between house owner and council. We submit, therefore, that Cockburn, in consultation with ratepayers, adopt a 'Local Planning Policy' that clearly defines what a 'major' expansion is.</p> <p>We submit that the 'Local Planning Policy' should define a "major expansion" as that which expands the 'footprint' of the existing residence by more than 50% or which would make an upper storey on an existing residence more than 50% of the existing footprint.</p> <p>3.2. Sheds, garages and other outbuildings</p> <p>The City's proposal speaks only of expansions to existing houses. It is silent on new or changed outbuildings, such as sheds, garages, and stables. We submit that if a new or changed outbuilding is further than 20 metres from an existing residence, then the proposed regulations would apply only to it and not to the existing residence. The same definition of a major expansion under a Local Planning Policy would apply equally to a change to an existing outbuilding as to an existing residence.</p>	<p>construct such a structure would be construed as development and require approval as per the City's Town Planning Scheme No. 3.</p> <p>Response to 2.8: The provision of a text message based alert system is a matter for the State Government through FESA.</p> <p>Response to 2.9: Comment noted. The City currently undertakes a number of education campaigns concerning fire preparedness, management and response.</p> <p>Response to 2.10: The City is proactively seeking to deal with bushfire risks, through ensuring the risk posed by bushfire prone areas are recognised and dealt with through all relevant planning, subdivision and development considerations. The disincentives of imposing higher building costs thorough bush fire designation; such as lowering property prices, insurance issues and</p>

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		<p>We accept that if a new or changed outbuilding is closer to an existing residence than 20 metres, then the footprints of the residence and outbuildings may be aggregated for the purpose of determining a 'major expansion'.</p> <p>3.3. Oversized clearance zone</p> <p>The 20m zone around a house is an excessive clearance footprint compared to the block size. Most blocks are 100m wide. With a 3-4m firebreak on either side, a house of 20 to 30m wide and then a potential 20m clearance again on either side. All we are left with is a narrow corridor of typically sparse vegetation of about 15m either side between neighbours.</p> <p>What "vegetation" can be expected around the house? Only lawn? High on water consumption that will further lower our water table. Banjup sits on the Jandakot water mound. We rely only on bore and rain water for water consumption. Also with lawn comes fertilizers that pollute the water we drink! This amendment will turn Banjup into a suburb of private footy ovals and cricket pitches, not the rural image we all came here to live for.</p> <p>4. BANJUP'S LANDSCAPE</p> <p>We understand that one of the reasons the City is proposing this amendment is the concern generated by recent bush fires in Margaret River and Roleystone. It should be noted that the terrain in Banjup is different. The landscape is flat and sparsely vegetated, with most trees no more than 7m tall. The ferocity and consequence of the above bush fires was exacerbated by the hills and dense trees with canopies reaching 30m in height, which combined caused the fire to accelerate and was thus uncontrollable. It is also believed that the vegetation in Banjup presents a smaller fireload on the ground.</p> <p>Banjup is close to water sources for helicopters to refill during bush fires (Harvest Lake for example).</p> <p>Banjup has a good network of road and tracks for bush access.</p> <p>5. THE CONTRADICTION</p> <p>Landowners are not allowed to cut native vegetation.</p> <p>Landowners are supplied with grants to plant native vegetation, but this amendment would have us clear areas of -1 000m<sup>2</sup>, a significant area of the block.</p> <p>Lawns are likely to be used as the buffer, contrary to your desire to minimize pollutants and water</p>	<p>potential developers viewing the imposition of additional construction standards as a disincentive to invest must be carefully weighed against the wider responsibility of Local Government</p> <p>Response to 3.1: Amendment 92 will provide NO powers to retrospectively require residents of Bushfire Prone Areas to upgrade their residences or outbuilding to the higher Australian Standard. The City however encourages residents to properly address and recognise the fire risk that may apply to their land.</p> <p>Amendment 92 will require all new single houses, extensions, outbuilding and swimming pools to undergo planning approval when identified as bushfire prone on a Bushfire Hazard Assessment Map. This will allow for those items noted in clause 6.6 of the Scheme Amendment to be statutorily imposed as a condition of planning approval.</p> <p>Where proposals to extend</p>

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		<p>usage.</p> <p>Council is trying to urbanize the rural lifestyle, but is selective in the services it provides us (e.g. no deep sewerage, scheme water, street lighting, pavements). It is noted that Council intends that residents will fund the cost of their urban based impositions on rural constituents. An example of this urban mindset would be the Fire Permit system, imposed even in mid winter, a system that no other council has imposed in these months, and the permit itself given the alarmist title, "Permission to set fire to the bush" when typically all we want to do is burn a pile of sticks?</p> <p><b>6. ENVIRONMENTAL CONSEQUENCES</b></p> <p>Many houses in Banjup are more than 20-30 years old and were not designed according to passive solar design principles. Clearing all vegetation around the house will increase the heat load in summer and accordingly increase the use of air conditioning, electricity consumption (cost) and associated greenhouse gases due to the lack of shade. Vegetation would also provide a cooling effect to new houses.</p> <p>In summer; when coming home from the city, the temperature drop once leaving the freeway and entering Banjup is noticeable, about 4-5 deg C less, an affect we attribute to the rural vegetation. The reduction in vegetation will play a role in increasing background temperatures throughout the suburb and may well lead to heat stress (death) of the remaining vegetation.</p> <p>Banjup's wildlife (bandicoots, snakes, bobtail lizard, etc) will be reduced.</p> <p><b>7. COST &amp; LIFESTYLE</b></p> <p>Whilst we appreciate the intent is to enhance the safety of Banjup residents, we are concerned that the amendments will detrimentally affect the value of our assets due to the liabilities that you are imposing upon them. Extensions to existing properties and new builds will be saddled with the cost of large water tanks, clearance of vegetation, upkeep of lawns, higher cooling costs, greater levels of insulation and a neighbourhood where we face of against our neighbour. Residents of Banjup accept the risk associated with their rural lifestyle, that is why we chose to live here, rather than the urban development the City seems intent on turning this into.</p>	<p>of alter an existing dwelling is proposed in a Bushfire Prone Area that proposal will be required to also appropriately address the bushfire risk of the area. Where a minor addition is proposed there is no intention to require the upgrade of the entire dwelling or the building of the extension to ASC2959-2009. Such requirements will only be required where an extension is deemed 'major'. It is seen as appropriate for consistency for residents, developers and City staff that such a distinction be defined and outlined through a planning mechanism.</p> <p>Response to 3.2: ASC2959-2009 notes requirements for outbuildings when proposed in a Bushfire Prone Area. For the most part when outbuildings are proposed in a Bushfire Prone Area such dwellings must meet the requirements of the dwelling or be appropriately fire separated when they are located with 6m of a dwelling.</p> <p>Response to 3.3: The Fire</p>

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			<p>Separation Zone or Hazard Separation Zone is usually a 20m wide buffer between the dwelling and the bushfire hazard. The vegetation is such an area must be of a reduced fuel load. Such an area does not have to be devoid of native vegetation.</p> <p>Response to 4: The City is not in a position to make comment on the proportional fire risk of its suburbs compared to Margaret River or Rolleystone. However, any Bushfire Attack Level Assessment will take into consideration the topography, vegetation, fuel load and other site specific factors.</p> <p>Response to 5: Comments noted. Such items have been responded to in preceding responses.</p> <p>Response to 6: As noted above those premises not undertaking development will not be required to undertake any action. As such there will be no required clearing of the vegetation around their house.</p>

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			Response to 7: Final comments noted.
11.	Frank Kroll, Water Corporation PO Box 100 LEEDERVILLE WA 6902	<p><b>Support</b></p> <p>Thank you for your letter dated 26 June 2012. The Water Corporation does not object to the application, subject to the following advice.</p> <p>It is noted the land is zoned is primarily Rural, and the land is predominantly not serviced not serviced by a scheme water supply. A potable scheme water supply consists of reticulation mains (below 300mm in diameter) and distribution mains which are designed to comply with the Corporation's Operating Licence and other criteria concerning the Department of Health, Department of Water and Australian Drinking Water Guidelines for the purpose of supplying drinking water.</p> <p>Reticulation mains have hydrants in accordance with the Corporation's Design Manual. Distribution mains through the area provide conveyance to their destinations and are not normally tapped for other purposes.</p> <p>Considering the above, the key bushfire protection measures should focus on fire prevention, as scheme water supplies will be spatially very limited for fire fighting in the areas designated.</p> <p>The Water Corporation's bushfire protection measures in Bushfire Protection Areas includes creating a sterile 20m (minimum) zone around its facilities such as pump stations, tanks and other above ground assets, as well as installing ember screens to buildings, and removing gutters from buildings in bushfire prone areas.</p> <p>The rezoning proposal is therefore noted, and the Corporation's assets will be classified in asset management plans for protection.</p> <p>Should you have any further queries, please do not hesitate to contact the undersigned.</p>	Submission Noted
12.	Greg & Mary Devlin 72 Collis Road WATTLEUP WA 6166	<p><b>Objection</b></p> <p>We believe this proposal is to a large extent an overkill of a problem which needs to be addressed from various authorities' ends rather than land owners. The majority of bushfires have been started by poor management of burns by DEC exuberated by some stupid demarcation lines between FESA and DEC and seldom have been started by owners.</p> <p>We believe that what is proposed regarding Bushfire Prone Areas is flawed on many fronts and an unnecessary overkill. Throwing more regulation at a problem is not the way to fix it. We need</p>	Amendment 92 identifies all land zoned Rural, Rural Living, Resource and Conservation under the Town Planning Scheme as being Bushfire Prone with a Special Control Area. The purpose of this Amendment is in part to

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		<p>to keep to simple and common sense prevention measures rather than stack on a raft of regulations and costs If an area is considered fire prone, we should make it NOT FIREPRONE. This would also mean that surrounding areas would need to made NOT FIREPRONE as well. (natural bush, Water Authority land, etc)</p> <p>As far as we can see, most of the Wattleup area is far safer than many hills areas. Where tall trees abound and cover most houses for kilometres on end. The hills terrain often makes access difficult as well. Almost no construction could save houses in that situation short of thick concrete walls and rooves and steel shutters over the windows.</p> <p>OWNER OCCUPIED LAND.</p> <p>In our area, most of the blocks are to a large extent cleared. There is road access front and rear and even the treed blocks also often have side access via neighbours' land as well. This is in addition to the fire breaks already required. We already have firebreak regulations which are policed and enforced. These have worked well in the past, and fire awareness has increased a lot more than years ago. Do not let us get bogged down in more red tape in trying to fix a nonexistent problem. Even for burning off in winter, a Council permit is required and a Ranger inspects the area before a permit is issued.</p> <p>If a fire were to start on an owner occupied block, the chances of containing it quickly are far and away greater than on the large areas.</p> <p>Latitude 32 land and Water Supply land in Henderson Road.</p> <p>These areas of land are often way larger and more heavily treed than the private land. Yet they are not included in the BPA. Why?? We see more of a threat coming from these areas than the private ones. Any recent large fires in this area have been from similar such places. Once it starts, containment is almost impossible until it reaches a road or other open space.</p> <p>If bushfires can be prevented from starting in these large areas, or effectively and quickly extinguished, there will be no need to alter the construction of houses.</p> <p>We also know that firebugs have been responsible for many fires too. Here again, they are generally started on these large areas, seldom on a private property.(detection is unlikely and they get more" bang for their buck") As an example, the Water Supply land has one firebreak around it, yet a similar sized owner occupied area can have 30 odd lots, all with a firebreak around each and far better access. It is obvious as to which is the safer area. We need to look at and address dangers where they actually exist (Cadastral maps actually show the Water supply area as many separate blocks. Should each be firebreaked?)</p>	<p>ensure that development affectively addresses the level of bushfire hazard applying to the land.</p> <p>Further mapping will be undertaken as part as part of the establishment of a Bushfire Hazard Assessment Map. This will take a more site specific approach to the identification of bushfire risk to current and future residences.</p> <p><b>OWNER OCCUPIED LAND</b></p> <p>A future bushfire hazard assessment map will allow Council to look at the site specific areas before requiring higher building and planning requirements. Areas not meeting the methodology set down for identification of bushfire prone land will not be indicated as such. These areas would not be subject to higher building and planning standards.</p> <p><b>Latitude 32 land and water supply land</b></p> <p>Land within the Latitude 32 area is regulated by the Hope Valley-Wattleup</p>

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		<p>The proposal is choosing to ignore these areas and is labelling the private land as fire prone!!!???. Let us check the perspective, here, for Goodness sake.</p> <p><b>BUILDING REGULATIONS</b></p> <p>These can certainly be examined, but they are already causing a lot of grief to owners and builders alike as per the current upgraded regs. Let us get those sorted out before we embark on more.</p> <p><b>INSURANCE</b></p> <p>Once an area is listed as BPA, it is a fair assumption that Insurance Co's will raise the premiums. This happened in flood prone areas, often to the tune of several thousand dollars. Households have already had massive power increases, water charges (for those who have water) are set to rise, also, so we do not need anymore. We now have the flow on from the Carbon Tax soon to come (anyone who says there will no flow on would also see Elvis at the local shops).</p> <p><b>FESA AND DEC</b></p> <p>We have a ridiculous situation where a demarcation existed between these two bodies. We know this is finally being addressed and a totally incompetent Minister has been removed from office (at last), so this area of stupidity will, hopefully be cleared up. Surely a fire is a fire, so whoever can attend should be able to deal with it, no matter who or where they are from. The Kelmscott fires may not have been so severe if this situation did not exist.</p> <p>It is noticeable that the severity and possibly the amount of fires has increased in recent years. Yet more land has been denuded of trees by developers. This points to some sort of mismanagement occurring. It has been reported that the admin staff in FESA numbered about 40 some 10 years ago, but now numbers 400 with no increase in fire fighters "on the ground" Yet we now have, allegedly, a major bushfire problem???</p> <p>Let us get back to practical basics and common sense before embarking on more red tape and regulations.</p> <p><b>BURN OFFS</b></p> <p>Many of these fires have been caused by extremely poorly managed burnoffs by DEC. This was NOT the fault of land owners, the fires came from Govt land.</p>	<p>Redevelopment Act. The City has limited authority or control over this land.</p> <p><b>Insurance Premiums</b></p> <p>The Insurance industry has been clear that the assessment of insurance premiums takes into consideration a number of factors. The bushfire risk of an area should already be factored in even without a designation.</p> <p>In the future properties that are built to a higher standard may be subject to lower premiums due to the higher level of survivability during a fire event.</p>

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		<p>LANDOWNERS RESPONSIBILITY</p> <p>As is largely the case now, landowners do need to remove excess vegetation from around buildings and remove tall trees that are within say 10 metres of buildings Stacks of firewood or other combustible material also needs to be stored some 10 metres away. Gas bottles need to be away from any source of combustible material also.</p> <p>These are commonsense matters which need to be flagged and enforced before we charge off down the road to more regulation</p>	
13.	Mark Taylor 287 Jandakot Road JANDAKOT WA 6164	<p>The proposed amendment is a case of bureaucratic overkill. A far simpler approach of achieving the same outcome would be to amend the Town Planning Scheme to make all new residences on resource (rural) land subject to meet higher building standards.</p> <p>I would like to propose that you drop your amendment and adopt the approach that I have outlined as above.</p> <p>There are anomalies with your proposed amendment:</p> <ol style="list-style-type: none"> <li>1) Reserves have not been included. These reserves could in future have ranger's residences amenities buildings and other structures.</li> <li>2) Airport land has been excluded this could also in the future have residences and I believe already does so.</li> <li>3) Land that has been cleared could be excluded from your mapping. This could in future regrow.</li> <li>4) Standards are not freely available. I tried to look up the standards through the government on the internet and would have to pay to download them.</li> <li>5) The name BFP is suggestive of a hazard which might not exist on some properties and is an incorrect term to use in all cases.</li> </ol> <p>There are also down falls for existing property owners:</p> <ol style="list-style-type: none"> <li>1) The suggestive term Bush Fire Prone could turn buyers off from purchasing property even if no hazard exists.</li> <li>2) Insurance companies will obviously review this and could potentially use this as an excuse to drive up premiums unnecessarily.</li> <li>3) If land use changes in the future costly bureaucracy will drive up land costs.</li> <li>4) If you own land adjoining a BFP area it could also drive down your own property value and increase your insurance.</li> </ol>	<p>Objection noted though not supported.</p> <p>The decision to include on the Rural, Rural Living, Resource and Conservation zones in the Bushfire Prone Special Control Area was on the basis that those zones reflect the clear and present risk of bushfires. While residential areas may be subject to risks, it was considered that their existence in a fully urban environment needed to be treated differently to our rural zones which are a significantly heightened risk in terms of fire.</p> <p>The exclusion of regional and local reserves from the Amendment area is not a reflection of their level of fire risk but more a reflection on the type of use such land is utilised for. Amendment 92 is primarily focused on the</p>



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		<p>It is interesting to note that in your proposal land owners adjoin BFP areas are not required to meet any new building standards. These new building standards could possibly be closer to the bush than buildings on BFP land.</p> <p>The name BFP is suggestive of a hazard which might not exist on same properties and is an incorrect term to use. Your proposal penalises small stake holders who have done the correct thing as promoted by the shire to retain bush. This further proposed impost by the shire could result in the degradation of land by clearing.</p> <p>I would ask you to please reconsider your approach and adopt a simpler one such as my suggestion.</p>	<p>protection of human life through the implementation of higher building standards. Such development is not traditionally found within reserved land.</p> <p>With regard to land within Jandakot Airport. This land is controlled via a masterplan approved under the Commonwealth Airports Act. The powers of the Planning and Development Act are limited with regard to airport land.</p> <p>It is envisioned that Bushfire Hazard Assessment Maps will be assessed regularly due to the changing nature of bushland. Moreover, any resident can request for a re assessment of their classification at any time.</p> <p>While Amendment 92 will place additional cost impositions on landowners and developers seeking to undertake development in bushfire prone areas, these are considered not to represent a magnitude which should dissuade this being seen for the broader importance of development</p>

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			<p>being undertaken in a more appropriate manner cognisant of the bushfire risk.</p> <p>It is believed that the current wording and formulation of Amendment 92 is appropriate and offers the best approach to tackling the bushfire threat within the City. A Local Planning Policy will be created to clarify for landowners and developers a number of points raised through community consultation.</p>
14.	Miljenko Garbin 340 Wattleup Road WATTLEUP WA 6166	<p><b>Objection</b></p> <p>This land should NOT be included in a Bush Fire Prone area. The land (340 Wattleup Road) is no more within a risk area than land facing Pearse Road and land next door facing Wattleup Road which in both cases are excluded from the Bush Fire Prone Area.</p>	<p>The City has identified all rural type land as being part of the Bushfire Prone Special Control Area as these environments are more predisposed to bushfire risk.</p> <p>Further mapping will be undertaken as part of a bushfire hazard assessment process that will look at the broad area in more detail.</p>
15.	Dr Katinka Ruthrof & Dr David Savat 124 Lorimer Road BEELIAR WA 6164	<p>A number of aspects of the amendment concern us, which we have outlined below:</p> <ol style="list-style-type: none"> <li>1. By designating a property part of a Bushfire Prone Area it places an additional financial encumbrance on the owners of that land through potential increases in insurance premiums;</li> <li>2. The amendment does not outline who will be undertaking the bushfire attack level assessment, what type of training this officer will have, how often this will occur, or</li> </ol>	<p>Comments Noted.</p> <p>Concerns on the matter of insurance premiums is noted above and in the Council Agenda Report.</p>

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		<p>whether a charge to the property owner will be associated with it;</p> <p>3. The amendment does not outline whether and how bushland conservation will be taken into account within the bushfire attack level assessment; and</p> <p>4. The Amendment does not explicitly state that existing houses will not require higher building standards. In the FAQ, it states, "In general, no".</p>	<p>A Bushfire Attach Level Assessment will be required should development be proposed. There will be no requirement to undertake regular assessments.</p> <p>A Local Planning Policy will be established that will clarify how the competing priorities of environmental protection and human life can be balanced in bushfire prone areas.</p> <p>There is no authority given or power available to retrospectively force landowners to upgrade existing houses when no development is proposed.</p>
16.	<p>Warren Mitchell, Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892</p>	<p>Thank you for your correspondence dated 26th June 2012 seeking our comment on Proposed Scheme Amendment No. 92 (PSA92) to Cockburn Town Planning Scheme No. 3, to create special control areas in specific locations within your municipality that are prone to bushfire. The details of the proposed amendments have been provided to the Department of Indigenous Affairs (DIA) in your letter of June 26th, 2012 with the specific location of the areas to be affected attached in Maps 1 and 2 of PSA92.</p> <p>The information you provided has been reviewed and based on that information it is advised that there are four registered Aboriginal heritage sites currently mapped on the Register of Aboriginal Sites that meet the requirements of Section 5 of the <i>Aboriginal Heritage Act, 1972 (AHA)</i> in the area under consideration. These are: DIA 3292 (<i>Thomsons Lake</i>) DIA 3447 (<i>Mather Reserve, Banjup</i>) DIA 4311 (<i>Acourt Road, Banjup</i>) DIA 21811 (<i>Kraemer Reserve</i>)</p> <p>In addition, there are four locations that may meet the requirements of Section 5 of the <i>AHA</i> with</p>	<p>Comment Noted</p>

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		<p>further recording. There is insufficient information about these sites within the files held at the DIA for the Australian Cultural Material Committee (ACMC) to assess each location by the criteria of Section 5 of the <i>AHA</i>.</p> <p>They are currently mapped on the Register to alert the public to the possibility that there may be Aboriginal heritage sites at each location. These sites are:</p> <p>DIA 3300 (<i>Readymix Sandpit 2</i>)  DIA 4108 (<i>Readymix Sandpit 1</i>)  DIA 4309 (<i>Prinsep Road</i>)  DIA 4339 (<i>Warton Road, Banjup</i>)</p> <p>Two further locations that are noted on the Register of Sites fall within the work area intended for use in PSA92. These are:</p> <p>DIA 3301 (<i>Banjup, Calsif</i>)  DIA 3446 (<i>Barlram Swamp, Banjup</i>)</p> <p>These two locations have been previously assessed by the ACMC. Based on the information available at that time, neither was deemed to be an Aboriginal heritage site because neither satisfies the criteria of Section 5 of the <i>AHA</i>. The information about these locations is retained on the Register as stored data only; while each place may not qualify as a site under the definition of Section 5 of the <i>AHA</i>, they may nevertheless be of some importance or significance to the Indigenous Australians of the area.</p> <p>The ten locations given above are either wholly within, share common ground with, or have a common boundary with the Bushfire Prone Areas as given in PSA92. Each location also has an "open" status in the DIA Register. Thus, the data that provide the precise boundaries of each location are available to the public in the DIA's Aboriginal Heritage Inquiry System, which can be accessed via the following link:</p> <p><a href="http://~.dia.wa.gov.au/en/Site-Search/Aboriginai-Heritage-Inquiry-System/">http://~.dia.wa.gov.au/en/Site-Search/Aboriginai-Heritage-Inquiry-System/</a></p> <p>GIS data taken from the Register reveals that places of relevance to Aboriginal heritage (over and above the ten already identified) are present on the land around and between the specific plots that are the subject of PSA92. These likely comprise a mixture of registered sites, sites that have not been assessed as of the date of this correspondence due to insufficiency of information, and locations that do not meet the criteria to be identified as a site under Section 5 of the <i>AHA</i> but are on the Register because they are nevertheless important to the local traditional owners.</p>	

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		<p>Moreover, aerial photographs show that some of the terrain in this locality is of the 'wetlands' ecological type, and it has been established from both historical sources and archaeological/ethnographic research that wetlands areas were highly prized as hunting and camping grounds by Indigenous Australians in the pre-contact and early colonial eras. Furthermore, in some parts of the PSA92 area the land is relatively undisturbed by post-contact rural, urban and industrial developments. All of these factors increase the possibility that there are unregistered Aboriginal heritage sites and subsurface archaeological deposits in the area under consideration for PSA92.</p> <p>Neither the letter received by the DIA on June 26 of this year nor the information provided in the City of Cockburn website regarding PSA92 make it clear whether ground disturbance activities are a component of the proposed amendment. If any such activities are intended (for example, the creation of firebreaks by bulldozers), the potential to adversely affect Aboriginal heritage sites (both known and unknown) increases by a considerable margin.</p> <p>All Aboriginal heritage sites (whether known to the DIA or not) are protected under the <i>Aboriginal Heritage Act, 1972 (AHA)</i>. Where rezoning of land is occurring for the purposes of development, we would like to reinforce that under the <i>AHA</i> it is the responsibility of the developer to inform its personnel and agents of the heritage values in the areas in question and assess the risks of potential impacts to Aboriginal heritage sites. It is recommended that advice on compliance with the <i>AHA</i> be provided to all parties affected by the proposed rezoning.</p> <p>Please find below a link to our Due Diligence Guidelines for assistance that will help in identifying the risk that proposed activities may have on adversely impacting Aboriginal heritage values:</p> <p><a href="http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20managementiAHA">http://www.dia.wa.gov.au/Documents/HeritageCulture/Heritage%20managementiAHA</a> Due Diligence Guidelines.pdf</p> <p>Should cultural material or a new site be discovered, there is an obligation under section 15 of the <i>AHA</i> to report the information to the Registrar of Aboriginal Sites .If you have any questions regarding this matter, please contact Senior Heritage Officer Warren Mitchell on (08) 6551 8136 or Warren.Mitchell@dia.wa.gov.au.</p>	
ci17	Urban Design Plan Development PO Box 1820 FREMANTLE WA 6959	<p><b>Final Submission</b></p> <p>This is the completed submission by urbanplan lodged on behalf of the owners of Lots 11 and 74 Beenyup Road Banjup, being A. Poli and Omega Management Services Pty. Ltd. and Bellridge Corporation respectively in accordance with Form 4 and Planning Regulations 16 and 20.</p> <p><b>Summary</b></p> <p>Urbanplan submits the proposed Scheme Amendment No. 92 is inadequate as it is a gross</p>	<p>Objection noted though not supported.</p> <p>Structure Planning is an appropriate approach to undertake the inclusion of bushfire prone</p>

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		<p>attempt to apply a head of power to formally implement Bushfire Protection Guidance. While implementation powers are arguably warranted for fire management, in summary, urbanplan is of the view that:</p> <ul style="list-style-type: none"> <li>• fire management should not strictly apply the 'bushfire' approach as used in rural areas</li> <li>• fire management should be addressed in the urban context</li> <li>• a more concise site specific application is warranted through Structure Plan provisions.</li> <li>• this concise approach should integrate wetland buffer management.</li> </ul> <p>A site specific and managed approach to urban fire and wetland buffer implementation will enable:</p> <ul style="list-style-type: none"> <li>• quick response defensible development</li> <li>• maintenance of functional wetland buffers</li> <li>• management responsibility placed onto property owners with</li> <li>• governance powers administered by Local Government through Structure Plan powers and Fire Break Orders and</li> <li>• application of the Australian Standard 3959.</li> </ul> <p><b>Observations On Proposed Scheme Amendment No. 92</b> It is evident the objective of Scheme Amendment Number 92 is to provide Heads of Power to ensure bush fire management is applied.</p> <p><b>urbanplan's</b> concerns about the proposed Amendment are numerous. The Amendment:</p> <ul style="list-style-type: none"> <li>• implies a presumption Against development within 100 metre Bushfire Prone Buffer</li> <li>• offers no criteria by which Fire Prone areas are mapped</li> <li>• applies Bushfire Protection Guidance to an Urban fire management situation where response times to fire risk are greatly reduced</li> <li>• defines areas prone to bush fire simply based on Cadastre based and not geomorphologic site or data assessment.</li> </ul> <p><b>urbanplan's</b> objective in lodging this submission is to demonstrate the ability to apply site specific urban fire assessment and management to Lots 11 and 7 4 as indicated in the advertised Amendment:</p> <p>"land owners retain a right to request review of the classification of their land on any Bushfire Hazard Assessment Map"</p>	<p>requirements. However, as the majority of the City and the entirety of the City's rural, rural living and resource zones do not feature a Special Control Area – Development Area overlay over the base zoning, therefore such an approach is not feasible. Moreover, the period of time it would take for these structure plans to be created, submitted and approved is deemed too great. This also assumes a Structure Plan would be submitted at all.</p> <p>Amendment 92 created a new Special Control Area (SCA), one solely concerned with the identification of bushfire prone land. This SCA covers the rural areas of the City deemed to have a greater disposition to bush fire risk.</p> <p>Amendment 92 will allow for the creation of bushfire hazard assessment maps, these maps take a more site specific approach to the identification of the more general threat of fire mentioned above.</p> <p>Only land identified on the</p>

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		<p>In lodging this submission on behalf of Bellridge Corporation, <b>urbanplan</b> wishes to ensure the City's willingness to accept and implement site specific urban fire hazard assessment and management applied through the Structure Plan provisions as supported by a Development Zone Amendment for Lots 11 and 74.</p> <p><b>Buffer To Conservation Wetland</b> In assessing and managing the fire risk, our proposal is to conserve the Conservation Category Wetland and manage the associated buffer.</p> <p>It is acknowledged part of subject land is classified Conservation Category Wetland. In accordance with the Planning Commission's guidance for management of wetlands, a buffer will be defined dependent upon values, functions and attributes of the wetland.</p> <p>Significantly, the buffer serves to protect against threats to the wetland from proposed residential development.</p> <p><b>Defining Wetland</b> A wetland is defined by the identification of geomorphic wetland attributes and analysis of their area requirements. With reference to 'tripartite' test for wetland definition, being Hydritic soils, Wetland vegetation and Hydrology, the western portion of CCW within</p> <p>Lots 11 and 74 has a rapid increase in topographic gradient and sharp delineation between fringing vegetation and upland vegetation as well as sharp decrease in groundwater depth below ground level. The wetland edge is well defined.</p> <p><b>Managed Approach to Urban fire Risk and Wetland Buffer</b> The proposed managed approach applies the Bushfire Attack Level assessment as supported and empowered by a Statutory Structure Plan associated Management Plan and Fire Break Orders. Therefore the proposed managed approach to Hazard Separation Zone and Wetland Buffers is:</p> <ul style="list-style-type: none"> <li>• Create larger Lots to edge of Wetland comprising: <ul style="list-style-type: none"> <li>o Building Protection Zone</li> <li>o Managed Parkland Hazard Separation Zone including: <ul style="list-style-type: none"> <li>Low Fire Load</li> <li>Wetland Buffer</li> </ul> </li> </ul> </li> <li>• 5 metre wide Trafficable Access Reserve including: <ul style="list-style-type: none"> <li>o Fire Fighting Access</li> <li>o Integrated fire access</li> <li>o Fire Hydrants to trafficable access reserve</li> </ul> </li> </ul>	<p>Bushfire SCA and shown on a bushfire hazard assessment map will be subject to the higher building standards and other development requirements.</p> <p>With regard to Lots 11 and 74 Beenyp Road Banjup, the matter of this submission. Should a request to rezone the land be forthcoming the inclusion of the land within the boundary of Amendment 92 will require an applicant to address the fire risk of the site as part of the rezoning process. Should the land be rezoned to an urban zone the bushfire prone SCA would cease to exist on such land. However, any future structure plan for this site would be required to address the surrounding bushfire risk to the satisfaction of the City, FESA and the WAPC.</p> <p>As such the process outlined in the submission will be undertaken should the subject land be rezoned to an urban zoning.</p> <p>However, as noted above,</p>

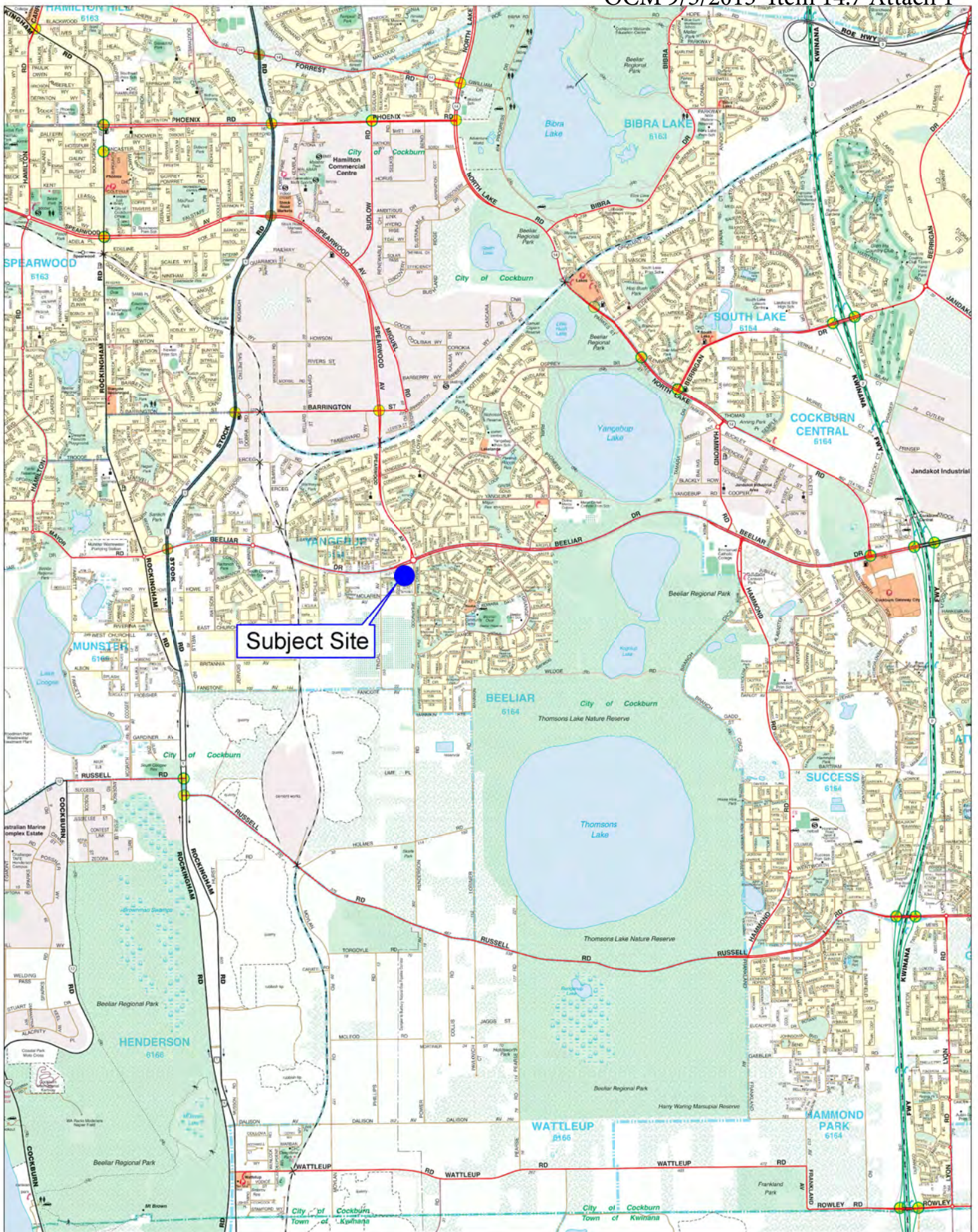
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<ul style="list-style-type: none"> <li>• Council administers Fire Regulations, statutory Structure Plan and Wetland Management.</li> <li>• Proposed residential buildings would comply with a rigorous AS 3959 fire hazard assessment.</li> </ul> <p>Refer Figure 1 Proposed Layout and associated figure demonstrating parkland edge treatment to large lots adjacent wetland.</p> <p>This approach warrants a specific portion of the site, unconstrained by the CCW, to be reclassified to Development Zone including supporting Structure Plan provisions pursuant to 6.2.6.3 of the Scheme that empower the management. Additional cost of housing construction could be applied during Building Licence procedures.</p> <p>FESA and City of Cockburn Officers would review the BAL interpretation in accordance with FESA's the Protection against Bush Fire Guidance. In due course it is requested such fire management guidance be appropriately applied to the urban fire risk context.</p> <p><b>Positive Outcomes</b>  The Proposal provides positive outcomes including:</p> <ul style="list-style-type: none"> <li>• heads of power to FESA and Council through a statutory Structure Plan process</li> <li>• management powers and access to Council</li> <li>• placing management responsibility onto property owners</li> <li>• larger lots to edge of Wetland with administered management structures</li> <li>• fire hydrants along the trafficable path to edge of wetland</li> <li>• pitches the larger edge of wetland lots to those interested in parkland wetland setting</li> <li>• alternative escapes route for existing LandCorp Subdivision</li> </ul> <p>A managed approach is superior to standard approaches that might result in:</p> <ul style="list-style-type: none"> <li>• greater fire risk and management by authorities</li> <li>• inadequate strategic fire access</li> <li>• total responsibility placed on Council.</li> </ul> <p><b>Conclusion</b>  In conclusion, while <b>urbanplan</b> has no overt objective to pursuing a Heads of Power to implement fire management, the criticism is the proposed Amendment No. 92 applies Bushfire Protection Guidance to an Urban fire management situation where response times to fire risk are greatly reduced.</p> <p><b>urban plan</b> recommends a site specific and managed approach to urban fire and wetland buffer implementation. This will have the ensuing benefits of applying urban fire management</p>	<p>such requirements cannot be implemented on rural land in areas not subject to structure planning. To take the approach suggested in this submission would be an unnecessary risk.</p>



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		<p>techniques such as Fire Hydrants to the source of risk, enabling quick response, defensible development and importantly places the management responsibility onto property owners as governed by Local Government through Structure Plan powers, Fire Break Orders and Australian Standard 3959.</p> <p>The proposed managed approach applies the Bushfire Attack Level assessment as supported and empowered by a Statutory Structure Plan and associated Management Plan.</p> <p>The proposed managed approach to Hazard Separation Zone and Wetland Buffers comprises creation of larger lots to edge of Wetland, being the responsibility of the owners, with a Building Protection Zone and managed Parkland Hazard Separation Zone separated from the fire risk by a 5 metre wide Trafficable Access Reserve including Fire Hydrants.</p>	
18	E Thorman, CLE Town Planning & Design PO Box 796 SUBIACO WA 6904	<p>This submission is lodged on behalf of Armadale Road Pty Ltd, the landowner of Lot 1 Armadale Road Banjup (Lot 1). The proposed Amendment 92 identifies Lot 1 within a Bushfire Prone Special Control Area and therefore subject to the proposed scheme text relating to Bushfire Prone Areas, specifically clause 6.6.</p> <p>Lot 1 is currently zoned Rural in accordance with the Metropolitan Region Scheme and Resource in accordance with the City of Cockburn Town Planning Scheme No.3 (TPS 3). Lot 1 forms part of MRS Amendment 1221/41 (Banjup Urban Precinct) which proposes a rezoning of the subject site from rural to urban. It is understood that this amendment is significantly progressed and is currently with the Minister for endorsement. Based on the continued favourable progression of the amendment to Parliament it is anticipated that the MRS urban zoning of Lot 1 will be effective by September / October 2012. The MRS urban zoning over Lot 1 will enable an intensification of development over the site to be determined as part of the local structure planning process and a local planning scheme amendment.</p> <p>Our understanding of Scheme Amendment 92, as it relates to Lot 1, is as follows:</p> <ul style="list-style-type: none"> <li>• Consistent with clause 6.6.1, <i>a Bush fire Prone Area means an area located in the Rural Zone, Rural Living Zone, Resource Zone and Conservation Zone, identified by the local government and shown on Bushfire Hazard Assessment Map.</i></li> </ul> <p>Lot 1 will be the subject of a future local planning scheme amendment which will amend the local planning scheme zone to one that is commensurate with the MRS urban zone (ie residential, mixed business, etc). This future local removal of the Special Control Area. In order to remove the Special Control Area zone from the subject site a bushfire hazard assessment will be undertaken. Following endorsement of the local planning scheme amendment and rezoning of Lot 1, the provisions of clause 6.6 Bushfire Prone Areas will no longer apply to the site. Any development applications subsequently lodged on the site will not be required to be supported by a bushfire</p>	Comments Noted.

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		<p>hazard assessment.</p> <p>It is acknowledged that the WAPC Planning for Bushfire Protection Guidelines sets out a range of matters that need to be addressed at various stages of the planning process and that these requirements may still apply to Lot 1 as part of the structure plan, subdivision or development process.</p> <p>Thank you for the opportunity to comment on the proposed Amendment 92 to the City of Cockburn TPS 3. Please do not hesitate to call if you wish to discuss this submission.</p>	
19	Mark Neave 133 Britannia Avenue BEELIAR WA 6164	<p><b>Objection</b></p> <p>I do not support the introduction of the proposed “special control area” for the area outlined in the Town Planning Scheme – Amendment No. 92, for the area bounded by Fanstone, East Churchill, Stock Road and the rail line. We comply with Council regulations in regard to Fire Control annually and I regard any overbearing Control as another step to a “nanny state”</p>	Objection noted though not supported.





# LOCATION PLAN LOTS 9000 NINGHAN LOOKOUT AND 9007 & 9032 BEELIAR DRIVE BEELIAR PLAN 3

Plan No. : 16055-2  
Revision : REV.0  
Scale : 1:45000@A4

0 450 900 1350

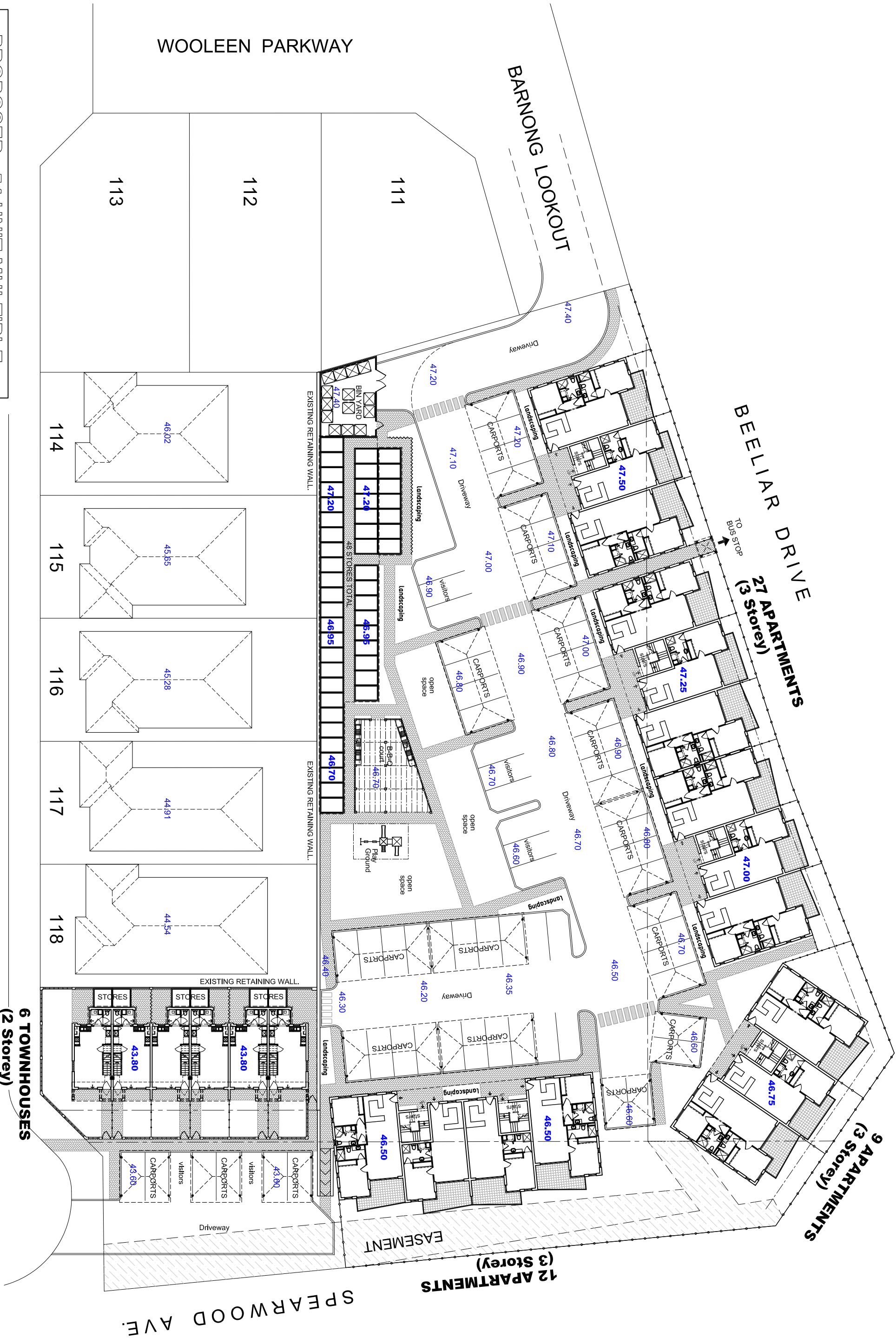
Notes:  
1. The proposed site is shown in red.  
2. All dimensions and areas are subject to survey.

Suite 4 First Floor 40 Hasler Road Osborne Park WA www.whelans.com.au

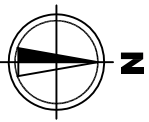
DATE DRAWN: 28/11/2012 FILE: 121129 Location Plan.dgn  
DRAWN BY: CdeL V DATUM: AHD  
CHECKED BY: JP H DATUM: MGA84 (50)







PROPOSED 54 UNIT MULTIPLE  
RESIDENTIAL DEVELOPMENT.  
LOTS 9000 & 9007 BEEILIAR DRIVE,  
BEEILIAR, WESTERN AUSTRALIA.



**SITE PLAN.**  
SCALE: 1:400 @ A3

NINGHAM LOOKOUT

11-01-2013

Architect  
John L. Silbert & ASSOCIATES  
300 Newcastle Street  
Northbridge Perth  
Western Australia 6000

dwg: **sk-8:1**

**SCHEDULE OF SUBMISSIONS**  
**PROPOSED STRUCTURE PLAN – LOT 9000 NINGHAN LOOKOUT, LOT 9007 BEELIAR DRIVE & LOT 9032 SPEARWOOD AVENUE, BEELIAR**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1	Brett Dunn, Department of Water PO Box 332 Mandurah WA 6210	<p><b>Support</b></p> <p>Thank you for the above referral dated 19 March 2013. The Department of Water (DoW) has reviewed the document and the DoW has no comment to offer at this stage of the planning process.</p> <p>If you wish to discuss the above further please contact Jane Sturgess at the DoW's Mandurah Office on (08) 9550 4228.</p>	Support Noted
2	Richard Bloor, Department of Education 151 Royal Street East Perth WA 6004	<p><b>Support</b></p> <p>Thank you for your letter dated 19 March 2013 regarding the Proposed Modified Structure Plan in Beeliar.</p> <p>The Department of Education has reviewed the document and advises that it has no objection to this Proposal.</p>	Support Noted
3	Norm Walkerden, Telstra Locked Bag 2525 Perth WA 6001	<p><b>Support</b></p> <p><b>Proposed Modified Structure Plan - Lot 9000 Ninghan Lookout, Lot 9007 Beeliar Drive and Lot 9032 Spearwood Avenue, Beeliar</b></p> <p>Thank you for the above advice. At present, Telstra</p>	Support Noted

**SCHEDULE OF SUBMISSIONS**  
**PROPOSED STRUCTURE PLAN – LOT 9000 NINGHAN LOOKOUT, LOT 9007 BEELIAR DRIVE & LOT 9032 SPEARWOOD AVENUE, BEELIAR**

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3	Norm Walkerden, Telstra Locked Bag 2525 Perth WA 6001	<p><b>Support</b></p> <p><b>Proposed Modified Structure Plan - Lot 9000 Ninghan Lookout, Lot 9007 Beeliar Drive and Lot 9032 Spearwood Avenue, Beeliar</b></p> <p>Thank you for the above advice. At present, Telstra Corporation Limited has no objection</p> <p>Any network extension that may be required for any</p>	Support Noted

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		<p>development within the area concerned, the owner/developer will have to submit an application before construction is due to start to NBN Co. or the Telstra Smart Community website: <a href="http://www.telstra.com.au/smart-community/developers/">http://www.telstra.com.au/smart-community/developers/</a> .</p> <p>More information regarding NBN Co. can be found on their website <a href="http://www.nbnco.com.au/">http://www.nbnco.com.au/</a> . I add this information about NBN Co. as it is not known when services will be available from NBNCo. Telstra may provide services if NBN Co. cannot.</p> <p>Please dial 1100 (Dial before You Dig) for location of existing services.</p>	
4	Lindsay Broadhurst, Main Roads Western Australia PO Box 6202 EAST PERTH WA 6892	<p><b>No Objection</b></p> <p>Thank you for your letter dated the 19th of March, 2013 requesting comment on the above proposed modification to a local structure plan.</p> <p>Main Roads has no objections to the proposed plan. If you require any further information please contact James McCallum on (08) 9323 4214. In reply please quote reference number 04/11588-08 (013#108689).</p>	Support Noted
5	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>Thank you for your letter dated 19 March 2013 regarding the above-mentioned proposed modification. I wish to submit my strong objection to this proposal.</p> <p>As the owner of the adjoining block, Lot 118 (11) Ninghan Lookout Beeliar, I entered into the purchase of the property on the understanding that, under the complete and adopted Cell</p>	<p>Objection Noted – Not Supported</p> <p>Objector notes that the proposal provides for an unfair disadvantage for their property but does not go into details, relating to a planning matter, beyond some concerns regarding the location of higher densities, as such it is difficult to address the concerns of</p>

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		<p>10 Structure Plan, the immediate adjoining properties were to also be developed as R20 and that within Cell 10 numerous pockets of higher density R40 zones were already approved as part of that adopted Structure Plan.</p> <p>It was a conscious decision not to purchase a property immediately next to the higher density zones. It would be a mockery of the integrity of adopting such a considered Structure Plan only to for it to be simply changed later thereby destroying any confidence any purchaser could have in the Structure Plan originally adopted. What is the point of having a Structure Plan in the first place then? The proposed modification represents more than a significant unfair disadvantage to my property and therefore I submit my strong objection to this proposal.</p> <p>As per your letter I wish to have my details kept confidential.</p> <p>Yours faithfully</p>	<p>the submissioner.</p> <p>With regard to the ability to modify a Structure Plan. The City's Town Planning Scheme allows for modification to endorsed Structure Plans and it is the prerogative of any landowner to lodge such a modification if they see fit. The City must on receipt of such a modification, and payment of the required fee, assess the proposal based on planning merit.</p>
6.	Hilary Smith, Department of Environment and Conservation Land Use Planning <a href="mailto:hilary.smith@dec.wa.gov.au">hilary.smith@dec.wa.gov.au</a>	<p><b>Support</b></p> <p>The Department of Environment and Conservation Swan Region has no comments on this proposal. It is the expectation of DEC that the planning system will appropriately address environmental planning issues.</p>	Support Noted
7.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>Thank you for your letter dated 19 March 2013 regarding the above-mentioned proposed modification. We wish to submit our strong objection to this proposal.</p> <p>As the owners of the property at 10 Ninghan Lookout, Beeliar, we entered into the purchase of the property on the understanding that, under the complete and adopted Cell 10 Structure Plan, the adjoining properties were to also be developed as R20 and that within Cell 10 numerous pockets of higher density R40 zones were already approved as part of</p>	<p>Objection Noted – Not Supported</p> <p>Objector notes that the proposal provides for an unfair disadvantage for their property but does not go into details, relating to a planning matter, beyond some concerns regarding the location of higher densities, as such it is difficult to address the concerns of the submissioner.</p> <p>With regard to the ability to modify a</p>



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		<p>that adopted Structure Plan.</p> <p>It was a conscious decision not to purchase a property immediately next to the higher density zones. It would be a mockery of the integrity of adopting such a considered Structure Plan only to for it to be simply changed later thereby destroying any confidence any purchaser could have in the Structure Plan originally adopted. What is the point of having a Structure Plan in the first place then? The proposed modification represents more than a significant unfair disadvantage to our property and therefore we submit our strong objection to this proposal.</p> <p>As per your letter we wish to have our details kept confidential.</p>	<p>Structure Plan. The City's Town Planning Scheme allows for modification to endorsed Structure Plans and it is the prerogative of any landowner to lodge such a modification if they see fit. The City must on receipt of such a modification, and payment of the required fee, assess the proposal based on planning merit.</p>
8.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>We believe the zoning change from R20 TO R60 is excessive. From 11 dwellings to 54 dwellings is an increase of 390% from the original structure plan. We believe the proposed amendment will put an increased strain on the existing services and be to the detriment to other Cell 10 land owners. Query for City of Cockburn Will the remaining Cell 10 landowners incur extra costs for services such as water, sewage and power if the proposed structure plan is approved?</p>	<p>Objection Noted - Not Supported</p> <p>Section 3.7 of the Local Structure Plan Report noted the available services within the immediate surrounds. The report notes the required upgrades and expansions of said services, these costs will be required to be met by the proponent. Therefore it is not expected that the increased possible dwelling yield will place an increased level of pressure on existing services in the immediate area.</p>
9.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>There are 6 townhouses facing east onto Spearwood Ave (very busy road during peak mornings and difficult to back on to!) with only two visitor bays between all of them? Also it seems there are only 14 bins for all the units to share?? There is a children's playground inside but is this not too close to the major intersection?</p>	<p>Objection Noted – Not Supported</p> <p>The proposal is related to the residential density of the site and not the development of residential apartments. A concept was included within the Local Structure Plan report to provide residents with some</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>That intersection is too busy and high density housing creates a traffic risk. Also height would cast a shadow over those homes toward the south, making them colder. It would not fit into the landscape and feeder roads do not allow for residents or visitors. What a ridiculous idea!</p>	<p>guidance and understanding as to how the land could be developed under a R60 zoning.</p> <p>It should be noted however that no direct access to either Beeliar Drive or Spearwood Avenue will be allowed from any development on the site.</p> <p>Comments regarding access, visitor bays and bin storage are therefore premature. That being said, any development proposed in the future will be required to comply with the requirements of the residential design codes and Council Policies relating to <i>inter alia</i>;</p> <ul style="list-style-type: none"> <li>• setbacks</li> <li>• overlooking</li> <li>• access</li> <li>• parking</li> <li>• bin storage</li> <li>• solar access</li> </ul>
10.	details kept confidential	<p><b>Objection</b></p> <p>It is the WRONG place for this type of project. Why do we have a relative small parcel of land, rezoned to R60? While all the land around it is R20? We don't agree with HIGH-RISE dwellings in our neighbourhood. Why is it that the real problems in our area, like the stink from the Cockburn cement plant is still an unsolved issue. Also the noise levels from Spearwood avenue and Beeliar drive are higher, and more likely 70 to 80 db at normal traffic hours. At peak traffic 85 to</p>	<p>Objection Noted – Not Supported</p> <p>Cockburn Cement is not a matter dealt with as part of this proposal.</p> <p>The subject land has an area of 0.6424 ha and forms a strategic landholding in the northern portion of Cell 10 of the Consolidated Structure</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>90 db.</p> <p>There is nothing to stop that noise, no sound barrier, no wall, no scrubs and trees. When the bus and big trucks take, off in front of our house, on the Beeliar Drive our windows rattle. We have been waiting for a bit of landscaping to stop the traffic noise, for more than three years, and only had 10 trees planted to stop the noise and the cement dust. This is a joke.</p> <p>Our comments STOP TURNING OUR NEIGHBOURHOOD, IN TO A CONCRETE JUNGLE. Address the real problems first</p>	<p>Plan. The site adjoins high frequency bus routes, is 400m from a future local centre on Beeliar Drive and is in close proximity to Neighbourhood and Regional Centres.</p> <p>Therefore such the land is well suited for medium density development.</p> <p>A preliminary noise report has been prepared as part of the Local Structure Plan report. A detailed Noise report outlining compliance with the relevant State Planning Policy will be required for any R60 proposal on the site.</p>
11.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>I'm writing to object the proposed structure plan amendment at lot 9000 Beeliar drive. The reasons we object are</p> <p>1: the disruption to sun light in our garden, the reason we brought this plot was due to the fact it has a north facing garden and so we benefit from the winter sun and as much sunlight all year round and these proposed plans would disrupt this.</p> <p>2: the noise of having the extra 54 apartments during the day and the BBQ area at night time would almost guarantee a high level of noise when trying to converse in the garden, a lot more than the original 5 plots originally planned.</p> <p>3: the traffic in the area of an extra 54 apartments could grid lock and make the easiest of journeys a lot more difficult, also the extra noise and pollution that an extra 54 apartments</p>	<p>Objection Noted – Not Supported</p> <p>With regard to point 1:</p> <p>The proposal is related to the residential density of the site and not a specific development application for residential apartments. However, any proposal to develop residential apartments will be required to comply with the provisions of the Residential Design Codes related to solar access for adjoining sites. In general this does not allow for more than 25% of the adjoining site and more than 50% of an adjoining properties outdoor living</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>would be very inconvenient.</p> <p>4: the security of our homes and family's would be more at risk due to the higher amount of people living in the area, the privacy in our garden and home would be massively reduced due to the higher volume of people and their potential guests.</p>	<p>area to be shadowed on 21 June (when shadows are longest). Therefore it is believed that this matter can be managed in a way that neighbours to the south will not be adversely affected.</p> <p>With regard to point 2:</p> <p>Noise is an unavoidable consequence of development in any urban environment. That being said, various mechanisms are in place to regulate the accepted level of noise emanating from such developments at various times of the day to ensure that the enjoyment of others is not impacted.</p> <p>A preliminary noise report has been prepared as part of the Local Structure Plan report. A detailed Noise report outlining compliance with the relevant State Planning Policy will be required for any R60 proposal on the site.</p> <p>With regard to point 3:</p> <p>The proponent has incorporated a traffic impact assessment as an appendix to the Structure Plan Report. This report notes that the expected increase in traffic from such a level of development can be handled by the existing road network. This has been verified by the City's Traffic Engineer.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>With regard to point 4:</p> <p>It is not clear as to what security issues the submission is commenting on. The design of any R60 proposal will be require the incorporation of design out crime elements and solutions as per the Residential Design Codes and Council Policies.</p> <p>On the matter of privacy, any R60 proposal will be required to adhere to the requirements of the Residential Design Codes relating to overlooking and setbacks. Further to this the Structure Plan only allows for 2 story development to occur where the subject land adjoins existing residences further ensuring that building built is sympathetic to the existing residential buildings.</p>
12.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>I don't believe high density housing as proposed "3 story apartments" should be constructed in a predominately R20 area. I feel this will greatly reduce the re sale value of neighbouring properties. 3 Storey apartment will not fit in visually in the current landscape "single, double storey residences" Increase in noise levels as a result of increased traffic &amp; number of residents on site will affect neighbouring properties.</p>	<p>Objection Noted – Not Supported</p> <p>The proposed R60 density is a medium density under the Residential Design Codes. No high density housing is proposed.</p> <p>The Residential Design Codes has provisions that deal with the need for such developments to be designed in a way to be sympathetic to the existing urban fabric of an area. Further to this the Structure Plan has been designed</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>in a way to ensure that any 3 story elements of future development will be focused on Spearwood Avenue and Beeliar Drive, away from existing lower scale residential developments.</p> <p>The proponent has incorporated a traffic impact assessment as an appendix to the Structure Plan Report. This report notes that the expected increase in traffic from such a level of development can be handled by the existing road network. This has been verified by the City's Traffic Engineer.</p> <p>Noise is an unavoidable consequence of development in any urban environment. That being said, various mechanisms are in place to regulate the accepted level of noise emanating from such developments at various times of the day to ensure that the enjoyment of others is not impacted.</p> <p>Matters of personal finance, such as land and house values, are not planning considerations.</p>
13.	Brett Coombes, Water Corporation PO Box 100 LEEDERVILLE WA 6902	<p><b>Support</b></p> <p>Thank you for your letter of 19 March 2013 seeking advice from the Water Corporation regarding the proposed structure plan modifications.</p> <p>The Corporation has no objections to the proposed changes and density code increases.</p>	Support Noted.

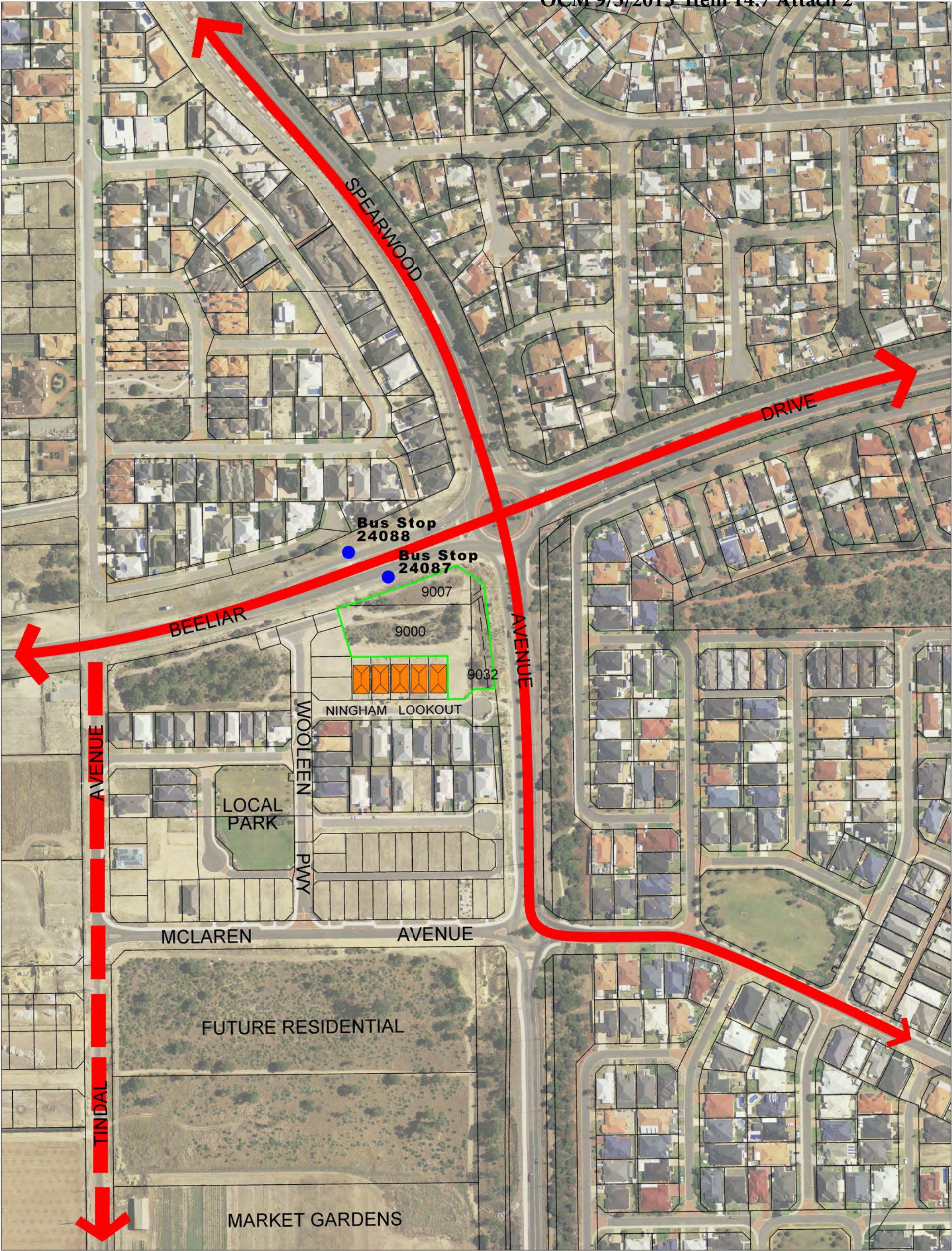
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>While the existing water and wastewater reticulation in the locality has been planned around lower densities, the proposed additional development should be able to be served from extensions and upgrades of the existing systems.</p> <p>The subject land is situated within the Thompson's Lake Gravity water supply scheme. It is noted that the third floors of the units are proposed to be at a height of approximately 47.5m AHO. The modelled hydraulic level in the reticulation system in the vicinity of the site is in the order of 69m AHO or less, which will place the upper dwellings units at close to the maximum supply limit from the gravity scheme. Prior to development, the proponent's engineering consultant will need to make arrangements with the Corporation to assess the available pressures in the nearby reticulation network to sustain adequate supply to the units. If adequate pressures are not available, then roof water storage tanks will need to be incorporated into the building design.</p> <p>If you have any further queries about water and wastewater servicing matters in this area, please call me on 9420-3165.</p>	
14.	<ul style="list-style-type: none"> <li>details kept confidential</li> </ul>	<p><b>Objection</b></p> <p>After considering the proposed structure plan amendment for Cell 10 we object to the proposal for the following reasons:</p> <p>As the owners of 3 Ninghan Lookout (Lot 114) Beeliar we were aware that the land behind our property was zoned for future residential development we are concerned however that the proposed change from R20 to R60 is inconsistent with existing residential development and will not be in keeping with the area. While there is the presence of smaller block sizes and grouped housing in support of increased housing density there are currently no multi-level apartment blocks in the vicinity.</p>	<p>Objection Noted – Not Supported</p> <p>With regard to traffic and noise. A traffic impact assessment has been prepared as part of the Local Structure Plan which shows that the existing road network can handle the expected traffic increases. This has been verified by the City's Traffic Engineer.</p> <p>Noise is an unavoidable consequence of development in any urban environment. That being said, various</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>The introduction of multi-level apartment blocks will, in our opinion as current residents, decrease the amenity of the area.</p> <p>We have concerns in regard to the additional traffic and noise that will be generated from the increased dwellings that rezoning from R20 to R60 will introduce to what is currently a quiet, cul-de-sac location. We would like to be informed of the proposed mitigation measures should noise, traffic and resident/visitor parking create problems for current residents.</p> <p>The rubbish collection point for the proposed development is located on our rear boundary fence. If the proposal does go ahead we are concerned that the odours from this will intrude on our property. Should the proposal go ahead it is our preference that the rubbish collection point be moved from our rear boundary and that appropriate safe guards (e.g. bin enclosures) are put in place to ensure that the odours do not intrude.</p>	<p>mechanisms are in place to regulate the accepted level of noise emanating from such developments at various times of the day to ensure that the enjoyment of others is not impacted.</p> <p>The proposal only relates to the residential zoning of the land and does not relate to the concept design incorporated as part of the Structure Plan documentation. This was provided to give existing landowners some insight into the type of development that could occur on R60 zoned land. Such matter would be addressed as part of a future Development Application.</p> <p>Any development proposed in the future will be required to comply with the requirements of the residential design codes and Council Policies relating to <i>inter alia</i>;</p> <ul style="list-style-type: none"> <li>• setbacks</li> <li>• overlooking</li> <li>• access</li> <li>• parking</li> <li>• visitor parking</li> <li>• bin storage</li> <li>• solar access</li> </ul>



NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>design codes and Council Policies relating to <i>inter alia</i>;</p> <ul style="list-style-type: none"> <li>• setbacks</li> <li>• overlooking</li> <li>• access</li> <li>• parking</li> <li>• visitor parking</li> <li>• bin storage</li> <li>• solar access</li> </ul>





Plan No. : 16055-1  
Revision : REV.1  
Scale : 1:2500@A3  
133 Scarborough Beach Road Mount Hawthorn WA 6010 www.whelans.com.au

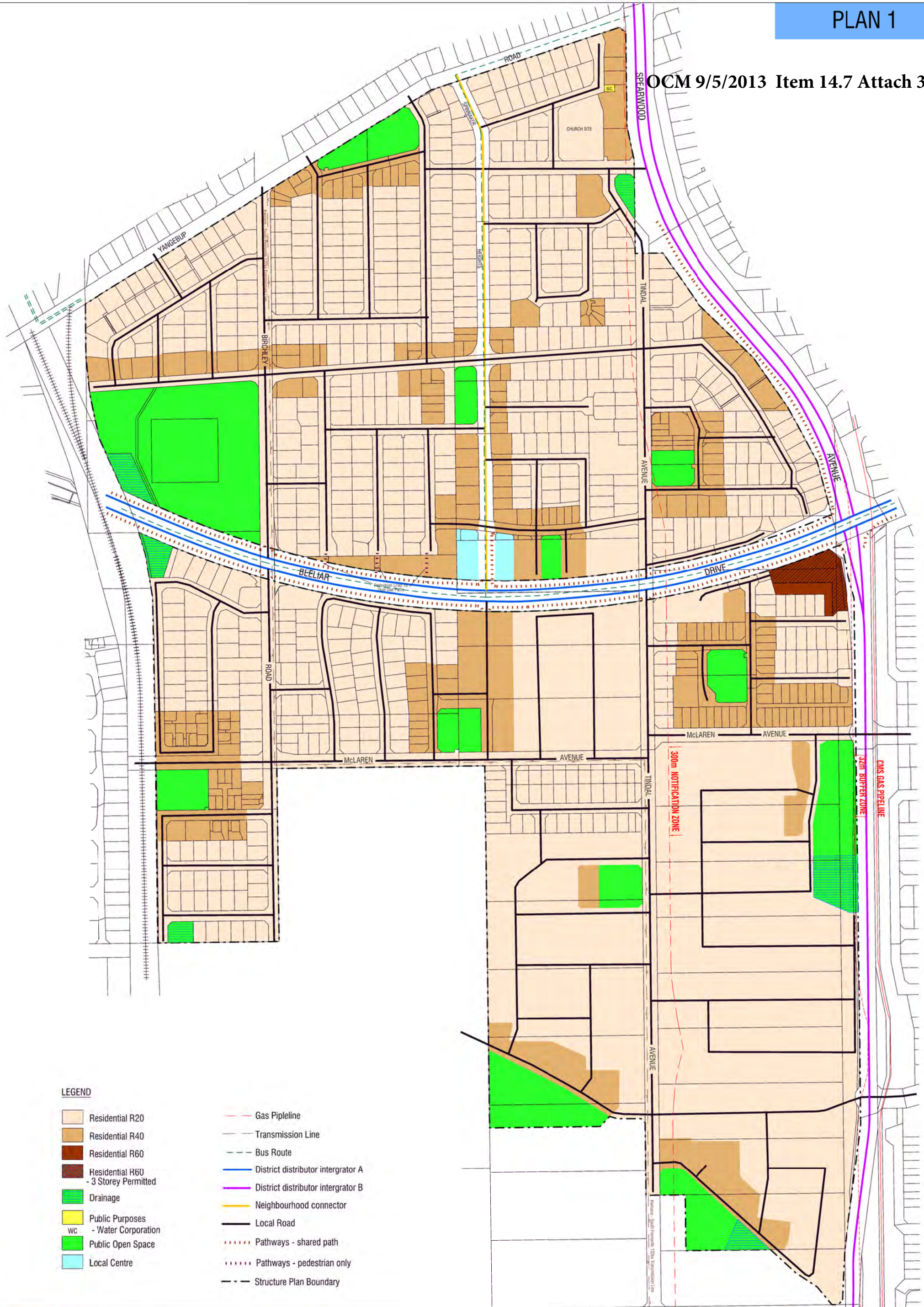
- LEGEND
- Existing Bus Route
  - Future Bus Route
  - Existing Houses (Ningham Lookout)

LOCAL CONTEXT ANALYSIS  
LOTS 9000 NINGHAM LOOKOUT  
AND 9007 & 9032 BEELIAR DRIVE  
BEELIAR  
PLAN 8

DATE DRAWN: 27/11/2012 FILE: 121121 Context Analysis.dgn  
DRAWN BY: CdeL V DATUM: AHD  
CHECKED BY: JP H DATUM: MGA84 (50)

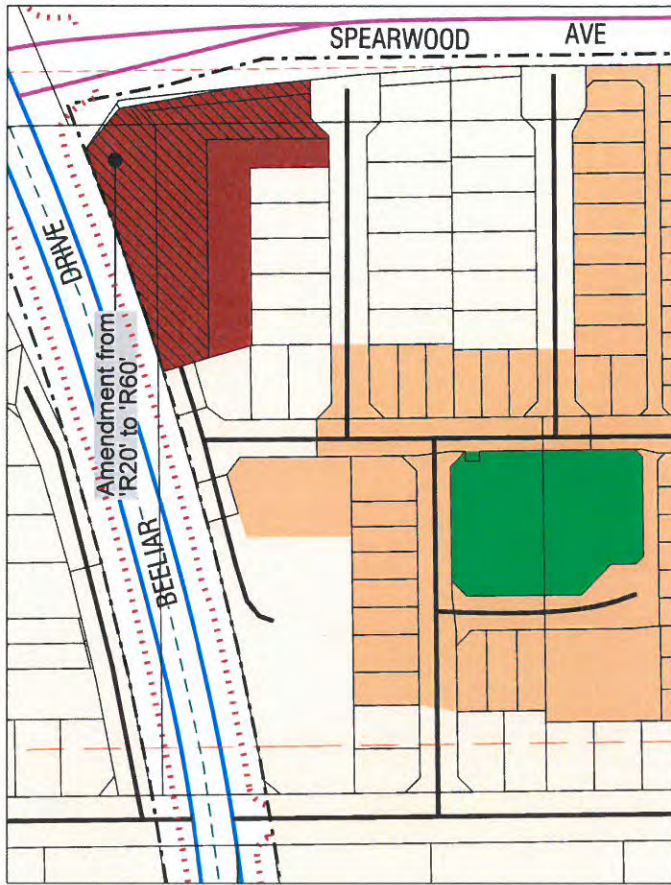






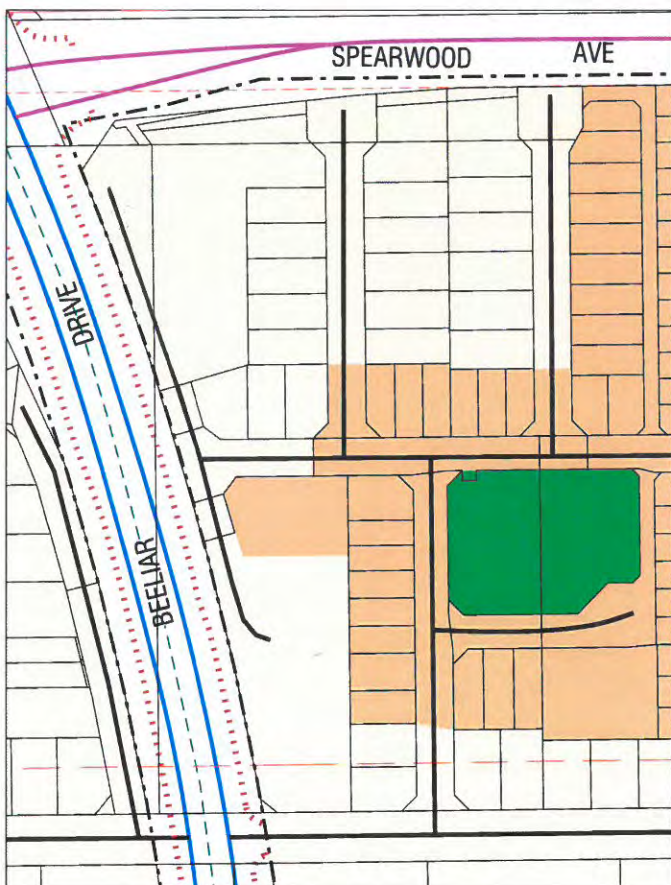


OCM 9/5/2013 Item 14.7 Attach 4



PROPOSED LOCAL STRUCTURE PLAN

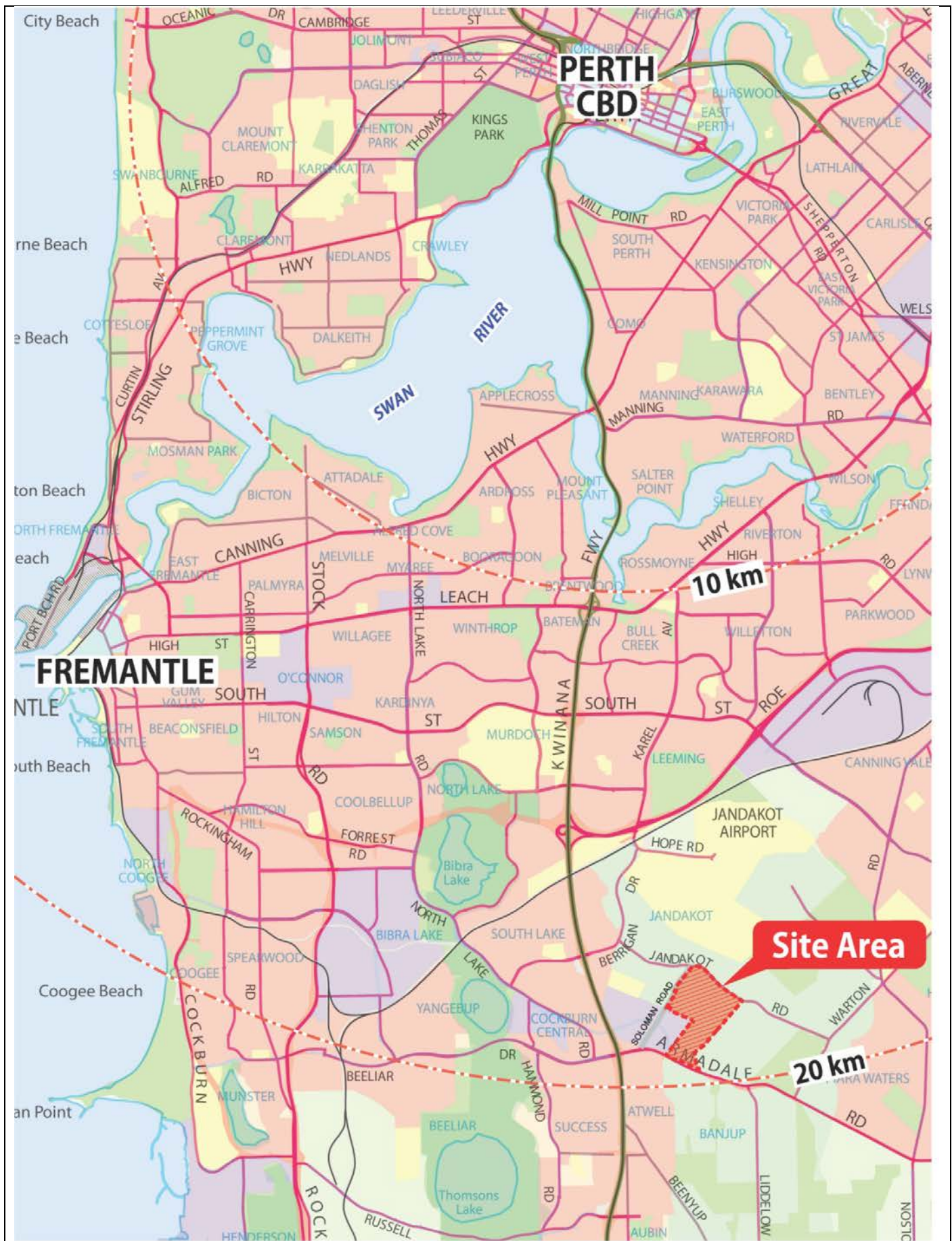
- Gas Pipeline
- Transmission Line
- Bus Route
- District distributor intergrator A
- District distributor intergrator B
- Neighbourhood connector
- Local Road
- Pathways - shared path
- Pathways - pedestrian only
- Structure Plan Boundary

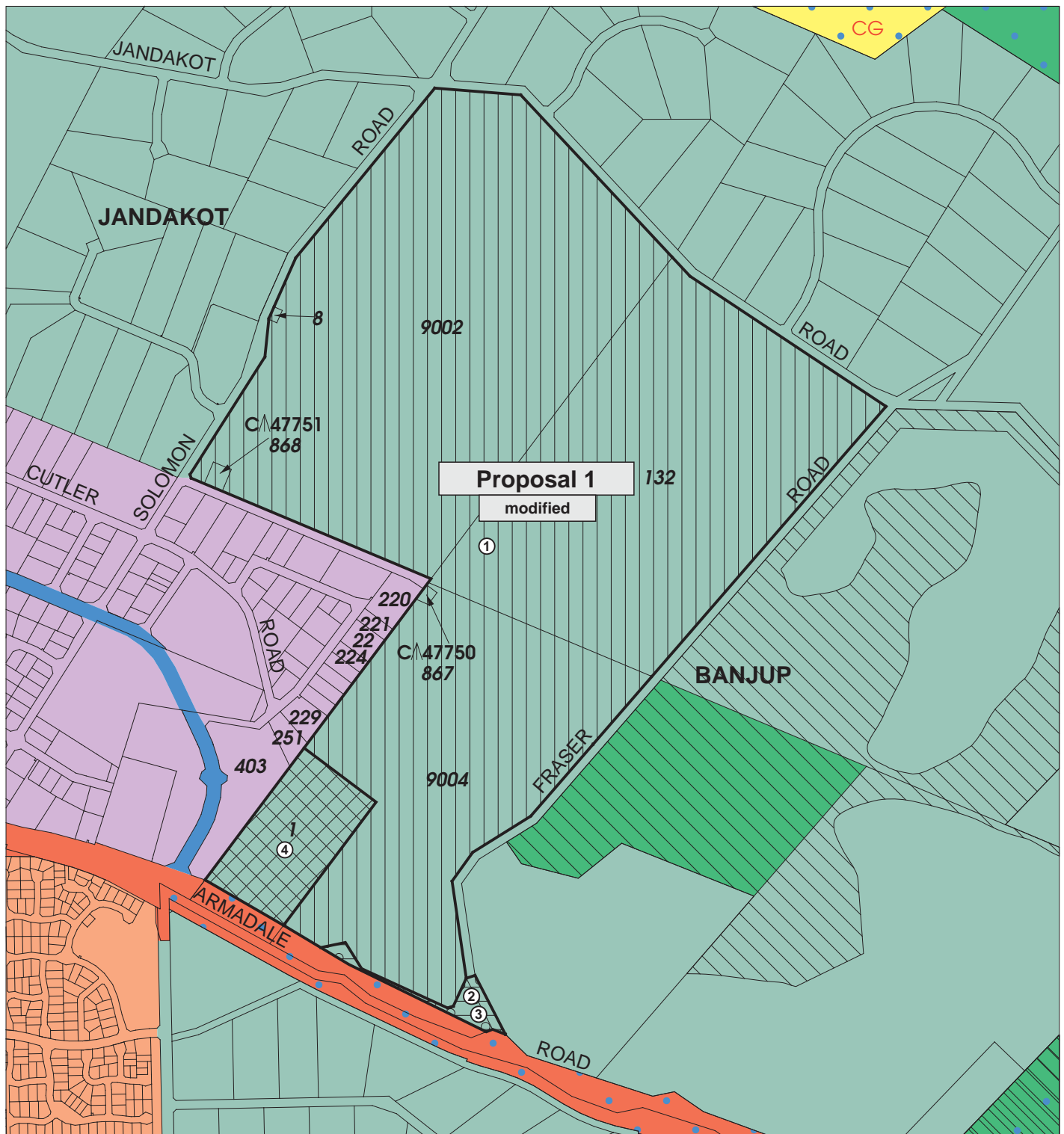


EXISTING LOCAL STRUCTURE PLAN

- Residential R20
- Residential R40
- Residential R60
- Residential R60 - 3 Storey Permitted
- Drainage
- Public Purposes
- Public Purposes - Water Corporation
- Public Open Space
- Local Centre







**Banjup Urban Precinct - proposed major amendment  
as modified**

26 June 2012

Figure 1

Legend		
<b>Proposed:</b>	<b>Existing:</b>	
① urban zone	industrial zone	other regional roads reservation
② primary regional roads reservation	urban zone	primary regional roads reservation
③ water catchments reservation	rural-water protection zone	water catchments reservation
④ urban deferred zone	CG public purposes (CG) reservation	Bush Forever area
	parks and recreation reservation	



# **CITY OF COCKBURN**

## **TOWN PLANNING SCHEME NO. 3**

### **AMENDMENT NO. 95**

SEPTEMBER 2012



# BANJUP QUARRY REDEVELOPMENT

## CITY OF COCKBURN

### TOWN PLANNING SCHEME NO. 3

### AMENDMENT NO. 95

### SEPTEMBER 2012

**Prepared for:** **Stockland Pty Ltd**  
Suite 1 / 2 Victoria Avenue, Perth WA 6000

**Prepared by:** **Development Planning Strategies**  
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SUBIACO WA 6008

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VERSION	COMMENT	PREPARED BY	REVIEWED BY	REVIEW DATE	APPROVED BY	ISSUE DATE
Revision 0		DPS	KB	120229	FA	
Revision 1		DPS	KB	120919	FA	
Revision 2		DPS	KB	120925	FA	

#### Document Status

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# ABBREVIATIONS

DA	Development Area
DCP	Development Contribution Plan
DoP	Department of Planning
EPA	[Western Australian] Environmental Protection Authority
ha	hectares
LSP	Local Structure Plan
MRS	Metropolitan Scheme Amendment
SPP	Statement of Planning Policy
TPS No. 3.	City Cockburn's Town Planning Scheme No. 3
WAPC	Western Australian Planning Commission

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PLANNING AND DEVELOPMENT ACT, 2005  
RESOLUTION DECIDING TO AMEND A TOWN PLANNING SCHEME  
CITY OF COCKBURN  
TOWN PLANNING SCHEME NO. 3  
AMENDMENT NO.95

RESOLVED that the Council, in pursuance of Section 75 of the Planning and Development Act 2005, amend the above Town Planning Scheme by:

1. Excluding Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' and including these in 'Development' zone;
2. Amending Schedule 11 of the Scheme Text to add new 'Development Area 37' as follows:

"Schedule 11 Development Areas

Ref. No.	Area	Provisions
DA37	Banjup Quarry Redevelopment	<ol style="list-style-type: none"><li>1. An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.</li><li>2. The Structure Plan is to provide for residential development, community and education facilities and a town centre.</li><li>3. The Structure Plan is to provide for safe and efficient pedestrian connections between DA37 and the Cockburn Central Railway Station.</li><li>4. Land uses classified on the Structure Plan apply in accordance with clause 6.2.6.3.</li><li>5. The Local Government may adopt Detailed Area Plan(s) pursuant to Clause 6.2.6.3 for any part of the Development Area as defined on the Approved Structure Plan. All land use and development for a particular lot or lots the subject of a Detailed Area Plan shall accord with the adopted Detailed Area Plan.</li><li>6. The standards and requirements applicable to zones and R Codings under the Scheme shall apply to the same extent to the areas having corresponding designations under the Approved Structure Plan. Notwithstanding this, an Approved Structure Plan may by a clear statement of intent to do so, make provision for any standard or requirement applicable to zones or R Codings to be varied, and the standard or requirement varied in that way shall apply within the area of the Approved Structure Plan, or any stipulated part of that area, as if it was a variation incorporated in the Scheme..</li></ol>

3. Amending the Scheme Map accordingly.

Dated this                      day of                      2012

\_\_\_\_\_  
CHIEF EXECUTIVE OFFICER



## AMENDING SCHEME REPORT

1.	LOCAL AUTHORITY	City of Cockburn
2.	DESCRIPTION OF TOWN PLANNING SCHEME:	Town Planning Scheme No. 3
3.	TYPE OF SCHEME:	District Zoning Scheme
4.	SERIAL NO. OF AMENDMENT:	Amendment No. 95
5.	PROPOSAL:	<p>To amend Town Planning Scheme No. 3 to rezone Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road, Banjup (the subject land) from 'Resource' to 'Development' and to allow appropriate Special Control Area provisions in the scheme text to control development which is the approach taken in respect to all development areas within the City.</p> <p>The specific elements of the Scheme Amendment are as follows;</p> <ol style="list-style-type: none"><li>1. Exclude Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' zone and include these in 'Development' zone;</li><li>2. Introduce a new 'Special Control Area' covering the subject land, to be known as 'Development Area 37' and formulating appropriate provisions;</li><li>3. Amend the Scheme Map accordingly.</li></ol>

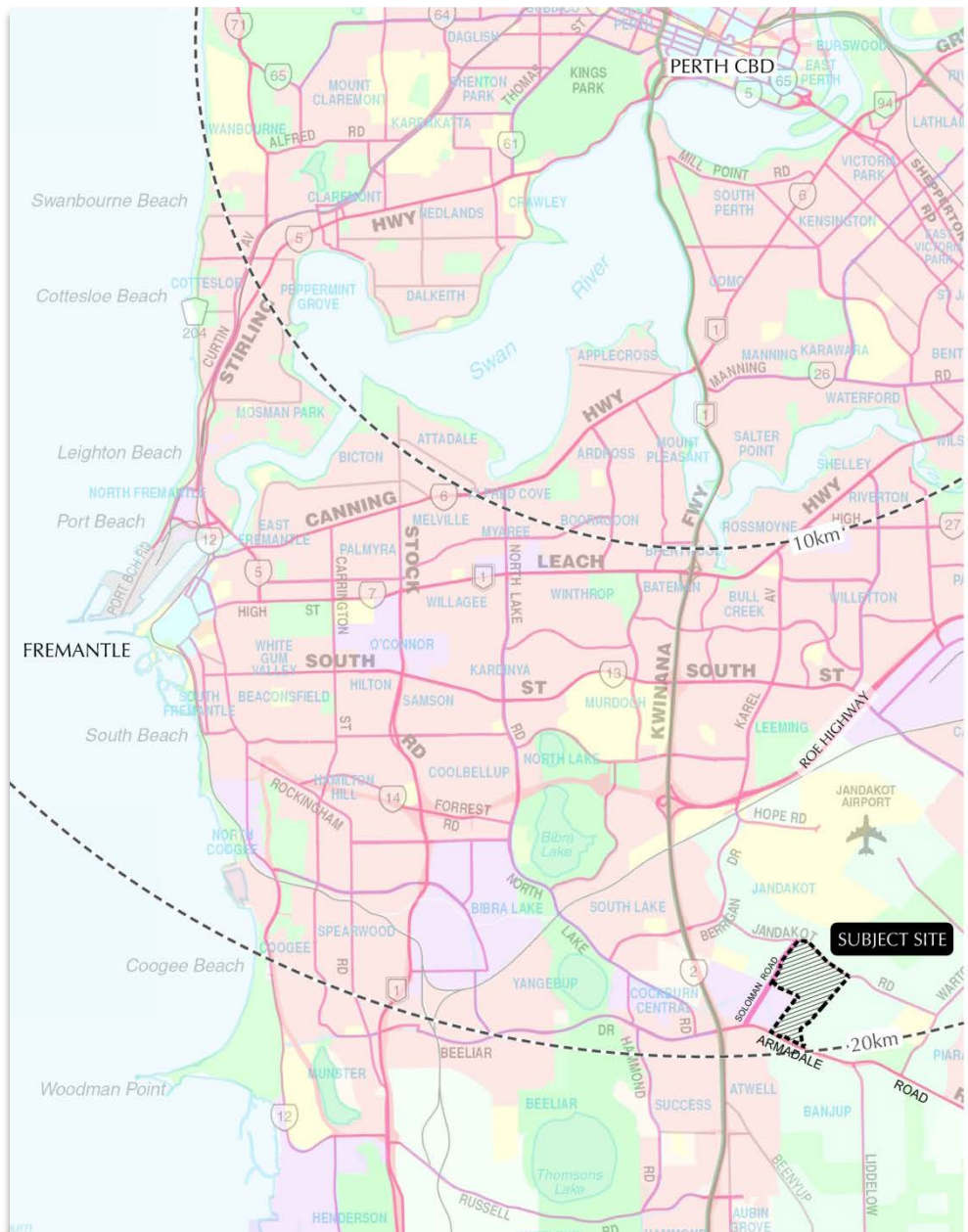
# SCHEME AMENDMENT REPORT

## 1 INTRODUCTION

### 1.1 Location

The subject land is located 18 kilometres from the Perth Central Business District and within the City of Cockburn, between Armadale Road, Solomon Road and Jandakot Road. The land is approximately 1.5 kilometres to the east of Cockburn Central Activity Centre and Rail Station, on the eastern side of the Kwinana Freeway. It is wholly contained within the locality of Banjup. Figure 1 shows the contextual location and extent of the subject land.

Figure 1: Location and extent of the subject land



## 1.2 Area and Ownership

The subject land comprises the following properties:

- Lot 1 comprising 8.08 ha in the ownership of Armadale Road Pty Ltd;
- Lot 9002 comprising 62.91 ha in the ownership of Stockland WA Development Pty Ltd;
- Lot 9004 comprising 36.52 ha in the ownership of Stockland WA Development Pty Ltd; and
- Lot 132 comprising 45.71 ha in the ownership of Stockland WA Development Pty Ltd.

The total area of the subject land is approximately 153 ha.

## 1.3 Land Use

The whole of the subject land is vacant.

Lots 9002, 9004 and 132 have been extensively cleared and excavated as part of a previous sand quarrying operation. A current conditional subdivision approval for 65 'special rural' 2 ha lots applies to Lots 9002, 9004 and 132, but has not been implemented.

# 2 Site Description and Environmental Considerations

## 2.1 Topography and Landform

The original natural topography of the site was described as gradually undulating, with a maximum AHD level of 50 metres located in the centre of the site. The site has however been extensively excavated as part of a previous sand quarry operation, thus the natural landform has been significantly altered.

## 2.2 Soils and Geotechnical

The subject land, forming part of the Swan Coastal Plain, is located on the alluvial plain which has developed west of the Darling Scarp. The Geological Survey of Western Australia (1978) mapped the site as predominantly 'Bassendean Sand', which can be described as white and grey quartz sand plain with low dunes and occasional swamps, iron or humus podzols and areas of complex steep dunes. It is moderately sorted and fine to medium grained sand (Churchwood and McArthur, 1978). This was confirmed during a site inspection undertaken in April 2010.

## 2.3 Acid Sulphate Soils

Acid Sulfate Soil (ASS) risk mapping compiled by the Department of Environment and Conservation indicates that the entire site has been classified as having a moderate to low risk of ASS occurring within 3m of natural soil surface or deeper.

## 2.4 Groundwater and Surface Water

The subject land is located at the northern end of the Jandakot Underground Water Pollution Control Area (UWPCA). The land was defined as a Priority 2 (P2) area in 1996. P2 areas are defined to minimise water quality contamination threats. In P2 areas 'Rural' zoning is common. A change to more intense land uses, such as 'Urban', is not recommended in P2 areas unless significant changes have occurred to the land and water factors considered when the original P2 area was determined. For the project area (excluding Lot 1) and as part of the recent MRS amendment over the subject site, it was determined by the relevant authorities that significant changes had occurred to the land and water factors since 1996 when the P2 area was defined and a Priority 3 (P3) area is more appropriate to reflect the current land and water circumstances.

Urban development is a permitted land use in P3 areas. The studies supporting the pending MRS Amendment demonstrate that the groundwater resource can be protected with respect to the water quantity and water quality of rainfall recharge to the aquifer.

In addition, a Wellhead Protection Zone (WHPZ) of 300 metre radius exists for the Water Corporation bore J380, located on the western boundary of the subject land adjacent to Solomon Road reserve. The WHPZ for Water Corporation bore J370 located in Atwell also extends to a minor extent over the south-west of Lot 1 Armadale Rd.

### 2.4.1 Groundwater

Across the site (at completion of the sand quarrying) there is an existing minimum of 2 metres depth to maximum groundwater levels, in accordance with the Environmental Protection Authority conditions of approval to the sand quarrying works.

### 2.4.2 Surface Water

The subject land is located on a surface geology of highly transmissive sands which does not generate surface water runoff. This is a contributing element to the existence of the Jandakot Groundwater Mound.

## 2.5 Environmental Assets and Constraints

Outside of the groundwater protection issues, the principal environmental issues associated with the subject site relate to flora and fauna and are summarised in the sections below. A detailed report has been completed by RPS to support the proposals for urbanisation of this land.

While the site's historical use as a sand quarry has resulted in the clearing of the vast majority of the original vegetation, peripheral areas of remnant vegetation and revegetated portions of the site required a botanical survey.

RPS undertook a targeted habitat survey in May 2010. As part of this work the site was mapped for remnant vegetation (RPS, 2010). As a result of the site's previous use as a sand quarry, the majority of the site is free of native vegetation and consequently identified as being *Degraded to Completely Degraded* with exotic naturalised grasses and Rye Grass (*Secale cereale*) spread throughout the site.

There are some small areas of remnant vegetation, including Banksia Woodland. Three vegetation units were mapped for the remnant vegetation on the periphery of the site (RPS, 2010).

## 2.5.1 Remnant Trees & Rehabilitation Works

There are some remnant native trees within the cleared sand areas as well as some rehabilitation plantings. These include:

- planted *Eucalyptus* spp. including *Eucalyptus conferruminata*, *Eucalyptus camaldulensis*, *Eucalyptus tottiana* and *Callitris preissii* with *Kunzea micrantha*, *Adenanthos cygnorum*, *Melaleuca nesophila*, *Agonis flexuosa*, *Acacia iteaphylla*, *Lecheanaultis floribunda* and *Scholtzia involucreata*,
- *Eucalyptus tottiana*, *E. marginata* with *Banksia attenuata*, *B. menziesii* and *B. ilicifolia* Open Woodland to Woodland;
- Scattered *Melaleuca preissiana* over *Astartea* and *Hypocalymma*; and
- Dampland with Scattered *Melaleuca preissiana* over Closed Shrubland of *Astartea affinis* and *Hypocalymma angustifolium* over an Open Sedgeland of *Lepidosperma longitudinale*.

A Declared Rare Flora species *Caladenia huegii*, listed under subsection (2) of Section 23F of the Western Australian *Wildlife Conservation Act 1950* and the *Environment Protection and Biodiversity Conservation Act 1999*, has been found in the adjacent Bush Forever Site 309 ('Fraser Road Bushland'), Banjup and within the bushland along the eastern boundary of the site.

The narrow band of remnant Banksia Woodland along the north eastern boundary of the site also presents suitable habitat for the orchid.

## 2.5.2 Fauna

RPS undertook a targeted habitat survey for two *Environment Protection and Biodiversity Conservation Act 1999* protected species, the Graceful Sun Moth and Carnaby's Black Cockatoo in May 2010.

### 2.5.2.1 Carnaby's Black Cockatoo

The survey found that the confined area of remnant vegetation remaining on site (particularly on the eastern side of the site) has some limited fauna habitat value as it includes Banksia trees which form part of Carnaby's Cockatoo natural foraging habitat. However, no Carnaby's Black Cockatoo or evidence of nesting or foraging was noted on the site during the survey notwithstanding that spring serves as a better opportunity for potential spottings.

### 2.5.2.2 Graceful Sun Moth

In addition to the targeted habitat survey undertaken, a targeted survey for Graceful Sun Moth was undertaken during the flying season in early April 2010. No moths were recorded on the site during these surveys and it was concluded that the site is unlikely to contain Graceful Sun Moths.

Based on the currently recorded location and anecdotal evidence of recent 2010 Department of Environment and Conservation (DEC) survey results, the site is not within the known distribution of the GSM across the Swan Coastal Plain. The 2003-2005 survey of 40 Bushland Reserves (undertaken by DEC) did not record the GSM within any of the (25) sites south of Perth within Banksia woodland.



## 2.6 Heritage

Ethnoscience was commissioned to conduct a desktop survey of the Aboriginal heritage values of the subject land. The desktop survey involved an examination of the Register of Aboriginal Sites and a review of other published and non-published materials.

The desktop research found two Aboriginal heritage site listings, as shown on the AHIS, overlapping with the subject land.

The first site (Site ID 3,300) is an artefact scatter located in the area near the intersection of Armadale Road and North Lake Road. This site had been extensively disturbed and there is currently insufficient information to confirm its Register status.

The second site (Site ID 4,108) was recorded as an artefact scatter immediately west of Fraser Road near the intersection with Armadale Road. The site was assessed in 2000 by the ACMC as having insufficient information to make a final determination of its status under the *Aboriginal Heritage Act 1972*. However, a subsequent inspection in 2002 concluded that the site has probably been destroyed and aerial photography shows the area as being disturbed.

It is highly likely therefore that if the site still exists it has been extensively disturbed and as such would probably not be considered a major impediment to future development. However, consent under Section 18 of the *Aboriginal Heritage Act 1972* may still be required. This could be confirmed following a site investigation.

It is currently unclear as to what extent the subject land has been surveyed for archaeological and ethnographic sites. Despite the fact that the land has been severely disturbed through sand quarrying activities, it is noted that there is still potential for archaeological and ethnographic sites to exist on the subject land. On this basis, an archaeological field assessment will be undertaken over areas on the site considered to have archaeological potential during the Local Structure Planning process.

Prior to its use as a sand quarry the site comprised natural vegetation dominated by Banksia Woodland. There is no known history of any use of the land or construction of buildings which have any European heritage values and this is consistent with the awarding of appropriate government approvals for sand extraction to proceed.

At the present time the only physical structures located on the site comprise the high voltage transmission lines, which would remain in place after urban development, and a small brick building in derelict condition which was part of the sand mining operation, and has no heritage value.

There are no known European ethnographic or archaeological sites or values associated with the site.

### 3 Statutory Planning and Context Considerations

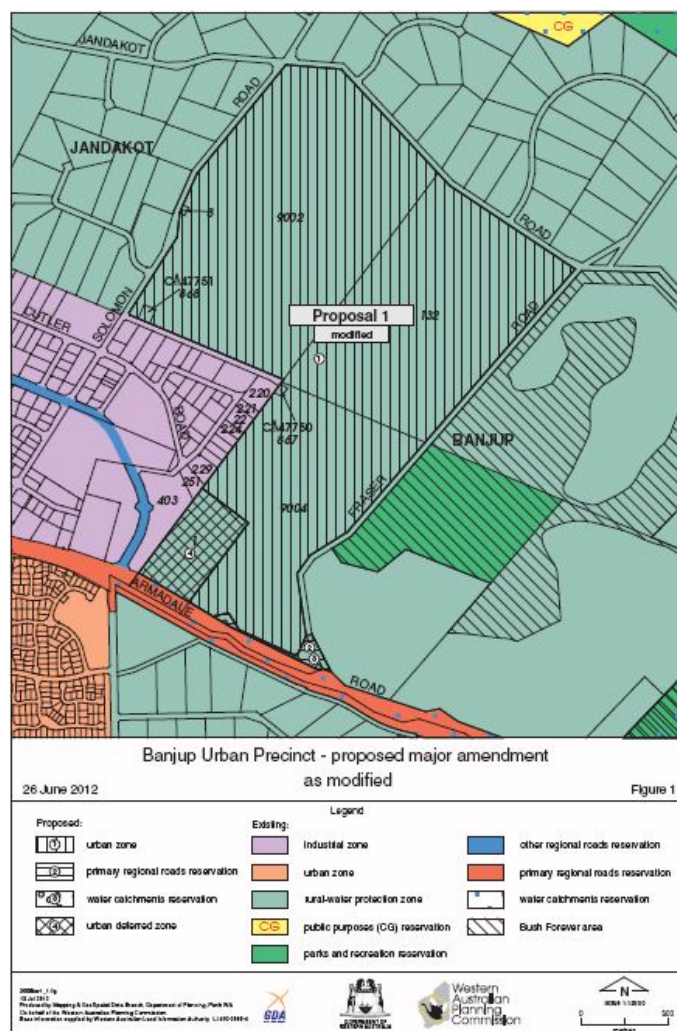
#### 3.1 Current Zoning

The subject land is currently zoned 'Rural – Water Protection' pursuant to the Metropolitan Region Scheme and 'Resource' under the City of Cockburn's District Town Planning Scheme No.3. The composite site is subject to MRS Amendment 1221/41 which proposes to rezone the subject land from 'Rural –Water Protection' zone to 'Urban' zone, to facilitate the intended residential development of the site.

The amendment sought to transfer the land from the 'Rural-Water Protection' zone to the 'Urban' zone and to reserve Primary Regional Roads (and associated water catchment reservation adjustment) associated with Armadale Road. The Amendment as advertised by the WAPC, proposed the whole of the subject land to be rezoned to Urban. Following public advertising, the proposal was modified by including Lot 1 Armadale Road in Urban Deferred zone, as Main Roads Wais undertaking a study to determine the long term alignment of Verde Drive.

Amendment 1221/41 was advertised for public comment between Tuesday 25 October 2011 and Friday 3 February 2012. It is to be introduced to Parliament in September 2012.

Figure 2: Proposed MRS Amendment 1221/41 (as modified)



### **3.2 Environmental Protection Authority Advice on proposed MRS Amendment 1221/41**

MRS Amendent 1221/41 was referred to the EPA for advice on whether environmental assessment would be required.

The EPA advised the the proposed Amendment does not require formal assessment under Part IV of the Environmental Protection Act 1986.

The EPA noted that the DoW has reviewed the Banjup Quarry redevelopment District Water Management Strategy and supports the strategy.

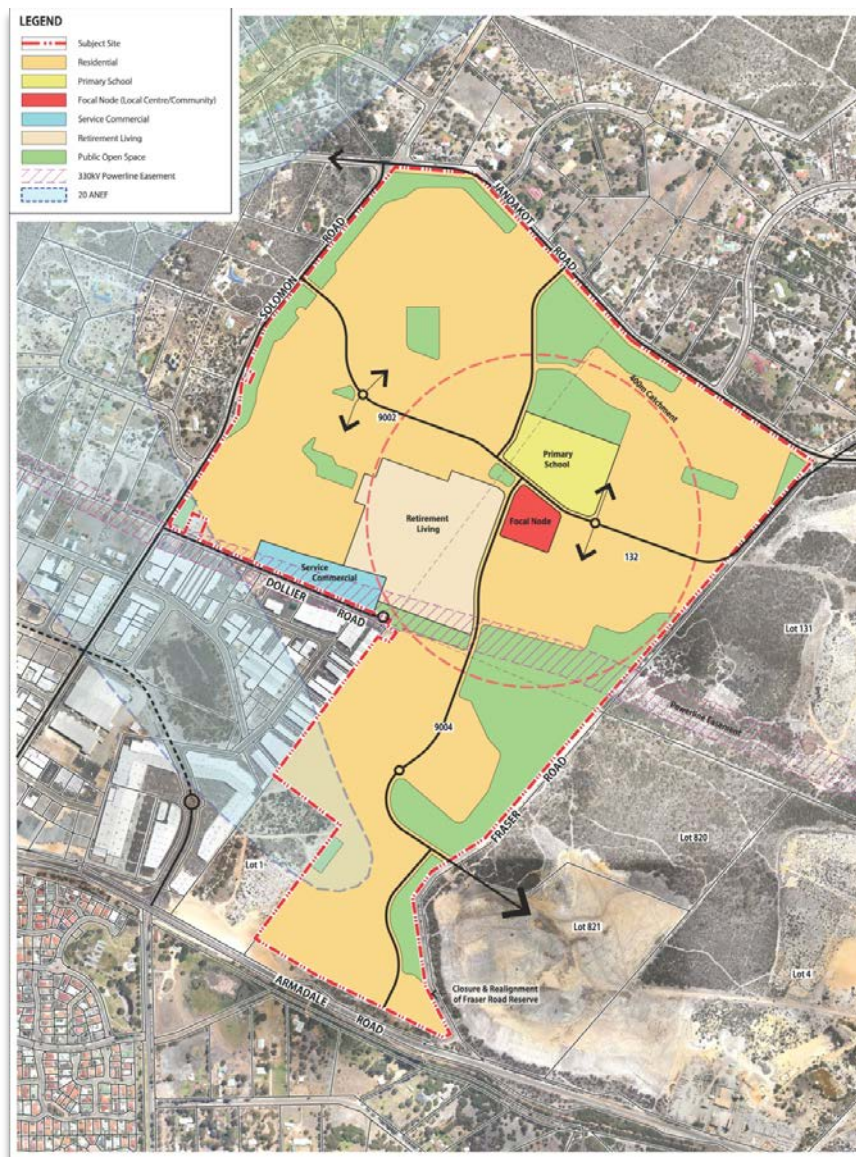
### 3.3 Banjup Draft Structure Plan

A draft structure plan has been prepared for Lots 9002, 9004 and 132 as part of the MRS Amendment documentation. Lot 1 Armadale Road, will be subject to a separate structure plan in the future.

This new structure planning area will transform an old quarry site and provide a home for an estimated 4,700 people living close to the heart of the city and adjacent to Cockburn Central, the freeway and rail line, the Gateways shopping complex and a variety of regional facilities. The (Draft) Structure Plan provides for residential development, retirement living, public open space, a town centre and a private school.

Indicative calculations suggest a total of around 1800 lots; which with an average density of 2.5 persons per household, plus the separate retirement living village, is likely to deliver a new resident population in the order of 4,750. This projection will be further refined as the structure plan is further developed.

Figure 3: Indicative Structure Plan



## 4 PROPOSED AMENDMENT

### 4.1 Contextual Setting

Lots 1, 9002, 9004 and 132 collectively total some 153 ha in area and therefore represent a considerable composite landholding within a 'middle ring' suburb of the Perth metropolitan area. Critically, the subject land lies in close proximity (within 1.5 kilometres) of the regionally significant Cockburn Central Railway Station and associated sub-regional centre. Similarly, the Kwinana Freeway, which is directly accessible via Armadale Road, is located within a kilometre of the subject land.

Currently, the subject site is predominantly cleared vacant land, effectively representing the remnants of an exhausted sand quarry. The quarry operated for a period of about 30 years, only ceasing some 3 years ago. The site was extensively mined, resulting in its environmental and visual qualities being significantly degraded.

A current conditional subdivision approval for 65 'special rural' 2 ha lots applies to Lots 9002, 9004 and 132, but has not been implemented.

As such, the historic and existing land use has little positive relationship to the immediate locality, the district and the sub-region and region as a whole. A change of use is therefore logical and necessary to facilitate efficient use of this strategically located vacant land.

The whole of the subject land lies within the northern portion of the Jandakot Underground Water Pollution Control Area within a 'Priority 2' (P2) classification and it is currently zoned 'rural-water protected zone'. In P2 areas rural zoning is common. A change to more intense land uses, such as urban, is not normally compatible within a P2 area unless it can be demonstrated that significant changes have occurred to the land and water factors considered when the original P2 area was determined.

Significant changes have occurred since 1996, when the P2 area was originally defined and that a Priority 3 classification is now more appropriate to reflect the current land and water circumstances.

Further to the groundwater issue, the subject land accommodates very little in the way of biodiversity value, having been extensively modified as part of the previous sand quarrying activities. In short, the site is generally devoid of known habitat for rare and endangered species, contains no mapped wetlands and has limited remnant vegetation. The relatively minor areas of biodiversity value should and can be captured and protected as part of any land use change.

The case for urbanisation of the subject land has been put forward as part of the proposed MRS Amendment 1221/41 which is in its final stages.

The area subject of MRS Amendment No1221/41 is ideally suited to urbanisation. The site is literally surrounded by major infrastructural services and is located within an existing urban setting. The site has minimal urban planning constraints and whilst providing significant urban planning opportunities, represents a 'blank canvas' for implementing cutting edge sustainability practices and exciting urban design initiatives, including housing affordability and diversity.

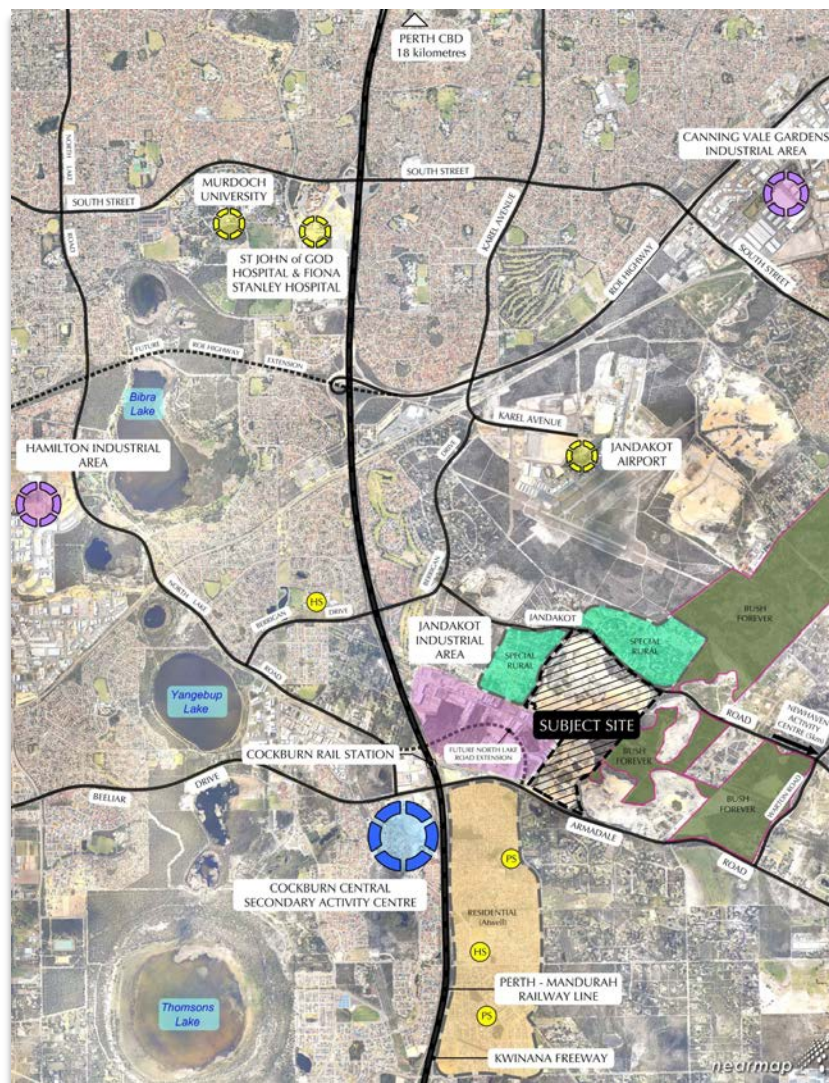


It has been scientifically demonstrated that the reclassification from P2 to P3 can be supported due to the combination of denuded site conditions and unique composition of the underlying aquifer formation specific to the area. Jandakot Mound (specifically south of Armadale Road) to be rezoned for urban development. It has been demonstrated that rezoning of the subject land to Urban will not prevent the continued use of the area for public water supply.

From a planning procedure perspective, following finalisation of the MRS Amendment, there is a requirement for TPS No 3 to be amended to rezone the subject land from 'Resource' to 'Development' zone. This Amendment seeks to do this.

The preparation of local structure plan(s) will then provide an overarching planning framework to guide and facilitate the development.

Figure 4: Regional Context.



## 4.2 Amendment Scope and Content

The purpose of this Amendment is to rezone the subject land from 'Resource' to 'Development' zone and to include appropriate Special Control Area provisions in the Scheme Text to control development which is the approach taken in respect to all development areas within the City of Cockburn.

The specific elements of the Scheme Amendment are as follows:

1. Exclude Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' zone and include these in 'Development' zone;
2. Introduce a new 'Special Control Area' covering the subject land, to be known as 'Development Area 37' and formulating appropriate provisions; and
3. Amend the Scheme Map accordingly.

## 5 CONCLUSION

The proposed Scheme amendment is consistent with the planning objectives for this area from both the local and State planning perspectives. Approval to the proposed Special Control Area provisions is consistent with orderly and proper planning. On that basis, support for the proposed Amendment is being sought

# SCHEME AMENDMENT

PLANNING AND DEVELOPMENT ACT, 2005

CITY OF COCKBURN

TOWN PLANNING SCHEME NO. 3

AMENDMENT NO. #

The City of Cockburn under and by virtue of the powers conferred upon in that behalf by the Planning and Development Act 2005, hereby amend the above Town Planning Scheme by:

1. Excluding Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' and including these in 'Development' zone;
2. Amending Schedule 11 of the Scheme Text to add a new 'Development Area 37' and to formulate appropriate provisions, as follows:

"Schedule 11 Development Areas

Ref. No.	Area	Provisions
DA37	Banjup Quarry Redevelopment	<ol style="list-style-type: none"><li>1. An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.</li><li>2. The Structure Plan is to provide for residential development, community and education facilities and a town centre.</li><li>3. The Structure Plan is to provide for safe and efficient pedestrian connections between DA37 and the Cockburn Central Railway Station.</li><li>4. Land uses classified on the Structure Plan apply in accordance with clause 6.2.6.3.</li><li>5. The Local Government may adopt Detailed Area Plan(s) pursuant to Clause 6.2.6.3 for any part of the Development Area as defined on the approved structure plan(s). All land use and development for a particular lot or lots the subject of a Detailed Area Plan shall accord with the adopted Detailed Area Plan.</li><li>6. The standards and requirements applicable to zones and R Codings under the Scheme shall apply to the same extent to the areas having corresponding designations under the Approved Structure Plan. Notwithstanding this, an Approved Structure Plan may by a clear statement of intent to do so, make provision for any standard or requirement applicable to zones or R Codings to be</li></ol>

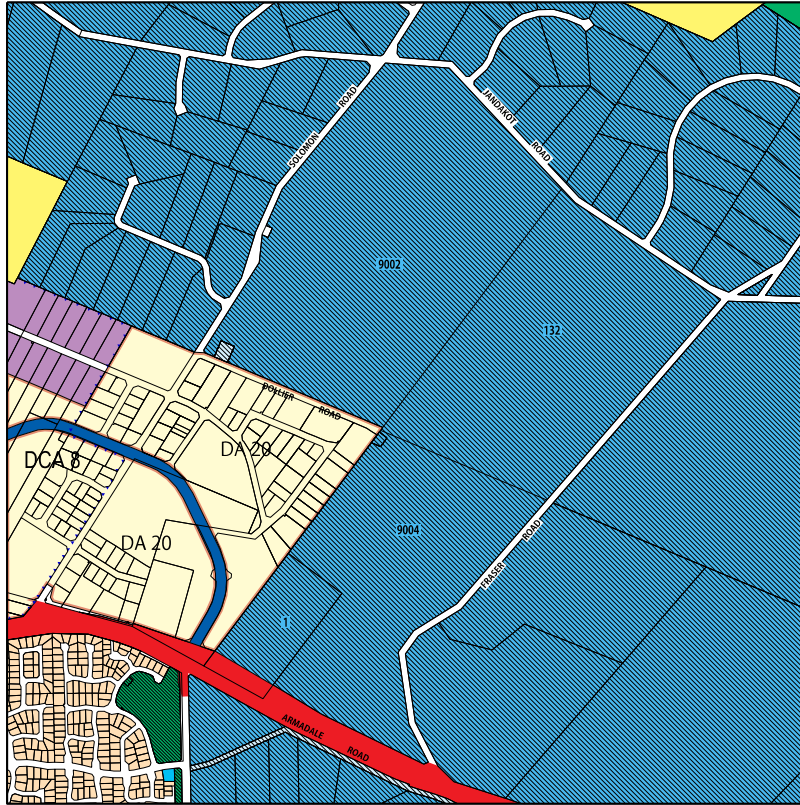


		varied, and the standard or requirement varied in that way shall apply within the area of the Approved Structure Plan, or any stipulated part of that area, as if it was a variation incorporated in the Scheme.
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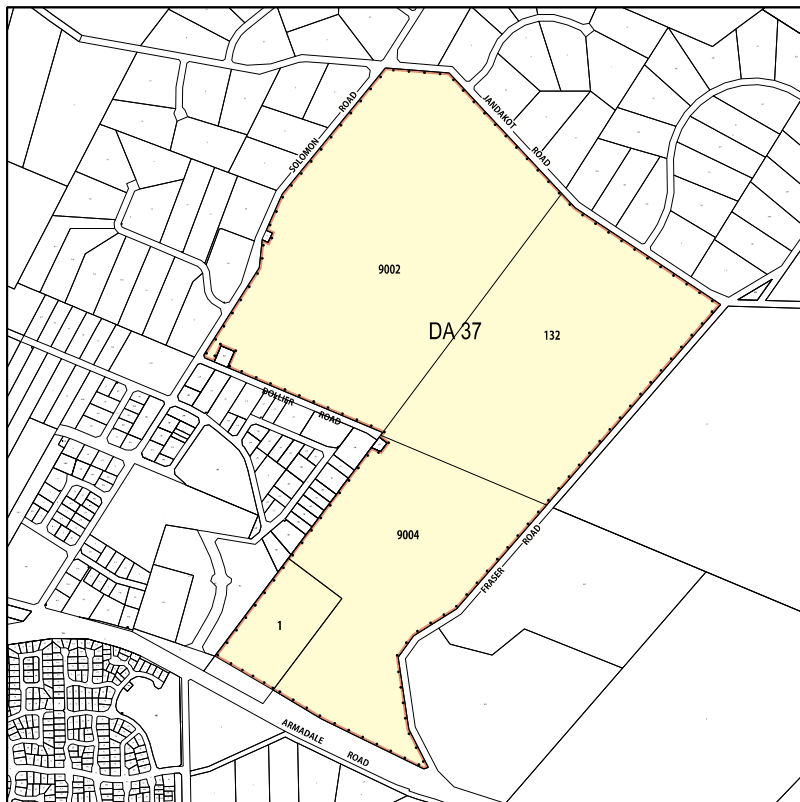
3. Amending the Scheme Map accordingly.

# SCHEME MAP AMENDMENT

## City of Cockburn Town Planning Scheme No.3 Amendment No. 95



**EXISTING ZONING**



**PROPOSED ZONING**

### LEGEND

#### METROPOLITAN REGION SCHEME RESERVE

- PRIMARY REGIONAL ROADS
- OTHER REGIONAL ROADS
- PUBLIC PURPOSES

#### LOCAL SCHEME RESERVE

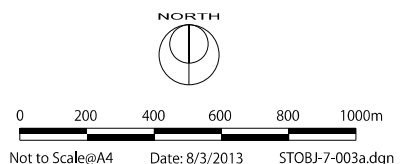
- LAKES & DRAINAGE
- LOCAL ROADS
- PARKS AND RECREATION

#### ZONES

- DEVELOPMENT
- RESIDENTIAL
- INDUSTRY
- RESOURCE

#### OTHER

- DA 37 DEVELOPMENT AREA
- DCA 8 DEVELOPMENT CONTRIBUTION AREA



## ADOPTION

Adopted by resolution of the Council of the City of Cockburn at the ordinary meeting of the Council held on the .....day of ..... 2012.

---

MAYOR

---

CHIEF EXECUTIVE OFFICER

## FINAL APPROVAL

Adopted for final approval by resolution of the City of Cockburn at the ordinary meeting of Council held on the ..... day of ..... 2012, and the Common Seal of the Municipality was pursuant to that resolution hereunto affixed in the presence of:

(Seal)

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
DATE

\_\_\_\_\_  
CHIEF EXECUTIVE OFFICER

\_\_\_\_\_  
DATE

RECOMMENDED/SUBMITTED FOR FINAL APPROVAL BY THE WESTERN AUSTRALIAN PLANNING  
COMMISSION

\_\_\_\_\_  
DELEGATED UNDER s16 PLANNING AND DEVELOPMENT ACT 2005

\_\_\_\_\_  
DATE

FINAL APPROVAL GRANTED

\_\_\_\_\_  
MINISTER FOR PLANNING

\_\_\_\_\_  
DATE



**LEGEND**

Structure Plan Boundary

Low Density Residential (Primarily R25-R40)

High Density Residential (Primarily R40+)

Primary School

Civic

Local Centre

Light & Service Industry

Primary Regional Road

Jandakot Road Widening (10m)

Public Open Space

Remnant Bushland

Wetland - REW

Wetland - 30m buffer

**MOVEMENT**

Neighbourhood Connector

Primary Path/Social/Pedestrian Linkages

330kV Powerline Easement

20 ANEF

25 ANEF

200m POS walkable catchment

**OTHER**

300m wellhead protection zone

400m Local Centre walkable catchment

Areas subject to DAP (also refer Provision 5 - Table A of Structure Plan report)

Generic Industrial Buffer (200/500m)

Bushfire Protection Zone (20m)

Temporary Bushfire Protection Zone required until hazard removed (20m)

# Plan 1 - Structure Plan

BANJUP QUARRY - Lot 9004 Armadale Road, Lot 132 Fraser Road & Lot 9002 Jandakot Road, BANJUP



Scale 1: 7500 @ A3  
0 50 100 150 200 250 300 metres

COMPILED: DPS, MAPS, EMERGE	DRAWN BY: JNP
DATE: 17/4/2012	REVISED: 19/2/2013
GRID: MGA 50	DATUM: AHD
DRAWING NUMBER: STOBJ-2-001c	JOB CODE: STOBJLSP
FILE ID: M:\STOBJ\BASE\STRUCTURE PLANS\STOBJ-2-001c.dgn	



Development Planning Strategies

28 Brown St, East Perth WA 6004 | PO BOX 6697 EAST PERTH 6892  
P (08) 9268 7900 | F (08) 9268 7999 | E dps@dpswa.com.au  
www.dpsplanning.com.au





**LEGEND:**

- Subject Site
- Low Density Residential (Primarily R25-R40)
- High Density Residential (Primarily R40+)
- 1 Community
- 2 Primary School
- 3 Town Centre - Mixed Use (Commercial & Residential)
- 4 Possible Retirement Living

# Plan 6 - Master Plan

BANJUP QUARRY

for: Stockland



NORTH	
Scale 1: 6000 @ A3	
0 60 120 180 240 300 metres	
COMPILED: DPS, MAPS, NEARMAP	DRAWN BY: JNP
DATE: 19/2/2013	REVISED:
GRID: MGA 50	DATUM: AHD
DRAWING NUMBER: STOBJ-6-002	JOB CODE: STOBJLSP
FILE ID: M:\STOBJ\BASE\MASTER PLANS\STOBJ-6-002.dgn	



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F (08) 9268 7999  
E dps@dpswa.com.au



**SCHEDULE OF SUBMISSIONS****SCHEME AMENDMENT NO. 95 AND DRAFT BANJUP QUARRY LOCAL STRUCTURE PLAN**

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
1.	Department of Education 151 Royal Street EAST PERTH WA 6004	<p><b>SUPPORT (subject to further information)</b></p> <p>The Department of Education has reviewed the document and wishes to make the following comments;</p> <ol style="list-style-type: none"> <li>1. The Department notes that approximately 153 hectares is subject to be rezoned as 'Development'. Should approval be granted, the Department would require details of the anticipated Lot yield to confirm the requirement for a primary school in the area.</li> <li>2. Currently the Department avoids development of schools on sites that have been remediated such as quarries. Prior to agreeing to a school site location in the structure plan, the Department will require a due diligence site investigation carried out by its consultants.</li> <li>3. Should the identified primary school site be found to be acceptable to the Department, clarification is required as to the size of the site as it appears that it is intended to be a shared facility with public open space and civic use.</li> <li>4. Advice is sought on the type of infrastructure that is intended on the 'Civic' use site that is abutting the proposed primary school.</li> <li>5. Could you also confirm if there an anticipated timeframe for the staging of development within this Structure Plan.</li> </ol> <p>The Department would welcome the opportunity to discuss the proposed Structure Plan at a convenient time in the future.</p>	<ol style="list-style-type: none"> <li>1. Noted. The DoE were advised by the applicant that the projected lot yield is 1810 lots.</li> <li>2. Noted. The DoE were advised by the applicant that a geotechnical survey undertaken over the entire site confirms that there is no uncontrolled fill located within the proposed primary school site.</li> <li>3. Noted. The DoE were advised by the applicant that the proposed primary school site will be 3.5ha with an additional 0.5ha to be made available for a multi-functional community facility.</li> <li>4. Noted. The DoE were advised by the applicant that the 'Civic' use site is proposed to include sporting clubrooms, multi-purpose clubhouse, arts and craft spaces and breakout areas.</li> </ol>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p><b>Second formal submission received</b></p> <p>6. The Department of Education has received further information from Stockland regarding the Banjup Local Structure Plan and wishes to make the following updated comments;</p> <p>A. The Department now understands that there is no uncontrolled fill in the location of the proposed primary school site. However, prior to agreeing to the school site location shown in the structure plan, the Department will still require a due diligence site investigation carried out by its consultants.</p> <p>B. The primary school site has been confirmed as 3.5ha in area. The abutting public open space is 2.35ha in area and will provide for a senior oval partly located on the public open space. The school site area requires that only 0.5ha of the shared oval is located on the primary school site.</p> <p>C. The Department has concerns as to whether the 0.5ha multi-functional community facility site will be large enough for the proposed facilities.</p> <p>D. Further discussion is sought on the type of infrastructure that is intended on the 'Civic' use site that is abutting the proposed primary school.</p>	<p>5. Noted. The applicant advised the DoE that should relevant approvals be received by mid-late 2013, construction will commence thereafter within the south of the precinct and yield approximately 450 lots over three years and then progress northwards. It is anticipated the project will take around 10 years to complete.</p> <p>6A. Noted. The final location of the primary school will be determined at the subdivision stage.</p> <p>6B. Noted. The shared arrangement will be determined at the subdivision stage in accordance with the requirements of Liveable Neighbourhoods.</p> <p>6C. Noted. The location, size and function of the facility is in accordance with draft Scheme Amendment 98 which seeks to modify the City's Development Contribution Plan 13 to introduce the community facility and sports oval.</p> <p>6D. Noted. As above.</p>
2.	Western Power GPO Box L921 PERTH WA 6842	<p><b>SUPPORT (subject to further information)</b></p> <p>1. Western Power have provided some high level comments that will need to be addressed when designing the layout</p>	<p>1. Noted. The submission relates to technical information and studies</p>



NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>of the development. Western Power will need to assess detailed plans before the development proceeds.</p> <ul style="list-style-type: none"> <li>• The development will be in close proximity to the Western Power Guildford Terminal to Southern Terminal (91) 330kV (GLT-ST 91) transmission line.</li> <li>• The electromagnetic fields generated (EMF) will be in close proximity to the properties and proposed development within the easement.</li> <li>• The easement is 80m wide for the provision of a future line and/or upgrade of the existing line. In the event of an upgrade, the new line will be located to the north of the existing line which will traverse the area light and service industry and POS.</li> <li>• Should a conductor failure or breakage occur close to or within the easement, the consequences could be severe and could place any development in the high risk category.</li> <li>• The development will attract a lot of people increasing the risk and probability.</li> <li>• Considering the high voltage, the close proximity of fencing/development to the overhead transmission line, Western Power strongly recommends a full EPR/LFI study conducted including any mitigation if required.</li> <li>• Western Power does not conduct earth potential rise and low frequency induction (EPR/LFI) studies for external customers. Any potential developers and landowners will need to engage their own electrical consultants for this work. However, Western Power will have to provide input into any EPR/LFI study undertaken by a consulting electrical engineer. A purchase order will be required for Western Power's input into any EPR/LFI study.</li> <li>• The electrical consultant will set the parameters for the EPR/LFI study and will request certain information</li> </ul>	<p>required to be completed at the detailed design and subdivision stage. The LSP includes provisions which relate to the Western Power easement and measures to ensure subdivision and development is designed to appropriately interface with the easement.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>from Western Power to complete the study. The study should also provide you and or the developers of these lots with mitigation advice if that is required.</p> <ul style="list-style-type: none"> <li>• Storage is not permitted within the easement area.(this includes boats, caravans, sheds and sea containers)</li> <li>• Roads, ground level changes and proposed lighting within the easement will need to be assessed by Western Power</li> <li>• Open drainage is not permitted within the easement area.</li> <li>• Depending on what is proposed within Western Power's easement, EPR/LFI studies may be required as a condition of the encroachment.(POS)</li> <li>• Depending on what is proposed adjacent to the easement, EPR/LFI studies may be recommended by Western Power.</li> <li>• There are also high voltage distribution lines traversing Lot 132 that may be required to be relocated underground - for more information please visit <a href="http://www.westernpower.com.au/lld/Large_subdivisions.html">http://www.westernpower.com.au/lld/Large_subdivisions.html</a></li> </ul> <p><b>Second formal submission received following provision of information to Western Power by applicant.</b></p> <p>2. Thank you for taking the time to provide us with this information.</p> <p>The planning advice you have provided has been noted in our planning database in advance of our next review of network capacity requirements. During this time, one of our planning officers may contact you to clarify development details.</p>	<p>2. Noted.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>A key planning consideration is to determine whether forecast demand for network capacity, which is comprised mainly of firm network connection applications, is in line with long-term trends or represents a significant change to trend. Relatively large changes in forecast demand will receive close attention.</p> <p>Western Power strives to continually improve the accuracy and timeliness of its planning information. Toward this objective, Western Power presents its plans via the Annual Planning Report (APR) and the Network Capacity Mapping Tool (NCMT)</p> <p>In addition Western Power supplies its NCMT data to the Department of Planning for integration into cross-agency publications and planning tools.</p> <p>I invite you to review the information provided via the APR and the NCMT for your area. Once again, thank you for assisting us in delivering quality information to our customers and the broader community.</p>	
3.	Alobale Pty Ltd 12 Peppworth Place JANDAKOT WA 6164	<p><b>SUPPORT (subject to conditions)</b></p> <ol style="list-style-type: none"> <li>1. I would like to confirm my interests that develop my property to urban use. I wish all my neighbours can develop their properties to urban use.</li> <li>2. I understand underground water is very important to be protected around my property. I believe it always can find a way to protect underground water in a very proper way and develop all lands into urban use, such as Japan, Europe, China, Beijing and Shanghai.</li> </ol>	<ol style="list-style-type: none"> <li>1. Noted but not supported. The subject site was rezoned from 'Rural – Water Protection' to Urban under the MRS based on the recommendations of the WAPC's Draft Outer Metropolitan Perth and Peel Sub-regional Strategy which identified the site as having urban potential based on its proximity to the Cockburn activity centre.</li> </ol>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>3. I agree with this proposal subject to my property be developed as well. It have to be equal</p>	<p>Council's support for the rezoning was subject to future applications and studies meeting and exceeding the planning and environmental objectives and requirements of the City and various State Government approval authorities. This has been demonstrated by the current application.</p> <p>At this stage there is no strategic basis for the City to be considering any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound). As mentioned the strategic basis for this proposal has been formed from Directions 2031 and its associated Sub-regional Strategy - such document do not support further urbanisation at this time.</p> <p>The strategic planning represented by the WAPC's Draft Strategy has been the mechanism through which the ex-sand quarry project in Banjup has been identified as having potential for urban development. It should be noted that this proposition is only being considered after extensive research into the potential environmental impacts and in</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>particular the impact on the groundwater mound. At this stage no other such investigations are being undertaken by the State Government for other areas of the 'Resource' zone above the Jandakot Water Mound. Notwithstanding, the resolution does confirm ongoing dialogue with the Department of Planning in respect of the review of the Jandakot Water Mound.</p> <p>2. Noted. As per 1 above.</p> <p>3. Not supported. As per 1 above.</p>
4.	Affected Landowner (Names & address withheld)	<p><b>OBJECT (subject to conditions)</b></p> <p>1. My concerns are what road changes are to be made with the increase of i.e.</p> <p>a) Will Solomon or Fraser roads be extended?</p> <p>b) Will Jandakot road be widened?</p> <p>c) Will Jandakot road speed limit of 80km/ph be reduced?</p> <p>2. Does the City of Cockburn have any thoughts on the future zonings towards all the present resource/rural adjoin properties?</p>	<p>1a). Noted. Solomon Road and Fraser Road will be required to be upgraded to an urban standard where they abut the subject site and form part of the road access arrangements. The recommendation deals with the appropriate securing of a legal agreement to cover required off site and adjoining road upgrades. This includes Solomon Road north of Dollier Road. Fraser Road isn't proposed to be created as a through route between Jandakot Road and Armadale Road, instead will only be utilised as intersection entries into the actual subdivision</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>area.</p> <p>1b). Jandakot Road is proposed to be widened where it abuts the subject site. It is likely that the remainder of Jandakot Road to Berrigan Drive and Warton Road will be widened sometime in the future however this will be subject to a detailed study and separate process. The adjoining section of Jandakot Road will be upgraded to a dual carriageway urban standard between Solomon Road and Fraser Road.</p> <p>1c). There is no current proposal to reduce the speed limit however this will be considered as part of the detailed design of the upgraded Jandakot Road.</p> <p>2. Noted but not supported. The subject site was rezoned from 'Rural – Water Protection' to Urban under the MRS based on the recommendations of the WAPC's Draft Outer Metropolitan Perth and Peel Sub-regional Strategy which identified the site as having urban potential based on its proximity to the Cockburn activity centre.</p> <p>Council's support for the rezoning was subject to future applications and studies meeting and</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
			<p>exceeding the planning and environmental objectives and requirements of the City and various State Government approval authorities. This has been demonstrated by the current application.</p> <p>At this stage there is no strategic basis for the City to be considering any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound). As mentioned the strategic basis for this proposal has been formed from Directions 2031 and its associated Sub-regional Strategy - such document do not support further urbanisation at this time.</p>
5.	F Ewing 237 Fraser Road JANDAKOT WA 6164	<p><b>OBJECT</b></p> <p>1. I am concerned about the increase in traffic on Jandakot Road and how this will be handled. Already since the growth of Piara Waters, we are finding it increasingly difficult to gain entry onto Jandakot Road from Cessna Road at peak times (it is near impossible plus highly dangerous to attempt entry from Fraser onto Jandakot Road). The intersections of Solomon/Jandakot, Solomon/Armadale and Jandakot/Berrigan Drive (albeit a roundabout) is becoming increasingly congested and dangerous.</p>	<p>1. Noted. It is the City's position that upgrades to Jandakot and Solomon Roads by the developer be secured in a timely manner to ensure the effective management of additional traffic generated by the development. It is for this reason that any approval of the LSP will be subject to the finalisation of a voluntary legal agreement with the developer to ensure the timely delivery of the required upgrades. The City will</p>

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			continue to monitor the situation and is already undertaking upgrades to the wider network together with Main Roads WA with the aim of improving traffic circulation in the locality.
6.	Department of Water PO Box 332 MANDURAH WA 6210	<p><b>SUPPORT (subject to conditions)</b></p> <p>Thank you for the referral of the abovementioned Local Structure Plan (LSP) received with correspondence on 14 March 2013. The Department of Water (DoW) has reviewed the proposal and wishes to provide the following advice:</p> <p>1. <u>Better Urban Water Management</u> Consistent with the Western Australian Planning Commission's (WAPC) Better Urban Water Management (BUWM) document and the policy measures outlined in State Planning Policy 2.9: Water Resources, the proposed LSP should be supported by a Local Water Management Strategy (LWMS) prior to the final approval of the proposed LSP.</p> <p>The DoW reviewed the Banjup Quarry Redevelopment LWMS (Emerson Stewart, March 2012) and provided comments within correspondence dated 29 May 2012. Please find correspondence attached as an Appendix.</p> <p>It is recommended the proponent revise the document in accordance with the attached advice and any comments from the City of Cockburn. In accordance with BUWM (WAPC, 2008) the LSP should not be finalised in the absence of a LWMS approved by the DoW and City of Cockburn.</p>	<p>1. Supported. Approval of the Structure Plan will be subject to the final approval of the associated LWMS by the DoW</p>
7.	Y Fang 1389 Albany Highway	<b>NO OBJECTION (subject to conditions)</b>	



NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
	CANNINGTON WA 6107	<p>1. I wish to see my property subdivided for urban use only. I do not want my neighbour subdivided for urban use and our property kept for rural only. I agree to develop with my neighbour together only. If my property is allowed to be subdivided, it will be very appreciated by me, with thanks.</p>	<p>1. Noted but not supported. The subject site was rezoned from 'Rural – Water Protection' to Urban under the MRS based on the recommendations of the WAPC's Draft Outer Metropolitan Perth and Peel Sub-regional Strategy which identified the site as having urban potential based on its proximity to the Cockburn activity centre.</p> <p>Council's support for the rezoning was subject to future applications and studies meeting and exceeding the planning and environmental objectives and requirements of the City and various State Government approval authorities. This has been demonstrated by the current application.</p> <p>At this stage there is no strategic basis for the City to be considering any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound). As mentioned the strategic basis for this proposal has been formed from Directions 2031 and its associated Sub-regional Strategy - such document do not support further urbanisation at this time.</p> <p>The strategic planning represented</p>

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			by the WAPC's Draft Strategy has been the mechanism through which the ex-sand quarry project in Banjup has been identified as having potential for urban development. It should be noted that this proposition is only being considered after extensive research into the potential environmental impacts and in particular the impact on the groundwater mound. At this stage no other such investigations are being undertaken by the State Government for other areas of the 'Resource' zone above the Jandakot Water Mound. Notwithstanding, the resolution does confirm ongoing dialogue with the Department of Planning in respect of the review of the Jandakot Water Mound.
8.	Schemes, Strategies and Amendments Department of Planning 140 William Street PERTH WA 6000	<b>NO OBJECTION</b> <ol style="list-style-type: none"> <li>1. The WAPC owns land which adjoins the land that is within the Local Structure Plan. I have received advice from our WAPC Property Services branch that they raise no objections to the proposed LSP.</li> <li>2. The DoP schemes, strategies and amendments team does not comment on amendments during the public advertising stage. Our assessment is undertaken when the amendment is sent to WAPC for final approval.</li> </ol>	<ol style="list-style-type: none"> <li>1. Noted.</li> <li>2. Noted.</li> </ol>
9.	S Bezuidenhout 147 Solomon Road	<b>OBJECT</b>	

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
	Jandakot WA 6164	<ol style="list-style-type: none"> <li>1. My Interests are affected as a private citizen.</li> <li>2. My only concern in regards to this proposed development is that the Traffic issue needs to be addressed before any development is commenced. Not just patched up and then later more patches to try to improve things like what is happening with Cockburn Central which I avoid like the plague.  I have trouble getting out of my driveway to go to work in the mornings now, because of everyone trying to avoid Armadale Road and then also because of residents of Piara Waters, trying to get to the freeway by avoiding South Street. So what will happen when we have a road entering opposite our property I shudder to contemplate.</li> </ol>	<ol style="list-style-type: none"> <li>1. Noted.</li> <li>2. Noted. It is the City's position that upgrades to Jandakot and Solomon Roads by the developer be secured in a timely manner to ensure the effective management of additional traffic generated by the development. It is for this reason that any approval of the LSP will be subject to the finalisation of a voluntary legal agreement with the developer to ensure the timely delivery of the required upgrades. The City will continue to monitor the situation and is already undertaking upgrades to the wider network together with Main Roads WA with the aim of improving traffic circulation in the locality.</li> </ol>
10	John Ewing 17 Riverbank Boulevard GUILDFORD WA 6055	<p><b>NO OBJECTION (subject to conditions)</b></p> <ol style="list-style-type: none"> <li>1. As the conservation Officer of the WA Native Orchid Study and Conservation Group (WANOSGC) I wish to make comment on the Banjup quarry local structure plan dated March 2013.</li> <li>2. The proposal has many good elements, particularly with respect to the retention of as much bushland as possible on the site, revegetation of degraded bushland, encouragement of solar passive and active housing, water management, and promotion of public and active transport. The Banjup quarry local structure plan dated March 2013 p30, indicates that <i>"A Declared Rare Flora</i></li> </ol>	<ol style="list-style-type: none"> <li>1. Noted.</li> <li>2. Noted.</li> </ol>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p><i>species Caladenia huegelii, listed under subsection (2) of Section 23F of the Western Australian Wildlife Conservation Act 1950 and the Environment Protection and Biodiversity Conservation Act 1999, has been found in the adjacent Bush Forever Site 309 ('Fraser Road Bushland'), Banjup and within the bushland along the eastern boundary of the site."</i></p> <p>3. I have personally been in this area in years past and know that Caladenia huegelii does grow in the Fraser Road bushland. I would like to see that this population is protected by an adequate buffer and other protective works between the orchids and the proposed residential areas. This would include avoiding the possibility of disturbance where slopes are steeper. The proposal also says that <i>"The narrow band of remnant Banksia Woodland along the north eastern boundary of the site also presents suitable habitat for the orchid."</i></p> <p>This statement is based on a flora survey undertaken in May 2010. Such a survey would have been unable to establish the presence or lack of presence of orchids as there would be no evidence of them in May. A survey would need to be done in late September and into early October to establish whether or not these orchids are present at that location. If a large population of this species was found in this area then it would be important for this area to be retained as well. If there were only a few plants present and if the area was of major importance to the overall development proposal then those plants should be 'rescued' and possibly some further offsets could be developed to compensate for this loss of important habitat.</p> <p>4. From the documents I have seen it is actually not very clear whether this part of the bushland is to be retained or</p>	<p>3. Supported. Prior to subdivision and/or clearing the Department of Environment and Conservation will need to be satisfied adequate studies and mitigation measures have been undertaken in relation to vegetation and flora. In terms of Fraser Road reserve and eastern adjoining bush forever site, there isn't a proposal to impact on the conservation aspects of this land.</p> <p>4. Noted. Prior to subdivision and/or clearing the Department of</p>

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		<p>not. I would like to propose that a survey is organized at a suitable time by the City of Cockburn to establish what orchids are still present in the areas of bushland as indicated on the proposal.</p> <p>5. As the Conservation officer of WANOSGC, I would be happy to organize members of my group to carry out such a survey in 2013 and to provide the City and the developers with the results. Other surveys in this area have been done in the past by other groups, including some done by DEC. What our survey would do is to provide accurate, up-to-date information compiled by people who have a sound knowledge of both this particular orchid and of other orchid species that are sure to be growing in the area. I look forward to your reply to our offer.</p>	<p>Environment and Conservation will need to be satisfied adequate studies and mitigation measures have been undertaken in relation to vegetation and flora.</p> <p>5. This is a matter that needs to be discussed with the proponent. As mentioned above environmental considerations of the site have been used to carefully underpin many aspects of the Structure Plan. This includes the protection of remnant bushland on the site, and remediation/buffer of the eastern adjoining bush forever site.</p>
11	Department of Indigenous Affairs PO Box 3153 EAST PERTH WA 6892	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>1. There are two Aboriginal heritage places which intersect with the area associated with the Banjup development. These are DIA 3300 (Readymix Sandpit 2) and DIA 4108 (Readymix Sandpit 1). Currently, there is not enough information known with respect to either place to enable an assessment under section 5 of the Aboriginal Heritage Act 1972 (AHA) to occur. Both of these places were reported upon the basis of their association with Aboriginal archaeological material.</p> <p>2. It is understood that Ethnoscience has completed a desktop survey of Aboriginal heritage values associated with the Banjup development. It is further understood that an archaeological field assessment will be undertaken over areas of the Banjup development considered to have archaeological potential. This approach is supported. It is suggested that the areas comprising DIA 3300 (Readymix</p>	<p>1. Noted. Prior to subdivision and/or development the proponent will be required to undertake the required studies and/or any design modifications to the satisfaction of the DIA.</p> <p>2. Noted.</p>

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		<p>Sandpit 2) and DIA 4108 (Readymix Sandpit 1) are inspected as part of the proposed survey and the information gathered is utilised to support a reassessment of these places under section 5 of the AHA by the Aboriginal Cultural Material Committee. This will enable prospective developers to have certainty as to whether these places can be considered as being protected under the AHA and whether approval is required should there be a need to impact upon these places.</p> <p>3. I also recommend that consideration is given to alerting relevant native title claimants to the Banjup development if not already undertaken.</p> <p>Prospective developers for the Banjup development should have their attention drawn to the State's Cultural Heritage Due Diligence Guidelines which can be found at <a href="http://www.heritage.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHADueDiligenceGuidelines.pdf">www.heritage.wa.gov.au/Documents/HeritageCulture/Heritage%20management/AHADueDiligenceGuidelines.pdf</a>.</p> <p>The guidelines are intended to assist prospective developers to assess the risk that a proposed development may hold with respect to its potential to impact upon Aboriginal heritage sites. It also assists a developer when assessing whether approval under the Aboriginal Heritage Act 1972 is required prior to development occurring.</p>	<p>3. Noted. The developer will be advised of the requirements of the DIA.</p>
12	Policy Development Department of Planning Locked Bag 2506 PERTH WA 6001	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>1. <u>Aircraft Noise</u></p> <p>Draft State Planning Policy 5.3 – Land use planning in the vicinity of Jandakot Airport (SPP 5.3) is the current policy guiding decision making by the Western Australian Planning Commission and the Department of Planning on</p>	<p>1. The City notes that the proposal fully complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is yet to be</p>

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		<p>applications within the surrounds of Jandakot Airport. SPP 5.3 has precedence over Australian Standard 2021 Acoustics-Aircraft noise intrusion-Building siting and construction, 2000 (AS2021), therefore Appendix 1:Building site acceptability of SPP 5.3 applies to this site. Dwelling (i.e. residential development) is unacceptable within the Core area (area greater than 20 ANEF) and is conditionally acceptable within the Frame area (major surrounding roads to 20 ANEF) – with the policy providing for discretionary control of such development. Figure 2.1: Banjup LSP – ANEF Contour Location of the Noise Impact Assessment showing the site and area proposed as Residential within the Core area, is not supported by Policy Development. Prior to any decisions on this proposal, Policy Development recommends the Noise Impact Assessment be forwarded to the relevant division within Department of Environment and Conservation (DEC) for review.</p> <p>2. <u>Basic Raw Materials</u></p> <p>The subject lot is in a State Planning Policy 2.4 – Priority Resource Location for Sand area. The situation represents an ideal sequential land use opportunity with the resource being extracted and then the site being used for its next purpose. It is suggested that the structure plan be approved, subject to the Depart of Mines and Petroleum providing sign-off of that the available sand extraction is completed, on the site, down to levels compatible with its future use. This process has a precedent at Flynn Drive Neerabup, where it has been used on a LandCorp Industrial site. There is a shortage of sand to meet expected demand in the southern corridor and any resources should be fully extracted rather than sterilised.</p>	<p>finalised by the WAPC (southwest corner of land). It is recommended that the City request that the WAPC consider the implications of the revised SPP5.3 in respect to the proposal as part of their endorsement of the Structure Plan. It will be at the WAPC's discretion whether they're prepared to endorse the Structure Plan prior to the finalisation of the revised SPP5.3. Notwithstanding this, it is also recommended that all sensitive development integrate appropriate noise amelioration standards as part of development AND an appropriate notification be placed on the title of all lots advising of this requirement to build to a high noise standard. Such approaches are used in respect of development adjoining major roads (Kwinana Freeway for example) and is considered appropriate in this case.</p> <p>2. Not supported. The subject site has been rezoned for Urban purposes under the MRS and the addition of such a condition would be inconsistent with the current MRS zoning and proposed TPS3 'Development' zone.</p>

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		<p>3. <u>Bush Forever</u></p> <p>The subject site is adjacent to Bush Forever area 390 – Fraser Road Bushland, Banjup. Bush Forever area 390 is of high conservation value, as it contains Declared Rare Flora and potential feeding habitat for the Carnaby's Black Cockatoo. The LSP site has been extensively cleared for sand mining, and has areas of remnant vegetation, which are proposed to remain, which is supported. The Green way link strategy is also supported. Policy Development recommends the following conditions be taken into consideration with regard to Bush Forever:</p> <ul style="list-style-type: none"> <li>- Hard edges being provided on the subject land adjacent to Bush Forever Area 390; and</li> <li>- No drainage of waste and/or stormwater into Bush Forever Area 390.</li> </ul>	<p>3. Supported. The protection of the adjacent Bush Forever site will be implemented with appropriate measures at the subdivision stage.</p>
13	CLE Town Planning & Design PO Box 796 SUBIACO WA 6904	<p><b>NO OBJECTION (subject to conditions/modifications)</b></p> <p><u>Scheme Amendment No. 95</u></p> <p>1. We support the rezoning of the subject land to allow for future development, consistent with the MRS zoning.</p> <p>2. As discussed at a recent meeting with Daniel Arndt and Andrew Trosic, the land use of Lot 1 is still to be determined. The preferred use is for commercial and show rooms on a single site (no subdivision proposed). This use is consistent with the existing businesses to the west.</p> <p>3. Commercial use on Lot 1 would be more suited to a Mixed Business zone, rather than a Development Zone. Where Lot 1 is developed for commercial uses, the layout and use will be largely self-contained and can be</p>	<p>1. Noted.</p> <p>2. No formal proposal has been lodged or considered for Lot 1 Armadale Road, Banjup. Accordingly it is too early to contemplate any degree of in principle (or otherwise) support for land use alternatives for Lot 1. The Scheme provides for the objective of the Development zone to <i>"To provide for future residential industrial or commercial"</i></p>



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		<p>adequately addressed through the Development Application process, rather than unnecessarily requiring a full structure plan over a small 8 ha site.</p> <p>4. The landowner supports the proposed Development Zone on the basis that the use of Lot 1 has not been finally determined. However in the instance that a commercial/showroom use is proposed, the landowner may seek a modification to the Amendment or a further Amendment to rezone the land to Mixed Use zone.</p> <p><u>Structure Plan</u></p> <p>5. The landowner supports the proposed Structure Plan and development of Lots 132, 9002 and 9004; however the following comments are provided as it relates to Lot 1.</p> <p>6. We request clarification as to the assumptions that have been made in the traffic model regarding the use of Lot 1. As mentioned above the preferred use is for commercial/showroom use. Preliminary concepts indicate that the site could support 3500sqm of commercial floorspace and this should be taken into account in the traffic model, and the road hierarchy and Plan 7 be updated accordingly. It should also be noted that future access from Lot 1 to Armadale Road is yet to be resolved.</p> <p>7. On the basis of the future commercial or residential use of Lot 1 we request that the indicative access street connection to Lot 1 from the east be amended to a neighbourhood connector. This will provide greater flexibility and improved access to Lot 1.</p> <p>8. We request a second access to Lot 1 be provided from the north to improve connectivity and flexibility as to</p>	<p><i>development in accordance with a comprehensive Structure Plan prepared under the Scheme."</i></p> <p>Accordingly there is built in flexibility within the Development zone to provide for land use alternatives, based upon broader assessment of State and local planning policy. In order to make this clear within the Amendment document, it is recommended that Provision 2 of DA37 be modified as follows:</p> <p>"The Structure Plan is to provide for an appropriate mix of residential and non-residential land uses, in order to support the objective for a mixed use neighbourhood. Non-residential land uses may include compatible commercial and industrial light and service industry) land uses, as a means to provide an appropriate interface and transition to the western adjoining Solomon Road Development Area 20."</p> <p>3. Not supported. The City believes that the magnitude and complexity of issues facing the future land use of Lot 1 are such that a comprehensive planning approach is required in order to deal with these.</p>

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		<p>future use and layout.</p> <p>9. The potential for commercial use on Lot I should be taken into account in the design and interface for the surrounding residential development. The Structure Plan shows those lots surrounding the site being subject to a DAP; however, there is no note in relation to this addressing the interface to the commercial use. We request that this be reviewed.</p> <p>10. Timing and staging of access to Lot 1 is critical to the site's development. Based on the current staging plan access is to be provided as part of stage 14, this does not align with the timing and staging of Lot 1.</p> <p>11. We request clarification that the structure plan is self-contained in terms of drainage and is not relying on open space within Lot I to take any drainage from the Structure Pion area.</p> <p>12. Minor modifications are also required to several plans:  A. Plan 5 - Lot 1 is incorrectly shown as part of the subject site  B. Plan 6 - the layout on Lot 1 should be removed as this is not the likely development layout and the site is not part of the structure plan area.</p>	<p>Accessibility and the future of the North Lake Road extension east of the Freeway was one of the key determinants for Lot 1 Armadale Road being identified as Urban Deferred. Accordingly the process of future structure planning to underpin the resolution of issues facing Lot 1 is considered the most effective means available to do this.</p> <p>4. Noted. The City believes that the magnitude and complexity of issues facing the future land use of Lot 1 are such that a comprehensive planning approach is required in order to deal with these. Accordingly it is recommended that the Development zone approach be retained.</p> <p>5. Noted.</p> <p>6. Noted. The traffic modelling for the Proposed Structure Plan deals with the structure plan area, and isn't appropriate to be contemplating a significant land use across Lot 1 Armadale Road at this point in time. A future Structure Plan for Lot 1 Armadale Road will need to address traffic issues, as well as accessibility,</p>

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			<p>given these remain the two key issues which have underpinned its designation as Urban Deferred.</p> <p>7. Not supported. It is too early to contemplate with any certainty the likely land use outcome for Lot 1 Armadale Road. Increasing the nature of the road hierarchy to neighbourhood connector starts to foreshadow a more intensive land use outcome for Lot 1 Armadale Road, which isn't appropriate to do at this stage. As mentioned previously, there remain issues associated with accessibility that will underpin how Lot 1 Armadale Road evolves as a Structure Plan, and accordingly this will be used to consider how it relates to the surrounding residential development within Banjup North. Should a commercial outcome be pursued for Lot 1 Armadale Road, it would appear that the City should focus its efforts on requiring access from the western adjoining industrial precinct, rather than contemplating commercial level traffic going through a future residential neighbourhood.</p> <p>8. Not supported. As per 7 above.</p> <p>9. Not supported. As per 7 above.</p>

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			<p>10. As mentioned above, there are a variety of issues complicating the future development of Lot 1 Armadale Road. This requires a comprehensive structure planning process to be undertaken, to investigate (inter alia) land use, accessibility, interface, servicing, pedestrian connection, open space provision and the like. It is also apparent that the City should be focussing a separation of traffic between commercial and residential uses, such that the future outcomes for Lot 1 Armadale Road should be focussed on investigating access opportunity from the western adjoining industrial/commercial precinct, as opposed to through the future residential neighbourhood.</p> <p>The future residential neighbourhood would be inappropriate to consider as the main access points for Lot 1 Armadale Road should a commercial outcome be a goal of the proponent.</p> <p>11. Noted. The associated LWMS demonstrates no implications or use of Lot 1 for water management.</p>

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			<p>12A. Noted. The applicant will be requested to update the plan.</p> <p>12B. Noted. The layout shown is notional only with no statutory weight and shows how future development 'may' integrate. This can be removed however by the proponent.</p>
14	Department of Health PO Box 8172 PERTH BUSINESS CENTRE WA 6849	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>1. <u>Water and Sewerage</u></p> <p>The DOH has no objection to the rezoning proposal and the Local Structure Plan subject to all developments complying with the provisions of the Government Sewerage Policy - Perth Metropolitan Region. In this regard, it is noted that reticulated sewerage and water are planned to serve the developments.</p> <p>2. <u>Health Impact Assessment</u></p> <p>This proposal provides the City of Cockburn the opportunity to minimise the land use conflicts and incompatible activities which is the most common issue in land use and development. Health impacts draw many attentions in those issues and hence, should be appropriately and adequately addressed at this level.</p> <p>Health concerns about buffers are not limited to the industries and infrastructures. Consideration must be given to the need for adequate buffers to protect residents from lifestyle and public health impacts such as dust from resource extraction activities or odour for</p>	<p>1. Supported. The provision of water and sewerage services will be implemented at the subdivision stage.</p> <p>2. Supported. Health issues as they relate to orderly and proper planning considerations have been an important assessment element of the Proposed Structure Plan.</p>

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		<p>example.</p> <p>Should residential development be proposed in or near the Amendment 95, consideration of public health impacts is required. Enclosed is a scoping tool that should be incorporated. The scoping tool is available for download at:  <a href="http://www.public.health.wa.gov.au/cproot/3965/2/Residential%20estates%20and%20precincts%20JUNE%202011.pdf">http://www.public.health.wa.gov.au/cproot/3965/2/Residential%20estates%20and%20precincts%20JUNE%202011.pdf</a></p> <p>You should also consider incorporating Health Impact Assessment (HIA) and/or Public Health Assessment (PHA) principles in your decision making process. For your information and guidance, you may access the relevant information at the following sites:</p> <p>HIA –  <a href="http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm">http://www.public.health.wa.gov.au/2/1400/2/health_risk_assessment.pm</a></p> <p>PHA –  <a href="http://www.public.health.wa.gov.au/2/1399/2/publichealth_assessment.pm">http://www.public.health.wa.gov.au/2/1399/2/publichealth_assessment.pm</a></p> <p>3. <u>Toxicology Programs and Services</u></p> <p>The Department of Health advises that any staged development that includes or is in the vicinity of residential dwellings, needs to be carried out in accordance with an appropriate air quality management plan. It is advised that an air quality management plan be incorporated into the provisions for 'Development Area 37'.</p>	
15	M and L Taylor 287 Jandakot Road	<b>OBJECTION</b>	<p>3. Issues surrounding air quality will be dealt with at each stage of subdivision. It is noted however that no existing uses affect the subject land which pose an air quality risk at this point in time.</p>

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	JANDAKOT WA 6164	<ol style="list-style-type: none"> <li>1. Increased road noise, increased traffic, destruction of wildlife/fauna, peaceful enjoyment of living amenities reduced.</li> <li>2. Remnant vegetation on proposed Fraser Road should be retained and Fraser Road relocated. This would improve the safety of the intersection by improving visibility if relocated to a more suitable location. The proposed intersection location is at the top of a hill. The increased noise associated with a roundabout would have an adverse effect on living amenities close to the roundabout. Carnaby cockatoos when flocking feed twice a year on existing banksias on the proposed Fraser Road site, and small groups at other times. The proposed location has low visibility from all directions and could pose a serious safety risk for traffic and residents within the local area.</li> <li>3. Also we were assured by the State Government and the Shire of Cockburn that Fraser Road would never be opened as a through road because of the safety risk associated with an intersection at this location and have planned our building with this in mind and relocated our building envelope which could now be to our detriment.</li> </ol>	<ol style="list-style-type: none"> <li>1. Noted but not supported. The subject land represents a strategic opportunity in which to provide for development which has excellent access to existing infrastructure, services, facilities and employment. This opportunity reflects the challenge set for urban infill targets via the State Government's Directions 2031 Strategic Plan, and is considered to represent an important opportunity for urban development. The focus upon sustainable development and particularly encouraging public transport usage will help to further demonstrate how forms of urban infill need to be managed in a growing City.</li> <li>2. Noted. The Structure Plan facilitates the retention of the majority of the Fraser Road Reserve and therefore the majority of the vegetation. The location of the intersection of Fraser Road and Jandakot Road will be subject to detailed design assessment by the City and Main Roads against relevant standards.</li> <li>3. Noted. The Structure Plan demonstrates only utilising the existing intersections of Fraser Road. At this point in time no</li> </ol>

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			consideration has been given to constructing Fraser Road between Jandakot Road and Armadale Road.
16	Banjup Residents Group 207 Liddlelow Rd BANJUP WA 6164	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>1. The Banjup Residents Group notes that the WAPC and Parliament have approved the rezoning of the Banjup Quarry area to Urban, so facilitating Stockland's development of the site. The Banjup Residents Group acknowledges that Stockland's structure plans appear to show a development that has qualities consistent with nearby urban areas and, we assume, is consistent with all applicable planning legislation.</p> <p>Therefore, the Banjup Residents Group makes no comment as to how the Banjup Quarry development will be structured within its boundaries. The City of Cockburn may infer from this that we do not object to the proposed structure. However, any support that we might give to the Scheme Amendment is subject to conditions that we maintain the City should impose on the development arising from its impacts upon the surrounding areas.</p> <p>2. <u>Conditions of Support</u></p> <p>We note that 2,000 homes will be built in the Banjup Quarry development. Banjup residents especially will be significantly affected not only by the 5,000 or so new residents brought into their immediate neighbourhood but also by the construction work that precedes them.</p> <p>The Banjup Residents Group urges the City of Cockburn to ensure that the following conditions are included in the</p>	<p>1. Noted.</p>



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		<p>any approval of the Structure Plan and Scheme Amendment.</p> <p>A. Road Upgrades</p> <p>Earthmoving vehicles, delivery vehicles, and workmen's vehicles will make heavy usage of the local major and minor road infrastructure around the Quarry area during its 5 year development, especially during peak traffic hours. This will add significantly to the congestion problems around Cockburn Central. Subsequently, residents will cause at least 4,000 more vehicle movements a day to use Armadale Road and Jandakot Road.</p> <p>Before any earthworks begin the City should require:</p> <ul style="list-style-type: none"> <li>i. Upgrading to dual carriageway of Armadale Road between Tapper Road and Liddelow Road, including adequate control of the intersection at the southern end of Fraser Road</li> <li>ii. Upgrading to dual carriageway of Jandakot Road between Solomon Road and the northern end of Fraser Road</li> <li>iii. Synchronising of all traffic lights from Gateway, across the freeway, to Tapper Road and on to Fraser Road so that cohorts of dozens of vehicles can traverse the area quickly and not be subjected to stop starts.</li> </ul> <p>B. Water</p>	<p>2A. Noted. It is the City's position that upgrades to Jandakot and Solomon Roads by the developer be secured in a timely manner to ensure the effective management of additional traffic generated by the development. It is for this reason that any approval of the LSP will be subject to the finalisation of a voluntary legal agreement with the developer to ensure the timely delivery of the required upgrades. The City will continue to monitor the situation and is already undertaking upgrades to the wider network together with Main Roads WA with the aim of improving traffic circulation in the locality.</p> <p>2B. Noted. A Local Water</p>

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		<p>The Banjup Quarry area is on the Jandakot Water Mound and adjoins 'Bushland Forever' areas. The Quarry development is already contingent upon not compromising the quality of the water beneath it, particularly as there is a Water Corporation production bore adjoining it on Solomon Road. The Banjup Residents Group also believes that the development should not affect the level of the water table below it. Banksia trees in Banjup have been dying in recent years because their roots can no longer reach the falling water table. Any further lowering could be disastrous for surrounding areas.</p> <p>Garden bores in the Quarry area could lower the water table significantly and so should be prohibited. The City should require that all title deeds for the Quarry area should include a caveat banning the installation of private garden bores.</p> <p>Sadly, the more people that are close to a natural bush area, so the likelihood of fires increases. To protect bushland adjacent to the Quarry, the City should require that fire hydrants be installed along Fraser Road.</p> <p>C. Remnant Vegetation</p> <p>The soils on the Jandakot Water Mound are weak. Consequently, the native vegetation is fragile. The impact of thousands of people walking and playing in the remnant vegetation of the Banjup Quarry could be seriously detrimental.</p> <p>These areas will need constant care and maintenance. The City of Cockburn should require</p>	<p>management Strategy has been prepared as part of the Local Structure Plan process and is subject to approval from the Department of Water and the City. The LWMS and future Urban Water Management Plan (submitted with subdivision) will deal with matter outlined in this point.</p> <p>2C. Noted. Prior to subdivision and/or clearing the Department of Environment and Conservation will need to be satisfied adequate studies and mitigation measures have been undertaken in relation to vegetation and flora.</p>

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		<p>that a comprehensive management plan is adopted for the long term protection of the remnant vegetation. The plan should ensure that any replanting is done using only species native to the Banjup area and not introduce species from elsewhere.</p> <p>D. Public Transport</p> <p>A high proportion of the residents in the 2,000 homes of the Banjup Quarry development will use the public transport facilities that are already almost at capacity. Not least, parking at Cockburn Central railway station will come under even more pressure. To avoid this, the City of Cockburn should require that Stockland make a fully funded commitment over 20 years to provide frequent shuttle buses in the mornings and evenings to all parts of the Banjup Quarry development.</p> <p>3. <u>Set a Precedent for Other Banjup Developments</u></p> <p>The Banjup Residents Group notes that apart from some 'Bushland Forever', the land between Armadale Road and Jandakot Airport has been significantly degraded by sand mining over many years. It seems likely, therefore, that Stockland's proposal to develop the Banjup Quarry will be just the first of several development proposals for the area, with the next likely to be from the Department of Housing for an adjacent development.</p> <p>The Banjup Residents Group urges the City of Cockburn to view Stockland's proposal as a precedent for future proposals for the area and to ensure that whatever minimum conditions would be placed on subsequent developers of the sand quarry area should also be placed</p>	<p>2D. The applicant has been in discussion with the Public Transport Authority regarding future public transport links through and around the proposed site. In addition, the City has required strong pedestrian and cycle links between the subject site and Cockburn Central Train Station as a requirement of Development Area 37. While the City would encourage a developer funded public transport link, it is beyond the capacity of the City to mandate this.</p> <p>3. Noted but not supported. The subject site was rezoned from 'Rural – Water Protection' to Urban under the MRS based on the recommendations of the WAPC's Draft Outer Metropolitan Perth and Peel Sub-regional Strategy which identified the site as having urban potential based on its proximity to</p>

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		upon Stockland's proposal.	<p>the Cockburn activity centre.</p> <p>Council's support for the rezoning was subject to future applications and studies meeting and exceeding the planning and environmental objectives and requirements of the City and various State Government approval authorities. This has been demonstrated by the current application.</p> <p>At this stage there is no strategic basis for the City to be considering any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound). As mentioned the strategic basis for this proposal has been formed from Directions 2031 and its associated Sub-regional Strategy - such document do not support further urbanisation at this time.</p> <p>The strategic planning represented by the WAPC's Draft Strategy has been the mechanism through which the ex-sand quarry project in Banjup has been identified as having potential for urban development. It should be noted that this proposition is only being considered after extensive research into the potential</p>

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			<p>environmental impacts and in particular the impact on the groundwater mound. At this stage no other such investigations are being undertaken by the State Government for other areas of the 'Resource' zone above the Jandakot Water Mound. Notwithstanding, the resolution does confirm ongoing dialogue with the Department of Planning in respect of the review of the Jandakot Water Mound.</p>
17	R Mews 7 Cessna Rise JANDAKOT WA 6164	<p><b>OBJECT</b></p> <p>1. <u>Environmental – Underground water supply contamination risk</u></p> <p>When we moved to Jandakot, we were advised of the stringent conditions adherent to all properties in the resource zone which was created to protect the Jandakot Water Mound, which is one of the state's main drinking water supplies. We made a lifestyle change that we were expressly told by the council that we could never ever redevelop. We paid a lot of money this lifestyle in which we believed was safe from greedy developers because of our precious environment.</p> <p>I would like to remind you of your publication, the "information sheet on Resource Zone guidelines for land use." Under the heading "<u>Background</u>," it goes on to state that the "<i>groundwater is a highly valued and important public resource and requires protection to avoid serious irreversible contamination.</i>" As well as under the heading, "<u>Subdivision</u>"(in the information sheet on Resource Zone</p>	<p>1. Noted but not supported. The subject land represents a strategic opportunity in which to provide for development which has excellent access to existing infrastructure, services, facilities and employment. This opportunity reflects the challenge set for urban infill targets via the State Government's Directions 2031 Strategic Plan, and is considered to represent an important opportunity for urban development.</p> <p>Council's support for the rezoning was subject to future applications and studies meeting and exceeding the planning and environmental objectives and</p>

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		<p>guidelines for land use) it goes on to state that “<i>much of the resource zone is already subdivided into 2 hectare lots.....<u>Larger lots may be subdivided into rural living purposes, provided the new lots are at least 2 hectares in area.</u></i>”</p> <p>What has changed? Is the Jandakot Water Mound no longer required and contamination no longer an issue? In March 2000 when the special rural zone was changed to resource zone, it was done for a reason to protect our precious ground water supply. 1800 extra dwellings does not fit into the template and legislation that the rezoning was designed to protect. It would be completely irresponsible to disregard previous efforts in protecting the Jandakot Groundwater Mound for the financial benefit of a few.</p> <p>A dense increase in dwellings on the mound will result in more chemicals like pesticides and fertilizers leaching into our precious ground water supply and contaminating it beyond repair.</p> <p>2. <u>Environmental – Wildlife habitat loss.</u></p> <p>Currently in Jandakot, we enjoy a local marsupial commonly called a Bandicoot (<i>Isoodon obesulus</i>). It is slowly diminishing from the area due to not only the destruction of virgin bushland, but the introduction of aggressive so called “pets,” like cats and dogs. An increase in population to the area would not only destroy their natural habitat. It will undoubtedly increase predators (cats and dogs) to the area. These poor little defenceless native creatures are also vulnerable to motor vehicles which would also substantially increase in the area. The bandicoot is an enjoyment in the area that will be pushed to extinction the way we are going. The natural habitat for</p>	<p>requirements of the City and various State Government approval authorities. This has been demonstrated by the current application.</p> <p>At this stage there is no strategic basis for the City to be considering any other rezoning, urbanisation or industrialisation within the 'Resource' zone (Jandakot Water Mound). As mentioned the strategic basis for this proposal has been formed from Directions 2031 and its associated Sub-regional Strategy - such document do not support further urbanisation at this time.</p> <p>2. As mentioned above environmental considerations of the site have been used to carefully underpin many aspects of the Structure Plan. This includes the protection of remnant bushland on the site, and remediation/buffer of the eastern adjoining bush forever site.</p>

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		<p>many wildlife creatures ranging from kangaroos to lizards and birds, and beyond, are all slowly disappearing. Why would we expedite this over the greed of a few?</p> <p>3. <u>Environmental – Traffic dangers</u></p> <p>As the council should be aware, Jandakot Road is not capable of handling the traffic it now has without adding another 5400 vehicles (average of 3 vehicles per dwelling) to it. The traffic back up for most of Jandakot Roads length in both directions at time making it extremely difficult to not only enter onto the road but to exit it as well. There has already been several serious (some resulting in death) accidents on Jandakot road and I fear with even more traffic, impatient drivers frustrated by the inability to progress on and off the road will lead to more accidents and loss of lives. I would encourage counsellors to visit Jandakot Road from 5.30 am when the traffic starts to build until 8.00am when the traffic is at its heaviest. Pull into a side street and try to get back onto the road..... Good luck! The traffic remains heavy until at least mid-morning and starts to increase again around 3.00 pm when you get to go through it all again!</p> <p>4. <u>Crime</u></p> <p>At present, we are of a lucky few in Western Australia. Being typically in a rural environment, the crime rate is exceptionally low in the area. An introduction of 1800 dwellings will probably equate to at least 7200 additional people to the area which I fear will no doubt result in a dramatic increase in local crime.</p> <p>5. <u>Summary</u></p> <p>I realise that with 1800 dwellings comes 1800 sets of</p>	<p>3. Noted. It is the City's position that upgrades to Jandakot and Solomon Roads by the developer be secured in a timely manner to ensure the effective management of additional traffic generated by the development. It is for this reason that any approval of the LSP will be subject to the finalisation of a voluntary legal agreement with the developer to ensure the timely delivery of the required upgrades. The City will continue to monitor the situation and is already undertaking upgrades to the wider network together with Main Roads WA with the aim of improving traffic circulation in the locality.</p> <p>4. Noted but not supported. There is no evidence that this proposed development will change or effect the potential for crime to occur in the local area. Principles underpinning the Structure Plan include passive surveillance and</p>

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		<p>rates and incomes for the council. However, I urge the council to only consider the irreversible environmental impact damage to wildlife and groundwater this proposal will make, should it be approved. The council should stand back and look at the bigger picture which is not only overpopulation of the local area, but also detrimental damage to our children's future.</p> <p>Protect the Jandakot Underground Water supply so that it won't be irreversibly contaminated. Protect the local wildlife from extinction. Avoid over populating our local roads which will reduce lives.</p>	<p>crime prevention through environmental design, both of which are shown to help reduce the potential for crime to occur.</p> <p>5. Noted but not supported.</p>
18	S and J Walton 1 Cessna Drive JANDAKOT WA 6164	<p><b>OBJECT</b></p> <p>Provisions need to considered as per attachment: Water Quality Preservations, Safety, Noise, Fire/Bushland Protection , Light Pollution</p> <p>1. Our objection is relative to our quest to retain the quality of life we selected in our purchase of land in the City of Cockburn in 1992. At that time we chose to do something different and in some ways, more difficult. Foregoing some services and residential conveniences, we chose a rural, quiet, bushland setting. That rural setting is being slowly eroded as the reach of residential and industrial needs encroaches on our land. Although we understand the projection and need for further housing in the City of Cockburn we would like to know that our Councillors, Council, Town Planners, Environmental Officers would endeavour to approve changes to meet the needs of the "new" residents to the City Of Cockburn, while not forgetting that these same needs need to be met for their "old" residents on the other side of the road to retain as much of their existing quality of life.</p>	<p>1. Noted. The subject land represents a strategic opportunity in which to provide for development which has excellent access to existing infrastructure, services, facilities and employment. This opportunity reflects the challenge set for urban infill targets via the State Government's Directions 2031 Strategic Plan, and is considered to represent an important opportunity for urban development.</p>



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		<p>2. The needs we would like considered include the following:</p> <p>A. Water Protection of the water mound for all residents. The Council needs reminding the existing residents are without water services and rely totally on the quality of the water being protected.</p> <p>B. Safety</p> <p>i. Roads</p> <p>Our safety exiting and entering Cessna Drive Currently there is no provision for our safe exit from and entry into the road. Cessna Drive is presently the exit point for all Fraser Road and Cessna Road householders as the Fraser Road exit point is on top of a hill and on a bend. Accordingly, those residents on Fraser Road generally exit from Cessna Drive. A proposed roundabout at this point needs further consideration as it is a danger point.</p> <p>ii. Cycle ways and footpaths</p> <p>Provision of footpaths and cycle ways from the development all the way to the freeway and the train station, not just to the edge of the development.</p> <p>C. Noise Structure Boundary to both sides of Jandakot Road. Provision to protect the increasing traffic noise level of Jandakot Road needs to be considered for both sides of Jandakot Road not just for the "new" residents. The projection of a four</p>	<p>2A. Noted. A Local Water Management Strategy has been prepared as part of the Local Structure Plan process and is subject to approval from the Department of Water and the City. The LWMS and future Urban Water Management Plan (submitted with subdivision) have a key focus on protecting the quality of the groundwater resource. This represents a key consideration underpinning all aspects of the Structure Plan.</p> <p>2Bi. Noted. A number of road upgrades will be required to be undertaken by the proponent. The location of the intersection of Fraser Road and Jandakot Road will be subject to detailed design assessment by the City and Main Roads against relevant standards.</p> <p>2Bii. Noted. Strong pedestrian and cycle links between the subject site and Cockburn Central Train Station are a requirement of Development Area 37.</p> <p>2C. Noted. The future design of an</p>

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		<p>lane –dual carriageway for Jandakot Road without such a structure boundary for our side of the major road increases noise and infringes on the quality of life we selected for ourselves and our family.</p> <p>D. Fire Protection/Bushland Protection Structure Boundary to protect endemic species from fire.</p> <p>E. Light Pollution Provision made for all lighting in the development plan to be full-cutoff fixtures, i.e. all light directed downward-no light emitting upward, and fitted with energy efficient lamps.</p>	<p>upgraded Jandakot Road forms part of the proposed works required to be funded by the developer where Jandakot Road adjoins the subdivision area. All issues surrounding the road design including noise will be dealt with as part of the future design.</p> <p>2D. Agreed. Both bushfire and conservation considerations represent issues which have been considered and integrated into the Proposed Structure Plan.</p> <p>2E. Supported (in part). Australian Standards exist regarding the spillage of light. This matter will be dealt with at the detailed design phase following any subdivision application.</p>
19	Affected Landowner (Names & address withheld)	<p><b>OBJECT</b></p> <p>1. I tune a race car in my premises late at night. One of the reasons I bought the property was so I could do this without disturbing neighbours. Regardless of warnings that people will be building next to a LIA, pressure will mount over the years on the tenants in the LIA, particularly over noise. What is the strength of prior occupancy?</p> <p>2. In conversation with Roberto Colalillo I was advised that house design was more effective in combatting noise than a POS barrier. What's wrong with having both? Include a strip of POS as wide as possible along the LIA</p>	<p>1. The Proposed Structure Plan is cognisant of the need to ensure that development adjoining the existing commercial / light and service industry area to the west is based upon appropriate interface as well as quiet house design. This underpins the strip of non-residential land to the north of Dollier Road, as well as the compact urban form adjoining the back of existing warehousing</p>

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		from Armadale Road to Dollier Road. Or perhaps owners in the LIA can be given advance opportunity to purchase a block as a buffer. I don't expect my objection is going to stop this development but there needs to be some recognition, and hopefully action, to allay concerns of owners in the LIA.	<p>along the common boundary. However, this does not remove the requirement for existing businesses to comply with noise regulations. Noise issues are dealt with in accordance with the legislation, and it is impossible to contemplate what may or may not result in the future.</p> <p>2. Noted. As per 1 above.</p>
20	Water Corporation P O Box 100 LEEDERVILLE WA 6902	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>1. Thank you for your letter dated 14 March 2013. The Corporation offers the following comments in regard to these proposals. As this land was within a water resource area that is normally not urbanised scheme planning has not been undertaken.</p> <p>Due to both water and wastewater servicing requirements indicated below in more detail it appears the structure plan will need to be amended so there is road access to Armadale Road from the subject land for this initial developer and not the adjoining land.</p> <p>2. Water</p> <p>The subject area falls outside an approved planned water supply scheme and therefore a reticulated potable water supply is not immediately available. Conceptual planning indicates that substantial headworks mains are required to be extended to the subject land. One main could be a DN500 from the DN760 south of Armadale Road. A control valve is also required to be relocated. Please note this is not the approved scheme planning and could</p>	<p>1. Noted</p> <p>2. Noted</p>

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		<p>change.</p> <p>The subject area falls within the Jandakot Underground Water Pollution Control Area (UWPCA). Developers within a UWPCA need to fulfil their legal responsibilities including those covering land use planning, environmental, health and building permit matters. The Department of Water is responsible for managing and protecting Western Australia's water resources. It is therefore recommended that this proposal is referred to the Department of Water for assessment in accordance to the <i>Land Use Compatibility in Public Drinking Water Source Areas</i> publication if it has not been already.</p> <p>A large portion of the subject area is within a 300m Wellhead Protection Buffer. These are declared to protect the quality of groundwater being extracted for drinking water. In these zones groundwater moves rapidly towards wells and any surface contamination moving through the soil could find its way into scheme water supplies. Activities and land uses in these areas are restricted and subject to the DEC approval processes in accordance with the Western Australian Planning Commissions current State Planning Policy 2.7. Land use restrictions essentially apply to the storage of materials such as petroleum products, pesticides and fertilisers (including manures) and the keeping of stock</p> <p>3. Wastewater</p> <p>The subject area falls outside a planned sewerage scheme and therefore a reticulated wastewater is not immediately available. Conceptual planning indicates that a permanent pump station (Type 90) is required for the catchment the subject area falls within. This pump stations pressure main should be discharging via a</p>	<p>3. Noted</p>

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		<p>gravity main into the Bibra Lake Main Sewer several kilometres to the west. Please note this is not approved planning and could change. A pump station will require appropriate land to be provided for the works and the odour buffer that will surround the works. The extent of the buffer should be determined at the planning/design stage to ensure that only compatible land use is within the buffer. A route for the headworks mains will also be required, up to 20 metres wide. The route should be in the form of a road reserve.</p> <p>4. Drainage</p> <p>Only a portion of the subject area falls within the Southern Lakes Drainage Catchment. The Water Corporation does not want any additional areas within the subject site to discharge into this catchment.</p> <p>5. General Comments</p> <p>It should also be noted that as this land is currently deemed to be "pioneer", it will be a requirement that all the costs associated with the delivery of the services to the land will be met by the proponent.</p> <p>The implementation of Water Corporation planning for the provision of the infrastructure to service the area is dependent on the timing of development within the area. Developers should liaise with the Water Corporation at the preliminary planning stage of any development to determine the Corporation's current servicing and land requirements.</p> <p>The principle followed by the Water Corporation for the funding of subdivision or development is one of user pays. A contribution for Water, Sewerage and Drainage</p>	<p>4. Noted</p> <p>5. Noted</p>

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		<p>headworks may be required. In addition the developer may be required to fund new works or the upgrading of existing works and protection of all works. Any temporary works that may be required are to be fully funded by the developer. The Corporation may also require land being ceded free of cost for works.</p> <p>The information provided above is subject to review and may change. If the proposal has not proceeded within the next 6 months, the Corporation should be contacted to confirm if the information is still valid.</p>	
21	D E & F C Martin 29 Windelya Road KARDINYA WA 6163	<p><b>NO OBJECTION (subject to conditions)</b></p> <ol style="list-style-type: none"> <li>1. Continue the vegetation buffer the full length of Jandakot Road.</li> <li>2. Provide cycle paths within the subdivision.</li> <li>3. With the increased traffic flow on Jandakot Road a dedicated cycle/pathway is required along Jandakot Road to link with Jandakot shops and freeway cycle lanes.</li> </ol>	<ol style="list-style-type: none"> <li>1. Supported. The proponent has lodged amended plans increasing the amount of vegetation being retained adjacent to Jandakot Road to include the continuous link.</li> <li>2. Supported. Suitable cycle routes and paths will be provided within the development area at the subdivision stage as well as linking to the nearby train station.</li> <li>3. Supported. Adequate provision will be made for pedestrian and cycle linkages along the upgraded Jandakot Road and surrounding network to the satisfaction of the City.</li> </ol>
22	Lynne Wrigglesworth Main Road Department P O Box 6202	<p><b>NO OBJECTION (subject to conditions)</b></p> <p><u>SCHEME AMENDMENT NO. 95</u></p>	

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
	EAST PERTH WA 6892	<p>1. Main Roads has no objections to the rezoning of Lots 1 and 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road from 'Resource' to 'Development', and the addition of a new 'Development Area 37' (DA37) to Schedule 11 subject to the following modifications to the provisions of DA37 being made.</p> <p>A. Provision 1 - An approved Structure Plan or approved Structure Plans which include Lot 1 Armadale Road together with all approved amendments shall apply to the land in order to guide subdivision and development.</p> <p>B. New Provision 2 - As no direct vehicle access will be permitted to Armadale Road, the Structure Plan or Structure Plans are to be aligned to ensure that vehicle access to Lot 1 Armadale Road can be obtained from internal roads within Lots 9002, 9004 and 132.</p> <p>C. Renumber the remaining provisions making minor changes to reflect the probability of multiple structure plans.</p> <p>The need for multiple structure plans has arisen due to the proposed Banjup Quarry Local Structure Plan having excluded Lot 1 Armadale Road.</p> <p><u>PROPOSED BANJUP QUARRY LOCAL STRUCTURE PLAN</u></p> <p>2. Main Roads are currently working with the proponents on several issues that have a direct impact on this structure plan. However, Main Roads is prepared to support the proposed Banjup Quarry Local Structure Plan subject to the following issues being resolved to</p>	<p>1. Noted.</p> <p>1A. Not supported. The current wording is consistent with existing development areas within the City and doesn't warrant modification in this case.</p> <p>1B. Not supported. This requirement is standard for all properties and development located adjacent to MRS Regional Roads and is dealt with at the Structure Plan stage.</p> <p>1C. Not supported. As above.</p> <p>2. Noted.</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Main Roads satisfaction</p> <p>A. Armadale Road is classified as a freight road and Primary Regional Road as gazetted in the Metropolitan Region Scheme (MRS). Main Roads has recently completed a Kwinana Freeway study from just south of Armadale Road/Beeliar Drive to just north of Berrigan Drive, which also considered the Armadale Road/Tapper Road/Verde Drive (future North Lake Road) intersection. Due to the excessive traffic demands at this location including freight traffic, a significant upgrade to this intersection is required. This intersection upgrade also considered PTA requests for bus priority through this intersection. Due to the scale of the intersection Main Roads is in consultation with the proponents to determine the exact location of the realigned intersection of Fraser Road with Armadale Road.</p> <p>B. As Council is aware, Main Roads has recently completed a design review of Armadale Road from Tapper Road I Verde Drive (future North Lake Road) to Anstey Road intersections. This design review has identified future land requirements outside of the existing Armadale Road MRS reservation as indicated on the attached plan 1.7103. This additional land requirement will need to be allowed for in the Structure Plan.</p> <p>C. During discussion with the proponents Main Roads have requested that they look at relocating the first full movement intersection along Fraser Road from the intersection of Armadale Road further north in order to provide greater separation from the traffic signalised intersection with Armadale Road.</p>	<p>2A. Noted and supported. The Proposed Structure Plan will need to ensure its intersection access from Armadale Road is located and design to the satisfaction of MRWA and constructed at the developer's cost.</p> <p>2B. Noted. A Planning Control Area will need to be declared under the Planning and Development Act 2005 in order to secure additional land outside the current MRS Regional Road Reservation. The Structure Plan itself is sufficiently flexible to be able to deal with this issue. The applicant will be advised of this additional land take requirement.</p> <p>2C. Noted. Subject to detailed design at subdivision approval.</p>



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		<p>D. No direct vehicle access to Armadale Road will be entertained. Therefore this Structure Plan must make adequate provision for direct vehicle access to adjoining Lot 1 Armadale Road.</p> <p>E. Main Roads has consistently stated that the Structure Plan must be configured to encourage traffic to utilise Jandakot Road to access Kwinana Freeway rather than Armadale Road. The major north-south route within the proposed Structure Plan does not achieve this outcome as it is not direct and requires negotiating two intersections around the proposed town square. This network needs to be modified to achieve an outcome that will reduce future traffic pressure on Armadale Road.</p> <p>F. There has also been discussion relating to the ultimate ground levels along the Armadale Road boundary, with the proponents requesting that the current 'excess soil' be utilised by them as fill in other locations within the development area. However, Main Roads will require this 'fill' for its road works when the future upgrading to dual lanes occurs. Therefore the removal of this 'fill' will have an impact on the results of the noise report that has already been undertaken.</p> <p>G. The proponents have advised that they are aware that this report will need to be reworked once the final ground levels of the development have been determined. Whilst the current noise impact report makes recommendations for noise walls and facade treatments, the revised report will need to be reviewed following the final design and</p>	<p>2D. Noted and supported in part. There are a variety of issues complicating the future development of Lot 1 Armadale Road. This requires a comprehensive structure planning process to be undertaken, to investigate (inter alia) land use, accessibility, interface, servicing, pedestrian connection, open space provision and the like. It is also apparent that the City should be focussing a separation of traffic between commercial and residential uses, such that the future outcomes for Lot 1 Armadale Road should be focussed on investigating access opportunity from the western adjoining industrial/commercial precinct, as opposed to through the future residential neighbourhood.</p> <p>The future residential neighbourhood would be inappropriate to consider as the main access points for Lot 1 Armadale Road should a commercial outcome be a goal of the proponent</p> <p>2E. Not Supported. The City supports the use of Regional Roads for</p>

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		<p>preparation of the subdivision layout. This noise assessment must consider the ultimate built form of Armadale Road including future forecast traffic projections.</p> <p>H. No stormwater drainage shall be discharged onto the Armadale Road road reserve.</p> <p>I. No earthworks shall encroach onto the Armadale Road road reserve</p> <p>J. The developer shall be responsible for all costs involved in the design and construction of the realigned Fraser Road intersection with Armadale Road. This includes signing, road markings, relocation of services, street lighting and Main Roads costs involved in the checking of the design and construction drawings and any site inspections.</p> <p>K. Main Roads approval for the construction drawings is required before any work is undertaken within the Armadale Road road reservation. A detailed traffic management safety plan while working within the road reservation is to be submitted as part of this approval.</p> <p><b><u>ADVICE TO COUNCIL</u></b></p> <p>3. It is also noted that a shared path is proposed along the northern side of Armadale Road. Whilst this shared path needs to be located on the northern side for the use of future residents of this area, a pedestrian safety issue that does need to be addressed, is how and where the users of this path are going to be able to cross Armadale Road as the currently proposed road width is in the order</p>	<p>Regional traffic. The use of local roads (Jandakot Road) as the primary access to the Freeway is not supported nor an appropriate solution to dealing with the need for State funded upgrades to occur to Armadale Road. Jandakot Road and Solomon Road are both required to be upgraded where they adjoin the subdivision area, however the intent of these upgrades is not to have them performing a regional road function which is what Armadale Road is designated for.</p> <p>2F. Supported. The use of fill within the Armadale Road reserve is an issue for Main Roads to decide on as it is their fill to own.</p> <p>2G. Supported. The future subdivision stages will each be subject to a detailed noise report. The stage of subdivision adjoining Armadale Road will need to be based on a noise report which considers the final design aspects of Armadale Road and importantly not be based on any assumptions which have changed as the design has changed.</p> <p>2H. Supported.</p> <p>2I. Supported.</p>

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		<p>of 45 m. Whilst there is an underpass on the western side of the Kwinana Freeway it is likely that users will wish to cross and connect to the residential area on the southern side before this point.</p>	<p>2J. Supported.</p> <p>2K. Supported.</p> <p>3. The design of a future footpath connecting the Structure Plan to the train station precinct is predicated on a movement on the north side of Armadale Road. Should users wish to cross Armadale Road to access the residential area to the south, this would need to take place at the light controlled intersections which exist at Tapper Road.</p>
23	<p>Development Planning Strategies PO Box 6697 EAST PERTH WA 6892</p>	<p><b>NO OBJECTION (subject to modifications)</b></p> <p>1. Thank you for the opportunity to comment on the proposed Banjup Quarry Local Structure Plan. We make this submission on behalf of our client Stockland WA Development Pty Ltd, which is the current owner of the lots to which the proposed Banjup Quarry Local Structure Plan (LSP) applies.</p> <p>The focus of our submission is to provide comment and seek Council's support for the reconfiguration of the Public Open Space (POS) along the northern boundary of the LSP area, and to apply the provisions of the current (2006 adopted) State Planning Policy No. 5.3 Jandakot Airport Vicinity (SPP 5.3) to the proposed LSP. We also provide comment on other issues, including a status update arising from our ongoing engagement with some of the major servicing authorities.</p> <p>Our comments are outlined in the sections below.</p>	<p>1. Noted</p>

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>2. POS Reconfiguration</p> <p>We seek Council's support for the proposed reconfiguration of POS area 'B3' to allow for additional vegetation retention in this location, given this vegetation is considered to be in good to very good condition. Attachment 1 illustrates the extent of the proposed modification.</p> <p>The proposed reconfiguration generates an additional POS area of 8559m<sup>2</sup>. An updated Plan 10 - Public Open Space is therefore provided as Attachment 2.</p>	<p>2. Supported. The additional POS is supported and will form part of the final Structure Plan design.</p>
		<p>3. State Planning Policy No. 5.3 Jandakot Airport Vicinity</p> <p>In preparation of the LSP it was implied that the Draft SPP 5.3 was a 'seriously entertained' document that would be adopted prior to approval of the LSP, and therefore any proposed new development was required to comply with the new standards. Given this Draft policy has not yet been formally adopted by the Western Australian Planning Commission, and is not anticipated to be prior to LSP approval and may be subject to review(to reflect envisaged changes to the Australian Standards concerning Airport Noise), it is not considered appropriate to apply the provisions of the Draft SPP 5.3 at this time. We therefore seek Council's support to identify the "Core Area 20 ANEF Contour" as per the current (2006 adopted) State Planning Policy No. 5.3 Jandakot Airport Vicinity (SPP 5.3).</p> <p>The current SPP 5.3 illustrates that no portion of the Structure Plan area is impacted by the "Core Area 20 ANEF Contour" Attachment 3 refers. Therefore, pursuant to the current SPP 5.3, the proposed LSP complies with the policy provisions as it sits outside the "Core Area 20</p>	<p>3. Noted. The City notes that the proposal complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is yet to be finalised by the WAPC (southwest corner of land). It is recommended that the City request that the WAPC consider the implications of the revised SPP5.3 in respect to the proposal as part of their endorsement of the Structure Plan. It will be at the WAPC's discretion whether they're prepared to endorse the Structure Plan prior to the finalisation of the revised SPP5.3. Notwithstanding this, it is also recommended that all sensitive development integrate appropriate noise amelioration standards as part of development AND an appropriate notification be</p>

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		<p>ANEF" boundary.</p> <p>4. Department of Education</p> <p>We confirm that since initiation of advertising, discussions have been held with the Department of Education (DoE) and Training about the proposed location and configuration of the primary school. The DoE supports the inclusion and location of a school, and its co-location with the district level playing field. The proposed 3.5 hectare size of the school (in light of its co-location with the district playing field) was also supported. DoE confirmed it was prepared to work with Stockland to possibly facilitate early delivery of the school, at which time its configuration will also be addressed. Stockland has confirmed the outcomes of the meeting, in writing, and has provided a copy of that letter to the City.</p> <p>5. Aboriginal Heritage</p> <p>In accordance with Section 3.6 of the LSP, we confirm that an Archaeological Field Assessment has been undertaken over the areas of the site considered to have archaeological potential. The assessment found "with certainty that Aboriginal Site Number 4108 is not within the area of proposes road works. However, that area does Include areas of bare sand, albeit empty of Aboriginal artefacts. The remote possibility of encountering subsurface Aboriginal skeletal or cultural material remains. Therefore, as a means of ensuring compliance with the with the above Act, this email recommends that an archaeologist should be on hand to monitor the stripping of vegetation and removal of the surface layers of topsoil during road construction". A copy of the assessment is provided as Attachment 4 and will be included in Part 3 of the Structure Plan report prior to</p>	<p>placed on the title of all lots advising of this requirement to build to a high noise standard. Such approaches are used in respect of development adjoining major roads (Kwinana Freeway for example) and is considered appropriate in this case.</p> <p>4. Noted. As per submission of DoE.</p> <p>5. Noted.</p>

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		<p>final adoption of the Structure Plan by Council.</p> <p>6. Main Roads WA Support</p> <p>As discussed in the LSP, it is Stockland's intention to construct a new signalised intersection at Armadale/Fraser Road to facilitate access into the Estate. Main Roads WA (MRWA) has provided its 'in principle' support for the construction of this new intersection. Stockland is also engaging in fortnightly design meetings with MRWA to secure early approval of the detailed intersection design.</p> <p>7. Local Water Management Strategy (LWMS)</p> <p>We can confirm that a revised LWMS has been lodged with the Department of Water, copied to the City, with approval anticipated by 10 May 2013.</p> <p>8. Water Corporation</p> <p>Stockland is undertaking ongoing meetings with the Water Corporation regarding water and wastewater servicing of the site, and has received 'in principle' approval of the servicing strategy. Regular ongoing meetings are proposed to 'flesh out' the detailed design and obtain their early approval.</p> <p>9. Summary</p> <p>Thank you for the opportunity to make a submission on the Banjup Quarry Local Structure Plan. In light of the above we request Council's favourable consideration of the above mentioned modifications:</p>	<p>6. Noted.</p> <p>7. Noted.</p> <p>8. Noted.</p>

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		<ul style="list-style-type: none"> <li>• Reconfiguration of POS area 'B3'; and</li> <li>• Identification of the provisions of the current SPP 5.3.</li> </ul> <p>A composite LSP Plan, incorporating the above mentioned modifications is provided as Attachment 3.</p>	
24	Jandakot Airport 16 Eagle Drive JANDAKOT WA 6164	<p><b>NO OBJECTION (subject to conditions)</b></p> <p>With reference to the proposed Banjup Quarry Local Structure Plan, Jandakot Airport Holdings Pty Ltd (JAH) comments as follows:</p> <ol style="list-style-type: none"> <li>1. Section 5 of the Noise Impact Assessment (Appendix 6) states that "The airport caters for general aviation aircraft such as Dash 8, Fokker F50's and the like". This is incorrect. Jandakot Airport is a code 2B airfield which does not currently accommodate code 3B aircraft such as Dash 8s and Fokker 50s. The aircraft operating at Jandakot Airport are much smaller, such as the Cessna 152 and 172 models, Piper 44, and Robinson R22 and R44 helicopters. Smaller aircraft achieve a much slower rate of climb than the larger more powerful aircraft, and thus are often perceived to be noisier due to the lower flying heights.</li> <li>2. Section 5 of the Noise Impact Assessment Report further states that the "maximum noise level at this proposed development site is calculated to be 58 dB(A)". This appears to be incorrect. The Jandakot Airport N60 Contours, based on the ANEF 2029/30, shows the number of aircraft noise events on an average day that are 60 dB(A) or higher. The N60 contours, as published in the Jandakot Airport Master Plan 2014 and on the Jandakot Airport website, identify that the Banjup Quarry site is expected to receive a daily average of 200 noise</li> </ol>	<ol style="list-style-type: none"> <li>1. Noted. The Noise Impact Assessment needs to be modified to correct this.</li> <li>2. Noted. This will need to be modified. It is noted however that the 'N60' noise contours aren't used as a measure to inform land use planning. Instead the ANEF contours as they interrelate with SPP5.3 represent the current tools to guide strategic land use planning.</li> </ol>

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		<p>events above 60dB(A). (Refer to the Noise Modelling section at <a href="http://www.jandakotairport.com.au/community/aircraft-noise.html">http://www.jandakotairport.com.au/community/aircraft-noise.html</a> for the N60 Contour diagram).</p> <p>3. It is well accepted that people who are aware of aircraft noise impacts before they move to an area tend to have a higher tolerance than those who were unaware. On that basis, JAH is pleased that Section 2.2 of Noise Impact Assessment includes “notifications on titles” on “conditionally acceptable dwellings”. However, given the aircraft noise impact as identified in the N60 Contours, JAH requests that notification of being in a “high aircraft noise impact area” be extended to all land titles within the Banjup Quarry development.</p> <p>4. The Banjup Quarry site is located under the Jandakot Airport fixed-wing aircraft circuit paths, as shown on the Indicative Flight Track diagrams in the Master Plan 2014 or on the Jandakot Airport website at <a href="http://www.jandakotairport.com.au/community/aircraft-noise.html#Aircraftnoiseimpacts">http://www.jandakotairport.com.au/community/aircraft-noise.html#Aircraftnoiseimpacts</a>. Aircraft conducting circuit training, which is a repetitive touch-down and take-off procedure vital for pilot training, are required to operate at 1,000ft (305m), and during periods of high training activity circuit aircraft will pass over the Banjup Quarry site every few minutes. The majority of aircraft noise complaints received for Jandakot Airport are related to circuit training, particularly during the summer months where the weather is ideal for flying training and residents are more likely to be outdoors or have their windows open. JAH therefore requests that documentation relating to the purchase of land within the Banjup Quarry site refers purchasers to the Jandakot Airport Aircraft Noise information webpage (<a href="http://www.jandakotairport.com.au/community/aircraft-noise.html">http://www.jandakotairport.com.au/community/aircraft-noise.html</a>).</p>	<p>3. It is expected that a condition of any future subdivision will include the requirement for notification on titles.</p> <p>4. Supported. Providing information regarding flight movements to future purchasers is warranted.</p>



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		<p><a href="#">noise.html</a>) with an acknowledgement that the webpage information has been viewed and understood.</p> <p>5. Within the Jandakot Control Zone (3 nautical mile radius of the airport) the majority of aircraft will operate at 1,000ft (305m). It is not clear from the Noise Impact Assessment whether this has been taken into account, but given the incorrect assumption about the types of aircraft operating and the incorrect maximum noise level, the 'deemed-to-comply package for noise insulation specifications (Item 5 of the Noise Impact Assessment) needs to be reviewed. In addition, the deemed-to-comply package recommendation in the Noise Impact Assessment has only been applied for lots within the ANEF 20 zone. Given that the entire development site falls within the N60 Contour of 200 average daily noise impact events above 60 db.(A), JAH requests that adequate noise attenuation measures be applied to all lots within Banjup Quarry.</p> <p>6. Another main source of aircraft noise complaints are people who rent dwellings. Rental tenants would not be expected to be provided with the aircraft noise impact information on the land title, and accordingly, JAH requests that signs are installed at each entrance to the Banjup Quarry development, identifying that the area is an "Aircraft noise impact area".</p> <p>7. The National Airports Safeguarding Framework was developed through the National Airports Safeguarding Advisory Group (NASAG). NASAG has recommended a review of Australian Standard AS 2021, and pending finalisation of the Standards Australia review process, is recommending that the National Airports Safeguarding Framework be used as <i>"guidance or consideration that planners and decision makers should take into account in conjunction with other factors"</i>. Guideline A of the</p>	<p>5. It is noted that the 'N60' noise contours aren't used as a measure to inform land use planning. Instead the ANEF contours as they interrelate with SPP5.3 represent the current tools to guide strategic land use planning. In this regard, the City notes that the proposal complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is yet to be finalised by the WAPC (southwest corner of land). It is recommended that the City request that the WAPC consider the implications of the revised SPP5.3 in respect to the proposal as part of their endorsement of the Structure Plan. It will be at the WAPC's discretion whether they're prepared to endorse the Structure Plan prior to the finalisation of the revised SPP5.3. Notwithstanding this, it is also recommended that all sensitive development integrate appropriate noise amelioration standards as part of development AND an appropriate notification be placed on the title of all lots advising of this requirement to</p>

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		<p>Framework acknowledges that the <i>"20 ANEF and 25 ANEF zones do not capture all high noise affected areas around an airport, and AS2021 recognises that the ANEF contours are not necessarily an indicator of the full spread of noise impacts, particularly for residents newly exposed to aircraft noise"</i>. Guideline A further states that as part of the review of AS2021-2000, Standards Australia will consider the application of the following approach to land use planning:</p> <p><i>"17(i) There should be no new designations or zoning changes that would provide for noise sensitive developments within a 20 ANEF where that land was previously rural or for non-urban purposes. Zoning for noise-sensitive development should be avoided where ultimate capacity or long range noise modelling for the airport indicates either:</i></p> <ul style="list-style-type: none"> <li>- <i>20 or more daily events greater than 70 dB(A);</i></li> <li>- <i>50 or more daily events of greater than 65 dB(A);</i></li> </ul> <p><i>or</i></p> <ul style="list-style-type: none"> <li>- <i>100 events or more daily events of greater than 60 dB(A).</i></li> </ul> <p><i>17(ii) Zoning for noise-sensitive development should take into account likely night- time movements and their impact on residents 'sleeping patterns. Specifically, where there are more than 6 events predicted between the hours of 11pm to 6am which create a 60 dB(A) or greater noise impact, measures for aircraft noise amelioration and restriction on noise sensitive development would be appropriate"</i>.</p> <p>JAH notes that in regards to 17(i) of Appendix A as stated above, the long range noise modeling for Jandakot Airport (per the N60 Contours) indicates 200 daily events of greater than 60 dB(A). With regards to 17(ii), Jandakot</p>	<p>build to a high noise standard. Such approaches are used in respect of development adjoining major roads (Kwinana Freeway for example) and is considered appropriate in this case.</p> <p>6. Supported. The inclusion of signs to note the entry to an aircraft noise area has been used in the past and is supported for this proposal.</p> <p>7. The City notes that the proposal complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is yet to be finalised by the WAPC (southwest corner of land). It is recommended that the City request that the WAPC consider the implications of the revised SPP5.3 in respect to the proposal as part of their endorsement of the Structure Plan. It will be at the WAPC's discretion whether they're prepared to endorse the Structure Plan prior to the finalisation of the revised SPP5.3.</p>

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		<p>Airport operates 24 hours per day. Although no noise modeling has been conducted solely for night operations, JAH can confirm that there are more than 6 events between the hours of 11pm to 6pm which will likely create a 60 dB(A) or greater noise impact.</p> <p>Given the close proximity of the proposed development to Jandakot Airport and aircraft operating areas, it is imperative that adequate information about the aircraft noise impact is conveyed to potential residents.</p>	
25.	<p>Stefan de Haan, Department of Environment and Conservation PO Box 1167 Bentley Delivery Centre WA 6983</p> <p>LATE SUBMISSION</p>	<p>I refer to your correspondence dated 14 March 2013 in respect of the above. The Department of Environment and Conservation (DEC) has reviewed the referral information and provides the following comments.</p> <p><b>Noise from Jandakot Airport</b></p> <p>DEC's Noise Regulation Branch (NRB) has reviewed the report <i>Draft Local Structure Plan - Scheme Amendment 95 - Banjup Quarry</i>, prepared by Stockland WA Development Pty Ltd, with particular reference to the <i>Noise Impact Assessment – Banjup Local Structure Plan</i> prepared by Lloyd George Acoustics, dated 5 August 2012 (LG report)</p> <p>The main noise implication of the LSP is for residences and other noise-sensitive uses that will be subject to the noise impacts from aircraft operations at Jandakot Airport. NRB understands that the proponent are preparing a Health Risk Assessment (HRA) for the Banjup Quarry development in relation to noise impacts but this has not been made available to NRB.</p> <p>The subject land lies under the main southern circuit paths used by the majority of aircraft undertaking training at Jandakot</p>	

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		<p>Airport. This southern area is used in preference to the northern circuit in acknowledgement of the significant community opposition in that area to flight training. An examination of the N60 noise contours (number of events above 60dB(A)) for Jandakot Airport for 2029/30 (Page 105 of the <i>Jandakot Airport Master Plan</i> 2009 (Master Plan)) shows that the vast majority of training circuits will be to the south over the subject land, and that the whole of the subject land would receive an average of more than 200 overflights per day at levels above 60dB(A). This compares with some 50 per day in the area to the north of the airport.</p> <p>It should also be recognised that the contours were only plotted for up to 200 events per day, and that the actual numbers may be much higher, given that the airport plans to accommodate some 1,410 fixed wing movements per day by 2029/30 per average, plus 208 helicopter movements. It should also be noted that Master Plan contains what appears to be a discrepancy between the aircraft movements proposed in the Master Plan and those on which the ANEF was based. Section 5.3 of the Master Plan indicates 1,618 movements per day (total including helicopters), while the number in the Runway Utilisation Table in Figure 27 of the draft Master Plan, on which the 2029/30 ANEF was based, is only 1,042 per day total (a difference of 55%). The number of helicopter movements differs by a factor of three (208 per day in Section 5.3 versus 73 per day in the ANEF map). The numbers of movements used in Section 5.3 of the draft Master Plan are therefore significantly greater than those in the model on which the ANEF – and presumably the N60 - was based. The implication of this is that both the 20ANEF and the N60 contours may underestimate the extent of the ultimate noise impact areas.</p> <p>The noise impacts tend to be greatest during the day on weekends, when flight training is busiest, and when many residents are at home and using the recreation areas in and</p>	

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		<p>around the home. Noise insulation of the dwellings, while it would provide some amelioration for indoor areas during the evenings and early mornings, would not benefit outdoor areas. There is now a significant body of research data showing health impacts in terms of increased blood pressure, leading over time to increased incidence of heart disease and more recently stroke. These health outcomes appear to be evident from noise levels roughly equivalent to the 15ANEF level (the 15ANEF contour would enclose the whole of the subject land). Vulnerable groups such as children and the elderly may be at greater risk. Severe annoyance can also be considered as a health issue in terms of the overall wellbeing of affected persons. There is also strong evidence of adverse impacts on children's educational performance, particularly from the Munich Airport Study and the RANCH study in Europe.</p> <p><i>State Planning Policy 5.3: Land Use Planning in the Vicinity of Jandakot Airport</i> (SPP 5.3) addresses land use planning issues relating to noise from Jandakot Airport. It defines a Core Area as the area within the 20ANEF noise contour. In this area no re-zoning of land is to take place where the predominant type of development is likely to be housing. However DEC notes that the LSP proposes residential development on the subject land that is within the Core Area, and this would contravene SPP5.3.</p> <p>The LSP report in section 2. 1.6 proposes two arguments in support of residential development within the 20ANEF contour –</p> <p>1) While the 20ANEF contour in the draft revised version of SPP5.3 (2011) does intrude into the southern portion of the site, the 20ANEF contour in the original approved version of SPP5.3 (2006) did not intrude onto the subject site, thus the LSP is 'technically compliant' with respect to 20ANEF; and</p> <p>2) SPP5.3 is inconsistent with <i>AS2021:2000 Acoustics-Aircraft noise intrusion Building siting and construction</i> (AS2021 :2000),</p>	

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		<p>which describes the area within 20- 25ANEF as 'conditionally acceptable'. The Western Australian Planning Commission (WAPC) is aware of the contradiction and the adopted version will address this together with the results of the review of AS2021 :2000 that is about to start. The LG report similarly suggests that SPP5.3 'contradicts' AS2021 :2000, and outlines possible deemed-to-comply noise insulation measures for residences within the Core Area.</p> <p>With regard to the first point above, the LSP report notes that approving agencies require compliance with the revised 20ANEF contour. The 2009 Master Plan proposes a level of activity that has influenced the ANEF and this should be taken into account in the LSP.</p> <p>On the second point above, it is noted that the 'inconsistency' between SPP5.3 and AS2021 :2000 was a considered decision during the policy's development, recognising that the noise character of an airport like Jandakot results in a greater noise impact at the same ANEF level than that of a large airport where the aircraft are louder, but the noise duration is shorter. Thus, a noise level at 20ANEF at Jandakot Airport should be seen as less likely to be acceptable than the same noise level from Perth Airport. This is borne out by the adverse community reaction to noise in suburbs around Jandakot Airport where noise levels are below 20ANEF but numbers of noise events are significant. It is also recognised in the (then) Federal Department of Transport and Regional Services document <i>Going Beyond Noise Contours - Local Approaches to Land Use Planning Around Smaller Australian Airports - Discussion Paper</i> (2003), which acknowledges the limitations of the ANEF system and proposes alternative approaches similar to the Core Area and Frame Area approach used in SPP5.3.</p> <p>The LSP also notes that AS2021 :2000 is under review. NRB have been involved in the work leading up to the review and can</p>	

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		<p>advise that the review is likely to consider noise criteria for regional and general aviation airports as a separate issue to noise criteria for major airports. It is unlikely that the revised AS2021 :2000 will endorse the notion of a 'conditionally acceptable' area within 20-25ANEF for regional and general aviation airports. It is the expectation of DEC that the draft SPP5.3 would be aligned with the revised AS2021 :2000 regarding regional and general aviation airports. DEC does not support the arguments in the LSP that the area within the 20-25ANEF will be 'conditionally acceptable'.</p> <p>Given that SPP5.3 recognises the differences in noise impacts relating to a general aviation airports with large numbers of movements, compared with a large airport with lesser number of noisier movements, it is clear that the Building Site Acceptability table in AS2021 should not be used in this case. It should be recognised that AS2021 is only a guide, and that SPP5.3 predominates.</p> <p>The LG report proposes noise insulation measures for residences in the area within the 20ANEF contour. In this regard, the LG report provides misleading information in regard to aircraft noise levels, suggesting that the maximum noise level may typically be 58dB(A) within the 20ANEF contour, from calculations based on the prediction method in AS2021 . The calculation however seems to ignore the curvature of the main take-off flight path for the circuit to the south-west turning to the south-east over the subject land. A calculation including the curvature of the flight path would predict a maximum noise level of about 70-72dB(A) for a twin-engine Beech 58 Baron aircraft, for receivers within about 250m of the flight path. With several hundred events per day at these noise levels, the outdoor areas are considered unacceptable, and this supports the basis of SPP5.3 that the area should not be developed for residential purposes. In this light, a discussion about noise insulation to protect indoor areas is not relevant, as the outdoor areas will</p>	

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		<p>remain at unacceptable levels. Thus noise insulation should not be seen as a remedy, and the area within 20ANEF should be considered unacceptable for residential development.</p> <p><b>1. Recommendation: That the area within the 20ANEF contour should not be developed for residential purposes, but be designated for alternative, less sensitive land uses in the LSP.</b></p> <p>Beyond the Core Area, SPP5.3 defines a Frame Area as the land between the 20ANEF contour and bounded by Roe Highway, Ranford Road, Warton Road, Armadale Road and Kwinana Freeway. The whole of the subject land is thus within the Frame Area (except where it is within the 20ANEF contour). While there is a general presumption against any re-zoning within the Frame Area which would permit development involving an increase in residential density above one dwelling every 2ha, the amendment to the Metropolitan Region Scheme in 2012 allows urban development. With this in mind, this response to the draft LSP seeks to ensure that the most sensitive land uses are located in the least noisy areas, and that adequate noise amelioration measures are identified for incorporation into the development.</p> <p>The land uses considered most sensitive in the context of this development would be the Primary School and Retirement Living Village, followed by general housing, caravan parks, churches and passive recreation. Uses such as short term accommodation, active recreation and the like may be considered less noise-sensitive in this context. The least affected area of the subject land is the land to the north of the power line easement along Dollier Road, except for the area near the 20ANEF contour near the corner of Dollier and Solomon Roads. The location of the Primary School as shown in the LSP map is therefore in about the most optimum position. DEC understands a Retirement Living Village may be located to</p>	<p>1. Noted but not supported. It is noted that the 'N60' noise contours aren't used as a measure to inform land use planning. Instead the ANEF contours as they interrelate with SPP5.3 represent the current tools to guide strategic land use planning. In this regard, the City notes that the proposal complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is yet to be finalised by the WAPC (southwest corner of land). It is recommended that the City request that the WAPC consider the implications of the revised SPP5.3 in respect to the proposal as part of their endorsement of the Structure Plan. It will be at the WAPC's discretion whether they're prepared to endorse the Structure Plan prior to the finalisation of the revised SPP5.3. Notwithstanding this, it is also recommended that all sensitive development integrate appropriate noise amelioration standards as part of development AND an appropriate notification be placed on the title of all lots advising of this requirement to</p>



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		<p>the west of the central area, however DEC would be concerned that it not be in the south-west corner near the 20ANEF contour and the industrial area.</p> <p>Further, using a predicted noise level of 70dB(A) for a Beech 58 Baron as a guide, there would be an area roughly within 250m of the 20ANEF contour where the more noise sensitive uses should be avoided where practicable, and some of the less-sensitive uses should be considered.</p> <p><b>2. Recommendation: That consideration be given to siting any aged care or retirement living within the 'less-affected' area near the primary school site, and that other noise-sensitive land uses be located further than 250m from the 20ANEF contour where practicable.</b></p> <p>It is likely that areas within about 250m of the 20ANEF contour will experience aircraft noise levels above 70dB(A), thus a degree of noise insulation would be needed to achieve the indoor noise level of 50dB(A) recommended in AS2021 for sleeping areas and dedicated lounges. In this regard, the deemed-to-comply specification in the LG report would be adequate.</p> <p><b>3. Recommendation: That noise-sensitive buildings within 250m of the 20ANEF contour in the Frame Area be provided with noise insulation equivalent to that specified in Section 5 of the LG report.</b></p> <p>In addition to the above, notification on title and provisions of adequate information to prospective purchasers are matters that need to be included at the appropriate stage. SPP5.3 does not require a notification on title as a condition of subdivision or planning approval</p>	<p>build to a high noise standard. Such approaches are used in respect of development adjoining major roads (Kwinana Freeway for example) and is considered appropriate in this case.</p> <p>To be clear to the DEC based on the current SPP5.3, development associated with the Proposed Structure Plan is indicated as 'conditionally acceptable' within the frame area of the SPP. This point does not appear to be acknowledged or discussed by the DEC. The DEC's comments will be sent to the WAPC with the Proposed Structure Plan, to ensure that their views are considered by the WAPC as part of final adoption. It is noted that the interim basis of the draft SPP5.3 will also be an important issue to consider in respect of how the ANEF20 portion of the land in the southwest corner is managed.</p> <p>2. The ANEF contours as they interrelate with SPP5.3 represent the current tools to guide strategic land use planning. In this regard, the City notes that the proposal complies with the current SPP5.3. However there are areas which do not satisfy the requirements of the draft revision to SPP5.3 which is</p>

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		<p><b>4. Recommendation: That provision be made to require notifications be placed on all relevant titles to ensure that purchasers are aware of the likely noise impacts.</b></p> <p><b>5. Recommendation: That a thorough information program be required to be implemented for prospective purchasers, based on numbers of aircraft movements occurring now and likely to occur in the future over the Frame Area.</b></p> <p><b>6. Recommendation: That, prior to confirmation of the LSP, the following information be sought by council and subjected to review</b></p> <p>1. Detailed noise contours for Jandakot Airport, in particular N60 contours for a wide range of numbers of movements - this information will guide placement of the most sensitive land uses and provide a basis for a thorough community information program about aircraft noise; and</p> <p>2. The Health Risk Assessment</p> <p><b>Industrial noise</b></p> <p>There is an existing industrial area adjacent to the south-western corner of the site, to the south of Dallier Road, thus the assigned levels in the Environmental Protection (Noise) Regulations 1997 (noise regulations) will apply to noise emissions from this area when received at new residential premises in the development area. The LG report assumes this land to be 'industrial' for the purposes of calculation of 'influencing factor' which sets the assigned level for noise emissions - in this case the LG report calculates an influencing factor of 5dB for the nearest residences.</p> <p>This calculation however fails to recognise the potential effect of noise regulation 10 on the new residences near the industrial area. Regulation 10 provides in effect that, where the zoning of</p>	<p>yet to be finalised by the WAPC (southwest corner of land). The proposed aged care / retirement living site is located outside the 20ANEF contour indicated by the draft revision to SPP5.3. As an additional point, it is recommended that all sensitive development integrate appropriate noise amelioration standards as part of development AND an appropriate notification be placed on the title of all lots advising of this requirement to build to a high noise standard. This deal with this request.</p> <p>3. Supported. As per 2 above.</p> <p>4. Supported. As per 2 above.</p> <p>5. Supported. This will form part of the requirements of the Structure Plan.</p> <p>6. Not supported. The 'N60' noise contours aren't used as a measure to inform land use planning. Instead the ANEF contours as they interrelate with SPP5.3 represent the current tools to guide strategic land use planning. In this regard, the City notes that the proposal complies with the current SPP5.3.</p>

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		<p>land changes from an industrial use to a noise-sensitive use, a nearby noise emitter may emit an amount of noise that he would have been allowed to emit if a residence had been on the land <i>when it was still zoned industrial</i>. This means that the land on which the new residences are located would need to be considered as 'industrial' in the calculation of influencing factor, resulting in a significant increase in the assigned levels. (The process is that the noise emitter receives acknowledgement from the EPA that the rezoning has taken place.)</p> <p>The implication for residences near the industrial area would be that the assigned levels may conceivably be up to about 54dB(A) at night and 64dB(A) during the day. The provision in regulation 10 thus serves to allow a noise emitter to emit a level of noise that may well be unacceptable to a residential receiver; and as a result the remedy is usually to allow a buffer or transition zone, to keep residences at a reasonable distance from the industry. The implications of noise regulation 10 thus need to be carefully considered in this case.</p> <p>It would appear that the development area has changed from Resource, i.e. Industrial under the noise regulations, to Urban under the MRS amendment of 2012. Under the proposed TPS 3 amendment the development area appears to change from Resource and Business to Residential. If this is correct, the provisions of regulation 10 would be available to noise emitters in the industrial area.</p> <p>DEC also notes that TPS 3 shows the industrial area to the south of Dallier Road as Residential. In this case, the remaining industries would still be able to use regulation 10 to have their own land considered as Industrial land in the calculation of influencing factor for any new residences. In this case a staged approach would be needed, in order to keep the new residential at a suitable distance from the remaining industries until they had relocated. DEC notes that the staging plan in the LSP</p>	

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		<p>proposes the southern areas as the first stages, and would see this as an issue in the light of the implications of noise regulation 10.</p> <p>The possible further implications of regulation 10 in terms of noise emissions from other adjacent lands to the east and north and north-west of the development area also need to be considered.</p> <p>The potential implications of noise regulation 10 should therefore have been identified and addressed in the LG report and in the LSP, and these implications need to be addressed thoroughly before the LSP is approved. Should regulation 10 be found to be applicable, its application would render much of the area identified above as being affected by aircraft noise as unsuitable for residential development because of the additional impact of industrial noise.</p> <p><b>7. Recommendation: Detailed consideration be given to the potential implications of noise regulation 10 for the development area.</b></p> <p><b>Road traffic noise</b> The application of SPP5.4 in relation to road traffic noise in the development area has been addressed adequately in the LG report. DEC notes that Detailed Area Plans are to be prepared for various areas, and would expect that noise amelioration measures for traffic noise can be incorporated at that stage.</p> <p><b>Contaminated sites</b> Lots 1 &amp; 9004 Armadale Road, Lot 9002 Jandakot Road and Lot 132 Fraser Road, Banjup have not been reported to DEC as known or suspected contaminated sites under the <i>Contaminated Sites Act 2003</i>.</p> <p>A brief review of cadastral information for this area indicates that</p>	<p>7. Not supported. The land zoning has not changed from industrial to residential as purported by the DEC. The Resource zoning is not an industrial zoning, and permits for noise sensitive development to take place currently. Therefore Regulation 10 does not apply. The Proposed Structure Plan is cognisant of the need to ensure that development adjoining the existing commercial / light and service industry area to the west is</p>

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		<p>the site is situated within a priority two area of the Jandakot Underground Pollution Control Area. Furthermore, DEC's Contaminated Sites Branch (CSB) notes that the western boundary of Lot 9002 Jandakot Road and the south-western boundary of Lot 1 Armadale Road are intercepted by a Wellhead Protection Zone.</p> <p>Following a review of the <i>Preliminary Site Investigation for Contamination, Cemex Site, Banjup</i> (RPS, June 2010) (PSI), it is understood that the site was historically used for sand quarrying for approximately 28 years, from 1980 to 2008. In addition, CSB notes that the site has, during its history, contained various other forms of infrastructure such as a fuel storage area, electrical substations, sedimentation ponds and a waste bund. These are land uses that have the potential to cause contamination, as specified in the guideline <i>Potentially Contaminating Activities, Industries and Land uses</i> (Department of Environment, 2004). Furthermore, the PSI documents evidence of asbestos-containing material in surface soils at multiple locations across the site, which is understood to have remained unmanaged since its identification in March 2010.</p> <p>CSB understands that in order to facilitate the development of the site for residential use, it is necessary to rezone the land from its current zoning of Resource to Development under the Town Planning Scheme. CSB therefore considers this proposal to be an interim step in the planning process and as such does not recommend that any contamination conditions be imposed at this stage. However, given the proposed residential use of the site and its former use and associated site infrastructure, DEC will be recommending that 'contamination conditions' (such as condition EN9 and advice ENa2, as published in <i>Model Subdivision Conditions Schedule</i> (Department of Planning and Western Australian Planning Commission, October 2012)) be placed on future Western Australian Planning Commission (WAPC) residential subdivision applications and/or local</p>	<p>based upon appropriate interface as well as quiet house design. This underpins the strip of non-residential land to the north of Dollier Road, as well as the compact urban form adjoining the back of existing warehousing along the common boundary. However, this does not remove the requirement for existing businesses to comply with noise regulations.</p>

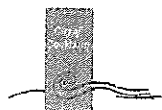
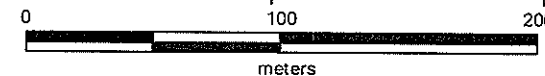
NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>government development approval applications in order to determine the contamination status of the site.</p> <p><b>Declared rare flora</b>  <i>Caladenia huegelii</i> is listed as declared rare flora (ORF) under the State <i>Wildlife Conservation Act 1950</i>. It is also listed in the Schedule to the Commonwealth <i>Environment Protection and Biodiversity Conservation Act 1999</i> as 'Endangered'.</p> <p>DEC supports the nomination of a number of the public open space (POS) areas as 'bushland' reserves within the structure plan area including areas where <i>Caladenia huegelii</i> has been recorded in vegetation. The POS areas A1 and B1 have been identified as Neighbourhood Passive POS including the area adjacent to the Resource Enhancement Wetland (UFI 6881). These areas have been mapped in the <i>Environmental Assessment Report</i> (RPS, 2011) as being in good to very good condition. The <i>Landscaping Strategies and Concepts</i> (Emerge, 2013) document states that existing vegetation will be retained and protected where possible. DEC supports the retention of native vegetation and requests that any facilities in these areas are located to minimise disturbance of this vegetation. DEC notes that the retention of native vegetation in POS shall be addressed within the wetland management plan and vegetation management plan that will be prepared prior to subdivision, as outlined in the <i>Environmental Assessment Report</i> (RPS 2011).</p> <p><b>Summary recommendation</b>  DEC recommends the LSP is revised to address the noise amenity and health issues, in relation to aircraft noise and industrial noise, as outlined above. While DEC understands that the City of Cockburn may proceed with the rezoning of the development area, the LSP requires revision. DEC's advice is that the LSP should not approved until the Health Risk Assessment has been completed and reviewed, and the above recommendations addressed.</p>	

NO.	NAME/ADDRESS	SUBMISSION	COUNCIL'S RECOMMENDATION
		<p>Thank you for the opportunity to comment on this development application and please contact Cho Lamb at DEC's Swan Region office on 9423 2982 or by email on <a href="mailto:cho.lamb@dec.wa.gov.au">cho.lamb@dec.wa.gov.au</a> if you have any queries regarding this advice.</p>	

WATTLEUP ROAD

00-128-24  
0-7322 1/2  
200  
No. 2221

## Subject Site



**City of Cockburn**  
G.LS Services Department

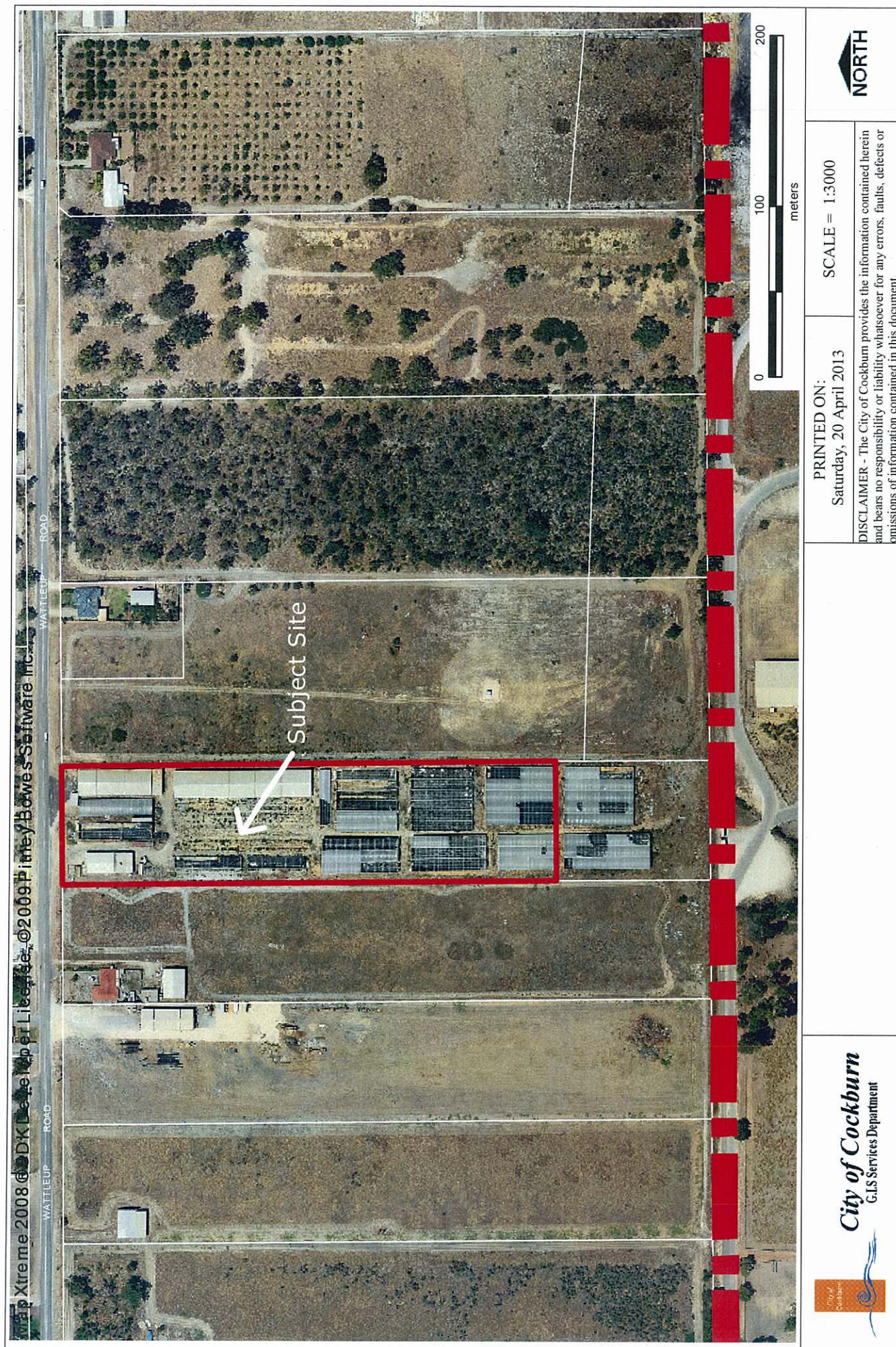
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Saturday, 20 April 2013

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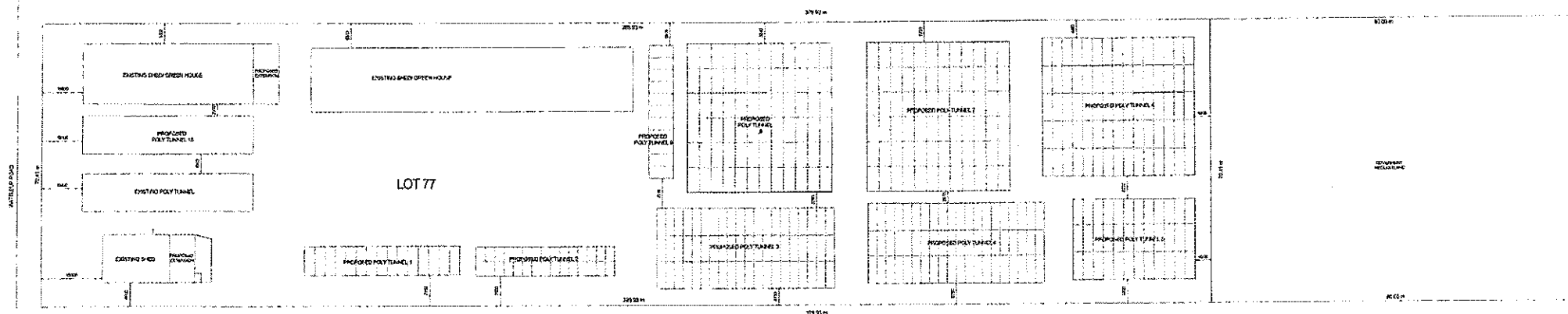
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**A SITE PLAN**  
SCALE 1/1500

REVISION	REV	BY	DATE	DESCRIPTION

### EXISTING GREEN HOUSE

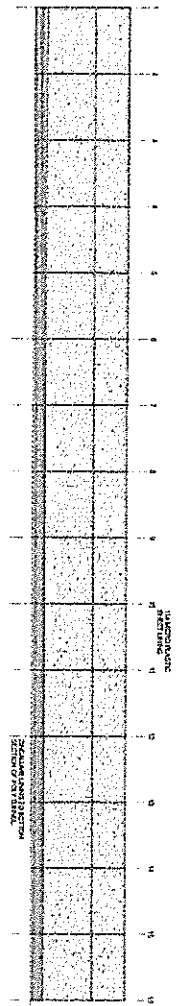
**CLIENT:** Mr VAN THANG NGUYEN.  
**PROJECT:** LOT 77 ( #365 ) WATTLEUP ROAD  
HAMMON PARK



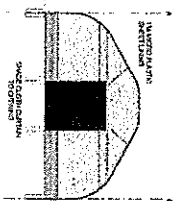
**DESIGN & DRAFTING SERVICE**  
107A WATERLOO STREET - TUART HILL, 6060. Ph 9201 1116 Fax 9443 2049  
Email: jctdrafting@vape.net.au

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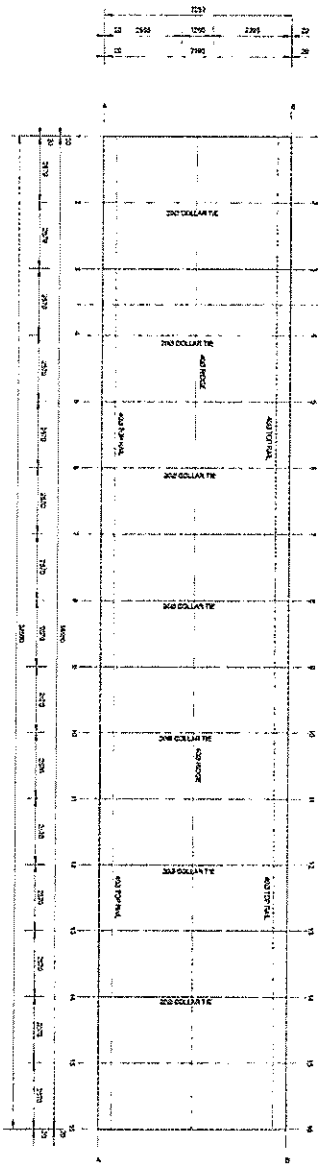
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DRAWING NUMBER	DATE
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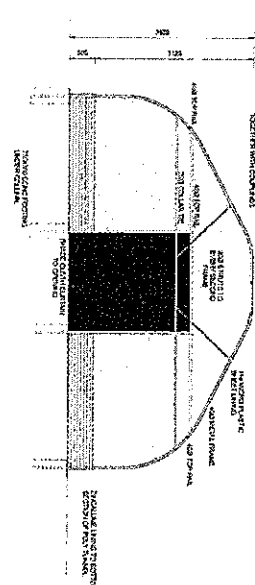
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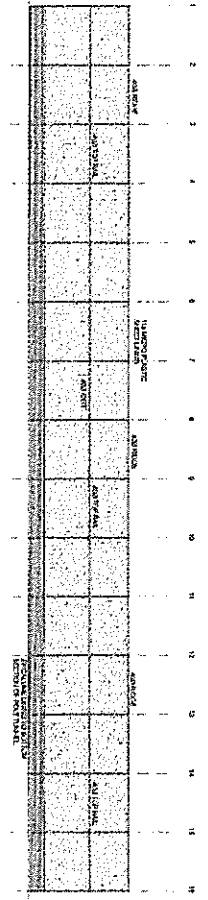


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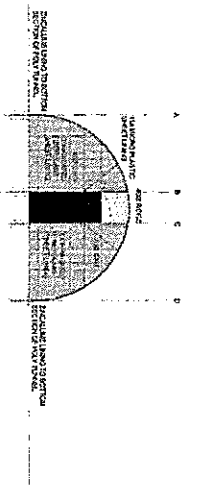


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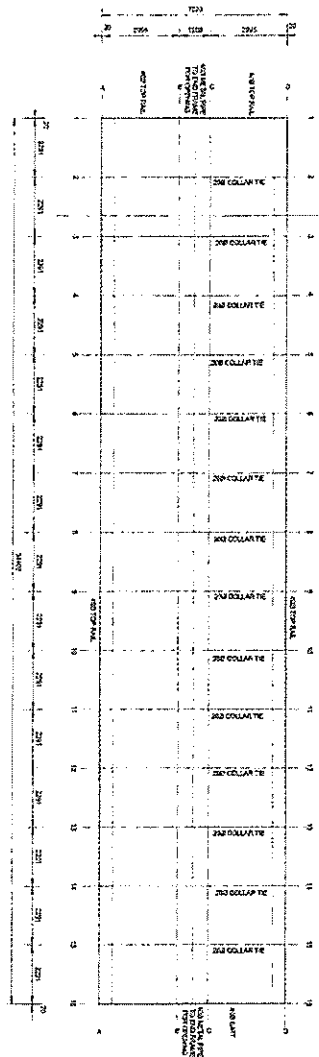
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REV. BY: DATE: DESCRIPTION:	PROJECT: LOT 77 (# 565) WAITTLEUP ROAD HANNOON PARK			



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**C. GREEN HOUSE 2 FLOOR PLAN**  
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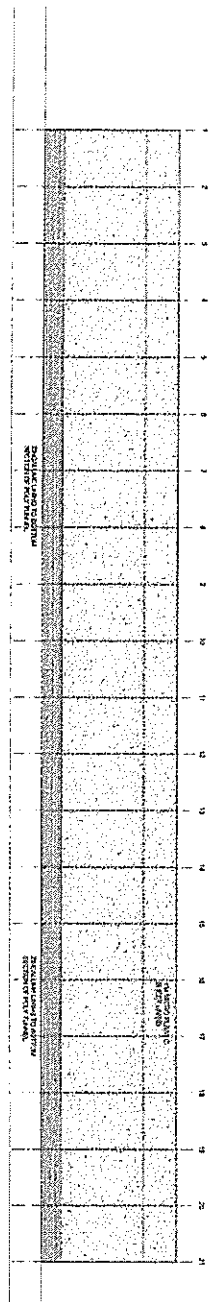
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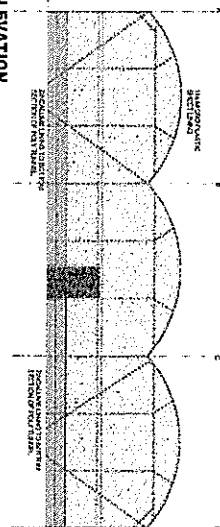
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**PROJECT: LOT 77 (4.56) WATTELOP ROAD**  
**HAMMON PARK**

**DESIGN & DRAFTING SERVICE**  
107A WATERLOO STREET - TUAT HILL, 606A  
Ph: 9201 1116 Fax: 9443 2040  
Email: jcdrafting@pcp.vn

**SCALE: AS SHOWN**  
**DATE: 04/12/2014**  
**DRAWN BY: JET**  
**CHECKED BY: JET**



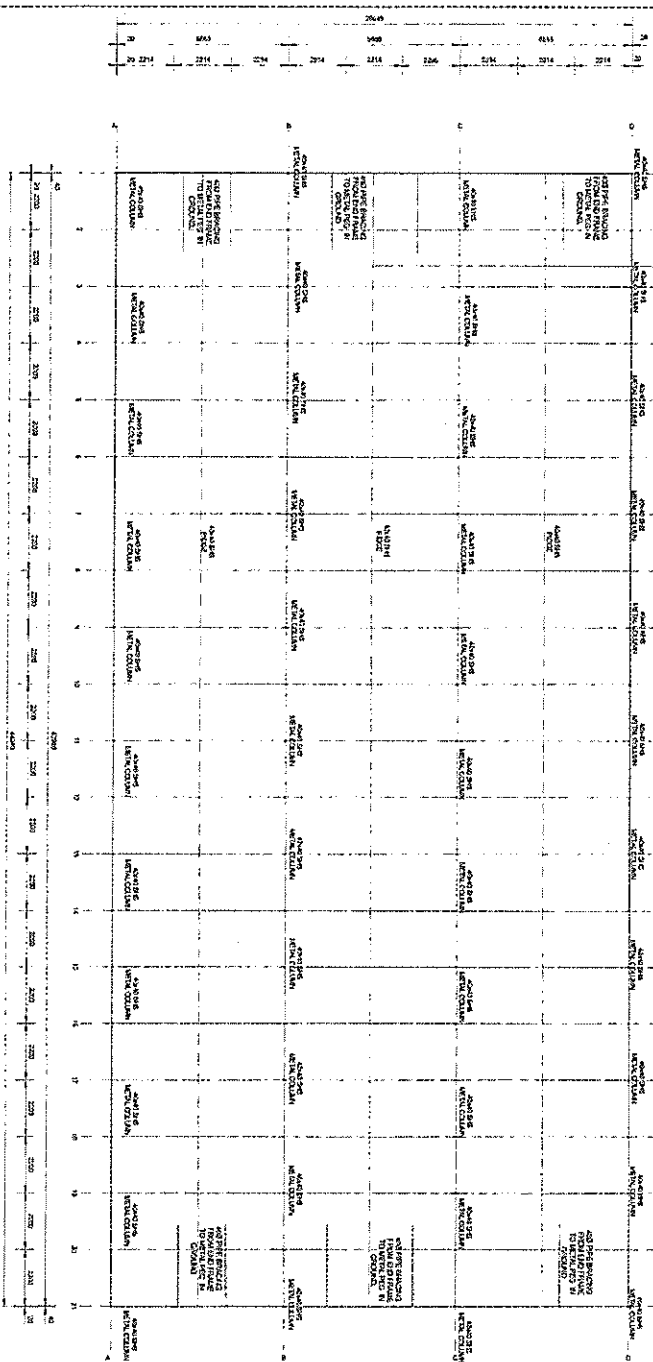
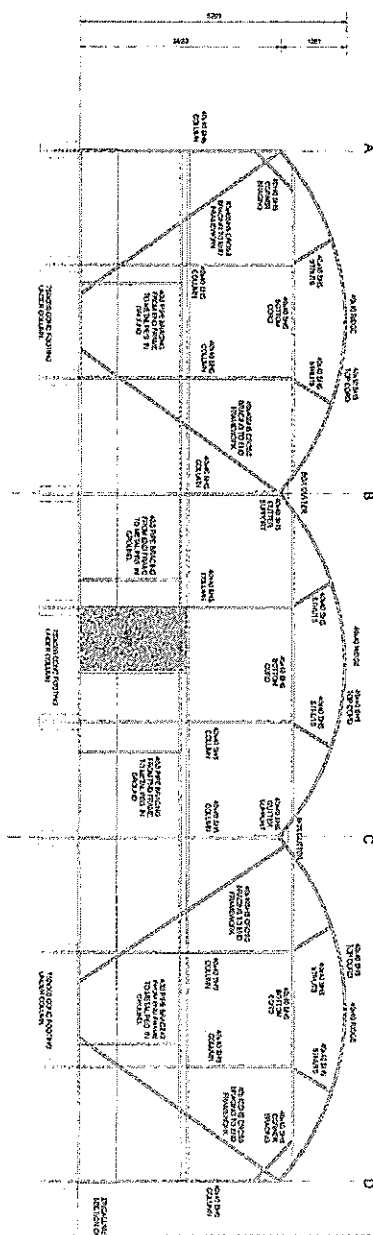
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**D3 ELEVATION**  
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**D5 ELEVATION**  
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<b>GREEN HOUSE 3 FLOOR PLAN</b> SCALE 1/100		<b>EXISTING GREEN HOUSE</b> AS SHOWN IN EXISTING		<b>CLIENT: M. VAN THANG NGUYEN</b> PROJECT: LOT 77 (# 86) WATTEUP ROAD HANAMON PARK		<b>DESIGN &amp; DRAFTING SERVICE</b> 101A WATERLOO STREET - TURTLE HILL, 680 Ph 9201 1116 Fax 9443 2040 Email: jeda@jeda.com.vn		<b>SCALE: AS SHOWN IN PLAN</b> DATE: 04/12/2014 DRAWN BY: JEDA CHECKED BY: E. TRIEN DATE: 04/12/2014	
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**E1 ELEVATION**  
5  
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**E3 ELEVATION**  
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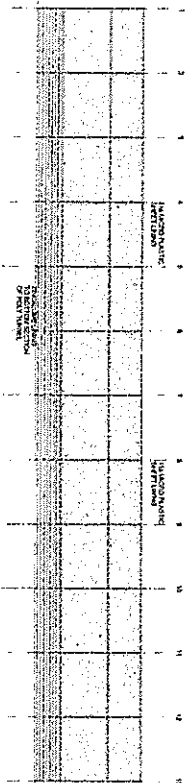
## EXISTING GREENHOUSE

**CLIENT:** MR VAN THANG NGUYEN,  
**PROJECT:** LOT 77 (#365) WATTHEUP ROAD

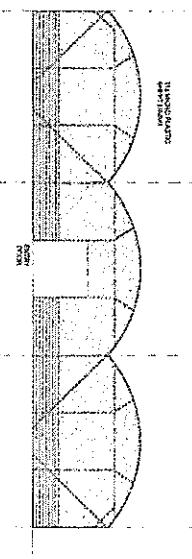
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**DESIGN & DRAFTING SERVICE**  
1024 WATTS RD. CO. ST. JOHN, TN 37057  
PH 9201 1116 FAX 9443 2049

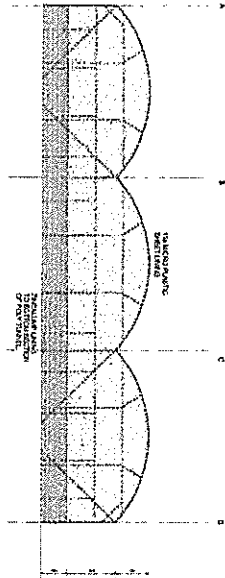
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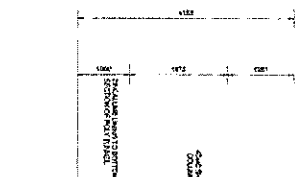
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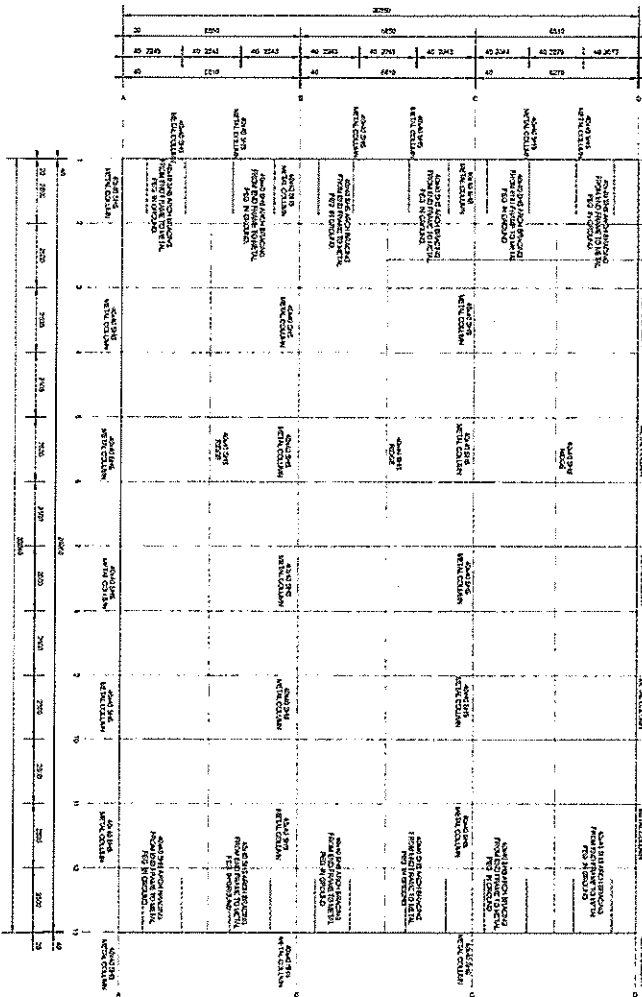
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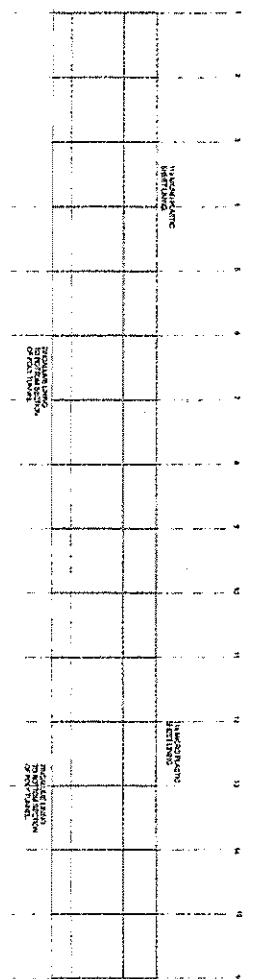
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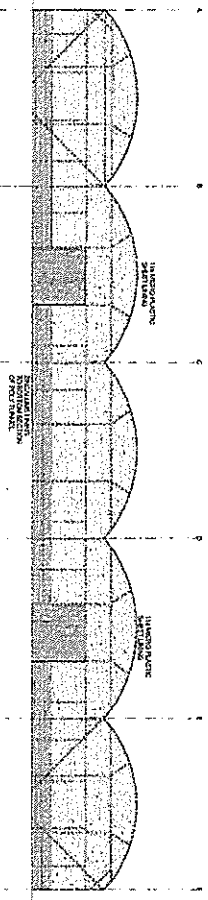
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SCALE 1/150



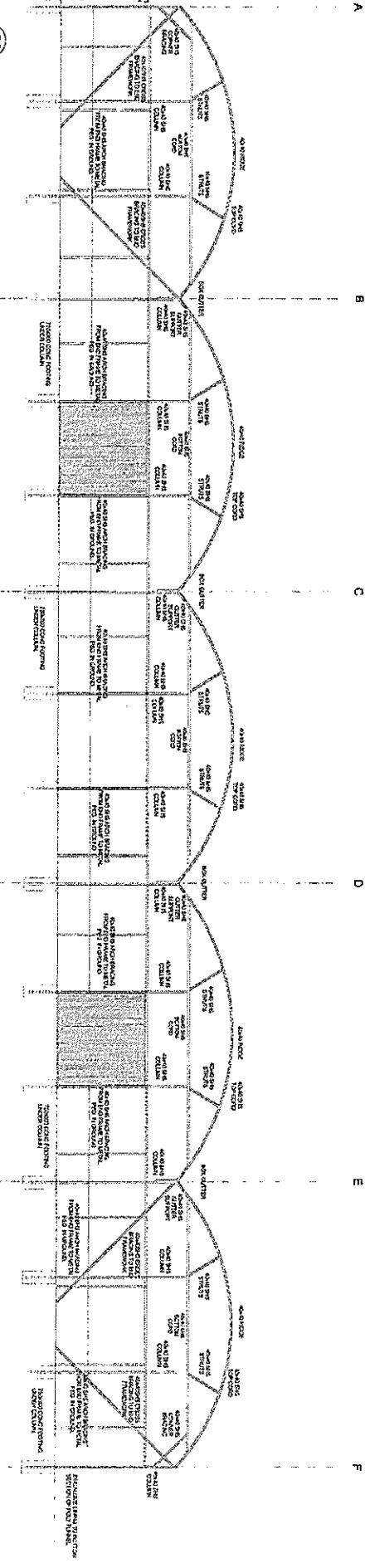
<b>GREEN HOUSE 5 FLOOR PLAN</b> SCALE 1/150		<b>EXISTING GREEN HOUSE</b>		<b>CLIENT: MR. VAN THANG NGUYEN</b> PROJECT: LOT 77 (#365) WATLEUP ROAD HAMMON PARK	
DRAWN BY: DATE: DESCRIPTION:	<b>DESIGN &amp; DRAFTING SERVICE</b> 100A WATERLOO STREET - TART HILL, ONT. Ph: 920 1116 Fax: 943 2340 Email: jendrafting@gmail.com				
SCALE: AS SHOWN SHEET: 5 OF 4 DRAWING NUMBER: JET-1210005 DATE: CHECKED BY: E. THERIAULT APPROVED BY:					



G1 ELEVATION  
8 SCALE 1/16"



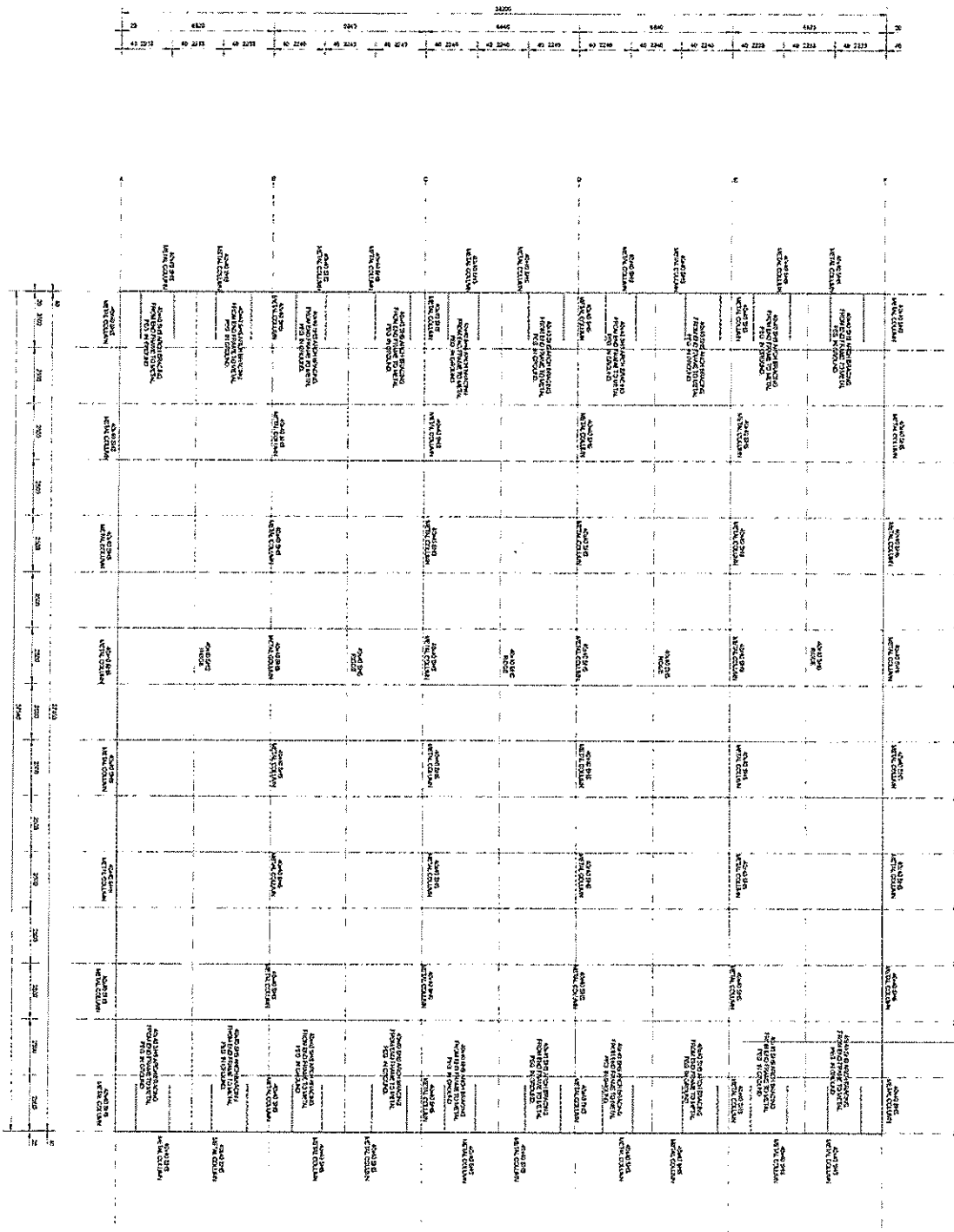
G2 ELEVATION  
8 SCALE 1/16"



G3 SECTION  
8 SCALE 1/16"

REV	BY	DATE	DESCRIPTION	CLIENT: MR. VAN THANG NGUYEN PROJECT: LOT 77 (# 365) WATTEUP ROAD HANAMON PARK	DESIGN & DRAFTING SERVICE 107A WATTEUP STREET - THART HILL, 6880 Ph: 920 1116 Fax: 943 2040 Email: jcdrafting@shaw.ca	SCALE: AS SHOWN SHEET: 06 OF 14 DATE: 01/12/2014 REV: 1
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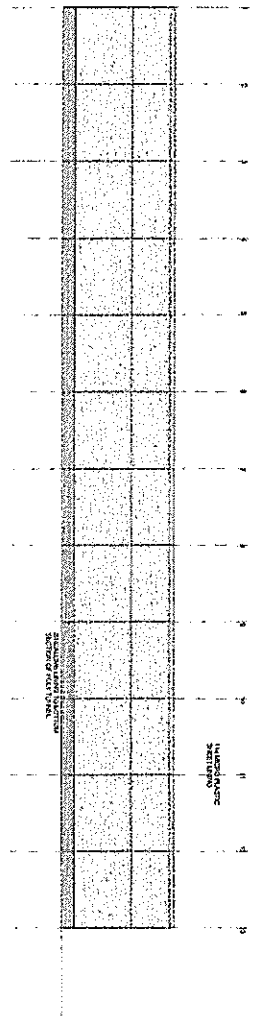




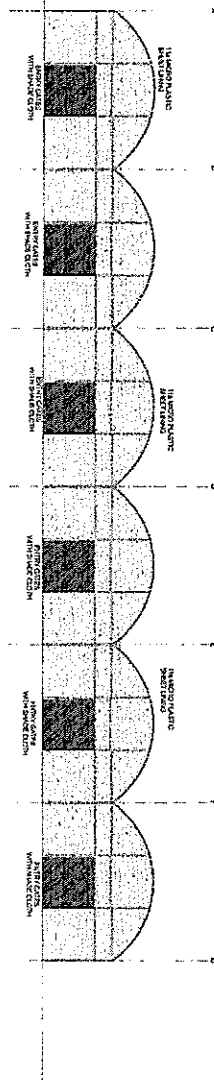
6 GREEN HOUSE 6 FLOOR PLAN  
SCALE 1:100

REV	DATE	DESCRIPTION
1	01/12/2014	EXISTING GREEN HOUSE
2	01/12/2014	CLIENT: M. VAN THANG NGUYEN PROJECT: LOT 77 ( # 36 ) WATTEUP ROAD HANAMON PARK
3	01/12/2014	SET DESIGN & DRAFTING SERVICE 101A WATTEUP ROAD - TIENT HILL 600 PH 021 1116 Fax 0445 2000 Email: jett@jett.com.vn
4	01/12/2014	SCALE: AS SHOWN DRAWN: NGUYEN CHECKED: NGUYEN DATE: 01/12/2014 APP: NGUYEN

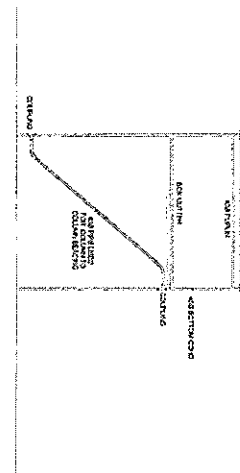
**H1 ELEVATION**  
10 SCALE 1:50



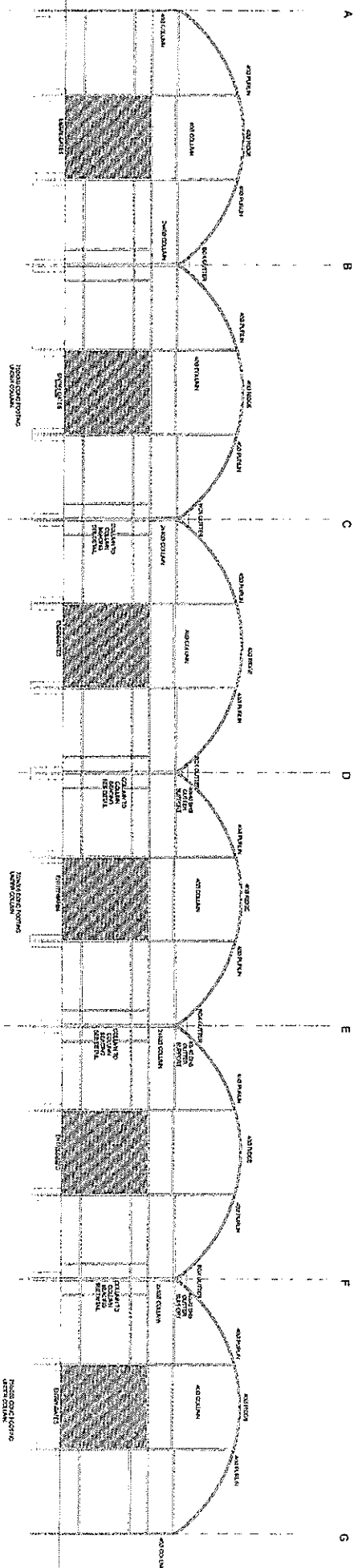
**H2 ELEVATION**  
10 SCALE 1:50

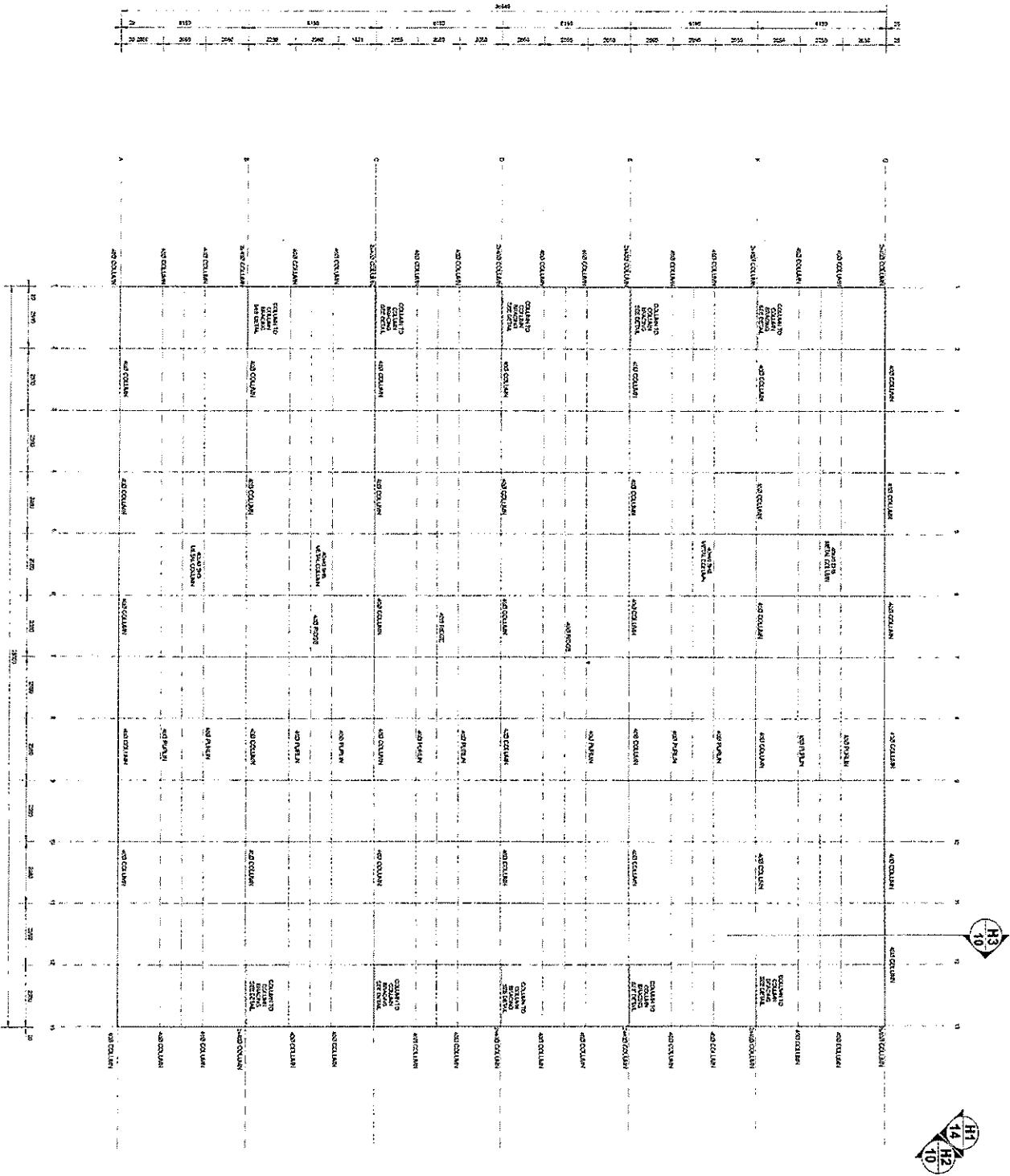


**G1 BRACING DETAIL**  
8 SCALE 1:50



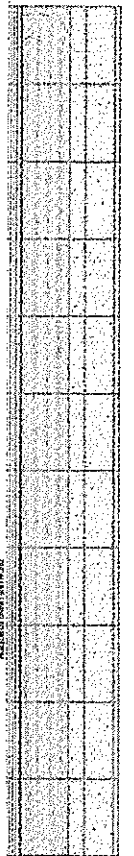
**H3 SECTION**  
10 SCALE 1:5





**H GREEN HOUSE 7 AND 8 FLOOR PLAN**  
SCALE 1:100

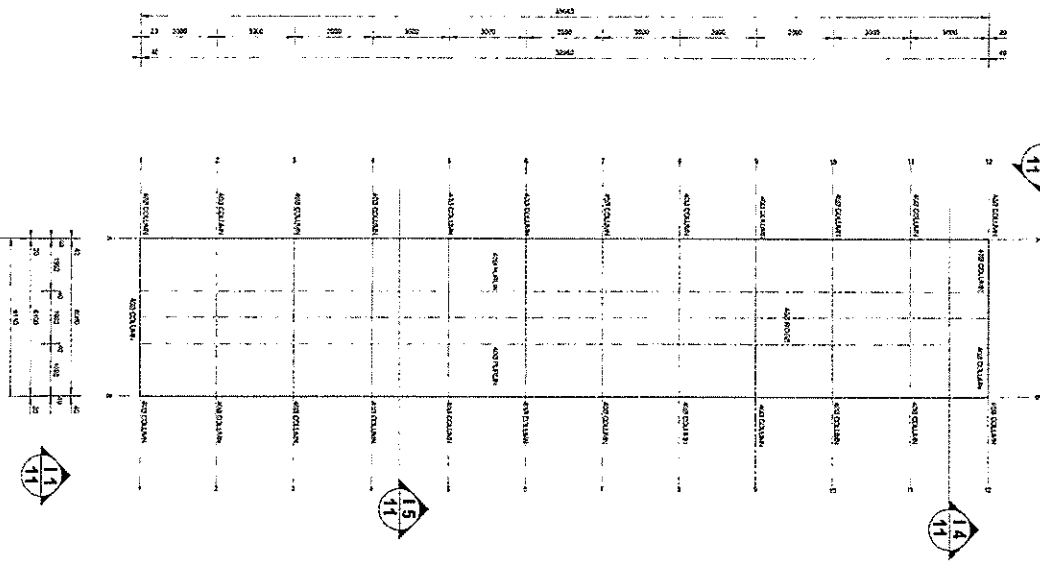
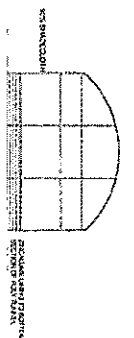
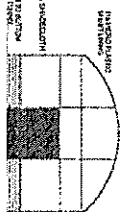
REV	BY	DATE	DESCRIPTION
<b>EXISTING GREEN HOUSE</b>			
<b>CLIENT: MR. VAN THIANG NGUYEN</b> <b>PROJECT: LOT 77 (# 365) WAT TIEUP ROAD</b> <b>HAMMON PARK</b>			
 <b>DESIGN &amp; DRAFTING SERVICE</b> <b>77A WATERLOO STREET - THART HILL, ONT.</b> Ph: 920 1116 Fax: 945 2140 Email: jendrafting@jet-drafting.com			
SHEET: AS SHOWN OF: 1 DATE: 04/12/2014 DRAWN BY: [Signature] CHECKED BY: [Signature] APPR FOR: [Signature]			



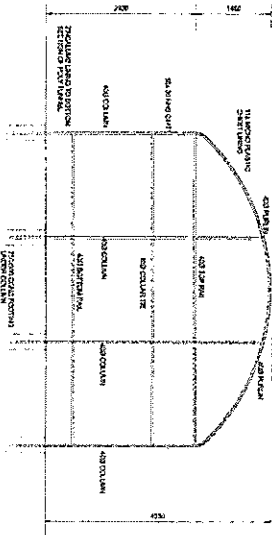
**11 ELEVATION**  
1/17 SCALE 1/100

**12 ELEVATION**  
1/17 SCALE 1/100

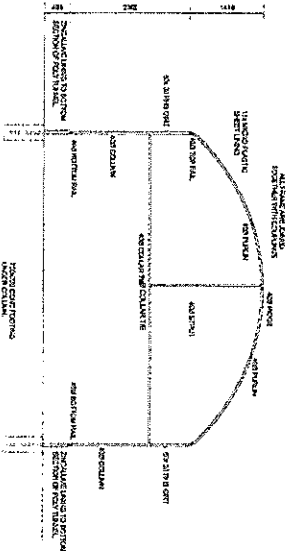
**13 ELEVATION**  
1/17 SCALE 1/100



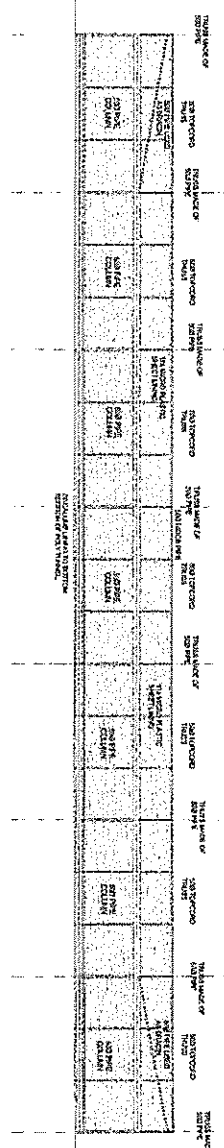
**14 SECTION**  
1/17 SCALE 1/10



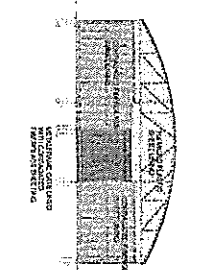
**15 SECTION**  
1/17 SCALE 1/10



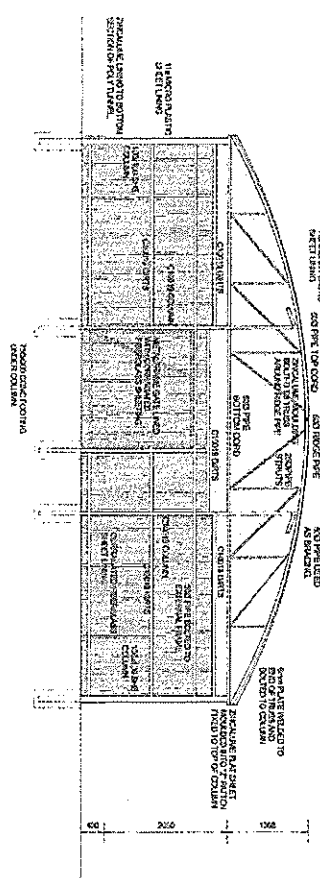
**1 GREEN HOUSE 9 FLOOR PLAN**  
1/17 SCALE 1/100



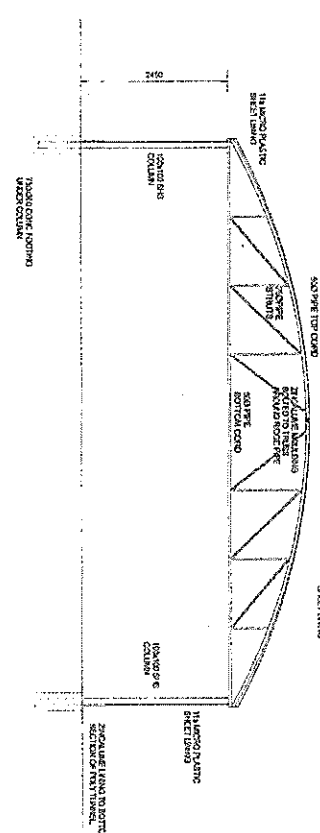
**11 ELEVATION**  
SCALE 1:100



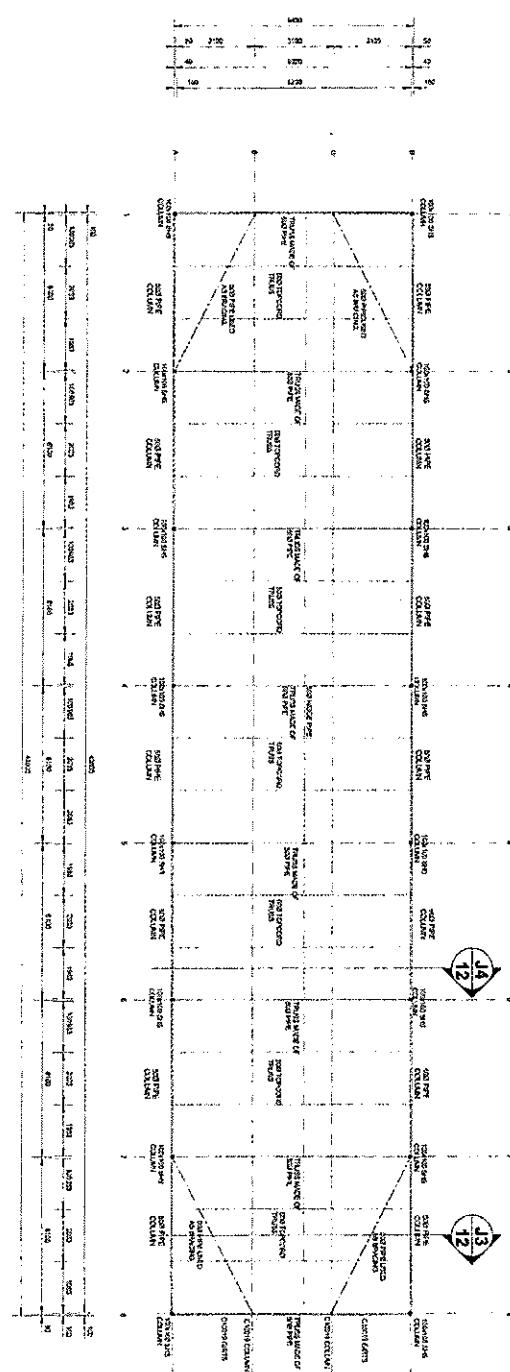
**12 ELEVATION**  
SCALE 1:100



**13 SECTION**  
SCALE 1:50



**14 SECTION**  
SCALE 1:50



**12 EXISTING SHED/ GREENHOUSE FLOOR PLAN**  
SCALE 1:100

NO.	REV.	DATE	DESCRIPTION

**EXISTING GREENHOUSE**

CLIENT: MR. VAN THANG NGUYEN  
PROJECT: LOT 77 / # 365 WATLEUP ROAD  
HAMMOND PARK

**DESIGN & DRAFTING SERVICE**  
171A WATERLOO STREET, TORRILL HILL, 6000  
Ph: 9207 1116 Fax: 9443 2940  
Email: info@design-drafting.com.au

SCALE: 1:100  
SHEET: 12 OF 14  
DATE: 12/12/2014  
DRAWN BY: J. THOMAS  
CHECKED BY: J. THOMAS  
APP. FOR: J. THOMAS

EXISTING SHED GREEN HOUSE

EXISTING SHED GREEN HOUSE

K1 ELEVATION  
13 SCALE 1/16

K2 ELEVATION  
13 SCALE 1/16

K3 ELEVATION  
13 SCALE 1/16

K EXISTING SHED/ GREENHOUSE FLOOR PLAN  
13 SCALE 1/16

EXISTING GREEN HOUSE

CLIENT: M. VAN THANG NGUYEN  
PROJECT: LOT 77 (#365) WATTELOUP ROAD  
HANHON PARK

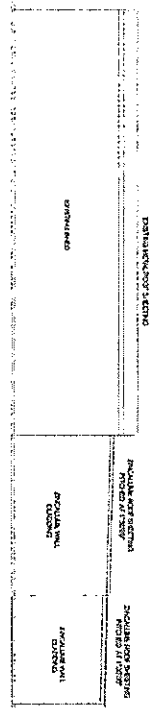
DESIGN & DRAFTING SERVICE

100A WATERLOO STREET - THART HILL, 660

Ph: (204) 1116 Fax: (204) 2040  
Email: jerdrafting@supernet.ca

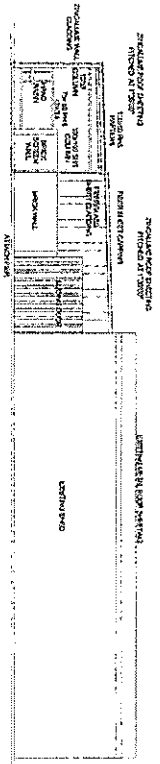
Design/Drawn  
Checked  
Reviewed  
Approved

SCALE  
SHEET: 10/14  
DRAWING NUMBER: JET-12120005  
DATE: 11/17/13  
DESIGNED BY: J. THANG  
CHECKED BY: J. THANG  
REVIEWED BY: J. THANG  
APPROVED BY: J. THANG



**12 ELEVATION**  
SCALE 1/8" = 1'-0"

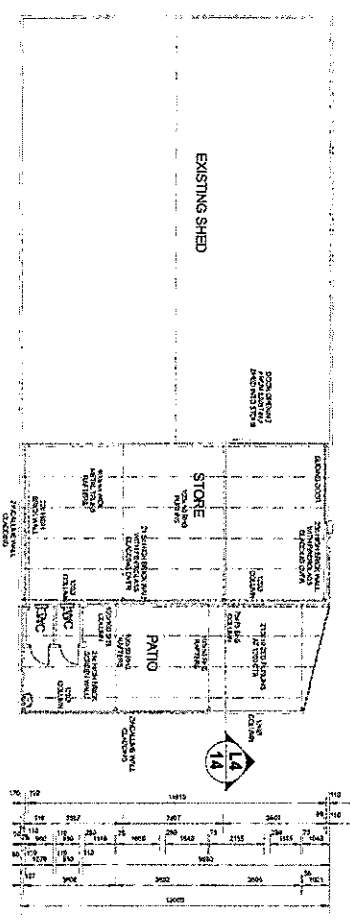
**12 ELEVATION**  
SCALE 1/8" = 1'-0"



**13 ELEVATION**  
SCALE 1/8" = 1'-0"



**14 SECTION**  
SCALE 1/8" = 1'-0"

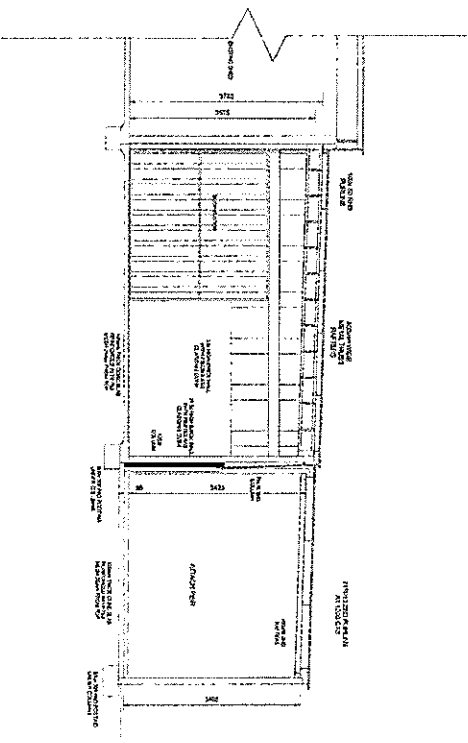


**15 SECTION**  
SCALE 1/8" = 1'-0"

**15 SHED 1 FLOOR PLAN**  
SCALE 1/8" = 1'-0"



**14 SECTION**  
SCALE 1/8" = 1'-0"



**14 SECTION**  
SCALE 1/8" = 1'-0"

EXISTING GREEN HOUSE

CLIENT: Mr. VAN THANG NGUYEN  
PROJECT: LOT 77 (#365) WATFIELD ROAD  
HARDON PARK

**DESIGN & DRAFTING SERVICE**  
100A WATERLOO STREET - QUARTER HILL, 680A  
Ph: 920 1116 Fax: 943 2340  
Email: [design@ddp.com.au](mailto:design@ddp.com.au)

SCALE: 1/8" = 1'-0"  
SHEET: 01/01  
DRAWING NUMBER: JPT-1-1000000  
DATE: 04/12/2014  
DRAWN BY: JPT-1-1000000  
CHECKED BY: JPT-1-1000000  
APPROVED BY: JPT-1-1000000

**PAYMENT SUMMARY**

**CHEQUE PAYMENTS**

CHQ025339 – CHQ025377

**CANCELLED PAYMENTS**

**ELECTRONIC FUNDS TRANSFER PAYMENT**

EF069747 – EF070299



## MUNICIPAL BANK ACCOUNT

<b>Cheque/ EFT</b>	<b>Account No.</b>	<b>Account/Payee</b>	<b>Date</b>	<b>Value</b>
EF069747	11865	<b>VALMA LUCY OLIVER</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069748	11867	<b>KEVIN JOHN ALLEN</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	1,833.33
EF069749	12740	<b>MAYOR LOGAN HOWLETT</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	6,166.67
EF069750	15883	<b>TONY ROMANO - COUNCILLOR</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069751	19059	<b>CAROL REEVE-FOWKES</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069752	20634	<b>LEE-ANNE SMITH</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069753	21185	<b>BART HOUWEN</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069754	23338	<b>STEVE PORTELLI</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069755	23339	<b>STEPHEN PRATT</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069756	23340	<b>SHAHYAZ MUBARAKAI</b> MONTHLY COUNCILLOR ALLOWANCE	1/03/2013	583.33
EF069757	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	5/03/2013	200,230.00
EF069758	10244	<b>BUILDING &amp; CONST INDUSTRY TRAINING FUND</b> LEVY PAYMENT	5/03/2013	46,712.71
EF069759	11789	<b>WALGA</b> ADVERTISING/TRAINING SERVICES	5/03/2013	16,863.76
EF069760	12656	<b>COOGEE BEACH SURF LIFESAVING CLUB INC</b> POOR GROVE SLSC DEVELOPMENT COSTS	5/03/2013	1,828.75
EF069761	15363	<b>JONES LANG LASALLE (WA) PTY LTD</b> SHOP RENT - GATEWAY SHOPPING CENTRE	5/03/2013	21,216.00
EF069762	18553	<b>SELECTUS PTY LTD</b> PAYROLL DEDUCTIONS	5/03/2013	10,573.39
EF069763	18685	<b>ADRIAN CHESTER</b> SALARY PACKAGED IPAD REIMBURSEMENT	5/03/2013	29.98
EF069764	21155	<b>D PATEL</b> COMPOST BIN REBATE	5/03/2013	35.90
EF069765	21403	<b>ROBERTA BUNCE</b> COMMUNITY CARE VOLUNTEER REIMBURSEMENTS	5/03/2013	116.00
EF069766	22868	<b>AKIYOSHI KAWAMURA</b> SALARY PACKAGED LAPTOP REIMBURSEMENT	5/03/2013	2,900.00
EF069767	23730	<b>DELOITTE TOUCHE TOHMATSU</b> AUDITING SERVICES - INTERNAL	5/03/2013	19,647.10
EF069768	24674	<b>PETER IKSTRUMS</b> TAFE FEES REIMBURSEMENT	5/03/2013	73.70
EF069769	24708	<b>EVERLEVEL PTY LTD</b> REIMBURSEMENT - SUBDIVISION FEES	5/03/2013	23,903.00
EF069770	24709	<b>WILMARI NEL</b> UNIVERSITY FEES REIMBURSEMENT	5/03/2013	5,240.00
EF069771	24710	<b>CHRISTINE PALUMBO</b> REFUND - INCORRECT CHARGE OF FEES	5/03/2013	52.00
EF069772	24711	<b>JOHN KENDALL</b> REFUND - INCORRECT CHARGE OF FEES	5/03/2013	4.00
EF069773	24712	<b>DAVID WATSON</b> SALARY PACKAGED LAPTOP REIMBURSEMENT	5/03/2013	2,798.00
EF069774	24713	<b>NATHAN JOHNSTON</b> SALARY PACKAGED IPAD REIMBURSEMENT	5/03/2013	534.00

EF069775	24714	<b>WILLIAM &amp; DINA FORTILLO</b> COMPOST BIN REBATE	5/03/2013	50.00
EF069776	24715	<b>ROSS PORTER</b> COMPOST BIN REBATE	5/03/2013	50.00
EF069777	24716	<b>MEGHAN WOOD</b> COMPOST BIN REBATE	5/03/2013	34.00
EF069778	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	11/03/2013	64,381.00
EF069779	10363	<b>COCKBURN SENIOR CITIZENS ASSOCIATION</b> DONATION	11/03/2013	10,000.00
EF069780	10888	<b>LJ CATERERS</b> CATERING SERVICES	11/03/2013	5,147.00
EF069781	10944	<b>MCLEODS</b> LEGAL SERVICES	11/03/2013	2,326.78
EF069782	11794	<b>SYNERGY</b> ELECTRICITY USAGE/SUPPLIES	11/03/2013	8,246.64
EF069783	14834	<b>GILDEN TREE FARM</b> PLANT/TREE SUPPLIES	11/03/2013	25,102.00
EF069784	15286	<b>GUNTHER &amp; HELKE FAHRNER</b> LANDOWNER BIODIVERSITY GRANT	11/03/2013	1,330.00
EF069785	18436	<b>JCS PLUMBING SERVICES</b> PLUMBING SERVICES	11/03/2013	3,989.03
EF069786	18685	<b>ADRIAN CHESTER</b> SALARY PACKAGED IPAD REIMBURSEMENT	11/03/2013	308.00
EF069787	19455	<b>ANDY JARMAN</b> EXPENSES REIMBURSEMENT - FIRST AID	11/03/2013	150.00
EF069788	21691	<b>ZETTANET PTY LTD</b> INTERNET/WEB SERVICES	11/03/2013	495.00
EF069789	23570	<b>A PROUD LANDMARK PTY LTD</b> LANDSCAPE CONTRUCTION SERVICES	11/03/2013	27,099.93
EF069790	24035	<b>NEXT POWER</b> RENEWABLE ENERGY	11/03/2013	4,400.00
EF069791	24135	<b>CHRISTOPHER DYSON</b> PRESCRIPTION GLASSES - CONTRIBUTION	11/03/2013	250.00
EF069792	24656	<b>JUSTINA SMITH</b> ENTERTAINMENT SERVICES	11/03/2013	300.00
EF069793	24657	<b>CHARLIE KICKETT</b> PERFORMANCE SERVICES	11/03/2013	200.00
EF069794	24720	<b>CAVALIERS NETBALL CLUB</b> SPORTING EQUIPMENT GRANT	11/03/2013	390.60
EF069795	24721	<b>ROHAN SIM</b> SALARY PACKAGED IPAD REIMBURSEMENT	11/03/2013	707.95
EF069796	24722	<b>MARK CALHOUN</b> PRESCRIPTION GLASSES - CONTRIBUTION	11/03/2013	250.00
EF069797	24723	<b>AMANDA SQUIRES</b> PRESCRIPTION GLASSES - CONTRIBUTION	11/03/2013	250.00
EF069798	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	18/03/2013	207,016.00
EF069799	10944	<b>MCLEODS</b> LEGAL SERVICES	18/03/2013	8,990.14
EF069800	14721	<b>BRONWYN &amp; PHIL GENONI</b> LANDOWNER BIODIVERSITY GRANT	18/03/2013	100.00
EF069801	18553	<b>SELECTUS PTY LTD</b> PAYROLL DEDUCTIONS	18/03/2013	10,493.30
EF069802	21362	<b>DANNY SANTOSO</b> SALARY PACKAGED LAPTOP REIMBURSEMENT	18/03/2013	1,499.00
EF069803	24426	<b>KATY BOSCHETTI</b> EXPENSES REIMBURSEMENT	18/03/2013	462.00
EF069804	24728	<b>MAMMOTH NOMINEES PTY LTD</b> REIMBURSEMENT OF BANK GUARANTEE FUNDS	18/03/2013	9,905.37
EF069805	24729	<b>JOHN CHUAH</b>	18/03/2013	366.00

		SALARY PACKAGED IPAD REIMBURSEMENT		
EF069806	10152	AUST SERVICES UNION PAYROLL DEDUCTIONS	11/03/2013	3,126.40
EF069807	10305	CHILD SUPPORT AGENCY PAYROLL DEDUCTIONS	11/03/2013	4,397.36
EF069808	10733	HOSPITAL BENEFIT FUND PAYROLL DEDUCTIONS	11/03/2013	2,068.81
EF069809	11001	MUNICIPAL EMPLOYEES UNION PAYROLL DEDUCTIONS	11/03/2013	892.40
EF069810	11856	WA LOCAL GOVERNMENT SUPER PLAN PAYROLL DEDUCTIONS	11/03/2013	308,888.06
EF069811	11857	CHAMPAGNE SOCIAL CLUB PAYROLL DEDUCTIONS	11/03/2013	1,122.80
EF069812	11859	STAFF SOCIAL CLUB PAYROLL DEDUCTIONS	11/03/2013	57.20
EF069813	11860	45S CLUB PAYROLL DEDUCTIONS	11/03/2013	48.00
EF069814	18005	COLONIAL FIRST STATE PAYROLL DEDUCTIONS	11/03/2013	359.20
EF069815	18247	ELLIOTT SUPERANNUATION FUND PAYROLL DEDUCTIONS	11/03/2013	226.02
EF069816	18432	HESTA SUPER FUND PAYROLL DEDUCTIONS	11/03/2013	2,592.17
EF069817	18718	FIRST STATE SUPER PAYROLL DEDUCTIONS	11/03/2013	971.99
EF069818	18719	COLONIAL FIRST STATE - DAVID GIBSON PAYROLL DEDUCTIONS	11/03/2013	194.17
EF069819	18795	SUPERWRAP PAYROLL DEDUCTIONS	11/03/2013	125.13
EF069820	19010	SUMMIT PERSONAL SUPER PLAN PAYROLL DEDUCTIONS	11/03/2013	359.60
EF069821	19193	REST SUPERANNUATION PAYROLL DEDUCTIONS	11/03/2013	43.02
EF069822	19726	HEALTH INSURANCE FUND OF WA PAYROLL DEDUCTIONS	11/03/2013	2,974.10
EF069823	19727	MTAA SUPER FUND PAYROLL DEDUCTIONS	11/03/2013	172.81
EF069824	19997	AUSTRALIANSUPER PAYROLL DEDUCTIONS	11/03/2013	11,016.34
EF069825	20056	CBUS PAYROLL DEDUCTIONS	11/03/2013	816.08
EF069826	20217	DOWNING SUPERANNUATION FUND PAYROLL DEDUCTIONS	11/03/2013	3,009.77
EF069827	20300	CATHOLIC SUPER & RETIREMENT FUND PAYROLL DEDUCTIONS	11/03/2013	649.80
EF069828	20337	THE LLOYDS SUPERANNUATION FUND PAYROLL DEDUCTIONS	11/03/2013	1,505.82
EF069829	20755	COLONIAL FIRST STATE - ROBERT GRAEME WATSON PAYROLL DEDUCTIONS	11/03/2013	82.58
EF069830	20929	AUSTRALIAN ETHICAL RETAIL SUPERANNUATION FUND PAYROLL DEDUCTIONS	11/03/2013	220.28
EF069831	21365	ING LIFE - ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	11/03/2013	100.44
EF069832	21526	TASPLAN SUPER PAYROLL DEDUCTIONS	11/03/2013	106.48
EF069833	21921	MAURICIO FAMILY SELF MANAGED SUPER FUND PAYROLL DEDUCTIONS	11/03/2013	2,572.52
EF069834	21996	ANZ ONEANSWER PERSONAL SUPER PAYROLL DEDUCTIONS	11/03/2013	365.21
EF069835	22067	STEPHENS SUPERANNUATION FUND PAYROLL DEDUCTIONS	11/03/2013	736.13

EF069836	22901	<b>FONTANA SUPER PLAN</b> PAYROLL DEDUCTIONS	11/03/2013	1,195.48
EF069837	23695	<b>NETWEALTH INVESTMENT &amp; SUPERANNUATION</b> PAYROLL DEDUCTIONS	11/03/2013	1,023.91
EF069838	23993	<b>ONEPATH LIFE LIMITED</b> PAYROLL DEDUCTIONS	11/03/2013	441.68
EF069839	24620	<b>E &amp; B PINTO SUPERANNUATION FUND</b> PAYROLL DEDUCTIONS	11/03/2013	1,065.82
EF069840	24642	<b>TWUSUPER</b> PAYROLL DEDUCTIONS	11/03/2013	46.57
EF069841	10590	<b>DEPARTMENT OF FIRE AND EMERGENCY SERVICES</b> 3RD QUARTER ESL PAYMENT	20/03/2013	2,993,894.72
EF069842	12565	<b>SOUTHERN METRO REGIONAL COUNCIL -LOANS</b> LOAN REPAYMENT	20/03/2013	391,997.79
EF069843	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	25/03/2013	65,445.00
EF069844	10195	<b>BIBRA LAKE PRIMARY SCHOOL</b> COMMUNITY GRANT	25/03/2013	550.00
EF069845	10244	<b>BUILDING &amp; CONST INDUSTRY TRAINING FUND</b> LEVY PAYMENT	25/03/2013	73,726.50
EF069846	10368	<b>COCKBURN WETLANDS EDUCATION CENTRE</b> COMMUNITY GRANT	25/03/2013	83,125.90
EF069847	10788	<b>JANDAKOT VOLUNTEER BUSH FIRE BRIGADE</b> EXPENSE REIMBURSEMENTS	25/03/2013	44.79
EF069848	11022	<b>NATIVE ARC</b> DONATION	25/03/2013	25,000.00
EF069849	11399	<b>SOUTH COOGEE VOLUNTEER BUSHFIRE BRIGADE</b> EXPENSE REIMBURSEMENTS	25/03/2013	2,701.49
EF069850	11867	<b>KEVIN JOHN ALLEN</b> INK EXPENSES REIMBURSEMENT	25/03/2013	137.01
EF069851	13860	<b>KRS CONTRACTING</b> VERGE COLLECTION SERVICES-	25/03/2013	19,763.81
EF069852	14426	<b>HARMONY PRIMARY SCHOOL</b> REIMBURSEMENT	25/03/2013	550.00
EF069853	15238	<b>NEW LIFE CHRISTIAN COLLEGE</b> REIMBURSEMENT - SAND COSTS	25/03/2013	400.00
EF069854	15653	<b>COOGEE BEACH PROGRESS ASSOCIATION</b> NEWSLETTER PRINTING CONTRIBUTION	25/03/2013	191.50
EF069855	19059	<b>CAROL REEVE-FOWKES</b> DATA CHARGES REIMBURSEMENT - SISTER CITIES	25/03/2013	832.03
EF069856	20634	<b>LEE-ANNE SMITH</b> REIMBURSEMENT - CHILD AWARENESS SEMINAR	25/03/2013	480.00
EF069857	24738	<b>GREG SIEGERT</b> PARTIAL REIMBURSEMENT - STOLEN PROPERTY	25/03/2013	500.00
EF069858	24739	<b>JANET PORE</b> LANDOWNER BIODIVERSITY GRANT	25/03/2013	775.50
EF069859	23351	<b>COCKBURN GP SUPER CLINIC PTY LTD</b> OPERATING FUNDS	26/03/2013	50,000.00
EF069860	10041	<b>AIR LIQUIDE WA PTY LTD</b> GAS SUPPLIES	28/03/2013	103.95
EF069861	10058	<b>ALSCO PTY LTD</b> HYGIENE SERVICES/SUPPLIES	28/03/2013	691.97
EF069862	10082	<b>ARMANDOS SPORTS</b> SPORTING GOODS	28/03/2013	1,000.00
EF069863	10091	<b>ASLAB PTY LTD</b> ASPHALTING SERVICES/SUPPLIES	28/03/2013	580.67
EF069864	10110	<b>AUSRECORD</b> STATIONERY SUPPLIES	28/03/2013	1,463.22
EF069865	10160	<b>DORMA AUTOMATICS</b> AUTOMATIC DOOR SERVICES	28/03/2013	1,796.56
EF069866	10207	<b>BOC GASES</b>	28/03/2013	1,781.37

		GAS SUPPLIES		
EF069867	10219	<b>BOUSFIELDS MENSWEAR</b> CLOTHING SUPPLIES	28/03/2013	521.00
EF069868	10221	<b>BP AUSTRALIA LIMITED</b> DIESEL/PETROL SUPPLIES	28/03/2013	14,102.10
EF069869	10226	<b>BRIDGESTONE AUSTRALIA LTD</b> TYRE SERVICES	28/03/2013	37,492.35
EF069870	10239	<b>BUDGET RENT A CAR - PERTH</b> MOTOR VEHICLE HIRE	28/03/2013	701.47
EF069871	10246	<b>BUNNINGS BUILDING SUPPLIES PTY LTD</b> HARDWARE SUPPLIES	28/03/2013	1,295.89
EF069872	10247	<b>BUNZL AUSTRALIA LTD</b> PAPER/PLASTIC/CLEANING SUPPLIES	28/03/2013	612.17
EF069873	10255	<b>CABCHARGE AUSTRALIA PTY LTD</b> CABCHARGES	28/03/2013	326.93
EF069874	10279	<b>CASTROL AUSTRALIA PTY LTD</b> GREASE/LUBRICANTS	28/03/2013	792.00
EF069875	10287	<b>CENTRELINE MARKINGS</b> LINEMARKING SERVICES	28/03/2013	2,915.00
EF069876	10307	<b>CHILDRENS BOOK COUNCIL WA</b> CHILDRENS BOOKS	28/03/2013	50.00
EF069877	10325	<b>CITY OF FREMANTLE</b> SPONSORSHIP - INDIAN OCEAN SKY SHOW	28/03/2013	3,000.00
EF069878	10333	<b>CJD EQUIPMENT PTY LTD</b> HARDWARE SUPPLIES	28/03/2013	5,513.45
EF069879	10338	<b>TRANSPACIFIC CLEANAWAY</b> WASTE DISPOSAL SERVICES	28/03/2013	510.40
EF069880	10346	<b>COATES HIRE OPERATIONS PTY LTD</b> EQUIPMENT HIRING SERVICES	28/03/2013	21,131.64
EF069881	10348	<b>COCA COLA AMATIL</b> SOFT DRINK SUPPLIES	28/03/2013	5,418.95
EF069882	10354	<b>COCKBURN COMMUNITY AND CULTURAL COUNCIL</b> DONATION / YOUTH ART SCHOLARSHIP	28/03/2013	2,000.00
EF069883	10358	<b>COCKBURN LIQUOR CENTRE</b> LIQUOR SUPPLIES	28/03/2013	497.40
EF069884	10359	<b>COCKBURN PAINTING SERVICE</b> PAINTING SUPPLIES/SERVICES	28/03/2013	924.00
EF069885	10375	<b>VEOLIA ENVIRONMENTAL SERVICES</b> WASTE SERVICES	28/03/2013	4,608.07
EF069886	10384	<b>COMMUNICATIONS AUSTRALIA PTY LTD</b> COMMUNICATION SERVICES	28/03/2013	2,597.54
EF069887	10394	<b>CD'S CONFECTIONERY WHOLESALERS</b> CONFECTIONERY	28/03/2013	1,779.71
EF069888	10408	<b>COOLBELLUP NEWSAGENCY</b> NEWSPAPER SUPPLIES	28/03/2013	406.90
EF069889	10422	<b>REITSEMA PACKAGING</b> ROAD LITTER BAGS	28/03/2013	1,437.04
EF069890	10483	<b>LANDGATE</b> MAPPING/LAND TITLE SEARCHES	28/03/2013	451.95
EF069891	10522	<b>DYMOCKS HAY ST</b> BOOKS	28/03/2013	6,593.00
EF069892	10526	<b>E &amp; MJ ROSHER PTY LTD</b> MOWER PARTS	28/03/2013	12,641.20
EF069893	10535	<b>ECOSYSTEM MANAGEMENT SERVICES</b> PLANTS	28/03/2013	1,128.60
EF069894	10557	<b>ENVAR SERVICE PTY LTD</b> PREVENTATIVE MAINTENANCE SERVICES	28/03/2013	2,664.06
EF069895	10580	<b>FC COURIERS</b> COURIER SERVICES	28/03/2013	1,814.79
EF069896	10597	<b>FLEXI STAFF PTY LTD</b> EMPLOYMENT SERVICES	28/03/2013	97,527.46

EF069897	10636	<b>FUJI XEROX AUSTRALIA PTY LTD</b> PHOTOCOPY CHARGES	28/03/2013	24,876.06
EF069898	10641	<b>GALVINS PLUMBING PLUS</b> PLUMBING SERVICES	28/03/2013	779.91
EF069899	10648	<b>GEOFABRICS AUSTRALASIA PTY LTD</b> GEOSYNTHETIC PRODUCTS	28/03/2013	10,669.93
EF069900	10683	<b>GRONBEK SECURITY</b> LOCKSMITH SERVICES	28/03/2013	1,978.91
EF069901	10692	<b>AECOM AUSTRALIA PTY LTD</b> CONSULTANCY SERVICES	28/03/2013	1,650.00
EF069902	10697	<b>HARDWARE DISTRIBUTORS WA</b> HARDWARE SUPPLIES	28/03/2013	2,123.54
EF069903	10709	<b>HECS FIRE</b> FIRE SYSTEM MAINTENANCE	28/03/2013	242.00
EF069904	10726	<b>HOLTON CONNOR ARCHITECTS &amp; PLANNERS</b> ARCHITECTURAL SERVICES	28/03/2013	55,000.00
EF069905	10737	<b>RAIN SCAPE WATERWISE SOLUTIONS</b> RETICULATION/IRRIGATION SUPPLIES	28/03/2013	18.90
EF069906	10741	<b>HYDROJET</b> GRAFFITI REMOVAL SERVICES/PRODUCTS	28/03/2013	1,980.00
EF069907	10743	<b>ICON-SEPTECH PTY LTD</b> DRAINAGE PRODUCTS	28/03/2013	9,186.44
EF069908	10767	<b>INST OF PUBLIC WORKS ENG AUST - NSW</b> TRAINING SERVICES	28/03/2013	550.00
EF069909	10768	<b>INST OF PUBLIC WORKS ENG AUST - WA</b> MEMBERSHIP FEES	28/03/2013	2,800.00
EF069910	10771	<b>INTERLEC PTY LTD</b> ELECTRICAL SERVICES	28/03/2013	88,497.93
EF069911	10778	<b>IWF FENCING</b> FENCING REPAIRS/MAINTENANCE	28/03/2013	13,752.20
EF069912	10779	<b>J F COVICH &amp; CO PTY LTD</b> ELECTRICAL SERVICES	28/03/2013	70,983.62
EF069913	10781	<b>JANDAKOT EARTHMOVING &amp; RURAL CONTRACTORS</b> FIREBREAK CONSTRUCTION	28/03/2013	42,545.40
EF069914	10783	<b>JANDAKOT METAL INDUSTRIES</b> METAL SUPPLIES	28/03/2013	1,555.73
EF069915	10787	<b>JANDAKOT ACCIDENT REPAIR CENTRE</b> PANEL BEATING SERVICES	28/03/2013	4,486.42
EF069916	10792	<b>JASOL AUSTRALIA</b> CLEANING PRODUCTS	28/03/2013	288.04
EF069917	10794	<b>JASON SIGNMAKERS</b> SIGNS	28/03/2013	3,800.50
EF069918	10803	<b>GECKO CONTRACTING TURF &amp; LANDSCAPE MTNCE</b> MOWING/LANDSCAPING SERVICES	28/03/2013	76,362.50
EF069919	10814	<b>JR &amp; A HERSEY PTY LTD</b> SAFETY CLOTHING SUPPLIES	28/03/2013	3,744.18
EF069920	10817	<b>JUST A BUNCH</b> FLOWER DELIVERIES	28/03/2013	180.00
EF069921	10836	<b>KERB DOCTOR</b> CONCRETE KERBING - SUPPLY & LAYING	28/03/2013	290.40
EF069922	10903	<b>LOVEGROVE TURF SERVICES PTY LTD</b> TURF MAINTENANCE SERVICES	28/03/2013	12,100.00
EF069923	10913	<b>MACDONALD JOHNSTON ENGINEERING CORP</b> REPAIR SERVICES	28/03/2013	11,723.02
EF069924	10917	<b>MAGIC NISSAN</b> MOTOR VEHICLES/PARTS/SERVICE	28/03/2013	213.68
EF069925	10923	<b>MAJOR MOTORS PTY LTD</b> REPAIRS/MAINTENANCE SERVICES	28/03/2013	2,773.88
EF069926	10939	<b>LINFOX ARMAGUARD</b> BANKING SECURITY SERVICES	28/03/2013	1,816.12
EF069927	10944	<b>MCLEODS</b>	28/03/2013	18,128.12

		LEGAL SERVICES		
EF069928	10959	<b>AUSTRAL BRICK</b> BRICK PALLETS	28/03/2013	17.72
EF069929	10960	<b>METRO FILTERS</b> FILTER SUPPLIES	28/03/2013	375.00
EF069930	10972	<b>MIRACLE RECREATION EQUIPMENT</b> PLAYGROUND/PARK EQUIPMENT	28/03/2013	1,471.80
EF069931	10997	<b>WILSON PARKING AUSTRALIA</b> SECURITY SERVICES	28/03/2013	155,896.95
EF069932	11026	<b>NESTLE FOOD SERVICES</b> CATERING SUPPLIES	28/03/2013	756.00
EF069933	11028	<b>NEVERFAIL SPRINGWATER LIMITED</b> BOTTLED WATER SUPPLIES	28/03/2013	1,498.70
EF069934	11036	<b>NORTH LAKE ELECTRICAL</b> ELECTRICAL SERVICES	28/03/2013	19,462.46
EF069935	11039	<b>NOVUS AUTO GLASS</b> WINDSCREEN REPAIR SERVICES	28/03/2013	360.00
EF069936	11068	<b>VODAFONE HUTCHISON AUSTRALIA PTY LTD</b> PAGING SERVICES	28/03/2013	1,371.77
EF069937	11070	<b>OTIS ELEVATOR COMPANY</b> ELEVATOR REPAIRS/MAINTENANCE	28/03/2013	1,796.28
EF069938	11077	<b>P &amp; G BODY BUILDERS PTY LTD</b> PLANT BODY BUILDING SERVICES	28/03/2013	30,580.00
EF069939	11079	<b>P &amp; M AUTOMOTIVE EQUIPMENT</b> VEHICLE INSPECTION SERVICES	28/03/2013	114.40
EF069940	11132	<b>PERTH ZOO</b> ENTERTAINMENT SERVICES	28/03/2013	263.00
EF069941	11136	<b>DONEGAN ENTERPRISES</b> FENCING REPAIRS/MAINTENANCE	28/03/2013	1,518.00
EF069942	11182	<b>PREMIUM BRAKE &amp; CLUTCH SERVICE</b> BRAKE SERVICES	28/03/2013	7,847.40
EF069943	11208	<b>QUICK CORPORATE AUSTRALIA PTY LTD</b> STATIONERY/CONSUMABLES	28/03/2013	5,534.69
EF069944	11235	<b>REINFORCED CONCRETE PIPES PTY LTD</b> CONCRETE PIPE SUPPLIES	28/03/2013	4,039.20
EF069945	11240	<b>PINK HYGIENE SOLUTIONS</b> SANITARY SERVICES	28/03/2013	1,700.89
EF069946	11244	<b>RESEARCH SOLUTIONS PTY LTD</b> RESEARCH SERVICES	28/03/2013	2,640.00
EF069947	11257	<b>RNR CONTRACTING PTY LTD</b> SUPPLY & DELIVER EMULSION	28/03/2013	5,158.85
EF069948	11264	<b>ROCLA PIPELINE PRODUCTS</b> CONCRETE LINER SUPPLIES	28/03/2013	10,286.46
EF069949	11294	<b>SAFEMAN (WA) PTY LTD</b> PROTECTIVE CLOTHING/EQUIPMENT	28/03/2013	1,715.41
EF069950	11304	<b>SANAX MEDICAL &amp; FIRST AID SUPPLIES</b> MEDICAL SUPPLIES	28/03/2013	399.25
EF069951	11307	<b>SATELLITE SECURITY SERVICES PTY LTD</b> SECURITY SERVICES	28/03/2013	3,840.00
EF069952	11308	<b>SBA SUPPLIES</b> HARDWARE SUPPLIES	28/03/2013	1,128.33
EF069953	11318	<b>SELECT SECURITY WA PTY LTD</b> SECURITY SERVICES	28/03/2013	293.70
EF069954	11337	<b>SHERIDANS FOR BADGES</b> NAME BADGES & ENGRAVING	28/03/2013	153.62
EF069955	11361	<b>SIGMA CHEMICALS PTY LTD</b> CHEMICAL SUPPLIES	28/03/2013	2,718.20
EF069956	11368	<b>SIRENCO</b> SIREN REPAIRS/PRODUCTS	28/03/2013	357.50
EF069957	11380	<b>SNAP PRINTING FREMANTLE</b> PRINTING SERVICES	28/03/2013	2,303.14

EF069958	11382	<b>SNAP PRINTING SUBIACO</b> PRINTING SERVICES	28/03/2013	119.70
EF069959	11387	<b>BIBRA LAKE SOILS</b> SOIL & LIMESTONE SUPPLIES	28/03/2013	1,119.00
EF069960	11392	<b>SOLVER PAINTS WA</b> PAINT SUPPLIES	28/03/2013	886.05
EF069961	11404	<b>SOUTH LAKE DOLPHINS SWIMMING CLUB</b> SWIMMING REGISTRATION / GRANT	28/03/2013	200.00
EF069962	11425	<b>SOUTHERN METROPOLITAN REGIONAL COUNCIL</b> WASTE DISPOSAL GATE FEES	28/03/2013	322,294.72
EF069963	11453	<b>SPEARWOOD NEWSROUND</b> NEWSPAPER SUPPLIES	28/03/2013	1,221.92
EF069964	11459	<b>SPEARWOOD VETERINARY HOSPITAL</b> VETERINARY SERVICES	28/03/2013	1,513.20
EF069965	11463	<b>SPECTRUM CABINETS</b> CABINET SUPPLIES	28/03/2013	3,460.00
EF069966	11469	<b>SPORTS TURF TECHNOLOGY</b> TURF CONSULTANCY SERVICES	28/03/2013	4,664.00
EF069967	11470	<b>SPORTSWORLD OF WA</b> SPORT SUPPLIES	28/03/2013	842.05
EF069968	11483	<b>ST JOHN AMBULANCE AUST WA OPERATIONS</b> FIRST AID COURSES	28/03/2013	840.00
EF069969	11488	<b>POSITION PARTNERS</b> INSTRUMENT SUPPLIES	28/03/2013	233.65
EF069970	11493	<b>SAI GLOBAL LTD</b> PUBLICATIONS - STANDARDS	28/03/2013	44.00
EF069971	11496	<b>STANLEE WA LTD</b> CATERING EQUIPMENT/SUPPLIES	28/03/2013	191.40
EF069972	11505	<b>STATE LIBRARY OF WESTERN AUSTRALIA</b> BOOK SUPPLIES	28/03/2013	1,503.70
EF069973	11511	<b>STATEWIDE BEARINGS</b> BEARING SUPPLIES	28/03/2013	816.00
EF069974	11531	<b>SUNNY INDUSTRIAL BRUSHWARE PTY LTD</b> BRUSH/ROAD BROOM SUPPLIES	28/03/2013	2,572.90
EF069975	11546	<b>T FAULKNER &amp; CO</b> INSTALLATIONS/SUPPLY OF HAND RAILS	28/03/2013	21,010.00
EF069976	11556	<b>TECHNIFIRE 2000</b> FIRE FIGHTING EQUIPMENT	28/03/2013	189.56
EF069977	11557	<b>TECHNOLOGY ONE LTD</b> IT CONSULTANCY SERVICES	28/03/2013	114,839.96
EF069978	11625	<b>TOTAL EDEN PTY LTD</b> RETICULATION SUPPLIES	28/03/2013	19,941.97
EF069979	11629	<b>TOUCAN DISPLAY SYSTEMS</b> DISPLAY SYSTEMS	28/03/2013	2,876.50
EF069980	11642	<b>TRAILER PARTS PTY LTD</b> TRAILER PARTS	28/03/2013	727.52
EF069981	11651	<b>TREE WATERING SERVICES</b> TREE WATERING SERVICES	28/03/2013	42,129.00
EF069982	11652	<b>TRENCHBUSTERS</b> HIRING SERVICES	28/03/2013	1,581.80
EF069983	11655	<b>TRISLEYS HYDRAULIC SERVICES PTY LTD</b> POOL EQUIPMENT/REPAIRS	28/03/2013	17,490.00
EF069984	11657	<b>TRUCKLINE PARTS CENTRES</b> AUTOMOTIVE SPARE PARTS	28/03/2013	1,147.34
EF069985	11697	<b>VAT MAN-FAT FILTERING SYSTEMS</b> FILTER CLEANING SERVICES	28/03/2013	877.00
EF069986	11701	<b>VIBRA INDUSTRIAL FILTRATION A/ASIA</b> FILTER SUPPLIES	28/03/2013	606.76
EF069987	11708	<b>VITAL PACKAGING PTY LTD</b> PACKAGING SUPPLIES	28/03/2013	3,311.00
EF069988	11722	<b>WA HINO SALES &amp; SERVICE</b>	28/03/2013	2,077.20



		REPAIRS/MAINTENANCE SERVICES		
EF069989	11726	<b>WA LIMESTONE</b>	28/03/2013	12,373.02
		LIMESTONE SUPPLIES		
EF069990	11729	<b>WA MOBILE PHONE INSTALLATIONS</b>	28/03/2013	1,155.00
		INSTALLATION SERVICES		
EF069991	11787	<b>DEPT OF TRANSPORT (WA GOVT)</b>	28/03/2013	269.70
		WA GOVT DEPARTMENT		
EF069992	11789	<b>WALGA</b>	28/03/2013	544.50
		ADVERTISING/TRAINING SERVICES		
EF069993	11793	<b>WESTERN IRRIGATION PTY LTD</b>	28/03/2013	27,825.86
		IRRIGATION SERVICES/SUPPLIES		
EF069994	11795	<b>WESTERN POWER</b>	28/03/2013	523,603.00
		UNDER GROUND POWER/ELECTRICAL SERVICES		
EF069995	11807	<b>WESTRAL</b>	28/03/2013	100.00
		BLINDS & CURTAINS		
EF069996	11828	<b>WORLDWIDE ONLINE PRINTING - O'CONNOR</b>	28/03/2013	1,649.00
		PRINTING SERVICES		
EF069997	11972	<b>COBEY MAINTENANCE SERVICES</b>	28/03/2013	56,907.30
		TURF MANAGEMENT		
EF069998	11974	<b>GREENWASTE SERVICES</b>	28/03/2013	422.40
		MULCHING/SHREDDING SERVICES		
EF069999	11987	<b>SAFETY ZONE AUSTRALIA PTY LTD</b>	28/03/2013	960.79
		SAFETY EQUIPMENT		
EF070000	11990	<b>EARTHCARE (AUSTRALIA) P/L</b>	28/03/2013	1,089.00
		LANDSCAPING SERVICES		
EF070001	12007	<b>SHANE MCMASTER SURVEYS</b>	28/03/2013	29,590.00
		SURVEYING SERVICES		
EF070002	12014	<b>TUTT BRYANT EQUIPMENT BT EQUIPMENT PTY LTD T/AS</b>	28/03/2013	9,773.16
		EXCAVATING/EARTHMOVING EQUIPMENT		
EF070003	12028	<b>CITY OF ARMADALE</b>	28/03/2013	4,400.00
		ANIMAL DISPOSAL SERVICES		
EF070004	12065	<b>ROMERI MOTOR TRIMMERS</b>	28/03/2013	1,705.00
		AUTOMOTIVE UPHOLSTERY SERVICES		
EF070005	12101	<b>THE GREAT ESCAPE</b>	28/03/2013	260.00
		ENTRY FEES		
EF070006	12153	<b>HAYS PERSONNEL SERVICES PTY LTD</b>	28/03/2013	16,587.74
		EMPLOYMENT SERVICES		
EF070007	12193	<b>SAGE CONSULTING ENGINEERS P/L</b>	28/03/2013	4,620.00
		CONSULTANCY SERVICES - LIGHTING		
EF070008	12207	<b>CIVICA PTY LTD</b>	28/03/2013	361.90
		SOFTWARE SUPPORT/LICENCE FEES		
EF070009	12215	<b>PERTH ICE WORKS</b>	28/03/2013	33.00
		ICE SUPPLIES		
EF070010	12542	<b>SEALIN GARLETT</b>	28/03/2013	400.00
		CEREMONIAL SERVICES		
EF070011	12589	<b>AUSTRALIAN INSTITUTE OF MANAGEMENT</b>	28/03/2013	4,345.00
		TRAINING SERVICES		
EF070012	12656	<b>COOGEE BEACH SURF LIFESAVING CLUB INC</b>	28/03/2013	282,554.62
		POOR GROVE SLSC DEVELOPMENT COSTS		
EF070013	12694	<b>SPECIALISED LIFTING SERVICE</b>	28/03/2013	695.71
		LIFTING EQUIPMENT & SERVICES		
EF070014	12712	<b>MISS MAUD</b>	28/03/2013	346.20
		CATERING SERVICES		
EF070015	12779	<b>WESTERN RESOURCE RECOVERY PTY LTD</b>	28/03/2013	1,042.66
		WASTE DISPOSAL SERVICES		
EF070016	12796	<b>MEDIA MONITORS AUSTRALIA PTY LTD</b>	28/03/2013	1,233.99
		MEDIA MONITORING SERVICES		
EF070017	12820	<b>MONTELEONE FENCING</b>	28/03/2013	7,656.00
		FENCING SERVICES/MAINTENANCE		
EF070018	12849	<b>GIUDICE SURVEYS</b>	28/03/2013	9,761.00
		SURVEYING SERVICES		

EF070019	12883	<b>CONSERVATION VOLUNTEERS AUSTRALIA</b> ENVIRONMENTAL SERVICES	28/03/2013	3,300.00
EF070020	12983	<b>INDUSTRIAL FOUNDATION FOR ACCIDENT PREVENTION</b> SAFETY COURSES	28/03/2013	1,265.00
EF070021	12998	<b>PLAYRIGHT AUSTRALIA PTY LTD</b> INSPECTION SERVICES - PLAYGROUNDS	28/03/2013	89,945.90
EF070022	13000	<b>BORAL ASPHALT WA</b> SUPPLY OF ASPHALT	28/03/2013	17,146.68
EF070023	13037	<b>PPCA LTD</b> LICENCE FEE - SOUND & MUSIC	28/03/2013	3,951.99
EF070024	13039	<b>WEST AUSTRALIAN REPTILE PARK</b> ENTERTAINMENT SERVICES	28/03/2013	340.00
EF070025	13044	<b>METTLER TOLEDO LTD</b> REPAIR/MAINTENANCE SERVICES	28/03/2013	2,200.00
EF070026	13111	<b>OCE-AUSTRALIA LIMITED</b> COPIERS/PRINTERS	28/03/2013	83.60
EF070027	13373	<b>THE HIRE GUYS</b> HIRING SERVICES	28/03/2013	2,936.00
EF070028	13409	<b>KLEENIT</b> CLEANING SERVICES	28/03/2013	29,369.40
EF070029	13563	<b>ECOJOBS ENVIRONMENTAL PERSONNEL</b> EMPLOYMENT SERVICES	28/03/2013	3,256.68
EF070030	13619	<b>YMCA OF PERTH</b> YOUTH & FAMILY SERVICES	28/03/2013	577.50
EF070031	13671	<b>STAPLES AUSTRALIA PTY LTD</b> OFFICE/STATIONERY SUPPLIES	28/03/2013	1,304.96
EF070032	13767	<b>ELLIOTTS IRRIGATION PTY LTD</b> IRRIGATION SERVICES	28/03/2013	5,701.30
EF070033	13832	<b>INSIGHT CALL CENTRE SERVICES</b> COMMUNICATION SERVICES	28/03/2013	4,114.68
EF070034	13834	<b>SULO MGB AUSTRALIA PTY LTD</b> MOBILE GARBAGE BINS	28/03/2013	76,296.00
EF070035	14035	<b>EXPO DOCUMENT COPY CENTRE (WA) PTY LTD</b> PORTABLE DISPLAY SYSTEMS	28/03/2013	467.51
EF070036	14111	<b>POLYTECHNIC WEST</b> EDUCATIONAL SERVICES	28/03/2013	287.37
EF070037	14258	<b>WARP GROUP PTY LTD</b> ROAD CONSTRUCTION MATERIALS	28/03/2013	2,118.60
EF070038	14459	<b>BIDVEST (WA) PTY LTD</b> FOOD/CATERING SUPPLIES	28/03/2013	998.13
EF070039	14593	<b>AUSTREND INTERNATIONAL PTY LTD</b> ALUMINIUM SUPPLIES	28/03/2013	2,079.00
EF070040	14631	<b>WASTE GAS RESOURCES PTY LTD</b> POWER GENERATION	28/03/2013	36,740.00
EF070041	14908	<b>OAKVALE CAPITAL LIMITED</b> CONSULTANCY SERVICES	28/03/2013	4,274.14
EF070042	15072	<b>DRUM PRINT &amp; PUBLICATIONS</b> PRINTING SERVICES	28/03/2013	203.20
EF070043	15193	<b>PRO TRAMP AUSTRALIA</b> ENTERTAINMENT SERVICES	28/03/2013	1,250.00
EF070044	15283	<b>LASER CORPS WA</b> AMUSEMENT PARK/CENTRE	28/03/2013	990.00
EF070045	15327	<b>LKL CONTRACTING</b> BOBCAT HIRE / LANDSCAPING SERVICES	28/03/2013	11,220.00
EF070046	15337	<b>CHUBB SECURITY SERVICES LTD</b> SECURITY SERVICES	28/03/2013	1,173.00
EF070047	15455	<b>PHOENIX PARK LITTLE ATHLETICS CLUB</b> SPORTS FEES	28/03/2013	400.00
EF070048	15462	<b>GREENSLADES &amp; CO P/L</b> PET FOOD SUPPLIES	28/03/2013	59.90
EF070049	15541	<b>JANDAKOT NEWS</b> NEWSPAPER SUPPLIERS	28/03/2013	192.80

EF070050	15588	<b>NATURAL AREA MANAGEMENT &amp; SERVICES</b> WEED SPRAYING	28/03/2013	2,178.00
EF070051	15625	<b>OPUS INTERNATIONAL CONSULTANTS (PCA) LTD</b> CONSULTANCY SERVICES	28/03/2013	4,455.00
EF070052	15678	<b>A2Z PEST CONTROL</b> PEST CONTROL	28/03/2013	1,087.00
EF070053	15746	<b>WESTERN AUSTRALIA POLICE SERVICE</b> POLICE CLEARANCES	28/03/2013	13.00
EF070054	15850	<b>ECOSCAPE</b> ENVIRONMENTAL CONSULTANCY	28/03/2013	20,331.30
EF070055	15862	<b>FREMANTLE MILK DISTRIBUTORS</b> MILK DELIVERY	28/03/2013	1,279.35
EF070056	15914	<b>T-QUIP</b> MOWING EQUIPMENT	28/03/2013	2,660.85
EF070057	16050	<b>SOUTH METROPOLITAN PERSONNEL</b> EMPLOYMENT SERVICES	28/03/2013	248.20
EF070058	16064	<b>CMS ENGINEERING PTY LTD</b> AIRCONDITIONING SERVICES	28/03/2013	463.10
EF070059	16291	<b>WA PROFILING</b> ROAD PROFILING SERVICES	28/03/2013	1,336.50
EF070060	16386	<b>LITTLE RED APPLE PUBLISHING</b> BOOK SUPPLIES	28/03/2013	81.87
EF070061	16396	<b>MAYDAY EARTHMOVING</b> GRADER HIRE	28/03/2013	20,561.86
EF070062	16403	<b>ROBINSON BUILDTECH</b> BUILDING SERVICES - ALTERATIONS	28/03/2013	8,402.41
EF070063	16497	<b>ACCESS UNLIMITED INTERNATIONAL PTY LTD</b> FABRICATION SERVICES	28/03/2013	2,486.00
EF070064	16704	<b>ACCIDENTAL FIRST AID SUPPLIES</b> MEDICAL SUPPLIES	28/03/2013	272.72
EF070065	16894	<b>TREBLEX INDUSTRIAL PTY LTD</b> CHEMICALS - AUTOMOTIVE	28/03/2013	633.60
EF070066	16985	<b>WA PREMIX</b> CONCRETE SUPPLIES	28/03/2013	28,048.02
EF070067	16997	<b>AUS SECURE</b> SECURITY SERVICES/PRODUCTS	28/03/2013	100.00
EF070068	17040	<b>DEPARTMENT OF PREMIER AND CABINET</b> PUBLICATIONS	28/03/2013	160.20
EF070069	17078	<b>PHOENIX KNIGHTS FOOTBALL CLUB</b> REGISTRATION FEES	28/03/2013	600.00
EF070070	17097	<b>VALUE TISSUE</b> PAPER PRODUCTS	28/03/2013	980.10
EF070071	17147	<b>DJ PALMER (WA) PTY LTD</b> FENCING MATERIALS	28/03/2013	256.39
EF070072	17213	<b>COCKBURN CITY SOCCER CLUB INC</b> SPORT EQUIPMENT GRANT	28/03/2013	1,800.00
EF070073	17316	<b>RICHARD L HILL</b> CONSULTANCY SERVICES	28/03/2013	1,005.40
EF070074	17339	<b>CITY OF SUBIACO</b> LSL ENTITLEMENTS	28/03/2013	2,382.34
EF070075	17362	<b>JOHN EARLEY</b> TRAINING	28/03/2013	480.00
EF070076	17471	<b>PIRTEK (FREMANTLE) PTY LTD</b> HOSES & FITTINGS	28/03/2013	160.40
EF070077	17587	<b>WEST COAST SHADE</b> SHADE STRUCTURES	28/03/2013	3,773.00
EF070078	17600	<b>ERECTIONS (WA)</b> GUARD RAILS	28/03/2013	1,926.10
EF070079	17608	<b>NU-TRAC RURAL CONTRACTING</b> BEACH CLEANING/FIREBREAK CONSTRUCTION	28/03/2013	6,756.00
EF070080	17798	<b>WESTERN DIAGNOSTIC PATHOLOGY</b>	28/03/2013	140.14

		ANALYTICAL SERVICES		
EF070081	17942	MRS MAC'S FOOD SUPPLIES	28/03/2013	575.65
EF070082	17987	FREMANTLE LEAK DETECTORS LOCATING SERVICES	28/03/2013	330.00
EF070083	18017	INSTANT PRODUCTS GROUP HIRE OF PORTABLE TOILETS	28/03/2013	363.00
EF070084	18038	COCKBURN NETBALL CLUB SPORTS GRANT	28/03/2013	2,600.00
EF070085	18086	DONALD CANT WATTS CORKE (WA) P/L PROJECT MANAGEMENT SERVICES	28/03/2013	3,850.00
EF070086	18126	DELL AUSTRALIA PTY LTD COMPUTER HARDWARE	28/03/2013	499.40
EF070087	18128	BESAM AUSTRALIA PTY LTD REPAIRS/MAINTENANCE SERVICES	28/03/2013	528.00
EF070088	18203	NATSYNC ENVIRONMENTAL PEST CONTROL	28/03/2013	3,075.00
EF070089	18244	MELVILLE LAKERS NETBALL CLUB NETBALL CLUB	28/03/2013	600.00
EF070090	18249	LASSO MEDIA ADVERTISING	28/03/2013	363.00
EF070091	18272	AUSTRACLEAR LIMITED INVESTMENT SERVICES	28/03/2013	44.97
EF070092	18286	IW PROJECTS PTY LTD CONSULTANCY SERVICES - CIVIL ENGINEERING	28/03/2013	11,561.00
EF070093	18293	EASTERN PRESS PRINTING	28/03/2013	1,440.00
EF070094	18389	GAVIN CONSTRUCTION CONSTRUCTION SERVICES - GP SUPER CLINIC	28/03/2013	3,406,371.63
EF070095	18436	JCS PLUMBING SERVICES PLUMBING SERVICES	28/03/2013	1,418.20
EF070096	18494	DEPARTMENT OF ENVIRONMENT AND CONSERVATION LICENCE RENEWAL	28/03/2013	18,688.00
EF070097	18508	JOHN TURNER BRICK LAYING SERVICES	28/03/2013	4,246.00
EF070098	18533	FRIENDS OF THE COMMUNITY INC. ACTIVE CITIZEN AWARD 2013	28/03/2013	780.00
EF070099	18613	ECO-HIRE EQUIPMENT HIRE	28/03/2013	10,259.10
EF070100	18628	UNILEVER AUSTRALIA LTD BEVERAGES	28/03/2013	695.88
EF070101	18639	HAMILTON HILL DELIVERY ROUND NEWSPAPER DELIVERY SERVICE	28/03/2013	138.92
EF070102	18721	LINC INTEGRATED MARKETING MARKETING CONSULTANCY SERVICES	28/03/2013	3,049.20
EF070103	18734	P & R EDWARDS ENTERTAINMENT SERVICES	28/03/2013	1,000.00
EF070104	18764	AFFIRMATIVE PAVING BRICK PAVING SERVICES	28/03/2013	1,518.00
EF070105	18799	DOWN TO EARTH TRAINING & ASSESSING TRAINING SERVICES	28/03/2013	1,980.00
EF070106	18884	SILICH ENTERPRISES PTY LTD BOLLARDS	28/03/2013	3,376.50
EF070107	18962	SEALANES (1985) P/L CATERING SUPPLIES	28/03/2013	2,787.61
EF070108	19017	AIRBRUSH TATTOOS WA TATTOOING SERVICES	28/03/2013	470.00
EF070109	19038	DOWSING CONCRETE CONCRETING SERVICES	28/03/2013	11,973.72
EF070110	19157	LOMBARD THE PAPER PEOPLE STATIONERY SUPPLIES	28/03/2013	130.99

EF070111	19306	<b>ZIF HEATERS (AUST) PTY LTD</b> HEATERS	28/03/2013	715.00
EF070112	19396	<b>ACP EVENTS</b> EVENT EQUIPMENT HIRE	28/03/2013	115,500.00
EF070113	19533	<b>WOOLWORTHS LTD</b> GROCERIES	28/03/2013	1,522.00
EF070114	19541	<b>TURF CARE WA P/L</b> TURF SERVICES	28/03/2013	3,168.00
EF070115	19545	<b>GRASSWEST</b> BUILDING & GARDEN MAINTENANCE	28/03/2013	3,388.00
EF070116	19619	<b>SKIPPER TRUCKS</b> TRUCKS	28/03/2013	1,757.22
EF070117	19623	<b>ERGOLINK</b> OFFICE FURNITURE	28/03/2013	948.25
EF070118	19628	<b>PAPERBARK TECHNOLOGIES</b> ARBORICULTURAL CONSULTANCY SERVICES	28/03/2013	4,643.00
EF070119	19657	<b>BIGMATE MONITORING SERVICES PTY LTD</b> COMPUTER HARDWARE/SOFTWARE	28/03/2013	3,576.54
EF070120	19755	<b>EMBROIDME MYAREE</b> EMBROIDERY	28/03/2013	1,684.65
EF070121	19830	<b>AUTO MASTERS - SPEARWOOD</b> MECHANICAL SERVICES	28/03/2013	250.00
EF070122	19847	<b>PFD FOOD SERVICES PTY LTD</b> CATERING SERVICES	28/03/2013	4,292.05
EF070123	19856	<b>WESTERN TREE RECYCLERS</b> SHREDDING SERVICES	28/03/2013	32,791.00
EF070124	19867	<b>VERTICAL TELECOMS (WA) PTY LTD</b> COMMUNICATIONS	28/03/2013	3,123.25
EF070125	19885	<b>SAFEGUARD INDUSTRIES</b> SECURITY SCREENS/DOORS	28/03/2013	1,480.00
EF070126	19967	<b>FINGER FOOD CATERING</b> CATERING SERVICES	28/03/2013	2,848.00
EF070127	20000	<b>AUST WEST AUTO ELECTRICAL P/L</b> AUTO ELECTRICAL SERVICES	28/03/2013	19,174.81
EF070128	20146	<b>DATA#3 LIMITED</b> COMPUTER SOFTWARE	28/03/2013	17,057.57
EF070129	20215	<b>POWERVAC</b> CLEANING EQUIPMENT	28/03/2013	305.00
EF070130	20230	<b>BUTT OUT AUSTRALIA</b> CLEANING SUPPLIES	28/03/2013	649.00
EF070131	20247	<b>CHRISTIE PARKSAFE</b> PARKS & RECREATIONAL PRODUCTS	28/03/2013	11,349.80
EF070132	20321	<b>RIVERJET P/L</b> EDUCTING-CLEANING SERVICES	28/03/2013	22,992.75
EF070133	20341	<b>WILHELMINA MARIA HOUWEN</b> GARDENING SERVICES	28/03/2013	2,520.00
EF070134	20427	<b>ENVIRONMENT HOUSE INC</b> ENVIRONMENT HOUSE	28/03/2013	11,562.80
EF070135	20539	<b>THE COX GROUP PTY LTD</b> CONCEPT DESIGNING	28/03/2013	3,696.00
EF070136	20547	<b>GARRARDS PTY LTD</b> INSECTICIDES / PESTICIDES	28/03/2013	379.50
EF070137	20549	<b>A1 CARPET, TILE &amp; GROUT CLEANING</b> CLEANING SERVICES - TILES/CARPET	28/03/2013	935.00
EF070138	20583	<b>ANIMAL PEST MANAGEMENT SERVICES</b> FERAL ANIMAL CONTROL	28/03/2013	12,585.10
EF070139	20622	<b>BLADE SKATE</b> SPORTING EQUIPMENT	28/03/2013	1,400.00
EF070140	20693	<b>RENTOKIL INITIAL PTY LTD</b> PEST CONTROL SERVICES	28/03/2013	647.58
EF070141	20748	<b>CRANEWORKS AUSTRALASIA</b>	28/03/2013	2,457.80

EF070142	20839	HYDRAULIC REPAIR SERVICES <b>SUCCESS PRIMARY SCHOOL</b> COMMUNITY GRANT	28/03/2013	550.00
EF070143	20882	<b>BELL-VISTA FRUIT &amp; VEGETABLE</b> FRUIT & VEGETABLE	28/03/2013	322.44
EF070144	20934	<b>GREENLINE AG P/L</b> AGRICULTURAL EQUIPMENT	28/03/2013	362.03
EF070145	20951	<b>ELECTROFEN PTY LTD</b> FENCING SERVICES	28/03/2013	385.00
EF070146	21005	<b>BRAIN TEASERS OZ PTY LTD</b> EDUCATIONAL PRODUCTS	28/03/2013	198.00
EF070147	21120	<b>SHOREWATER MARINE PTY LTD</b> MARINE CONSTRUCTION SERVICES	28/03/2013	4,235.00
EF070148	21127	<b>JOANNA AYCKBOURN</b> INSTRUCTION - SINGING	28/03/2013	600.00
EF070149	21139	<b>AUSTRAFFIC WA PTY LTD</b> TRAFFIC SURVEYS	28/03/2013	5,527.50
EF070150	21198	<b>STUDIO KRAZE</b> VIDEO PRODUCTIONS	28/03/2013	215.00
EF070151	21234	<b>RIVER ABILITIES</b> HEALTHCARE PRODUCTS	28/03/2013	62.00
EF070152	21290	<b>ONSITE RENTALS</b> EQUIPMENT HIRE /TOILETS ETCE	28/03/2013	192.50
EF070153	21291	<b>CHITTERING VALLEY WORM FARM</b> ENVIRONMENTAL EDUCATION	28/03/2013	616.00
EF070154	21298	<b>UDLA</b> ARCHITECTURAL DESIGN	28/03/2013	7,249.00
EF070155	21300	<b>LEASECHOICE</b> LEASE AGREEMENT	28/03/2013	620.82
EF070156	21363	<b>TENDERLINK.COM PTY LTD</b> COMPUTER SOFTWARE	28/03/2013	550.00
EF070157	21371	<b>SANPOINT PTY LTD</b> KERBING SERVICES	28/03/2013	46,705.87
EF070158	21401	<b>MILKY MONSTER</b> CATERING SERVICES	28/03/2013	295.00
EF070159	21527	<b>TOUCHWOOD NURSERY</b> PLANT SUPPLIES	28/03/2013	10,930.15
EF070160	21535	<b>DEBBIE EDSON</b> COCKBURN IDOL WINNER - SENIOR	28/03/2013	250.00
EF070161	21594	<b>GREENSENSE PTY LTD</b> CONSULTANCY - CLIMATE	28/03/2013	2,200.00
EF070162	21664	<b>ACT INDUSTRIAL PTY LTD</b> SKIP BINS - MANUFACTURE	28/03/2013	8,030.00
EF070163	21678	<b>IANNELLO DESIGNS</b> GRAPHIC DESIGN	28/03/2013	610.50
EF070164	21747	<b>REHAB RENTAL</b> WHEELCHAIR HIRE	28/03/2013	137.00
EF070165	21778	<b>HILTON SQUASH AND FITNESS</b> SPORTING SERVICES	28/03/2013	200.00
EF070166	21796	<b>GREEN LEAF GARDENS</b> LANDSCAPING SERVICES	28/03/2013	2,740.00
EF070167	21916	<b>DAVIES FIRST NATIONAL REAL ESTATE</b> CONSULTANCY SERVICES - REAL ESTATE	28/03/2013	15,550.28
EF070168	21946	<b>RYAN'S QUALITY MEATS</b> MEAT SUPPLIES	28/03/2013	1,783.69
EF070169	21986	<b>RECLINK AUSTRALIA INC</b> CHARITABLE ORGANISATION	28/03/2013	100.00
EF070170	21990	<b>MEDIBANK HEALTH SOLUTIONS PTY LTD</b> MEDICAL SERVICES	28/03/2013	1,615.35
EF070171	22126	<b>HAPPY VALLEY RESOURCES P/L MOLTONI</b> WASTE DISPOSAL	28/03/2013	1,518.00

EF070172	22133	<b>AIR-BORN AMUSEMENTS</b> AMUSEMENT SERVICES	28/03/2013	3,366.00
EF070173	22169	<b>GREENSTAR GROUP WA PTY LTD GREENSTAR GROUP WA</b> AIR CONDITIONING SERVICES	28/03/2013	1,862.96
EF070174	22177	<b>ADVERTISING DESIGN SERVICES (WA) PTY LTD</b> ADVERTISING DESIGN SERVICES	28/03/2013	495.00
EF070175	22242	<b>ASPHALT SURFACES PTY LTD</b> ASPHALTING SERVICES	28/03/2013	2,390.30
EF070176	22332	<b>MACQUARIE EQUIPMENT RENTALS PTY LTD</b> LEASE RENTAL	28/03/2013	16,174.25
EF070177	22342	<b>MIDWAY DRYCLEANERS</b> DRYCLEANING SERVICES	28/03/2013	320.00
EF070178	22388	<b>CARRINGTON'S TRAFFIC SERVICES</b> TRAFFIC MANAGEMENT SERVICES	28/03/2013	3,356.10
EF070179	22448	<b>CAKES WEST PTY LTD</b> CATERING	28/03/2013	52.97
EF070180	22511	<b>JOHNNY'S TILING</b> TILING SERVICES	28/03/2013	824.00
EF070181	22541	<b>SURFING WESTERN AUSTRALIA INC.</b> TRAINING SERVICES - SURFING	28/03/2013	840.00
EF070182	22553	<b>BROWNES FOOD OPERATIONS</b> CATERING SUPPLIES	28/03/2013	2,446.73
EF070183	22569	<b>KINETIC HEALTH GROUP PTY LTD</b> MEDICAL SERVICES	28/03/2013	3,020.99
EF070184	22572	<b>IRONBARK ENVIRONMENTAL &amp; SUSTAINABLE DEVELOPMENT</b> CONSULTING SERVICES - ENVIRONMENTAL	28/03/2013	7,095.00
EF070185	22619	<b>KSC TRAINING</b> TRAINING SERVICES	28/03/2013	1,318.00
EF070186	22639	<b>SHATISH CHAUHAN</b> TRAINING SERVICES - YOGA	28/03/2013	325.00
EF070187	22653	<b>PCYC FREMANTLE</b> SPONSORSHIP	28/03/2013	200.00
EF070188	22720	<b>DIAMONDS NETBALL CLUB WA INC.</b> SPORTS - NETBALL	28/03/2013	1,000.00
EF070189	22751	<b>WORKFORCE CLOTHING PTY LTD</b> CLOTHING - INDUSTRIAL	28/03/2013	103.40
EF070190	22805	<b>COVS PARTS PTY LTD</b> MOTOR PARTS	28/03/2013	688.81
EF070191	22806	<b>AUSTRALIAN FUEL DISTRIBUTORS PTY LTD</b> FUEL SUPPLIES	28/03/2013	107,386.20
EF070192	22863	<b>CHOICES FLOORING BY CARPET EXPO</b> CARPET/FLOOR COVERINGS	28/03/2013	3,480.00
EF070193	22864	<b>SUPA COOL REFRIGERATION &amp; AIR CONDITIONING</b> AIR CONDITIONING	28/03/2013	3,650.00
EF070194	22914	<b>LADY LATTE</b> CATERING SERVICES	28/03/2013	212.50
EF070195	22963	<b>WEST BIOFUEL TECHNOLOGIES PTY LTD</b> EARTHMOVING /EXCAVATION SERVICES	28/03/2013	36,855.50
EF070196	22970	<b>WASHPOD CONSOLIDATED PTY LTD</b> CLEANING - EQUIPMENT	28/03/2013	291.50
EF070197	23213	<b>SPOTLESS FACILITY SERVICES PTY LTD</b> LAUNDRY SERVICES	28/03/2013	470.36
EF070198	23253	<b>KOTT GUNNING</b> LEGAL SERVICES	28/03/2013	5,722.42
EF070199	23258	<b>CARLISLE EVENT HIRE PTY LTD</b> HIRE-PARTY/FUNCTION EQUIPMENT	28/03/2013	374.00
EF070200	23284	<b>ELITE OUTDOORS</b> BUILDING-PATIOS & OUTDOORS	28/03/2013	12,211.00
EF070201	23288	<b>ARIANE ROEMMELE</b> AMUSEMENT - CHILDREN'S ACTIVITIES	28/03/2013	370.00
EF070202	23306	<b>KEVIN &amp; FIONA ETHERTON</b>	28/03/2013	360.00

		ENTERTAINMENT SERVICES		
EF070203	23309	<b>FUN IN TRAINING AUSTRALIA PTY LTD</b>	28/03/2013	2,299.00
		FITNESS CLASSES-INSTRUCTIONS		
EF070204	23348	<b>ZUMBA WITH HONEY</b>	28/03/2013	286.00
		FITNESS CLASSES		
EF070205	23507	<b>LOCAL GEOTECHNICS</b>	28/03/2013	2,585.00
		GEOTECHNICAL/ANALYTICAL SERVICES		
EF070206	23570	<b>A PROUD LANDMARK PTY LTD</b>	28/03/2013	7,269.46
		LANDSCAPE CONTRUCTION SERVICES		
EF070207	23611	<b>PHILLIP FARMER</b>	28/03/2013	380.00
		TRAINING SERVICES		
EF070208	23621	<b>THE STABLE MANAGEMENT GROUP</b>	28/03/2013	880.00
		PRESENTER-MC		
EF070209	23628	<b>DISMANTLE</b>	28/03/2013	9,900.00
		RECONDITIONED BICYCLES & BIKE EDUCATION		
EF070210	23669	<b>WA IVECO</b>	28/03/2013	389,532.60
		PURCHASE OF NEW TRUCKS		
EF070211	23678	<b>SKATEBOARDING AUSTRALIA PTY LTD</b>	28/03/2013	1,371.00
		TRAINING-SKATEBOARD COACHING		
EF070212	23735	<b>SIDRA SOLUTIONS</b>	28/03/2013	308.00
		SOFTWARE		
EF070213	23736	<b>THE COCOA CONNECTIONS</b>	28/03/2013	352.00
		SUSTAINABILITY GRANT		
EF070214	23806	<b>PAVY RESOURCES (AUST) PTY LTD</b>	28/03/2013	8,378.68
		CONSULTANCY - COMPUTER		
EF070215	23818	<b>AM &amp; IE MUTCH ENGINEERING CONSULTANTS</b>	28/03/2013	15,411.00
		CONSULTANCY SERVICES		
EF070216	23971	<b>FIND WISE LOCATION SERVICES</b>	28/03/2013	2,124.65
		LOCATING SERVICES - UNDERGROUND		
EF070217	23985	<b>WINNACOTT KATS JUNIOR FOOTBALL CLUB INC</b>	28/03/2013	400.00
		REGISTRATION FEES-KIDSPORT		
EF070218	24036	<b>MULTI SWEEP PTY LTD (WA)</b>	28/03/2013	4,546.40
		SWEEPING SERVICES		
EF070219	24041	<b>ALLEN AIR &amp; REFRIDGERATION</b>	28/03/2013	283.80
		AIR CONDITIONING SERVICES		
EF070220	24056	<b>KATHERINE DONEGAN</b>	28/03/2013	550.00
		EXERCISE PHYSIOLOGY		
EF070221	24127	<b>MAD SLUSH PTY LTD</b>	28/03/2013	713.00
		SOFT DRINK SUPPLIES		
EF070222	24145	<b>TOMPKINS PARK TOUCH ASSOCIATION</b>	28/03/2013	200.00
		KIDSPORT REGISTRATION FEES		
EF070223	24154	<b>UTS SOILTEC PTY LTD</b>	28/03/2013	165.00
		GROUND TESTING SERVICES		
EF070224	24157	<b>PERTH FACE PAINTING COMPANY</b>	28/03/2013	528.00
		FACE PAINTING SERVICES		
EF070225	24183	<b>WELLARD GLASS</b>	28/03/2013	642.40
		GLASS REPAIR SERVICES		
EF070226	24186	<b>ELAN ENERGY MANAGEMENT PTY LTD</b>	28/03/2013	1,678.23
		RECYCLING SERVICES - TYRES		
EF070227	24187	<b>TOTAL GREEN RECYCLING</b>	28/03/2013	3,604.51
		E-WASTE RECYCLING SERVICES		
EF070228	24195	<b>PAYNE'S WINDOW CLEANING AND SERVICES</b>	28/03/2013	9,675.27
		WINDOW CLEANING SERVICES		
EF070229	24199	<b>COFFEY GEOTECHNICS P/L</b>	28/03/2013	10,373.00
		CONSULTANCY SERVICES - GEO TECHNICAL		
EF070230	24275	<b>TRUCK CENTRE (WA) PTY LTD</b>	28/03/2013	326,114.24
		PURCHASE OF NEW TRUCKS - 1EDH840/1EDH841		
EF070231	24281	<b>ECO LOGICAL AUSTRALIA PTY LTD</b>	28/03/2013	25,236.90
		MAPPING SERVICES		
EF070232	24443	<b>AUSTRALASIAN SAFETY SERVICES PTY LTD</b>	28/03/2013	10,725.00
		SAFETY SERVICES		



EF070233	24444	<b>ROSEMARY ALLAN</b> WORKSHOPS	28/03/2013	720.00
EF070234	24506	<b>AMARANTI'S PERSONAL TRAINING</b> PERSONAL TRAINING SERVICES	28/03/2013	480.00
EF070235	24510	<b>AQUATIC AGENCY</b> AQUATIC SUPPLIES	28/03/2013	1,020.50
EF070236	24515	<b>WEST STONE (WA) PTY LTD</b> LANDSCAPING SERVICES	28/03/2013	25,124.00
EF070237	24521	<b>GEMBA GROUP PTY LTD</b> FUNCTION CENTRE BUSINESS CASE	28/03/2013	9,416.00
EF070238	24524	<b>CALO HEALTH</b> HEARTMOVE CLASSES	28/03/2013	1,270.00
EF070239	24525	<b>SPECIALTY CASTLES</b> HIRE SERVICES	28/03/2013	1,320.00
EF070240	24537	<b>ASTRONOMY EDUCATION SERVICES (AES)</b> EDUCATION SERVICES	28/03/2013	380.00
EF070241	24557	<b>AVELING</b> CONSULTANCY SERVICES	28/03/2013	1,628.00
EF070242	24558	<b>MACQUARIE BANK LIMITED</b> LEASE REPAYMENT	28/03/2013	41,749.34
EF070243	24595	<b>CONTEMPORARY IMAGE PHOTOGRAPHY PTY LTD</b> PHOTOGRAPHY SERVICES	28/03/2013	911.90
EF070244	24619	<b>MCS SECURITY</b> SECURITY SERVICES	28/03/2013	4,063.30
EF070245	24643	<b>BIBLIOTHECA RFID LIBRARY SYSTEMS AUSTRALIA PTY LTD</b> PURCHASE OF LIBRARY TAGS	28/03/2013	26,400.00
EF070246	24644	<b>GBC AUSTRALIA</b> STATIONERY SUPPLIES	28/03/2013	448.25
EF070247	24645	<b>AGRIFOOD TECHNOLOGY</b> LABELS	28/03/2013	88.00
EF070248	24654	<b>SYSAID AUSTRALIA PTY LTD</b> COMPUTER SOFTWARE	28/03/2013	1,281.50
EF070249	24655	<b>AUTOMASTERS SPEARWOOD</b> VEHICLE SERVICING	28/03/2013	225.00
EF070250	24662	<b>CIPS AUSTRALASIA</b> TRAINING SERVICES	28/03/2013	1,980.00
EF070251	24663	<b>RED TENT EVENTS</b> EVENT MANAGEMENT	28/03/2013	6,895.00
EF070252	24717	<b>DOWN UNDER SIGNS</b> SIGNAGE SERVICES	28/03/2013	1,731.40
EF070253	24718	<b>PREMIER WORKPLACE SOLUTIONS</b> TRAFFIC CONTROL SERVICES	28/03/2013	3,872.00
EF070254	24720	<b>CAVALIERS NETBALL CLUB</b> SPORTING EQUIPMENT GRANT	28/03/2013	400.00
EF070255	24724	<b>QUALITY MARINE COATING SYSTEMS P/L</b> CLEANING SERVICES - ROAD SURFACES	28/03/2013	1,100.00
EF070256	24731	<b>SCOTT HODGEN</b> PRESENTATION SERVICES	28/03/2013	220.00
EF070257	24740	<b>ROBERT KERR</b> JUNIOR TRAVEL ALLOWANCE	28/03/2013	400.00
EF070258	24741	<b>RYAN NUTTALL</b> JUNIOR TRAVEL ASSISTANCE	28/03/2013	400.00
EF070259	24742	<b>JAXON EWE-EDMONDS</b> JUNIOR TRAVEL ASSISTANCE	28/03/2013	650.00
EF070260	24743	<b>JULIA NICOLAS</b> JUNIOR TRAVEL ASSISTANCE	28/03/2013	400.00
EF070261	24744	<b>RHEANNE MENEZES</b> COCKBURN IDOL WINNER - JUNIOR	28/03/2013	500.00
EF070262	24745	<b>JASMINE MURRAY</b> COCKBURN IDOL WINNER - SENIOR	28/03/2013	500.00
EF070263	24746	<b>LEAH CHANTAL DE LUCA</b>	28/03/2013	1,000.00

COCKBURN IDOL WINNER - SENIOR

EF070264	11794	<b>SYNERGY</b> ELECTRICITY USAGE/SUPPLIES	28/03/2013	118,643.25
EF070265	12025	<b>TELSTRA CORPORATION</b> COMMUNICATIONS SERVICES	28/03/2013	21,235.16
EF070266	21469	<b>JOHN HUGHES VOLKSWAGON</b> PURCHASE OF NEW VEHICLE	28/03/2013	12,443.67
EF070267	24749	<b>ADAM &amp; PETA GRIFFITHS</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070268	24750	<b>DUSKO BRAKUS</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070269	24751	<b>SHARON LEE PUGLIA</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070270	24752	<b>SURESH K ETHISWARAN</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070271	24753	<b>RAY BARTON</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070272	24754	<b>MELISSA SULLIVAN &amp; JAMIE BOLT</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070273	24755	<b>SHELFORD CONSTRUCTION P/L</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070274	24756	<b>DONG SUK KIM</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070275	24757	<b>JEREMY JAGERSBERGER</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070276	24758	<b>LUKE HENDERSON</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070277	24759	<b>BRENDAN ROBERTS</b> CROSSOVER CONTRIBUTION	28/03/2013	300.00
EF070278	24760	<b>LORETTA DRYSDALE</b> DOG REGISTRATION REFUND	28/03/2013	6.00
EF070279	24761	<b>NATALIE DERRINGTON</b> DOG REGISTRATION REFUND	28/03/2013	12.50
EF070280	24762	<b>NILZA DELPUP</b> DOG REGISTRATION REFUND	28/03/2013	6.00
EF070281	24763	<b>IMAD NAHIM</b> DOG REGISTRATION REFUND	28/03/2013	60.00
EF070282	24764	<b>SENDY SUNANTO</b> DOG REGISTRATION REFUND	28/03/2013	30.00
EF070283	24765	<b>PAUL WOJTYSIAK</b> DOG REGISTRATION REFUND	28/03/2013	12.00
EF070284	24766	<b>KYM ENDERSBY</b> DOG REGISTRATION REFUND	28/03/2013	10.00
EF070285	24767	<b>KATH STERLING</b> DOG REGISTRATION REFUND	28/03/2013	57.00
EF070286	24768	<b>SPIROS KARLOVSKI</b> DOG REGISTRATION REFUND	28/03/2013	12.00
EF070287	24769	<b>JULIE NOLAN</b> DOG REGISTRATION REFUND	28/03/2013	18.00
EF070288	24770	<b>RITA PESELJ</b> CAT STERILISATION REFUND	28/03/2013	100.00
EF070289	24771	<b>MANDY ORRITT</b> CAT STERILISATION REFUND	28/03/2013	100.00
EF070290	24772	<b>NICOLE MAREE ANDERTON</b> CAT STERILISATION REFUND	28/03/2013	50.00
EF070291	24773	<b>CHRISTIE EVANS</b> CAT STERILISATION REFUND	28/03/2013	50.00
EF070292	24774	<b>PHYSIOTHERAPY PLUS PTY LTD</b> INTERIM ADJUSTMENTS	28/03/2013	1,230.90
EF070293	24775	<b>K S TAN &amp; LUL SHI</b> INTERIM ADJUSTMENTS	28/03/2013	303.37

EF070294	24776	<b>JET CHEN</b> RATES REFUND	28/03/2013	3,754.00
EF070295	24791	<b>DARLOT NOMINEES P/L</b> RATES REFUND	28/03/2013	542.23
EF070296	10154	<b>AUST TAXATION DEPT</b> PAYROLL DEDUCTIONS	2/04/2013	203,856.00
EF070297	10888	<b>LJ CATERERS</b> CATERING SERVICES	2/04/2013	3,369.00
EF070298	15363	<b>JONES LANG LASALLE (WA) PTY LTD</b> SHOP RENT - GATEWAY SHOPPING CENTRE	2/04/2013	21,216.00
EF070299	18553	<b>SELECTUS PTY LTD</b> PAYROLL DEDUCTIONS	2/04/2013	10,493.30
025339	13932	<b>ARMAGUARD</b> BANKING SERVICES	6/03/2013	3,093.05
025340	13932	<b>ARMAGUARD</b> BANKING SERVICES	13/03/2013	2,700.45
025341	13932	<b>ARMAGUARD</b> BANKING SERVICES	20/03/2013	4,119.45
025342	11790	<b>WESTERN AUSTRALIAN PLANNING COMMISSION</b> PLANNING APPLICATION	25/03/2013	1,135.60
025343	99999	<b>IRONBRIDGE PROPERTY</b> BOND REFUND - HAMMOND HEIGHTS STAGE 1A & 1B	25/03/2013	13,500.00
025344	99999	<b>GORAN GRUBIC</b> PLANNING REFUND - 1 BIRCHLEY	25/03/2013	139.00
025345	99999	<b>SKC DEVELOPMENTS</b> BOND REFUND - LOT 800 LOMAX CRT	25/03/2013	94,304.74
025346	99999	<b>VIVA DEVELOPMENTS PTY LTD</b> BOND REFUND - 447 YANBUP RD	25/03/2013	11,250.00
025347	99999	<b>FIDUCIA DEVELOPMENT PTY LTD</b> PLANNING REFUND - 10 SENNA CLOSE	25/03/2013	1,600.00
025348	99999	<b>IAN G HOFFMAN</b> PLANNING REFUND - 1 SUSSEX ST	25/03/2013	512.00
025349	99999	<b>WA PATIOS</b> PLANNING REFUND - 24 ST JOSEPH FAIRWAY	25/03/2013	139.00
025350	99999	PLANNING REFUND - 24 MONACO AVE REFUND	25/03/2013	320.00
025351	13932	<b>ARMAGUARD</b> BANKING SERVICES	27/03/2013	1,833.90
025352	10330	<b>CITY OF STIRLING</b> REPLACEMENT OF LIBRARY SUPPLIES	27/03/2013	5,364.44
025353	10382	<b>COMMISSIONER OF POLICE</b> LICENCE FEE	27/03/2013	109.70
025354	10747	<b>IINET LIMITED</b> INTERNET SERVICES	27/03/2013	629.45
025355	11341	<b>CITY OF BUSSELTON</b> LONG SERVICE LEAVE	27/03/2013	3,669.32
025356	11760	<b>WATER CORPORATION</b> SEWER EASEMENT	27/03/2013	12,460.15
025357	12041	<b>BIBRA LAKE SECOND HAND</b> SECOND HAND FURNITURE PURCHASES	27/03/2013	150.00
025358	17664	<b>BIODIVERSAFETY PTY LTD</b> WEED CONTROL SERVICES	27/03/2013	9,400.00
025359	22903	<b>UNIQUE INTERNATIONAL RECOVERIES LLC</b> DEBT COLLECTORS	27/03/2013	345.60
025360	24146	<b>RIVERSIDE PARK PONY CLUB</b> KID SPORT REGISTRATION FEES	27/03/2013	200.00
025361	24454	<b>GOODWIN-MCCARTHY HELICOPTERS</b> HELICOPTER HIRE	27/03/2013	13,062.50
025362	24777	<b>BARRY &amp; RITA ALLEN</b> PENSION REFUND	28/03/2013	421.51
025363	24778	<b>JEROLIM JACKOVCEVIC</b>	28/03/2013	401.04

		PENSION REFUND		
025364	24779	<b>ROSA MASTROPIERRO</b>	28/03/2013	652.68
		PENSION REFUND		
025365	24780	<b>RONALD BENTON</b>	28/03/2013	341.10
		PENSION REFUND		
025366	24781	<b>CCH &amp; WE PUNCH</b>	28/03/2013	360.05
		PENSION REFUND		
025367	24782	<b>BB &amp; JA GRAY</b>	28/03/2013	263.44
		PENSION REFUND		
025368	24783	<b>ROBERT KENNINGTON</b>	28/03/2013	369.81
		PENSION REFUND		
025369	24784	<b>GEORGE PRATT</b>	28/03/2013	383.11
		PENSION REFUND		
025370	24785	<b>TP &amp; LI HENLEY</b>	28/03/2013	486.38
		PENSION REFUND		
025371	24786	<b>BERYL CUMMING</b>	28/03/2013	389.31
		PENSION REFUND		
025372	24787	<b>AMY MCGARRY</b>	28/03/2013	534.73
		PENSION REFUND		
025373	24788	<b>KF &amp; BL RESPINI</b>	28/03/2013	238.25
		PENSION REFUND		
025374	24789	<b>KE &amp; RL WALTON</b>	28/03/2013	435.86
		PENSION REFUND		
025375	24790	<b>PETER CONDRIO</b>	28/03/2013	237.09
		PENSION REFUND		
025376	10047	<b>ALINTA ENERGY</b>	28/03/2013	137.70
		GAS SUPPLIES		
025377	11758	<b>WATER CORP</b>	28/03/2013	14,625.20
		WATER USAGE SUPPLIES		
		<b>ADD RETENTION HELD</b>		
		NIL		
		<b>LESS PRIOR PERIOD CANCELLED CHEQUES/EFTS</b>		
		NIL		
		<b>TOTAL</b>		<b>13,475,910.34</b>
		<b>TOTAL AS PER AP SOURCE 13GLACT9991000</b>		13,475,910.34
		<b>TOTAL AS PER TR SOURCE 13GLACT9991000</b>		<b>13,475,910.34</b>
		<b>ADDITIONAL DIRECT PAYMENTS</b>		
		<b>BANK FEES</b>		
		MERCHANT FEES COC		6,916.29
		MERCHANT FEES SLLC		1,156.99
		MERCHANT FEES VARIOUS OUT CENTRES		194.60
		NATIONAL BPAY CHARGE		4,093.70
		RTGS/ACLR FEE		8.50
		NAB TRANSACT FEE		-
				<b>12,370.08</b>
		<b>FAMILY DAY CARE AND IN HOME CARE PAYMENTS</b>		
		FDC PAYMENTS		46,091.02
		IHC PAYMENTS		109,279.34
				<b>155,370.36</b>
		<b>PAYROLL TRANSACTIONS</b>		
	COC 05/03/13	CITY OF COCKBURN 042958		248,884.01
	COC 06/03/13	CITY OF COCKBURN 042958		1,180.54
	COC 07/03/13	CITY OF COCKBURN 042958		934.97
	COC 08/03/13	CITY OF COCKBURN 042958		11,673.38
	COC 12/03/13	CITY OF COCKBURN 042958		730,499.01
	COC 14/03/13	CITY OF COCKBURN 042958		9,418.43
	COC 19/03/13	CITY OF COCKBURN 042958		242,914.98
	COC 26/03/13	CITY OF COCKBURN 042958		728,021.82

**CREDIT CARD PAYMENTS**

CBA CREDIT CARD PAYMENT

**TOTAL PAYMENTS FOR MARCH**

14,050.37

**1,987,577.51**

57,415.24

**57,415.24**

**15,688,643.53**

# STATEMENT OF FINANCIAL ACTIVITY

OCM 9/5/2013 Item 15.2 Attach 1

for the period ended 31 March 2013

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
<b>Operating Revenue</b>						
Governance	63,022,184	62,044,718	2%	977,466 ✓	63,940,162	64,033,597
Financial Services	618,610	457,821	35%	160,790 ✓	581,100	581,100
Information Services	450	2,727	-83%	(2,277)	3,627	3,627
Human Resource Management	155,014	91,102	70%	63,912	145,395	130,340
Library Services	45,947	36,840	25%	9,108	47,601	44,791
Community Services	5,540,303	5,783,434	-4%	(243,132) ✗	6,642,301	6,295,066
Human Services	5,601,528	5,081,614	10%	519,914 ✓	6,660,915	6,221,506
Corporate Communications	17,814	11,687	52%	6,126	13,007	8,640
Development Services	3,054,615	2,987,931	2%	66,684	3,770,407	3,126,770
Planning Services	953,104	1,107,147	-14%	(154,043) ✗	1,342,615	1,236,715
Waste Services	30,178,054	28,732,810	5%	1,445,245 ✓	34,135,492	31,994,738
Parks & Environmental Services	256,722	200,470	28%	56,252	202,163	6,760
Engineering Services	316,928	242,399	31%	74,529	286,545	176,560
Infrastructure Services	324,959	261,260	24%	63,698	263,319	8,150
	110,086,232	107,041,961	3%	3,044,271	118,034,649	113,868,360
Less: Restricted Grants & Contributions b/fwd	(2,611,520)	(2,739,963)	-5%	128,443	(2,739,963)	-
<b>Total Operating Revenue</b>	<b>107,474,712</b>	<b>104,301,998</b>	<b>3%</b>	<b>3,172,713</b>	<b>115,294,686</b>	<b>113,868,360</b>
<b>Operating Expenditure</b>						
Governance	(2,590,907)	(2,651,392)	-2%	60,485	(3,641,209)	(3,456,151)
Financial Services	(3,755,168)	(3,795,386)	-1%	40,218	(4,462,879)	(4,471,879)
Information Services	(2,797,636)	(2,986,803)	-6%	189,167 ✓	(3,973,598)	(3,881,598)
Human Resource Management	(1,538,257)	(1,623,906)	-5%	85,649	(2,160,690)	(2,189,739)
Library Services	(1,854,070)	(2,058,515)	-10%	204,445 ✓	(2,831,632)	(2,783,692)
Community Services	(6,104,441)	(6,709,559)	-9%	605,118 ✓	(8,947,362)	(8,317,725)
Human Services	(5,521,712)	(5,746,563)	-4%	224,851 ✓	(7,743,811)	(7,350,808)
Corporate Communications	(1,578,740)	(1,924,458)	-18%	345,718 ✓	(2,519,511)	(2,429,044)
Development Services	(3,087,487)	(3,377,413)	-9%	289,926 ✓	(4,727,439)	(4,232,525)
Planning Services	(983,757)	(1,454,659)	-32%	470,902 ✓	(1,862,616)	(1,774,180)
Waste Services	(14,870,379)	(14,049,329)	6%	(821,051) ✗	(18,596,176)	(17,902,061)
Parks & Environmental Services	(7,160,310)	(8,003,504)	-11%	843,194 ✓	(10,700,332)	(10,406,522)
Engineering Services	(6,798,454)	(7,174,266)	-5%	375,812 ✓	(9,051,293)	(7,553,872)
Infrastructure Services	(5,761,571)	(5,847,029)	-1%	85,457	(7,729,661)	(7,448,857)
	(64,402,890)	(67,402,781)	-4%	2,999,890	(88,948,208)	(84,198,652)
Less: Net Internal Recharging	2,394,945	2,338,155	2%	56,789	3,117,425	3,115,859
Add: Reverse Impairment Charge - Investments	-	-	0%	-	-	-
Add: Depreciation on Non-Current Assets	-	-	-	-	-	-
Computer & Electronic Equip	(198,048)	(155,088)	28%	(42,960)	(206,784)	(206,784)

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 March 2013

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Furniture & Equipment	(135,243)	(135,882)	0%	639	(181,143)	(181,143)
Plant & Machinery	(2,267,824)	(2,442,069)	-7%	174,245 ✓	(3,256,091)	(3,256,091)
Buildings	(2,369,600)	(2,539,521)	-7%	169,921 ✓	(3,386,022)	(3,386,022)
Roads	(6,625,047)	(6,750,000)	-2%	124,953 ✓	(9,000,000)	(10,500,000)
Drainage	(1,606,456)	(1,710,000)	-6%	103,544 ✓	(2,280,000)	(2,280,000)
Footpaths	(798,787)	(657,954)	21%	(140,833) ✗	(877,274)	(877,274)
Parks Equipment	(1,438,617)	(1,320,006)	9%	(118,611) ✗	(1,900,000)	(1,480,000)
	<b>(15,439,622)</b>	<b>(15,710,520)</b>	<b>-2%</b>	<b>270,898</b>	<b>(21,087,314)</b>	<b>(22,167,314)</b>
<b>Total Operating Expenditure</b>	<b>(77,447,568)</b>	<b>(80,775,145)</b>	<b>-4%</b>	<b>3,327,577</b>	<b>(106,918,098)</b>	<b>(103,250,107)</b>
<b>Change in Net Assets Resulting from Operations</b>	<b>30,027,144</b>	<b>23,526,853</b>	<b>28%</b>	<b>6,500,291</b>	<b>8,376,588</b>	<b>10,618,253</b>
<b>Non-Operating Activities</b>						
<b>Profit/(Loss) on Assets Disposal</b>						
Plant & Machinery	173,217	(117,381)	-248%	290,598 ✓	(128,364)	(315,364)
Freehold Land	18,227,875	16,728,987	9%	1,498,889 ✓	17,590,909	1,175,000
Furniture & Office Equipment	(431)	-	0%	(431)	-	-
Buildings	(264,070)	-	0%	(264,070) ✗	-	15,000
	<b>18,136,592</b>	<b>16,611,606</b>	<b>9%</b>	<b>1,524,986</b>	<b>17,462,545</b>	<b>874,636</b>
<b>Less: Underground Power Infrastructure Contribution</b>	<b>(3,360,034)</b>	<b>(3,417,000)</b>	<b>-2%</b>	<b>56,966</b>	<b>(5,025,000)</b>	<b>(5,025,000)</b>
<b>Asset Acquisitions</b>						
Land and Buildings	(18,239,282)	(30,075,329)	-39%	11,836,047 ✓	(43,834,544)	(35,818,923)
Infrastructure Assets	(11,691,992)	(16,901,712)	-31%	5,209,720 ✓	(24,096,665)	(17,259,411)
Plant and Machinery	(3,200,562)	(4,682,061)	-32%	1,481,499 ✓	(5,543,561)	(3,627,000)
Furniture and Equipment	-	(11,736)	-100%	11,736	(11,736)	(40,000)
Computer Equipment	(1,322,029)	(2,605,827)	-49%	1,283,798 ✓	(2,984,727)	(1,167,500)
<b>Note 1.</b>	<b>(34,453,865)</b>	<b>(54,276,665)</b>	<b>-37%</b>	<b>19,822,800</b>	<b>(76,471,233)</b>	<b>(57,912,834)</b>
<b>Add: Transfer to Reserves</b>	<b>(10,931,279)</b>	<b>(24,476,283)</b>	<b>-55%</b>	<b>13,545,004</b> ✓	<b>(53,604,672)</b>	<b>(31,392,984)</b>
	<b>(581,443)</b>	<b>(42,031,489)</b>	<b>-99%</b>	<b>41,450,046</b>	<b>(109,261,772)</b>	<b>(82,837,928)</b>
<b>Add Funding from</b>						
Grants & Contributions - Asset Development	6,676,343	9,830,928	-32%	(3,154,585) ✗	12,032,331	10,936,929
Less: held in restricted funds from prior years	(236,916)	(429,655)	-45%	192,739 ✓	(439,655)	(219,500)
Proceeds on Sale of Assets	21,208,760	22,803,987	-7%	(1,595,226) ✗	23,693,909	7,106,000
Reserves	28,339,041	41,834,931	-32%	(13,495,890) ✗	55,965,662	38,638,204
Loan Funds Raised	-	1,000,000	-100%	(1,000,000) ✗	4,865,000	4,865,000
Contributed Developer Assets	-	-	0%	-	-	-
	<b>55,405,786</b>	<b>33,008,701</b>	<b>68%</b>	<b>22,397,084</b>	<b>(13,144,526)</b>	<b>(21,511,296)</b>

# STATEMENT OF FINANCIAL ACTIVITY

for the period ended 31 March 2013

	Actuals	YTD Revised Budget	Variance to YTD Budget	\$ Variance to YTD Budget	Revised Budget	Adopted Budget
	\$	\$	%	\$	\$	\$
Less: Transfer from Reserves - Impaired Investments	-	-	0%	-	-	-
<b>Non-Cash/Non-Current Item Adjustments</b>						
Depreciation on Assets	15,439,622	15,710,520	-2%	(270,898) X	21,087,314	22,167,314
Profit/(Loss) on Assets Disposal	(18,136,592)	(16,611,606)	9%	(1,524,986) X	(17,462,545)	(874,636)
Non-Current Accrued Debtors	-	-	0%	-	-	-
Non-Current Leave Provisions	512,549	-	0%	512,549 ✓	-	-
Net Change in Restricted/Committed Cash	2,848,436	3,169,618	-10%	(321,182) X	3,179,618	219,500
Deferred Pensioners Adjustment	-	-	0%	-	-	-
	<b>56,069,802</b>	<b>35,277,234</b>	<b>59%</b>	<b>20,792,568</b>	<b>(6,340,139)</b>	<b>882</b>
Opening Funds	6,355,407	6,355,912	0%	(504)	6,355,912	-
<b>Closing Funds</b>	<b>Note 2, 3.</b>	<b>62,425,208</b>	<b>50%</b>	<b>20,792,063</b>	<b>15,773</b>	<b>882</b>
	-	-		-	-	-





## City of Cockburn Road Safety and Travelsmart Reference Group

### Terms of Reference

TERM OF REFERENCE ADMINISTRATION	
DOCUMENT CODE:	XXXX
DIRECTORATE:	Engineering and Works Services
BUSINESS UNIT:	Engineering
SERVICE UNIT:	Road Construction and Maintenance
RESPONSIBLE OFFICER:	John McDonald
FILE NO.:	
DATE FIRST ADOPTED:	9 May 2013
DATE LAST REVIEWED:	
ATTACHMENTS:	
VERSION NO.	1

GOVERNANCE REFERENCE	
STATUTORY COMPLIANCE	NIL
INDUSTRY COMPLIANCE:	WALGA RoadWise Program
ORGANISATION COMPLIANCE:	Policy
PROCESS LINKS:	Corporate Procedure – Meeting Management - Reference Group

## VISION

The vision of the City of Cockburn Road Safety and Travelsmart Reference Group is for the City of Cockburn to have a robust, safe and integrated transport network that meets people and industry needs while minimising environmental impacts.

## GUIDING PRINCIPLES

This Reference Group has been established with the guiding principles to:

- Promote an integrated transport system which balances environmental impacts and community needs;
- Raise community awareness of road safety issues and initiatives in local communities;
- Review road safety strategies that may be adopted by the City of Cockburn, Main Roads WA, the Western Australian Police Service or any other statutory authority that has the ability to influence road safety in the community;
- Identify community concerns about road safety and road safety issues, potential black spot projects and poor road user behaviour and develop initiatives to address these identified road safety issues;
- Facilitate and promote healthy transport opportunities by promoting the City's TravelSmart initiative and implementation of walkway, bike and trails master plans;
- Identify a holistic regional approach to freight management.

## DELEGATED AUTHORITY

The City of Cockburn Road Safety and Travelsmart Reference Group will not have any authority to act on behalf of the City of Cockburn. The Reference Group cannot direct staff, call tenders, award contracts, expend monies, direct volunteers or do anything that is the responsibility of the City of Cockburn.

## ESTABLISHMENT AND MEMBERSHIP

The Reference Group is to be established and Membership appointed by Council in accordance with these Terms of Reference. The membership of the Road Safety and Travelsmart Reference Group shall generally comprise the following:

- Up to four (4) elected members as delegates of the City of Cockburn. The Elected Member representation will consist of the Mayor (or his delegate) and an elected Member from each Ward.
- One (1) WALGA RoadWise representative
- Up to six (6) representatives of organisations relevant to the promotion of road safety issues, which may be drawn from groups such as the following:

- Western Australian Police Service
- Main Roads Western Australia
- Travelsmart Officer
- Youth Advisory Committee (YAC representative)
- Emergency Services
- Associations (Road User or Road Safety Group representative)

### **PRESIDING MEMBER**

The presiding member shall be appointed by the Reference Group at its inaugural meeting under a procedure general agreed to by members present.

Where the Presiding Member is unable to attend a scheduled meeting of the Reference Group, an Acting Presiding Member is to be elected from the membership in attendance and is to preside for the duration of that meeting only.

The Presiding Member is responsible for the good and reasonable conduct of Reference Group meetings and shall determine the meeting procedures as required.

### **MEETINGS**

Meetings will generally be held on a quarterly basis in February, May, August and November, with the start time and venue being determined by the Group. The Group will however determine meeting frequency based on the level of business required to be transacted.

### **DEPUTATIONS**

With the prior approval of the Presiding Member any person/s or organisation/s may be invited to attend a meeting. At the discretion of the Presiding Member such person/s may address the meeting and/or contribute to discussion. They are however not entitled to vote on any decision arising at such meetings.

### **SUB REFERENCE GROUP**

The Reference Group may appoint a Sub-Reference Group of its members to undertake a scope of work determined by the Group. Such work is to be at the direction of the Reference Group and be within the purpose of this Terms of Reference and the activities and budget allocations approved by Council.

### **CONDUCT OF MEMBERS**

Members will act in good faith, with honesty and integrity, demonstrating due regards for the authority of the Presiding Member and courtesy and respect in their dealings with all persons whilst undertaking the activities of the Road Safety and Travelsmart Reference Group.

### **DECLARATION OF MEMBERS INTEREST**

Members of the Reference Group are bound to disclose their interests on any issue which may influence their impartiality as determined by their financial or other likely involvements.

### **QUORUM**

The quorum of any meeting shall be a half plus one of the number of appointed members.

### **ATTENDANCE AT MEETING**

Members of the Reference Group shall endeavor to attend all scheduled meetings of the Reference Group.

A member unable to attend a meeting of the Reference Group shall inform either the Presiding Member or the City's nominated officer of his or her inability to attend and the reason for such, prior to the scheduled commencement of the meeting.

Where a member is absent without reasonable grounds throughout three (3) consecutive meetings of the Reference Group, the continuance of that membership position may be referred to Council for determination in order to ensure that the effective operation of the Reference Group is supported through sufficient membership participation.

### **VOTING**

Voting shall be by consensus of the members present or by a simple majority if deemed necessary by the Presiding Member.

### **MINUTES**

Minutes or notes of the Reference Group meetings shall be kept as a record of the Group's activities. Where a resolution of the Reference Group is required to be endorsed by Council, it will be subject to an officer report to an appropriate Meeting of the City of Cockburn.

### **ADMINISTRATIVE SUPPORT**

The Traffic and Transport Engineer is the officer nominated to provide administrative support to the Reference Group. All activities and communications will be coordinated through the Traffic and Transport Engineer. All enquiries and requests for support should be directed through this officer.

**Rockingham Road****Using warrants criteria and weightings**

Reference: Guide to Traffic Engineering Practice Part 10 - City of Stirling Model

Road name: **ROCKINGHAM Road**  
 Location detail: **Between Lancaster Street and Coleville Crescent**  
 Road hierarchy: **District Distributor A**  
 Reason for analysis: Requested by Jadranka

Investigation officer: Stephen Lim  
 Analysis officer: Stephen Lim

Investigation date: 18/04/2013  
 Analysis date: 18/04/2013

**TABLE 1 – TRAFFIC PARAMETER SCORE****Note:** Maximum road length for each analysis = 500 metres

<b><u>PARAMETER</u></b>	<b><u>VALUE</u></b>	<b><u>SCORE</u></b>	<b><u>REMARKS</u></b>
85 <sup>th</sup> Percentile speed	65.2	2	Traffic count Feb 2013
Traffic volume (AWT)	17470	0	Traffic count Feb 2013
Traffic as Peak-hour percentage of 24 hours volume	8%	0	
Heavy vehicle percentage of total traffic flow	4.0%	0	Ausroads class 3 or greater
Crash data 2012 (5 year period)	- Fatalities	0	0
	- Injuries	10	8
	- Non-injuries	48	19.2
Topography	- Restricted sight	No	0
	- Steep grade	No	0
	- Long straight	Yes	0
	Tight bend	No	0
Activity generators	- Passive reserve	No	0
	- Active reserve	No	0
	- Normal residential <=R25	Yes	1
	- Medium residential >=R30	No	0
	- Primary School	No	0
	- Secondary School	No	0
	- College	No	0
	- Small retail/industrial	No	0
	- Large retail/industrial	Yes	12
	- Bicycle crossing	No	0
	- Major bicycle route	No	0
	- Major on-street parking	No	0
	- Major pedestrian crossing	No	12

**Sub total: 54.2**

### TABLE 2 - REDUCTION SCORE

*Note: Score(s) to be omitted when existing remedial treatment(s) can clearly be demonstrated to be ineffective*

<u>PARAMETER</u>	<u>VALUE</u>	<u>SCORE</u>	<u>REMARKS</u>
Mid-Street treatments			
- Pedestrian/cycle refuge	Yes	-4	
- Slow point	No	0	
- Embayed parking	No	0	
- Part road closure	No	0	
- Hump/plateau	No	0	
- Continuous median	Yes	-5	
- 40km/h School Zone	No	0	
Intersection treatments			
Traffic island	Yes	-5	
Roundabout	No	0	
Threshold	Yes	-4	
Part road closure	No	0	
Stop/Give Way	Yes	-3	
<b>Sub total:</b>		<b>-21</b>	
<b>OVERALL SCORE:</b>			<b><u>33.2</u></b>

### TABLE 3 – ACTION PRIORITY

Overall score is below 40, no consideration will be given for traffic calming treatment.

Comments:

Overall score 33.2 - consider there is insufficient safety or amenity concern to be listed for any future treatment.

Please be noted that this assessment is based on Districtor Distributor B criteria

**Table 1. WARRANT CRITERIA AND WEIGHTINGS**

Traffic Parameter	Range / Item	Point scores for each parameter		
		Local Road	Local Distributor	District Distributor B
Traffic Speed as 85th percentile relative to speed limit	At or below speed limit	0	0	0
	+1 to 4km/h	6	2	1
	+5 to 7km/h	11	4	2
	+8 to 10km/h	20	7	4
	+11 to 14km/h	30	12	7
	+15 to 19km/h	42	20	12
	+20km/h	55	35	20
Traffic volume in vehicles per day (Average Weekday Traffic Flow)	Less than 1000	0	0	0
	1000 – 1499	4	0	0
	1500 – 1999	7	0	0
	2000 – 2499	10	0	0
	2500 – 2999	14	0	0
	3000 – 3999	18	4	0
	4000 – 4999	24	7	0
	5000 – 5999	30	12	0
	over 6000	39+ 9 per 1000	18+ 7 per 1000	0
Peak hour traffic volume as Percent of 24-hour Volume	10% - 10.9%	0	0	0
	11% - 11.9%	2	1	0
	over 12%	4	2	0
Heavy vehicles per percent of total traffic flow (Austroads Class 6 or greater)	Points per percentage Over 3%	1	1	0
Crash Data (5-Year Period)	Per Fatality	2	2	2
	Per Injury	0.8	0.8	0.8
	Per non-injury	0.4	0.4	0.4
Topography	Restricted sight	4	5	6
	Steep Grade	4	1	1
	Long Straight	5	2	0
	Tight Bend	3	2	3
Activity Generators	Passive reserve	1	1	1
	Active reserve	2	2	1
	Normal residential	1	1	1
	Medium residential	2	2	2
	Primary school	6	8	15
	Secondary school	6	8	12
	College	10	10	12
	Small retail/indust.	6	8	10
	Large retail/Indust.	8	10	12
	Bike crossing	3	5	7
	Major bike route	4	6	8
	Maj. On-street parking	10	10	10
	Maj Ped. Crossing	5	8	12

**Table 2. REDUCTION SCORES**

**Note:** Score(s) to be omitted when existing remedial treatment(s) can be clearly demonstrated to be ineffective.

Traffic Parameter	Range / Item	Point scores for each parameter		
		Local Road	Local Distributor	District Distributor B
Mid-street Improvements	Ped/Cycle Refuge	-4	-4	-4
	Slow Point	-8	-8	N/A
	Embayed Parking	-6	-6	-6
	Part Road Closure	-8	-8	-8
	Hump/Plateau	-8	-8	N/A
	Continuous Median	-5	-5	-5
	40km/h School Zone	-3	-3	N/A
Intersection Treatments	Traffic island	-5	-5	-5
	Roundabout	-8	-8	-8
	Threshold	-4	-4	-4
	Part Road Closure	-8	-8	-8
	Stop/Give Way	-3	-3	-3



**Table 3. ACTION PRIORITY**

Total Point Score	Comment	Action
<b>Greater Than 60</b>	Considered to be significant traffic management issue - Denoted as Problem Site.	<ul style="list-style-type: none"> <li>- List on traffic calming priority register</li> <li>- Prepare concept treatment details</li> <li>- Item to Council advising of funding and staging options</li> <li>- Issue concept to stakeholders for comment</li> </ul>
<b>41 to 60</b>	Considered to be moderate traffic management issue - Denoted as Technical Problem Site.	<ul style="list-style-type: none"> <li>- List on traffic calming priority register</li> <li>- Nominate project for consideration in next financial year budget</li> </ul>
<b>21 to 40</b>	Considered a minor difficulty but not sufficiently serious to warrant funding (even in the long term).	<ul style="list-style-type: none"> <li>- No action required</li> </ul>
<b>20 or less</b>	Considered there is insufficient safety or amenity concern to be listed for any future treatment.	<ul style="list-style-type: none"> <li>- No action required</li> </ul>

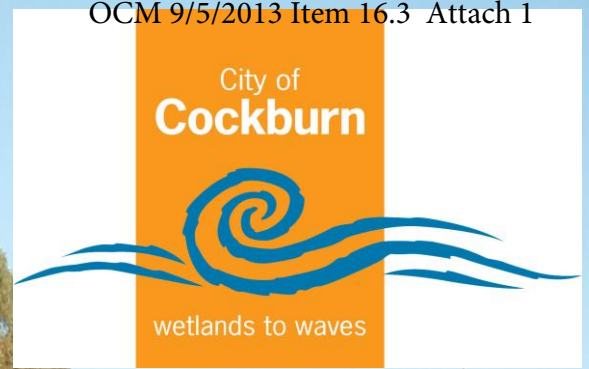


MapXtreme 2008 © SDK Developer License, © 2009 Pitney Bowes Software Inc.



 <p><b>City of Cockburn</b> GIS Services Department</p>	<p><b>Rockingham Road, Spearwood Phoenix Rd to Spearwood Ave</b></p>	<p>PRINTED ON: Tuesday, 30 April 2013</p> <p>DISCLAIMER - The City of Cockburn provides the information contained herein and bears no responsibility or liability whatsoever for any errors, faults, defects or omissions of information contained in this document.</p>	<p>SCALE = 1:5000</p>	 <p><b>NORTH</b></p>
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The background of the cover page is a photograph of a large green sports field. Several water sprinklers are active, creating multiple arcs of water spraying across the grass. In the background, there are tall trees and a small building with a green roof. The sky is clear and blue.

# **CITY OF COCKBURN**

# **WATER**

# **CONSERVATION**

# **PLAN**

## **2013 - 2018**

Document Information:			
Document Title: Water Conservation Plan 2013 - 2018			
Prepared by:	Anton Lees, City of Cockburn		
Doc. Status:	Final	Version:	1.0

<b>Version History:</b>			
Version:	Description:	Issue Date:	Authorised By:
1.0	Draft		Manager Parks & Environment
2..0	Endorsed by Council		Council

## EXECUTIVE SUMMARY

As a Waterwise Council, the City of Cockburn recognises the importance of the sustainable use of water within its Public Open Space (POS) and the need to promote water conservation and water efficiency within the community. Sustainable water management will ensure the protection of the groundwater and scheme water resources to POS, including Streetscapes, in a changing environment.

The Water Conservation Plan 2013 – 2018 has been developed to provide strategic direction in water conservation and water quality improvement initiatives within the City's POS. This Plan reviews and builds on the actions emanating from the 2007 Water Conservation – A Sustaining Strategy. The key actions to be implemented over the next 5 years include;

1. Ensure developers have a licenced water allocation for the POS associated with the subdivision development and the licence is transferred to the City and at the expiration of the maintenance period;
2. Adopt the City of Cockburn's Irrigation Operating Strategy April 2011- April 2014 and the Hammond Road Sporting Complex Irrigation Operating Strategy Sept 2011-Sept 2014;
3. The City adopts hydrozoning principles to Public Open Space;
4. The City adopts a uniformity coefficient (CU) of >80% for all reticulated open space;
5. The City continues to implement optimum irrigating operation conditions;
6. The City adopts adaptive irrigation scheduling for all irrigation systems;
7. The City continues to monitor Groundwater Abstraction, Scheme Water Usage and standing groundwater on all production bores and report annually to the Department of Water
8. The City undertake a comprehensive review of suitable Central Control Systems and receive a report by December 2013 on the preferred Central Control System;
9. Investment in a Weather Station in line with the preferred Central Control System;
10. The City continue to invest in Soil Monitoring Devices
11. The City complete Milestone 4 of the ICLEI campaign and commence works to achieve Milestone 5 and maintaining Waterwise Council endorsement
12. The City continue to engage with the Department of Water on the "First in First Served" policy review;
13. The City reduces its groundwater abstraction levels to achieve the City's Sustainability Action Plan 2013/14, Environment 5.1 KPI "To improve efficiency in corporate groundwater use by reducing consumption by 10 percent below the 207/08 DoW allocations per hectare by 2017/18"
14. The City continues to monitor the Port Coogee Groundwater Interception resource and consider the availability of this resource for future strategies;
15. The City continue to adopt the annual maintenance budgets;
16. The City will implement the irrigation asset renewal program outlined in the Parks & Environment Asset Management Plan and annual budget submissions.
17. The City will review the plan and report on performance against targets through an annual report. The next report will be September 2014.

The Water Conservation Plan 2013 – 2018 demonstrates the City's' commitment to a proactive and better managed water resource through sound policies and guidelines.



## 1.0 Purpose

The Water Conservation Plan 2013 – 2018 provides a coordinated approach for the City to sustainably manage water resources within City operations and education objectives. Our water conservation plan is based on the following goals and objective

***Goal – to provide high quality recreational and aesthetic amenity for the community of Cockburn in a manner the conserves groundwater resources***

***Objective – reduce groundwater consumption to volumes less than that allocated to the City of Cockburn by the Department of Water***

The implementation of the Water Conservation Plan 2013 – 2018 will allow the City to demonstrate leadership in meeting its water conservation and water quality improvement targets and create community awareness regarding the need to manage water resources for the future.

### 1.1 Strategic Alignment – City of Cockburn

The City of Cockburn *Strategic Community Plan (2012-2022)* was developed in conjunction with the community and guides the City's strategic directions and priorities to 2022. The Community Strategic Plan commits to the following actions on Water Conservation:

- Implement sustainable resource management strategies;
- Promote sustainable practices in the community;
- Adopt best practice management for our natural environment;
- Actively pursue remediation and adaptation strategies in areas where the natural environment is at risk;
- Implement energy management strategies; and
- Develop infrastructure provisions and renewal strategies that direct investment in ongoing infrastructure provisions and management

This strategy is linked to a number of other strategic documents including the:

- Sustainability Strategy;
- Climate Change Adaptation Plan;
- Local Water Action Plan 2011 - 2017;
- Long Term Financial Plan 2012/13 – 2022/23
- Parks & Environment Asset Management Plan 2013

### 1.2 Strategic Alignment – WA State Government

The Western Australian state government has implemented a number of initiatives to manage the availability of water and how it is used by the community. Water restrictions have been enacted by the state government and education campaigns have been run through both state and local governments. The state's water utility, Water Corporation, has identified the reduction in water availability and has constructed two desalination

plants which provide 30% of water through the integrated water supply scheme. The Western Australian government has implemented the following strategies to improve water management:

- State Planning Policy 2.9, Water Resources, 2006;
- State Water Strategy, 2003;
- State Water Plan, 2007;
- Water Forever: Towards Climate Resilience, 2009;
- Storm Water Management Manual;
- DoW Strategic Plan 2010-2015;
- Urban Water management Plans, 2008; and
- Water Sensitive Urban Design

### **1.3 Legislative Water Reform**

The Department of Water (WA) is currently undertaking extensive legislative water reform that will address:

- Implement the Water Reform Program and National Water Initiatives in Western Australia
- Modernise and consolidate water resource management and water service delivery legislation; and
- Optimise operational efficiency and streamline processes

### **1.4 Regulation**

The Western Australian State Government has introduced a number of measures in recent years to ensure Local Governments take action to reduce groundwater abstraction. Measures include monitoring of groundwater abstracted and daytime and winter sprinkler bans. All local governments that acquire a groundwater license are required to develop Water Conservation Plans and Operating Strategies. Future regulation is being developed with specific consideration of the “First in First Served” water allocation policy, and water licencing and usage charges.

## **2.0 Background**

The City of Cockburn has demonstrated its commitment to sustainable water management through the implementation of the Water Conservation Strategy 2007 which outlined the delivery of projects and programs which improved water efficiency and quality. Further restrictions on water availability are proposed to be applied by the Water Corp and Department of Water in response to the need to adapt to climate change. The confirmation that Perth continues to have a reduction in rainfall and increase in average temperatures has necessitated the focus to manage our impact on the environment more diligently.

Public consciousness of water saving issues is extremely high and it is anticipated that Local Government Authorities shows leadership, strong water governance and promote water saving initiatives to the community.

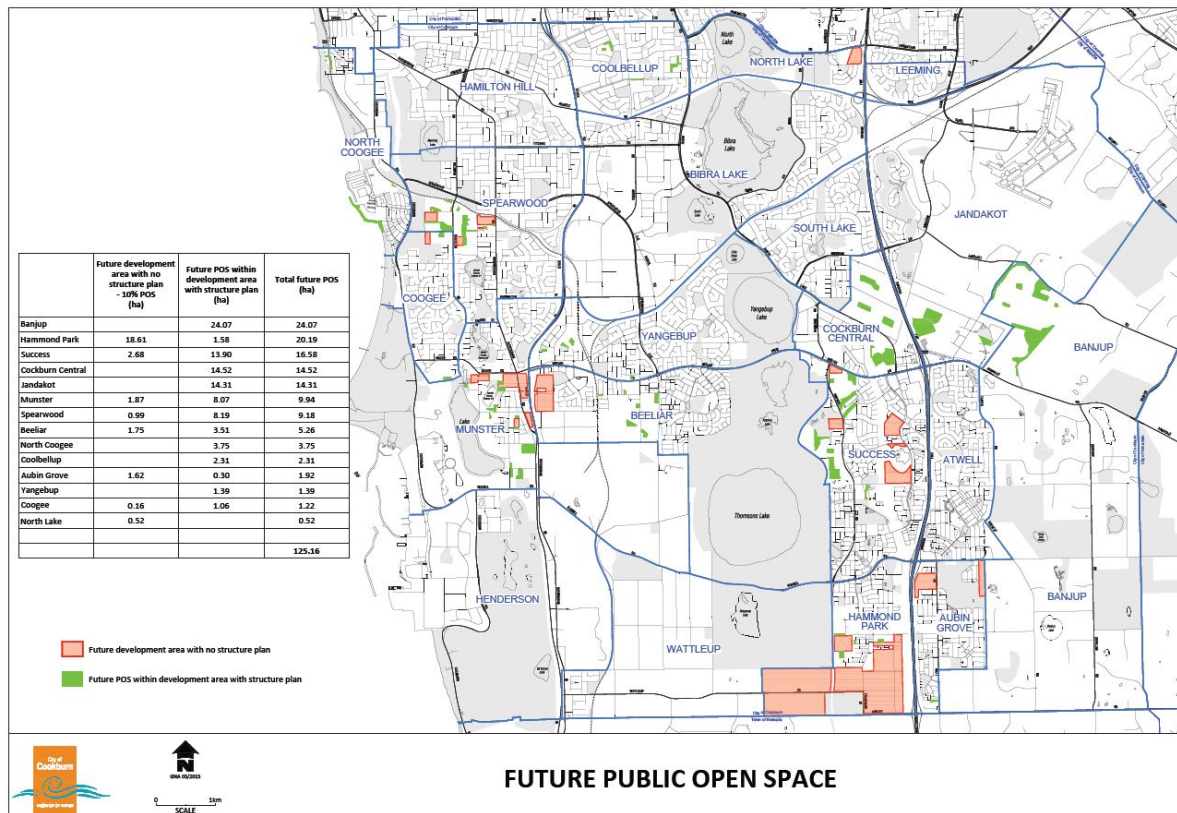
## 2.1 Future Public Open Space

The City of Cockburn is one of WA's major coastal cities encompassing an area totalling 170 square kilometres comprising a diverse mix of residential, retail, commercial, industrial and rural areas. The City has at its heart a chain of five lakes running north to south with the larger ones being Yangebup and Thompson Lakes.

The City of Cockburn is within the south west growth corridor with a current population of 91,000 (2011) which is expected to exceed 100,000 by 2013. To meet this anticipated growth in population, the City has developed strategies for the development of greenfield lots and redevelopment (infill) of older areas. The City can expect to receive 125ha of POS from future development areas with the majority of these areas requiring groundwater for irrigation purposes

The City of Cockburn "Recreation Strategy" includes the provision of the following major Regional Community Infrastructure projects which include the provision of POS:

- Cockburn Central West Regional Recreation Facility ;
- Golf Complex North Coogee;
- Frankland Park Sport and Recreation Facility;
- Sport and Recreation Facility Cockburn Coast
- Sport and Recreation Facility Banjup



## **2.2 Water Sources**

The City primarily relies on groundwater resources for the irrigation of public open space and streetscape. Scheme water is accessed where the availability of groundwater is limited or the quality will have adverse effects on the landscape treatment. Both groundwater and scheme water are under pressure from climate changes and the reduction in rainfall over the past century.

### **2.2.1 Groundwater**

The City currently has eight (8) groundwater licences (GWL 49535, GWL 49549, GWL 110703, GWL 62672, GWL 99188, GWL 99722, GWL 49545 and GWL 151752) for irrigation of Public Open Space, Streetscapes and Community Facilities. The GWL's are located in the Department of Water's Cockburn, Jandakot and Perth Groundwater areas.

### **2.2.2 Scheme Water**

Scheme Water is accessed via the Integrated Water Supply System (IWSS) for the irrigation of small POS or Community Facilities. The IWSS is supported by three sources; Dams (rainfall, surface water), Desalination (conversion of sea water) and groundwater (Leederville, Jandakot, etc.)

### **2.2.3 Stormwater**

Stormwater has traditionally been collected and conveyed to water storage bodies otherwise known as dams or aquifers recharge basins without any treatments. The City has incorporated Water Sensitive Urban Design principles to prevent untreated water entering the surrounding ground and through to the underlying groundwater. The implementation of treatment swales in new POS and relocation of older drainage basins into existing POS has enabled the recharge of the aquifer with clean water. This system of aquifer recharge is not recognised by the Department of Water and cannot be offset against our groundwater allocations.

### **2.2.4 Rainwater**

The collection of Rainwater via roof tops or larger hard surfaced areas is currently under investigation as a potential alternative water source. The City's design for the new Integrated Community Facility, Success includes for the collection of rainwater tanks for fire fighting and potential use for irrigation to the landscape around the complex.

## **2.3 Climate Change**

Australia is the driest inhabited continent in the world, yet our domestic per capita water consumption is amongst the highest in the world.

Reduced rainfall, rapid population growth and increasing groundwater extraction are putting significant demands on the quality and availability of our water resources.



Climate Change is likely to result in further declines in rainfall over the South West of Western Australia, which will have a significant impact on our lifestyle and environment.

Over the past 40 years, rainfall patterns have changed in Australia, with the south becoming drier and the north becoming wetter. The Australian summer over 2012 and 2013 was the hottest on record and shows that climate change is already adversely affecting Australians. During this time temperature records were set in every state and territory in Australia and Perth experienced seven consecutive days above 37 °C from the 25th – 31st December 2012.

These changing conditions are affecting our ecosystems and water supplies. The City of Cockburn recognises its responsibility to take action on climate change and has developed a Greenhouse Gas Emission Reduction Plan and Climate Change Adaptation Action Plan.

The City's Climate Change Adaptation Action Plan includes 9 climate change risks. Of these, only two risks have been related as extreme which means they are almost certain to occur with catastrophic consequences to the Council Service it serves. Each of these extreme risks relates to reduced water availability:

Risk	Risk Source	Specific Consequences
Reduced water availability for watering park areas	Reduced Rainfall	Lack of vegetation resulting in closure of sports fields Impact on community lifestyle Complaints from community regarding field quality
Reduced water available for natural wetlands	Reduced Rainfall	Loss of biodiversity (thousands of species within area) Possible loss of wildlife corridor. Some pest species will be reduced (e.g. midges). Community complaints. Detrimental effect on other plant species (e.g. Banksias)

The Water Conservation Plan will assist in mitigating these climate change risks.

### 3. Actions

The following have been identified in this strategy which will assist in the City in delivering a sustainable management approach to water conservation.

#### **MEETING THE DEMAND**

The City is currently managing 320ha of irrigated POS, which has the potential to increase by an additional 125ha based on future development. The increase in irrigated area will predominately be undertaken via developers through the subdivision process whereby POS land is identified and depending on the embellishment level required a licence to abstract groundwater from Department of Water is generated. Granting of allocations for new development areas is dependent on the amount of groundwater available and determined at the time a license application is lodged through the “First in First Served” policy. There are no guarantees that licenses will be granted and there are no special conditions or allocations set aside for new development areas.

The City’s licences are due for renewal in 2014 however a licence does not guarantee that water is available for allocation. During drought periods restrictions are applied so that the available resource is shared and the damage to the environment, resource and users is minimised. Conditions are placed to define how and when water may be taken and to specify obligations the licence holder must meet when using the water.

Requesting a water allocation in advance of applying to the City for a project or development approval is currently not supported by the Department of Water. The DoW’s practice of considering applications on a first-in-first served basis are currently the preferred mechanism for water resource allocation, however this process is subject to a review, which is considered further in this plan. The DoW will only provide water allocation in exceptional situations where the applicant requires a reasonable time to achieve full operating capacity of their development and use of their water entitlement.

The City is required to ensure that its current water allocations are maintained and a mechanism exists to increase the allocation as the area of POS increases. The preferred method for securing required groundwater allocation for new POS is to ensure that all areas of POS associated with new subdivision developments have an approved water allocation from the DoW and that the license is transferred at the expiration of the maintenance period.

#### **ACTION 1:**

- Ensure developers have a licenced water allocation for the POS associated with the subdivision development and the licence is transferred to the City at the expiration of the maintenance period

## **Irrigation Operating Strategy**

The Department of Water licence approval conditions for water abstraction include the requirement for the City to submit an Irrigation Operating Strategy. These strategies are comprehensive and are legally binding on the licensee. The City has two Irrigating Operating Strategies endorsed by the Department of Water:

1. City of Cockburn Irrigation Operating Strategy April 2011 – April 2014: covering GWL's 49535, 49549, 110703, 62672, 99188, 99722 and 49545. This is an amalgamated strategy to enable more efficient management of groundwater.
2. Hammond Road Sporting Complex Irrigation Operating Strategy Sept 2011- Sept 2014: covering GWL 151 752. This single POS strategy was a requirement by the DoW due to the surrounding environment conditions.

The main focus of each strategy is the shut down procedure and hierarchy that the City will invoke, should abstraction of the groundwater exceed current monthly water budgets and / or the Department of Water reduce the current allocation level of 7500kL / ha / per annum. In addition the City is required to submit an annual summary to the DoW detailing the amount of water abstracted under each GWL, the abstraction does not cause excessive draw on the relevant aquifers and specific water quality parameters have not altered significantly over concurrent years.

The key components of the strategy include:

1. Water Source;
2. End Uses;
3. Administration Requirements;
4. Operating Rules;
5. Monitoring Requirements – Water Supply;
6. Monitoring Requirements – Irrigation Systems;
7. Contingency Plans – Local Impact;
8. Contingency Plans – Over use and /or reduction in allocation;
9. Water Use Efficiency – Irrigation Systems;
10. Water Use Efficiency – Scheduling; and
11. Summary of Licence Commitments

### **ACTION 2:**

- Adopt the City of Cockburn Irrigation Operating Strategy April 2011 – April 2014
- Adopt the Hammond Road Sporting Complex Irrigation Operating Strategy Sept 2011- Sept 2014

## **HYDROZONING OF PUBLIC OPEN SPACE**

Hydrozoning is the segregation of open space areas into categories based on water use and demand to enable the irrigation system to be designed for optimal water delivery. Hydrozoning of POS ensures the following key outcomes are achieved:

1. High profile turf areas demand a higher quality of turf than low profile low use areas. Accordingly, the water requirement for each corresponds with the amount of use and aesthetic profile.
2. Different usage patterns and different aesthetic profiles exist at and between different sites.
3. Hydrozoning will enable officers to apply different volumes of water to different areas of turf at individual sites, according to the differing requirements, as opposed to applying the same volume of water to the entire irrigated area of a given site, because of limitations in the irrigation system design.
4. While hydrozoning may not reduce the overall volume of groundwater used, it will enable officers to apply the required volume of water to high use high profile areas and achieve high quality recreational and aesthetic amenity at specific locations.

The total area of irrigated turf will be divided into well-defined categories, based on usage patterns and aesthetic profile requirements. High use, high profile areas will be categorised “High” and low use low profile areas will be categorised “Low”. Sites categorised “High” will receive higher allocations of groundwater compared with sites categorised “Low”.

Each area of turf shall be assigned a hydrozone category. The current categories as outlined in the Water Operating Strategies are as follows:

<b>Hydrozone</b>	<b>Reserve Classification</b>	<b>Water Allocation</b>
High	Sports Ovals & High Profile Regional Parks	9,100 kl/ha
Medium	Sports Oval surrounds, Neighbourhood and Local parks	6,500 kl/ha
Low	Streetscapes	3,000 kl/ha
Dry	Dry park	0 kl/ha

All new POS and Streetscape developments within the City are designed in accordance with the Hydrozoning principles and practices outlined above. In addition the irrigation industry continues to develop new products that enable improvements to the designs of irrigation systems to ensure water optimisation. All existing irrigation systems are continually being refined to achieve hydrozoning with irrigation systems due for renewal is designed in accordance with the hydrozoning principles.

### **ACTION 3:**

- The City adopts Hydrozoning principles to Public Open Space

### **UNIFORMITY COEFFICIENT**

Optimum efficiency of water use is best achieved by ensuring water is very evenly distributed across each Hydrozone. Coefficient of Uniformity (CU) is a method of measuring how uniformly an irrigation system applies water, expressed as a percentage. The higher the number, the more uniform the rate of application. CU is determined by placing catch cups across a turf area and comparing the average precipitation reading and the deviation from the average. Adoption of a uniformity coefficient CU of > 80% will mitigate the following:

1. Where water is applied with less than perfect uniformity, part of the turf will receive more water;
2. If irrigation systems are operated so that the part receiving the most water has its requirements met, then the remainder of the turf sward will be under-irrigated;
3. If irrigation systems are operated so that the part receiving the least water has its requirements met, then the remainder of the turf sward will be over-irrigated;
4. Non-uniform irrigation unavoidably results in some degree of under or over irrigation;
5. Where under watering occurs, the quality of turf is compromised; and
6. Where over watering occurs, water is wasted.

All new POS developments achieve a CU > 80% due to the improved design and products available on the market. All existing irrigation systems are measured every two (2) years to ensure compliance with the CU of > 80% with the results assisting on the update of the irrigation component of the Parks & Environment Asset Management Plan. Renewal of irrigation systems are designed to achieve a uniformity coefficient of not less than 80%.

### **ACTION 4:**

- The City adopts a uniformity coefficient (CU) of >80% for all reticulated open space.

## **OPTIMUM IRRIGATION OPERATION CONDITION**

Maintenance of irrigation systems at optimum operating condition is paramount to ensuring the supply and distribution of water in accordance with the individual system design specifications. Irrigation systems not maintained at optimum operating condition are inefficient and could lead to the following:

1. Not apply water uniformly throughout specific irrigated areas (resulting in a low coefficient of uniformity);
2. Lose water through leakage and breaks;
3. A reduction in the City's identification of vandalism;
4. Identification of a reduction in water supply from the bore: and
5. Identification of electrical equipment failure.

Maintenance programs to ensure irrigation systems are maintained at optimum operating condition are facilitated through One (1) Irrigation Supervisor, Five (5) Irrigation Fitters and the engagement of suitably qualified irrigation service providers. Routine maintenance through pre-determined irrigation inspection programs enables the monitoring of bores, pumps and irrigation infrastructure to meet operational functionality and identifies faults and breakages. To facilitate the City's POS the minimum technical level of service are outlined below:

- Active Ovals – 52 services per annum
- Neighbourhood & Local POS – 26 services per annum
- Streetscapes & Landscapes – 26 services per annum

### **ACTION 5:**

- The City continues to implements optimum irrigating operation conditions

## **ADAPTIVE IRRIGATION SCHEDULING**

Adaptive Irrigation Scheduling is the process used by irrigation system managers to determine the correct frequency and duration of watering based on actuality.

Evapotranspiration is the sum of evaporation and plant transpiration. Evaporation accounts for the movement of water to the air from sources such as the soil, canopy interception, and water bodies. Transpiration accounts for the movement of water within a plant and the subsequent loss of water as vapour through stomata in its leaves. Effective Rooting Depth is the depth of the roots of plants to be watered, which affects how much water can be stored in the soil and made available to the plants.

Adaptive irrigation scheduling limits the following:

1. If irrigating at frequencies and durations that provides more water than the current plant requirements, over watering occurs.
2. If irrigating at frequencies and durations that provides less water than the current plant requirements, under watering occurs.
3. Where over watering occurs, water is wasted.
4. Where under watering occurs, the quality of turf is compromised.

Irrigation system running frequencies and durations will be calculated on a seven-day basis during spring and autumn and on a fourteen-day basis during summer. Schedules shall be calculated from the following data:

1. Precipitation rate of individual irrigation system sprinklers
2. Rainfall events
3. Soil moisture content
4. Evapotranspiration
5. Effective rooting depth of turf
6. Hydrozone allocations.

### **ACTION 6:**

- The City adopts Adaptive Irrigation Scheduling for all irrigation systems

## **MONITORING OF GROUNDWATER AND SCHEME WATER**

### **GROUNDWATER MONITORING**

Monitoring the volume of groundwater abstracted enables the continual comparison with allocations provided under the licence issued by the Department of Water. Monitoring will ensure the current allocation of 7,500 kilolitres per hectare per annum, is not exceeded and water reduction targets are achieved. Monitoring the volumes of water abstracted each month guide the water application rates for the following month/s to achieve the targets identified.



All bores in the City will be upgraded with flow meters that are permanently fitted and shall be read within the first week of each month. A log of records shall be kept for the purpose of comparing monthly and annual water allocations with total volumes of water extracted. An annual report will be submitted by 30<sup>th</sup> September each calendar year to the Department of Water outlining our actual use, per GWL, does not exceed the allocation



Upgrading of bores with flow meters will be accordance with the City's Capital Works Program, which is subject to allocation of funds on an annual basis by Council.

## **SCHEME WATER MONITORING**

Monitoring the volume of scheme water enables comparisons with budgeted allocations and determines the City's costs for this ongoing service. The City has only 6 POS/ Streetscapes/ Community Facility Landscapes accessing scheme water which limits our impact on groundwater. Monitoring the volume of water applied will guide the rates for the following month and provide reference data for bills issued by the Water Corp.

## **PRODUCTION BORE STANDING WATER LEVEL MONITORING**

Monitoring production bores standing water levels enables the timely detection of changes in aquifer water levels and may indicate an adverse environmental impact. Early detection of these potential environmental changes will enable remedial action to minimise adverse outcomes.

Production bore standing groundwater levels will be measured in the last week of September and April of each year. An annual report will be submitted by 30<sup>th</sup> September each calendar year to the Department of Water outlining whether the standing water levels are being impacted by our draw down on the aquifer.

### **ACTION 7:**

- The City continues to monitor Groundwater Abstraction, Scheme Water Usage and standing groundwater on all production bores and report annually to the Department of Water

## **CENTRAL CONTROL SYSTEMS**

Central Control Systems enable real time information on water management at each individual site and collectively across the City. Central Controls Systems are a valuable management tool that integrates a complete system from one source and will provide instant feedback on the irrigation system in the field. The ability to remotely access the system through a mobile phone will enable system configurations are functioning at full capacity and enable immediate shut down of the system in an emergency.

- Manage irrigation scheduling;
- Graph representation of actual water usage against budgeted amount;
- Manage specific water delivery to high use zones and hydrozoning;
- Collate and report on water volumes distributed through each individual station and irrigation system;
- Instant "rain off" of any or all sites;
- Integration of Soil monitoring sensors which can override programs if warranted;



- Identify pump and bore faults;
- Identify power failures; and
- Integration of Weather Station, adjust programs based on current conditions.

The City's irrigation infrastructure specifications have been developed to facilitate the provision of a Rainbird irrigation controller to each new irrigation systems. Rainbird controllers are considered to be one of the better products on the market and have the capacity to connect to a central control system. With 45% of the irrigation systems being controlled via the Rainbird controller it would be prudent to proceed with the next stage of implementation of a central control system. Initial discussions with service providers have determined that the communication links would be via the corporate mobile phone network with a base station located at the Operation Centre. Initial costs for the setup of the central control system are \$50,000, however, further investigation is warranted to ascertain the compatibility of the Rainbird System with the City's existing IS network and considers if there are any integration issues.

### **ACTION 8:**

- The City undertakes a comprehensive review of suitable Central Control System and receives a report by December 2013 on a preferred Central Control System.

## **WEATHER STATIONS**

Weather stations are observation posts where weather conditions are monitored and recorded. Weather stations can be configured to record various environmental data; with the following parameters most commonly recorded:

- Rainfall;
- Air Temperature;
- Relative Humidity;
- Wind Speed;
- Wind Direction;
- Solar Radiation; and
- Evaporation Rate.



Weather stations can be located in all areas and can be configured with solar panels or a direct power supply depending on the situation. Information can be logged every 15mins, hourly or daily with the data stored internally then collected via a personal computer or sent through a digital mobile link / radio link.

Weather stations designed for irrigation systems linked to a central control system enable "real time" weather conditions to be logged, collated, analysed and stored. The retrieved information can adjust the station runtimes according to each days ET (evapotranspiration) and rainfall to ensure only the minimum water is applied to the turf

or garden bed. In addition the system can be configured to shut down on extremely windy conditions to mitigate water wastage.

The installation of a weather station will form a component of the specifications to be developed for the central control system.

### **ACTION 9:**

- The City to invest in a Weather Station in line with the preferred Central Control System

## **SOIL MONITORING DEVICES**

Soil monitoring devices allow the identification of moisture levels and leachates in the soil profile to improve the scheduling of irrigation and mitigate the potential risk of nutrients entering the groundwater.

### **1. Lysimeters**

The City currently has 14 Lysimeters (soil monitoring devices) located in the soil profile of sporting ovals and regional important parks. Lysimeters are designed to catch the leachate passing below the turf root zone. The leachate is collected and stored in a 10 litre container for extraction at the surface using a vacuum pump. The volume of leachate is measured and the leachate is analysed for nitrogen and phosphorus. Monitoring of the quantity and nutrient content of the leachate will ensure that the groundwater is not being compromised by the City's turf management practices.



Lysimeters will be installed to all existing and any new sporting oval, regional reserves and environmentally sensitive locations throughout the City.

### **2. Soil Moisture Monitoring Devices**

Soil moisture monitoring devices are access tubes installed in a grid pattern over a designated area of the POS. The length of the tubes can vary depending on site conditions; typically 70cm tubes are required. Readings are conducted on a monthly basis with each 10cm of soil



profile assessed for water content. A monthly report outlines the moisture levels within the soil profile to assist in irrigation scheduling to ensure the most efficient use of water. Soil moisture devices can be linked with a weather station and connected to a Central Control system to provide a comprehensive approach to water management.

A soil monitoring device will be trialled at Success Reserve, Hammond Rd in the 2013/14 Financial Year.

#### **ACTION 10:**

- The City will continue to invest in Soil Monitoring Devices

### **Water Campaign**

The City of Cockburn is a participant of the International Council for Local Environmental Initiatives (ICLEI) Water Campaign which is a voluntary program which aims to assist in the local government reduce water consumption and improve water quality. The program involves progressing through five milestones, that guide participating councils through a process of local research, policy making, action planning, implementation and evaluation.

Milestone 1:	Undertake a water consumption inventory and water quality checklist
Milestone 2:	Establish a water consumption reduction goal and water quality improvement goal
Milestone 3:	Develop and adopt a local action plan
Milestone 4:	Implement policies and actions to work towards integrated freshwater resource management and quantify the benefits
Milestone 5:	Monitor and report on water consumption reductions, water quality improvements and water management initiatives.

The City has been a participant of the ICLEI water campaign since 2007 and has been able to achieve:

- Completion of Milestone 1
- Completion of Milestone 2
- Completion of Milestone 3
- An 800,000L saving in community water consumption with the Cockburn free home audit and retrofit program; and
- A reduction in corporate groundwater consumption of 17% below the 2007/08 Department of Water licence allocations.
- "Waterwise Council" status endorsed by Water Corp and Dept. of Water.

## **ACTION 11:**

- The City to complete Milestone 4 and commence works to achieving Milestone 5 and maintaining Waterwise Council endorsement.

## **DEPARTMENT OF WATER - FIRST IN FIRST SERVED POLICY**

The Department of Water (DoW) completed a review of the “First in First Served” (FIFS) Policy in October 2011 to ensure that it encourages the highest value use of water and address other contemporary issues. The FIFS approach is a well-established approach to managing multiple applications in many areas of government. This approach is appropriate where water is plentiful and little competition, however a reducing water resource with significant competition and alternative mechanism is required. The FIFS approach does not result in the best outcomes, as once the available water resource reaches full allocation it does not evaluate the applications concurrently and direct water to the highest priority.

The Department of Water has identified the following mechanism that could be used to prioritise applications for unallocated water under the Rights in Water and Irrigation Act 1914 as an alternative to First in First Served approach

- First-in First - Served – refers to a process in which applications for water entitlements are assessed in chronological order or their receipt. Once the resource is fully allocated no further entitlements will be offered.
- Merit Selection – refers to a process in which criteria are developed to assess economic, social, cultural and other values. Each water application will be assessed on the respective merits, with entitlements award until the resource is fully allocated.
- Auctions or tenders – involve a competitive process in which bids are sought from the market. Water would be allocated to the highest bidder.
- Direct Sale – the DoW would nominate a fixed price for water for a specific timeframe. Water users could assess the value of water against the potential project and elect to pay the nominated price if deemed feasible
- Ballot – applications would be “drawn out of a hat” to determine who wins, with water allocated to the first winner out, then the second and so on until the water is fully allocated.

There is no ideal mechanism to prioritise water allocations as each option has advantages and disadvantages. The mechanism adopted need to meet specific objectives to support economic development, direct water to the highest value use, develop strong communities, promote water efficiencies and ensure the cost do not outweigh the benefits.

## **Preferred Approach – Department of Water**

The FIFS approach is cost effective and generally accepted at lower levels of water allocation. The approach is acceptable until the allocation reaches a limit where the deficiencies of the FIFS application become apparent. The DoW proposes that once 70% of the water resource allocation has been reached, through the FIFS approach, market based allocations are used. Auctions are preferred because they directly address water scarcity by allowing the competing market water users to bid for the remaining limited resource. However the DoW may consider alternative mechanisms depending on the specific circumstances. The 70% allocation limit should ensure that appropriate management controls can be implemented before the resource is fully allocated and reduce the risk of recovering entitlements if capacity is exceeded.

## **Preferred Approach – City of Cockburn**

City officers provided the following response on the mechanism to prioritise applications for unallocated water:

- **First-in First-Served**

It is recognised that this allocation option has been used for some time. Council's that are experiencing significant and sustained urban development are concerned that if and when water entitlements in a specific location have been fully allocated, how can necessary and important infrastructure including active and passive open space be developed and maintained to a safe and satisfactory standard that meets the needs of the community.

- **Merit Selection**

Merit selection based on economic, social and cultural values would provide a consistent approach to application to access water. These values are somewhat subjective, therefore clear criteria would need to be established to ensure that community as well as other values and needs are met and respected.

- **Auctions and tenders**

The City does not support an auction or tender process to allocate water. This process will only be satisfactory to those who have the financial resources to competitively bid for an allocation of water.

- **Direct sale**

The City does not support the direct sale of water allocation.

- **Ballot**

A ballot process to access a water allocation is considered at best "ad hoc" and not based on any credible criteria, conditions or format.

The proposed mechanism of FIFS to the allocation limit of 70% is considered acceptable; however the Merit selection approach should apply to the remaining 30% unallocated resource. This method would ensure a sustainable element is applied to all applications with a particular focus on future land use and the provision of POS.

The Department of Water have advised that a “position” on the FIFO policy has been determined; however with the recent change of Government Ministers no resolution has been enacted. It is recommended that the City continue to operate under the current FIFO policy and wait until further advice is received from the DoW.

### **ACTION 12:**

- The City continues to engage with the Department of Water on the “First in First Served” Policy review.

## **WATER REDUCTION TARGETS**

Our current license threshold for water usage presents the City with an arduous task of setting ambitious targets for the reduction in groundwater abstraction. The City needs to find a balance between amenity and functionality for our public open space whilst at the same time reducing our impact on the environment.

Currently the City is licenced to extract 7,500kilolitres of groundwater per hectare per year from the superficial aquifer. The Department of Water is currently reviewing the volumes for water abstraction, with an anticipated figure of 7,200 kilolitres per hectare per year being adopted in the near future. This 300kL reduction is considered as the first step in a series of water management mechanism to be introduced by the DoW and considered an achievable target with minimal impact on the existing landscape treatments. However on going research at the University of WA (School of Plant Biology, Faculty of Natural and Agricultural Science) in collaboration with the turf industry, has determined the average water required to sustain turf in Perth is 35mm to 40mmper week. To achieve this requirement a water allocation of 9,000 to10,000 kL per hectare per annum is required. The City can retain this allocation to sporting ovals and major regional parks by reducing allocations to general parks and implementing hydrozoning mechanisms, installation of soil monitoring devices, central control systems and weather stations.

The installation of water meters has facilitated water volume extraction measurements that ensure compliance with our licence. Currently 140 bores are metered, with only 24 remaining bores requiring installation of a meter. The 2011/12 water year realised a saving of 391,473kL across the metered sites, which clearly highlights that measuring water extraction levels can lead to significant water savings. Water readings, to date, for the 12/13 water year are indicating solid savings however with the 2013 summer period extending late into April water saving will be reduced.

The City’s current average groundwater extraction across all sites is below the proposed DoW allocation of 7,200 and is well on its way to achieving the City’s Sustainability Action Plan 2013/14 Environment 5.1 KPI “To improve efficiency in corporate groundwater use by reducing consumption by 10 percent below the 207/08 DoW allocations per hectare by 2017/18”. To ensure these objects are achieved the following targets for the next 5 years.



Reserve Classification	Water Allocation (kL/ha/per annum)					
	2012/13	2013/14	2014/15	2015/16	2016/17	2017/18
Sports Ovals & Regional Parks	9,100	9,100	9,000	9,000	9,000	9,000
Sports Oval surrounds, Entry Statements, Regional, Neighbourhood and Local parks	6,500	6,400	6,300	6,200	6,100	6,000
Low profile passive parks & median strips	3,000	2,900	2,800	2,700	2,600	2,500
Dry park	0	0	0	0	0	0

Setting ambitious targets with consideration of DoW targets will potentially impact on the presentation of the POS or streetscape, as the landscape treatment may not be suitable in light of the reduced water allocation. A program, which will identify deficiencies of the landscape, as the water is reduced, will be implemented along with community consultation.

### **ACTION 13:**

- The City reduces its groundwater abstraction levels to achieve the City's Sustainability Action Plan 2013/14, Environment 5.1 KPI "To improve efficiency in corporate groundwater use by reducing consumption by 10 percent below the 207/08 DoW allocations per hectare by 2017/18"

### **PORT COOGEE – GROUNDWATER INTERCEPTION**

The Port Coogee Marina Development is influenced by an unusual set of circumstances, regarding groundwater usage. For this reason it is assessed separately from the City's overall irrigation watering strategy, in order that potential advantages can be leveraged from these circumstances.

The Port Coogee Marina / Residential Estate have undergone intensive environmental evaluation and public scrutiny. Concerns for the ultimate water quality within the marina have led to an extensive modelling of water quality within the proposed layout. Modelling was also completed and compared to other marina developments, particularly those which have existed for longer time periods.

The modelling and other hydrogeological studies identified that the flow of nutrient rich groundwater from the east had the potential to cause water quality issues within the marina. Impacts on the larger water body of Owen Anchorage by the nutrient rich groundwater are difficult to identify. The studies confirmed the need to intercept the groundwater flow and dispose of the intercepted water in a method to reduce nutrients or ensure bypass of the marina site. The estimated volume of groundwater moving from east to west to be intercepted is 7,400 cubic metres per day. This volume has been derived from hydrogeological studies by Rockwater Pty Ltd and is related to the nominal width of the marina lying across the groundwater flow.

The interception of 7,400 m<sup>3</sup>/day of water represents a significant opportunity to augment our water supply for the future if the City can obtain the supply in perpetuity and if the water could be traded to offset the demand in other areas of the City.

The Port Coogee Marina Development is currently utilising the intercepted water to irrigate the entry statements and POS within the estate with the excess water being reinjected into the superficial aquifer through re-injection bores located to the north of the site. Initial estimates contained in the Waterways Environmental Plan (WEMP) identify approximately 294 m<sup>3</sup>/day will be used for POS and entry statements. Monitoring of the water volumes being intercepted have been undertaken by the Port Coogee Developers since the commencement of the project with the results currently being prepared for submission to the City mid-2013.

The intercepted water currently considered excess to the Port Coogee's requirements is being considered in the following strategies:

## **1. WATER RE-USE SYSTEM**

Engineering plans submitted by the Port Coogee Developers to the City for approval during April 2006, indicate a proposal to reticulate groundwater intercepted as part of the waterways environmental management plan (WEMP) to private lots within the development. Preliminary estimates identify approximately 1,650 m<sup>3</sup>/day of water would be used by the secondary non-potable supply. This will largely depend on the demand and the level of participation by the landowners.

The developers are currently establishing a secondary reticulation system within the development (lilac pipe system to denote the non-potable supply) to a similar specification that the Water Corporation would apply. The initial intention was for the Water Corporation to assume responsibility for the secondary system once established. The developers have since received advice from the Water Corporation declining to accept the reticulation system. The developers have now approached the City of Cockburn to assume responsibility for the system beyond PC and the maintenance



period. No commitments have yet been given and a comprehensive business plan has been requested.

Prior to any decision being made it is imperative that this proposal is examined in detail to determine if there will be any adverse impacts on the Council's financial and social ambitions for the Cockburn community. Importantly, a business plan for the proposal has not yet been submitted to the City for evaluation. Key issues which need to be addressed in the business plan include:

#### **Customer Service Agreement**

- Water Charges to Consumers (Cost Recovery) Volumetric or annual fixed charge
- Ownership of Water Meters
- Liability for:
  - Non supply
  - Contamination of Supply
  - Discontinuation of Supply
  - Reduction in Supply
  - Time of Supply

#### **Quality of Asset**

- Design and Construction Specifications

#### **Asset Replacement Costs**

- Asset Management Plan

#### **Annual Ongoing Costs**

- Maintenance Costs
- Pumping Costs (Electricity Charges)
- Meter Reading and Billing Costs
- Water Quality Monitoring - Salt Intrusion
- Bacterial Contamination

#### **Legal Implications**

- Water License - Continuity of Water Extraction Allocation from Department of Water
- License for the City to sell water
- Local Law and prosecution for stealing water etc
- Health Department Matters - Cross Connection Issues

## **2. COCKBURN COAST STRUCTURE PLAN**

The Cockburn Coast development will provide an opportunity to divert the intercepted water resource at Port Coogee for general reticulation throughout the precinct. The Local Structure Plans have identified the following POS provisions in each precinct:

- Emplacement – 2Ha
- Robb Jetty – 6.6Ha
- Power Station – 1.4Ha

Although planning for the development is in the initial stages, the volume of water required to irrigate the POS will be 1,442m<sup>3</sup>/day. Access to the groundwater intercepting infrastructure has been provided to ensure the opportunity to irrigate this POS is not lost.

Further consideration of this strategy will need to be undertaken during the development phase of the Cockburn Coast in consultation with Department of Water.

### **3. GOLF COURSE PROPOSAL**

The Long Term Final Plan 2012/13 -2022/23 lists the establishment of a 9 hole golf course on the Coogee Regional Open Space for 2019/20. A preliminary feasibility study was presented to Council identifying land requirements, environmental impacts, concept designs and planning options. Although the land is owned freehold by the City, development approval is currently being requested from the Western Australian Planning Commission and Department of Regional Development and Lands.

Access to the intercepted groundwater from the Port Coogee Development will be a component of the business case to be presented to Council on the golf course proposal should approvals from the relevant State Govt entities be received. Preliminary estimates for the Golf Course proposal indicates that 3,333 m<sup>3</sup>/day would be required for golf course irrigation

### **OVERVIEW**

This intercepted water supply is a potential resource to facilitate future projects in proximity to the Port Coogee Development however a number of key issues have been identified which makes it difficult to rely on the resource in the longer term. These issues will need to be overcome if we are to capitalise on the resource.

1. Allocated groundwater intercepted as part of the waterways environmental management plan may not be licensed for extraction in perpetuity;
2. It may be cost prohibitive to use allocated groundwater intercepted as part of the waterways environmental management plan, in other parts of the City;
3. There is little to no environmental advantage in redirecting water intercepted as part of the waterways environmental management plan to currently irrigated nearby parks, as these parks are irrigated by groundwater extracted from the same aquifer as the intercepted water;
4. Trading of water entitlements will probably continue to be limited to trading within water resource management units (defined areas identified as groundwater licence areas), restricting the Council's ability to take advantage of surplus water from the development area.
5. Whilst this report does not explore the opportunities of water trading, as access to water becomes constrained water trading is more likely to become an accepted practice. This water resource makes those opportunities more accessible and could potentially enable the City to trade water in this precinct against securing water allocations in another precinct where our demand may be higher.

## **ACTION 14:**

- The City continues to monitor the Port Coogee Groundwater Interception resource and ensure the availability of this resource for future strategies.

## **ANNUAL MAINTENANCE BUDGETS**

### **ANNUAL OPEARTING MAINTENANCE**

The City's annual ongoing irrigation maintenance cost forms approximately 12% of the Parks Services annual operating budget of \$1,183,000 for the 2012/2013 Financial Year. However the percentage against the total Parks operating budget has reduced since 2006/2007 which was 15% of the total Parks operating budget. Comparisons with other LGA's show a range of 10% - 15% dependent on total irrigation infrastructure and adopted service levels. The City should endeavour to achieve an irrigation maintenance budget commensurate with its financial ability and required service level.

Future growth will realise an additional 100 Ha of POS in the next 10 years which will require additional funding and the appointment of two (2) irrigation fitters. The City's Workforce Plan 2012 – 2017 provides for irrigation fitter in 2014/15 and 2017/18. The appointment of these positions in these listed years will ensure the optimum irrigation operating conditions are achieved.

### **WATER LICENSING AND USAGE CHARGES**

Existing state government policy does not include charges for groundwater planning and management or volumetric charges for groundwater used by local governments. However the "Discussion Paper Water Resources Management Options", November 2009, highlights water resource management as becoming increasingly complex and expensive with the recovery of costs from people who benefit from the service. The Council of Australian Governments (COAG) and subsequently National Water Initiative (NWI) committed states and territories to consider "user pays" and cost recovery of water planning and management. The Economic Regulatory Authority is current holding an inquiry into water resource management and planning charges to provide the Government with a range of options and recommendations for cost recovery. The government will consider these recommendations prior to making a decision about fees and charges. The anticipated cost is \$8,000 per annum.

A full report to council will be submitted following the outcomes of the review and the impact of the cost recovery for groundwater planning and management on the City

## **ACTION 15:**

- The City continue to adopt the Annual Maintenance Budgets

## **IRRIGATION ASSET RENEWAL PROGRAM**

The condition of the City's irrigation infrastructure varies according to the age of individual systems. The useful life applied to each component of irrigation systems is determined by the by manufacturer, trends identified through the irrigation industry and historical data held by the City. The useful life mechanism is the only option for determining renewal programs as the majority of irrigation infrastructure is below ground and a visual inspection to determine the condition the asset is impracticable.

The Parks & Environments Asset Management Plan 2013 has the value of the City's irrigation assets at \$18,597,363. There is currently \$1.06m of irrigation assets that are considered to be past their projected renewal date and form the basis of the 10 year renewal plan. The 10 year cumulative funding gap for irrigation infrastructure is \$3,937,547.

The Parks & Environments Asset Management Plan 2013 is included with the 10 Year Long Term Financial plan which aims to increase renewal funds over the 10 to mitigate the funding gap for irrigation



### **ACTION 16:**

- The City will implement the Irrigation Asset Renewal Program outlined in the Parks & Environment AMP and annual budget submissions

## **GOVERNANCE**

This strategy is a long term plan which is intended to be a 'living' document that is reviewed and adapted to stay up to date with changes in policy, water reform, water management strategies, operating strategies, future growth and impacts from climate change. This process will also help to monitor progress towards goals, assess the effectiveness of irrigation operating techniques and assist in the preparation of annual budgets.

The City commits to reviewing the strategy, its action plan, funding requirements, changes in legislation and reporting on performance against targets on annual basis.

The annual report will be compiled following the completion of the water year (July to June) and will incorporate the City's annual reports to the Department of Water.

This process will be coordinated by Parks Services who will ensure that staff, Councillors and the community are kept informed of the output.

#### **ACTION 17:**

- The City will review the plan and report on performance against targets through an annual report. The first report will be September 2014.

Hand-drawn site plan of a property, likely a residential or commercial site, showing various features and annotations. The plan includes a central area labeled '786' and a surrounding area labeled '789'. A large blue area at the top is labeled 'Temp car park' and 'Approx 40m x 90m'. A pink area at the top left is labeled 'Access to site'. A yellow area at the bottom left is labeled 'Fencing'. The plan shows several streets: 'NAPOLEON PARADE' at the top, 'SOCRATES PARADE' on the left, and 'ORSINO BOULEVARD' on the right. A green area at the bottom is labeled '8025' and '8015'. The plan also shows various utility features: 'Electric Cable Markers', 'Telstra Pit', 'Power Dome', 'Stop Valve', and 'Electric Cable'. The plan is annotated with numerous numerical values, likely representing elevations or distances, and includes a scale bar at the bottom right.

17.06.18	1	ISSUE FOR CONSTRUCTION
Date	Rev	Revised Description
KEY PLAN		

**ARCHITECT**

**Part:**

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Nave 1914-17  
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CLIENT  
TRG PROPERTIES Pty Ltd



PROJECT  
NORTH COOGEE APARTMENTS  
LOT 786 MARINA VILLAGE  
NORTH COOGEE

TITLE EXISTING SITE SURVEY PLAN	DATE 11/11/2011	DRAWN BY J. L. ...	CHECKED BY J. L. ...
------------------------------------	--------------------	-----------------------	-------------------------

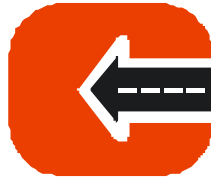
## CONSTRUCTION

ISSUED FOR CONSTRUCTION			
DATE DRAWN: JULY 2012		PLOT DATE: 24/05/2012 10:36:44	
DRAWN: LA		CHECKED: VT	APPROVED: DS
SCALE		1 : 200 (A)	
PRG#	STAGE	ZONE	DESC / DWG#
P10020			A01.01

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## Attachment No.2 – Traffic Management Plan



Carringtons Traffic Services  
 38 Beaconsfield Avenue,  
 Midvale, WA. 6056  
 Ph. (08) 9 356 7750  
 Fax: (08) 9 356 7751  
 Email : paul@carringtonswa.com

## ROAD WORKS TRAFFIC MANAGEMENT PLAN

### LOT 786 MARINA VILLAGE NORTH COOGEE

Prepared for  
**DIPLOMA**

#### Declaration

I, Paul Ashfold, declare that I have designed this Traffic Management Plan, following a site visit on the 1<sup>st</sup> of March 2013. The Traffic Management Plan has been prepared in accordance with the Main Roads Code of Practice and AS 1742.3.

Signature:

Date: 05/03/2013

	Name/Company	Accreditation Details	Date	Sign
<b>TMP designed by</b>	Paul Ashfold Carringtons Traffic Services	K 34450 Exp: 07/10/2013	05/03/2013	
<b>Checked by</b>	Paul Ashfold Carringtons Traffic Services	K 34450 Exp: 07/10/2013	05/03/2013	
<b>RTM review &amp; Approved by</b>	N/A			
<b>Compliance Audit to be Undertaken by:</b>				
<b>Service Authority Approval</b>				
<b>Road Authority Approval</b>	I, ....., being an authorised officer of the MAIN ROADS WA (Authorised Body) approve this Traffic Management Plan (TMP) for implementation subject to compliance with the details in the Traffic Management Plan (TMP) and Traffic Control Drawings (TCD).  Signed ..... Date.....			

TMP No.	Rev. No.	Date	TCD Nos.	Rev. No.	Date
6104-01	0	05/03/2013	Refer to TCD Table on Page 2		

## **TRAFFIC CONTROL DIAGRAMS:**

<b>TCD No</b>	<b>Revision</b>	<b>Issue Date</b>	<b>Description</b>
6104-01-01	0	01/03/2013	Stage 1: Long-term Footpath closure and construction traffic access
6104-02-01	0	01/03/2013	Stage 2: Short-term works – Single lane shuttle operation on Socrates Parade
6104-03-01	0	01/03/2013	Stage 3: Short-term works – Full road closure on Socrates Parade for large deliveries / crane lifts.



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## **Revisions**

<b>Revision</b>	<b>Date</b>	<b>Author</b>	<b>Reason</b>	<b>Sections</b>
0	05/03/2013	Paul Ashfold	Initial Release	All

## **Revision 1:**

<b>Section Title</b>	<b>Section Number</b>	<b>Amendment Summary</b>

# 1. PROJECT INFORMATION

## 1.1 Purpose and Scope

This Traffic Management Plan (TMP) provides the traffic management procedures to be implemented to allow Diploma to undertake Building construction work on lot 786 within the Marina Village, North Coogee, as part of the North Coogee Apartments project.

The construction activities will generally be undertaken from within the building site with a long-term footpath closure required to provide sufficient room for a gantry to surround the proposed building.

Works from the existing carriageway will involve site deliveries and crane lifts from Socrates Parade as various times throughout the project.

## 1.2 Site Location

The work site is located on the corner of Orsino Boulevard and Napoleon Parade, North Coogee.

The work site location is detailed in Figure 1.0 below.

**Figure 1.0 –Locality Plan for Works**



### 1.3 Site Constraints and Impacts

Orsino Boulevard / Napoleon Parade and Socrates Parade are under the care, control and management of the City of Cockburn and are classed as Access Roads which carry low volumes of traffic. Approvals from the City of Cockburn will be required prior to commencing the works.

Due to the location of the works Napoleon Parade and Socrates will be very low volume roads, as Napoleon Parade is currently a no through road and Socrates Parade does not currently provide access for any nearby properties. Orsino Boulevard is expected to carry higher volumes of traffic however the works will not impact on traffic movements on Orsino Boulevard.

The works require scaffolding to be placed surrounding the construction site during the construction activities and will impact on the pedestrian path surrounding the property. Diploma proposes to close the affected sections of path to prevent pedestrian access within the scaffold area. The pedestrian path shall be closed and pedestrians detoured using the alternate path network. As pedestrian activity is expected to be minimal and there is sufficient alternate paths nearby the path closures are not expected to impact on pedestrian movements during the project.

Diploma proposed to utilize the vacant property opposite the construction site for work vehicle parking and site offices. Access to the temporary parking will be via Napoleon Parade and will require access across the footpath. Pedestrian warning signage shall be displayed to warn pedestrians of the site access.

### 1.4 Traffic Management Objectives and Strategies

The objectives of the TMP are to:

- Provide for a safe environment for all road users;
- Provide protection to workers, visitors, agents of the Principal and the general public from traffic hazards that may arise as a result of the construction activity;
- Minimise the disruption, congestion and delays to all road users;
- To ensure network performance is maintained at an acceptable level throughout the term of the work;
- Ensure access to adjacent commercial premises is maintained at all times.

To achieve the above objectives, the Traffic Management Plan will:

- Ensure whenever possible, that a sufficient number of traffic lanes to accommodate vehicle traffic volumes are provided (if not possible, an APPROVAL TO VARY THE REQUIREMENTS OF AS 1742.3 OR MRWA CODE OF PRACTICE may be required).
- Ensure that delays and traffic congestion are kept to a minimum and within acceptable levels
- Ensure that appropriate/sufficient warning and information signs are installed and that adequate guidance is provided to delineate the travel paths through the work site.
- Ensure that the work area is free of hazards and that all road users are adequately protected from excavations and obstructions.
- Ensure that all needs of road users, motorists, pedestrians, cyclists, public transport passengers and people with disabilities are accommodated at and through the work site.
- Provide for work activities to be undertaken sequentially to reduce the adverse impacts of the work.
- Provide for safety procedures to enable work personnel to enter and leave the work area in a safe manner.

## 1.5 Responsibilities

Diploma will take the utmost care to prevent the risk of injury and/or property damage to employees, subcontractors, other contractors, road users and members of the public.

Work will not commence or continue at any location until all appropriate signs, devices and barricades are in place and in accordance with the requirements of the Traffic Management Plan. All necessary signs and traffic control devices will be installed at the work site to direct and regulate traffic movements around the work activity and ensure that adverse impacts associated with the works are kept to a minimum.

To assist in meeting these objectives the TMP provides information on:

- The Scope of Works
- Site Conditions
- Permissible working times
- Procedures and Responsibilities
- The Traffic Management Scheme
- The Traffic Control Diagram (TCD)

## 2. WORKS ON ROAD

### 2.1 Project Scope

ITEM	DESCRIPTION
<b>Project Scope</b>	Construction of new building on Lot 786 Marina Village
<b>Road Authority</b>	City of Cockburn
<b>Local Government</b>	City of Cockburn
<b>Project Stakeholder</b>	TRG Properties
<b>Main Contractor</b>	Diploma
<b>Scope of Works</b>	Construction of North Coogee apartments at lot 786 Marina Village.
<b>Staging of Work</b>	Due to the duration of the works the works have been shown in 3 stages.  Stage 1 – Long-term footpath closure and site access  Stage 2 – Short-term works – Single lane shuttle operation on from Socrates Parade  Stage 3 – Short-term works – Full road closure on Socrates Parade
<b>Project Date</b>	March 2013
<b>Hours of Work</b>	07 00 – 17 00hrs proposed
<b>Duration of Work</b>	18 months
<b>Other Issues</b>	

## 2.2 Exiting Traffic and Speed Environment

There is no existing traffic data available for Orsino Boulevard / Napoleon Parade and Socrates Parade prior to the completion of this TMP. Due to the location of the works, the traffic volumes are expected to be very low, less than 200 vehicles per day.

The work area is located within a residential area and as such all affected roads are 50km/h speed zones.

## 2.3 Roles and Responsibilities

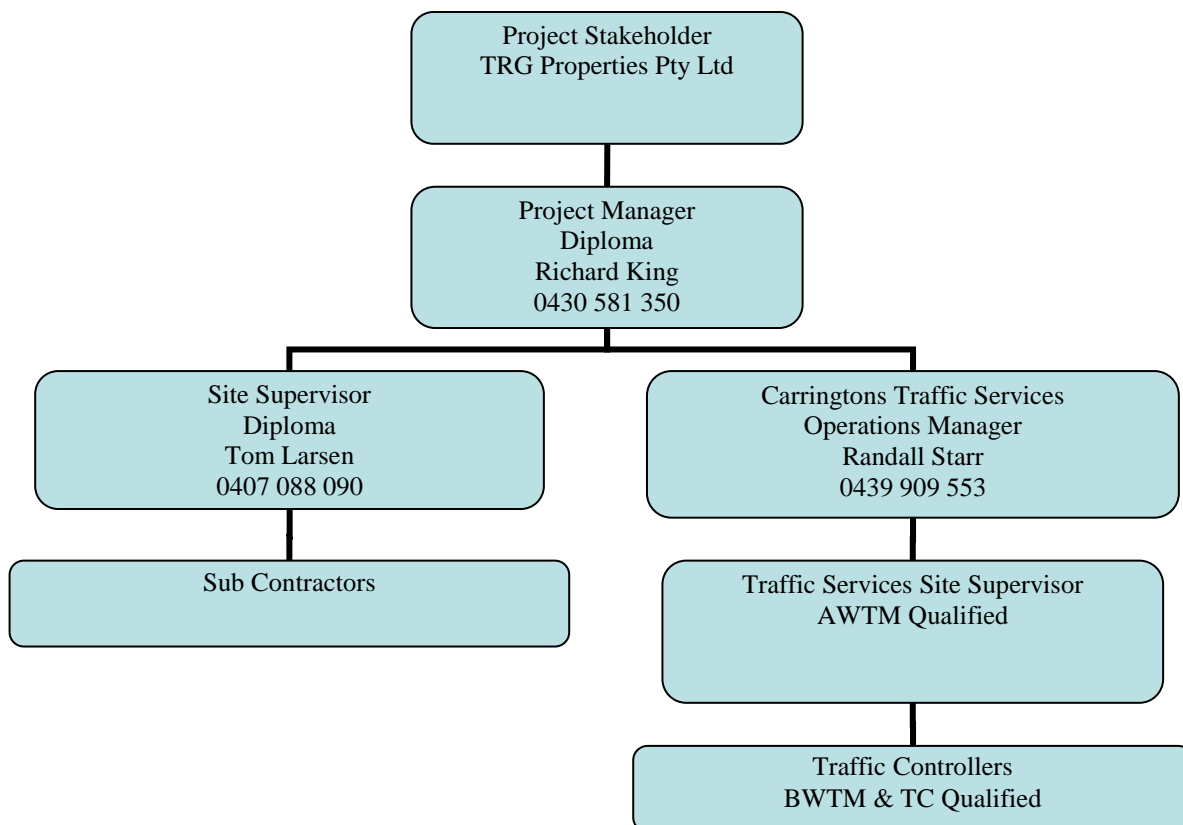
The Diploma Project Manager has the ultimate responsibility and authority to ensure the TMP is implemented for the prevention of property damage and injury to employees, contractors, sub-contractors, road users and all members of the public. He/she will ensure all site personnel are fully aware of their responsibilities, and traffic-controllers are appropriately trained and accredited. He/she will ensure that sufficient controllers are available to ensure appropriate breaks are taken.

All personnel engaged in the field activities will follow the correct work practices as required by AS1742.3.

The Site Supervisor may direct erection, relocation or removal of signs or devices, which, in his opinion are not in accordance with the TMP and do not provide for sufficient safety for road users. If such directions are not complied with, the Site Supervisor may arrange for erection, relocation or removal by others at the cost of the Contractor.

The following outlines the management hierarchy that will apply to the project.

## 2.4 Traffic Management and Responsibility Hierarchy





## 2.5 Project Representatives

Road Authority	<b>Orsino Boulevard</b> City of Cockburn 9 Coleville Crescent Spearwood WA 6965 Ph. (08) 9411 3444 Fax. (08) 9411 3333
<b>Local Government</b>	<b>City of Cockburn</b> 9 Coleville Crescent Spearwood WA 6965 Ph. (08) 9411 3444 Fax. (08) 9411 3333
<b>Main Contractor</b>	<b>Diploma</b> First Floor, 140 Abernethy Road, Belmont Western Australia 6104 Ph: (08) 9475 3500 Fax: (08) 9475 3501
<b>Project Manager</b> <i>Contact</i>	Richard King  0430 581 350
<b>Site Supervisor</b> <i>Contact</i>	Tom Larsen  0407 088 090

## 2.6 Traffic Management Administration

TMP Design	<b>Paul Ashfold K 34450</b> 38 Beaconsfield Ave, Midvale ,WA 6056 Tel: (08) 9356 7750 Fax: (08) 9356 7751 Email: <a href="mailto:paul@carringtonswa.com">paul@carringtonswa.com</a> Mob: 0439 909 551
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Operations Manager	Randall Starr 38 Beaconsfield Ave, Midvale ,WA 6056 Tel: (08) 9356 7750 Fax: (08) 9356 7751 Email: <a href="mailto:randall@carringtonswa.com">randall@carringtonswa.com</a> Mob: 0439 909 553



### 3. STATUTORY REQUIREMENTS

#### 3.1 Occupational Safety and Health

Principals, employers and persons in control of workplaces have a statutory duty of care to provide a safe workplace for all personnel working at the site, accessing the site or impacted by the construction activity including employees, contractors, subcontractors, visitors to the site and the general public.

This TMP forms part of the overall project Safety Management Plan, and provides details on how all road users considered likely to travel through, past, or around the worksite and those impacted by the works will be safely and efficiently managed for the full duration of the site occupancy and works.

All traffic management works and control devices shall be in accordance with:

- OS&H Act (1984)
- OS&H Regulations (1996)
- Australian Standard AS1742.3; Traffic Control for Works on Roads - 2009
- MRWA Traffic Management for Works on Roads - Code of Practice (CoP) – April 2011
- Road Traffic Code 2000
- Australian Standard AS/NZS ISO 31000; Risk management
- Australian Standard AS/NZS 4602; High visibility safety garments  
(*except where expressly overridden by the MRWA Traffic Management for Works on Roads – Code of Practice (CoP).*)

#### 3.2 Responsibilities

##### 3.2.1 Project Manager

The project manager shall:

- Ensure all traffic control measures of this TMP are placed and maintained in accordance with this plan and the relevant Acts, Codes, Standards and Guidelines.
- Ensure suitable communication and consultation with the affected stakeholders is maintained at all times.
- Ensure inspections of the Traffic Controls are undertaken in accordance with the TMP, and results recorded. Any variations shall be detailed together with reasons
- Review feedback from field inspections, worksite personnel and members of the public, and take action to amend the traffic control measures as appropriate following approval from the Traffic Supervisor.
- Arrange and/or undertake any necessary audits and incident investigations.

##### 3.2.2 Traffic Supervisor

The traffic supervisor is responsible for overseeing the day-to-day activities, and is therefore responsible for the practical application of the TMP, and shall:

- Instruct workers on the relevant safety standards; including the correct wearing of high visibility safety vests, safety boots and other equipment as required (See 3.2.4).
- Ensure traffic control measures are implemented and maintained in accordance with the TMP
- Undertake and submit the required inspection and evaluation reports to management
- Render assistance to road users and stakeholders when incidents arising out of the works affect the network performance or the safety of road users and workers
- Take appropriate action to correct unsafe conditions, including any necessary modifications to the TMP.

### **3.2.3 Traffic Management Personnel**

At least one person on site shall be accredited in Basic Worksite Traffic Management, and shall have the responsibility of ensuring the traffic management devices are set out in accordance with the TMP.

At least one person accredited in Advanced Worksite Traffic Management shall be available to attend the site at short notice at all times to manage variations, contingencies and emergencies, and to take overall responsibility for traffic management.

### **3.2.4 Traffic Controllers**

Traffic Controllers shall be used to control road users to avoid conflict with plant, workers, traffic and pedestrians, and to stop and direct traffic in emergency situations. Traffic Controllers shall:

- Operate in accordance with the “Traffic Controllers Handbook” (MRWA fifth Edition).
- Hold a current Traffic Controller’s accreditation in Western Australia.
- Take appropriate breaks as required by AS1742.3 and/or OS&H Regulations.

### **3.2.5 Workers and Subcontractors**

Workers and Subcontractors shall:

- Correctly wear high visibility vests, in addition to other protective equipment required (e.g. footwear, eye protection, helmet, sun protection etc), at all times whilst on the worksite
- Comply with the requirements of the TMP and ensure no activity is undertaken that will endanger the safety of themselves, other workers or the general public.
- Enter and leave the site by approved routes and in accordance with safe work practices.

### **3.2.6 Personnel Protective Equipment**

All personnel entering the work site shall correctly wear high:

- Visibility vests to meet the requirements of AS/NZS 4602
- Eye protection.
- Steel Cap ankle height protective boots.
- Long sleeves / pants
- Sun protection including sunscreen, with adequate application at suitable intervals.
- Addition other protective equipment as required to meet work site regulations, and may include hard hat, respiratory devices, hearing protection.

### **3.2.7 Plant and Equipment**

All plant and equipment at the workplace shall meet statutory requirements and have the required registration, licences or certification where required. All mobile equipment shall be fitted with suitable reversing alarms. All mobile plant and vehicles shall be fitted with a pair of rotating flashing yellow lamps in accordance with AS1742.3 clause 3.12.1. All workers will be made aware of the safe work practice at the time of the site induction.

### **3.2.8 Incident / Accident Procedures**

In the event of an incident or accident, whether or not involving traffic or road users, all work shall cease and traffic shall be stopped as necessary to avoid further deterioration of the situation. First Aid shall be administered as necessary, and medical assistance shall be called for if required. For life threatening injuries an ambulance shall be called on telephone number 000. The Police shall also be called on 000 for traffic accidents where life threatening injuries are apparent. Any traffic crash resulting in non-life threatening injury shall immediately be reported to the WA Police Service on 13 14 44.

Broken down vehicles and vehicles involved in minor non-injury crashes shall be temporarily moved to the verge as soon as possible after details of the crash locations have been gathered and noted. Where necessary to maintain traffic flow, vehicles shall be temporarily moved into the closed section of the work area behind the cones, providing there is no risk to vehicles and their occupants or workers. Suitable recovery systems shall be used to facilitate prompt removal of broken down or crashed vehicles. Assistance shall be rendered to ensure the impact of the incident on the network is minimised.

Details of all incidents and accidents shall be reported to the site supervisor and project manager using the incident report form at Appendix “D” (or similar).

### **3.2.9 Trip Hazards**

The worksite and its immediate surroundings shall be suitably protected and free of hazards, which could result in tripping by non-motorised road users. Hazards, which cannot be removed, shall be suitably protected to prevent injury to road users, including those with sight impairment.

## **4. PLANNING**

### **4.1 Risk Identification and Assessment**

Risk analysis of the proposed works has identified a number of risk events/items that will be managed by effective traffic management planning and the implementation of this TMP. A risk analysis table is attached at Appendix “B”. The assessment process has been undertaken in accordance with Australian Standard AS/NZS ISO 31000-2009, Risk Management.

All identified risks have been treated by development of this TMP. Unforeseen risks arising during the works will be treated in accordance with standard work practices and procedures where appropriate, specifically using the Job Safety Analysis contained in **Appendix G** to assess these risks.

### **Risk Identification and Response Table**

RISK	Pre-Treatment Risk Rating			RISK RESPONSE	Residual Risk Rating		
	L	C	RATING		L	C	RATING
Injury to pedestrians near the work site due to scaffolding being placed on the adjacent footpaths during the project.	A	2	H	Close affected footpaths adjacent to the worksite and detour pedestrians around the work area using the alternate path network.	D	2	L
Accident or injury to pedestrians on Napoleon Parade at construction site entry due to construction traffic crossing an existing footpath.	C	3	H	Install pedestrian warning signage at the construction site entry to warn path users of construction traffic.	D	3	M
Accident on Socrates Parade during site deliveries due to work vehicles being parked on existing carriageway	B	2	M	Implement temporary traffic management signage and devices to close one traffic lane to allow for vehicle parking. Utilise active traffic control on Socrates Parade to assist with traffic movements past the work area	D	2	L
Accident on Socrates Parade due to large vehicles or crane lifts from existing carriageway conflicting with through traffic	A	3	H	Close affected carriageway on Socrates Parade to prevent through traffic and reduce conflict with construction vehicles	D	2	L
Impact on road network due to temporary closures of Socrates Parade.	C	2	L	Due to the location of the works, the temporary closure will not impact on the surrounding road network and is not expected to cause any significant delays.	D	2	L
<b>Generic Risks associated with traffic management which may occur within the Project</b>							
Construction traffic entering or leaving the work site may stop unexpectedly or behave in unexpected manner	C	3	H	Construction vehicles fitted with warning devices. Operators instructed on safe procedures and "Spotters" will assist drivers in entering or leaving worksite. All vehicles shall exit work site under normal traffic conditions and adhere to road laws.	D	3	M
Workers hit by vehicles during set up and dismantling of traffic management	C	3	H	Shadow vehicle with flashing lights used to protect workers	D	3	M
Vehicles crashing into / through the worksite, injuring workers	C	3	H	Provide traffic management as per this TMP. Traffic arrangements to be evaluated for effectiveness following initial opening to traffic.	D	3	M
Visibility of workmen on site	C	3	H	All personnel on site shall wear high visibility vest with Class 1 retro reflective strips	D	3	M
Visibility of operating plant and associated work vehicles to vehicular traffic.	C	3	H	All plant and vehicles associated with the progression of the works shall have orange flashing lights attached and shall be operational when being used.	D	3	M
<b>Other unforeseen risks that may occur within the Project</b>							

## **4.2 Legal and Other Requirements**

Diploma recognise that the traffic management plan has been developed and shall be implemented with due consideration and in accordance with the following legislative, environment and industry standards.

- Occupational Safety and Health Act 1984 and Regulations 1996
- Road Traffic Act
- Road Traffic Code 2000
- Australian Standard AS 1742.3 – 2009 - Traffic Control for Works on Roads
- Risk Management Standard AS/NZS ISO 31000:2009
- Australian Standard - Mobility and Access Standard for People with Disabilities AS 1428
- Traffic Control Handbooks HB 81 series
- MRWA - Traffic Management for Works on Roads Code of Practice – April 2011
- Utility Providers Code of Practice
- Local Government Act

Diploma shall ensure that the requirements of these documents and other relevant information will be monitored and the Traffic Management Plan adjusted to meet changing requirements where necessary.

## **4.3 Traffic Assessment (Vehicular Traffic)**

### **4.3.1 Volume and Composition**

There is no existing traffic data available prior to the completion of this TMP.

The existing traffic volumes are expected to be very low (less than 200 vehicles per day) for the duration of the works.

Due to the location of the works, the vehicles expected past the work area will consist of mainly residential traffic on Orsino Boulevard. Traffic flow on Napoleon Parade and Socrates Parade will be minimal as these roads do not provide access to any residential properties.

### **4.3.2 Existing and Proposed Speed Zones**

The existing speed limit for Napoleon Parade and Socrates Parade is 50km/h.

No temporary speed zones will be required during the proposed works.

### **4.3.3 Intersection Capacity**

The works are not impact on any nearby intersections.

### **4.3.4 Existing Parking Facilities**

There are nearby parking facilities on Socrates Parade with street parking on Napoleon Parade. These parking bays will be closed during the works which require a road closure, however due to the location of the works the closure of the bays is not expected to impact on parking in the vicinity of the work area as there is sufficient alternate parking bays available.

### **4.3.5 Heavy and Oversize Vehicles**

The works are not expected to impact on the movement of heavy vehicles.

Permit type loads will be advised of the scheduled road works at the specified dates and times via the **MRWA heavy haulage section**, as they will be notified at least one week in advance of the scheduled road works, refer **APPENDIX E – NOTIFICATION OF ROAD WORKS**. Traffic controllers will be on hand to provide assistance, if required.

#### **4.3.6 Public Transport**

The works will not impact on public transport facilities.

The PTA shall be advised of any scheduled road works affecting public transport services at the specified dates and times at least one week in advance of the scheduled road works refer **APPENDIX E – NOTIFICATION OF ROAD WORKS**.

#### **4.3.7 Special Events and Other Works**

No other works or special events are expected during the proposed works.

### **4.4 Non -Motorised Road Users**

#### **4.4.1 Cyclists and Pedestrians**

The proposed works will require the closure of the pedestrian paths surrounding the construction site to allow for scaffolding to be erected around the work area. Pedestrians shall be detoured around the work area for the duration of the project while the scaffolding is in place.

There are suitable alternate paths surrounding the work area and pedestrians shall be detoured using the alternate path network.

Due to the location of the works pedestrian activity is expected to be minimal.

#### **4.4.2 People with Disabilities and Other Vulnerable Road Users**

There is no specific facility or service nearby that would increase normal use of the road facilities by people with disabilities and other vulnerable road users. All paths will be of suitable design and construction to meet the requirements of AS 1428 and will be left fully accessible at all times.

#### **4.4.3 School Crossings**

There are no school crossings located near the proposed work site; hence no significant numbers of children shall be expected.

### **4.5 Site Assessment**

#### **4.5.1 Access to Adjoining Properties**

There are no adjoining properties on Napoleon Parade or Socrates Parade. The works will not impact on traffic movements on Orsino Boulevard.

#### **4.5.2 Environmental Conditions**

##### **4.5.2.1 Weather**

*(Rain, Floods, Heat, Sun Glare, Fog)*

Stormwater drainage for the existing road pavements has been installed; hence no significant flooding or water sheeting is expected. In the unlikely event of rain, work will cease at the discretion of Diploma. Should work continue Carringtons Traffic Services may vary the position of signage by up to 25% to allow for the change in conditions. All changes shall be recorded in the daily diary.

The glare conditions should be minimal due to the orientation of the carriageway but traffic controllers will vary the location of signage to suit. All changes shall be recorded in the daily diary.

#### **4.5.2.2 Road Geometry / Terrain**

*(Horizontal and Vertical approach geometry, Safe stopping distances, Visibility, Vegetation)*

There are adequate sight distances on approach to the work site which will not cause any concern for motorists approaching the work site. There is no landscaping adjacent to the road to cause problems and the current geometry meets safe stopping sight distances for the existing 50 km/h zones. All signs shall be regularly inspected and repositioned as required to reduce the effects of shadows. All changes shall be recorded in the daily diary.

Upon completion of site set up, the Carringtons Traffic Services Traffic Supervisor shall drive through the entire site and check that visibility is at a maximum to all motorists.

#### **4.5.2.3 Existing Signage**

*(Obstruction, Visibility of temporary signage)*

All existing speed limit signs on the carriageway within the work site shall be covered for the duration of the works whilst temporary speed limit signs are in place.

The onsite traffic controllers, when implementing the traffic management will make certain that any permanent regulatory signs are not operative through the work site to avoid confusion for all vehicular traffic approaching, entering and exiting the designated work site.

There are no other traffic or advertising signs in the vicinity which could cause distractions or confusion, or which restrict sight lines.

#### **4.5.2.4 Other**

*(Structures, Dust, Noise, Fumes)*

There are no structures affecting sight lines or access. There is minimal landscaping adjacent to the traffic lanes. There are no significant dust-producing elements. Traffic on the sealed surfaces will not generate excessive dust.

### **4.5.3 Impact on Adjoining Road Network**

The proposed works are not expected to impact on the surrounding road network, as there will be no significant redirection of traffic during the works.

## **4.6 Works Programming**

### **4.6.1 Work Sequence**

Work Stage	TCD Numbers
Stage 1 – Long-term footpath closure and site entry	6104-01-01
Stage 2 – Short-term works – Single lane shuttle operation on Socrates Parade	6104-02-01
Stage 3 – Short-term works – Road closure on Napoleon Parade / Socrates Parade	6104-03-01

#### 4.6.2 Night Work Provisions

No night works are expected during the project. The footpath is expected to be closed for the majority of the works and signage will be in place overnight.

### 4.7 Emergency Planning

Prior to the works all emergency services will be contacted and advised of the works; however any emergencies during the shift traffic controllers on site will provide immediate access for any of the vehicles. After hours will have no effect on any emergencies as the road will revert back to normal operating conditions.

#### 4.7.1 Emergency Services

Emergency services shall be notified via FESA (phone 9323 9300) of the proposed works nature, location, date and times as well as contact details for the site supervisor.

#### 4.7.2 Dangerous Goods

Refer FESA Contact details above.

#### 4.7.3 Damage to Services

In the event that gas services are damaged, all work shall cease immediately, machinery and vehicles turned off and the area cleared of personnel as soon as possible. Traffic Controllers (and other personnel if necessary) shall be deployed immediately to ensure no traffic or other road users approach the area. The Police Service and relevant supply authority shall be called immediately. Damage to any other services shall be treated in a similar manner except machinery may remain operational and access may be maintained where it is safe to do so.

All site personnel shall be briefed on evacuation and control procedures.

#### 4.7.4 Failure of Services

In the instance that a failure of any existing service occurs during the progression of the works because of the work activities, or in the instance that a failure of any existing service occurs naturally that may impact on the works being progressed, the following Authorities shall be notified as soon as practical to attend.

- **Failure of Traffic Signals – TOC 9323 4848**
- **Failure of Street lighting – Western Power 13 13 51**
- **Failure of Power – TOC 9323 4848; Western Power 13 13 51**
- **Failure of Gas – Alinta 13 13 52**

### 4.8 Consultation and Communication

#### 4.8.1 Approvals

Refer to front cover for approvals by road and service authorities.



#### **4.8.1.1 Road Authority**

Approvals for the implementation of this TMP shall be obtained in accordance with the Code of Practice from City of Cockburn.

#### **4.8.1.2 Service Providers**

It is the responsibility of Diploma to adhere to the requirements and guidelines of this department.

#### **4.8.1.3 Environmental Protection Agency**

It is the responsibility of Diploma to adhere to the requirements and guidelines of this department.

#### **4.8.1.4 Department Of Conservation and Land Management**

It is the responsibility of Diploma to adhere to the requirements and guidelines of this department.

### **4.8.2 Public Notification**

Due to the low impact nature of the works, no public notification campaign will be required prior to the commencement of works.

Should any public notification be required by the City of Cockburn, this will form part of the permit conditions and shall be undertaken at their request a five business days prior to commencement of the works by Diploma.

### **4.8.3 Notification of Other Agencies**

In accordance with the MRWA Code of Practice, all relevant agencies shall be notified using the 'Notification of Road Works' form attached at **APPENDIX E**. A distribution list is provided on the bottom of the form. Other agencies shall be notified as required.

## **5. IMPLEMENTATION**

### **5.1 Hazard Identification, Risk Assessment and Control**

In establishing adequate controls for the hazards identified in Section 4.1, Carringtons Traffic Services have used a structured approach via the use of the hierarchy of control as outlined below.

- Elimination
- Substitution
- Engineering
- Administration
- Personal Protection Equipment

Carringtons Traffic Services traffic management practices require that the Traffic Supervisor (or representative) evaluate all traffic arrangements before they are open to traffic and immediately following the opening to traffic. Adjustments are to be made as required and recorded in the daily diary, including reasons for the changes. The Traffic Supervisor (or representative) is also required to evaluate the traffic arrangements where site conditions change, new hazards that arise throughout the work will be subject to risk assessment and incorporated onto the Risk Register.

## 5.2 Traffic Control Diagrams

The Traffic Control Diagrams outlined in Appendix “F” have been provided for the following stages to demonstrate the type of controls that will be implemented throughout the term of the contract.

Activity / Risk Treatment	TCD No
<b>Stage 1 – Long-term works – Installation of scaffolding adjacent to the construction site /</b> Closure of affected footpaths and pedestrian detours as required. Warning signage for construction site entry on Napoleon Parade.	<b>6104-01-01</b>
<b>Stage 2 – Short-term works - Site deliveries on Socrates Parade /</b> Single lane shuttle operation on Socrates Parade for short-term deliveries work and tower crane lifts.	<b>6104-02-01</b>
<b>Stage 3 – Short-term works – Large deliveries and crane lifts from carriageway /</b> Full road closure on Socrates Parade to allow for vehicles to be set-up on existing carriageway	<b>6104-03-01</b>

## 5.3 Traffic Control Devices

Traffic control devices shall be erected in accordance with the TCD's (refer **Appendix F**).

Before work commences, signs and devices at the approaches to the work area shall be erected in accordance with the installation plan in the following sequence:-

- (a) Advance warning signs. (Erect approach and departure signs on approaches to the work site)
- (b) All intermediate advance and positional signs and devices required in advance of the taper or start of the work area.
- (c) All delineating devices required to form the taper including the illuminated flashing arrow sign at the end of the taper where required. (Install delineation devices and lane closures).
- (d) Delineation past the work area.
- (e) All other required warning and regulatory signs.

A vehicle displaying a vehicle mounted warning device shall be used in advance of the signs and traffic control devices to protect workers setting out the signs or traffic cones associated with the taper. (*Note: Vehicle mounted warning devices are approved under the Vehicle Standards Regulations. These devices shall not be used outside the limits of the road works*).

At the end of the work shift, all lane closure and speed restrictions signs and equipment will be taken off the road. Following this, all signage will be removed from the affected carriageways as the works will be complete.

The signs and traffic control devices are to be removed in the reverse order of installation. A vehicle displaying a vehicle mounted warning device shall be used in advance of the signs and traffic control devices to protect workers removing the signs or traffic control devices.

Should the use of additional (not shown on the TCD or listing of devices) or reduced number of devices be required due to unforeseen needs, they shall be recorded within the Daily Diary as a variation to the TMP, following prior approval.

Work will not commence or continue until all signs, devices and barricades are in place and operational in accordance with the requirements of the TMP. The number, type and location of signs, devices and barricades shall be to a standard not less than Appendix “F” of this plan and AS1742.3 (except where specifically detailed in this TMP with reasons for the variations). Devices no longer required shall be promptly and completely removed from road user’s lines of sight.

### **5.3.1 Signs**

All signs shall be in accordance with AS 1742 (and manufactured in accordance with AS 1742.3), shall be at least size ‘A’ and shall be Class 1 retro-reflective. The Symbolic Worker sign shall also be fluorescent. Prior to the installation all signs shall be checked for damage and cleanliness and repaired, replaced or cleaned as necessary.

- They are properly displayed and securely mounted;
- They are within the driver’s line of sight;
- They cannot be obscured from view;
- They do not obscure other devices from the driver’s line of sight;
- They do not become a possible hazard to workers or vehicles; and
- They do not deflect traffic into an undesirable path.

### **5.3.2 Pavement Marking**

Not applicable.

### **5.3.3 Variable Message Boards**

N/A.

### **5.3.4 Delineation**

Traffic cones shall be used to delineate the verge area as required erected in accordance with the TCD’s in **APPENDIX F**. Traffic Cones shall be fitted with suitable white retro-reflective tape placed in accordance with AS 1742.3.

Traffic Cones shall be at least 700mm high, fluorescent red and fitted with Class 1 retro-reflective tape. Alternatively fluorescent red cones with Class 1 retro-reflective tape may be used.

All traffic cone spacings shall be in accordance with Table 3.7 from AS 1742.3, as below:

### **5.3.5 Temporary Speed Zones**

No temporary speed restrictions will be required during the project.

## **5.4 Emergency Arrangements**

Emergency services will have continual access to all properties and the worksite; hence no specific facilities are required. A Traffic Controller shall assist emergency vehicles requiring entering and/or travelling through

the worksite. Emergency services shall be notified via FESA (phone 9323 9300) of the proposed works nature, location, date and times as well as contact details for the site supervisor.

Vehicle breakdown and/or crashes can cause considerable delay and congestion. Police communications will be requested to render assistance where required.

## **5.5 Site Access**

Access to the works site shall be via the existing road network from Orsino Boulevard onto Napoleon Parade.

Works shall also be undertaken from Socrates Parade as required during the project.

Diploma proposes to utilize the vacant land opposite the work site on Napoleon Parade as construction site offices and parking.

Work vehicles, plant and personnel entering and leaving the work site shall do so at each end of the work site i.e. not to pull out mid-stream into flowing traffic, unless under the direction of a traffic controller). Drivers accessing the work site are required to enter with the direction of traffic flow. They should decelerate slowly and signal their intention by indicator to leave the traffic stream, the vehicle's rotating yellow lamps should be activated once a speed of 20 km/h has been reached at 50 metres from the point of entry. A traffic controller will remove traffic cones to allow work vehicles to enter the work site. Drivers leaving the work site shall have the vehicle's rotating yellow lamps activated and only enter the traffic stream when advised by the "spotter" when it is safe to do so. The rotating yellow lamps should be turned off once a speed of 40 km/h is reached.

**These requirements shall be communicated to all Diploma personnel on site by the Diploma Site Supervisor prior to the commencement of the work shift.**

## **5.6 Communicating TMP Requirements**

The requirements of the TMP will be communicated to all personnel entering the site through the site induction program.

# **6. MONITORING AND MEASUREMENT**

## **6.1 Site Inspections and Record Keeping**

The Diploma Project Manager will ensure that the Traffic Management Plan is implemented and evaluated for effectiveness. The Site Supervisor shall inspect and monitor traffic movements around the site in conjunction with the personnel who have erected the control measures. The outcomes of the inspection will be diarised for the information of the Project Manager.

Inspections shall be undertaken as required and at a minimum on the following occasions:

- Before the start of work activities on site;
- During the hours of work;
- Closing down at the end of the shift period; and
- After hours.

A daily record of the inspections should be kept indicating:

- When traffic controls were erected;
- When changes to controls occurred and why the changes were undertaken;
- Any significant incidents or observations associated with the traffic controls and their impacts on road users or adjacent properties.

Where significant changes to the work or traffic environment or adverse impacts are observed, the controls should be reviewed as a matter of urgency. Daily Inspection Sheets shall be completed by the person undertaking the inspections and reviewed by the Site Supervisor. All variations to the TMP/TCD, non-conformances, incidents and accidents shall be recorded. Copies of the completed report shall be forwarded to the Project Manager and the Traffic Supervisor. A suggested Daily Inspection Report Form is at Appendix “C”. One sheet per inspection should be used, with the relevant section to be filled in.

## **6.2 TMP Auditing**

One compliance audit (using the ‘Compliance Audit Checklist for Traffic Management for Works on Roads’ available on the Main Roads website) may be conducted following setting-up of the traffic management and prior to commencement of the works in accordance with Main Roads Specifications. Audit findings, recommendations and actions taken shall be documented and copies forwarded to the Project Manager.

The traffic supervisor shall ensure that regular site checks are undertaken approximately every hour during the stage works to ensure that the traffic management on site conforms to the requirements of this TMP. All site inspections shall be recorded in the daily diary.

## **6.3 Public Feedback**

Diploma will implement a procedure that ensures comments and complaints received from the public are registered. The Site Supervisor shall be responsible for the monitoring of the Register on a daily basis.

## **6.4 References**

- Australian Standard AS1742.3; Traffic Control for Works on Roads
- Australian –New Zealand Standard AS/NZS ISO 31000; Risk management
- Australian Standard AS/NZS 4602; High visibility safety garments
- MRWA Traffic Management for Works on Roads - Code of Practice (CoP) April 2011
- OS&H Act (1984)
- OS&H Regulations (1996)
- Road Traffic Code 2000
- MRWA Specification 202

# **7. MANAGEMENT REVIEW**

## **7.1 TMP Review and Improvement**

As this project is of a long-term nature, a review of the effectiveness of the TMP will be undertaken by the Project Manager periodically within the project and also as part of the close-out procedure.

## **7.2 Variations to Standards and Plans**

There has been no departure from AS 1742.3 – 2009 or MRWA Code of Practice.

On-site variations, if required, shall generally only be made following approval by the Traffic Supervisor and recorded in the daily diary. In emergency situations, on-site variations shall be made and recorded in the daily diary, and the Traffic Supervisor notified as soon as practicable.

# **APPENDIX A**

## **TRAFFIC VOLUME COUNTS**

No traffic data available for the affected roads prior to the completion of the TMP

## **APPENDIX B**

### **TRAFFIC RISK CLASSIFICATIONS AND RISK ANALYSIS TABLES**

## TRAFFIC RISK CLASSIFICATION

1. In order to clearly understand the risks associated with this Contract and then outline the manner in which identified risks will be managed, the Contractor shall Undertake an assessment of all significant foreseeable risks associated with the Contract and determined the treatment measures that, so far as practicable, minimise the risk.
2. The identification and assessment process must be undertaken in accordance with AS/NZS ISO 31000 and the likelihood and consequences rated before the application of risk treatments (Primary Risk) and after (residual risk) the determined controls utilizing Table 202B.1, Table 202B.2 and Table 202B.3 of this Annexure 202B.
3. The Contractor shall, so far as practicable, control or reduce identified risks in accordance with the hierarchy of control as defined by AS/NZS 4801. Treatment measures shall be authorised and managed by the Contractor in accordance with Table 202B.4 Management Approach for Residual Risk Rating. Risk Control and Reduction
4. The Superintendent may direct the Contractor as to the Primary Risk Rating and the Residual Risk Rating to apply to any risk. The Contractor shall reassess, authorise and manage its risk control measures in accordance with the level of risk directed by the Superintendent.
5. A Residual Risk Rating of Extreme is not permissible under the Contract.
6. The Contractors shall use the OSH risk classification in accordance with Road Users Specification 203 OCCUPATIONAL SAFETY AND HEALTH when addressing safety hazards of the general public and road users moving through the Site.



## RISK TABLES (SPECIFICATION 202)

**TABLE 203B.1 – QUALITATIVE MEASURES OF CONSEQUENCE OR IMPACT**

Level	Descriptor	Description
1	Insignificant	<ul style="list-style-type: none"> <li>• Minor first aid treatment required;</li> <li>• Immediate return to work.</li> </ul>
2	Minor	<ul style="list-style-type: none"> <li>• Minor medical treatment required;</li> <li>• Not a lost injury time.</li> </ul>
3	Moderate	<ul style="list-style-type: none"> <li>• Medical treatment required;</li> <li>• Lost time injury;</li> <li>• WORK SAFE report not required.</li> </ul>
4	Major	<ul style="list-style-type: none"> <li>• Significant injuries;</li> <li>• Hospitalisation required;</li> <li>• WORK SAFE report required.</li> </ul>
5	Catastrophic	<ul style="list-style-type: none"> <li>• Permanent and severe disablement;</li> <li>• Fatality.</li> </ul>

**TABLE 202B.1 – QUALITATIVE MEASURES OF CONSEQUENCE OR IMPACT**

Level	Descriptor	Description
1	Insignificant	<ul style="list-style-type: none"> <li>• Mid block hourly traffic flow per lane is equal to or less than the allowable lane capacity detailed in AS1742.3. No impact to the performance of the network.</li> <li>• Affected intersection leg operates at a Level of Service (LoS) of A or B</li> <li>• No property damage</li> </ul>
2	Minor	<ul style="list-style-type: none"> <li>• Mid block hourly traffic flow per lane is greater than the allowable road capacity and less than 110% of the allowable road capacity as detailed in AS1742.3. Minor impact to the performance of the network.</li> <li>• Intersection performance operates at a Level of Service (LoS) of C</li> <li>• Minor property damage</li> </ul>
3	Moderate	<ul style="list-style-type: none"> <li>• Mid block hourly traffic flow per lane is equal to and greater than 110% and less than 135% of allowable road capacity as detailed in AS1742.3. Moderate impact to the performance of the network.</li> <li>• Intersection performance operates at a Level of Service (LoS) of D</li> <li>• Moderate property damage</li> </ul>
4	Major	<ul style="list-style-type: none"> <li>• Mid block hourly traffic flow per lane is equal to and greater than 135% and less than 170% of allowable road capacity as detailed in AS1742.3. Major impact to the performance of the network.</li> <li>• Intersection performance operates at a Level of Service (LoS) of E</li> <li>• Major property damage</li> </ul>
5	Catastrophic	<ul style="list-style-type: none"> <li>• Mid block hourly traffic flow per lane is equal to and greater than 170% of allowable road capacity as detailed in AS1742.3. Unacceptable impact to the performance of the network.</li> <li>• Intersection performance operates at a Level of Service (LoS) of F</li> <li>• Total property damage.</li> </ul>

**TABLE 202B.2 – QUALITATIVE MEASURES OF LIKELIHOOD**

Level	Descriptor	Description
A	Almost Certain	The event or hazard: <ul style="list-style-type: none"> <li>• is expected to occur in most circumstances,</li> <li>• will probably occur with a frequency in excess of 10 times per year.</li> </ul>
B	Likely	The event or hazard: <ul style="list-style-type: none"> <li>• will probably occur in most circumstances,</li> <li>• will probably occur with a frequency of between 1 and 10 times per year.</li> </ul>
C	Possible	The event or hazard: <ul style="list-style-type: none"> <li>• might occur at some time,</li> <li>• will probably occur with a frequency of 0.1 to 1 times per year (i.e. once in 1 to 10 years).</li> </ul>
D	Unlikely	The event or hazard: <ul style="list-style-type: none"> <li>• could occur at some time,</li> <li>• will probably occur with a frequency of 0.01 to 0.1 times per year (i.e. once in 10 to 100 years).</li> </ul>
E	Rare	The event or hazard: <ul style="list-style-type: none"> <li>• may occur only in exceptional circumstances,</li> <li>• will probably occur with a frequency of less than 0.01 times per year (i.e. less than once in 100 years).</li> </ul>

**IMPORTANT NOTE:** The likelihood of an event or hazard occurring shall first be assessed over the duration of the activity (i.e. “period of exposure”). For risk assessment purposes the assessed likelihood shall then be proportioned for a “period of exposure” of one year

Example: An activity has a duration of 6 weeks (i.e. “period of exposure” = 6 weeks). . The event or hazard being considered is assessed as likely to occur once every 20 times the activity occurs (i.e. likelihood or frequency = 1 event/20 times activity occurs = 0.05 times per activity). Assessed annual likelihood or frequency = 0.05 times per activity x 52 weeks/6 weeks = 0.4 times per year. Assessed likelihood = C (i.e. Possible)

**TABLE 202B.3 – QUALITATIVE RISK ANALYSIS MATRIX – RISK RATING**

Likelihood	Consequences				
	Insignificant 1	Minor 2	Moderate 3	Major 4	Catastrophic 5
A (almost certain.)	M	H	H	E	E
B (Likely)	L	M	H	E	E
C (Moderate)	L	M	H	E	E
D (Unlikely)	L	L	M	H	E
E (Rare)	L	L	M	H	H

**TABLE 202B.4 – MANAGEMENT APPROACH FOR RESIDUAL RISK ANALYSIS RATING**

Residual Risk Rating		Required Treatment
E	Extreme risk	Unacceptable risk. HOLD POINT. Work cannot proceed until risk has been reduced.
H	High risk	High priority, OSH MR and Road Traffic Manager (RTM) must review the risk assessment and approve the treatment and endorse the TMP prior to its implementation.
M	Moderate risk	Medium Risk, standard traffic control and work practices subject to review by accredited AWTM personnel prior to implementation.
L	Low risk	Managed in accordance with the approved management procedures and traffic control practices.

## **APPENDIX C**

### **DAILY DIARY AND INSPECTION REPORT FORM**

## DAILY DIARY

Record details of all changes to the approved Traffic Management plan, who directed/made the changes and who authorised the changes (if applicable).

### PROJECT DETAILS:

LOCATION:

DATE:

Contract No.

TMP Document No.

TCD Dwg No.

Revision No.

Date:	Time:	Location:			
Inspection/ changes	By:	Signed:	Changes authorised	By:	Signed:
Detail/Comments:					

Date:	Time:	Location:			
Inspection/ changes	By:	Signed:	Changes authorised	By:	Signed:
Detail/Comments:					

Date:	Time:	Location:			
Inspection/ changes	By:	Signed:	Changes authorised	By:	Signed:
Detail/Comments:					

# APPENDIX D

## INCIDENT REPORT FORM

# ANNEXURE 202D

## Traffic Incident Reporting Form

Region
Contract Number

Incident Report No.
Contractor

Major Incident Reports must be forwarded to the Superintendent within 48 hours of the incident occurring or becoming apparent.

Contractors shall use this Form for reporting of Traffic incidents on works under Contract and this form supplements the OSH Incident Reporting Form.

<b>1.0 Details of Incident</b>	Reported to:	<input type="checkbox"/> Supervisor	<input type="checkbox"/> TMR	<input type="checkbox"/> Other -----
OSH Incident Report No		<b>Atmospheric Conditions</b>	<b>Light Conditions</b>	
Fatality <input type="checkbox"/>		Clear <input type="checkbox"/>	Day Light <input type="checkbox"/>	
Injury <input type="checkbox"/>	<b>Road Surface</b>	Overcast <input type="checkbox"/>	Night Time <input type="checkbox"/>	
Property Damage <input type="checkbox"/>	Unsealed <input type="checkbox"/>	Raining <input type="checkbox"/>	Dawn/Dusk <input type="checkbox"/>	
Police Attended <b>Yes/No</b>	Sealed <input type="checkbox"/>	Fog/Smoke/Dust <input type="checkbox"/>	<b>Street Lighting</b>	
Time and Date of incident		<b>Road Condition</b>	On <input type="checkbox"/>	
	<b>AM / PM</b>	Wet <input type="checkbox"/>	Off <input type="checkbox"/>	
	Day Month Year	Dry <input type="checkbox"/>	Not Provided <input type="checkbox"/>	

Other relevant details, (Last maintenance grade, watering and dust conditions):

<b>2.0 Details of Traffic Management in place:</b>
--

TCD No: \_\_\_\_\_ Name of individual that prepared the TCD \_\_\_\_\_

Time last inspected: \_\_\_\_\_ Accreditation No: \_\_\_\_\_

TCD Approved: \_\_\_\_\_ Day Month Year TMP Approved: \_\_\_\_\_ Day Month Year

<b>3.0 Descriptions of Vehicles:</b>
--------------------------------------

Detail (make, model/ped/cyclist/VRU) Registration No Direction of Travel Age of Driver

3.1 Vehicle 1

3.2 Vehicle 2

3.3 Vehicle 3

Comments:

<b>4.0</b>	<b>Description of Incident:</b>
------------	---------------------------------

Draw the incident including the direction of travel, traffic control signs, fixed structures and East point.

A blank grid with a circle in the top-left corner and the word 'East' in the top-right corner. The grid is composed of small squares. A circle is drawn in the top-left corner, and the word 'East' is written in the top-right corner.

<b>5.0 Attachments:</b>	The following copies MUST be submitted with this Incident Report.
-------------------------	---

The following copies **MUST** be submitted with this Incident Report.

Approved TMP ☐

Approved TCD ☐

Approvals for temporary  
speed restrictions ☐

Daily Diary ☐

<b>6.0 Police Report:</b>
---------------------------

Accident reported  
to Police:

☐ YES      ☐ NO

Report made by

☐ Phone    ☐ Fax

☐ Mail or E-mail

Date Report Made

Day      Month      Year

Police WA  
Reference Number

## 7.0 Details of Person Completing this Incident Form:

---

Name:

Contractor Name:

Position:

Date:

Signature:

## **APPENDIX E**

### **NOTIFICATION OF ROAD WORKS FORM**

Note. No notifications will be required to emergency services prior to the works commencing.



**NOTIFICATION OF ROAD WORKS FORM**

Anticipated start date				Anticipated finish date			
Daily work hours				Weekend work applicable	Yes	No <input checked="" type="checkbox"/>	
Location of works							
Description of works							
Road type							
Posted Speed Limit		Worksite speed limit		After hours speed limit			
Brief description of traffic management during works							
Description of traffic management devices used							
What is the anticipated effect on traffic flows?				Will there be restricted width for oversize escorted vehicles?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	
Are lanes closed at signals?	Yes	No <input checked="" type="checkbox"/>	N/A <input type="checkbox"/>	Are signal loops or hardware affected?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	N/A <input type="checkbox"/>
Will signal phases need time changes?	Yes	No <input checked="" type="checkbox"/>	N/A <input type="checkbox"/>	Will signals need to revert automatically?	Yes	No <input type="checkbox"/>	N/A <input type="checkbox"/>
Date of signal "black out"	n/a			Times of signal "black out"	n/a		
Will Police attendance be required?	Yes <input type="checkbox"/>		No <input checked="" type="checkbox"/>	Dates for Police attendance (See note below) <sup>(1)</sup>	n/a		
Are warden controlled school works located in area of works	Yes <input type="checkbox"/>		No <input checked="" type="checkbox"/>	Will crossings be altered during works	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	

Construction Authority							
Postal address							
Telephone		Facsimile		Email			
Contact							
Telephone		Mobile		Email			
Construction contractor							
Postal address							
Telephone		Facsimile		Email			
Contact							
Telephone		Mobile		Email			
After hours contact	Nathan			Telephone		Mobile	
Traffic management contractor							
Postal address							
Telephone		Facsimile		Email			
Contact							
Telephone		Mobile		Email			
After hours contact				Telephone		Mobile	

Notification is to be given at least three (3) weeks in advance where Police attendance is required, one (1) week otherwise – except in an emergency

(1) Where Police attendance is required specific arrangements shall be made with WA Police State Traffic Coordination, ☎ (08) 9222 1469

Distribution List (Notification through email preferred)	Email	Facsimile
WA Police State Traffic Coordination	<a href="mailto:state.traffic.coordination@police.wa.gov.au">state.traffic.coordination@police.wa.gov.au</a>	(08) 9222 1766
MRWA Traffic & Safety <sup>(2)</sup>	<a href="mailto:roadtraff@mainroads.wa.gov.au">roadtraff@mainroads.wa.gov.au</a>	(08) 9323 4174
MRWA Traffic Operations Centre	<a href="mailto:dlmrwatoc@mainroads.wa.gov.au">dlmrwatoc@mainroads.wa.gov.au</a>	(08) 9428 2220
MRWA Heavy Haulage	<a href="mailto:htv@mainroads.wa.gov.au">htv@mainroads.wa.gov.au</a>	(08) 9311 8455
St Johns Ambulance	<a href="mailto:comms@ambulance.net.au">comms@ambulance.net.au</a>	(08) 9334 1207
Fire & Emergency Services	<a href="mailto:fesa@fesa.wa.gov.au">fesa@fesa.wa.gov.au</a>	(08) 9323 9384
Transperth <sup>(3)</sup>	<a href="mailto:sfisk@pta.wa.gov.au">sfisk@pta.wa.gov.au</a>	(08) 9326 2487
Downer Electrics (Traffic signals only)	<a href="mailto:traffic_signals@depower.com.au">traffic_signals@depower.com.au</a>	(08) 9351 9211

<sup>(2)</sup> Perth metro only. Elsewhere, the relevant MRWA Regional Office shall be notified.

<sup>(3)</sup> Perth metro only. Elsewhere, the relevant public transport / school bus services shall be notified.

# **APPENDIX F**

## **TRAFFIC CONTROL DIAGRAMS**

# LOT 786 MARINA VILLAGE

## STAGE 1: LONG-TERM FOOTPATH CLOSURE



Table 3.7 AS 1742.3-2009  
CONE SPACINGS

Purpose and Usage	Traffic Speed, km/h	Recommended Maximum Spacing (m)
All purposes	<50	4

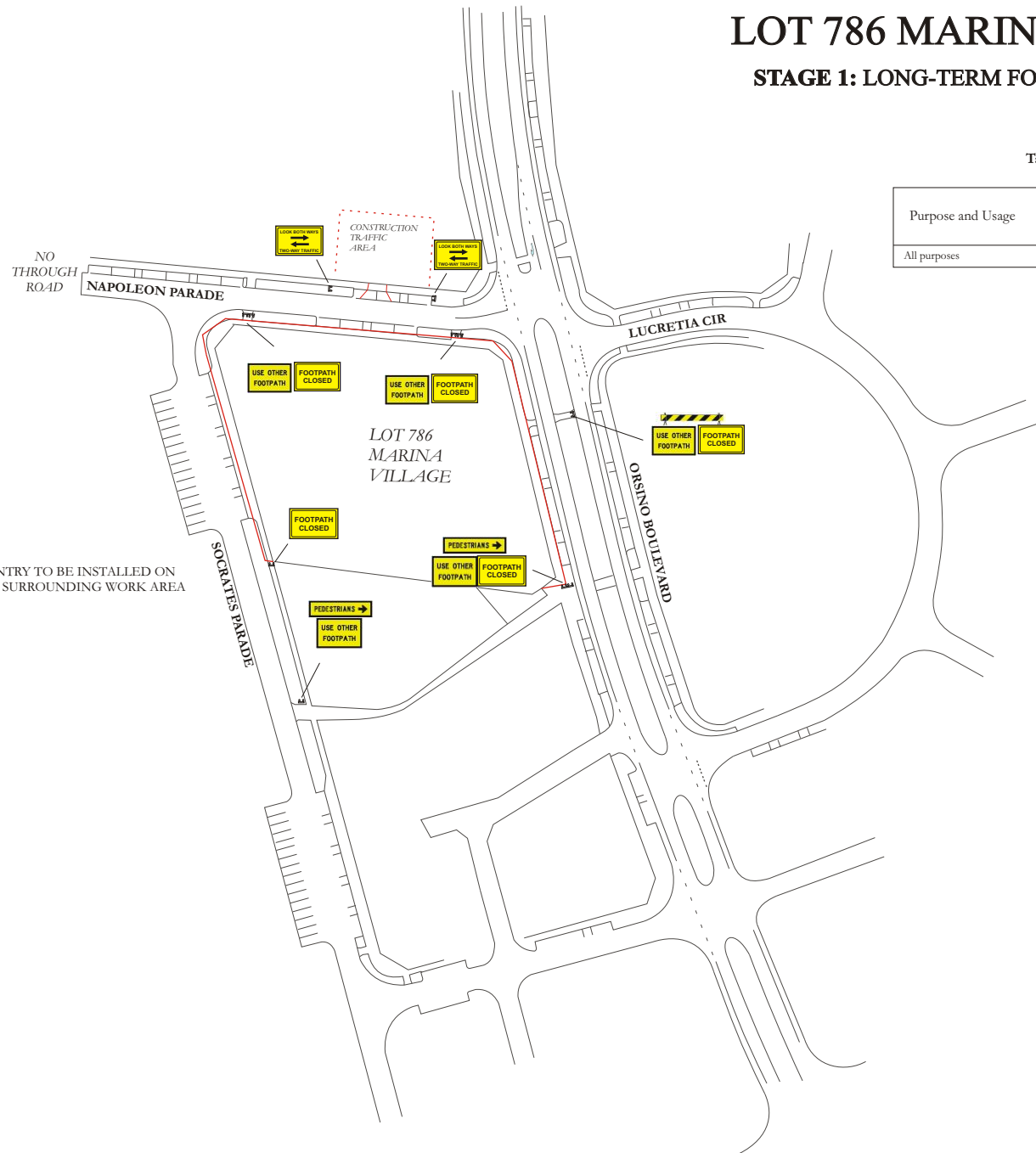


TABLE 4.2 AS 1742.3-2009  
VALUE OF DIMENSION D

SPEED OF TRAFFIC KM/H	DIMENSION D m
55 or less	15
56 to 65	45
Greater than 65	Equal to speed of traffic, in km/h

### NOTES

- All sign locations are to be checked prior to setout and positions adjusted to allow for specific site constraints such as vegetation, other signs and roadside furniture.
- Minimum trafficable lane width of 3.0m is to be maintained past the worksite at all times.
- All existing permanent speed signs to be covered for the duration of the daily works. Covers to be removed on completion of works each day.
- Emergency vehicles are to be escorted through the site by traffic controllers as required.
- Symbolic worker signs to be displayed only when workers are on site.
- P.T.S signs to be displayed only when traffic controllers are required to stop traffic.
- Positioning of signs, lengths of tapers or markings:
  - Minimum, 10% less than the distances or lengths given.
  - Maximum, 25% more than the distances or lengths given.
- Reflective traffic cones to be spaced as per Table 3.7 of AS 1742.3-2009.
  - No minimum.
  - Maximum, 10% more than the spacing shown.
- Spacing of delineation devices:
  - No minimum.
  - Maximum, 10% more than the spacing shown.

**DISCLAIMER:** THE USE OF THIS TRAFFIC CONTROL DIAGRAM IS INTENDED FOR IMPLEMENTATION SOLELY BY CARRINGTONS TRAFFIC SERVICES AND / OR AN ASSIGNED CONTRACTOR. CARRINGTONS TRAFFIC SERVICES WILL NOT BE HELD RESPONSIBLE FOR MISUSE OR ALTERATIONS NOT AUTHORIZED BY A QUALIFIED CARRINGTONS TRAFFIC SERVICES SIGNATORY.

### LEGEND

- WORK AREA**
- SIGN LOCATION**
- DELINEATION**

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
50 km/h	50 km/h
LOCATION	
LOT 786 MARINA VILLAGE	
CLIENT	
DIPLOMA CONSTRUCTION	
CONTACT	
RICHARD KING 0430 581 35023	
DATE	SCALE
01/03/2013	NOT TO SCALE
DRAWN BY	SIGNED
PAUL ASHFOLD AWTM E34450	
DESIGNED BY	SIGNED
PAUL ASHFOLD AWTM E34450	
APPROVED BY	SIGNED
PAUL ASHFOLD AWTM E34450	

**LOT 786  
MARINA VILLAGE**  
STAGE 1: LONG-TERM FOOTPATH  
CLOSURE

DRAWING NUMBER	REVISION	SIZE
6103-01-01	0	A3



Carringtons Traffic Services  
38 Beaconsfield Avenue,  
Midvale, WA, 6105  
Ph: (08) 9356 7750  
Fax: (08) 9356 7751  
www.carringtonswa.com

# LOT 786 MARINA VILLAGE

## STAGE 2: SINGLE LANE SHUTTLE OPERATION

Table 3.7 AS 1742.3-2009

CONE SPACINGS

Purpose and Usage	Traffic Speed, km/h	Recommended Maximum Spacing (m)
All purposes	<50	4

NOTE. SINGLE LANE SHUTTLE OPERATION AS REQUIRED. 3.0m MIN PAST WORK AREA TO BE MAINTAINED

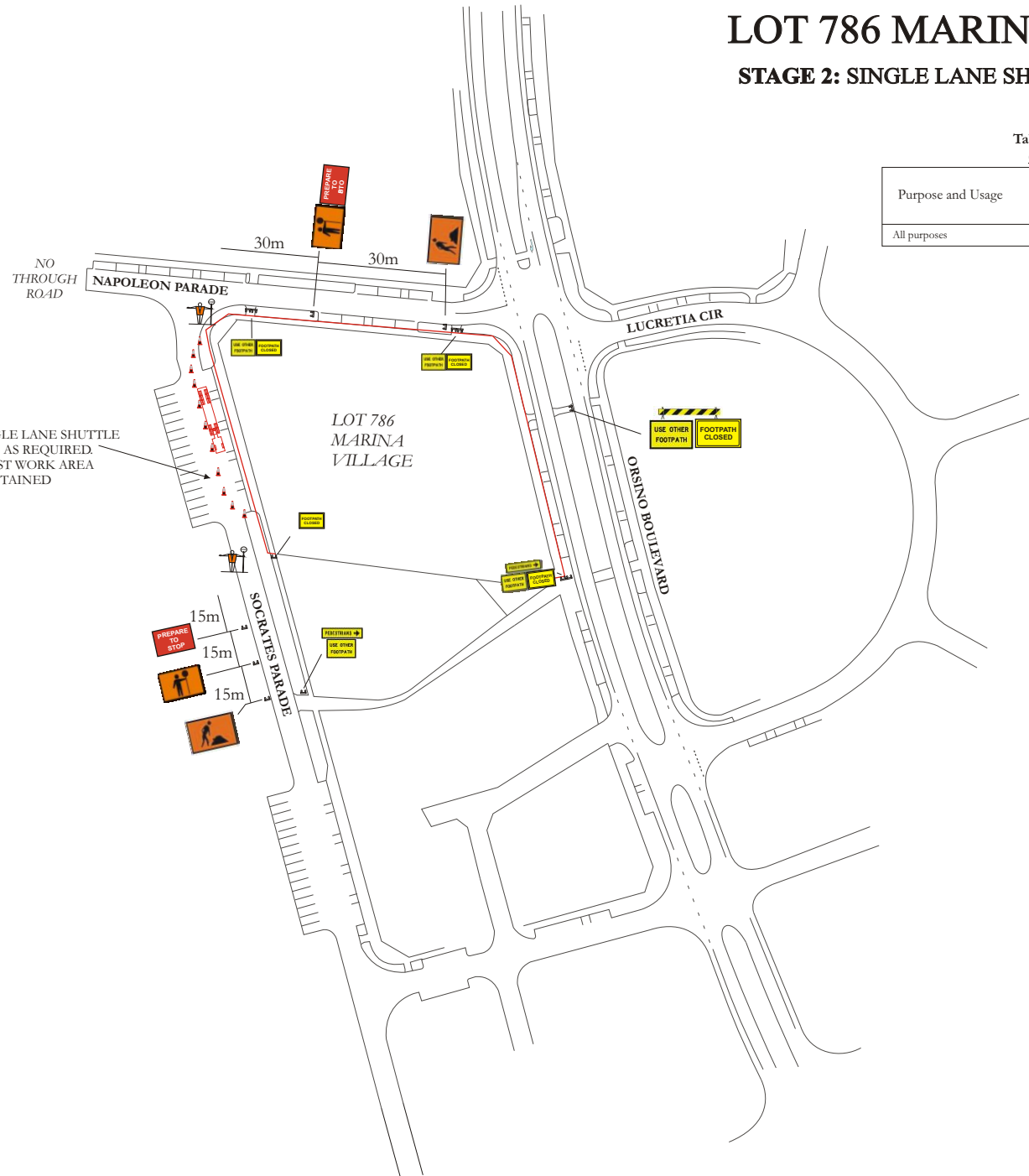


TABLE 4.2 AS 1742.3-2009  
VALUE OF DIMENSION D

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56 to 65	45
Greater than 65	Equal to speed of traffic, in km/h

### NOTES

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  - Maximum, 25% more than the distances or lengths given.
- Reflective traffic cones to be spaced as per Table 3.7 of AS 1742.3-2009.
  - No minimum.
  - Maximum, 10% more than the spacing shown.
- Spacing of delineation devices:
  - No minimum.
  - Maximum, 10% more than the spacing shown.

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### LEGEND

WORK AREA	
SIGN LOCATION	
DELINEATION	

POSTED SPEED LIMIT	WORK SITE SPEED LIMIT
50 km/h	50 km/h

LOCATION  
LOT 786 MARINA VILLAGE

CLIENT  
DIPLOMA CONSTRUCTION

CONTACT  
RICHARD KING 0430 581 35023

DATE  
01/03/2013

SCALE  
NOT TO SCALE

DRAWN BY  
PAUL ASHFOLD AWTM K34450

SIGNED

DESIGNED BY  
PAUL ASHFOLD AWTM K34450

SIGNED

APPROVED BY  
PAUL ASHFOLD AWTM K34450

SIGNED

TITLE  
LOT 786  
MARINA VILLAGE  
STAGE 2: SINGLE LANE SHUTTLE  
OPERATION

DRAWING NUMBER  
6103-02-01

REVISION  
0

SIZE  
A3

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38 Beaconsfield Avenue,  
Midvale, WA, 61056  
Ph: (08) 9356 7750  
Fax: (08) 9356 7751  
www.carringtonswa.com

# LOT 786 MARINA VILLAGE

## STAGE 3: SHORT-TERM ROAD CLOSURE

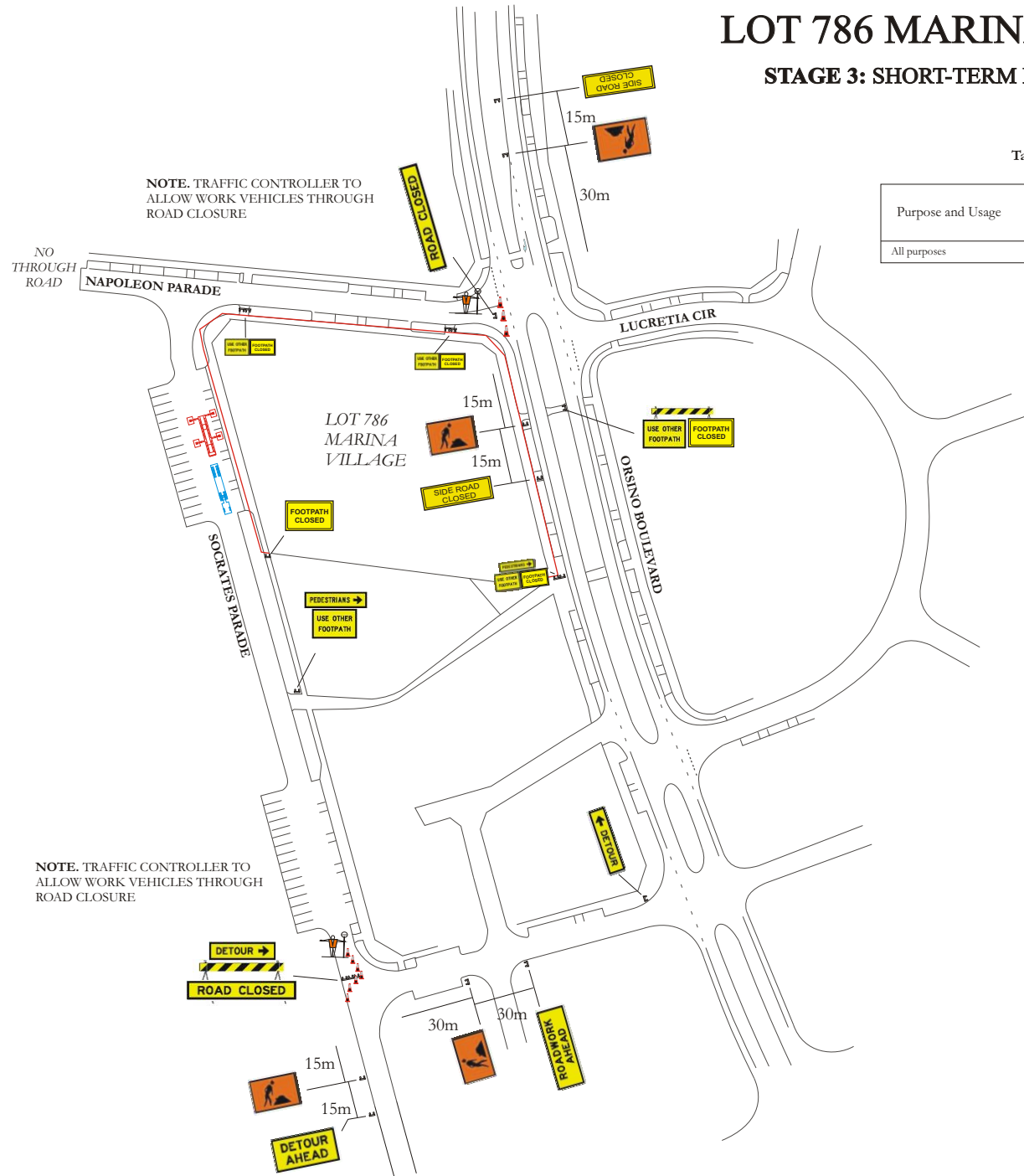


Table 3.7 AS 1742.3-2009  
CONE SPACINGS

Purpose and Usage	Traffic Speed, km/h	Recommended Maximum Spacing (m)
All purposes	<50	4

TABLE 4.2 AS 1742.3-2009  
VALUE OF DIMENSION D

SPEED OF TRAFFIC KM/H	DIMENSION D m
55 or less	15
56 to 65	45
Greater than 65	Equal to speed of traffic, in km/h

### NOTES

- All sign locations are to be checked prior to setout and positions adjusted to allow for specific site constraints such as vegetation, other signs and roadside furniture.
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- Reflective traffic cones to be spaced as per Table 3.7 of AS 1742.3-2009.
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- Spacing of delineation devices:
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### LEGEND

<b>WORK AREA</b>	
<b>SIGN LOCATION</b>	
<b>DELINEATION</b>	
<b>POSTED SPEED LIMIT</b>	<b>WORK SITE SPEED LIMIT</b>
50 km/h	50 km/h
<b>LOCATION</b>	
LOT 786 MARINA VILLAGE	
<b>CLIENT</b>	
DIPLOMA CONSTRUCTION	
<b>CONTACT</b>	
RICHARD KING 0430 581 35023	
<b>DATE</b>	<b>SCALE</b>
01/03/2013	NOT TO SCALE
<b>DRAWN BY</b>	<b>SIGNED</b>
PAUL ASHFOLD AWTM E34450	
<b>DESIGNED BY</b>	<b>SIGNED</b>
PAUL ASHFOLD AWTM E34450	
<b>APPROVED BY</b>	<b>SIGNED</b>
PAUL ASHFOLD AWTM E34450	

**LOT 786 MARINA VILLAGE**  
**STAGE 3: SHORT-TERM ROAD CLOSURE**

<b>DRAWING NUMBER</b>	<b>REVISION</b>	<b>SIZE</b>
6103-03-01	0	A3



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# **APPENDIX G**

## **JOB SAFETY ANALYSIS**



CARRINGTONS  
TRAFFIC SERVICES

## JOB SAFETY ANALYSIS

FORM

JSA 001  
Revision 2  
March 2009

PROJECT:	
CLIENT:	

DATE:	
LOCATION:	

REQUIRED PPE:

THIS FORM SHALL BE SIGNED OFF BY ALL TRAFFIC MANAGEMENT PERSONNEL AND SITE SUPERVISOR DAILY. HAZARDS IDENTIFIED WITHIN THIS FORM SHALL BE TREATED ACCORDINGLY AND ALL WORK SITE PERSONNEL SHALL BE DULY NOTIFIED OF THE IMMEDIATE OUTCOMES.

The following matrix and hierarchy of controls are intended as an aid in undertaking the risk analysis and establishing the control.

RISK CALCULATOR			
HOW LIKELY IS SOMEONE TO BE HURT	HOW SEVERELY COULD SOMEONE BE HURT		Class 1 Major Injury/Death
	Class 3 Medical/ First Aid	Class 2 LTI	
Certain	M	H	H
Possible	L	M	H
Unlikely	L	M	H

Each hazard must be assessed individually using the matrix. Where the hazard is assessed as "M" or "H", the "hierarchy of control" must be utilised to reduce risks and the control identified. Not doing the work must be considered where risks cannot be reduced to an acceptable level.

HIERARCHY OF CONTROL
<ul style="list-style-type: none"><li>• <b>Elimination</b> - removing the hazard or hazardous work practice.</li><li>• <b>Substitution</b> - substituting or replacing a hazard or hazardous work practice with a less hazardous one.</li><li>• <b>Isolation</b> - isolating or separating the hazard or hazardous work.</li><li>• <b>Engineering control</b> - if a hazard cannot be eliminated, substituted or isolated, an engineering control is the next <b>preferred</b> measure. This may include modifications to tools or equipment, or providing guarding barriers, ventilation or insulation.</li><li>• <b>Administrative control</b> - includes introducing work <b>practices</b> that reduce the risk, for example, job rotation, limited entry or limited time in hazard areas.</li><li>• <b>Personal protective equipment</b> - (eg masks, respirators, fall arrest equipment) should be considered only when other control measures are not practicable, or to increase protection.</li></ul>

Traffic (Sign)	Supervisor		Print Name		Date:	
Traffic (Sign)	Controller		Print Name		Date:	
Traffic (Sign)	Controller		Print Name		Date:	
Traffic (Sign)	Controller		Print Name		Date:	





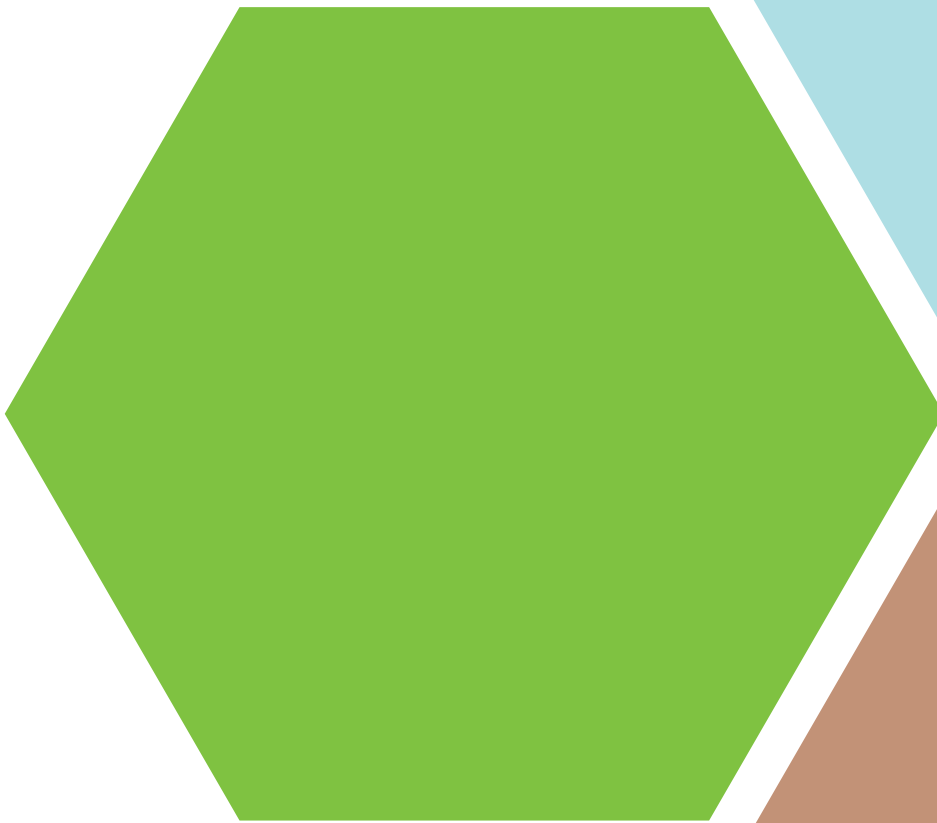


**CITY OF COCKBURN**

# Sustainability Action Plan 2013/14



**Governance** *Environment* *Society* *Economy*



The City of Cockburn's *Sustainability Action Plan 2013 – 2014*, forms part of the City's integrated reporting platform. This platform guides the strategic direction of the City towards sustainability. The reporting platform is comprised of the following strategic documents:



This action plan forms the basis of the City's annual *State of Sustainability Report*. An updated Action Plan will be presented to Council for adoption in May each year. In September each year the City's Executive and Strategic Business Management Group will be required to report progress against indicators, with the results of this being publicly reported in the City's annual State of Sustainability Report in November.

The City will achieve its vision for *A Better Tomorrow*, as the most attract place to live, work, visit and invest in, within the Perth Metropolitan area, by pursuing governance excellence to meet the needs of current and future generations through an integration of the environment, society and economy.



Governance is the cornerstone of the City's approach to sustainability. Through this the City is able to listen to and lead, its residents and ratepayers, in building a sustainable future.

## GOVERNANCE

Moving toward a sustainable future, the City recognises its role in leading while listening to the needs of the community. Being a progressive and responsible public sector organisation requires an approach that involves accountability and transparency.

### Strategic Community Plan:

- Attract, engage, develop and retain our staff in accordance with a long term Workplace Plan (7.6.1).

#### FOCUS AREA

Management, Accountability, Transparency and Engagement

#### OVERARCHING OBJECTIVE

Facilitate employee retention through strategies, processes and training

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 1.1	To attract, engage, develop and retain employees in accordance with a long term WorkPlace Plan.	Implement the City's long term Workforce Plan.	No significant adverse trends identified in employee attraction and engagement.	Manager, Human Resources.
Gov 1.2	Ascertain whether the City is offering an appropriate Employee Value Proposition (EVP) by measuring voluntary turnover and reporting it to the Executive.	Measure voluntary turnover annually and report to Executive.	Voluntary turnover for each financial year does not exceed the target set in the annual Business Plan.	Manager, Human Resources.
Gov 1.3	Maintain sustainable and competitive base salary rates for employees.	Benchmark collective agreement rates with similar Councils annually and report to Executive.	Collective agreement salary increases are attractive and affordable.	Manager, Human Resources.
Gov 1.4	Optimise the safety, health and wellbeing of the workforce.	Develop and promote a program of health and wellbeing activities.	Minimise the number of lost time injuries.	Manager, Human Resources.

### Strategic Community Plan:

- Develop infrastructure provision and renewal strategies that direct investment in ongoing infrastructure provision and management (5.2.1).
- Continue to implement the long term Asset Management Plan to deliver sustainable asset management (7.5.1).
- Implement a long term Financial Plan to deliver a sustainable financial future (7.5.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 2.1	Develop mechanisms which support best practice sustainable procurement.	Review status of sustainability clauses in tender documents.	100% of tenders contain a 10% sustainability weighting.	Executive.
Gov 2.2	Asset Consumption Ratios for the Asset Management Plan (AMP) adopted Based on Department of Local Government Framework (DLGF) <sup>1</sup> .	Annual report to determine average proportion of "as new" condition remaining of assets.	Between 50% and 75%	Asset Manager.
Gov 2.3	Asset Sustainability Ratios for the AMPs adopted Based on DLGF.	Annual report to determine if assets are being replaced at the rate they are wearing out.	Between 90% and 110%	Asset Manager.
Gov 2.4	Asset Renewal Funding Ratio's for the AMPs adopted Based on DLGF.	Annual report to determine if there is sufficient future funding for renewal and replacement of assets.	Between 95% and 105%	Asset Manager.

<sup>1</sup> Department of Local Government, *Asset Management Framework and Guidelines*, May 2011. Available at: <http://integratedplanning.dlg.wa.gov.au/>

### Strategic Community Plan:

- Establish and maintain effective communication channels and processes (7.1.1).
- Ensure appropriate governance systems are in place (7.3.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 3.1	Incorporate sustainability into the City's annual report.	Include a summary of sustainability performance in the City's annual report.	Summary of State of Sustainability included.	Manager, Corporate Communications.
Gov 3.2	Ensure sustainability is considered in every strategic report prepared by the City.	Align strategic reports with the City's Sustainability Strategy.	Strategic Documents to consider sustainability according to focus areas identified in Sustainability Strategy.	Executive.

#### FOCUS AREA

Management, Accountability, Transparency and Engagement

#### OVERARCHING OBJECTIVE

Adopt best practice in sustainable procurement and asset management

#### FOCUS AREA

Management, Accountability, Transparency and Engagement

#### OVERARCHING OBJECTIVE

Ensure sustainability forms an integral part of corporate strategic planning



## Community Strategic Plan:

- Develop and manage relationships with key stakeholders (7.2.2).
- Determine community requirements and report on performance and outcomes (7.3.1).
- Identify and implement initiatives to improve customer service, business processes and innovation in service delivery (7.4.1).
- Develop a framework for continuous business process improvement (7.4.2).
- Identify and manage corporate risk (7.7.1).
- Ensure active compliance with relevant legislation, policy and guidelines (7.7.2).

### FOCUS AREA

Management, Accountability, Transparency and Engagement

### OVERARCHING OBJECTIVE

Maintain best practice public service delivery for the City's key stakeholders

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 4.1	Continuously improved feedback in customer service surveys.	Monitor and report annual customer survey results.	Initiatives identified and implemented annually to maintain or improve service outcomes.	Manager, Corporate Communications.
Gov 4.2	To be a leader among local government in service delivery.	Identify and respond to key areas of concern as identified by the community in the annual Perceptions Survey.	Initiatives identified and implemented.	Manager, Corporate Communications.
Gov 4.3	Identify and manage corporate risk.	Develop and maintain Corporate Risk Register.	Risk register for each service unit prepared.	Executive.
Gov 4.4	Ensure active compliance with relevant legislation, policy and guidelines.	Complete annual compliance return.	100% compliance.	Executive.



## Strategic Community Plan:

- Ensure our strategic land use planning embraces sustainable development principles and reflects the values held by the community (1.1.1).
- Apply structure planning for new development areas which embrace best practice and community creation (1.2.2).
- Ensure that neighbourhoods are interconnected physically, economically, socially and technologically, to minimise energy dependency (1.3.2).
- Ensure our strategic land use planning in the form of: the Local Planning Strategy, Town Planning Scheme, revitalisation strategies and structure plans, achieves a robust planning framework delivering adequate housing supply and diversity in housing choice (1.4.1).
- Enhance the City's public transport advocacy programs (6.1.2).
- Develop and implement walkway, bike and trails master plans (6.2.1).
- Develop and promote the City's TravelSmart initiative (6.2.2).
- Develop a transport network that effectively caters for demand and growth across various modes (6.3.2).
- Work with stakeholders to provide and support end of journey facilities (6.5.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 5.1	To plan the efficient size, shape and composition of neighbourhoods, based upon a 5 minute (400m) walk from the neighbourhood's centre to its perimeter.	Incorporate Element 1 of Liveable Neighbourhoods into all structure plans <sup>2</sup> .	100% of structure plans comply with Element 1 of Liveable Neighbourhoods.	Manager, Strategic Planning.
Gov 5.2	To plan each neighbourhood with a focal point, which comprises a minimum mix of uses which come together to form a community focus.	Incorporate Element 7 of Liveable Neighbourhoods into all Structure Plans.	Each structure plan contains a minimum mix of uses and should include urban open space, community facilities, retail uses, postal facilities and public transport stops.	Manager, Strategic Planning.
Gov 5.3	To plan neighbourhoods with a range of residential densities which increase towards the neighbourhood's centre.	Incorporate Element 3 of Liveable Neighbourhoods into all Structure Plans.	Minimum of three residential codings in each structure plan.	Manager, Strategic Planning.

### FOCUS AREA

Sustainable Planning and Development

### OVERARCHING OBJECTIVE

Support increased walking, cycling and public transport use through the development of neighbourhoods with mixed housing types and densities

<sup>2</sup> Liveable Neighbourhoods: a Western Australian sustainable cities initiative. Western Australian Planning Commission and the Department for Planning and Infrastructure, October 2007. Available at: <http://www.planning.wa.gov.au/publications/919.asp>

## Community Strategic Plan:

- Continue with the development of existing urban revitalisation strategies and plan for new ones (1.2.1).
- Ensure that our neighbourhoods are designed to be more compact, attractive and energy efficient to accommodate a mixture of uses (1.3.2).
- Work with stakeholders to establish, renew or expand commercial centres that increase diversity, accessibility, employment and amenity (1.5.2).

### FOCUS AREA

Sustainable Planning and Development

### OVERARCHING OBJECTIVE

Support local employment, service provision and leisure opportunities through the delivery of mixed use neighbourhoods

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 6.1	To plan new neighborhoods such that sufficient and appropriate sites are identified (and secured) in structure plans for local employment opportunities.	Incorporate Element 7 of Liveable Neighbourhoods into each structure plan.	Minimum of two non-residential land uses in each structure plan.	Manager, Strategic Planning.
Gov 6.2	To plan the layout and location of land for local employment and business uses as part of mixed use neighbourhood centres and co-located with the major transport networks (including public transport).	Incorporate Element 2 of Liveable Neighbourhoods into each structure plan.	Degree to which neighbourhood design complies with Element 2 of Liveable Neighbourhoods.	Manager, Strategic Planning.
Gov 6.3	To implement the Phoenix Central Revitalisation Strategy.	Review and progress the Phoenix Central Revitalisation Strategy.	Actions completed against timelines by service units across the organisation.	Manager, Strategic Planning.
Gov 6.4	To implement the Hamilton Hill Revitalisation strategy.	Review and progress the Hamilton Hill Revitalisation Strategy.	Actions completed against timelines by service units across the organisation.	Manager, Strategic Planning.





### Strategic Community Plan:

- Develop and implement strategies to facilitate the efficient and sustainable movement of people and goods (6.1.1).
- Enhance the City's public transport advocacy programs (6.1.2).
- Develop and implement walkway, bike and trails master plans (6.2.1).
- Identify and address safety issues across the transport networks (6.3.1).
- Develop a transport network that effectively caters for demand and growth across various modes (6.3.2).
- Work with stakeholders to provide and support end of journey facilities (6.5.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 7.1	Enhance the amenity of public streetscapes to facilitate walking.	Review the Footpath Strategy and Greening Plan.	Strategies reviewed.	Manager, Engineering Services; Manager, Parks and Environment.
Gov 7.2	Increase the opportunity for cycling through the City.	Implement the City's Bike Plan.	\$200,000 annual allocation to the Bike Plan.	Manager, Engineering Services.
Gov 7.3	Improve modal transport opportunities.	Identify suitable routes for the potential development of rapid bus/light rail within the City.	Preparation of a Plan for endorsement by Council.	Manager, Strategic Planning.
Gov 7.4	Encourage commercial/business developments to incorporate end of trip facilities.	Preparation and endorsement of a local planning policy to ensure appropriate end of trip facilities.	Adoption by Council.	Manager, Statutory Planning.

#### FOCUS AREA

Sustainable Planning and Development

#### OVERARCHING OBJECTIVE

Facilitate opportunities for sustainable transport

### Strategic Community Plan:

- Adopt best practice management for our natural environment (4.2.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Gov 8.1	Ensure significant natural areas are identified and documented in all new structure plans for the purposes of long-term retention.	Develop a definition of a significant natural area for inclusion in the Bushland Retention Plan for the purpose of environmental conservation.	Definition developed and included in Plan.	Manager, Parks and Environment.
Gov 8.2	Ensure significant natural areas are identified and documented in all new structure plans for the purposes of long-term retention.	Develop a Bushland Retention Plan, to be included in the Natural Area Management Strategy, which identifies significant natural areas and develops measures, procedures and actions to protect these areas both on public and private lands.	Plan included in Natural Area Management Strategy in 2013/14 financial year.	Manager, Parks and Environment.
Gov 8.3	Safeguard long term protection of the City's actively managed natural areas.	Undertake an audit of the City's actively managed natural areas to ensure that each has been identified with a reserve purpose and management order that is consistent with the aim of conservation as mandated in the Land Administration Act 1997.	All natural areas managed by the City are identified with a reserve purpose and management order consistent with the aim of conservation reserve.	Manager, Strategic Planning; Manager, Parks and Environment.

#### FOCUS AREA

Sustainable Planning and Development

#### OVERARCHING OBJECTIVE

Strategically plan for the sustainable long-term retention of significant natural areas



The environment is the foundation for sustainability in the City of Cockburn. Our natural areas and resources must be sustainably managed into the future.

## ENVIRONMENT

Moving toward a sustainable future, the City recognises its role in leading and listening to the needs of the community. Being a progressive and responsible public sector organisation requires an approach that involves accountability and transparency.

### Strategic Community Plan:

- Adopt best practice management for our natural environment (4.2.1).
- Develop a coastal area management strategy (4.2.2).
- Actively pursue remediation and adaptation strategies in areas where the natural environment is at risk (4.2.3).

#### FOCUS AREA

Environmental Management

#### OVERARCHING OBJECTIVE

Maintain, conserve and enhance ecosystems for present and future generations

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 1.1	Manage coastal environments for the long term benefit of the community.	Develop a Coastal Adaptation Plan.	Draft Adaptation Plan Developed.	Manager, Infrastructure Services.
Env 1.2	Improve the condition of vegetation within bushland reserves.	Compare the condition of vegetation within bushland reserves by mapping 25% of the bushland area annually (100% over a 4 year period) and reviewing the condition against previous surveys.	Vegetation in good or better condition is increasing against base year figure of 62% in 2010.	Manager, Parks and Environment.
Env 1.3	Reduce the prevalence of invasive weed species in the City.	To control and manage environmental weeds within Council managed natural areas.	A reduction in high priority weeds identified within reserves.	Manager, Parks and Environment.
Env 1.4	Increase investment in the City's natural areas.	Annual increase in natural area funding as detailed in the Natural Area Management Strategy.	Increase in annual funding per hectare.	Manager, Parks and Environment.

## Strategic Community Plan:

- Adopt best practice management for our local environment (4.2.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 2.1	To ensure the ongoing rehabilitation of degraded natural areas.	Plan to revegetate a minimum of 2.5 hectares annually.	Complete 2.5 hectares of revegetation annually with an emphasis on enhancing ecological corridors linking natural areas.	Manager, Parks and Environment.
Env 2.2	To maintain genetic diversity and genetic viability across natural areas.	Develop incentives and information packages and offer training to private landowners and residents to encourage management of natural areas on private property and the use of local species within gardens.	Annual increase in the number of private landowners participating in incentive programs and number of training workshops delivered by the City.	Manager, Parks and Environment.
Env 2.3	To create streetscapes that enhance the ecological viability and aesthetic appeal of the road network.	Develop a native species list for streetscapes	% increase in the number of local native species being used in land streetscape design and enhancement programs.	Manager, Parks and Environment.
Env 2.4	To support local residents in increasing the urban biodiversity value of their properties.	Promote and subsidise the purchase of local native plants by property owners.	Minimum 5% annual increase in funding for this program.	Manager, Parks and Environment.

## Strategic Community Plan:

- Actively pursue remediation and adaptation strategies in areas where the natural environment is at risk (4.2.3).
- Implement human health risk management strategies (4.3.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 3.1	Monitor local urban air quality.	Investigate complaints relating to air pollution including dust and odour	Reduction in number of air pollution complaints (per capita).	Manager, Health Services.
Env 3.2	Minimise risks to human health through food.	Conduct health assessments of all food premises.	Reduction in the number of improvement notices served on food premises.	Manager, Health Services.
Env 3.3	Ensure all City public buildings are safe for the community.	Conduct health assessments of all public buildings in the City.	Reduction in the number of non-compliant public buildings.	Manager, Health Services.
Env 3.4	Monitor and manage incidence of pests in the City.	Investigate complaints associated with vectors/pests.	Reduction in number of vector/pest complaints across the City (per capita).	Manager, Health Services.

### FOCUS AREA

Environmental Management

### OVERARCHING OBJECTIVE

Establish and enhance ecological corridors

### FOCUS AREA

Sustainable Planning and Development

### OVERARCHING OBJECTIVE

Identify, manage and minimise risks to human health.

### Strategic Community Plan:

- Actively pursue remediation and adaptation strategies in areas where the natural environment is at risk (4.2.3).
- Implement human health risk management strategies (4.3.1).

#### FOCUS AREA

Environmental Management

#### OVERARCHING OBJECTIVE

Promote stewardship of the natural environment.

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 4.1	Improve the vegetation condition within natural areas.	Undertake community planting events.	Conduct a minimum of three events annually.	Manager, Parks and Environment.
Env 4.2	Build environmental action partnerships.	Develop partnerships with external agencies (corporate, NGOs or State Government).	Develop and implement a minimum of two programs	Manager, Parks and Environment.
Env 4.3	Enhance community participation in environmental action.	Develop and implement community training programs.	Deliver a minimum of two community training activities annually.	Manager, Parks and Environment.
Env 4.4	Promote environmental stewardship in the community.	City Environmental Services to have a presence at public events.	Attend two events annually.	Manager, Parks and Environment.

### Strategic Community Plan:

- Implement sustainable resource management strategies (4.1.1).

#### FOCUS AREA

Efficient settlements and use of resources

#### OVERARCHING OBJECTIVE

Implement best practice water management strategies.

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 5.1	To decrease potable and non-potable water use across the City.	Implement local water action plan.	To reduce community per capita and corporate scheme water consumption by 5 percent below 2007/08 levels by 2017/18.  To improve efficiency in corporate groundwater use by reducing consumption to 10 percent below the 2007/08 Department of Water allocations per hectare by 2017/18.	Manager, Parks and Environment.
Env 5.2	Demonstrate leadership in water conservation.	Complete waterwise Council criteria.	Waterwise Council status achieved.	Manager Parks and Environment.
Env 5.3	To improve the urban environment – water quality	Collect samples of water from beaches, public swimming pools and businesses without scheme water.	Reduce the number of substandard water samples collected.	Manager, Health Services

### Strategic Community Plan:

- Implement energy management strategies (4.5.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 6.1	Continued investment in renewable energy generation.	Implement the City's renewable energy target.	20% renewable energy generation by 2020.	Manager, Infrastructure Services.
Env 6.2	Strategically plan the City's investment in renewable energy.	Develop an overall renewable energy implementation plan.	Council endorsement of renewable energy implementation plan.	Manager, Parks and Environment.

#### FOCUS AREA

Environmental Management

#### OVERARCHING OBJECTIVE

Increase the use of renewable energy.

### Strategic Community Plan:

- Implement programs to reduce and manage the City's and community's carbon footprint (4.5.1).
- Implement energy management strategies (4.5.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 7.1	Reduce Greenhouse Gas Emissions (GGE) from electricity and fuel consumption.	Implementation of Greenhouse Gas Emission Reduction Strategy and Action Plan.	20% below 2008/09 levels by 2020.	Manager, Parks and Environment.
Env 7.2	Minimise GGE from waste at Henderson Waste Recovery Park (HWRP).	Implementation of waste minimisation and management actions.	No more than 45% above 2008/09 levels by 2020.	Manager, Parks and Environment.
Env 7.3	Facilitate community greenhouse gas emissions reduction.	Develop a business case for the construction of a sustainable home in the City.	Business case developed.	Manager, Parks and Environment.

#### FOCUS AREA

Sustainable Planning and Development

#### OVERARCHING OBJECTIVE

Reduce greenhouse gas emissions

### Strategic Community Plan:

- Review the Strategic Waste Management Strategy (4.4.1).
- Investigate and implement appropriate waste minimisation programs and new technologies (4.4.2)

KPI No.	Annual Objective	Action	KPI	Responsibility
Env 8.1	Increase the recovery rate of re-useable materials at HWRP.	Deploy two excavators to the active face.	Increase total recovery to 4%.	Waste Services Manager.
Env 8.2	Commercial Materials Recovery Facility (CMRF)	Budget for the Scoping, Design and Documentation of a CMRF.	Business Case Approval.	Waste Services Manager.
Env 8.3	Promotion of waste separation and recycling.	Development of the City's Strategic Waste Management Strategy.	Adoption of the Strategy.	Waste Services Manager.

#### FOCUS AREA

Sustainable Planning and Development

#### OVERARCHING OBJECTIVE

Reduce waste and increase recycling.



Society is the heart of sustainability in Cockburn. Our people – from our residents, ratepayers and businesses, to schools, visitors and employees – inform the way we develop now and into the future.

## SOCIETY

### Strategic Community Plan:

- Develop local community plans across the City that create cohesiveness and embrace diversity (2.1.1).
- Facilitate equal access for our community to facilities and services (2.4.1).
- Recognise, engage, include and celebrate the significance and richness of local Indigenous and diverse multicultural groups (2.7.1).

#### FOCUS AREA

Sense of Place and Healthy Communities

#### OVERARCHING OBJECTIVE

Enhance social inclusion, equity and diversity.

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 1.1	Facilitate an equitable and inclusive Community, particularly for those who experience disadvantage.	Implement the Disability Access and Inclusion Plan.	90% of annual Disability Access and Inclusion Plan (DAIP) actions completed.	Manager, Human Services.
Soc 1.2	Provision of inclusive and accessible community services and leisure activities to meet diverse community needs.	Provide the community services, leisure activities and facilities contained in the Age Friendly Strategic Plan, The Children's Services Strategic Plan, the Youth Services Strategic Plan, the Disability Access and Inclusion Plan, the Reconciliation Action Plan and the Recreation Services Strategic Plan.	90% of Actions contained within the Strategic Plans are implemented in accordance with identified time frames each year.	Manager, Human Services.
Soc 1.3	To assist those who are most vulnerable in our Community.	Develop and implement a Social Cohesion Plan.	Social Cohesion Plan adopted by Council. Implementation commenced.	Manager, Human Services.



## Strategic Community Plan:

- Provide and facilitate quality community services that meet diverse recreational, cultural and community needs of all age groups (2.2.1).
- Provide and facilitate community activities, events and programs that draw a wide cross-section of the community (2.3.1).
- Provide and promote activities, services and recreational facilities that encourage our community toward an active and healthy lifestyle (2.6.1).
- Develop multi-use facilities that cater for all ages, abilities and cultures to promote community interaction (5.1.2).
- Develop and promote the City's TravelSmart initiative (6.2.2).
- Advocate for the needs of the community and continue to progress opportunities for the City (7.2.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 2.1	Increase the capacity of the City to develop and deliver Healthy Lifestyle initiatives.	Employ a qualified health promotion officer.	Officer employed in the 13/14 financial year.	Manager, Health Services.
Soc 2.2	Provide information, incentives and ongoing support and motivation to encourage people to be more active and travel less by car.	Implement the ActiveSmart/ TravelSmart program in targeted areas.	<ul style="list-style-type: none"> <li>• Walking trips increased by 20%</li> <li>• Cycling trips increased by 50%</li> </ul>	Manager, Health Services.
Soc 2.3	To centrally locate health and community facilities for residents on the eastern side of the municipality.	Complete Detailed Design for the Cockburn Regional Aquatic and Recreation Community Facility.	Adopted by Council.	Manager, Community Services.

### FOCUS AREA

Sense of Place and Healthy Communities

### OVERARCHING OBJECTIVE

Provide well located community services and facilities to meet identified community needs and facilitate healthy lifestyles.



### Strategic Community Plan:

- Provide and facilitate initiatives that improve safety for our communities (2.5.1).
- Identify and address safety issues across the transport networks (6.3.1).

#### FOCUS AREA

Sense of Place and Healthy Communities

#### OVERARCHING OBJECTIVE

Develop safe communities.

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 3.1	To ensure the City responds to the Community's safety concerns in a timely manner.	Ensure the response times of CoSafe Officers are within 15minutes of original call being placed.	85% of call outs responded to within 15 minutes.	Manager, Community Services.
Soc 3.2	To reduce the likelihood of criminal offence for at-risk youth in the community.	Meet the annual targets of engagement as part of the City's Youth Diversion Program.	168 youth engaged in the program annually.	Manager, Human Services.
Soc 3.3	Increase the sense of security at identified sites within the City.	Review the implementation of priorities identified in the City's CCTV Strategy.	CCTV Implementation Strategy funded.	Manager, Community Services.
Soc 3.4	Incorporate crime prevention through environmental design into the City's development processes.	Develop a Crime Prevention through Environmental Design Policy.	Policy adopted.	Manager, Community Services.

### Strategic Community Plan:

- Recognise, engage, include and celebrate the significance and richness of local Indigenous and diverse multicultural groups (2.7.1).
- Protect the heritage of the City through advocacy, statutory controls, promotion and education (2.8.1).

#### FOCUS AREA

Sense of Place and Healthy Communities

#### OVERARCHING OBJECTIVE

Protect and promote the City's cultural heritage and diversity.

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 4.1	To value and celebrate Indigenous culture, heritage and participation.	Implement the Reconciliation Action Plan	90% of actions contained within the plan are implemented within the required timeframe.	Manager, Human Services.
Soc 4.2	Promote Indigenous Community Development.	To seek ongoing sources of funding to increase the role of the Aboriginal Community Development Officer from part time to full time.	Funding secured and officer employed as a full time staff member in the 2013/14 financial year.	Manager, Human Services.
Soc 4.3	Celebrate and promote cultural diversity.	Identify funding for the development of a multi-cultural strategy and officer.	Funding sourced identified.	Manager, Human Services.
Soc 4.4	Safeguard the protection of City heritage sites.	Regularly update the City's Heritage List.	Review of list no longer than 12 months.	Manager, Strategic Planning.



## Strategic Community Plan:

- Provide and facilitate community activities, events and programs that draw a wide cross-section of the community (2.3.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 5.1	Provide annual events in response to identified community needs.	Undertake research in 2012/13 to determine what the community wants events-wise and use this information to develop an Events Strategy.	Adoption of Events Strategy in the 13/14 financial year.	Manager, Corporate Communications.
Soc 5.2	Promote City sponsored events widely in the community.	Develop comprehensive marketing plan including social media.	Annual increase in community attendance at major City events.	Manager, Corporate Communications.
Soc 5.3	Ensure City events are accessible across the community.	Work with the Disability, Access and Inclusion Officer to increase accessibility across City events.	All event planning in the 13/14 financial year to include a checklist for disability access and inclusion.	Manager, Corporate Communications.
Soc 5.4	Implement a strategic approach to increasing community awareness of City events, activities, services and facilities.	Implement a Corporate Communications Strategy.	% of actions met against target in Strategy.	Manager, Corporate Communications.

### FOCUS AREA

Community Involvement

### OVERARCHING OBJECTIVE

Encourage community involvement in local events and activities



### Strategic Community Plan:

- Provide and facilitate community activities, events and programs that draw a wide-cross section of the community (2.3.1).
- Promote sustainable practices within the community (4.1.2).

#### FOCUS AREA

Community Involvement

#### OVERARCHING OBJECTIVE

Develop a strategic approach to community engagement and education

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 6.1	Adopt a strategic approach to community engagement.	Develop a Community Engagement Strategy.	Strategy adopted in the 2013/14 financial year.	Manager, Community Services.
Soc 6.2	Deliver a minimum of 10 environmental education for sustainability initiatives and events.	Deliver a range of diverse environmental education initiatives and events to the community.	Complete minimum number of events.	Manager, Parks and Environment.
Soc 6.3	Deliver a minimum of 10 social and/or cultural education initiatives.	Deliver a range of social and/or cultural awareness events and initiatives to the community.	Complete minimum number of events.	Manager, Human Services.

### Strategic Community Plan:

- Maintain urban art investment and other initiatives that create interesting community places and encourage creativity (5.4.2).

#### FOCUS AREA

Community Involvement

#### OVERARCHING OBJECTIVE

Enhance a sense of community ownership and promote Cockburn as an attractive place to live, work and visit

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 7.1	To support the development of vibrant community spaces through investment in urban art.	Maintain a constant investment in public art.	Annual investment in public art.	Manager, Corporate Communications.
Soc 7.2	To ensure the City of Cockburn primary brand is used consistently in all marketing, communication and service points.	Update the City's Corporate Style Guide.	Up to date Style Guide adopted by Council.	Manager, Corporate Communications.
Soc 7.3	Promote the City's response to recognised community priorities.	Monitor and report on results from perceptions survey.	Perceptions survey results.	Manager, Corporate Communications.



## Strategic Community Plan:

- Develop local community plans across the City that creative cohesiveness and embrace diversity (2.1.1).
- Provide and facilitate quality community services that meet diverse recreational, cultural and community needs of all age groups (2.2.1).
- Support the development of local community associations and other advocacy groups (2.3.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Soc 8.1	Strengthen the capacity of community groups.	Monitor the % of completed projects for community groups in the City through the community grants program.	% of completed projects against grants received.	Manager, Community Services.
Soc 8.2	Strengthen networks between community groups.	Facilitate linkages between community groups in the City.	Increased number of joint projects between community groups.	Manager, Community Services.
Soc 8.3	Facilitate partnerships between business and community groups.	Host an annual business and community group networking event.	80% capacity attendance at the event.	Manager, Community Services.
Soc 8.4	Actively encourage volunteering in the City of Cockburn.	Market and promote volunteering opportunities in the City.	Annual increase in the number of registered volunteers.	Manager, Community Services.

### FOCUS AREA

Community Involvement

### OVERARCHING OBJECTIVE

Build community resilience through strengthened community groups, relationships and linkages.





A strong economy underpins the city's sustainable development and must be robust and resilient in the face of future uncertainty and risk. The City's economy is integrated with its society and environment.

## ECONOMY

### Strategic Community Plan:

- Engage stakeholders on the delivery of industrial, commercial and infrastructure projects (3.2.1).
- Facilitate and promote economic development aligned to business centre growth (3.3.1).
- Work with stakeholders to identify a holistic regional approach to freight management (6.4.1).

#### FOCUS AREA

Economic Development

#### OVERARCHING OBJECTIVE

Implement a strategic approach to economic development.

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 1.1	Pursue a strategic approach to economic development.	Develop an economic development strategy for the City of Cockburn.	Strategy to be adopted by Council.	Executive.
Eco 1.2	Implement Economic Development in the City.	Determine the priority for an economic development office.	An economic development officer is identified in the Workforce Plan.	Executive.
Eco 1.3	Consider what role tourism will play for the City.	Determine whether tourism should be incorporated into an economic development strategy or as a stand-alone strategy.	Identified within the Economic Development Strategy.	Executive.
Eco 1.4	Consider what future role the tertiary education sector will occupy in the City's economic development.	Consider the role of the tertiary sector in the City's economic development strategy.	Identified within the Economic Development Strategy.	Executive.

## Strategic Community Plan:

- Ensure that the City's sustainable development framework drives and enables diverse business investment and activities (3.2.2).
- Develop and implement a City infrastructure plan that meets current and future needs (5.1.1).
- Develop and implement strategies to facilitate the efficient and sustainable movement of people and goods (6.1.1).
- Develop and manage relationships with key stakeholders (7.2.2).

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 2.1	Ensure the City continues to attract business investment through the construction of an enhanced road network.	Ensure funds are available to meet the road development program as outlined in the Long Term Financial Plan.	Number of projects completed against target.	Executive.
Eco 2.2	Lobby for greater investment in public transport within the City of Cockburn.	Continue to advocate for the construction of the Success Railway Station and car parking facilities.	Report of advocacy efforts to achieve this objective.	Executive.
Eco 2.3	Identify gaps in strategic infrastructure which inhibit business investment in the City.	Survey business to determine gaps in strategic infrastructure.	Report received.	Executive.

### FOCUS AREA

Economic Development

### OVERARCHING OBJECTIVE

Attract business investment through the facilitation of strategic infrastructure

## Strategic Community Plan:

- Facilitate and promote economic development aligned to business centre growth (3.3.1).
- Work in partnership with Federal and State Government and other key stakeholders to provide infrastructure (5.3.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 3.1	To relieve traffic congestion in Cockburn Central.	Seek a commitment from the appropriate stakeholders for the timely construction of the North Lake Road Freeway Overpass, and the upgrades to the surrounding road network, which have been identified in the City's Major and Regional Road Network Strategy.	Report on agency responsibility and proposed timeframes for upgrades to the network as identified in the Major and Regional Road Network Strategy.	Executive.
Eco 3.2	Facilitate the development of vibrant local activity centres.	Implement the Local Commercial and Activity Centres Strategy.	% of actions within the Strategy complete against target.	Executive.
Eco 3.3	Support local enterprise in the City.	Engage with the Melville Cockburn Chamber of Commerce, South West Group and Business Foundations Group to determine the needs of existing businesses within the City.	Bi-annual meeting with stakeholder groups.	Chief Executive Officer.

### FOCUS AREA

Economic Development

### OVERARCHING OBJECTIVE

Facilitate the development of local enterprise<sup>3</sup>

<sup>3</sup> See economic profile for the City of Cockburn, <http://economy.id.com.au/Default.aspx?id=349&pg=12000>

### Strategic Community Plan:

- Promote sustainable practices within the community (4.1.2).
- Provide and facilitate community activities, events and programs that draw a wide-cross section of the community (2.3.1).

#### FOCUS AREA

Economic Development

#### OVERARCHING OBJECTIVE

Grow tourism in Cockburn through the management, improvement and promotion of the City's key natural, cultural and commercial features.

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 4.1	Promote Cockburn as the most attractive place to live, work, visit and invest in the Perth Metropolitan area.	Develop a registry of the City's key natural, cultural and commercial features.	Registry developed.	Executive.
Eco 4.2	Support the continued establishment of the Sustainability Precinct at Bibra Lake.	Complete the architectural designs for the construction of the Sustainability Centre to enable grant funding to be sourced.	Design drawings completed and consultation for user groups concluded.	Executive.

### Strategic Community Plan:

- Work with stakeholders to ensure serviced and accessible industrial land incorporating technology and education is planned and delivered (1.5.1).
- Identify initiatives and incentives to broaden the range of educational facilities, programs and partnerships (3.4.1).

#### FOCUS AREA

Employment Opportunities

#### OVERARCHING OBJECTIVE

Support Cockburn residents accessing local high value jobs through targeted programs of training and development.

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 5.1	Support the development of Vocational Education and Training Educational Institutions in the City of Cockburn.	Advocate for increased funding and resources for Challenger TAFE from State and Federal Government.	Report from Challenger TAFE on funding achievements.	Chief Executive Officer.
Eco 5.2	Support the development of further education facilities in the City of Cockburn.	Continue discussions with Curtin University to locate a Department in Cockburn Central West.	An agreement is resolved between the City and Curtin University.	Executive.

### Strategic Community Plan:

- Attract, engage, develop and retain our staff in accordance with a long term Workplace Plan (7.6.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 6.1	Continue to maintain a strong trainee development program.	Maintain the funding to facilitate the trainee program.	Continue to provide a minimum of five trainee positions annually.	Manager, Human Resources.
Eco 6.2	Enhance the trainee develop program In the City of Cockburn.	Seek additional external funds to employ more trainees.	External funding received.	Executive.
Eco 6.3	Maintain the City's Employee Development Policy.	Secure consistent annual funding for this program.	Annual investment in Employee Development Policy maintained.	Manager, Human Resources.



### Strategic Community Plan:

- Engage stakeholders on the delivery of industrial, commercial and infrastructure projects (3.2.1).
- Ensure that the City's sustainable development framework drives and enables diverse business investment and activities (3.2.2).
- Work in partnership with Federal and State Government and other key stakeholders to provide infrastructure (5.3.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 7.1	Enhance access to public transport via a paid parking facility for non-commuter uses of the rail facility.	To seek partners for the potential development of a paid parking station at Cockburn Central.	Partnership established – Yes/No	Executive.
Eco 7.2	Enhance existing retail precincts.	To work with Perron Group for the next stage of the Gateway Precinct and construction of the associated infrastructure.	Development plans finalised and road network designs improved.	Executive.
Eco 7.3	Work with the State Government, LandCorp and associated stakeholders in the delivery of the Latitude 32 and Australian Marine Complex Precincts.	Continue to engage with all stakeholders regarding planning and development of these precincts.	Precinct Structure Plan finalised in the 13/14 financial year.	Manager, Strategic Planning.

#### FOCUS AREA

Employment Opportunities

#### OVERARCHING OBJECTIVE

Develop and promote strategic partnerships with stakeholders involved in the industrial/commercial precincts.

### Strategic Community Plan:

- Facilitate and promote economic development aligned to business centre growth (3.3.1).

KPI No.	Annual Objective	Action	KPI	Responsibility
Eco 8.1	Support for business operators within the municipality.	Continue to support and co-fund the Melville Cockburn Chamber of Commerce (MCCC).	Report from MCCC on activities held within the municipality.	Executive.
Eco 8.2	Promote small businesses in the City of Cockburn.	Establish a 'Cockburn Business Corner' page on the City's website promoting local business	Website page established.	Executive.

#### FOCUS AREA

Employment Opportunities

#### OVERARCHING OBJECTIVE

Promote the small business sector in Cockburn through key strategic alliances.

## Sustainability Action Plan Implementation

Review current Sustainability Action Plan (SAP)	Jan - Mar
Develop SAP for following financial year	Jan - Mar
Presentation of reviewed SAP to Council	June
Seek Feedback on Progress towards achievement of KPIs in current SAP	August
Publicly report on Current SAP in State of Sustainability Report in November	Sept - Oct
State of Sustainability Report to Council for adoption	November



# City of Cockburn

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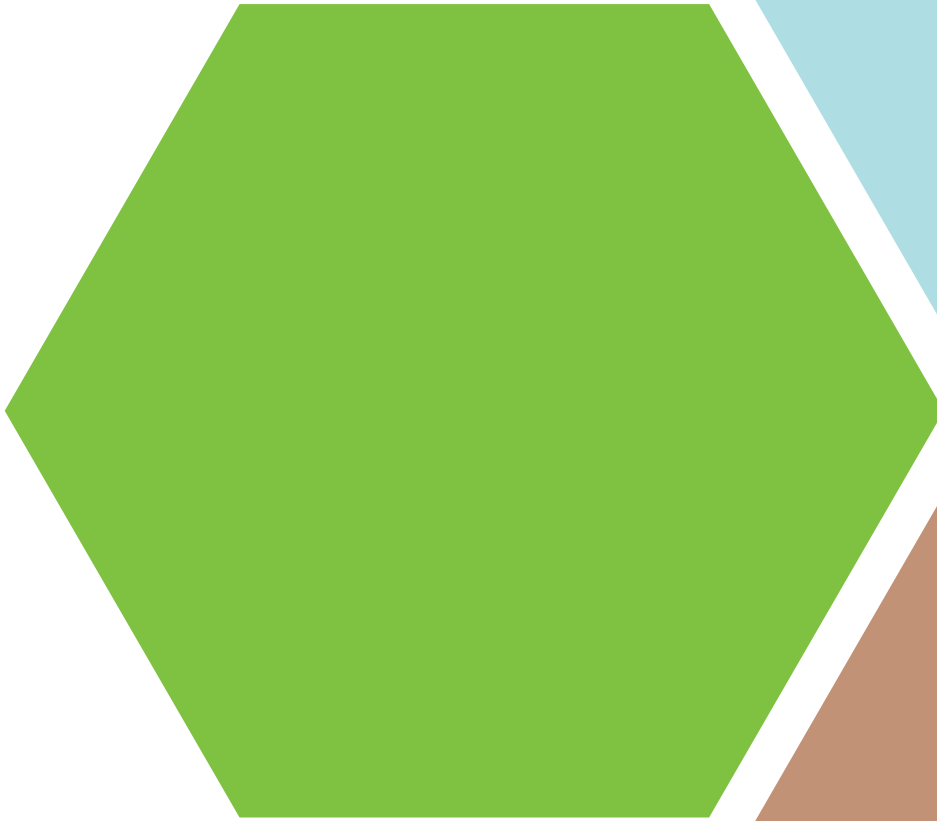


CITY OF COCKBURN

# Sustainability Strategy 2013 - 2017



Governance Environment Society Economy



## INTRODUCTION

The City of Cockburn's *Sustainability Strategy* is the document that outlines the City's overarching approach to sustainability. It is set within the context of a broader *Integrated Reporting Platform*, which guides the strategic planning of the City of Cockburn. The hierarchy of documentation is outlined here:



## A BRIEF HISTORY OF SUSTAINABLE DEVELOPMENT

### 1987 – World Commission on Environment and Development

In 1987, the term ‘sustainable development’ was first developed in response to an international awareness that development was not only causing environmental and resource degradation, but that the gap between the world’s rich and poor was widening. The original definition of sustainable development is:

*Development that meets the needs of the present without compromising the ability of future generations to meet their own needs.*

### 1990 – Australian Government

In 1990 the Australian Government recognised that ecologically sustainable development (ESD) would present itself as one of the greatest challenges to the nation’s government, industry and society in coming years. The definition of ecologically sustainable development was suggested as:

*Using, conserving and enhancing the community’s resources so that ecological processes, on which life depends, are maintained, and the total quality of life, now and in the future, can be increased.*

The goal for ESD was identified as being:

*Development that improves the total quality of life, both now and in the future, in a way that maintains the ecological processes on which life depends.*

### 2003 – Western Australian Government

In 2003, the Western Australian (WA) Government launched its Sustainability Strategy (WA Sustainability Strategy). This strategy focused on the need to simultaneously integrate a consideration of the environment, the economy and society into decision-making.

In the WA Sustainability Strategy, sustainability is defined as:

*Meeting the needs of current and future generations through an integration of environmental protection, social advancement and economic prosperity.*

The WA Sustainability Strategy provides a framework that enables private enterprise, the community and government to maximise the benefits in all three areas.





## 2004 - Western Australian Local Government Act 1995

In 2004 the *Local Government Act 1995* was amended to deal with a range of matters including provisions to incorporate the sustainability themes into the content and intent of legislation. Section 1.3 (3) of the Act now states that:

*In carrying out its functions a local government is to use its best endeavours to meet the needs of current and future generations through an integration of environmental protection, social advancement and economic prosperity.*

## 2005 – Planning and Development Act 2005

The *Planning and Development Act 2005* introduced a specific purpose of the Act regarding sustainability. Section 3 (1) (c) states:

*To promote the sustainable use and development of land in the State.*

The emphasis on sustainability within the principal legislation governing planning practice in WA is an important reflection of the role for promoting sustainable development through planning.

## 2006 – City of Cockburn Adopts Definition of Sustainability

In 2006, the City of Cockburn adopted its first definition of sustainability and in 2011 this was expanded to include governance. Sustainability in Cockburn is defined as:

*Pursuing governance excellence to meet the needs of current and future generations through an integration of environmental protection, social advancement and economic prosperity.*



## STRATEGIC ALIGNMENT

Sustainability affects a number of the City of Cockburn's strategic planning documents, which guide the City's development into the future. The primary strategic documents are the Strategic Community Plan 2012 – 2022 and Long Term Financial Plan.

The City's Strategic Community Plan provides the vision for where the City wishes to be in 2022. It includes seven key themes of focus for Cockburn, which are each accompanied by a vision and associated strategic actions. The Long Term Financial Plan 2012/13 – 2021/22 is the City's need's based infrastructure plan designed to deliver major capital works in a timely and financially viable manner.

The Annual Business Plan provides the operational link between the City's Strategic Community Plan and Long Term Financial Plan with the Annual Budget ensuring that adequate funds are available each year to complete specific projects. The City reports its achievements and provides a range of information to its residents in its Annual Report.

Some of the City's other key strategic documents include:

- Corporate Governance Charter
- Disability Access and Inclusion Plan 2012 – 2017
- Community Development Strategic Plan 2011 – 2014
- Reconciliation Action Plan 2011 – 2013
- Youth Services Strategic Plan 2011 – 2016
- Greenhouse Gas Emission Reduction Strategy 2011 – 2020
- Local Water Action Plan 2011 – 2017



## PURPOSE

This strategy acknowledges that sustainable development in the City of Cockburn is a continuous and long-term process, which will guide the City's planning into the future.

The purpose of this strategy is to:

1. Create a culture of sustainability within the organisation and the community, with an emphasis on integrating social, economic and environmental considerations into decision making for the delivery of outcomes,
2. Act as an 'informing strategy' to the City's integrated planning framework,
3. Outline the City's key sustainability objectives; and
4. Establish the most appropriate reporting structure for inclusion in the City's State of Sustainably Report.

The City manages, measures and reports its governance, economic, environmental and social outcomes in a consistent manner through the TBL+1 framework.

This strategy contains eight key focus areas, which each contain four overarching objectives. The focus areas are closely aligned with the seven key themes in the Strategic Community Plan while the overarching objectives are aligned with Policy SC37.

This strategy will also help with the identification, coordination and realisation of key sustainability objectives located in the City's various strategies and plans.

# SUSTAINABILITY THEMES, FOCUS AREAS AND OVERARCHING OBJECTIVES

The focus areas and overarching objectives have been reviewed by the City's Executive team and Strategic Business Management Group (SBMG) and are as follows:



## GOVERNANCE

### Management, Accountability, Transparency and Engagement

- Gov 1. Facilitate employee retention through strategies, processes and training.
- Gov 2. Adopt best practice in sustainable procurement and asset management.
- Gov 3. Ensure sustainability forms an integral part of corporate strategic planning.
- Gov 4. Maintain best practice public service delivery for the City's key stakeholders.

### Sustainable Planning and Development

- Gov 5. Support increased walking, cycling and public transport use through the development of neighbourhoods with mixed housing types and densities.
- Gov 6. Support local employment, service provision and leisure opportunities through the delivery of mixed use neighbourhoods.
- Gov 7. Facilitate opportunities for sustainable transport.
- Gov 8. Strategically plan for the sustainable long-term retention of significant natural areas.



## ENVIRONMENT

### Environmental Management

- Env 1. Maintain, conserve and enhance ecosystems for present and future generations.
- Env 2. Establish and enhance ecological corridors.
- Env 3. Identify, manage and minimise risks to human health.
- Env 4. Promote stewardship of the natural environment.

### Efficient Settlements and Use of Resources

- Env 5. Implement best practice water management strategies.
- Env 6. Increase the use of renewable energy.
- Env 7. Reduce greenhouse gas emissions.
- Env 8. Reduce waste and increase recycling.





## SOCIETY

### **Sense of Place and Healthy Communities**

- Soc 1. Enhance social inclusion, equity and diversity.
- Soc 2. Provide well located community services and facilities to meet identified community needs and facilitate healthy lifestyles.
- Soc 3. Develop safe communities.
- Soc 4. Protect and promote the City's cultural heritage and diversity.

### **Community Involvement**

- Soc 5. Encourage community involvement in local events and activities.
- Soc 6. Develop a strategic approach to community engagement.
- Soc 7. Enhance a sense of community ownership and promote Cockburn as an attractive place to live, work and visit.
- Soc 8. Build community resilience through strengthened community relationships and linkages.



## ECONOMY

### **Economic Development**

- Eco 1. Implement a strategic approach to economic development.
- Eco 2. Attract business investment through the facilitation of strategic infrastructure.
- Eco 3. Facilitate the development of local enterprise.
- Eco 4. Grow tourism in Cockburn through the management, improvement and promotion of the City's key natural, cultural and commercial features.

### **Employment Opportunities**

- Eco 5. Support Cockburn residents accessing local high value jobs through targeted programs of training and development.
- Eco 6. Maintain the City's Employee Development Program.
- Eco 7. Develop and promote strategic partnerships with stakeholders involved in the industrial/commercial precincts.
- Eco 8. Promote the small business sector in Cockburn through key strategic alliances.



## IMPLEMENTATION

The City implements the sustainability strategy through the following process:

Annual review of key performance indicators relevant to each overarching objective.

Ensure the indicators are based upon SMART principles and aligned to the City's existing strategic planning mechanisms.

Monitor and annually report on progress in the City's State of Sustainability Report.

Include a summary of performance in the City's Annual Report.



## METHOD OF SUSTAINABILITY REPORTING

This strategy, together with the City's sustainability policy and action plan, culminates in an annual *State of Sustainability Report*. In December 2011 the City became the first local government in Western Australia to produce a specific SoS. This strategy builds on the City's commitment to realise its TBL+1 objectives in Policy SC37 – Sustainability, by developing a comprehensive and integrated sustainability reporting system.

There are few reporting guidelines to assist local governments to undertake sustainability reporting. Some local governments report on some of the Global Reporting Initiatives (GRI) in their Annual Reports. The GRI is an internationally recognised standard that sets out the principles and performance indicators that organisations can use to measure and report on their economic, environmental, and social performance.

The direct relevance of GRIs to local government is somewhat limited at the present time and consequently, the take up of GRI reporting among local governments throughout Australia is relatively small. The City has modelled its KPIs on SMART principles, which are outlined below.

**SMART** principles are specific, measurable, achievable, realistic and timely.

- **Specific** – ensure that the objectives are clear and well defined;
- **Measurable** – know if the objective is obtainable and when it has been achieved;
- **Achievable** – outline a practical path to achievement;
- **Realistic** – within the availability of resources and knowledge; and
- **Timely** – provide enough time to achieve the goal but not too much to affect performance.

This strategy enables the City to develop its own TBL+1 indicators with the option of including GRI indicators in the future should it wish to. As such the reporting structure will be modelled on the GRI guidelines, but will be fully aligned with Policy SC37, and the Sustainability Action Plan.

As stated above, Policy SC37 outlines the broad sustainability objectives. The SBMG reviews and updates the Sustainability Action Plan in March each year. At the review the annual sustainability objectives are set, responsibility assigned for realising those objectives along with the timeframe for completion, and the KPI's for measuring progress.

The Sustainability Action Plan is therefore also structured along the four TBL+1 themes and the annual sustainability objectives and KPI's are detailed under the focus area headings. This strategy therefore recommends that the KPI numbers in the sustainability action plan follow those listed above.

The same numbering format is used in the City's State of Sustainability Report, which enables ease of identification and ensures consistency in reporting.



## SUSTAINABILITY IN COCKBURN

For more information on the City's programs for sustainability, please visit

**[www.cockburn.wa.gov.au/sustainability](http://www.cockburn.wa.gov.au/sustainability)**

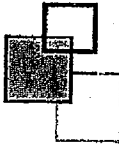


## City of Cockburn

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## *South Beach Structure Plan Report*

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September 2002 702.164



THE PLANNING GROUP





## 6.0 TRAFFIC AND TRANSPORTATION CONSIDERATIONS

### 6.1 Introduction

This section of the report relates to traffic and transport planning for South Beach and presents information and recommendations for:

- Bus Service Planning;
- Pedestrian/Cycle Facilities Planning;
- Regional Road Planning;
- Site Access; and
- Local Traffic Conditions: Existing and Post Development.

### 6.2 Bus Services

#### 6.2.1 Existing Services

Several bus routes operate along Cockburn Road/Hampton Road to and from Fremantle City Centre via South Street and South Terrace (refer *Plan 4*). Service frequency is very good at approximately every 10 minutes during weekday peak periods and approximately 20-30 minutes at other times. The high frequency is due to the number of bus routes involved (ie. 126, 135, 136, 138, 510, and 920). Existing bus stops with shelters are located near the following intersections:

- Cockburn Road/Boyd Crescent;
- Hampton Road/Clontarf Road; and
- Hampton Road/Douro Road.

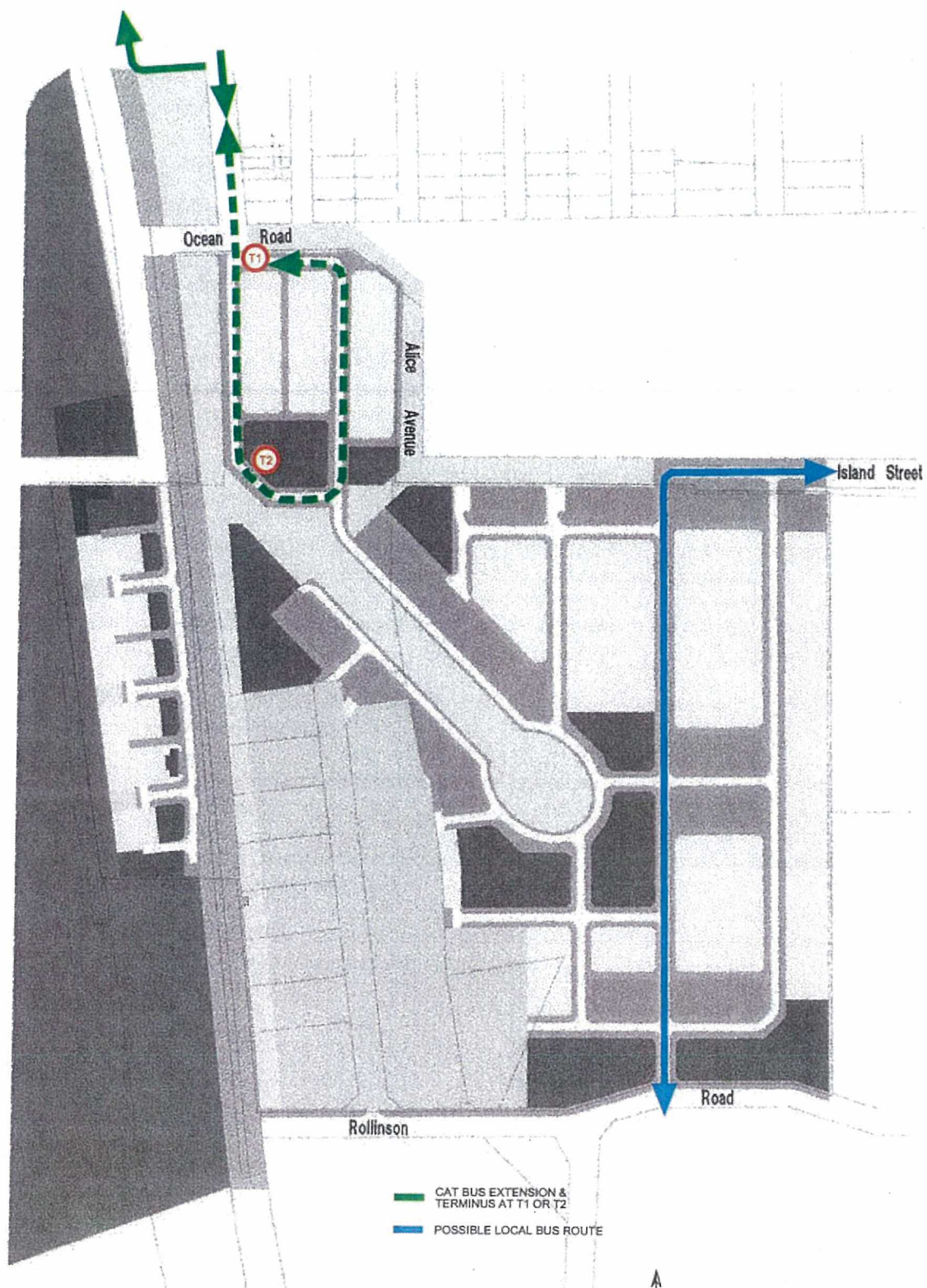
Rockingham Road and Hampton Road form part of the evolving Fremantle-Rockingham Transitway. The Hampton Road 'bus' lanes are a recent development that forms part of the Transperth planning for this high frequency public transport service.

Route 141 (Coolbellup-Fremantle) operates along Douro Road/South Terrace. The bus stops for this route are located near Douro Road/Chester Street intersection. This service has a 30-minute frequency on weekdays and 60 minutes outside of working hours and on weekends.

The Fremantle Central Area Transit System (CAT) operates in a one-way clockwise direction down South Terrace to Douro Road and then north up Marine Terrace. The CAT bus stop closest to South Beach is at the corner of Marine Terrace and Douro Road. Service frequency is every 10 minutes and travel time to Fremantle Station is approximately 7 minutes. The complete roundtrip ('figure 8' route taking in Ord Street and James Street loop) takes 30 minutes.

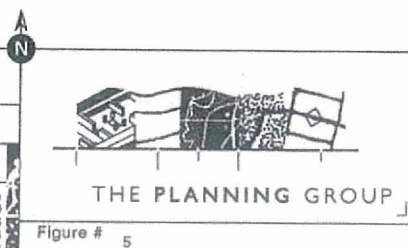






— CAT BUS EXTENSION & TERMINUS AT T1 OR T2  
— POSSIBLE LOCAL BUS ROUTE

TITLE: <b>Possible Bus Services</b>	
Date Sep 2002	E. Ref Figure 5
Scale NTS	Job # 702.164
152-156 St George's Terrace Perth WA 6000 Tel +61 08 9322 1318 Fax +61 08 9321 4786 perth@planninggroup.com.au	





## South Beach Structure Plan Report

The CAT is being funded 50/50 by the City of Fremantle and Transperth under a 3-year agreement that ends in August 2003. Given the early popularity and success of the CAT, it is hoped that a new agreement for funding (shared or wholly by Fremantle) will be arranged and the CAT will continue to operate beyond August 2003.

### 6.2.2 Possible New Services

The South Beach development structure plan has been designed to cater for possible bus service additions as indicated in *Plan 5* and described below:

- CAT bus service extension along South Terrace to either Ocean Road or Island Street within the South Beach Development; and
- New north-south local bus route into and through the site, possibly using Bennett Ave. and Island Street.

A preliminary analysis of the CAT service extension options is summarised in Table 6 below.

Table 6 - CAT Route Extension: Options and Costs

Route Description	Extend south to Ocean Road	Extend south to Island Street
Number of Buses and resulting headway	3 buses at 10.7 minutes headway	4 buses at 8.8 minutes headway
Extra Bus Travel Distance	560 metres	1060 metres
Extra Capital Cost	\$0 (no additional bus required)	\$400,000 for new CAT bus
Extra Operating Cost (based on \$2.50/km)	\$32,000*	\$230,000*

\*The costs for a new CAT bus and the cost for extending the operation of the CAT were provided by Transperth. Table 6 cost information does not allow for bus depot overhead charges that may accrue to a route extension nor does it include construction of a turnaround facility/stop at the southern end within the South Beach Development.

The CAT currently operates with 3 buses at 10 minute average headways. As shown in Table 6, extending the CAT as far south as Island Street will require an additional CAT bus because otherwise the headway will increase to 12 minutes (unacceptable for this type of service).

The possible new local bus route shown in *Plan 5* will most likely only be viable if Bennett Avenue connected through to Cockburn Road and the existing industrial area south of Rollinson Road some day converted to an urban zoning. With residential and commercial uses adjacent to Bennett Avenue and with a link through the South Beach development to Island Street, this could provide a very useful local bus route to Fremantle.

## **8.0 COMMUNITY AND STAKEHOLDER CONSULTATION**

Community and stakeholder input into the structure planning process for South Beach was identified as vital at the outset of the project. The community and stakeholder consultation for South Beach conducted to date, forms the first phase of the overall consultation for this project. The objective of this phase was to prepare a structure plan based on the synthesis of ideas generated from feedback from the community. The second phase will occur during the advertising of this structure plan.

The extensive public and stakeholder consultation culminated in a community design workshop, which was held from the 16th-21st of March 2002 at the administration office of the old Wesfarmers administration building at South Beach. The workshop was attended by stakeholders including landowners, state and local government agencies and members of the community and were advised and directed by the consultant team to achieve practical design solutions focussing on desirable objectives and overcoming potential conflicts.

The main objectives of the workshop were to understand the stakeholders' aspirations, priorities and values in creating a community-driven vision of South Beach, the outcome being a structure plan.

A final concept plan was prepared at the workshop (refer *Plan 12*). The main difference between the resulting structure plan (see Section 10) and the final concept plan is that Island Street has been redirected down its existing reserve. This has been done to fulfil the landowners objective of solar efficiency, as the concept plan cuts the area into four triangles, making it impossible to achieve a high percentage of solar efficient lots.

Detailed information on the outcomes of the design workshop can be obtained from the Community Consultation Report prepared by The Planning Group (May 2002).

The main issues raised and discussed during the workshop are listed below:

### **8.1 Transport and Access**

- Ensure appropriate street network planning, including consideration of an east-west access;
- Traffic calming measures to discourage traffic speeds and volumes;
- Discourage the development area's use for through traffic;
- Restrict non-local parking;
- Promote safe pedestrian and cycle access;
- The railway should be given consideration for use as a passenger rail; and
- Provide accessible public transport.

## **9.0 STRUCTURE PLAN CONTEXT**

The issues arising from the design workshop were reviewed in light of current initiatives relating to urban design, statutory planning, environmental planning, transport, service and infrastructure and community development planning.

Listed below are the outcomes of a detailed consideration of the issues, which directly influenced the recommendations within the South Beach structure plan (discussed in section 10 of this report).

### **9.1 Transport and Access**

The transport and access components of the structure plan focus on the following elements as raised by the community. These issues relate to the South Beach development and adjacent South Fremantle and North Coogee areas:

- Bus services;
- Pedestrian and cycle facilities;
- Regional Road Planning; and
- Local traffic conditions and site access possibilities.

Key recommendations include:

#### **9.1.1 Public Transport Services**

The Fremantle CAT service should be extended to Ocean Road as early as possible. Traffic calming on South Terrace (south of Douro Road) will need to be changed to accommodate vehicles and public transport. Extending the CAT service further south to the Island Street reserve will require an additional bus and increase annual operating costs but will provide improved service levels along the whole CAT route.

#### **9.1.2 Pedestrian/Cycle Facilities**

Shared paths and footpaths are to be constructed by the developer, City of Cockburn and City of Fremantle in order to provide good access to the existing bus services at the perimeter of the site. The City of Fremantle should fast track the upgrading of slab footpaths along the existing residential streets to the north of the site (ie South Terrace, Hickory, Walker, Hulbert, Thomas, and Daly) as these streets will cater for increased pedestrian and cycle traffic due to the South Beach development. The City of Fremantle should also upgrade the footpath on the north side of Douro Road to a shared path of 2.0m minimum width as this is necessary to provide a safe off-road cycling alternative along Douro Road.

The City of Cockburn and Main Roads Western Australia should extend and widen the path on the western side of Cockburn Road southward to Rollinson Road. This extension is important due to the current poor cycling environment along Cockburn Road and a lack of footpaths.



## 10.0 SOUTH BEACH STRUCTURE PLAN

### 10.1 Design Philosophy

The philosophy behind the South Beach structure plan (refer *Plan 1.3*) is based on several broad design principles, as outlined below:

#### Accessibility

The development area will feature an efficient movement network with good circulation and accessibility and will be well integrated with public transport facilities. Pedestrian accessibility is also vital, and good linkages are provided within the development and to the beach.

#### Social Viability

The structure plan demonstrates that the development area can be developed in such a way as to create a vibrant and self-sustaining neighbourhood community. South Beach is intended as a village-style development with an appropriate form, scale and character which will encourage community interaction and ownership.

#### Surrounding Context

Whilst the development will be self-sustaining, it will also have strong relationships with its surrounding context. The structure plan shows the relevance of linkages and interface with surrounding areas and uses, including:

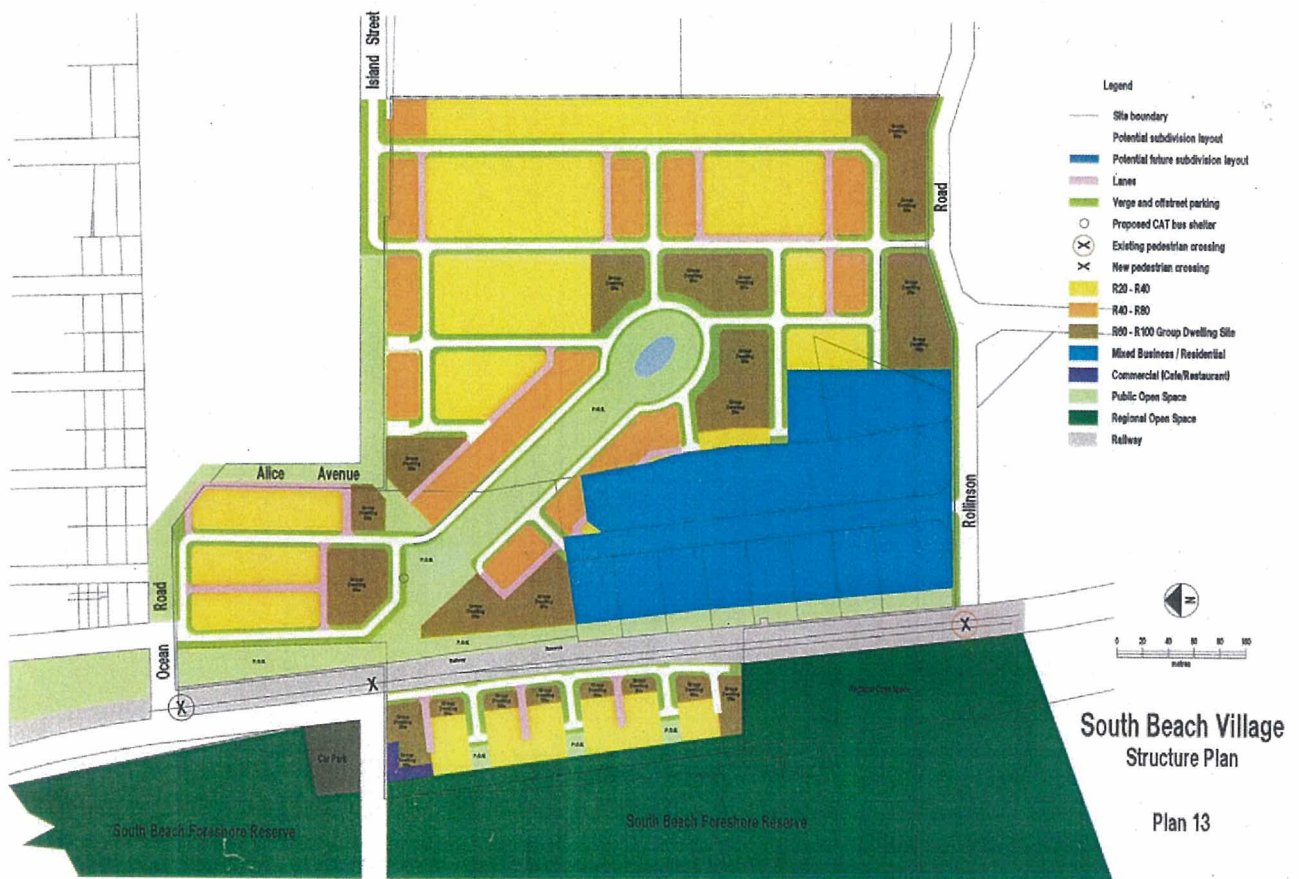
- Road and pedestrian links;
- Public transport;
- Visual Interface; and
- Environmental links.

#### Sustainability

The proponents' intention for the development is to create an urban village with a strong sense of place that is based on principles of social, economic and environmental sustainability.

Their aim is to create a practical example of a medium density urban settlement in the Perth metropolitan area that addresses many of the problematic social and environmental issues often associated with conventional urban development, and works toward solving them.

The development will focus on the conservation of water, energy and resources, the preservation and re-vegetation of the natural landscape and the creation of a cohesive sense of place through community design.



## South Beach Structure Plan Report

Residential lots will be subjected to architectural and landscaping standards to ensure both environmental and aesthetic objectives are met. These will include requirements such as solar passive design principles, the use of appropriate water and energy saving technology, and restrictions on the planting of invasive exotic species.

The development objectives are:

- To acknowledge the existing local community and to encourage an interactive relationship that builds a proactive involvement enhancing the design process,
- To acknowledge and enhance existing community facilities including beach access, surrounding public open space, and historic equestrian uses,
- For the design to reflect the highest and best use of urban standards with a strong emphasis on unity and diversity i.e. the whole development will have a strong overall theme whilst allowing for differing interpretations of the theme. Design guidelines should include strict height and scale restrictions that acknowledge and complement the architectural heritage of Fremantle and the coastal location,
- To protect the existing freight line that runs through the site, and accommodate it aesthetically into the overall design, ensuring that future residents are also protected from the environmental impact of the line,
- For the design to acknowledge the importance of accommodating all age groups and to include some opportunities for affordable housing whilst developing a physical environment that fulfils psychological needs such as security, community and identity,
- To incorporate energy efficient features into the overall structure plan, firstly, by providing good solar access to all lots by means of street alignment and lot orientation, and secondly, by setting building envelopes and solar passive architectural design guidelines for all homes (single and group). To assist and encourage all future residents to take advantage of State and Federal renewable energy grants,
- To make the best use of open space, including elements that encourage social interaction and healthy lifestyles that contribute to the on-going health and wellbeing of the community. Plantings will include the appropriate indigenous flora, productive and traditional coastal species and where appropriate deciduous trees to assist with solar access in winter,
- To enhance pedestrian and bicycle usage by minimising the overall impact of motorized traffic and linking green corridors and open space with pedestrian and cycling paths that also connect to the local corridors identified in the Fremantle Green Plan. To work with the City of Fremantle towards the extension of the existing public transport link from the City to the South Beach Village, and
- To provide for the efficient management of water resources by developing an integrated urban water management plan that will include; landscaping guidelines for public and private open space, the efficient use and management of rain and storm water within the settlement and water conserving, recycling technology throughout the development.

### 10.2 Design Objectives

To achieve the above mentioned design principles, there are a number of objectives that must be met. These are to:

- Include the principles of the Liveable Neighbourhoods code;
- Design and develop a quality residential subdivision offering competitively priced, well planned home sites to meet the needs of the South Fremantle market;
- Provide sustainable community and recreation opportunities which meet a high level of public amenity and community need;
- Offer a variety of lot sizes in a solar efficient orientation;
- Reserve and rehabilitate the existing dunal vegetation.
- Create a distinct road hierarchy offering interesting and engaging vistas, permeability and structure; and
- Provide opportunities for pedestrian and cycle networks located on both the local road network and within open space reserves.

### 10.3 Land Uses

The South Beach development proposes a mixture of land uses including residential, commercial and open space as shown in Table 8 below:

Table 8 - Land Uses

Land Use	Area	%
Single Lots (residential)	73,530m <sup>2</sup>	33.3
Group Dwelling Sites	30,089m <sup>2</sup>	13.6
Mixed Business	40,500m <sup>2</sup>	18.3
Public Open Space	23,650m <sup>2</sup>	10.7
Roads	53,325m <sup>2</sup>	24.1
Total	221,094m <sup>2</sup>	100

#### 10.3.1 Single Lots

The 300 single residential lots within South Beach have been planned to an urban density ranging from R20 to R80 and adopt the design philosophy that permits and encourages permeability and linkages to open space areas.



### 10.3.2 Grouped Dwelling Sites

There are 22 grouped dwelling sites planned at densities between R60 and R100 and ranging from 324m<sup>2</sup> to 2942m<sup>2</sup>. These sites are located to frame views and mark key focal points within the development.

### 10.4 Public Open Space

The public open space and landscaping within the South Beach development will provide a series of spaces that function as a linear park system creating strong open space links through the development. The landscape will not just be a visual amenity, but an integral part of the drainage system utilising surface water for passive irrigation and potentially also recycled "grey water". Species selection will be appropriate to the coastal location and will range from locally indigenous dunal and coastal heath to selected exotics in locations remote from the foreshore.

In addition to the open space network, the streetscapes will play an important part in defining the character of the place. It is not intended to have a very formal street planting regime across the development. Street trees will be positioned to create shade, reduce wind velocities and be part of a solar access management strategy. This entails that in some circumstances street trees will not be present on both sides of a street. Tree species will be selected and positioned to provide the best shade in summer and light access in winter. In selected locations the use of deciduous species will be appropriate and these will be selected to ensure they do not seed into or compromise indigenous environments. In addition, it is intended to supply new residents and land purchasers with guidance information regarding planting related to buildings to meet enhanced environmental standards. This will include information on waterwise gardening, species and placement.

The public open space layout compliments the existing parks and reserves, extending them into the new community. New parks will be designed to provide character references back to existing but will also provide opportunities to create places that are distinctive and representative of the new residential area.

The foreshore will be an integral part of the emerging community and is considered an educational resource and local amenity, as well as being an environmental asset. It is intended to provide full environmental interpretation facilities, which will include managed access, signage, information boards and planting to extend the coastal heath over that part of the industrial land that is transferred to be regional reserve.

The approach will be to ensure the environmental integrity is secure while providing sufficient interpretive material and accessibility to encourage the community to value and respect the area.

The general character of the new landscape will take its references from the existing South Fremantle locality. Indigenous species will form the foreshore areas. The core parklands will utilise coastal species as the structural elements and may also include accent specimens such as Norfolk Island pines. It is the intention to create spaces and places that are relaxed, informal and low in maintenance.



### 10.5 Roads

The main focus of the road network design is the ease of movement within the site and connections and accessibility to the foreshore. The widths of the road reserves range from 6m (laneways) to 20m, depending on their designation and function.

Good vehicular access is provided via Cockburn Road and South Terrace to all local roads in the area. These regional roads provide good connections to the vast majority of local and regional employment, recreation and education destinations.

### 10.6 Services

The South Beach development will provide all normal urban services being reticulated water, sewerage and gas, comprehensive stormwater, underground power and telecommunications. Existing sewer infrastructure to the south of the site will be utilised, whereas existing water infrastructure along Cockburn Road will be extended into the site. Western Power, Telstra and Alinta Gas service networks already exist within the site.

### 10.7 Performance of the Structure Plan

The structure plan incorporates the latest policy directions from the Department for Planning and Infrastructure and in doing so, provides a land product that will be compatible with the surrounding area. Above all, it will provide a range of interesting housing lots, well located in terms of accessibility, linkages and views. It is considered that these refinements of the plan will result in a more attractive and efficient village design.

## Red CAT Timetable

### Monday to Thursday

7:30am - 6:30pm **Every 15 minutes**

The last trip departs from Stop 1 (Maritime Museum & E-Shed Markets) at 6:15pm

### Friday

7:30am - 8:00pm **Every 15 minutes**

The last trip departs from Stop 1 (Maritime Museum & E-Shed Markets) at 7:45pm

### Saturday, Sunday and Public Holidays\*

10:00am - 6:30pm **Every 15 minutes**

The last trip departs from Stop 1 (Maritime Museum & E-Shed Markets) at 6:15pm

\* (except Good Friday, Christmas Day and Boxing Day)

## Blue CAT Timetable

### Monday to Thursday

7:30am - 6:30pm **Every 10 minutes**

The last trip departs from Stop 1 (Fremantle Station) at 6:20pm, terminating at Stop 10 (Orient Street).

### Friday

7:30am - 8:00pm **Every 10 minutes**

The last trip departs from Stop 1 (Fremantle Station) at 7:50pm, terminating at Stop 10 (Orient Street).

### Saturday, Sunday and Public Holidays\*

10:00am - 6:30pm **Every 10 minutes**

The last trip departs from Stop 1 (Fremantle Station) at 6:20pm, terminating at Stop 10 (Orient Street).

\* (except Good Friday, Christmas Day and Boxing Day)

## Places of Interest

Cappuccino Strip  
Blue Stop 2 & 3

Fremantle Station  
Red Stop 3 & Blue Stop 1

Fremantle Hospital  
Blue Stop 4

Fremantle Prison  
Red Stop 6

Fremantle Arts Centre  
Red Stop 8

Fremantle Leisure Centre  
Red Stop 8

Fremantle Markets  
Blue Stop 3

Fremantle Oval  
Blue Stop 3

Maritime Museum and E-Shed Markets  
Red Stop 1

Notre Dame University  
Blue Stops 17 & 18, Red Stop 2

Shopping area  
Red Stop 4 & Blue Stop 2

The Esplanade Hotel  
Blue Stop 16

Town Hall  
Red Stop 4

For further information on the various sights and attractions in Fremantle, visit <http://www.freofocus.com> or visit the Fremantle Tourist and Information Centre Red Stop 4.

The Fremantle CAT is a free service brought to you by Transperth and the City of Fremantle



The Fremantle CAT Service is more than just a free ride around our historic port city. Enjoy a coffee, go to a meeting or do some shopping. Even drop into the fabulous Maritime Museum - our CAT buses go there too.

The Blue CAT services depart every 10 minutes and the Red CAT services depart every 15 minutes, so you'll never have to wait long to get where you want to go, quickly and comfortably. Best of all, you can do all of this without any driving or parking hassles.

Train services to and from Fremantle depart around every 15 minutes during the day, so why not leave your car at home, then hop on a CAT outside Fremantle Station? You can also connect easily with other Transperth bus services too.

All Fremantle CAT buses are modern and high-tech complying with stringent EURO2 emission standards so you will be helping the environment as well as enjoying a great day out.

All of our CAT buses are easily accessible, with kneeling actions, a driver activated entry ramp and low floor. To board, simply wait at any CAT stop (just look for the CAT logo) and there will be one along in no time.

It's the smartest way to get around Fremantle, FREE.

### Want to know more about Transperth?

InfoLine	13 62 13
CommentLine	13 16 08
People with hearing disabilities TTY	9428 1999
Translating and Interpreting Service TIS	13 14 50
MobileWeb	136213.mobi
Website	<a href="http://www.transperth.com.au">www.transperth.com.au</a>
SMS	SMS your stop number to 13 62 13 for your next 5 trips

Get timetables by email, register with TravelEasy at [www.transperth.com.au](http://www.transperth.com.au)



CAT  
TIMETABLE **204**  
**FREMANTLE**  
Effective 26/06/2011



Accessible Service

**Transperth**

Effective 26/06/2011



**Blue CAT**

- 1 Fremantle Station
- 2 Cappuccino Strip
- 3 Fremantle Markets
- 4 Fremantle Hospital
- 5 Wray Ave
- 6 Price Steet
- 7 Gold Street
- 8 Charles Street
- 9 Wardie Street
- 10 Orient Street
- 11 Douro Road
- 12 Fremantle Sailing Club
- 13 Capo D'Orlando Drive
- 14 South Street
- 15 Howard Street
- 16 Esplanade Hotel
- 17 Fishing Boat Harbour
- 18 Victoria Quay

**Red CAT**

- 1 Maritime Museum & E-Shed Markets
- 2 Victoria Quay
- 3 Fremantle Station
- 4 Queen Street Shopping Precinct
- 5 Kings Square
- 6 CBC Fremantle
- 7 Fremantle Park
- 8 Fremantle Arts & Leisure Centres
- 9 Beach Street
- 10 Parry Street
- 11 Woolstores Shopping Centre
- 12 Fremantle Station

**Legend**

- Red CAT
- Blue CAT
- 6 15 CAT Bus Stop
- Road - one way
- Fremantle Station
- Railway
- Tourist Information
- Commercial Precinct
- Post Office
- Sporting Venue
- Hospital
- Police
- CHC
- Police Station
- Child Health Centre